

# LANDSCAPE GUIDELINES

## Introduction

The Landscape Guidelines section is organized into two general components each addressing the range of applicable unique situations and typical landscape elements. Its intent is to outline the desired method for design and installation of landscape, paving, lighting, street furniture, signage, and utilities. This ensures that the Core Sub-Area will receive consistent quality and quantity; coordinated styles; and an integrated appearance.

The Landscape Guidelines refer to applicable ordinances and codes and are not intended to supersede City ordinances and codes. They are to meet or exceed the Conditions, Policies and Development and Design Guidelines established in the *Master Project Plan*.

All landscape must meet the standards established in the *City of San Diego's Landscape Technical Manual, Standard Specifications for Public Works Construction, and the San Diego Regional Standard Drawings*.

The following Landscape Guidelines are presented in this document and are divided into two categories: **Standard Practices and Character Districts**.

**Standard Practices** are the minimal standards that must be attained throughout the entire Sub-Area. Their intent is to allow simple, uncomplicated forms that have a timeless nature; quality installation that will reduce long term maintenance; and, responsible practices that promote sustainability. Standard Practices are guidelines for the design and construction of the typical streetscape and common area improvements, including the following elements:

- Street Trees and Vegetation
- Pedestrian pavement
- Lighting
- Street Furniture (Seating, waste receptacles, bicycle racks and newspaper dispensers)
- Fencing and Site Walls
- Utilities
- Signage

**Character Districts** guidelines expand upon the Standard Practices by describing details that are particular to one specific district within the sub-area. They contain an introduction to the special situations (streetscapes, medians, open spaces and plazas, and alley/service access) found in that district. These are followed by guidelines for typical site elements (such as street trees, vegetation, pedestrian paving, lighting, street furniture, fencing and site walls) which require

specific responses based upon location. Character District guidelines are for the:

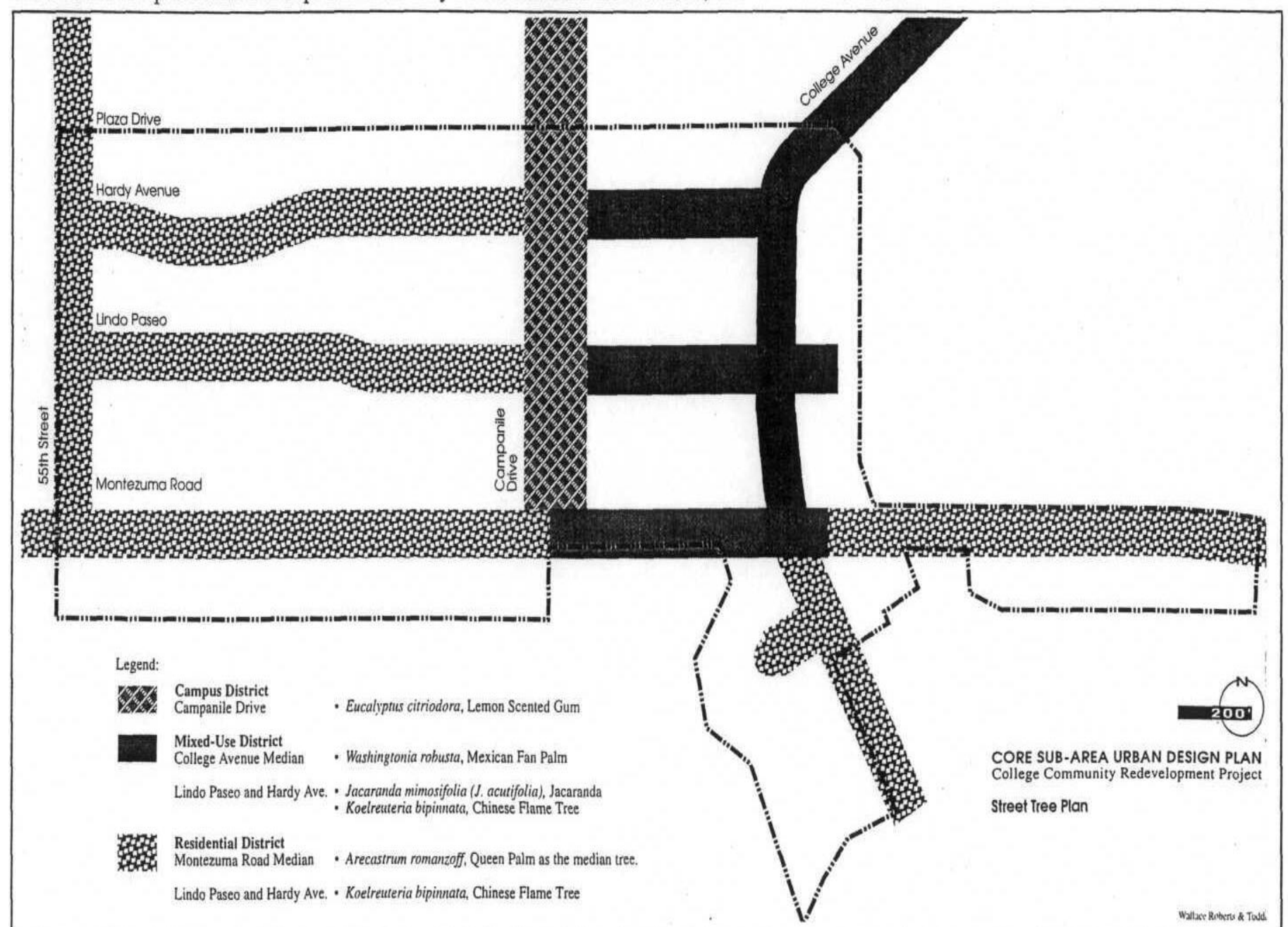
- Campus District (Campanile Drive)
- Mixed-Use District
- Residential District

## Standard Practices

The following guidelines are applicable to the whole of the Core Sub-Area.

## Street Trees and Vegetation Standards

Control of the street trees and other planted vegetation in the right-of-way ensures that the plant material is planted correctly for its situation and location, and



is able to be maintained in the proper manner.

- ‡ The size of street trees at the time of planting shall be a minimum of 2 inches in caliper, with a clear zone (between the top of pavement and bottom limb) of 8 feet above the sidewalk and 13 feet 6 inches above the street.
- ‡ Street trees shall be planted within the public right-of-way between the curb and the property line that abuts the property. The required quantity of trees should be at the rate of one 24" box for every 30 linear feet of property line that abuts the public right-of-way.
- ‡ All street trees in the right-of-way must be approved by the City's Urban Forester.
- ‡ Palm trees are not to be planted beneath overhead wires or within ten feet of existing wires.
- ‡ Provide 40 square feet of water and air permeable landscape area at the base of each street tree. This area shall not have an impervious surface, it shall be either a tree grate (in highly used pedestrian areas) or planted with shrubs and mulch (in low use areas).
- Soil testing should be conducted in the site development stages to determine if subsurface drainage and aeration systems are required to maintain the growth of street trees.
- Where possible, plant street trees in continuous planting trenches to provide maximum root zone.
- ‡ Tree grates shall have a minimum 12 inch diameter opening for the tree and allow for the removal of sections for the growth of the tree.
- ‡ Tree grates, whether prefabricated or one-of-a-kind, shall not have openings greater than 1/4 inch or to meet current ADA code.
- ‡ The space between the finish grade of the tree and the tree grate shall be filled with gravel larger than 1/4 inch to limit the accumulation of debris under the grate while still allowing air penetration.
- ‡ Parking structures open to the sky on the highest floor shall provide landscape area and plant materials per the City Wide Landscape Regulations for grade parking areas. Landscape area and points may be substituted with architectural overhead trellis structures that cover 30% of the parking surface open to the sky.
- ‡ Groundcover shall be drought tolerant.
- ‡ Automatic irrigation must be provided within all planted areas in the public right of way.
- Deep well watering, to ensure deep root development, should be utilized on all trees within the public right of way. This will reduce the possibility of sidewalk heaving by roots.

## **Fencing and Site Walls Standards**

The forms and materials of the fences and/or site walls should compliment the property's architecture. Chain link fencing is not appropriate.

- ‡ Fencing and/or site walls must not be taller than 6 feet.

## **Pedestrian Pavement Standards**

Paving is intended to be background, allowing the street life and the activity of the use to predominate. Patterns should be simple in nature and respond to their appropriate district. In all cases, the paving materials should be used in their primary forms, rather than as imitations of another material.

- ‡ Pedestrian pavement must meet the criteria of Title 24, and the Americans with Disabilities Act (ADA).
- ‡ All sidewalks are to have a minimum clear zone of 5 feet in horizontal width and 8 feet in vertical clearance.
- Crosswalks must meet the safety criteria established for public streets. Additional enhancements to the crosswalk may include the extension of the pedestrian pavement material across the street to define the crosswalk.
- Pavement materials shall be in their elemental form, such as concrete (plain, colored, scored or with aggregate), brick, tile or stone.
- Concrete stamped to resemble another material such as brick or stone should be prohibited. Interlocking unit pavers should be avoided. These tend to create complicated patterns which can be visually disturbing in large areas. Both present maneuvering difficulties for persons with disabilities.
- Pavement should not have beveled edges which can cause excessive bouncing for cart and wheelchair users.
- Sidewalks should be contiguous (attached to the curb) in the Campus and Mixed-Use Districts, and non-contiguous (a parkway planted strip between the curb and the sidewalk) in the Residential District.

## **Lighting Standards**

Lighting guidelines are intended to present a unifying scheme of lighting throughout the area, ensure that safety and security criteria are met, follow the City of San Diego's 'dark sky' ordinance and protect adjacent users from undesirable light spillage.

- ‡ Illumination should comply with the City of San Diego's 'dark sky' ordinance and use low pressure sodium ('yellow') fixtures for street lights or surface parking lights unless a variance is given where safety or aesthetic issues are of a concern.
- Illumination levels and lighting sources should be used that minimize points of glare while providing adequate levels of light for safety and security.
- If in compliance with the City of San Diego's ordinance, metal halide or high pressure sodium light sources should be used in pedestrian areas and parking garages for their white color of light that contributes to the comfort of users. Higher lighting levels should be in areas where potential conflict

between pedestrians and vehicles occur, such as crosswalks and parking areas. In pedestrian areas, lighting distribution should overlap at a height of approximately seven feet above the finish grade to allow visual recognition of pedestrians.

- Street light and parking luminaries should have cutoff shades so as not to intrude on residential uses.
- Parking structure lighting should be screened and not be visible from either streets or buildings surrounding the parking structure.
- To minimize sidewalk clutter, pedestrian and vehicular lights should utilize the same pole.
- The spacing of the combined fixtures shall be dictated by City illumination standards for roadways. Pedestrian-only lights shall be spaced to yield an average illumination of approximately 2 footcandles and a 4:1 average-to-minimum footcandle ratio.
- Light fixtures in the Campus District shall follow the lighting standards of the campus mall.
- Alleys should have pedestrian scale lights attached to the face of buildings or walls at a maximum height of 12 feet above the pavement.

### Street Furniture Standards

Seating surfaces are to be provided throughout the Core Sub-Area to promote congregation, enjoyment of the outdoor space, and enhance the pedestrian friendliness of the neighborhood.

- ‡ Seating surfaces should be between 14 inches and 18 inches above the pedestrian pavement. The depth of each seat should be between 14 inches and 18 inches. The length of each seat should be no more than 36 inches, unless divided by arm rests at a maximum of 36 inches on center.
- Seating should be located to provide visibility to the areas of activity on both the sidewalk and the street.
- At least half of the seating should have back supports. Back supports should conform to the human body and contribute to a relaxing place to sit.
- Seating should be designed in a way that prevents inappropriate use by skateboards, skates, or activities other than seating.

Bicycle Racks are encouraged to promote non-vehicular traffic and to provide secure areas for bicycle storage.

- Bicycle racks should be located in service alleys and parking garages, within view of the alley or garage entrance. Where possible, they should be secured for residential storage.
- Signage should be provided to direct bicyclists to the facilities.
- Bicycle racks should not be sited where they interfere with pedestrian movement.

Waste receptacles are encouraged in high traffic areas, all plazas, parks and

gathering areas and adjacent to any transit facility.

- Waste receptacles should not be sited where they interfere with pedestrian movement. They should be located in conjunction with building entries, outdoor smoking areas and seating areas.
- Waste receptacles should have ash urns attached wherever they are near building entries or outdoor smoking areas.

Newspaper dispensers are encouraged in high traffic areas, all plazas, and adjacent to any transit facility.

- Dispensers should not be sited where they interfere with pedestrian movement. They should be located in conjunction with other clusters of furniture.
- Consider incorporating kiosks in areas of medium to high pedestrian activity. Kiosks should be designed for use by people of all abilities.
- An 'Encroachment Removal Agreement' is required for all street furnishings in the public right-of-way.

### Utilities Standards

Utilities will be required for all new or revitalized projects within the Core Sub-Area. The intent of the guidelines is to assure that they are well placed to meet aesthetic, operational and functional criteria.

- Utilities should not be located above ground in the sidewalk. In no case shall a utility box limit the sidewalk clear-zone to less than 5 feet clear. Utility boxes should be integrated into the architectural design of the adjacent building. Hatches for below ground utilities should be designed to be "paved" to match the adjacent sidewalk.
- Utility hatches located in the sidewalks may become a public art opportunity for the community, including the University's Art Department, to uniquely mark the place. This shall be done in coordination with the regulating utility provider.
- Public telephones and/or emergency phones should be provided according to the recommendations of the campus and city police.

### Signage Standards

A complimentary system of signs should be developed to meet the functional and aesthetic needs of the Core Sub-Area. It is recommended that a signage design program be undertaken to finalize the theme and patterns of signs beyond these guidelines. Three types of sign palettes are identified, City of San Diego regulatory signs; University signs; and Community signs.

City of San Diego signage includes standard street identification signs, traffic safety and regulatory signs.

- ‡ Standards set by the regulatory agencies such as the City of San Diego and Caltrans should be strictly enforced.
- Directional signage should be provided for the future light rail station and the Transit Center.

University Signs are those signs which provide identification and direction to campus facilities and events, identification of campus buildings, or gateway monuments into the campus.

- ‡ The design of University signs shall follow the campus standard format.
- Location of these University signs should be carefully coordinated with the University and complement the community signage program.

Community signs include the privately owned and maintained signs for the variety of retail, commercial, religious, office and residential uses within the Core Sub-Area.

- A unifying theme should be developed for all community signs, with the MesoAmerican style encouraged. The theme should be compatible with the University signs, however different so that the two areas are distinguishable.
- To minimize clutter of the physical and visual streetscape, directional, street identification and regulatory signs should be designed to be clustered onto existing poles wherever possible. Identification signs should be attached to the building, adjacent to the main entry.
- All signs should have *white or light toned type* on a dark background.
- Background colors should be distinct from the University signage.
- Type styles should be *sans serif*, in a simple type style and large enough for optimal readability by pedestrians and motorists, as appropriate.
- ‡ Billboards and backlit, box signs are prohibited.
- ‡ Monument signs, or those that are over 24 square feet, shall not occur along Campanile Drive, where they would compete with campus gateway monuments.
- ‡ A variance from the monument sign size restrictions may be considered for a collaborative group of tenants/owners who process an approved sign plan in accordance with the procedures of the City of San Diego.
- An exception to the guideline for monument signs will be made for signs in the Religious Cluster that identify more than one religious organization.

## Character Districts

Within the Core Sub-Area, three "overlay" areas require specific guidelines in addition to the Standard Practices. The *Campus District* along Campanile Drive, the *Mixed-Use District* east of Campanile Drive and the *Residential District* all have particular criteria that expand upon the Standard Practices.

### Campus District

The Campus District is the public right of way of Campanile Drive, from Montezuma Road on the south to the University boundary on the north, including the setbacks of buildings to the east and west of the public right-of-way. The intent of this area is to extend the campus mall into the Core Sub-Area, thereby establishing the presence of the University towards Montezuma Road and setting up a transitional "gateway" into the campus environs. Views onto the campus should be open from all points along Campanile Drive to ensure this presence and reinforce the landmark. Vegetation should frame this long view onto the campus not shield or interrupt it. Materials in this district should follow the traditional materials of the campus, so that the transition is smooth and uninterrupted. The repetition and symmetry of the campus mall should establish the order of street furnishings, pedestrian scale improvements and planting. The character of this district should clearly relate to the University.

#### Campus District Streetscape:

Streetscape design on Campanile Drive should allow open views to the historic University towers that mark the north end of the campus.

#### Campus District Medians:

- Medians in the center of the drive should be of a simple drought tolerant ground cover planting to compliment the lawn panels aligned down Campanile into the campus. Trees should not be included because they will block the views along the Campanile.
- Grade the median to drain into center inlets connected to the storm drainage system.

Retaining walls associated with the ramps to the Transit Center/parking structures should be planted and irrigated with a single species of spreading, clinging vine, again relating to the lawn panels of the Campanile.

#### Campus District Open Spaces/Plazas:

The long open space of the campus mall and its extension down Campanile Drive should be the predominate open space in this area. Entry courtyards at buildings that face Campanile Drive should be secondary.

**Campus District Alleys and Service Access:**

Service alleys and bikeways run east-west through the Core Sub-Area and cross Campanile Drive. Service access should be provided from the roads perpendicular to Campanile Drive. Trash dumpsters should be screened from pedestrians' views from Campanile Drive with the use of site walls, screens or fences.

**Campus District Landscape Elements:**

The landscape elements along Campanile Drive should extend the plant palette and formal character of the university out to Montezuma Road. Project developers should closely coordinate with the University in the development of the materials which will appropriately meet the functional and aesthetic needs of the Campus District.

**Campus District Street Trees and Vegetation :**

- | Street trees and vegetation should not obstruct the north-south view along Campanile Drive between the intersection with Montezuma Road to the historic Bell Tower and the Hardy Memorial Tower of the University.
- Vegetation should be utilized to mitigate views of inarticulated and/or unattractive architecture or fencing.
- | The primary street tree to be planted along Campanile Drive should be *Eucalyptus citriodora*, Lemon Scented Gum. These trees should be spaced 30 (minimum) feet on center in a single row aligned with the periphery mall planting. Street trees should not be planted along the curb line along Campanile Drive but set back to the property line in order to ensure vistas down the mall. Additional vegetation planted in association with each new building entry should include a wide variety of flowering trees, shrubs and groundcover and shall not impede the views to the campus.

**Campus District Pedestrian Pavement:**

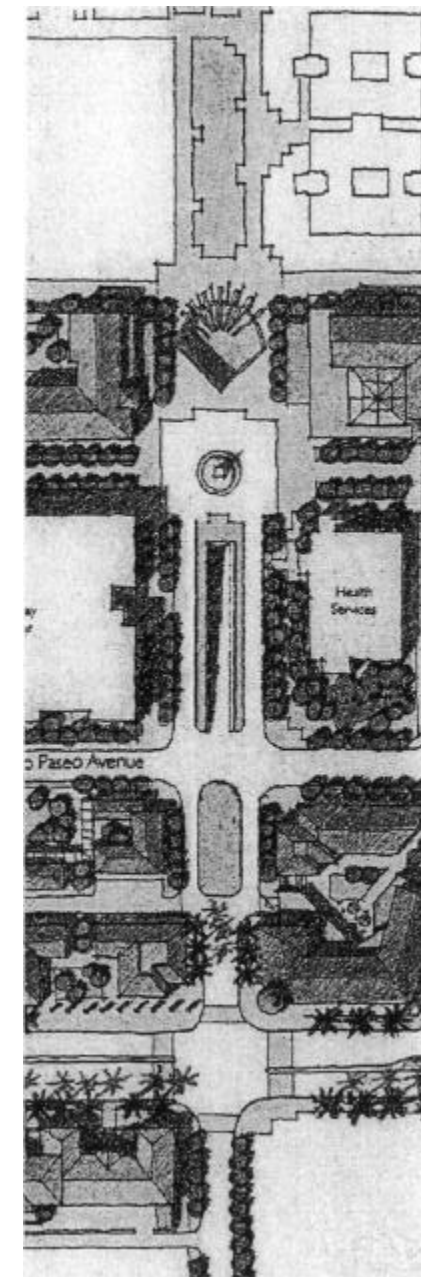
- Sidewalks should be straight and aligned with those extending onto the campus. Sidewalk and site pavement should follow the patterns and colors found on the University's Campanile mall. This is a field of gray concrete within a grid of red rectangular pavers.
- The right-of-way should be paved with street trees in tree grates.

**Campus District Fencing and Site Walls:**

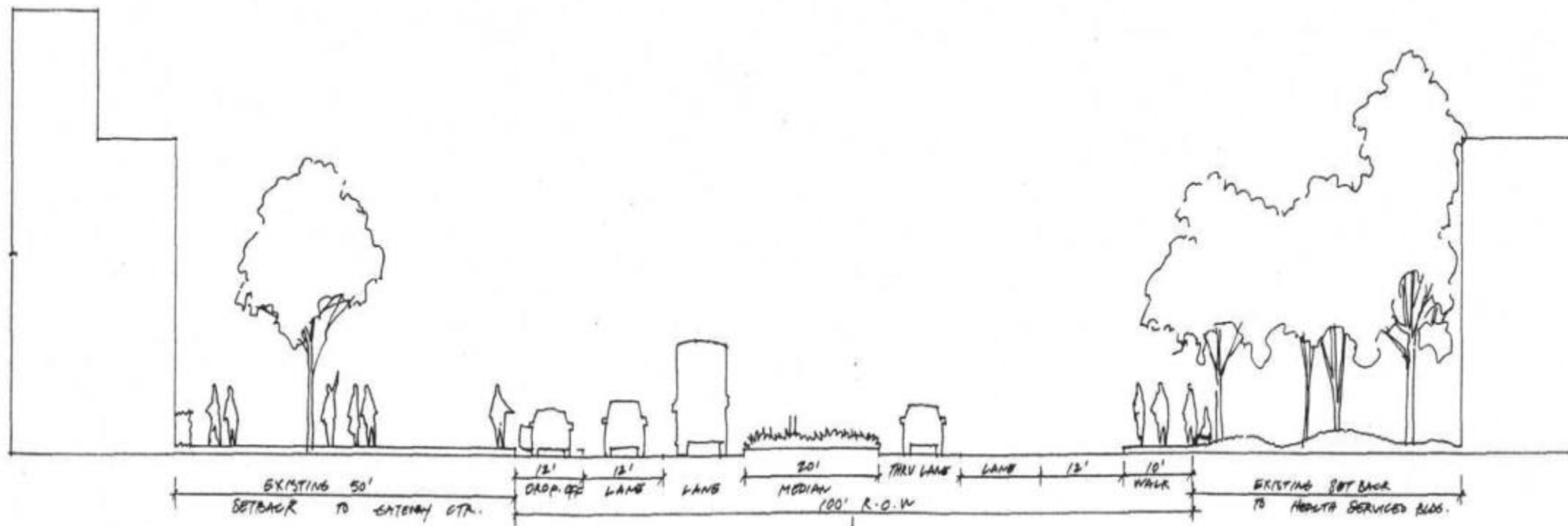
- Fencing on Campanile Drive is prohibited.
- Site walls should compliment the building's architecture in their material and form. The maximum height of site walls should be 6 feet.

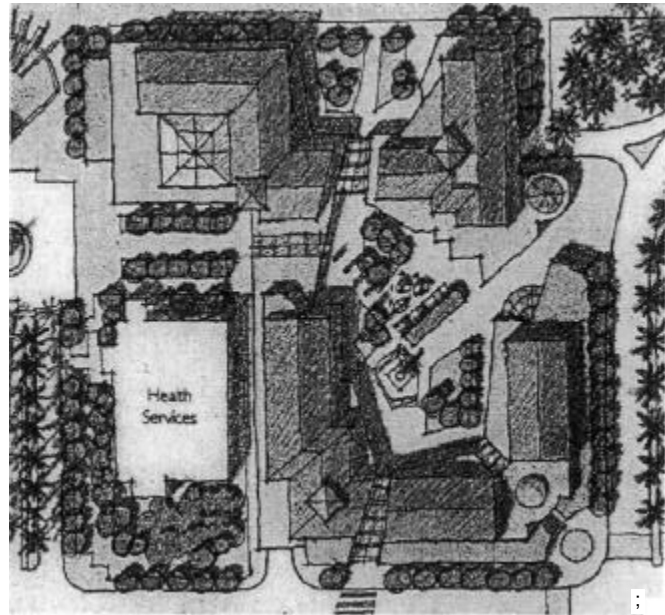
**Campus District Street Furnishings:**

- Site furniture materials should be primarily wood and concrete to compliment the furnishings on the University's Campanile.



*The Campus District received its clues from the materials and spaces on the campus*





*The Mixed-Use District*



*A rich variety of the landscape character in the Mixed-Use District enhances its retail liveliness.*

### Mixed-Use District

The Mixed-Use District is defined by Montezuma Road on the south, the University on the north, Campanile Drive to the west, and the service alley to the east of College Avenue. The intent of these additional guidelines is to provide a landscape character that reinforces the lively pedestrian atmosphere of the district, recognizes its urban character and welcome users with a variety of amenities including sun, shade, seating, and service. Pedestrian access and visual recognition to all establishments in the Mixed-Use Area is of primary importance. In all cases, the scale of this district should be in relationship to the activity. For example, bolder scale site amenities are recommended adjacent to streets where they will be viewed by automobile traffic. Here repetition and symmetry will reinforce the visual continuity seen from the street. However, in pedestrian only areas within the interior of the blocks, much finer detail, asymmetry and diversity is recommended to entice the pedestrian and enliven their experience. Here, scale of site materials should relate to the pedestrian, concentrating on amenities that can be viewed while walking.

#### Mixed-Use District Streetscape:

The streetscape environment must welcome pedestrians with a variety of amenities, including clusters of seating, opportunities for both sun and shade, and pedestrian serving features such as telephones and newspaper racks clustered alongside of the pedestrian passageway. Streetscapes should be an opportunity for gathering and enticing pedestrians into the mixed use district.

The pedestrian streetscape should be primarily paved between the curb and the building facade, with tree grates at all street tree planter wells to ensure visual and physical access to the commercial enterprises. Obstructions to clear pedestrian movement should be eliminated.

#### Mixed-Use District Medians:

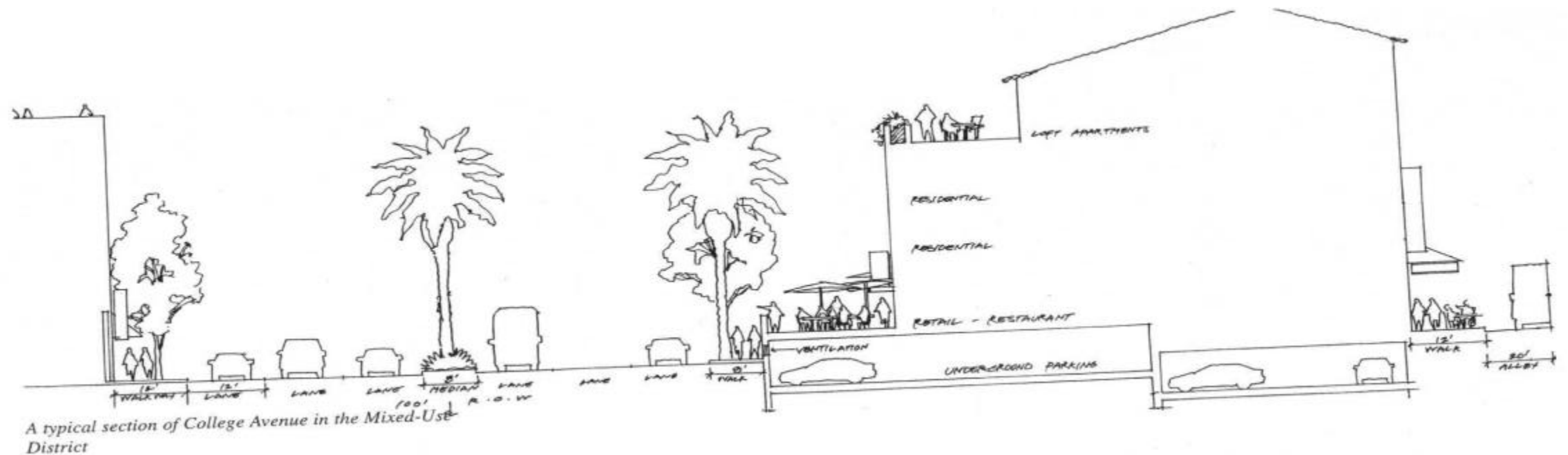
The curbed median on College Avenue should be redesigned and reconstructed to contain street trees where the width of the median allows and low shrub vegetation surrounded by a raised (6 inches) concrete curb. This will mark the entrance into the Core Sub-Area and still allow visibility across the street. Crosswalks should be fully at street grade and should contain a "pedestrian refuge" at the medians.

- Q Planted areas should have a minimum width of two feet. A two foot maintenance walk should be provided around the perimeter of medians, inclusive of curbing.
- Q Trees in medians shall be located a minimum of four feet from the face of the curb.

#### Mixed-Use District Open Spaces/Plazas:

Public gathering spaces in courtyards and plazas should open onto the sidewalks within the Mixed-Use Area. Motorists should be able to see the sidewalk activity and have glimpses into internal spaces. Public gathering spaces along the east side of College Avenue may be 4 feet (maximum) above the level of the sidewalk with multiple provisions for pedestrian and disabled persons access.

- Special features, such as small fountains, sculpture, sundials and/or canopy trees should be included within each public gathering space.
- Open spaces should serve a variety of activities throughout the year. For example, individuals should have the opportunity to sit in the sun or shade. Shade may be provided with deciduous trees, fabric structures or architectural elements.







*Residential open spaces create a sense of neighborhood.*



*Simple site walls can serve as elegant benches.  
A typical section of Montezuma Road in the Residential District*

### Residential District

The Residential District is defined as two areas within the Core Sub-Area. On the west side, it is from 55th Avenue east to Campanile, and from the University property south to the alley south of Montezuma (Fraternity and student-oriented housing). Additional segments of the Residential Area are on the south east side of the Core Sub-Area, including properties along the south side of Montezuma Road east of College Avenue to the intersection of East Fall View Drive and south of Montezuma along College Avenue (Sorority housing).

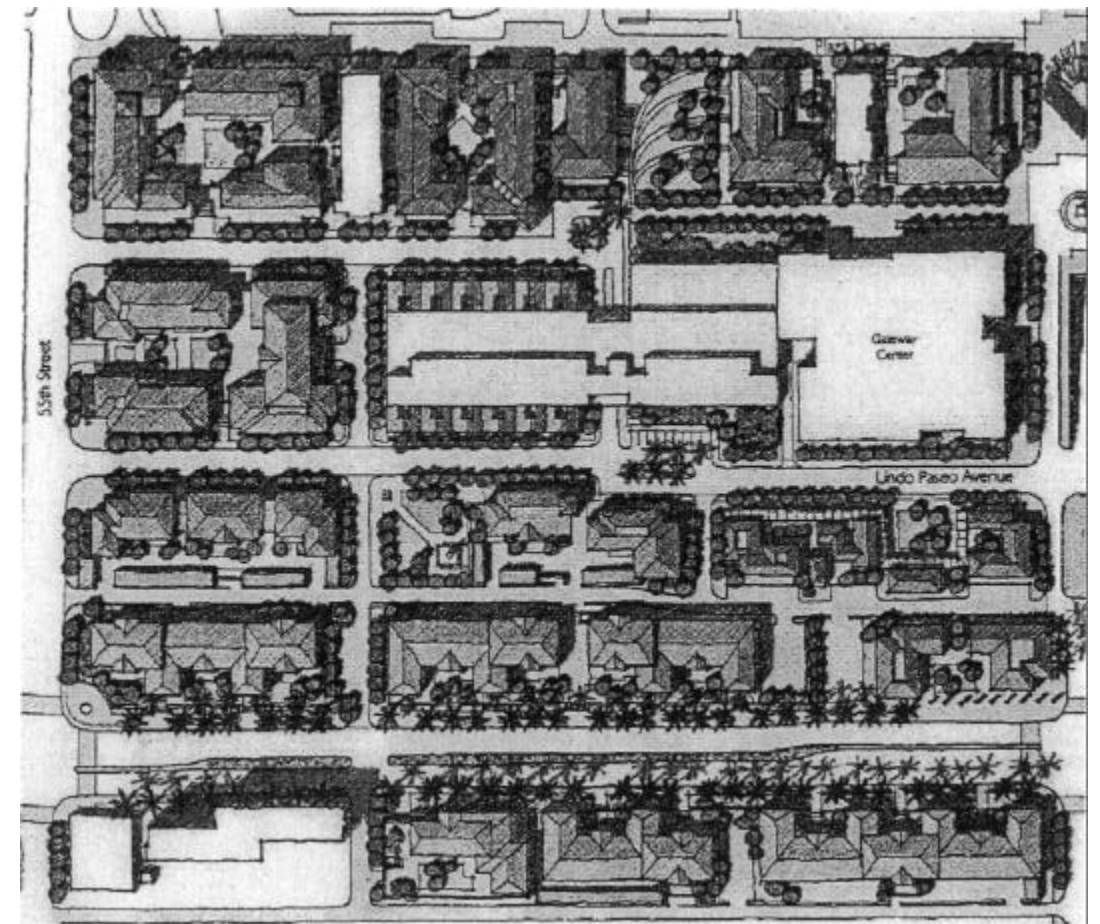
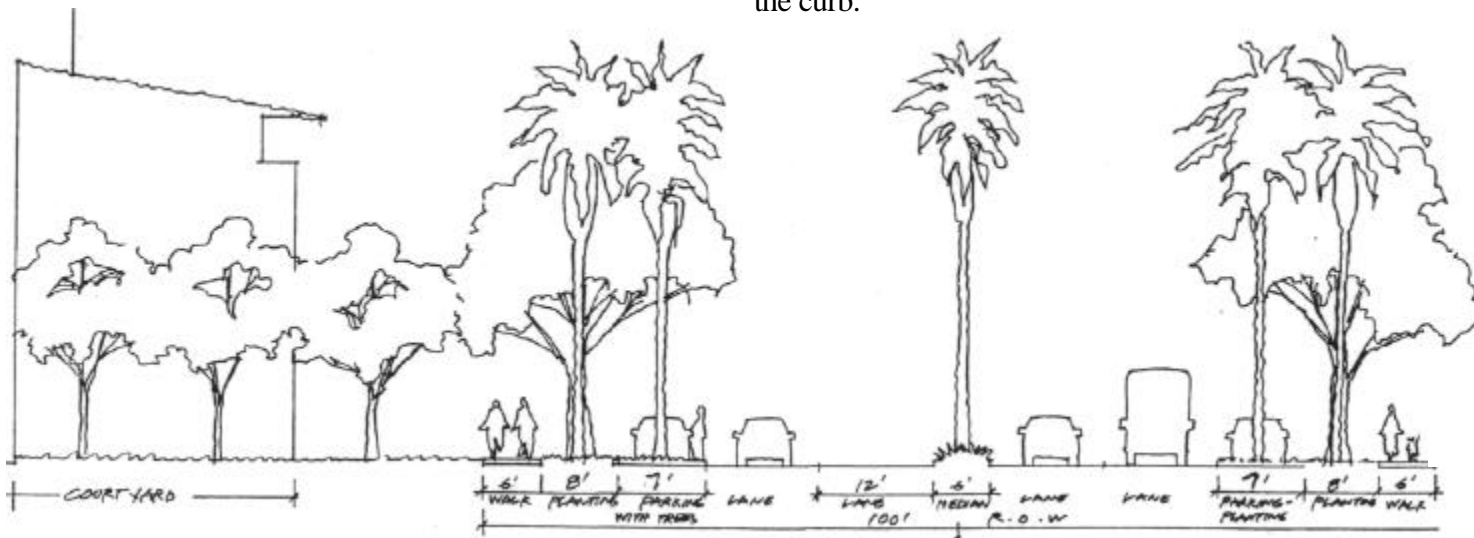
The intent is to create pleasing residential zones that allow safe and friendly pedestrian access. Scale of site improvements in the residential district should be smaller or lesser than that of the other two districts, it should relate to the character of "home." Institutional type, repetitive or mass produced improvements are not recommended. Rather, diversity is encouraged much like one would find in a typical residential neighborhood of many different homes.

Materials for site improvements should be durable and easy to maintain, as use will be constant. Materials should be similar to those found in residential areas, such as wood, wrought iron, concrete, cobbles, and the like.

#### Residential District Streetscape:

Montezuma Road only: On-street parking should be defined by extensions (fingers) of the parkway planting into the on-street parking area. The parkway planting extensions should measure 6 feet along the curb and 6 to 9 feet out into the on-street parking area. One should be located every 100 feet along Montezuma Road (or every fifth stall) from 55th Street to Campanile Drive, on both sides of the street. These should be planted with street trees and drought tolerant groundcovers to compliment the turf grass of existing front yards..

Sidewalks should be non-contiguous allowing a parkway planting strip at the curb.



#### Residential District Median:

Montezuma Road only: A median should be designed and constructed on Montezuma Road where feasible within the required street width. The median should continue from the Mixed-Use District through the Residential District, and as part of a wider Median Beautification Program, it should continue towards the west and the intersection with Fairmount Avenue. The median should include:

- *Arecastrum romanzoff*, Queen Palm as the median tree.
- Ground cover in the median along Montezuma Road should include the following low growing shrubs and groundcover (24 inch height maximum).
  - *Aloe species*, Aloe
  - *Bougainvillea species*, Bougainvillea
  - *Ceanothus gloriosus*, Ceanothus groundcover (requires minimal to no irrigation once established)
  - *Lantana montevidensis*, Trailing Lantana
- Montezuma Road's median should be extended beyond the Core Sub-Area of this study. Between 55th Street and Yerba Santa Street, the median could be continued with a corresponding palette of plant materials as it traverses the residential neighborhoods. West of Yerba Santa Street, Montezuma transitions into the Caltrans improvements at Fairmount Road. Embankment planting should be undertaken to limit erosion and enhance the area.



Suggested species could include:

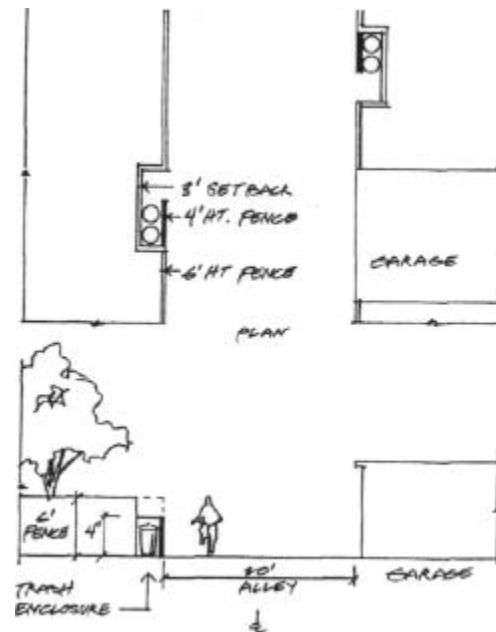
- *Ceanothus species*, Ceanothus
- *Eriogonum fasciculatum*, California Buckwheat
- *Ribes viburnifolium*, Evergreen Currant
- *Acacia redolens*, Acacia
- Planted areas should have a minimum width of two feet. A two foot maintenance walk should be provided around the perimeter of medians, inclusive of curbing.
- Trees in medians shall be located a minimum of four feet from the face of the curb.

**Residential District Open Spaces/Plazas:**

- Public and/or private open spaces should be located with access to the sidewalks.
- Open spaces will serve as spatial expansions of the streetscape. Views into these open spaces from the streetscape should not be blocked or heavily screened. Each should have provisions for pedestrian access and use of both sunny and shaded areas throughout the year.
- Public open spaces may be secured nightly with retractable wrought iron type fencing.
- Lighting within the open spaces should not be brighter than that of the adjacent streetscape.

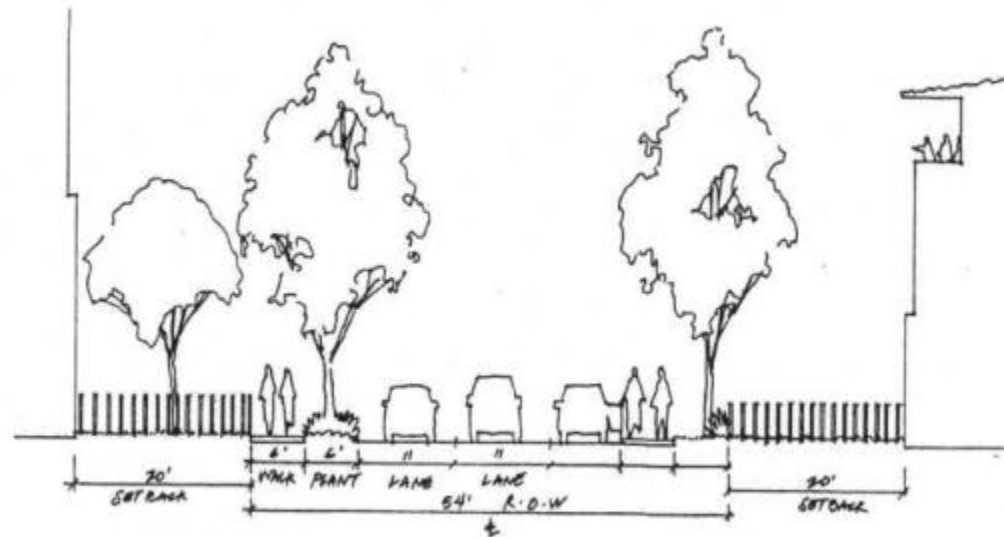
**Residential District Alleys and Service Access:**

Alleys run mid-block east/west throughout the residential area. They provide vehicular access to parking garages, stalls or structures, trash collection, and utility service. Additionally, the alleys will serve as an effective route for bicyclists where their potential conflict with pedestrians is generally reduced. Alleys should be defined by a 6 foot tall fence or wall along the setback line. Trash cans and dumpsters should be screened by a 6 foot tall wall and fully located within the property line. Vegetation in the alleys should be limited to irrigated vines and/or planted within a 2 foot wide strip parallel to the base of the wall/fence. The planting area should be contained within a 6 inch concrete curb to protect the plants from vehicular traffic and runoff water. All planting and associated curbs shall be within the property line so as to leave the alley free and clear for service vehicles.



**West Plaza Mall:**

The West Plaza Mall is within the jurisdiction of the University and therefore any improvements that are made within the Mall should follow the guidelines of the *SDSU Physical Master Plan*. West Plaza Mall should express the character of a pedestrian mall with very limited vehicular access where it abuts the Student Activity Center. West Plaza Mall should be entirely dedicated to pedestrians between the University's "J" Lot and Campanile Drive where it transitions to East Plaza Mall.



*A typical section through the streetscape in the Residential District*

**Hardy Mall:**

Adjacent to the Gateway Center, Hardy Avenue may be converted to a pedestrian-only mall. Street tree planting and furniture shall respect the right of way of the former street (to allow for emergency vehicular use), however the pavement may be enhanced to delineate the pedestrian quality. Bollards (removable for emergency access) should be used to close the street at both ends.

**Residential District Street Trees and Vegetation:**

Street trees and other vegetation within the Residential District should be selected based on ease of maintenance, climatic response and scale. The following trees are suggested species that will respond to either existing mature trees or the residential street trees of surrounding neighborhoods. In all cases, trees should be selected based upon their ease of maintenance and water demands.

- A 5 foot wide (minimum) continuous parkway planting strip shall be located on both sides of the streets (between the curb and sidewalk) for the maintained growth of street trees, lawn, groundcover and/or shrubs. Consideration should be given to the planting material in relation to the location of on-street parking. For example, lawn or other groundcovers that absorb constant foot traffic, may be the most suitable groundcover along on-street parking.

- ‡ Montezuma Road: The existing *Arecastrum romanzoff*, Queen Palm on Montezuma Road should be retained. The primary structure of street trees to be planted along Montezuma Road and in the median should be an alternating pattern of Queen Palms, and *Koelreuteria bipinnata*, Chinese Flame Tree. Chinese Flame Trees should be spaced 30 (minimum) feet on center, with the Queen Palm in between, also at 30 feet on center. Additional vegetation planted in association with new building entries, open spaces, or courtyards should include a wide variety of flowering trees, shrubs and groundcover.
- ‡ Street trees on Lindo Paseo and Hardy should one of the following trees planted along and parallel to the curb at a minimum spacing of 30 feet on center.
  - *Koelreuteria bipinnata*, Chinese Flame Tree

**Residential District Site Furnishings:**

- ‡ Seating for 3 persons (6 linear feet of seating) shall be provided for every 300 feet of sidewalk. Seating may be for individual or groups.
- Seating may be provided on site walls.
- ‡ Waste receptacles shall be located every 300 feet along the curb, in the parkway planting strip. They should be constructed on a concrete pad, matching to the adjacent sidewalk pavement, and be accessible to maintenance vehicles.

**Residential District Pedestrian Pavement:**

- Pedestrian pavement should follow the simple style of San Diego's older neighborhoods. These are plain concrete sidewalks with a slip resistant finish (such as broom or sandblast finish) running parallel to the curb and parkway planting strip. The concrete should have a gridded scoring pattern (2 feet on center in both directions).
- The parkway planting strip may be paved in association with the entrance to a building. In these cases the entrance pavement to the building is repeated adjacent to the curb. In all cases, the concrete sidewalk of the Residential District should remain uninterrupted.

**Residential District Fencing and Site Walls:**

- ‡ Fencing must be a maximum of 72 inches tall and be constructed with a minimum 40% transparency. As an example, a picket fence with equal spacing of the wood picket and the openings would be approximately 50% transparent.
- ‡ Chain link fencing is prohibited in the front and side yards. If chain link is used along the rear yard, it is only acceptable in alleys where it can be covered with vines or other screening material.
- ‡ Site walls shall be utilized in front and side yards, and shall be 18 inches maximum height. Site walls may be topped with a fence, if necessary. Together their height may not exceed 72 inches.
- Any walls that are developed for sound attenuation on properties abutting single family residential areas should be designed sensitively and aesthetically to interface with the adjacent homes.



*The parkway creates a pleasant sidewalk in residential neighborhoods.*



*Low walls and transparent fencing brings the residential life to the street.*

## A FINAL GUIDELINE TO SUCCESS....

- ! **Care, commitment, cooperation and common sense should always be applied in making decisions relating to redevelopment in the Core Sub-Area.**

Rebuilding an urban neighborhood is no small task. It takes extraordinary amounts *of* time, money, expertise and a significant quota of vision. More important, perhaps, than any of these is the enthusiastic participation of the numerous stakeholders involved in the process - property owners and existing tenants, neighboring communities and institutions, developers and investors, project architects, engineers and landscape architects, city development agencies and permitting authorities - and future residents and neighbors who can not even be identified today. Satisfying the objectives of such a disparate group of players and reaching agreement on the vast array of technical, economic and aesthetic issues involved in redevelopment is an often monumental, and sometimes impossible, task.

What we have to remember, however, is that beyond every controversy is an underlying consensus on the goals of the College Community Redevelopment Project and shared belief in its value to the present and future users of the SDSU area. This common ground provides the foundation for success - an essential commitment to progress which, tempered with care, cooperation and common sense, will lead the way through the exciting period of implementation ahead.