

# **GOLDEN HILL**

## **FERN STREET and 30TH STREET**

### **REVITALIZATION PLAN**

PREPARED FOR THE  
CITY OF SAN DIEGO

BY

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**MAY 29, 1987**

The City of San Diego.

Mayor:

Maureen O'Connor

City Council:

Celia Ballesteros

Bill Cleator

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Judy McCarty

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Fern Street and 30th Street Citizens' Task Force:

Gail Macleod, Task Force Leader

Ken Baldwin

Beverly Davis

Judy Forman

Liz Fox

Alice Gilbert

Edward Kinney

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## ACKNOWLEDGEMENTS

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Andy Schlaefli, Traffic Engineer  
John Andrews  
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GOLDEN HILL

Revitalization can be described as an act or instance of giving new life or vigor. Strategy is defined as the art of devising or employing plans toward a goal. The aggregation of the two words into one term, revitalization strategy, best describes the contents of this document. The FERN STREET AND 30TH STREET REVITALIZATION PLAN attempts to do three things:

- \* It identifies a series of immediate specific and practical streetscape improvements tailored to the evolving character of the community and integrates the desires and wishes of local residents and merchants.
- \* It acknowledges long-term improvements, such as Maintenance Districts and Business Improvement Districts, on which a community consensus may not yet exist. Information concerning the costs and benefits of these districts are provided.
- \* It identifies specific actions by City agencies, community merchants, and resident groups that need to happen if the revitalization strategy or design vision is to become reality. Costs are keyed to individual public projects, funding sources are identified, timing or phasing of improvement work is mentioned, and implementation steps are discussed.

## PLAN ORGANIZATION

This document is organized under the following categories:

IMAGES and ISSUES contains a brief history of the community and a summary of the initial assets, opportunities and issues of the study area as established through project workshops and a community survey. During the course of the study, further issues and recommendations were developed as part of the Action Plan.

The DESIGN PLAN sets a vision and illustrates the potential result of the improvements recommended with the Action Plan.

The ACTION PLAN is the heart of the FERN STREET AND 30TH STREET REVITALIZATION PLAN. This section of the document is a "blueprint" for implementing the Design Plan. In many ways the Action Plan is also a "blueprint" for the community to guide the on-going restoration of the study area.

The Action Plan, on page 36, identifies neighborhood improvement proposals that are an outgrowth of concerns and desires expressed by the community and the consultant during the first part of the work effort.

The SCHEDULE OF PUBLIC IMPROVEMENTS and COST ESTIMATE together form the basis for implementation of the recommendations outlined in the Action Plan.

The APPENDIX is a source of further information of value to the implementation of the plan.

## RECOMMENDATIONS

The following is a summary of specific recommendations called for by the Fern Street and 30th Street Action Plan. A complete discussion concerning individual improvement concepts and their rationale, as well as timing, costs, and funding sources, are provided respectively within the eight chapters of the Action Plan and Schedule of Public Improvements.

The Action Plan is organized under eight headings which represent the broad concerns addressed by this study.

### 1. CIRCULATION AND PARKING

- \* Maintain the existing traffic flow pattern on Fern Street and 30th Street as two-way streets.
- \* Provide an all-way stop at the intersection of Fern Street and Grape Street. Consider the installation of additional all-way stops at Fern Street and Juniper Street, as well as Fern Street and Beech Street.
- \* Designate Fir Street and Date Street (between 30th and Fern) as one-way east bound streets.
- \* Redesign the offset at the intersection of Fern Street and Grape Street to alleviate pedestrian and vehicular conflicts.

- \* Incorporate additional curb/pavement modifications as part of the overall circulation management concept.
- \* Remove on-street parking from the west side of Fern Street between Grape Street and one-quarter block south of Date Street (Fern Central).
- \* Provide diagonal parking on all east/west streets and on the east side of 30th Street where feasible.

### 2. TRANSIT

- \* Remove the north-bound and south-bound direction of the bus to turn from Fern Street to 30th Street and vice versa, at Juniper Street (as opposed to Ivy Street).
- \* Relocate certain existing bus stops to provide more convenient and safer locations for transit users.

### 3. STREETSCAPES

- \* Create a focus of pedestrian activity by way of special design at the intersections of Grape and Fern and Beech and 30th.
- \* Improve pedestrian walking conditions along Fern Street and 30th Street by way of new sidewalks, street furniture, street trees, and additional street lighting.

\* Guarantee that the quality and intent of the initial streetscape improvements along Fern Street and 30th Street will be maintained to the satisfaction of the community and the City by adaption of a Lighting Landscape Maintenance District within the study area.

#### 4. OVERHEAD UTILITY LINES

\* Begin discussions with SDG&E this year (1987) in order to facilitate planning and design in 1988 with undergrounding of the 12KV lines in early 1989.

#### 5. BILLBOARDS

\* Establish a "sunset" period for removal of billboards within residentially zoned areas of the study area. Maintain an on going dialogue with the City Attorney's Office concerning removal of all billboards within the neighborhood.

#### 6. IMPROVEMENT RECOMMENDATIONS

\* Extend the Golden Hill Planned District boundaries, by way of a new sub-area classification to include portions of the study area immediately fronting on Fern Street and 30th Street.

\* Adopt the Pedestrian Commercial Overlay Zone for commercially zoned properties within the study area.

\* In the interim, prior to the expansion of the Golden Hill Planned District boundary, establish a formal review procedure for the 30th and Fern Street Task Force to review new development proposals within the study area.

\* Begin discussions with area merchants and property owners concerning long term strategies for commercial revitalization. Explore the pros and cons of establishing a Business Improvement District (B.I.D.) within the study area or in combination with other commercial districts within Golden Hill.

#### 7. BEECH STREET CENTER AND

#### 8. GRAPE STREET SQUARE

\* Establish two nodes for community activities and events. Acknowledge these nodes as special gathering places within the community by way of special design of public and private features. Include these nodes as part of the Phase One Improvement Program.

#### FIRST THINGS FIRST

The question that remains is: What should happen first within the next couple of months? Following review and approval of this document by the Planning Department, Engineering and Development Department, the Eighth District Council Office, SDG&E, San Diego Transit, and the Economic Development Com-

mission, construction drawings for the first phase streetscape improvements should be started.

The Phase One streetscape improvements include Fern Street between Juniper Street and 'A' Street. It also includes the full improvement of Grape Street Square and Beech Street Center and the undergrounding of the overhead utility lines. The completion of the phase one streetscape improvement drawings are seen as a necessary first step to show all parties involved that "something is being done." Consultant fees for the work effort are estimated to cost \$97,000 which can be paid for from the initial \$150,000 Community Development Block Grant (CDBG) allocated to the Fern Street and 30th Street study area. The remaining \$55,000 can be allocated to pay construction costs associated with first phase improvements.

The total cost for constructing the Phase One streetscape improvements is \$872,465. The initial CDBG allocation will not be sufficient to cover the total cost for the improvements. The additional funding will be required. CDBG continues to be the most likely candidate to supply additional funds. A CDBG application should be completed and submitted to the Planning Department prior to July 1987 in order to secure funding in 1988. This essentially is the first step.

Implementation of the proposals under the headings of Circulation and Parking and Transit are the next step. These proposals are designed to occur within a one to two year period. The circulation and parking proposals are to be expedited sooner than others because of the ability to use City forces and resources to complete improvements associat-

ed with these relatively small projects. It should also be mentioned that circulation improvements need to occur first because of the urgency associated with safety issues. A complete discussion of the individual project proposals concerning Circulation and Parking and Transit are outlined on page 37.

Other Action Plan proposals such as improving streetscapes, undergrounding overhead utility lines, Grape Street Square, Beech Street Center, and the establishment of a maintenance district should be implemented within two to three years. These are the mid-range improvement projects. The timing for construction, or implementation, of these projects is established because of the pre-planning required on the part of the various agencies involved.

Billboard removal, reviewing Improvement Recommendations and the formation of a Business Improvement District are long-range proposals which could occur beyond the first three years. Although all of the proposals should be implemented as soon as possible, the urgency associated with completing early and mid-range improvement projects requires some prioritizing of the work effort on the part of the City and community. The concentration of public commitment to the study area will set the stage for implementation of the long-range proposals.

This report is not the final product. It proposes future improvements which the community can support and implement, as well as more complex proposals which the community must genuinely debate. However the time to start is now.

## A. HISTORY

### SOUTH PARK

Initial development of the neighborhood along Fern Street and 30th Street began in 1906 as a suburban community. Originally called South Park, the community's process of development modeled the evolution of similar "streetcar suburbs" in Boston, New York, and other eastern cities. At the turn of the century, Golden Hill proper had already been developed into a fashionable place to reside for mayors, senators, and judges. Having a similar vision for another portion of the community, E. Bartlet Webster formed the Bartlet Estate Com-

pany and began sub-dividing lots along the eastern side of City Park city (now Balboa Park.) Bartlet's goal was to create a high-class residential district which he subsequently called South Park, Figure 1 and 2.

As an incentive to buy a lot in South Park, Bartlet established the South Park and East Side Railway to service the transit needs of the community. The electric street railway started was construction in March, 1906. The line began at the end of Broadway, continued

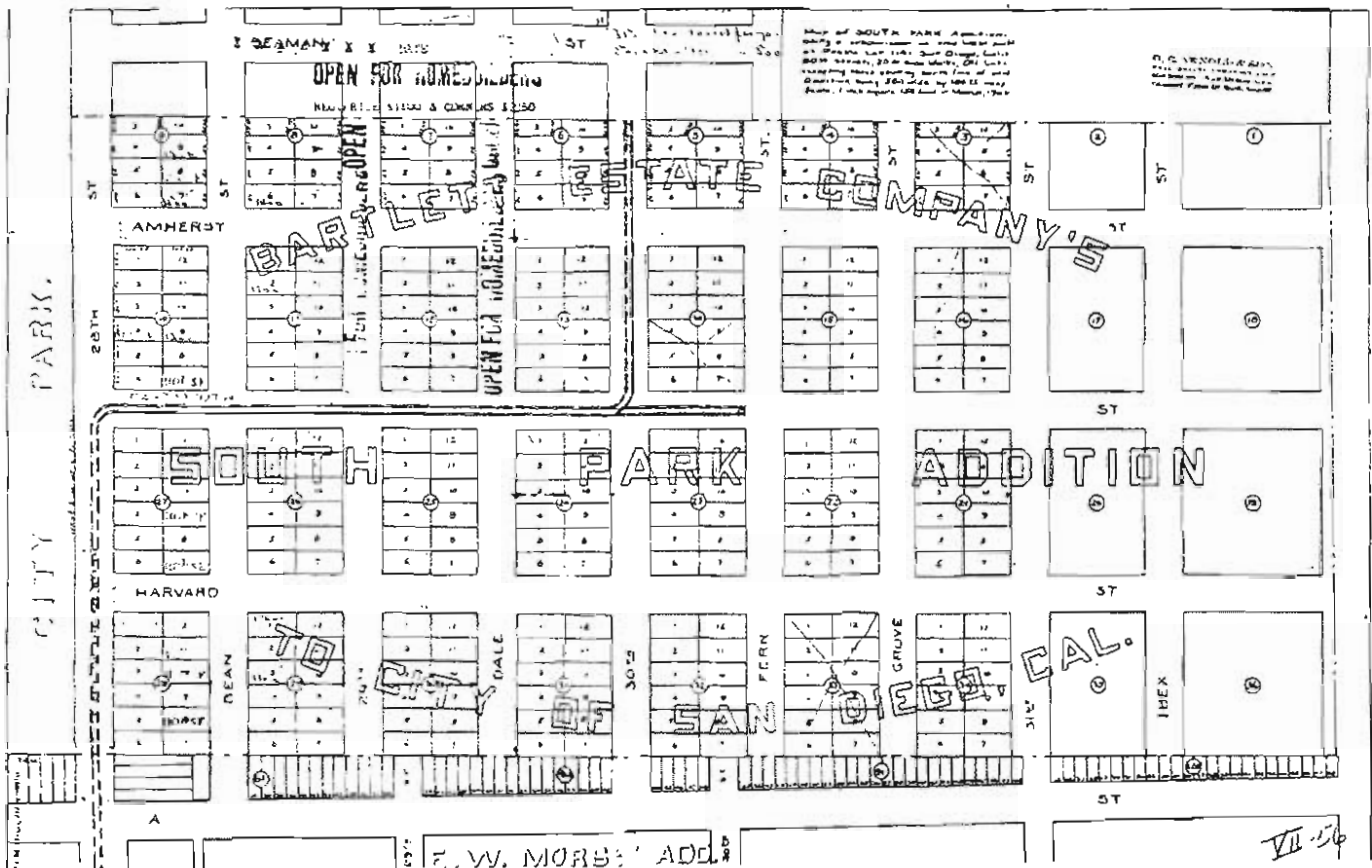


FIGURE 1

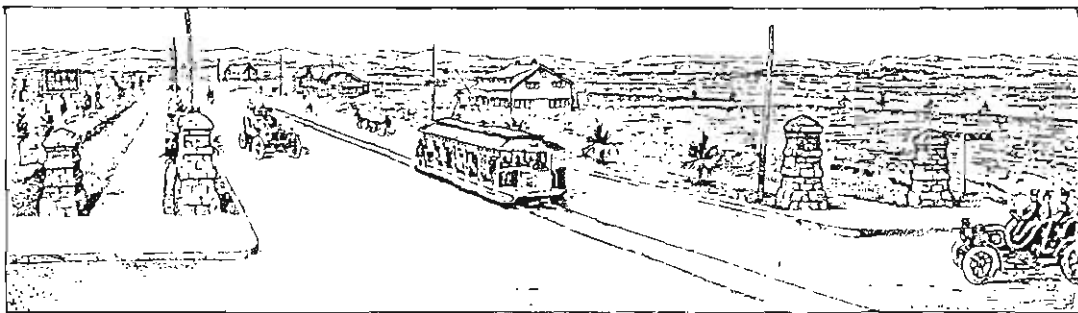
through downtown and Golden Hill on to Dartmouth (now Beech) and ended at Amhurst Street (now Cedar Street) and 30th Street. A power house was built at the line's terminus.

Bartlett eventually lost control of the South Park and East Side Railway to John D. Spreckels, who extended the line along 30th Street and on to North Park. The only sign today of the railway's past existence is an open slice of land which diagonally bisects the block bordered to the south by Ivy and to the east by Fern Street. A trolley stop was located at this point. Although not verifiable at this time, the old powerhouse at the corner of

Cedar and 30th is also said to be still in place.

Trolley service along 30th Street was eventually discontinued. The automobile, as was predominant at this time, became the primary mode of transportation for urban dwellers. It can be said that a certain amount of the neighborhood's special character was lost with the disruption of trolley service.

Many examples of the neighborhood's past, however, are still intact. The most notable examples are the individual shops and craftsman style residences scattered throughout the neighborhood. Within the study area



VIEW OF 28TH STREET, SOUTH PARK

**SOUTH PARK** Addition to San Diego is the only high-class residence district of its kind in the city

ALL MODERN IMPROVEMENTS

*Are made, which means no more worry and expense for grading, sewers, water, gas, sidewalks, etc.*

**PERFECT ELEVATION ABOVE THE SEA** WITH SPLENDID CLIMATE AND MAGNIFICENT VIEW

**DON'T PURCHASE A HOME SITE IN SAN DIEGO**

*Until you have seen or investigated South Park*

**BARTLETT ESTATE COMPANY**

CORRESPONDENCE INVITED.

BOX B, SAN DIEGO, CAL.

itself, the 2200 block of 30th Street contains several buildings of architectural merit. The Burlingame Garage and the adjacent firehouse were built in the early part of this century.

The Christ United Presbyterian Church, built in 1914, remains a significant cultural and architectural landmark in the neighborhood. The old trolley powerhouse at the corner of Cedar Street is one of several buildings along the 30th and Beech commercial district that can be said to have at least neighborhood landmark status.

## FERN & 30TH TODAY

### LOCATION

The study area is a 10-block stretch of commercial and residential land located in what is now called the community of Greater Golden Hill. Greater Golden Hill's central location and close proximity to major points of interest in San Diego has made it an increasingly desirable place of residence, Figure 3 and 4.

For purposes of identifying area limitations of this study, the eastern and western bounda-

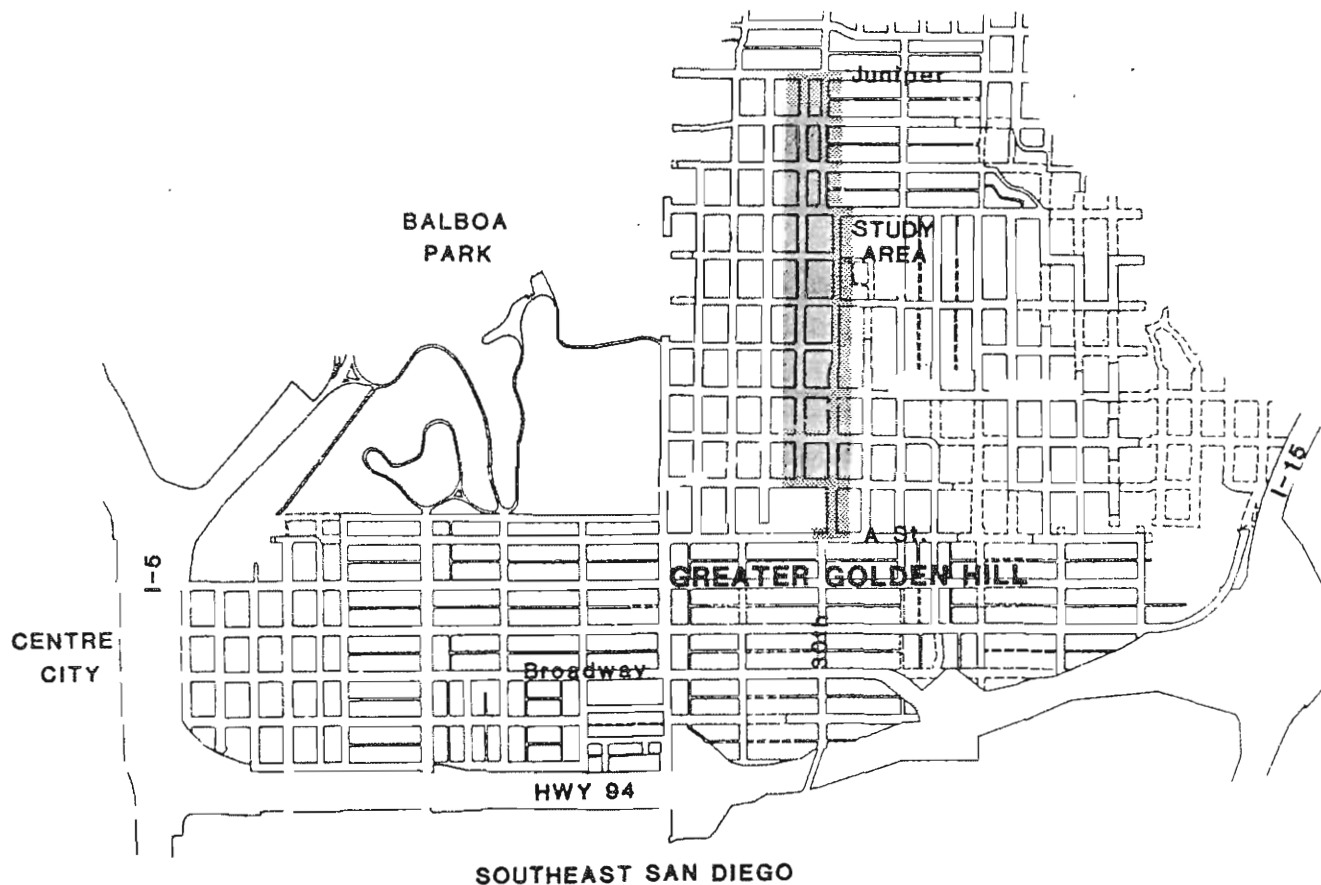
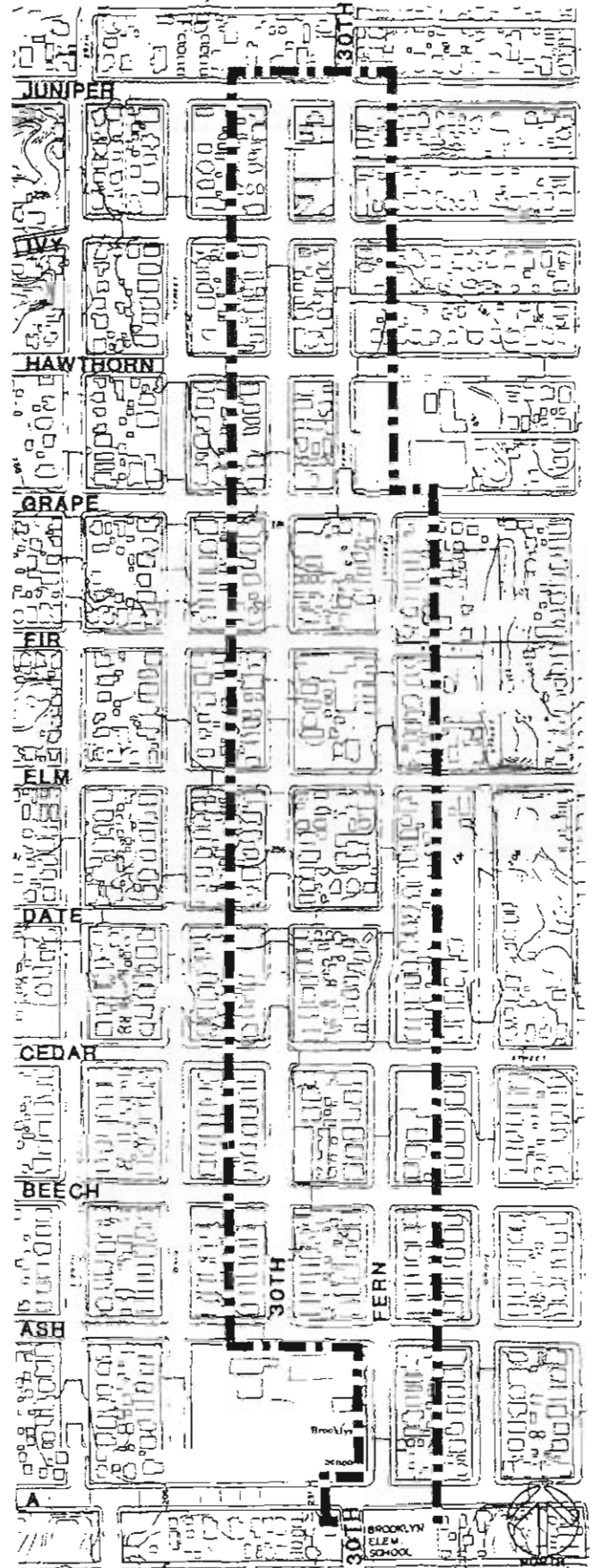


FIGURE 3



ries extend one-half block from Fern Street and 30th Street respectively. A Street and Juniper Street are respective southern and northern limits. Other east west streets such as Ivy, Hawthorn and Grape are also a part of this study. Although the focus of the work effort centers on the resolution of design problems associated with Fern Street and 30th Street, a complete understanding of the problems and potentials of the study area is difficult to achieve without a clear insight into the neighborhood as a whole. Figure 5 depicts the residential neighborhoods surrounding the study area.



STUDY AREA BOUNDARY

FIGURE 4

## B. ASSETS and OPPORTUNITIES

There are a number of ways to define a good neighborhood. A good neighborhood has a special character. It can be derived from the streets, the buildings, commerce, natural features, or the people who live there. Fortunately this study area, and the residential enclaves surrounding it, has many of these special characteristics already in place, Figure 5.

A visitor arriving to the neighborhood for the first time would be impressed by the framework of physical assets already in place. A clear sense of boundaries, or edges, is one asset of the neighborhood. Although illustrated as a subtle distinction, the way

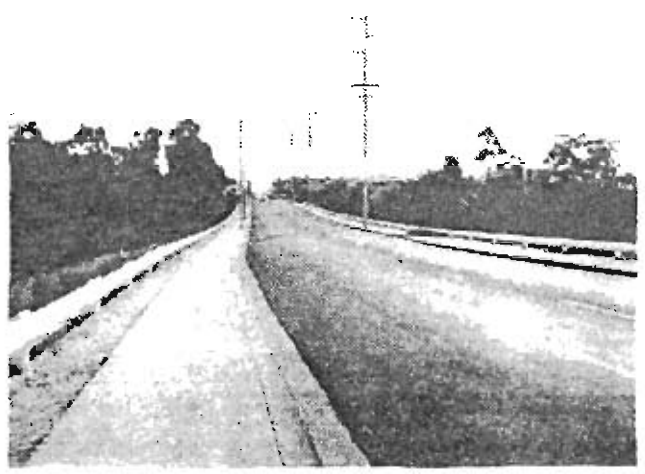
the street grid change between blocks north and south of A Street creates a separateness between two districts. A Street, as a southern border, creates a sense of arrival for visitors crossing north into the study area.

Balboa Park on the western edge is an easily identifiable signature of the neighborhood by the line of Eucalyptus trees along the skyline. This edge is heightened by the views down the east/west streets, such as Grape and Beech, toward the park.

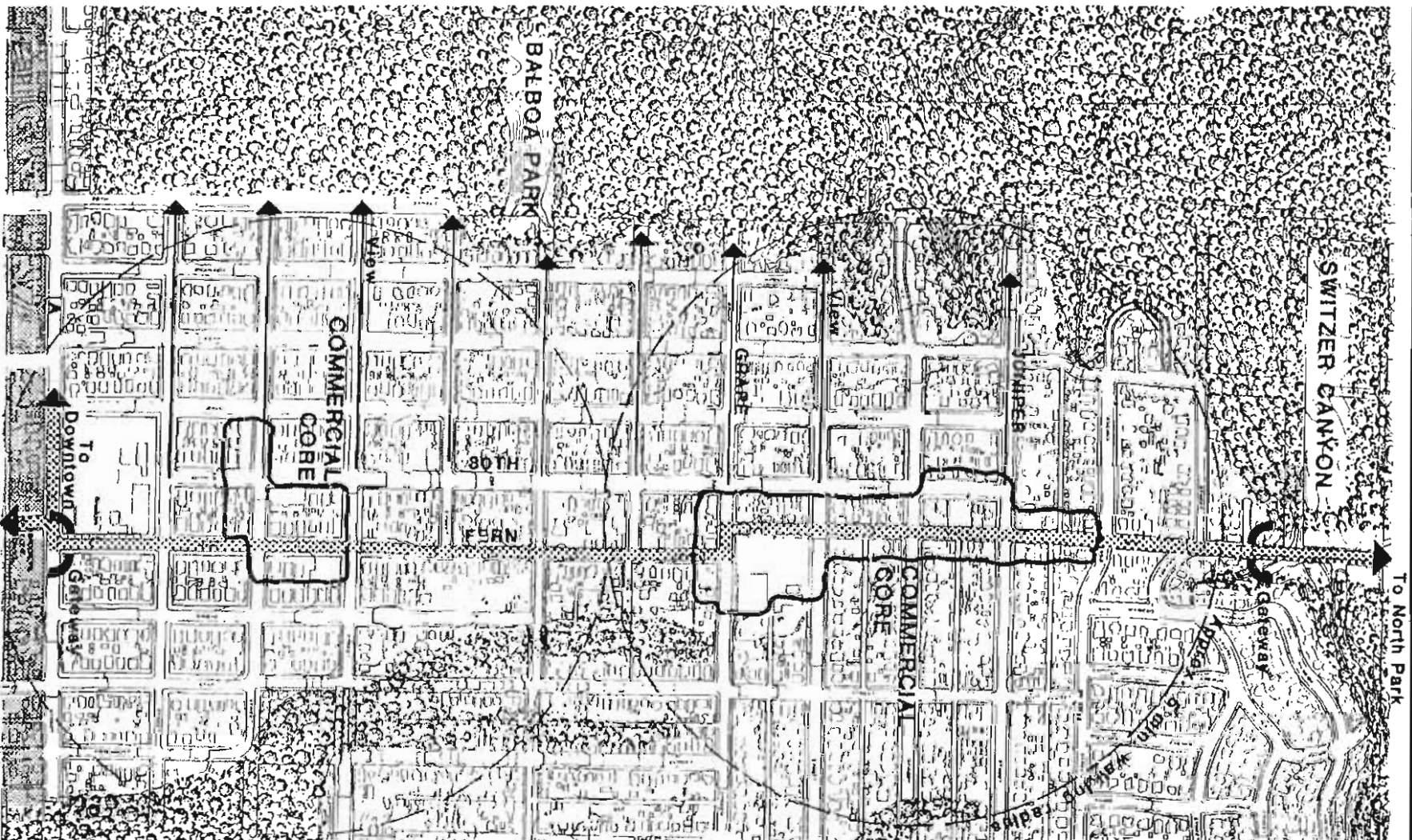
Following the edge of the park, the open space feature turns east to form the northern bound-



BALBOA PARK EDGE



SWITZER CANYON CROSSING



COMMUNITY ASSETS

FIGURE 5

dary of the neighborhood. 30th Street between Maple Court and Palm Street has the illusion of a bridge crossing Switzer Canyon. The sense of entry, or gateway, is heightened by the canyon crossing. A series of small canyons to the east of Fern Street completes the image of a compact and identifiable neighborhood.

Arrival to the neighborhood is essentially restricted to Fern Street or 30th Street. This, again, accentuates the sense of entry to the neighborhood. The street name change (30th Street vs. Fern Street) at A and Juniper, however, is often a source of confusion to visitors. Fern Street, for all purposes, can be described as the "Main

Street" of the community. The majority of commercial and retail facilities face Fern Street, thus elevating the image of the street as the heart of the neighborhood. Because of the slow reinvestment activity of the 1960's and '70's, Fern Street and 30th Street have been spared much erosion of the existing building street wall character.

Two existing commercial districts along Fern Street and 30th Street provide a focus for community activity in the neighborhood. A number of commercial establishments, such as the Big Kitchen restaurant, are already acknowledged gathering places in the neighborhood.



STREET NAME CHANGE



CHRIST UNITED PRESBYTERIAN  
CHURCH

The dispersed location of the two districts, guaranteed by the existing zoning legislation, maintains the pattern of commercial nodes in the neighborhood, as opposed to a continuous strip image along Fern Street or 30th Street.

The existing building character is one of the neighborhood's most remarkable assets. Within the study area, the majority of buildings are one to two stories in height. With the exception of the Safeway supermarket, most buildings are less than 1,000 square feet in size. The combination of existing building height and size have created a small scale "village" atmosphere.

Several buildings within the study area can be described as having local landmark status either because of their age or unique architectural character. Three buildings, the Christ United Presbyterian Church, the Burlingame Garage, and the adjacent firehouse, are structures of special note.

The homes within the residential enclaves surrounding the study area are typical of the craftsman style built in San Diego between 1905 and 1930. The wide, tree-lined streets, large front yards, and distinctive architectural character have already established the neighborhood as a unique community within the City.



**BURLINGAME GARAGE &  
FIREHOUSE**



**CRAFTSMAN BUNGALOW**

## C. CURRENT ISSUES

### THE WORKSHOP PROCESS

Workshops, field observations, and an opinion survey formed the basis for developing an understanding of the problems and potentials of the study area. During the first workshop on June 18, 1986, and a subsequent awareness walk on June 28th, prominent concerns were expressed by Task Force members and residents. These concerns involve: traffic speed on Fern Street and 30th Street, inadequate parking conditions on Fern Street, the unsightliness of the overhead utility lines and billboards, and lack of upkeep on public and private yard areas, to name a few.

Some desires expressed during the first workshop were: maintaining the neighborhood's existing residential character, preserving the existing resource of older homes, improv-

ing the streetscape image by adding new trees, better lighting and sidewalks, and creating a stronger and improved identity for the neighborhood.

Response to the opinion survey and discussions during workshop meetings dealt with many issues. However, in sorting through the many concerns and desires of the community, three central issue headings surfaced as the most prominent. These later became the framework for more thorough analysis. The central issue headings are:

- \* CIRCULATION
- \* VISUAL QUALITY
- \* REINVESTMENT

These three central issues also suggest objectives to which design and planning of the study area should follow.



AWARENESS WALK



AWARENESS WALK

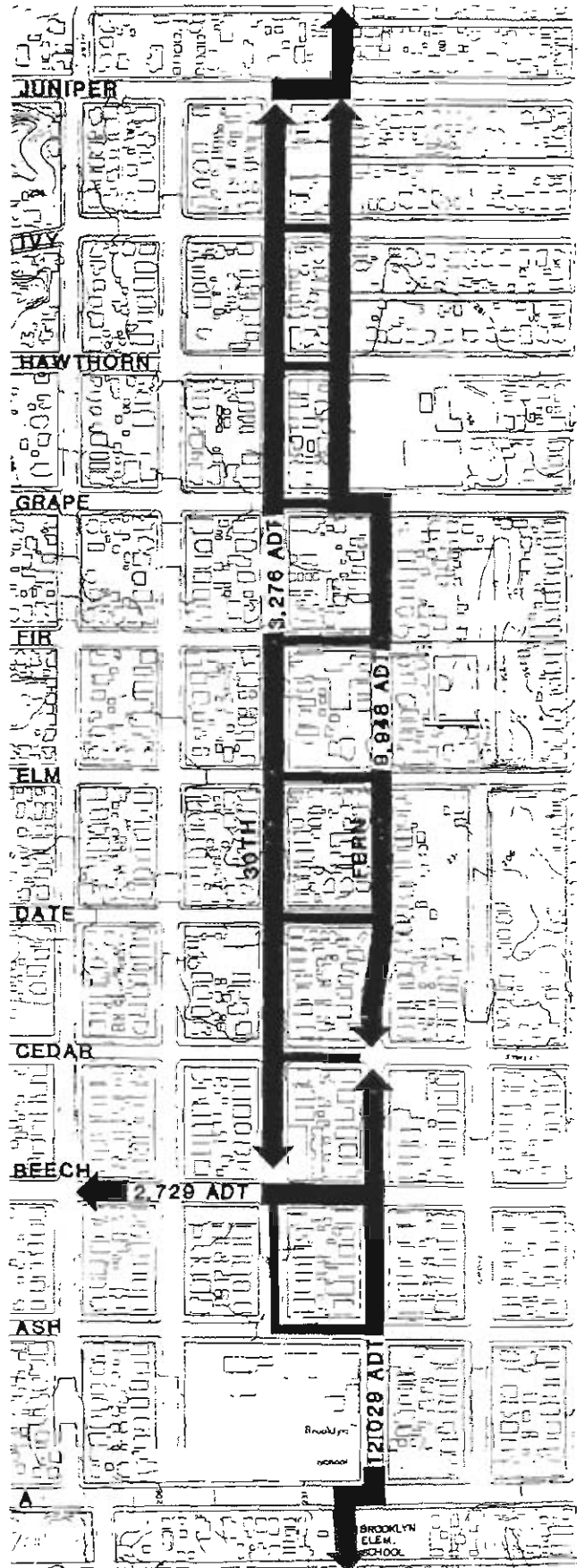
**CIRCULATION**

**STREET CHARACTERISTICS**

The present circulation pattern within the neighborhood can be characterized as a series of fragmented north/south and east/west streets which often dead-end on either canyon edges or Balboa Park. Although the canyons and other large open space features have served to heighten the neighborhood's compact and isolated image, they have also served to restrict traffic movement into, out of, and especially through the neighborhood. Fern Street and 30th Street, which are central to the neighborhood, have been forced to absorb the majority of the traffic volume, Figure 6.

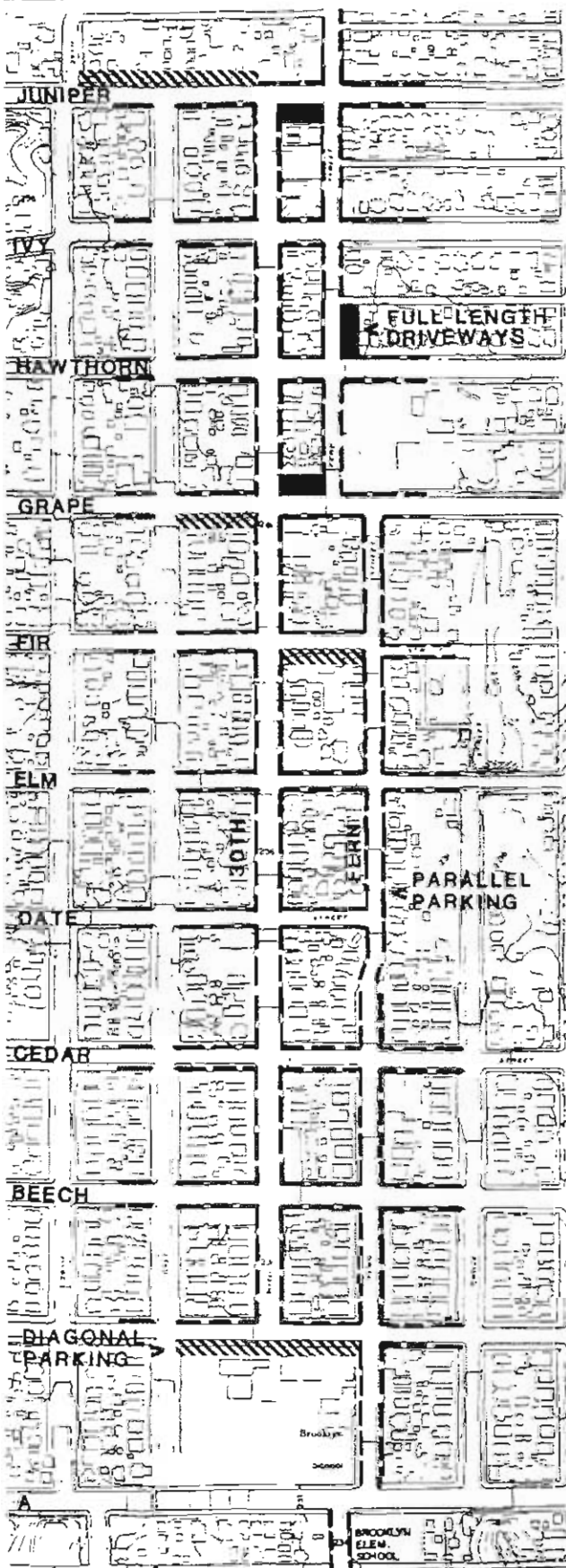
Fern Street, which confusingly changes its name to 30th Street north of Juniper Street and south of A Street, is the primary north/south street entering and leaving the neighborhood. Beyond providing access and egress to residents within the immediate vicinity, it also services motorists traveling to and from Golden Hill to the south and North Park to the north. Fern Street also functions as a centralized link to the Highway 94 ramp connections which, again, accounts for the street's popularity with motorists.

Ironically, Fern Street, considering its importance as a neighborhood and subregional circulation link, is also the narrowest of the north/south



**TRAFFIC FLOW AND VOLUMES**

**FIGURE 6**



## PARKING CONDITIONS

FIGURE 7

streets within the study area. The current pavement width from curb to curb for Fern Street varies from 34 feet to 40 feet. This cross-sectional dimension should place Fern Street within the City of San Diego's local street classification. However, Fern Street is currently operating at approximately 10,000 to 12,000 ADT (Average Daily Trips) which should place the street in the next higher street classification of "collector". The minimum curb to curb width for a collector is 40 feet to 50 feet.

30th Street, which parallels Fern, serves as a distributor link within the neighborhood. Motorists traveling north or south through the neighborhood appear to frequent 30th Street in order to avoid congestion on Fern Street, or to take a shortcut via Beech Street to Golden Hill proper and downtown. 30th Street is also the transit link through the neighborhood. The Route 2 bus services the neighborhood providing connections to North Park and Normal Heights, as well as Centre City.

The volume of traffic on 30th Street is significantly less than on Fern Street. The current ADT on 30th Street is 3,300. This is a marked contrast to the volume on Fern Street. 30th Street is also remarkably different from Fern Street in its curb to curb cross-sectional dimension. 30th Street's paved area is 52



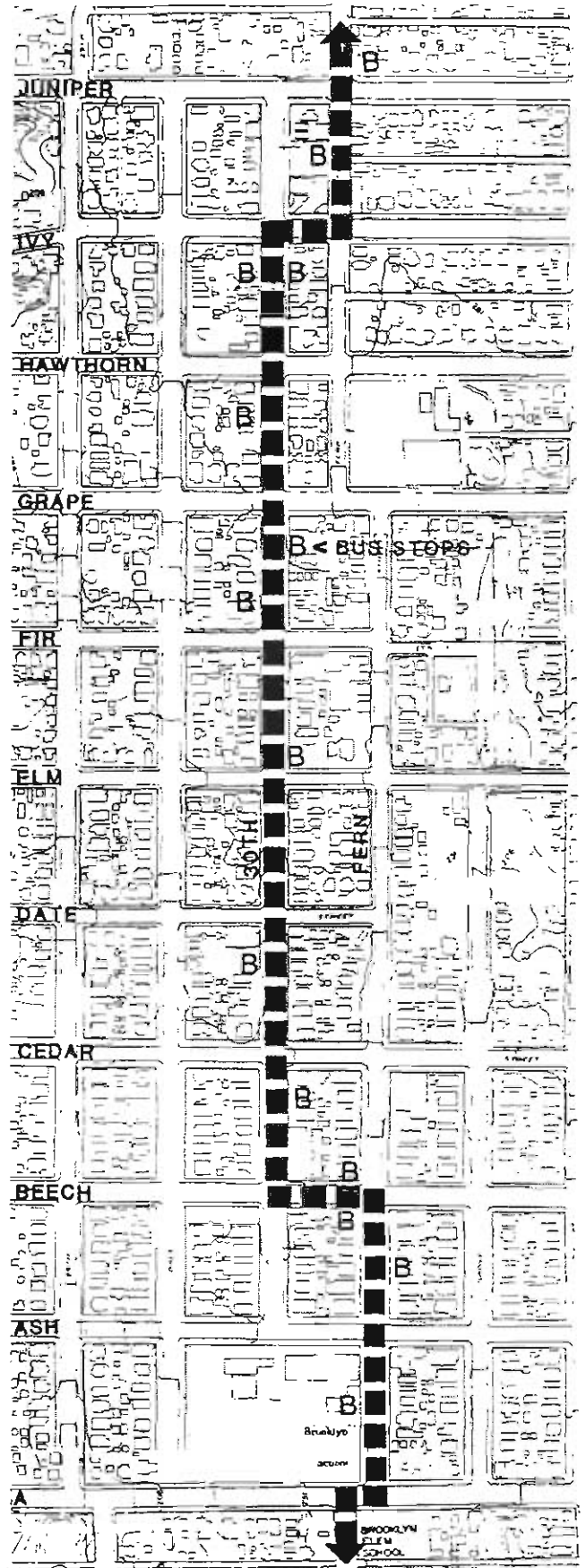
feet wide, placing it comfortably in the City's collector street classification.

On-street parking predominates throughout the study area due to the severe lack of off-street parking spaces. This parking pattern, according to early traffic studies, cuts down road visibility, causes congestion on streets, and is one of the contributing factors to the high accident rate in the greater Golden Hill area, Figure 7.

**TRANSIT**

The study area is well served by public transit and the existing Route 2 bus line is well used by residents. The Route 2 line provides connections to North Park and Normal Heights, as well as Centre City, Figure 8.

Issues concerning transit raised by residents conform to the location, and attention to design and maintenance of bus stops. Considering the bus lines high degree of usage and their prominent location in the public right-of-way, greater attention should be placed on making these facilities more comfortable and attractive to users.



**TRANSIT ROUTE**

**FIGURE 8**

## TRAFFIC ISSUES

Vehicular traffic, and associated issues concerning parking and pedestrian movement, were overwhelmingly stated in workshops and the opinion survey as needing improvement throughout the study area. Concern for vehicular speed on Fern Street and 30th Street was often emphasized by participants in the workshops. This concern was similarly followed by comments or recommendations for stop signs or improved pedestrian crossings along both streets. The intersection of Fern Street and Grape Street was often singled out as a particular problem area.

Statistics compiled by the Traffic and Engineering Department for 1985 document an above the normal occurrence of accidents along Fern Street. Along Fern Street between Cedar Street and Juniper Street the accident rate was 13.6 percent higher than the citywide accident rate for collector streets. It is important to keep in mind that the width of Fern Street, as discussed earlier, is below the citywide standard for collector street design. Thus, within the local street classification, the accident rate, citywide, may be significantly higher.

As a means of reducing the accident rate within the study area, the 1978 Greater Golden Hill Precise Plan recommends several possible solutions to the problem. The most promi-

nent recommendation was the creation of a one-way pair street circulation system. Traffic flow along Fern Street would be two lanes northbound between Ash Street and Grape Street. Similarly, 30th Street would be two lanes southbound between Ash and Grape.

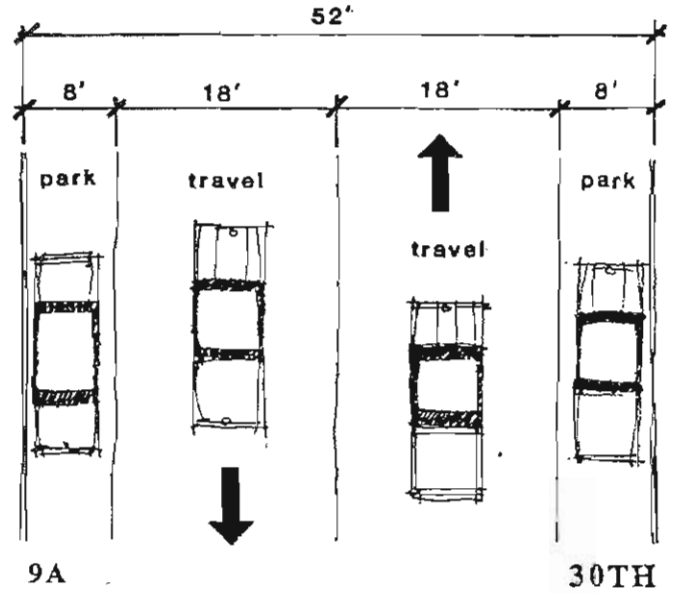
The one-way pairing system on Fern Street and 30th Street was subsequently recommended to be removed from the 1978 Greater Golden Hill Precise Plan by the Transportation and Traffic Engineering Division of the City of San Diego. The March 18, 1986 memo titled, Proposed Greater Golden Hill Precise Plan Transportation Recommendations, stated that the forecasted volumes on Fern Street and 30th Street can be accommodated on the existing network. The memo further states that Transportation and Traffic Engineering would have no objection if it was the desire of



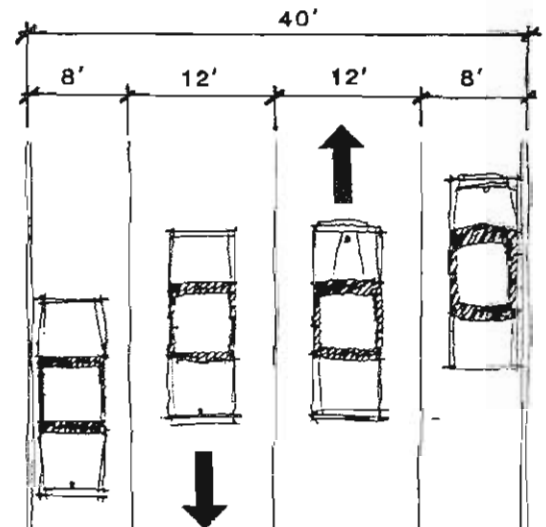
PEDESTRIAN CONCERNS



30TH



FERN SOUTH

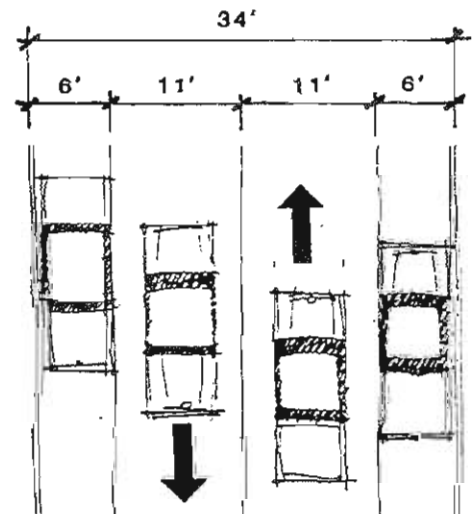


9B

FERN SOUTH/FERN NORTH

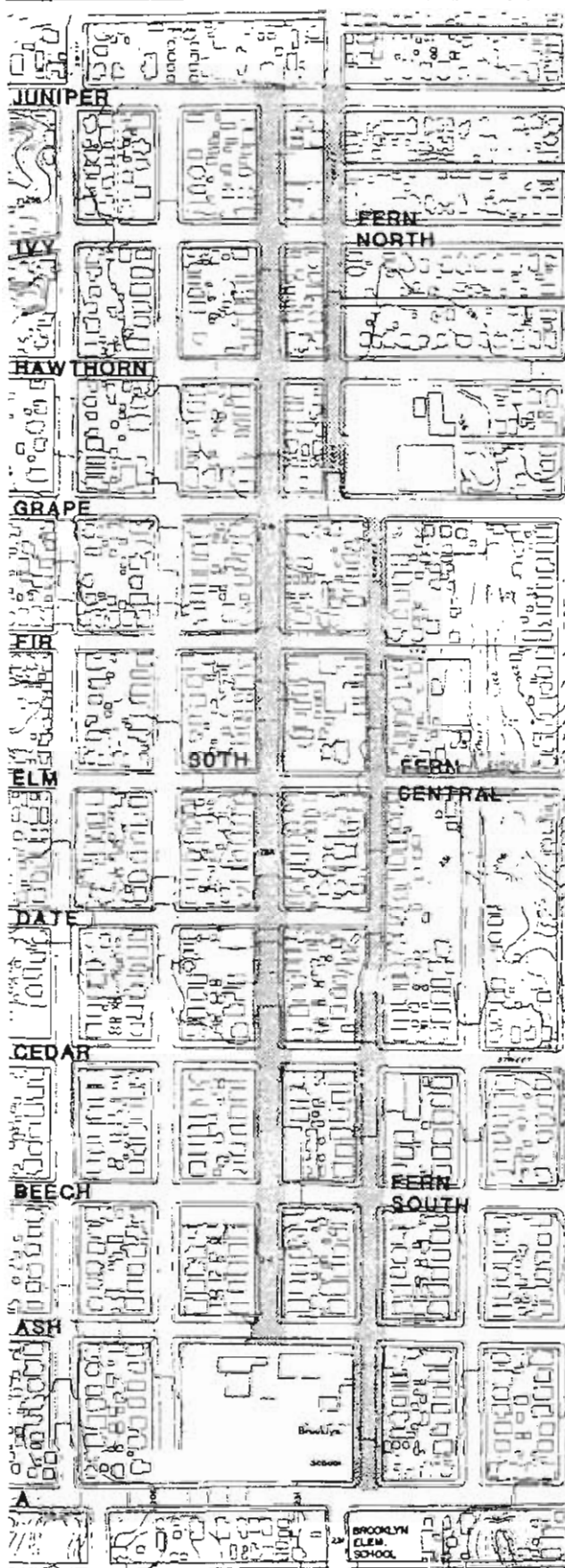


FERN CENTRAL



9C

FERN CENTRAL



STREET ZONES

FIGURE 9

the community to implement two one-way north-bound lanes on Fern Street, and two south-bound lanes with one north-bound lane on 30th Street.

The proposal for adding a lane on 30th Street was rejected by the Community Task Force early in the workshop process. Additionally, as the workshops progressed, the issue of pedestrian safety heightened. Although the one-way pairing system would make traffic flow through the neighborhood more efficient, the perception of increased vehicular speed brought on by the concept of non-opposing traffic became a priority concern. This concern eventually led to a rejection of the one-way pairing concept by the Community Task Force at the July 15th workshop. The direction, however, given at the July 15th workshop was that future design studies should continue to take into account issues concerning traffic speed and pedestrian safety along Fern Street and 30th Street.

Removal of parking on certain streets and the removal of parking near intersections to improve visibility is another traffic improvement recommendation. During the workshop process, comments by residents concerning on-street parking were mixed. Although the majority of respondents to the opinion survey advocated the goal of improving parking conditions based on safety concerns, several individuals commented on the need to in-

crease parking opportunities in the neighborhood. Village-style parking, or diagonal parking, currently exists on several streets within the study area and was mentioned in the opinion survey as a possible model for increasing parking options.

An evaluation of the study area illustrated that parking as a safety concern is most prominent along Fern Street, with special emphasis placed on the segment between Grape Street and Cedar Street. Figure 8 illustrates varying street zones within the study area. The street segment zones have been classified as distinct areas based on a combination of existing and potential design characteristics. The drawings to the left of the street zone map are plans of the individual street zones. Fern Central represents the street zone between Grape and Cedar Streets, Figure 9.

As shown in the illustrations, Fern Central presents the narrowest street cross-section of the four zones. The curb to curb width of 34 feet is less than the recommended City of San Diego Design Standard for collector streets. Of greater significance is the current striped dimension for parallel parking. The six foot width has been expressed by residents adjoining the street as being too narrow. Entering and leaving a vehicle often places the driver and passenger within the adjoining travel lane. Cur-

rently, residents along Fern Central have been forced to partially park on the adjoining sidewalk or suffer the possibility of having their vehicle sideswiped. Damage to vehicles is not uncommon along the street zone, as has been attested to by several respondents in the opinion survey.

Bicycle traffic further compounds the issue of safety and congestion on Fern Street. Being the most convenient, and thus primary, route through the neighborhood, bicyclists, like motorists, are prone to using Fern Street. The street's narrow dimension coupled with its volume of vehicular traffic, however, has created an unfavorable environment for bicyclists. An expressed desire exists on the part of residents for more convenient, and especially safer, bicycle pathways within the neighborhood than presently exists.

## VISUAL QUALITY

### THE PUBLIC REALM

One respondent to the opinion survey described the current image of the neighborhood as "a bad foot forward for the traffic passing through". A majority of the residents and workshop participants agreed with the comment, but an equal number acknowledged that the "image will improve when the neighborhood is cleaned up".

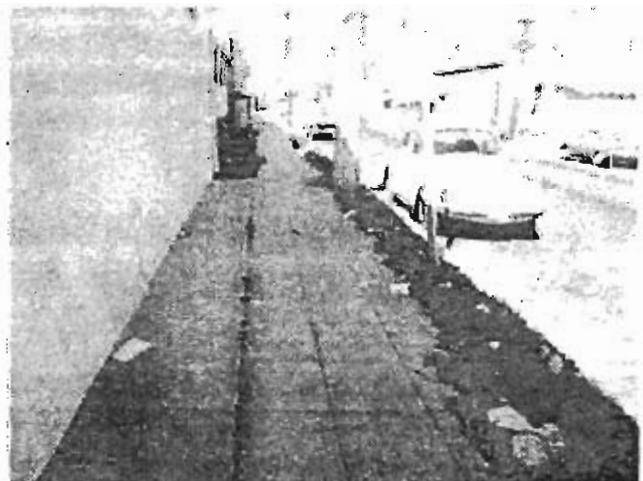
The term "cleaned up" voiced during the workshop process often implied a general improvement in the neighborhood's visual quality. Visual quality is the apparent physical condition of the neighborhood's public and private areas, such as streets, homes, and businesses. For example, recurrent complaints about public areas needing improvement involved cracked or broken sidewalks, lack of street trees, weeds and litter in the public right-of-way, poor street lighting, or unattractive street fixtures such as bus benches. Litter is a particularly annoying issue for residents as evidenced in the opinion survey by the numerous requests for more trash containers, especially at bus stops, Figure 10.

The overhead utility lines on Fern Street and 30th Street is the most conspicuous visual eyesore in the public realm according to the workshop participants. The 12,000 volt (12 KV) lines are the most visible service lines and are located on the east and west sides of

Fern Street and 30th Street respectively. The 12 KV lines which provide local electrical service from San Diego Gas and Electric Company (SDG&E) are supported by 35-foot high wooden poles. Pacific Bell Telephone and Cox Cable also share usage of the poles.

Located only on Fern Street are the 69,000 volt (69 KV) SDG&E tower transmission lines. The 69 KV lines are supported by 50-foot high poles and are prominent visual elements against the skyline. The wider spacing of the 69 KV poles and their light grey color at times makes them less obtrusive than the darker 12 KV poles, Figure 11.

The undergrounding of the overhead utility lines coupled with the removal of the poles was



"A BAD FOOT FORWARD"

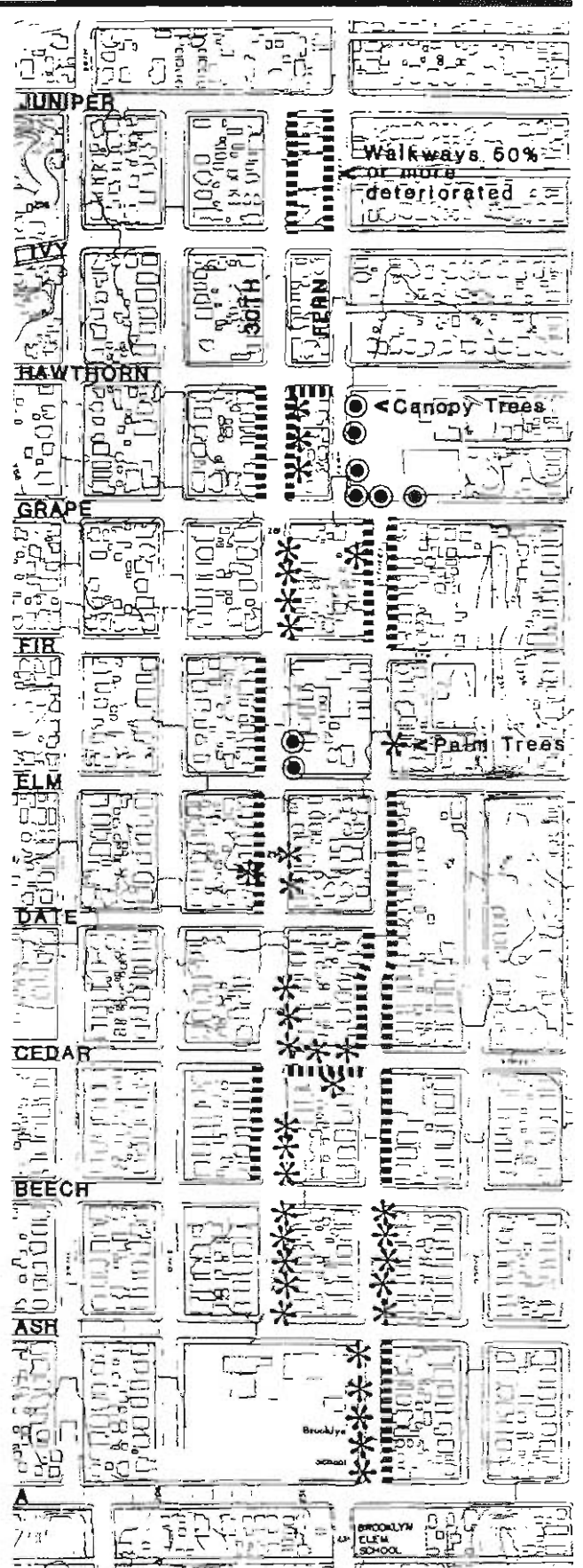
often mentioned during the workshop as a first step in the street improvement program.

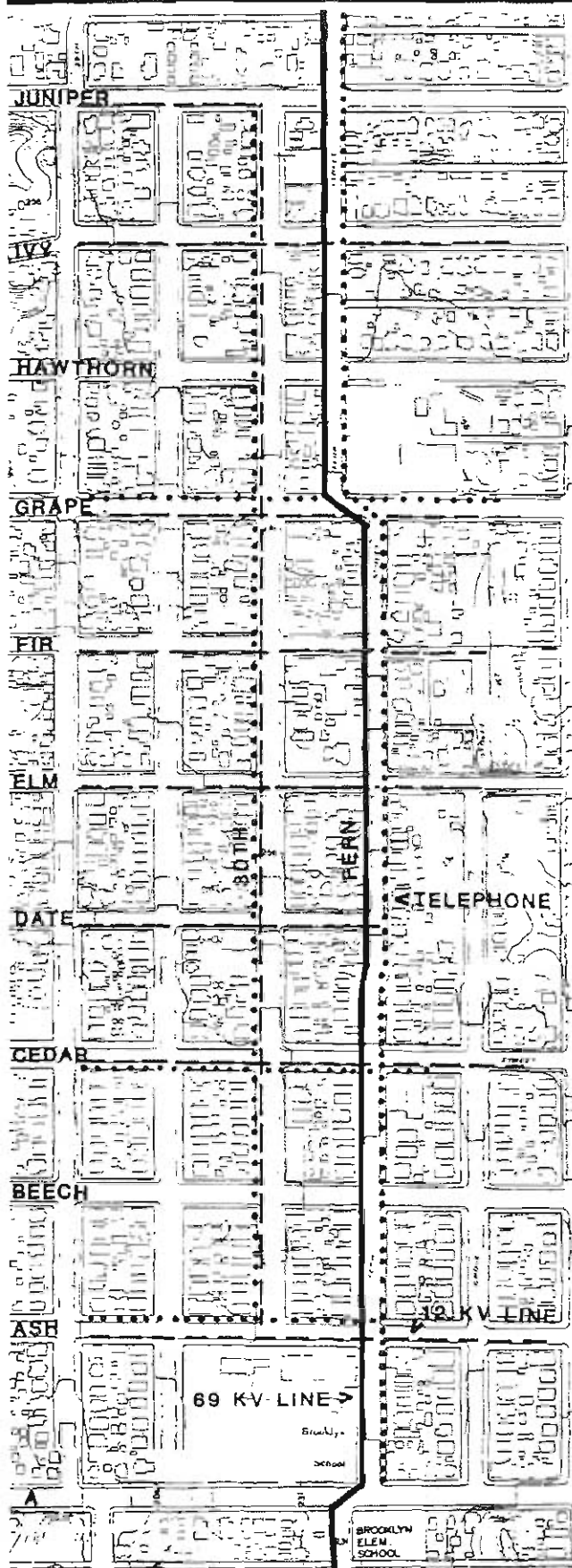
The billboards along Fern Street are another noticeable, and perhaps inappropriate, feature along the skyline. The billboards were commonly mentioned as a source of irritation to residents during the workshop process. Negotiations have begun for the removal of two of the four billboards between Community Task Force members and the owners of the billboards.

Although not mentioned by workshop participants or respondents to the opinion survey, on-street parking is a significant issue within the study area. Beyond passage through the neighborhood, the streets within the immediate vicinity



BILLBOARDS





OVERHEAD UTILITIES

FIGURE 11

of the study area serve other purposes as well. Parking by residents and visitors to the neighborhood is the most obvious related use. Service vehicles such as delivery vans, are also prominent users of Fern Street and 30th Street. An unlikely third element is the parked cars being serviced by several auto repair facilities located along Fern Street and 30th Street.

The removal of on-street parking is a sensitive issue with neighborhood residents and merchants. However, the combination of increasing vehicular traffic, limited on-street parking spaces daily service deliveries to merchants, and roadside car repairs has created a street scene that is not conducive to maintaining a residential atmosphere.



UTILITY POLES



## THE PRIVATE REALM

Regulating the visual quality of a public street is an easier task than controlling how a property owner will maintain his or her private lot. The street, as the public realm, can be improved or upgraded based on the wishes of the community, assuming the funds are available. The appearance of homes and businesses along Fern Street and 30th Street are dependent solely on the care and concern of property owners. This is a significant issue with community residents, almost to the point of their feeling that the fate of the entire neighborhood rests with those who either own property or reside along Fern Street and 30th Street.

An informal survey of homes and businesses was conducted along Fern Street and 30th Street with an approximately equal number rated in either good or fair condition. A building in good condition meant that the structure was well maintained, and with landscaped grounds in need of some careful maintenance. The majority of homes in the neighborhood, approximately 73 percent, were built prior to 1950. Fair condition equated to a building in good maintenance, but the grounds were in need of repair. Insufficient maintenance of front yards is a common visual quality problem for homes along Fern Street and 30th Street.

Several buildings were rated as poor, and the majority of these were located on Fern Street, in particular, the Fern Central area. A poor structure was classified as a building in need of a lot of exterior care, paint, landscape, new roof, etc. One building was listed as being in bad condition, being that structure was essentially boarded up.

Adding to the overall visual quality problems of the neighborhood are several business establishments that might be considered inappropriate in their use and location. Much of the existing commercial activity in the neighborhood was developed under the earlier C (Commercial) Zone regulations which do not require off-street parking, setbacks, landscaping, or screening of outdoor storage areas. The result has been that several commercial developments have evolved which are incompatible with adjacent residential uses. Examples of this situation are automobile paint and repair facilities with unscreened storage yards, bars, and 24-hour laundromats. Two automobile paint repair facilities currently are operating in the study area in direct violation of the municipal zoning code. The two establishments opened subsequent to the 1980 rezoning from C to CC (Community Commercial).

## REINVESTMENT

### RESIDENTIAL AND COMMERCIAL

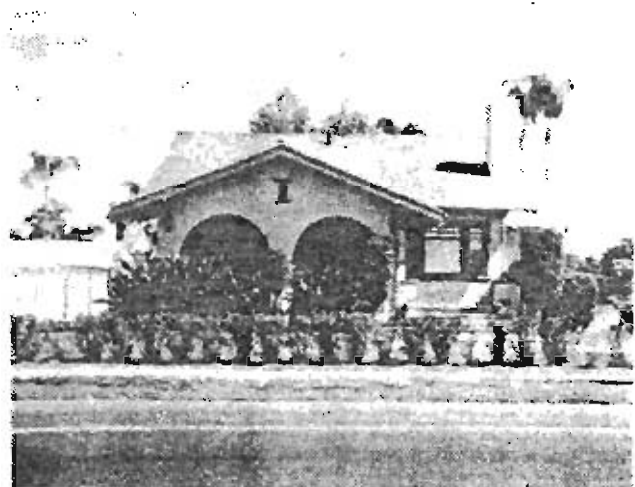
Re-use and reinvestment activity within the neighborhood has been limited primarily to residential property. Increased improvement can be seen in homes that are located away from Fern Street and 30th Street. This analysis is not surprising, considering the traffic and visual quality issues associated with Fern Street and 30th Street. Interviews with residents, merchants, and real estate brokers verify that the key to an overall revitalization of the neighborhood is physical improvements in the study area.

The neighborhood's proximity to Downtown San Diego and Balboa Park, good transit, and an existing stock of habitable older homes, are often cited by residents as valuable assets to the neighborhood. The price of existing homes is also an asset of the neighborhood to prospective home buyers. Current prices for single family structures averages approximately \$95,000. The 1980 census lists the median market value of homes in the area as \$71,800. A six percent annual increase is considered good for the area. Most real estate agents interviewed indicated optimism in the neighborhood's residential resurgence.

Beyond a growing precedence for home ownership in the area, the neighborhood maintains a high degree of rental property. Recent statistics reveal an al-

most two-to-one ratio of rentals to homeowners in the study area. Although not verifiable, interviews with residents indicate that the negative image associated with Fern Street and 30th Street is mainly attributable to the large proportion of absentee landlords in the area. Discussions with real estate brokers indicate that property owners are hesitant to improve rental property along Fern Street and portions of 30th Street, for two reasons. The lack of optimism in future public improvement along the two streets was mentioned as one cause. A second reason is the present uncertainty that exists with apartment builders over revisions to the federal tax code.

There are two commercially zoned districts within the study area. The Beech Street district is located along 30th Street and Fern Street from

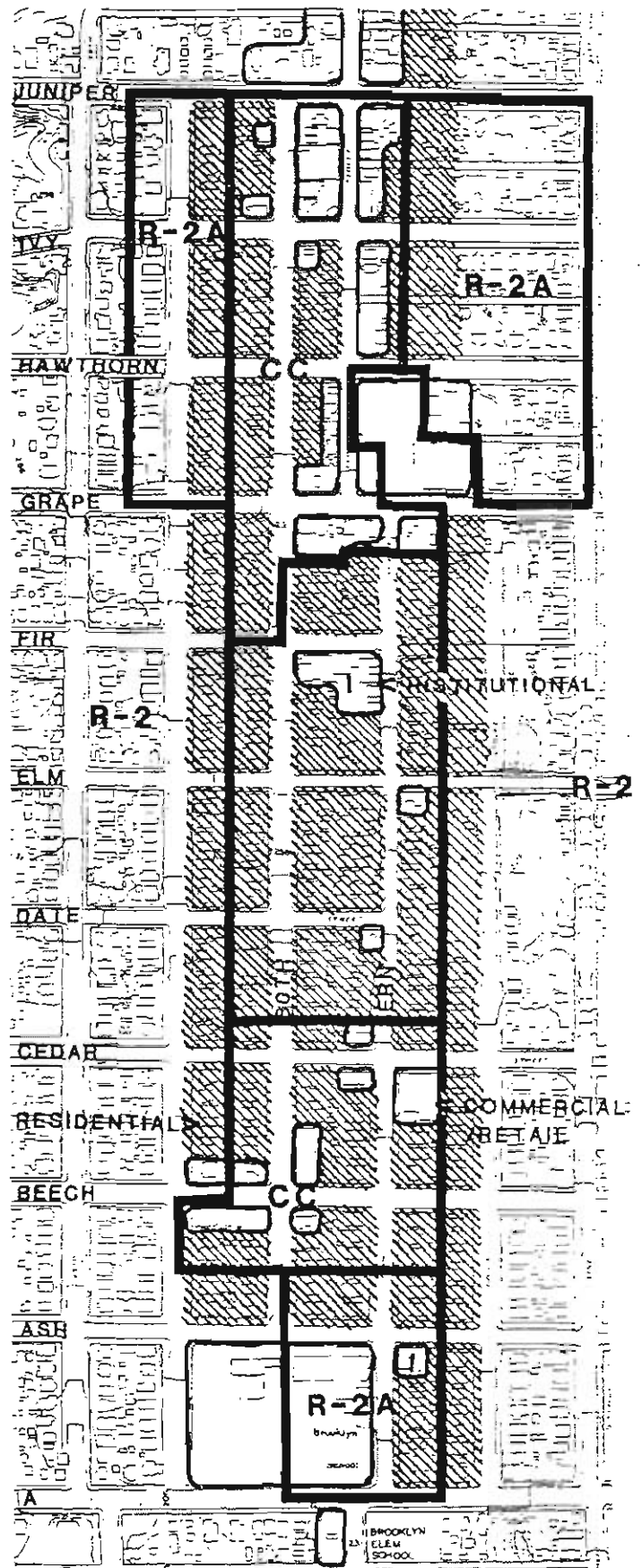


EXISTING RESIDENTIAL STOCK

Cedar Street to Beech Street. Approximately 17 small commercial activities are located here, including a drug store, food market, and catering business. The Peach Tree, a catering business, has been cited as a recent positive sign of an improvement in the Beech Street district. The establishment, which primarily services its customers outside the Golden Hill area, has become something of a landmark in the area. New paint, store front displays and a unique mural are new improvements to the existing building. Figure 12.

The Grape/Fern Street district is the second, and larger, commercial district within the study area. The boundaries of the district include both sides of Fern Street and 30th Street from Juniper Street in the north, to Grape Street in the south. Approximately 36 commercial activities are in operation here, including a supermarket, restaurants, liquor stores, and professional offices. A number of auto paint and repair facilities exist in the district. The establishments, which are incompatible with adjacent residential uses, are remnant uses that existed in the area prior to the re-zoning of commercial property in 1980.

Opportunities for increased commercial retail activity in the area are strong. The renewed interest in the neighborhood exhibited by recent residential real estate activity is



ZONING

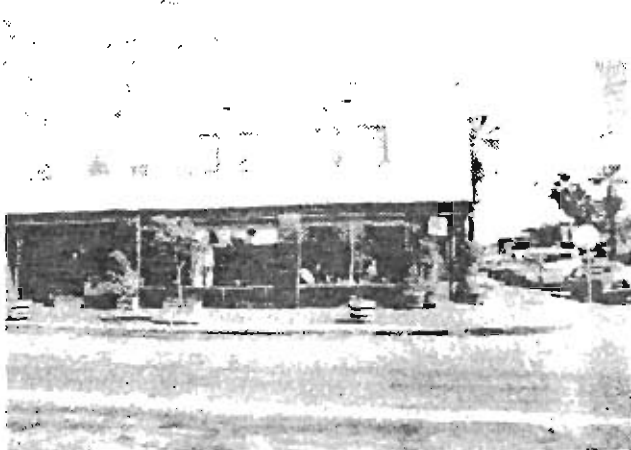
FIGURE 12

evidence of a general improvement. Rick Epstein, a refurbisher of antique cars and a tire distributor, voiced intentions to rehabilitate several buildings in the 2200 block of 30th Street. One of the buildings is a fire station and the old Burlingame Garage. The visual quality and traffic issues associated with Fern Street and 30th Street, as well as the existing incompatible uses, however, are seen by merchants as a major roadblock to recovery.

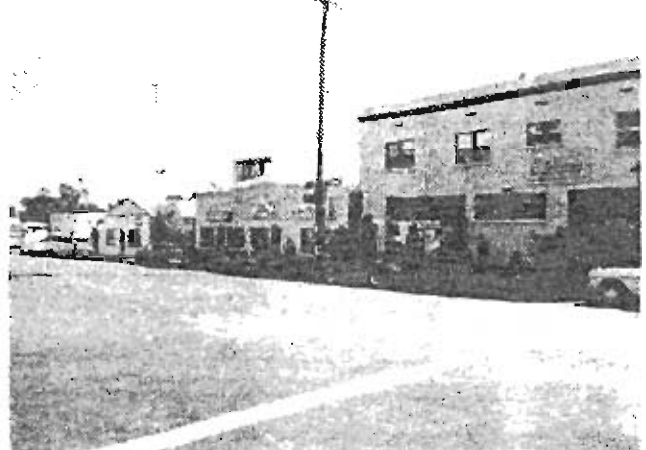
Residents, as well, are dismayed by the current state of commercial services in the neighborhood. Comments from the opinion survey about improving existing commercial services centered on two issues. Improving existing food market facilities was the most prominent issue. Specific criticisms concerning quality of products or cost were not mentioned. Most comments by respondents simply called for "clean up and updating of existing services or facilities".

Opinions about types of retail facilities form the second group of commercial service issues. In general, provisions for more neighborhood-oriented shopping facilities was a prominent request. Hillcrest, Mission Hills, and North Park were mentioned as shopping districts representative of the scale and quality desired in the community. The following commercial uses are a sampling of services requested by the respondents in the opinion survey.

Bakery  
 Dry Cleaners  
 Bank  
 Drug Store  
 Larger Supermarket  
 Ice Cream Shop  
 Flower Shop  
 Gift Shop  
 Small Restaurants  
 Fast Food Store  
 Service Station  
 Theater  
 Produce Store  
 Health Food Store  
 Newsstand  
 Shoe Repair



30TH AT BEECH



GRAPE STREET

## SUMMARY OF WORKSHOP ISSUES/RECOMMENDATIONS

The following section is a summary of major issues or concerns voiced by community merchants and residents during the needs assessment phase of the study. The adjacent general recommendations are responses to major issues by residents and merchants. The recommendations were used in creating the framework and objectives of the Design Plan and Action Plan that follows.

### ISSUE

### RECOMENDATION

#### TRANSPORTATION

1. Traffic conditions along Fern Street and 30th Street deter pedestrian movement within the study area.

1. Incorporate traffic control devices and/or develop traffic management techniques throughout the study area in order to facilitate safer pedestrian movement.

Maintain the existing two-way traffic flow patterns on Fern Street and 30th Street in order to reduce driver temptation to increase vehicular speed.

2. The existing parallel parking pattern along the Fern Central segment of Fern Street presents an unpleasant condition for motorists, as well as deteriorates the adjacent walkway area.

2. Remove on-street parking from one side of Fern Street along the Fern Central segment only. The additional space gained from the removal of one side of parking can be applied to widen the two existing travel lanes and remaining single parking lane. Additional parking spaces for the approximately 80 spaces lost on Fern Street should be sought on adjacent streets such as Grape, Fir, Elm, Date, and Cedar.

## ISSUE

3. The lack of on-street parking spaces within the commercial district is a constraint to future commercial revitalization.
4. Accommodations associated with alternative modes of transit, such as buses and bicycles, are deficient.

## VISUAL QUALITY

1. The deteriorated state of public walkways and landscape areas along Fern Street and 30th Street are a detriment to future reinvestment in the study area.
2. The inferior maintenance of certain private yard areas and buildings further aggravates the depressed visual quality of the neighborhood along Fern Street and 30th Street.

## RECOMENDATION

3. Increase parking opportunities within the study area by exploring alternative methods for on-street parking. Determine the feasibility of creating additional village-style (diagonal) parking spaces where appropriate.
4. Dedicate more convenient and easily accessible bicycle routes within the neighborhood. Provide more comfortable accommodations (seating, trash receptacles, shelters) for bus riders.
1. Improvement of the public streetscape image should be a priority concern of an overall neighborhood conservation effort. Recommendations should focus on: methods for improving varying sidewalk surface conditions, street tree selection and location, lighting fixture types, bus shelter design, and other street furniture hardware. A possible theme should be identified that is representative of the neighborhood's past and current image.
2. Move active neighborhood involvement in policing the appearance of private yards, homes, and businesses is required. A neighborhood task force group should be formed to focus specifically on this issue. An information brochure should be developed to assist property owners in improving the appearance of yards and buildings.

## ISSUE

3. The billboards and electrical transmission lines on Fern Street and 30th Street remains a major obstacle in guaranteeing the total improvement of study area's street scene.
4. Beyond adding to traffic congestion in the study area, the continuous line of on-street parked cars and service vehicles is a hindrance to creating a neighborhood village atmosphere within the study area.

## REINVESTMENT

1. The absence of an organized commercial revitalization program will prolong the study area's perceived decline.

## RECOMENDATION

3. Ongoing discussions with City staff and outdoor advertising agencies should continue. A neighborhood task force group should be developed to lobby City agencies in accelerating the process for removing the overhead utility lines.
4. Develop alternative parking patterns within the study area in order to increase opportunity for landscaping or other aesthetic treatment.

1. Organization of property owners and businesses in the study area is another first step in launching a successful revitalization program. The program should involve a coordinated effort involving streetscape improvements, storefront rehabilitation, and methods for attracting new development opportunities.

A community coordinator should be engaged by the City to organize, implement, and direct the program.

**ISSUE**

2. Efforts to attract reinvestment proposals to the neighborhood will not occur without some assurances of a long-term commitment to maintaining the quality of public and private improvements.

Review service delivery requirements of merchants within the study area. Allocate specific loading zones for service vehicles.

**RECOMENDATION**

2. Design standards for future development need to be adopted so as to maintain the unique urban design quality of the neighborhood.

City legal action is needed to remove nonconforming land uses that harm adjacent residential areas.



## A. DESIGN PLAN

In 1867, when Alonzo Horton arrived in young San Diego, he had a vision of how 960 acres of sagebrush could become a bustling city. Almost forty years later, in 1906, E. Bartlet Webster had a vision of how open land just east of City Park could become a pleasant residential district to house the people who had come to enjoy the realities that had developed from Horton's vision.

Now, one-hundred and twenty years after Horton bought his first property in San Diego, a new generation of San Diegans in Webster's South Park Addition have a vision which builds on the dreams-turned-to-reality of those before them. This new vision has been discussed and described in the previous chapter, Images and Issues. While the discussion mentioned vague concepts (like pedestrian safety) and specific issues (like cracked sidewalks), the real heart of the matter is a comprehensive image of what the neighborhood will be.

To translate, clarify, and understand the nebulous qualities of the vision, and to begin building the bridge from desire to reality, we have developed a Design Plan. The Design Plan is simply an image of the end result; the fruition of the vision. It incorporates both desire and potential; it was started at the beginning of the project with the initial community meetings and surveys, but could not be completed until in-depth analysis and



DESIGN PLAN

FIGURE 13

design had been done. When considering the depth of the residents' vision, the size of the neighborhood, the many components of the Action Plan, and the scale of the Plan as reproduced in this document, much of the Plan would remain vague and unattainable without further explanation, Figure 13.

The design plan seeks to recreate the image of the neighborhood as it was originally envisioned. This will be accomplished through several inter-related design concepts. They are:

- \* Removal of overhead power lines and billboards.
- \* Planting of canopy type street trees on Fern Street and all east/west streets and vertical accent trees on 30th Street.
- \* Installation of new pedestrian scale street lighting and street furniture such as benches and trash receptacles.
- \* Reconstruction of existing concrete sidewalks and addition of enhanced paving between sidewalk and curb along Fern Street and portions of 30th Street and cross streets.

- \* Redesign of the pedestrian and traffic areas of Grape Street and Beech Street between 30th and Fern creating two concentrated commercial centers.
- \* Relocation of parking along Fern Central to allow for a better flow of traffic along Fern Street and the creation of "woonerfs" in certain cross streets.
- \* Installation of additional traffic controls, relocation of some bus stops, minor rerouting of the bus line, and one-way traffic on some cross streets for convenience and safety.
- \* Inclusion of design guidelines and administrative controls for coordinated redevelopment of private property.

By dissecting the Design Plan into its various components, an Action Plan for implementation is created. When each component of the Action Plan has been completed, the vision will have come full circle and the Design Plan will no longer be a description of a dream, it will be a picture of reality, a testament to the foresight of this, and previous, generations of San Diegans.

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## B. ACTION PLAN

The previous section, establishes a vision of what Fern Street and 30th Street could be some day. Guaranteeing that the vision will be in place will require diligence and dedication on the part of residents and merchants, to follow a "blueprint" to guide the revitalization process.

The Action Plan, Figure 14, is the blueprint for guiding the revitalization effort. The Plan is organized under four main issue headings which were substantially developed during the inventory and needs assessment phase of the revitalization plan work effort. The main issue headings are:

- \* Transportation
- \* Visual Quality
- \* Reinvestment

Under each heading is a series of individual Action Plan components. Each component is intended to represent a specific action by City agencies, local merchants and/or resident groups that need to occur in order to implement the Design Plan. Funding sources and programs are identified and, in one case is, at this time, committed. Improvement programs are discussed in terms of their individual value to the overall revitalization strategy. Timing and steps to implementing Action Plan components are also discussed.

The Action Plan has been segregated into eight individual projects or programs to make

this document easy to use and understand for reviewing public agencies, merchants, and residents of the community.

Several of the Action Plan components can only begin at the initiative of the merchants and residents of Fern and 30th Streets. Thus, identifying specific individual tasks and describing the why, who, and how of completing the task is seen as the most important chapter of the Fern Street and 30th Street Improvement Program.

LEGEND

TRANSPORTATION

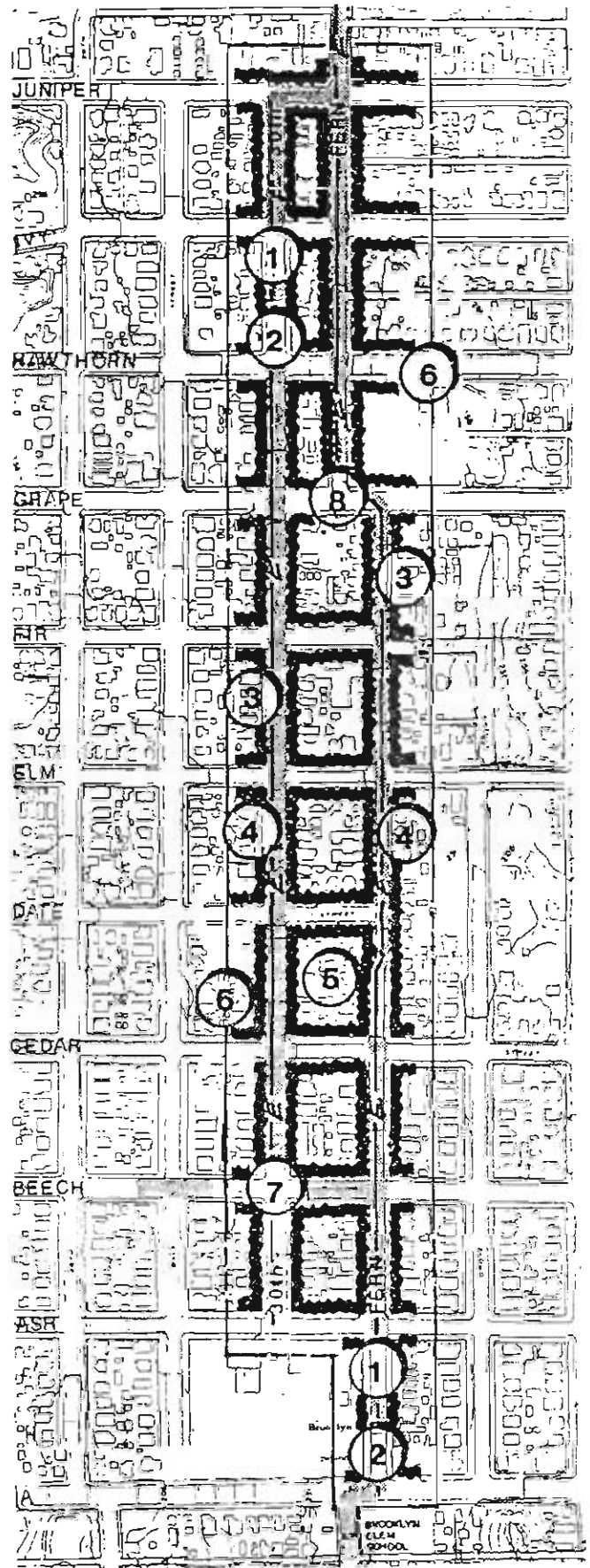
- ① CIRCULATION & PARKING
- ② TRANSIT

VISUAL QUALITY

- ③ STREETSCAPES
- ④ OVERHEAD UTILITIES
- ⑤ BILLBOARDS
- ⑥ IMPROVEMENTS/  
RECOMMENDATIONS

REINVESTMENT

- ⑦ BEECH STREET CENTER
- ⑧ GRAPE STREET SQUARE



ACTION PLAN

FIGURE 14

## TRANSPORTATION

1

### CIRCULATION and PARKING

Resolving existing transportation issues are the most important components of the overall Action Plan for the study area. As discussed in the Summary of Issues And Recommendations, the chaotic nature of present traffic flow and parking conditions creates an image of Fern Street and 30th Street as an uninterrupted channel designed for exclusive use of the motorist. The goal of the circulation component of the Action Plan is to create, in the drivers' and residents' minds, a definite image that the streets and surrounding neighborhood are a stable place for resident activity rather than merely a channel for drivers.

Creating the image of a stable place for human use along Fern Street and 30th Street begins with the incorporation of several circulation management techniques within the context of the overall Action Plan. Specific management objectives identified for Fern Street and 30th Street are:

- \* Increasing on-street parking opportunities where appropriate
- \* Protection of pedestrians at busy intersections
- \* Improving walking conditions for pedestrians
- \* More space for pedestrian street activity
- \* Attention to bicyclists
- \* Improving street appearances

All of the above circulation management objectives identified fulfill the intent and purpose of the basic goal, plan concepts, and objectives of the Transportation Element of the Golden Hill Precise Plan.

It is important to keep in mind the individual circulation management objectives identified for Fern Street and 30th Street. It is the belief of the community that the goal of an improvement program is not to achieve the best possible traffic solution. The goal of the improvement program is to achieve the best comprehensive revitalization solution including a traffic component that works. The following section discusses the required circulation and parking improvements of the Action Plan.

- \* Balancing the traffic flow between 30th Street and Fern Street
- \* Reducing vehicular speeds through optical design
- \* Improving on-street parking conditions

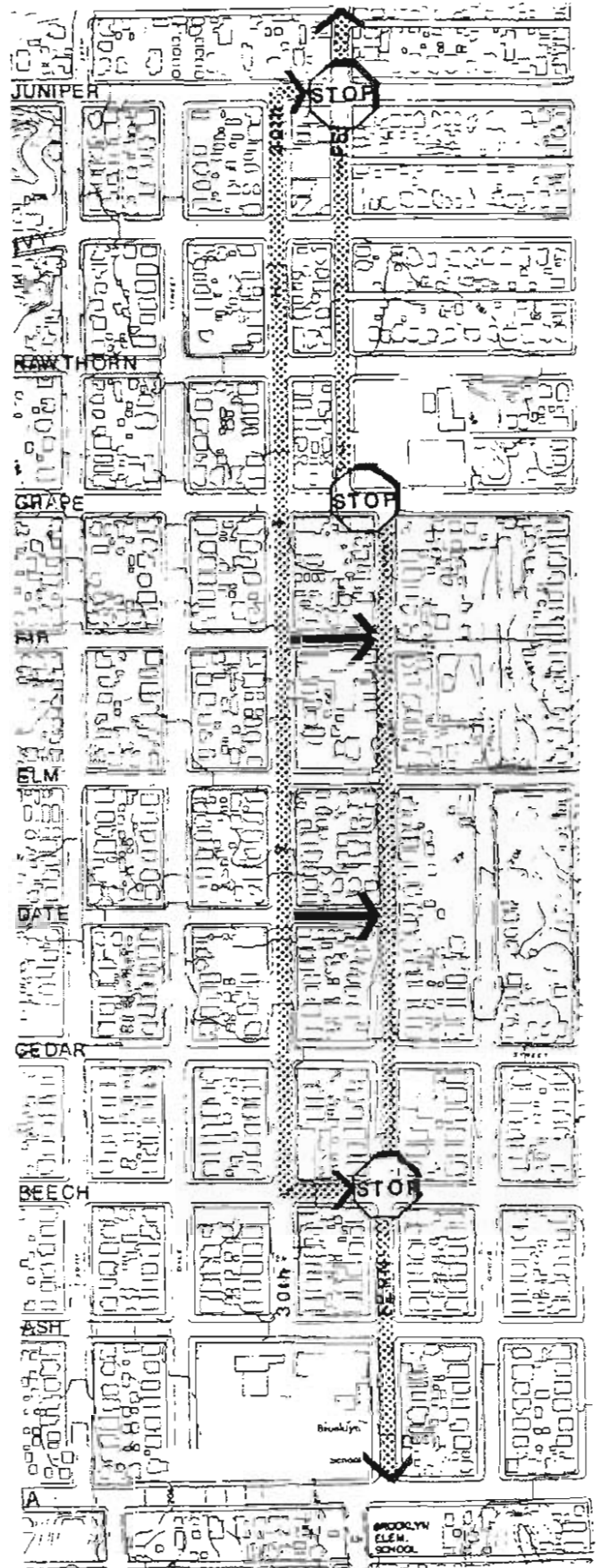
## TRAFFIC IMPROVEMENTS

Implementing the circulation management objectives requires a coordinated approach to solving current traffic problems within the study area. The reduction of vehicular flow conflicts, at the expense of pleasant and safe pedestrian movement, defeats the purpose of a streetscape improvement program. Conversely, undue restrictions on vehicular movement through and within the study area can cause traffic congestion which could have a negative effect on commercial revitalization efforts.

Figure 15 is a comprehensive strategy for pedestrian and vehicular circulation improvements along Fern Street and 30th Street. The plan has the following objectives:

- \* MAINTAIN 30TH STREET AND FERN STREET AS TWO-WAY STREETS. -- The greater Golden Hill 1987 Precise Plan recommends a one-way north/south pair between Grape Street and Beech Street. Subsequent studies, however, by the Traffic and Engineering Department, as well as the Planning Department, recommend that this proposal be removed from the current Precise Plan since forecasted volumes can be accommodated on the existing road network.

Research by the consulting group further validates the



CIRCULATION IMPROVEMENTS

City's recommendations. Maintaining the existing two-way flow reduces driver temptation to increase vehicular speed typically associated with one-way streets.

- \* PROVIDE AN ALL-WAY STOP AT THE INTERSECTION OF FERN STREET AND GRAPE STREET, AND CONSIDER THE INSTALLATION OF ADDITIONAL ALL-WAY STOPS AT FERN STREET AND JUNIPER STREET, AS WELL AS FERN STREET AND BEECH STREET. -- Pedestrian crossing at the intersection of Fern and Grape can be described as chaotic at best, particularly in the morning and evening rush-hour period. Although the intersection does not meet all the criteria outlined by the City Traffic and Engineering Department for an all-way stop, a combination of related conditions create the need for greater control at the intersection. These conditions are illustrated and described more fully in Figure 18. This figure shows recommended locations for stop signs as well as curb realignment locations.

The Fern and Juniper and the Fern and Beech intersections do not warrant additional traffic control, based on preliminary investigations. However, as part of an overall circulation improvement strategy, all-way stops are recom-

mended at these intersections for two reasons. Stop signs along Fern Street, at Beech Street and Juniper Street, are intended to redistribute some of the daily traffic from Fern Street to 30th Street. Fern Street is currently operating at 199 percent of the recommended capacity of its street classification. A certain proportion of drivers traveling in the district may find it more convenient to turn at Juniper, or Beech, to use 30th Street instead of Fern Street. A second reason for all-way stops would be to facilitate greater ease of pedestrian crossing at these important community nodes. The intersections at Juniper, Grape, and Beech are the hearts of their respective commercial cores, and thus have the highest number of pedestrian crossings.

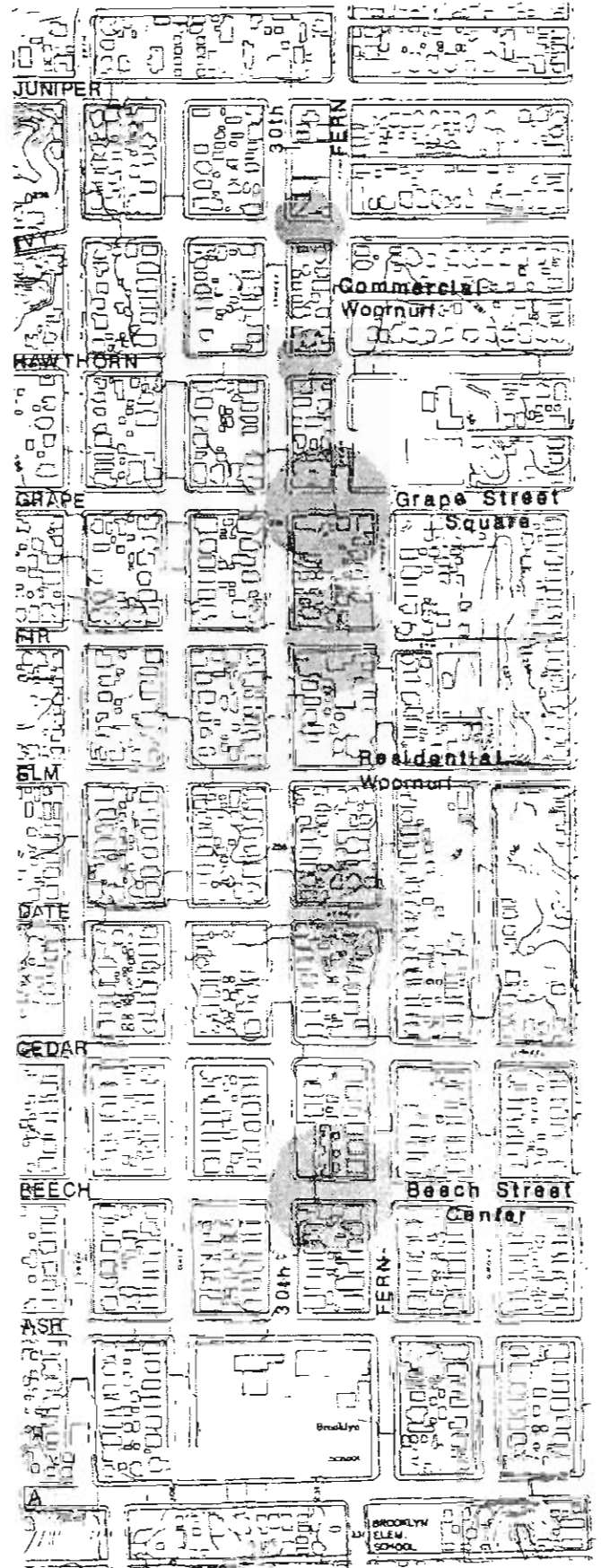
- \* DESIGNATE FIR STREET AND DATE STREET (BETWEEN 30TH AND FERN) AS ONE-WAY EAST BOUND STREETS. -- Accident rates along Fern Street between Cedar Street and Juniper Street are approximately 13.6 percent higher than the citywide norm for collector streets. The incidents of intersection versus non-intersection accidents are about equal. An analysis by the City of San Diego Traffic and Engineering Department indicates that accidents in-

volving vehicles making a left turn in front of oncoming traffic, and accidents involving vehicles being struck in the rear while waiting to make a left turn account for a significant amount of intersection accidents in the Golden Hill Precise Plan Area.

The designation of Fir Street and Date Street to one-way east-bound streets is designed to limit the number of left turn movement opportunities along Fern Street, thus reducing the incident of intersection accidents.

The one-way designation for Date Street, between 30th Street and Fern Street only, is also designed to reduce through traffic to the golf course along what is essentially a narrow residential street. As part of this concept, the street sign marking the direction to the golf course at Date Street should be moved to the Beech Street and Fern Street intersection.

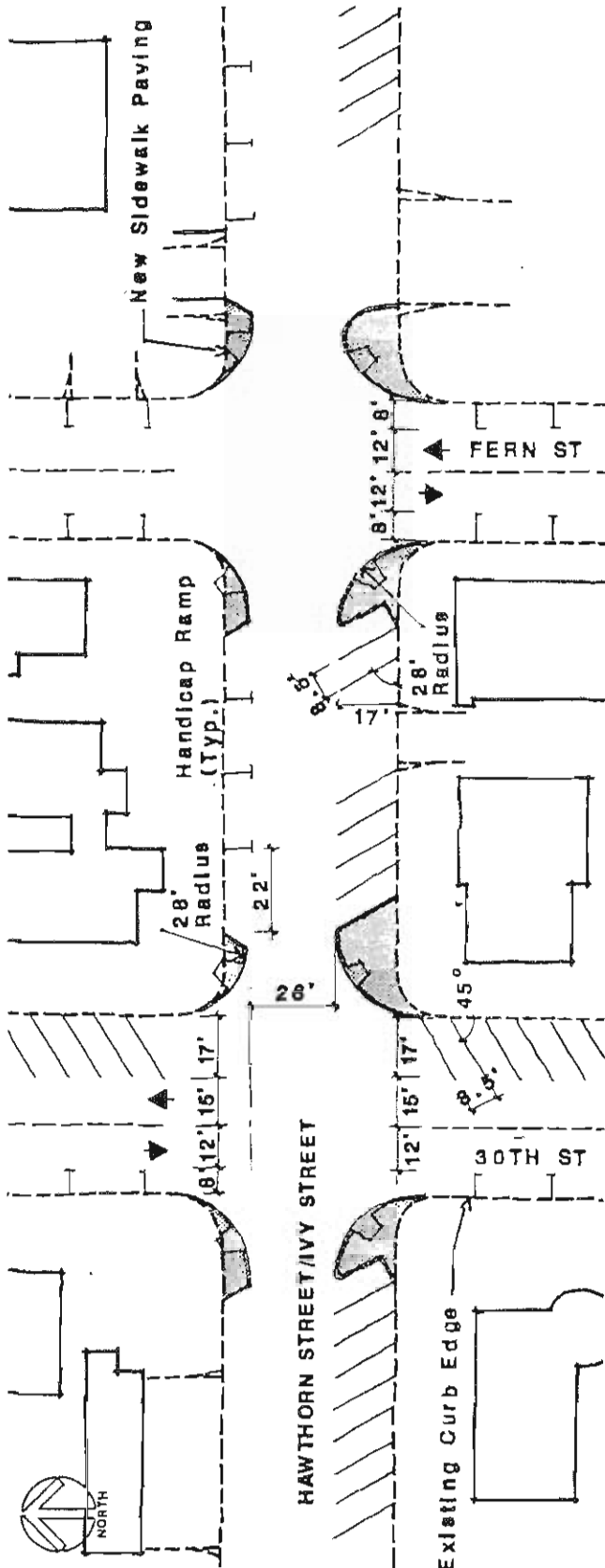
\* REDESIGN THE OFFSET AT THE INTERSECTION OF FERN STREET AND GRAPE STREET TO ALLEVIATE PEDESTRIAN AND VEHICULAR CONFLICTS. -- The curbs at Fern Street and Grape Street were cut back many years ago to provide easier vehicular movement and to facilitate the movement of



CURB/PAVEMENT MODIFICATIONS

FIGURE 16





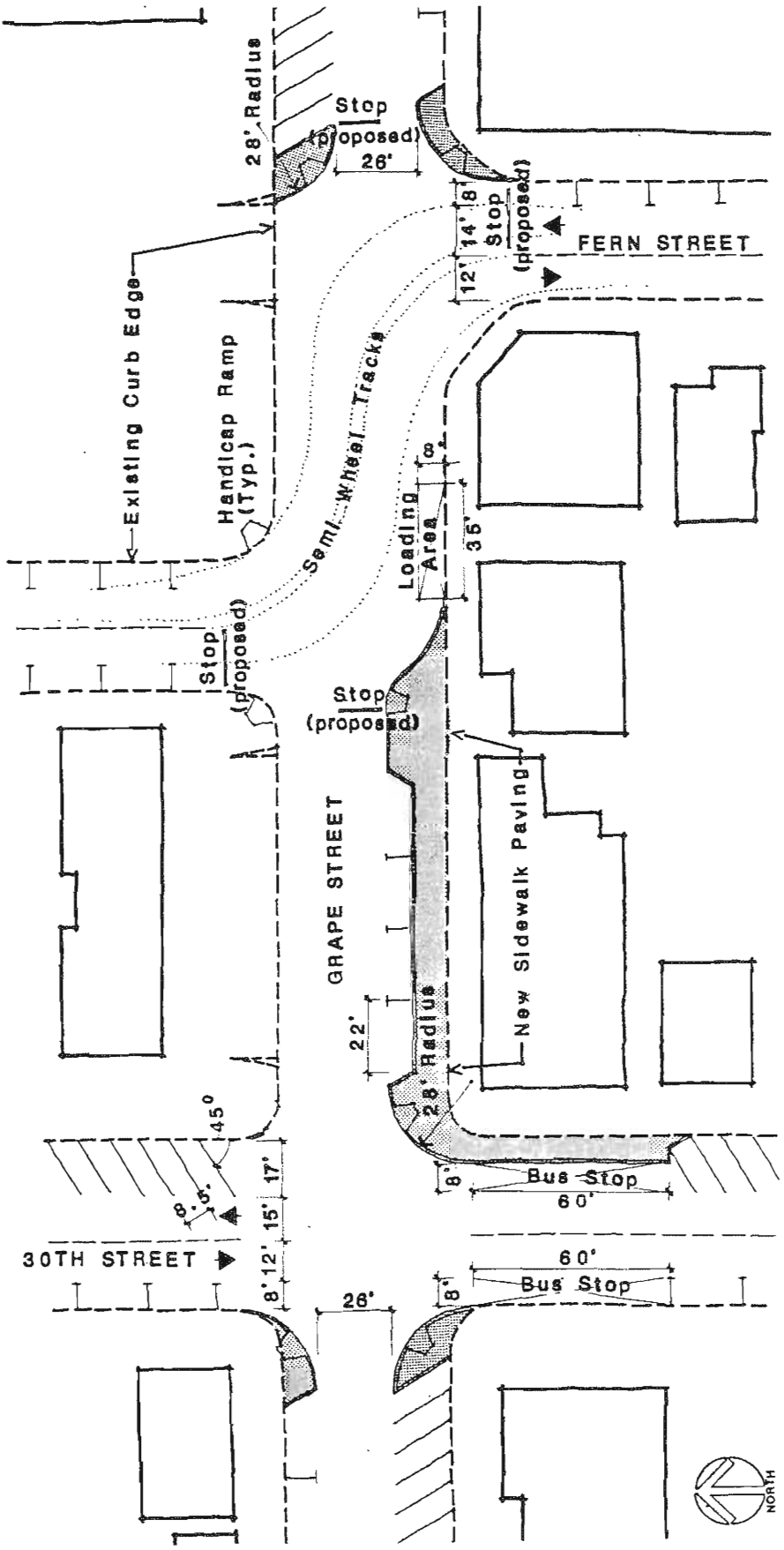
COMMERCIAL WOORNURF

FIGURE 17

larger trucks through this offset intersection. However, over the ensuing years, traffic volumes, as well as vehicular speed, has increased to the point of creating a dangerous intersection for cars and pedestrians.

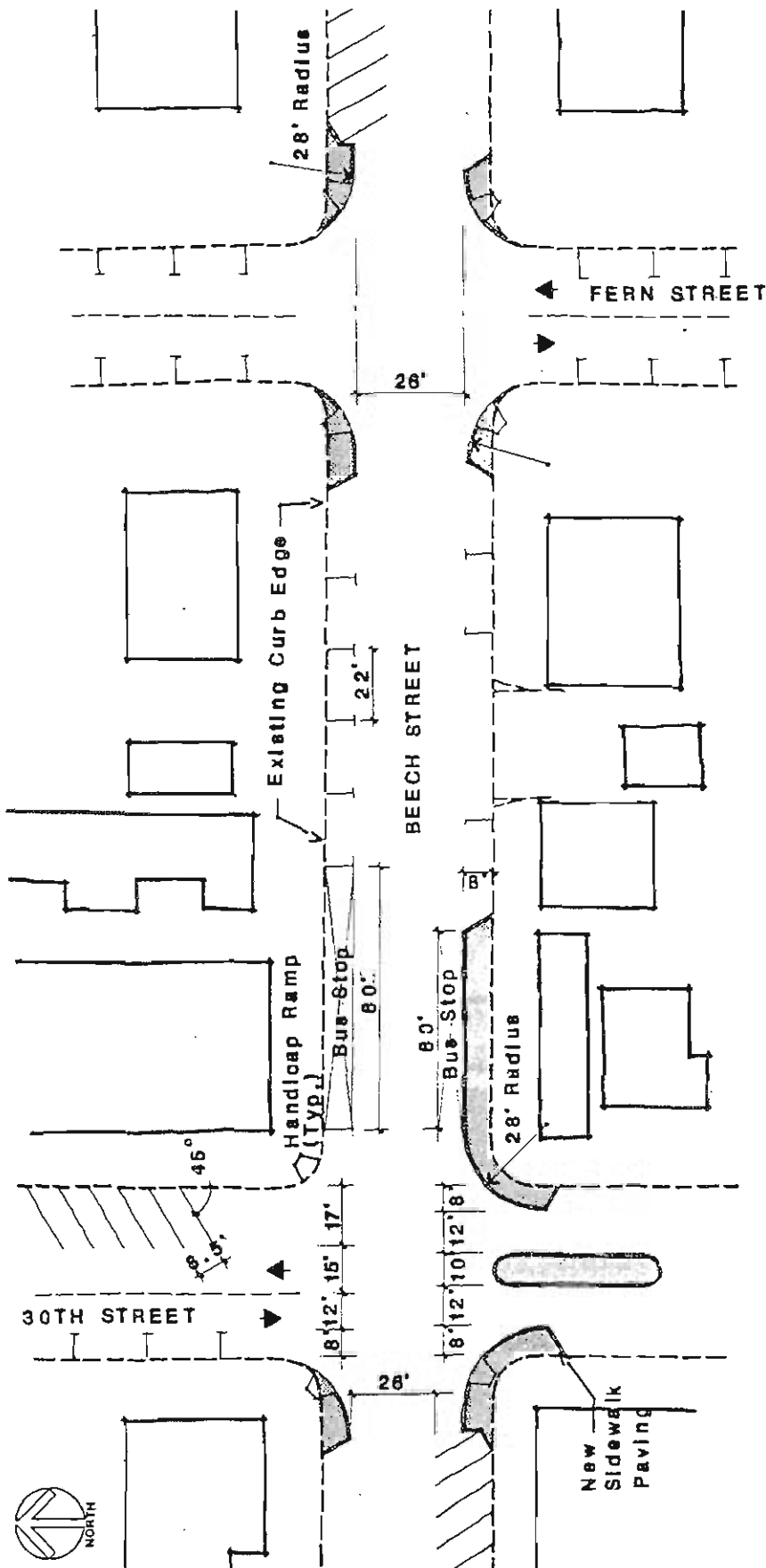
Figure 18, Grape Street Square, promotes a design for not only guiding vehicles through the offset in a safe and efficient manner, but also creates a community focus in the form of a "village square", with expanded sidewalks and street amenities.

\* INCORPORATE ADDITIONAL CURB/PAVEMENT MODIFICATIONS AS PART OF THE OVERALL CIRCULATION MANAGEMENT CONCEPT. -- Four typical curb/pavement improvement designs have been developed for specific locations in the study area. Figure 16 illustrates their location. Figures 17, 18, 19, and 20 are recommended individual design layouts.



GRAPE STREET SQUARE

FIGURE 18



BEECH STREET CENTER

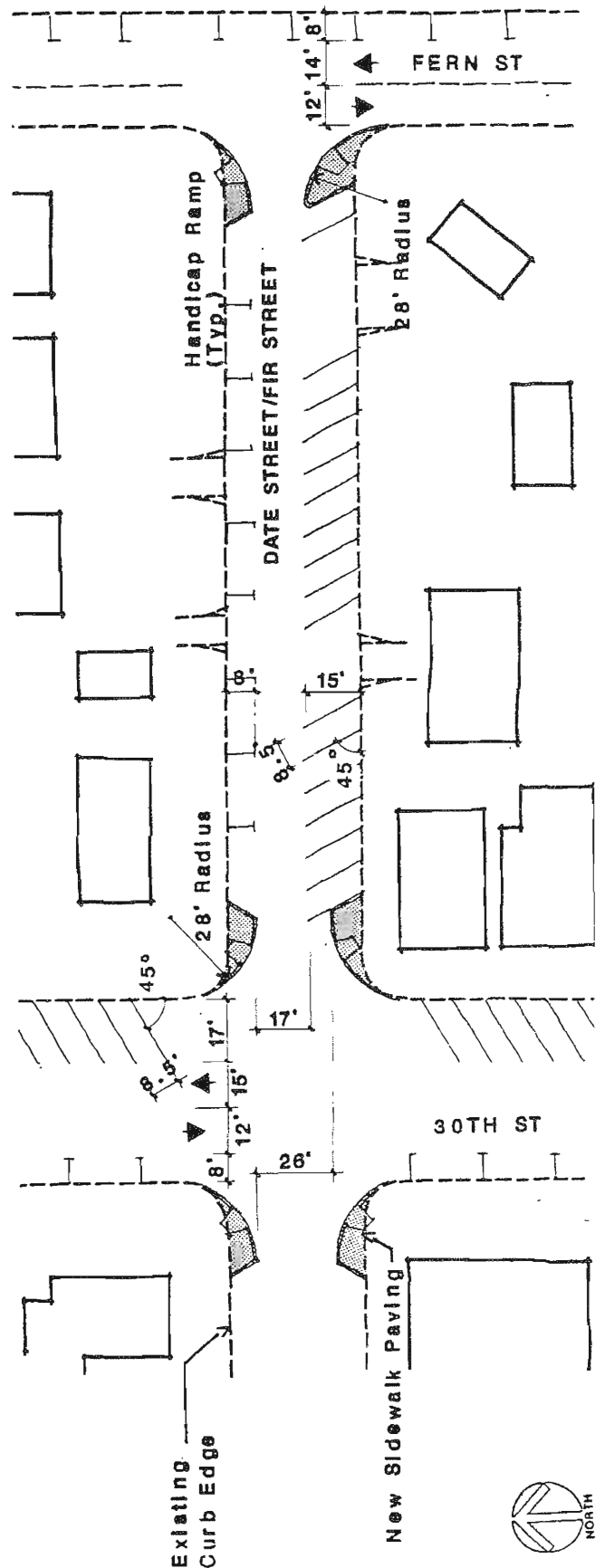
FIGURE 19

**PARKING IMPROVEMENTS**

The issues surrounding parking within the study area center on two primary concerns on the part of residents and merchants. Safety is a major concern along the narrow segments of Fern Street because of the number of incidents of motorists hitting parked vehicles. The substandard six-foot width of parallel parking bays aggravates the situation further as motorists maneuver to avoid conflicts with drivers entering and leaving parked cars. Approximately one-half of the non-intersection accidents in Golden Hill involve motorists hitting parked vehicles.

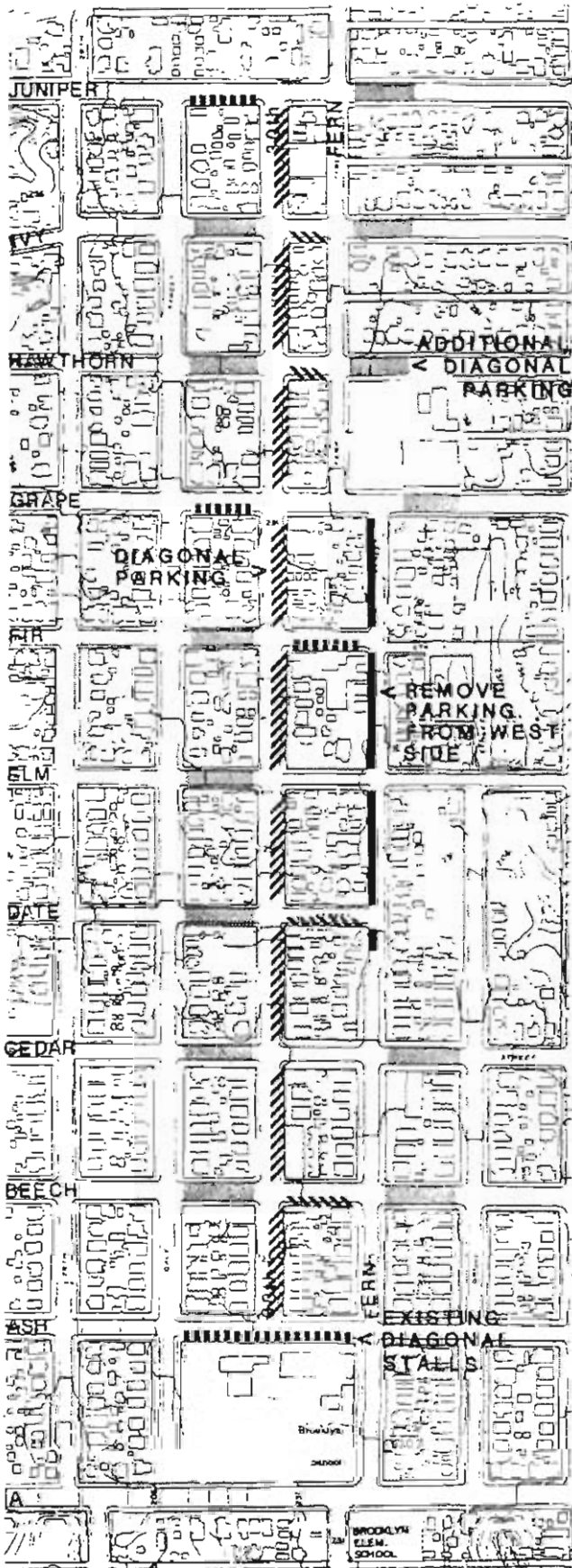
Maintaining an adequate reservoir of parking, particularly within the commercially zoned areas, is a second concern. There is not one commercial district in San Diego that feels that it has enough parking. Businesses located along Fern Street and 30th Street are no exception.

What appears to be a severe problem is really the aggregation of many little problems, and the solution lies in the aggregation of many little opportunities. Figure 21, Parking Plan, illustrates the opportunities available for increasing on-street parking areas within the study area, while at the same time alleviating a number of safety concerns. The following are characteristics of the Parking Plan:



**RESIDENTIAL WOORNURF**

**FIGURE 20**



\* REMOVE ON-STREET PARKING FROM THE WEST SIDE OF FERN STREET BETWEEN GRAPE STREET AND ONE QUARTER BLOCK SOUTH OF DATE STREET (FERN CENTRAL). -- As discussed in an earlier section of this document, Traffic Issues, Fern Central presents the greatest hazards in terms of on-street parking conditions.

The 1978 Precise Plan recommends removal of parking from one side of the street, as well as near intersections where accident rates are abnormally high. Figure 22, A and B, depicts the before and after conditions of removing parking from one side of Fern Central.

Approximately thirty-five existing parallel parking spaces would be lost along Fern Central should the proposed plan be implemented. However, Figure 24 illustrates that an equal amount of spaces, if not more, could be reclaimed within the immediate vicinity if diagonal parking were provided on adjacent perpendicular streets and 30th Street. A complete discussion of the benefits associated with diagonal parking follows.

\* PROVIDE DIAGONAL PARKING ON ALL EAST/WEST STREETS AND ON THE EAST SIDE OF 30TH STREET WHERE FEASIBLE. -- Diagonal parking is not a

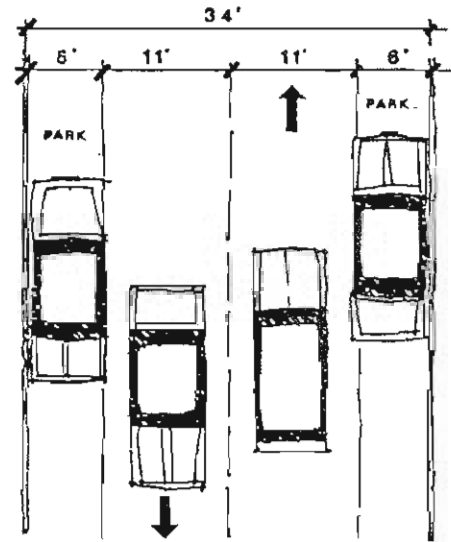
PARKING PLAN

FIGURE 21

new concept within the study area. Juniper, Grape, Fir, and Ash are all streets that presently have diagonal parking provided. The benefits associated with continuing the diagonal parking concepts are twofold:

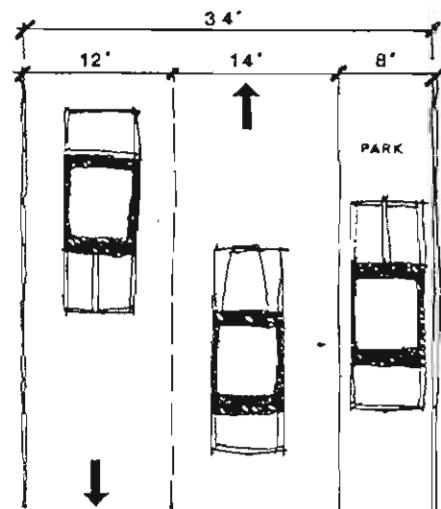
1. In terms of simple arithmetic, the present reservoir of on-street parking is increased by fifty percent over the existing pattern of parallel parking.
2. Diagonal parking can reduce traffic speed by narrowing the street and by making drivers more alert to the threat of cars backing out. This is a key concept associated with 30th Street and part of the overall traffic objective of reducing vehicular speed through optical design.

Figure 24 also illustrates how the diagonal parking concept can be provided on a typical block within the constraints of existing driveways and curb cuts on 30th Street.



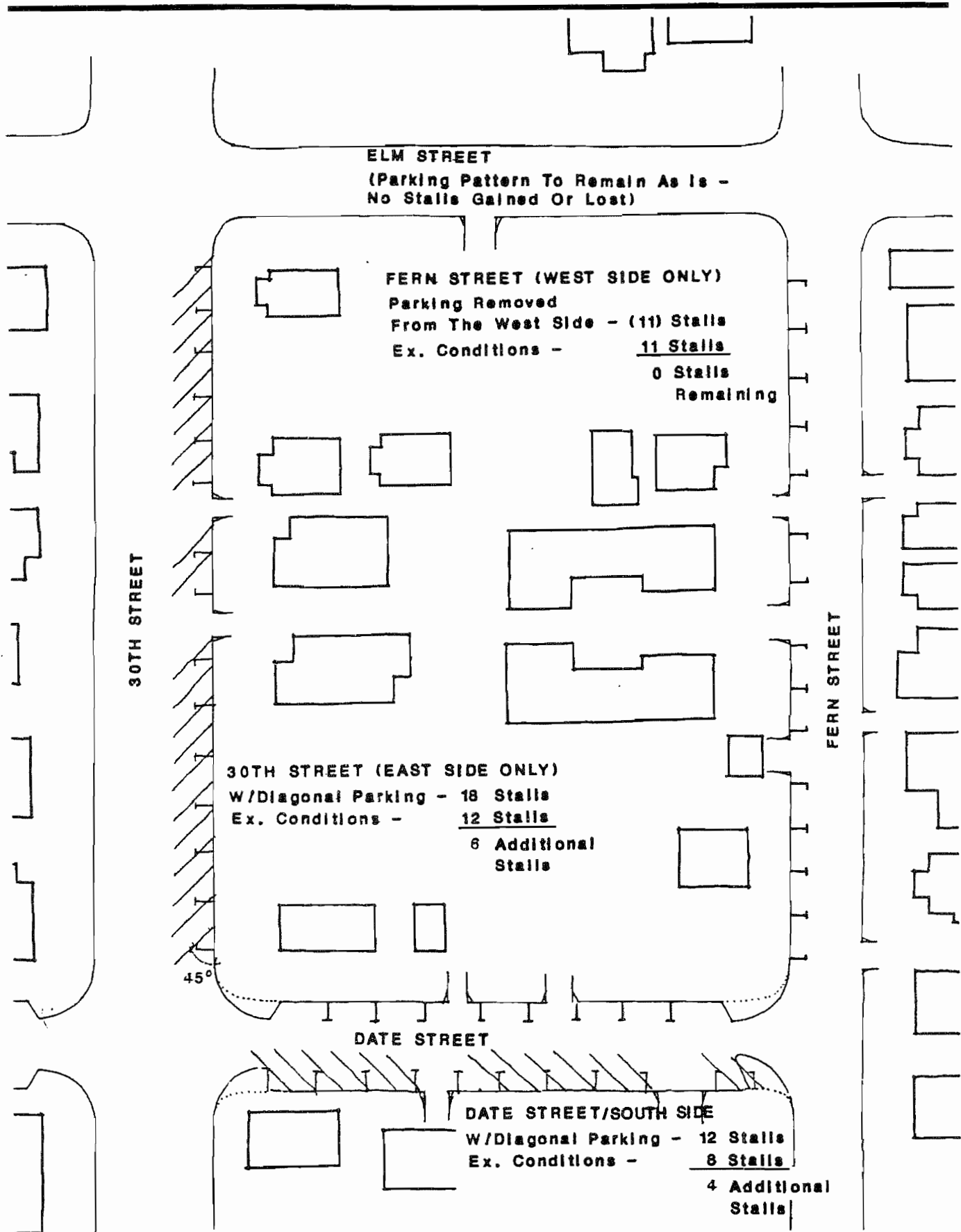
FERN CENTRAL EXISTING

FIGURE 22



FERN CENTRAL PROPOSED

FIGURE 23



**PARKING ANALYSIS**

**FIGURE 24**

## FUNDING AND IMPLEMENTATION

The first priority of the overall Action Plan should be the implementation of the circulation and parking improvements. These improvements are based on the safety concerns of the residents of the community and as such should receive immediate attention. The previous sections of the traffic and parking component identified a number of improvement recommendations.

These recommendations can be translated into a series of individual projects. The following individual projects are structured in their ranked order of importance or priority. The improvements could be completed simultaneously or in a phased order based on ease of implementation for the City.

1. Remove on-street parking from the west side of Fern Street between Grape and one-quarter block south of Date Street (Fern Central).
2. Install north-bound and south-bound stop signs on Fern Street at the intersection of Grape Street. The existing stop signs along Grape Street are to remain in place.
3. Install northbound and southbound stop signs along Fern Street at the intersections of Juniper Street and Beech Street respectively.
4. Designate Fir Street and Date Street (between 30th and Fern) as one-way east-bound streets. Also relocate the golf course directional sign at Date Street to Beech Street.
5. Stripe the east side of 30th Street between Juniper Street and Beech Street and the south side of all east west streets with 60 degree diagonal parking. The striping should be done only where driveways or other public right-of-way elements do not present an obstruction.
6. Incorporate curb/pavement modifications as part of future street improvement programs.

Implementing the traffic and parking improvements first is significant for three reasons. Safety is the first reason and needs no further discussion as to why. Timing is a second reason since the realization of a number of other components of the Action Plan are contingent on the Traffic and Parking Program being in place, or showing some progress in completion.

As stated earlier, the present image of 30th Street and Fern Street is that of an uninterrupted channel designed for the driver. Changing that image first will lead to improvements in the community's visual quality, which will in turn lead to



reinvestment on the part of merchants and residents along 30th and Fern Streets.

A third reason for initiating the traffic and parking improvements first is funding. All six individual projects outlined above, with the possible exception of the curb/pavement modifications, can be implemented as a City improvement project. These projects are funded by individual public works division operating budgets. Selection of projects for public works improvements can be competitive. Financial resources for improvement projects are limited and are thus allocated to projects where the issues of public health and safety are prominent. Several of the individual projects, however, could clearly fall under the heading of health and safety concerns.

There are two other processes to fund and implement the traffic and parking improvements. One process could be implemented sooner than the other. The shorter route would involve a review by the Traffic and Engineering Department of the six individual improvement projects. Following review and approval of the project(s), a work request would be issued to the City's streets division. If funds are available from the divisions current operating budget, the work could proceed immediately. This process appears to be the most appropriate route for removing parking

from Fern Street; striping for diagonal stalls; installing stop signs; and installing one-way signs (since their improvement costs will be relatively minor). Community Development Block Grant (CDBG) funds could also be used if funds are not available from the current operating budget.

The Capital Improvement Program (CIP) is a second, although longer, process to implement more complex construction improvements. Capital Improvement Projects are introduced into the programming process through approval of Capital Improvement Project Proposals by the Assistant City Manager or Deputy City Managers. An approved project proposal is a prerequisite to inclusion in the six-year program. Accordingly, a proposal should be initiated as soon as the sponsor/preparing department concludes that the project should compete for funding during the current six-year period.

Annually, in July, the Financial Management Director will advise operating departments of the deadline for submission of proposals to be considered for inclusion in the preliminary program. The CIP appears to be the most viable method for funding the more extensive improvement projects such as the curb/pavement modifications. CDBG funds can be used to fund these improvements as well.

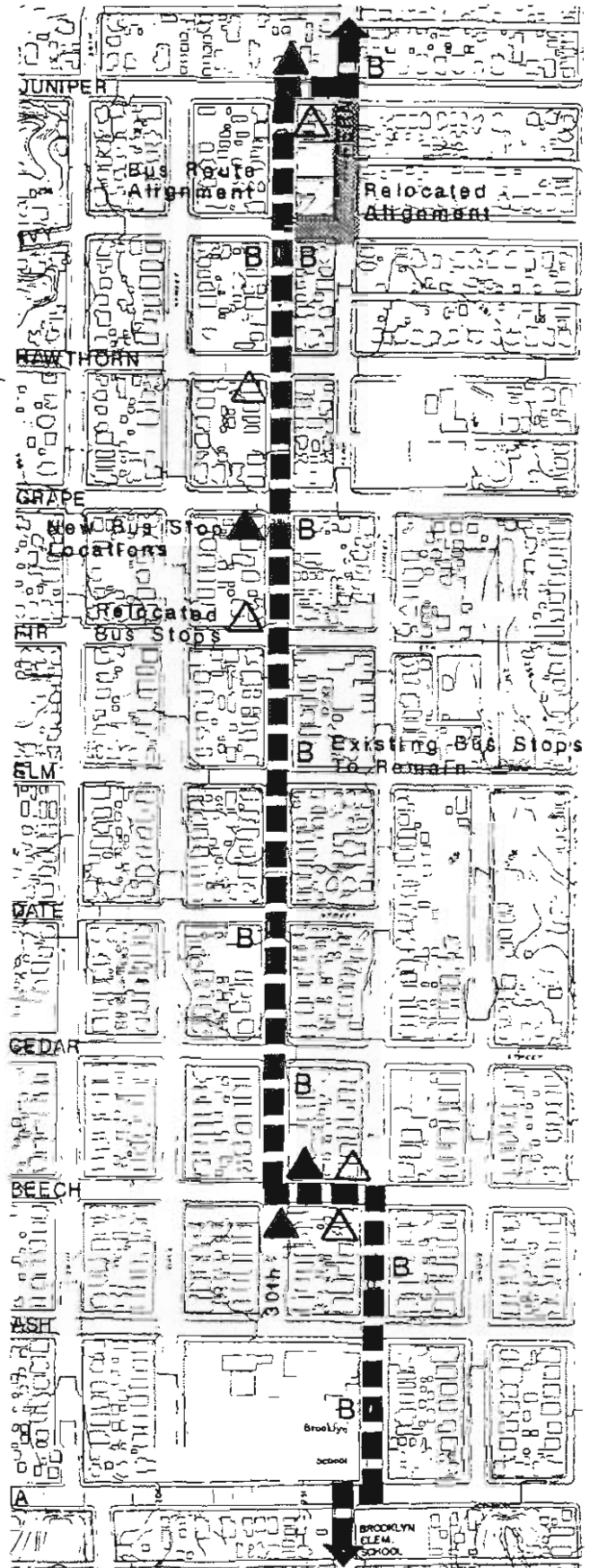
## 2

## TRANSIT

Figure 25 depicts changes to the existing bus route and stop locations. The revisions are the result of discussions with community residents and San Diego Transit, as well as design research on the part of the consultant team. The recommended changes to the existing service plan are seen as necessary in order to implement the overall circulation and design plan developed for the Fern Street and 30th Street study area. The following are specific characteristics of the transit plan.

- \* REROUTE THE NORTH AND SOUTHBOUND DIRECTION OF THE BUS TO TURN FROM FERN STREET TO 30TH STREET, AND VICE VERSA, AT JUNIPER STREET (AS OPPOSED TO IVY STREET). -- The revitalization strategy calls for Ivy, Hawthorn, and Grape to be developed as pedestrian oriented commercial streets with expanded sidewalk treatment, street neckdowns and diagonal parking. These improvement ideas preclude the continued use of Ivy Street as a bus route because of the wide turning radius required to guarantee safe and efficient movement of the bus.

Juniper Street is presently designated, by way of street directional signage,



TRANSIT PLAN

FIGURE 25

## VISUAL QUALITY

### 3 STREETSAPES

as the preferred vehicular street to connect to 30th Street. For these reasons the bus route should be moved to Juniper Street.

- \* RELOCATE CERTAIN EXISTING BUS STOPS TO PROVIDE MORE CONVENIENT AND SAFER LOCATIONS FOR TRANSIT USERS. -- Three location changes are proposed and are shown in Figure 25. The most prominent change is the relocation of the bus stop on Fern Street between Juniper and Ivy to the location shown in the plan. The rationale for the relocation is obviously tied to the rerouting of the bus to Juniper. However, the move is also intended to provide for more efficient vehicular flow within the study area by eliminating the traffic delay caused by the bus presently stopping at the Fern Street location.

Initiation of the recommended transit improvements are subject to the review and approval of San Diego Transit. The above recommendations have been discussed with San Diego Transit staff and have received a generally favorable reception. Concern on the part of staff was expressed about the re-routing of the buses from Ivy Street to Juniper Street. Property owners along Ivy, however, have expressed a need to move the bus route to a more appropriate street, such as Juniper.

As an integral part of Golden Hill's visual quality, it will be the elements which make up the streetscape that will provide a significant contribution to a positive and unified image for residents and visitors alike. Because no urban experience is an isolated act, every element must be considered as it relates to another. In this way, the experience of the Fern Street and 30th Street neighborhood will be one of a



STREETSAPES OVERVIEW

composite of choreographed events, and thus less object oriented.

This discussion is not intended to diminish the need for "objects" in this plan; it is intended to clarify the difference between intention and implementation. A comprehensive overall visual quality in the streetscape is the intention. The objects (i.e., street trees, sidewalks/paving, street furniture, and lighting) and their placement in the neighborhood is the implementation.

The overall streetscape is based on the recognition of differing neighborhood zones. These zones are the two neighborhood commercial anchors at opposite ends of the study area (Grape Street and Beech Street) with two primary residential/circulation corridors (Fern Street and 30th Street) forming a north/south link between the commercial centers. A cross pattern of secondary residential/circulation elements forms an east/west connection system between Fern Street and 30th Street, and beyond into the adjacent residential areas, see Figure 16. The streetscape plan has the following primary objectives.

- \* CREATE A FOCUS OF PEDESTRIAN ACTIVITY BY WAY OF SPECIAL DESIGN AT THE INTERSECTIONS OF GRAPE AND FERN, AND BEECH AND 30TH.

-- Because of their prominent location and use as crossroads with regard to the circulation element, and because they are the most active pedestrian zones due to their neighborhood commercial nature, the commercial anchors at Grape Street and Beech Street are to receive a concentration of streetscape elements with special interest with regard to the placement of these elements. These two areas, which we call Grape Street Square and Beech Street Center, will be discussed in greater detail in this document.

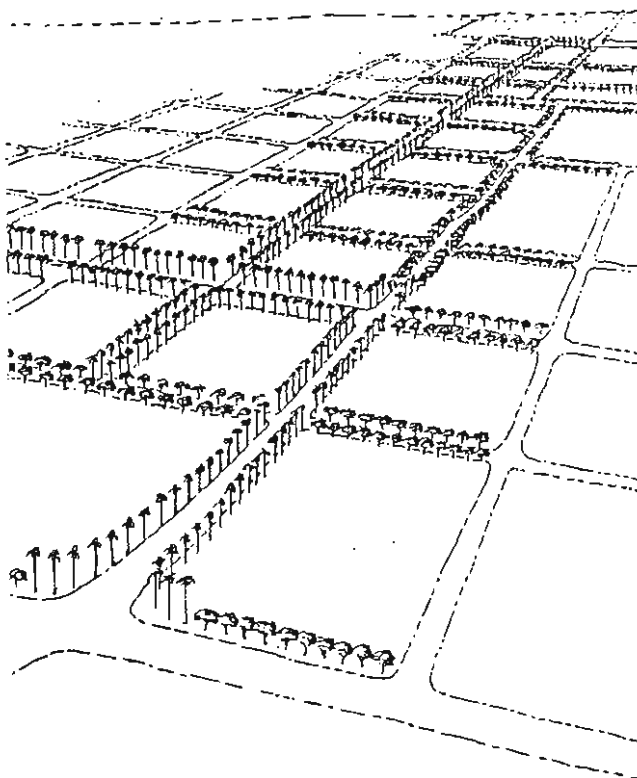
- \* IMPROVE PEDESTRIAN CONDITIONS ALONG FERN STREET AND 30TH STREET BY WAY OF NEW SIDEWALKS, STREET FURNITURE, STREET TREES, AND ADDITIONAL STREET LIGHTING. -- The Fern Street and 30th Street residential/circulation corridors will receive streetscape treatments which, in keeping with the goals of the transportation element, will enhance the overall linear character of this area for pedestrian and motorist alike.
- \* GUARANTEE THAT THE QUALITY AND INTENT OF THE INITIAL STREETScape IMPROVEMENTS ALONG FERN STREET AND 30TH STREET WILL BE MAINTAINED TO THE SATISFACTION OF THE COMMUNITY AND THE CITY BY

ADOPTION OF A LIGHTING AND LANDSCAPE MAINTENANCE DISTRICT WITHIN THE STUDY AREA. -- Although funding for preparation of construction drawings for the proposed street improvements are presently available, the funds required to construct the improvements within the study area have not been allocated. Additional funding for construction from CDBG is more a likelihood if an "investment" is made on the part of residents and property owners by initiating a maintenance district to care for the initial street improvements. A complete discussion about the need to establish a maintenance district is included under the title, Funding and Implementation.

#### STREET TREES

The quality of vegetation in San Diego is widely known and appreciated. Currently, however, there is a severe shortage of tree planting in the right-of-way of this study area. This shortage has created commercial zones insensitive to pedestrian comforts, and residential areas of vast and ambiguous open space. This situation is incompatible with the scale and nature of the neighborhood commercial districts and the historic housing types existing throughout the area.

To supplement and enhance what existing trees there are a palette of four trees for all future right-of-way plantings has been developed. A palette of trees and groundcovers for private yards which front on streets has also been prepared. By choosing vegetation from these palettes, consistency with subtle variety will emerge. This will further the intention of an image of a singular cohesive neighborhood.



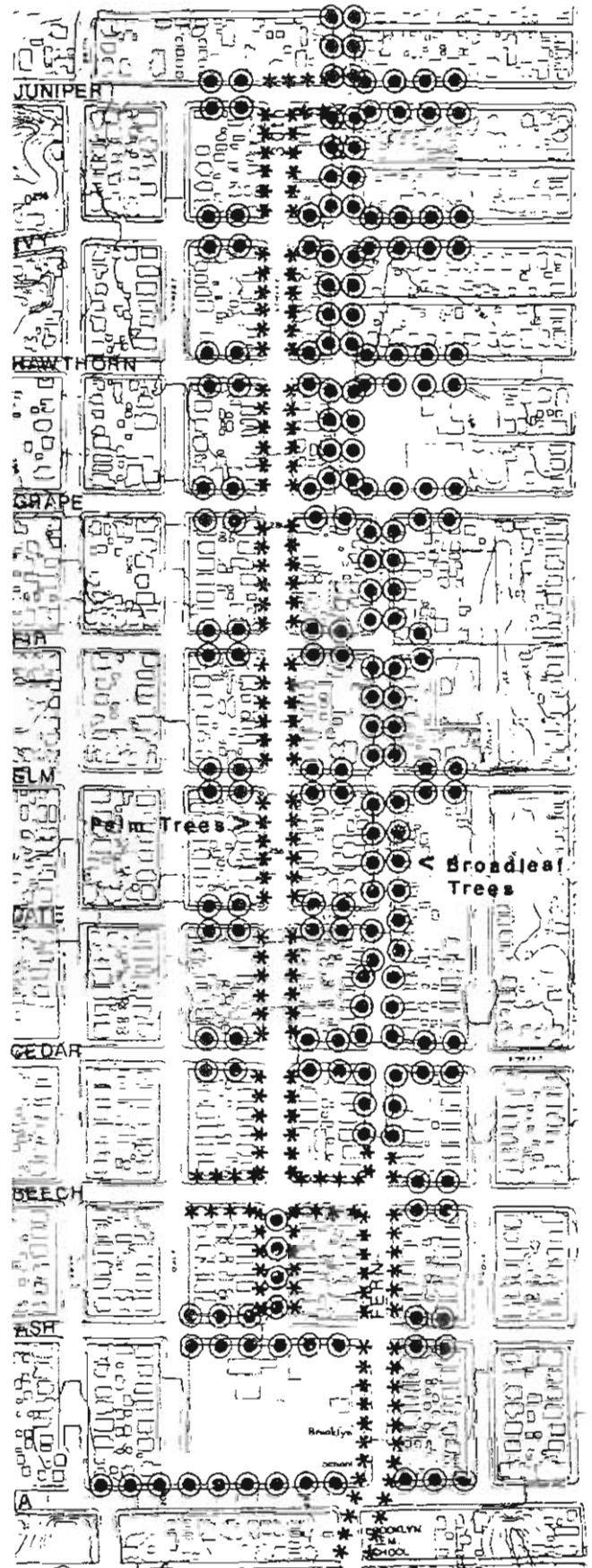
STREET TREE CONCEPT VIEW

FIGURE 26

For the street right-of-way two basic types of trees are recommended; broadleaved and palm. The general placement of those types can be seen in Figure 26. The palms are placed to correspond to the transportation corridor, as well as enhance the prominent linear quality, and spatial scale of 30th Street between Ash and Juniper and the southern portion of Fern. The broadleaved trees are used along most of Fern Street, and the cross streets, to enhance their smaller scaled, pedestrian nature. Figure 27 shows right-of-way tree planting which should occur at a rate of two trees per fifty foot lot. When spaced approximately twenty-five feet apart and at a distance of five feet to twelve feet six inches from the property lines perpendicular to the street, the future installation of underground utilities will not disrupt the tree pattern.

The watering of plants in the public right-of-way will be handled by a flood bubbler watering system. To avoid potential damage, this system should not be installed until after the overhead utilities have been undergrounded. Until this watering system is installed, truck supplied watering should be continued.

In addition to the above noted placement and spacing, the San Diego Citywide Landscape Ordinance will be followed for all new development. The Park and Recreation Department also has



STREET TREE PLANTING

the following regulations regarding planting of trees in the City right-of-way:

1. Do not plant within five feet of underground utilities such as water/sewer lines, gas lines, etc., or driveways or above ground utilities such as fire hydrants. Do not plant within twenty-five feet of any intersection, pedestrian crosswalk, or street light standard.
2. All species and locations shall be approved by the Deputy Director or an assigned representative from the Planning Department and/or the Park and Recreation Department.



CARROT WOOD

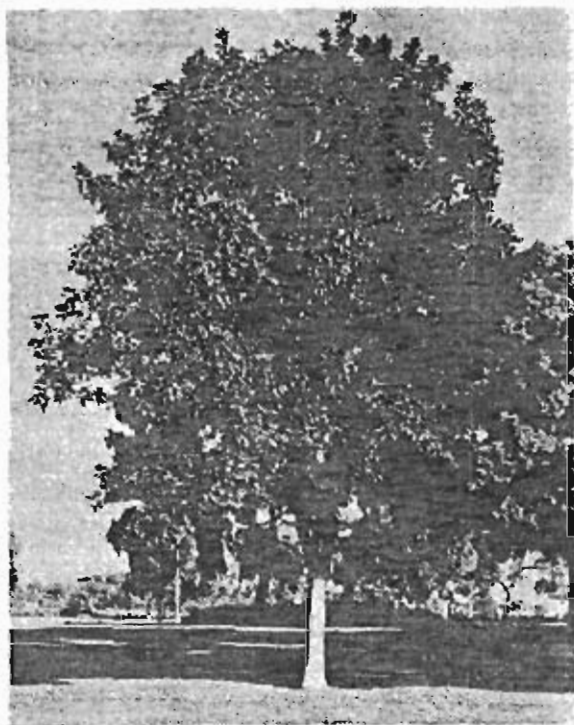
The following are the recommended plant material species for the street right-of-way:

Broadleaved:

- \* Cupania Anacardioides - Carrot Wood
- \* Koelreuteria Bipinnata - Goldenrain Tree
- \* Magnolia Gradiflora - Southern Magnolia

Palm:

- \* Washington Robusta - Mexican Fan Palm



SOUTHERN MAGNOLIA



MEXICAN FAN PALM

## Groundcover:

- \* Lawn
- \* *Carissa Grandiflora*  
'Horizontalis'
- \* *Hedera Helix*
- \* *Drosanhemum*

The following are the recommended plant material species for private yards fronting streets:

## Broadleaved:

- \* *Albizia Julibrissin*
- \* *Arbutus Unedo*
- \* *Bauhinia Variegata*
- \* *Cinamomum Camphora*
- \* *Cupania Anacarioides*
- \* *Ficus Benjamina*
- \* *Juniperus Chinensis*  
'Torulosa'
- \* *Jacaranda Mimosifolia*
- \* *Koelreutaria Bipinnata*
- \* *Magnolia Grandiflora*
- \* *Podocarpus Microphylus*

## Palm:

- \* *Archontophoenix Cunninghamiana*
- \* *Washingtonia Robusta*

## Shrubs:

- \* *Acanthus Mollis*
- \* *Agave Attenuata*
- \* *Azalea Sp.*
- \* *Buxus Microphilla*  
'Japonica'
- \* *Crassula Argentia*
- \* *Gameolepis Chrysanthemoines*



- \* Hibiscus            Rosa-
- Sinensis
- \* Ligustrum Sp.
- \* Myrsine Africanus
- \* Strelitzia Reginae
- \* Strelitzia Nicol-
- aia
- \* Syzigium Paniculata

#### Ground Cover:

- \* Lawn
- \* Carissa Grandiflora
- 'Prostrata'
- \* Pelargonium
- \* Hedera Helix
- \* Lantana
- \* Vinca
- \* Agapanthus
- \* Ice Plant

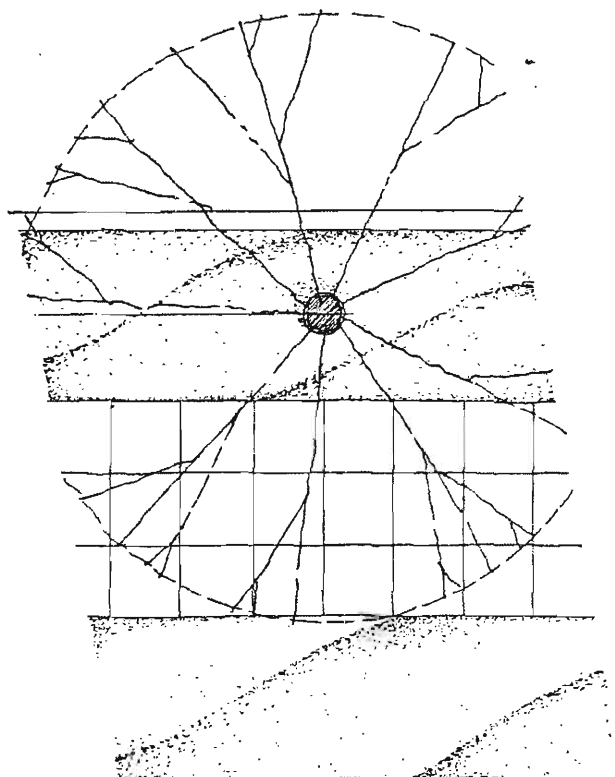
#### SIDEWALKS/PAVING:

Throughout the study area there exists a variety of types and quality of paving in the right-of-way. The variety is due primarily to the age of the neighborhood, the variety of land uses, and changes in the building setbacks. Typically, there is a five feet four inch wide sidewalk placed between two planting areas (though not always centered) of various dimensions depending on location. The sidewalks are concrete and the historic pattern of control joints is three squares wide (one foot nine inch square), as shown in Figure 28.

In many instances the planting area between the sidewalk and the curb has been replaced by an impervious paving material. In a number of cases it is an

installation of masonry pavers, but typically it is simply concrete placed with no consideration for the existing pattern of control joints. In the case of the commercial districts, it is common to find the entire area of right-of-way between curb and property line filled with concrete. While not visually appealing, this does speak to the need for greater pedestrian circulation space in the commercial zones.

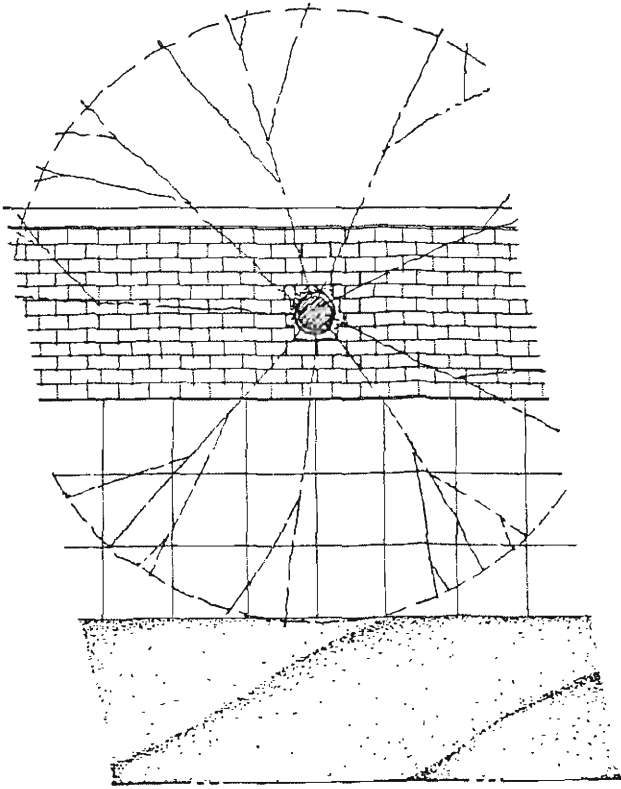
It is recommended that, as shown in Figure 29, in high pedestrian use areas, the area between the sidewalk and curb be masonry pavers. The areas



EXISTING SIDEWALK PATTERN

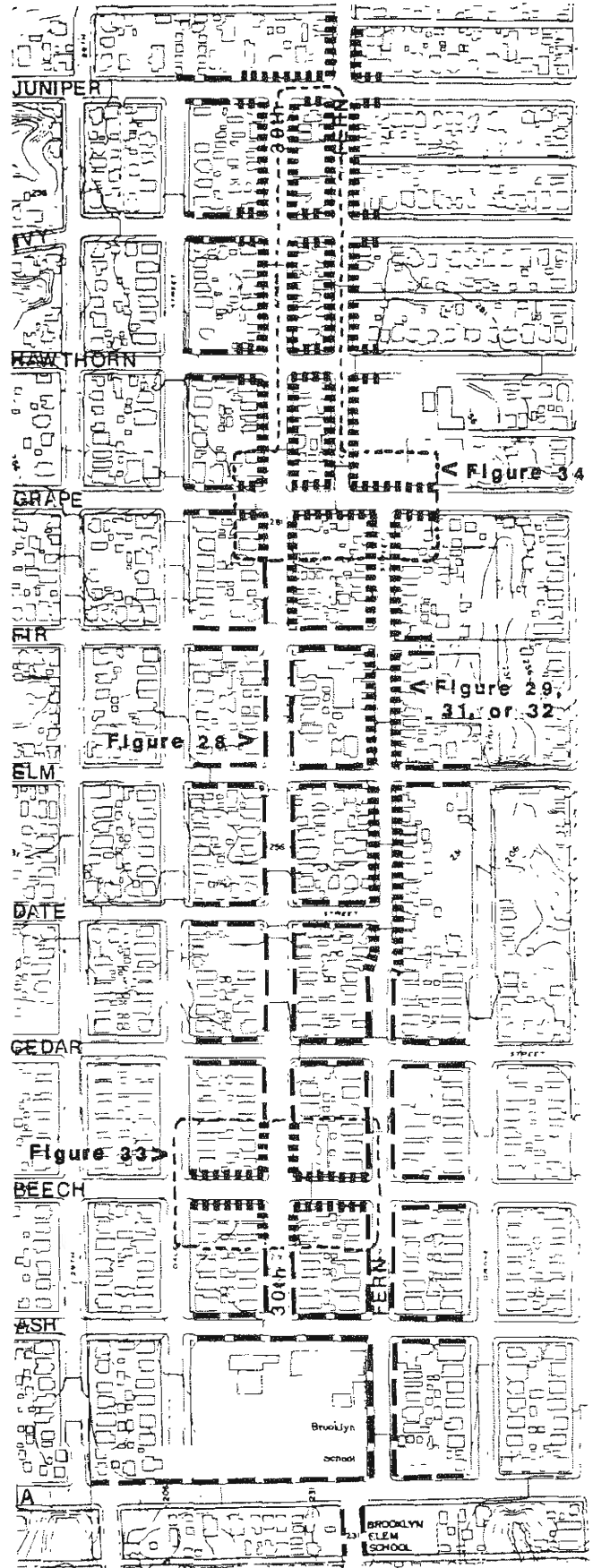
FIGURE 28

between the sidewalk and the adjacent property lines now of concrete should also be replaced with masonry pavers and any future paving in this area should likewise be masonry. The masonry should be a standard brick paving unit approximately 4" X 2-2/3" X 8" in size. They should generally be brick red, though minor color variation is encouraged. The units should be laid in a running bond over an even, compacted sand base. The bricks can be laid mortarless in infill areas outside areas of primary circulation. The top of the masonry should be flush with the top of the adjacent sidewalk.



PROPOSED INFILL #1

FIGURE 29



SIDEWALKS/PAVING

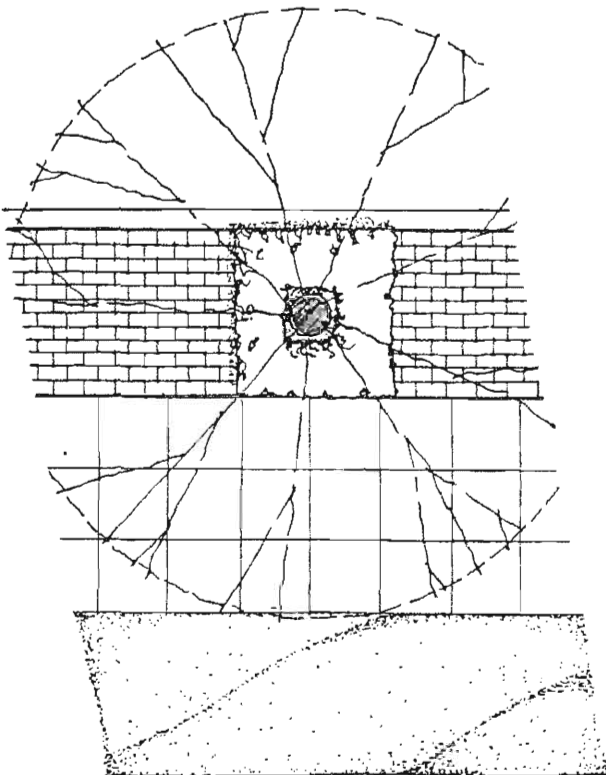
FIGURE 30

Where trees occur, the masonry should either be laid loosely up to the trunk as in Figure 29 or held back to form a planter box no more than four feet zero inches as in Figure 31. In some cases it may be desirable to place masonry on both sides of the sidewalk per Figure 32. In this situation, to provide continuity, it is recommended that private property owners match their own adjacent paving to that in the right-of-way.

At Beech Street Center, where concrete is the currently dominant paving material, it is

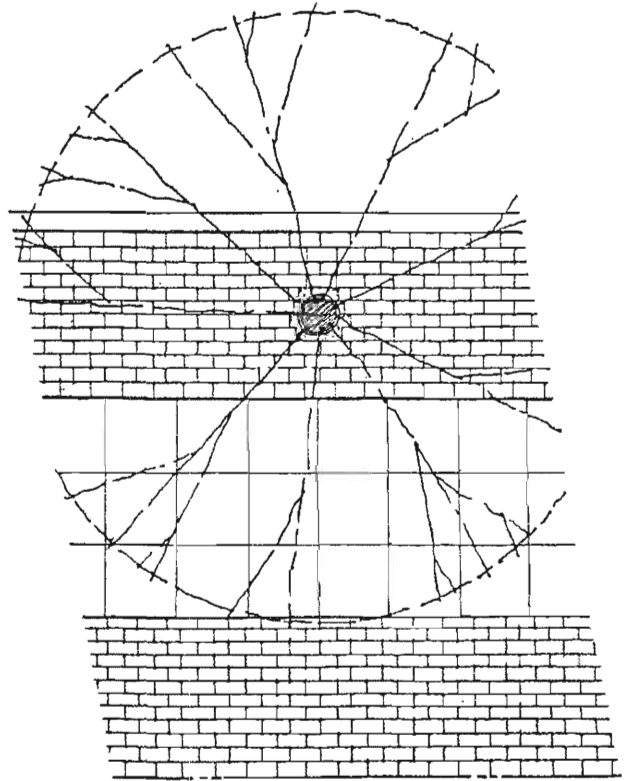
suggested that the historic pattern be continued with the inclusion of tree grates. The tree grates will facilitate pedestrian circulation, protect the tree's root system, and enhance the overall image of the commercial area, see Figure 33. Grape Street Square, the central component of this Plan, will also receive full right-of-way paving though, in this case, it should be masonry pavers, also with tree grates, per Figure 34.

The areas between the sidewalk and curb called out in Figure 30 to be planting shall be



PROPOSED INFILL #2

FIGURE 31



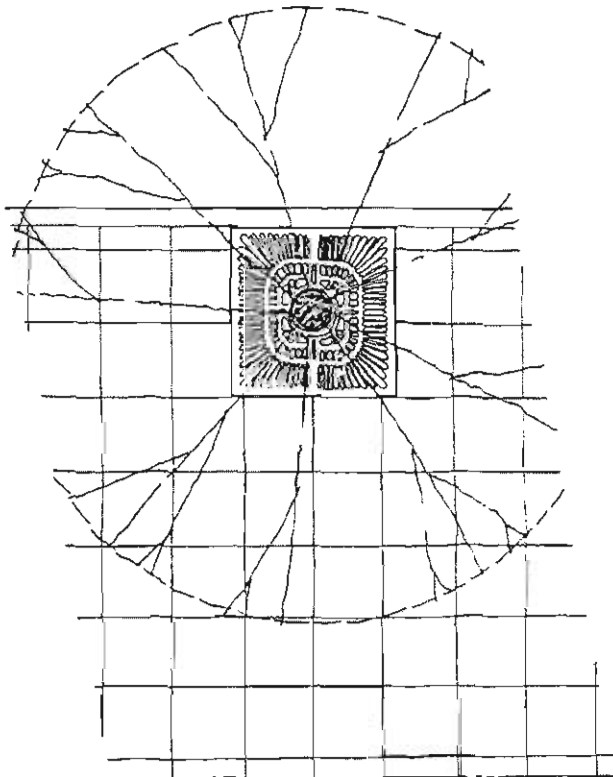
PROPOSED INFILL #3

FIGURE 32

selected from the groundcover planting list in the previous section.

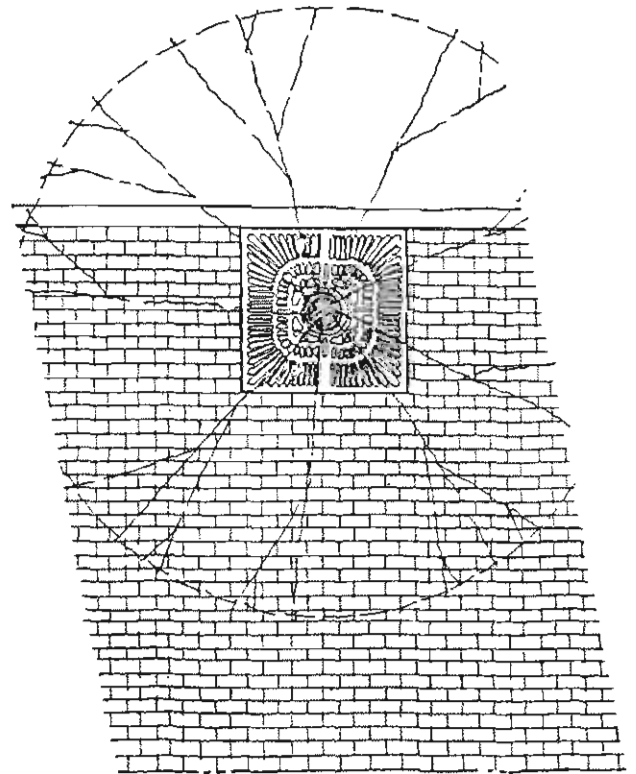
In the event that concrete is placed in the areas which we have recommended to be masonry, and in the case of existing sidewalk repair or replacement, the quality, color, workmanship, and especially the scoring pattern of the original sidewalks must be maintained. This would include all repair work associated with the undergrounding of overhead utilities.

The areas between the sidewalk and the adjacent property line, unpaved, shall also have groundcover plants selected from the previous section. The adjacent property owners are encouraged, however, to treat this area as if it were their own by coordinating their own frontyard landscaping with the right-of-way landscaping. Under no circumstances should a fence or retaining wall be built in the street right-of-way.



BEECH STREET CENTER

FIGURE 33



GRAPE STREET SQUARE

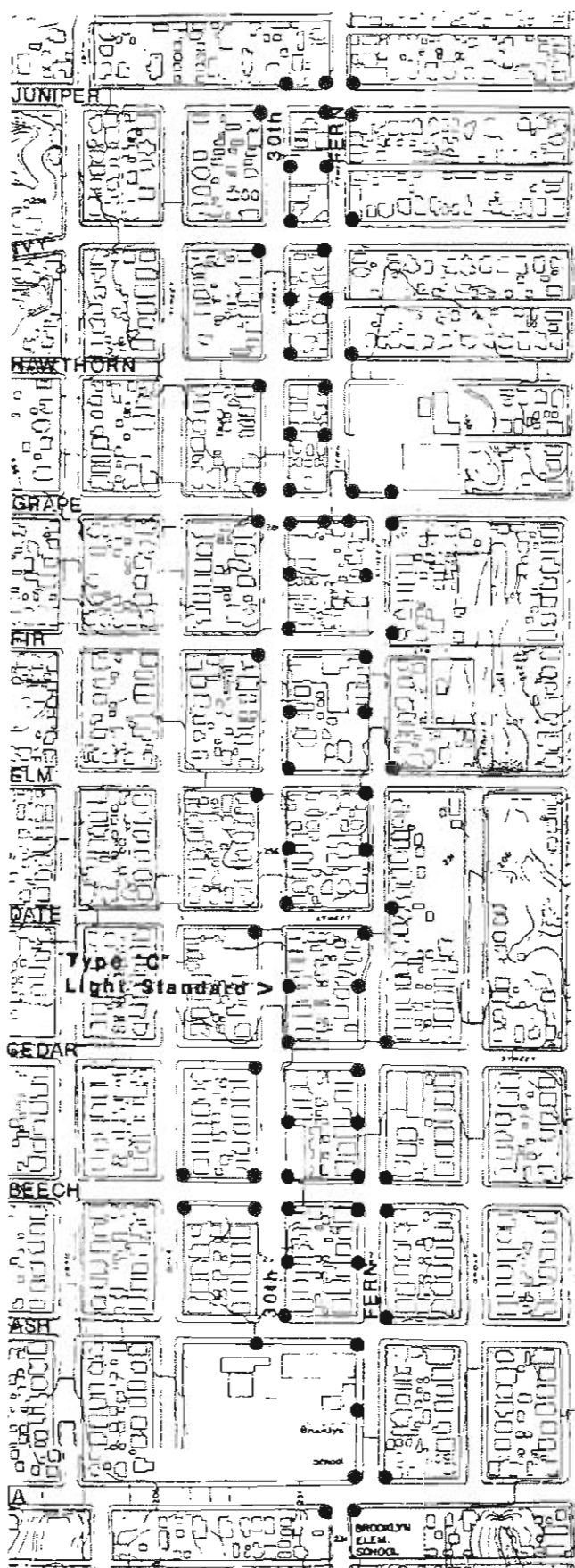
FIGURE 34

## LIGHTING

The quality and quantity of both lighting and light fixtures can do much to create a consistent ambiance throughout a neighborhood. The Fern Street and 30th Street corridor is presently, while conforming to City of San Diego standards for minimum lighting, under lit. This is especially true of the residential mid-block areas, and of the commercial areas in general. The desire for increased lighting will become even more evident in the commercial areas as use and activity intensifies.

As mentioned before, this Revitalization Plan is recommending the undergrounding of overhead utility lines. While directly benefiting the visual qualities of the skyline, it also impacts lighting in the neighborhood. Most of the existing light fixtures are fastened to the power poles which are destined to be removed.

To address both the issues of power undergrounding and desirable light levels, and to incorporate the lighting into the overall character of the neighborhood, it is recommended that the use of period light fixtures be placed two on opposite corners of each intersection and one fixture at mid-block. The commercial districts on Beech Street and Grape Street are to receive more detailed treatment. The proposed layout of fixtures can be seen in Figure 35.



LIGHT FIXTURE PLACEMENT

FIGURE 35

As is typical for the City of San Diego during the undergrounding process the street lights are replaced with a simple "cobra head like" utilitarian fixture, one at each intersection. And, while the City may feel that this is satisfactory and economical, this typical solution is not being recommended for the neighborhood. Due to the special character of the neighborhood, the utilitarian nature of the fixture and the spotty nature of the resulting light, this typical solution would be incompatible with the overall image for the neighborhood.

The lighting fixture that is recommended is the San Diegan Series Type "C" Lighting Standard, 12 feet high, with one 55 watt, low pressure sodium lamp per fixture, manufactured by Western Lighting Standards, Fountain Valley, California, Figure 36. This is the same fixture being used throughout downtown San Diego. It has been selected for use here for several reasons:

1. Because it is a common fixture downtown, it will provide a visual connection to the City which initially spawned this residential community.
2. It is a period design which reflects the time in history when the neighborhood was first developed.

3. The texture, scale and light output of the fixture are appropriately scaled to the neighborhood.
4. The fixture has developed a history of success in use in other locations in San Diego.
5. It is readily available.
6. It is durable.

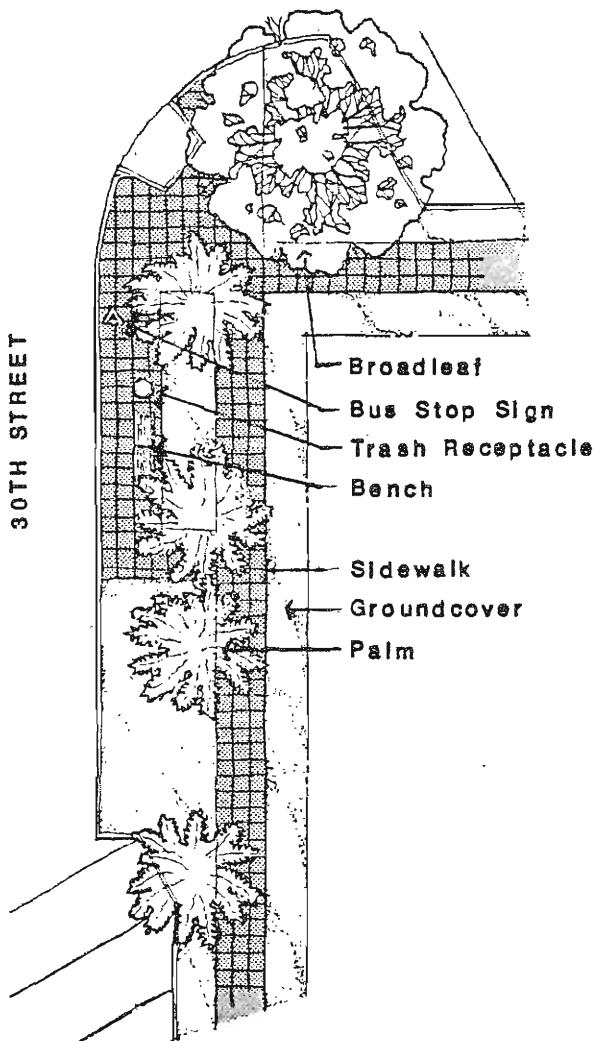


TYPE "C" LIGHT STANDARD

## STREET FURNITURE

Just as a couch, chairs, and end tables can turn a living room into a comfortable and useable space, so, too, can street furniture turn a public sidewalk into a place for relaxation and pleasant social encounters, both organized and spontaneous.

To continue the home furniture analogy, street furniture should be properly placed with consideration for use. For

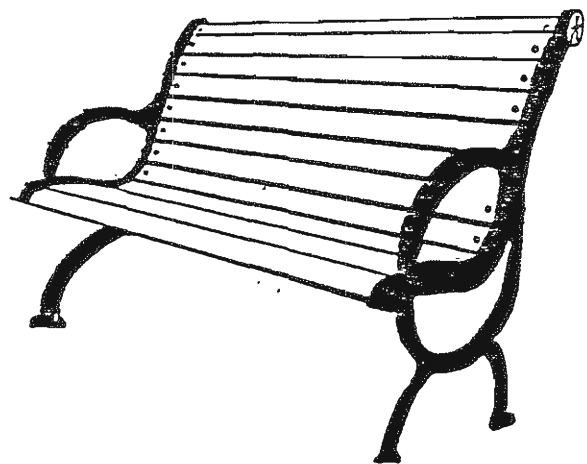


BUS STOP

FIGURE 37

example, one does not typically find a couch in the kitchen. In the residential areas, therefore, where the public right-of-way is used primarily for circulation, pedestrian, and vehicular, we are proposing no street furniture. The bus stops in these areas, however, are where people gather at frequent intervals and become natural locations for benches and trash receptacles. The new bus stop configuration shown in Figure 37 occurs in six locations throughout the study area where the bus stop is adjacent to angle parking.

The Fern Street and 30th Street neighborhood has two public "living rooms" which deserve special attention. It is at these locations, Grape Street Square and Beech Street Center, that the neighborhood should put its best collective foot forward. These locations,

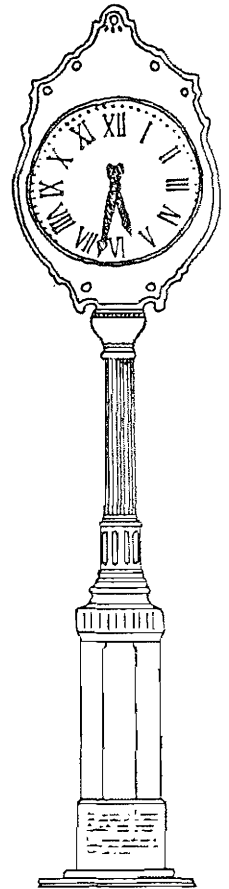


BENCH

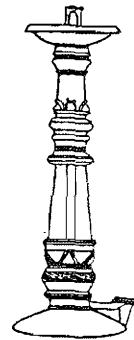
FIGURE 38

especially Grape Street Square, will be the source of a lasting image of this neighborhood. They form natural community centers with the commercial and social services that are, and should be, further encouraged to be located there.

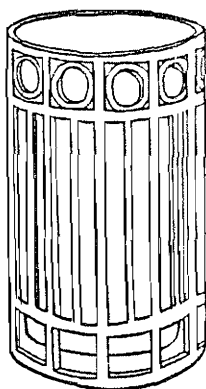
To take advantage of these two locations, we are proposing the addition of several specific items of street furniture. The selection of these fixtures was made to enhance existing activity in these locations and to encourage their further use and development. The recommended elements can be seen in Figures 38, 39, 40, 41, 42, and 43. As with the street lights, these fixtures were selected utilizing criteria regarding style, period design, scale, use, tested success, availability, and durability.



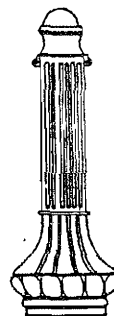
CLOCK  
FIGURE 40



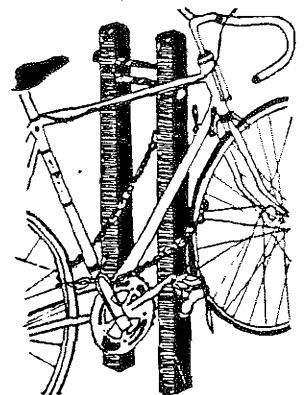
WATER FOUNTAIN  
FIGURE 39



TRASH RECEPTICLE  
FIGURE 41



BOLLARD  
FIGURE 42



BIKE RACK  
FIGURE 43



## FUNDING AND IMPLEMENTATION

### Streetscape Construction

For purposes of funding allocation and timing the street improvements for Fern Street and 30th Street are divided into two phases. Figure 53 depicts the limits of work for Phases One and Two. The Schedule of Public Improvements on page 81 discusses the year of construction for Phase One and Phase Two streetscape projects, their estimated construction cost, funding sources, and implementation procedures.

The first step to realizing the street improvement program is the preparation of Phase One construction drawings. This work effort can be funded from the initial \$150,000. CDBG fund available to the study area. The fee for preparing Phase One construction drawings is estimated to cost \$97,000. Hearings for review and allocation of City-wide CDBG funding are held annually in October. The goal of the first step is to have Phase One construction drawings significantly completed by the October hearing. Approval of funding in October will allow for construction to begin in 1988.

A key component in the Phase One construction process will be the timing of the 12KV utility undergrounding in Fern Street. The planning and design of this work effort should begin immediately. Major portions of the Phase One con-

struction process can proceed prior to the utility undergroundings (Grape Street Square and Beech Street Center). The current strategy is to have the streetscape construction effort along Fern Street follow immediately the utility undergrounding. Discussions with SDG&E indicate that the undergrounding construction process can begin in early 1989.

### Maintenance District

Streetscape enhancement programs such as the one being advocated for Fern Street and 30th Street are costly endeavors which will require additional ongoing funding to maintain the integrity of the initial investment. Funds however for streetscape maintenance beyond the basic needs provided by City crews, such as street sweeping and trash collection, are limited if not completely absent. Thus, the responsibility for the on-going maintenance of street trees, sidewalk repair, replacing lighting fixtures, fixing irrigation systems, maintaining street furniture, as well as other items, will rest to those who stand to benefit the most from the initial improvements; the residents, merchants, and property owners within and around the study area.

There are several alternatives for maintaining streetscapes, however, the most viable and realistic is the formation of a Lighting and Landscape Maintenance District. Maintenance dis-

tricts are not a new phenomenon to San Diego neighborhoods. Several communities such as Centre City, Mission Beach and Adams Avenue have their own place. They are, however, more common in suburban communities such as North City West and Rancho Bernardo.

It is important to remember that the basic intent of a maintenance district is to allow neighborhoods to go beyond the City's minimum maintenance standards of streets and other public areas. The concepts and plans developed for Fern Street and 30th Street go beyond the city's minimum criteria for improvements and consequent maintenance. A decision on the part of the community to move forward with the proposed improvements will require the formation of a maintenance district.

Establishment of a maintenance district will require a fee contribution from property owners who serve to benefit from the improvements. Determining how it is done and how much it will cost is beyond the scope of work called for as part of the initial design study process. An analysis should be prepared prior to detailed design and planning of street improvements to determine the assessment costs to individual property owners affected by the improvements. However, as an aid to help in answering further questions about Lighting and Landscape District, a discussion of how they are established and administered is included in the appendix portion of this document.

## 4 OVERHEAD UTILITY LINES

The overhead utility lines are the most conspicuous visual eyesore in the public realm, a situation which is not unique to Golden Hill. The City currently has a long-term program to underground all 12 KV lines. For economic and technical reasons there are at present no plans to underground 69 KV lines.

The 12 KV program is, however, very comprehensive and involves the coordination of many public and private agencies including SDG&E, Pacific Bell Telephone, Cox Cable, as well as the City's own various departments. The financial allocations for this program have already been made as a part of the City's annual budgets so there are only two remaining issues concerning undergrounding which affect this study.

The first is timing. Because the undergrounding program is long-term, the neighborhoods which receive undergrounding

first are based on a combination of need and resident interest.

The second is coordination. Extensive improvements to landscaping and sidewalks are being recommended by other sections of this study. Coordination is required so that these items will not be denied timely installation by delays of the undergrounding, and to avoid destruction of any improvements made prior to undergrounding.

It is, therefore, highly recommended that this neighborhood be prioritized so that undergrounding will occur as soon as possible (typically two years minimum) to facilitate installation of other improvements. A review of the Schedule of Public Improvements will clarify the need for coordination and timing.

Citizens, merchants, and neighborhood organizations should make it a singular cause that undergrounding is a high priority in this area. The success of the visual improvement programs rests on it.



UTILITY LINES-BEFORE



UTILITY LINES-AFTER

## 5 BILLBOARDS

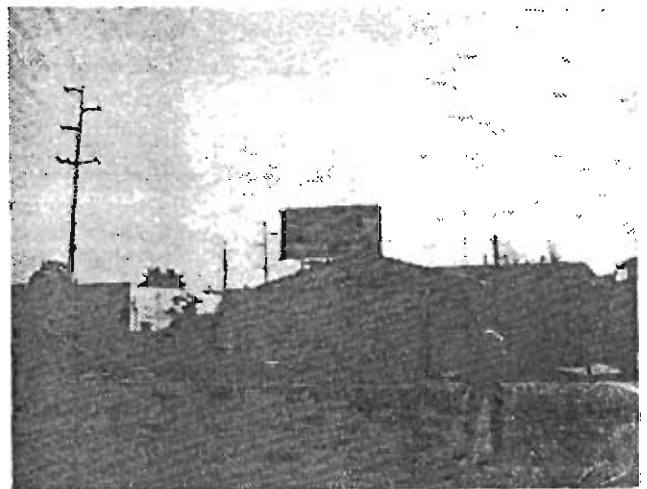
Billboards, which at times provide a useful medium for the communication of public service messages, can also be a severe eyesore within the community. This is particularly so within residential communities such as Golden Hill. The signs are often grossly oversized to the scale and character of a residential neighborhood. Their presence overwhelms any other symbols, such as landscape or architecture, which are more suitable to defining a community's image.

Four billboards are currently located within the Fern Street and 30th Street study area. The billboards were erected within the community when the majority of the study area was zoned commercial. Since that time a large segment of the study area was rezoned to residential (R-1500). Two of the billboards located in the R-1500 zone are thus deemed to be a legal nonconforming use. It is recommended as a first step to complete removal of all billboards from the neighborhood. A schedule for relocation or removal should be established by way of the City Attorney's Office.

According to section 5412.1 of the Business and Professions Code, a City can require the removal without compensation of a billboard on residentially

zoned land if the billboard owner is given an abatement period commensurate with fair market value.

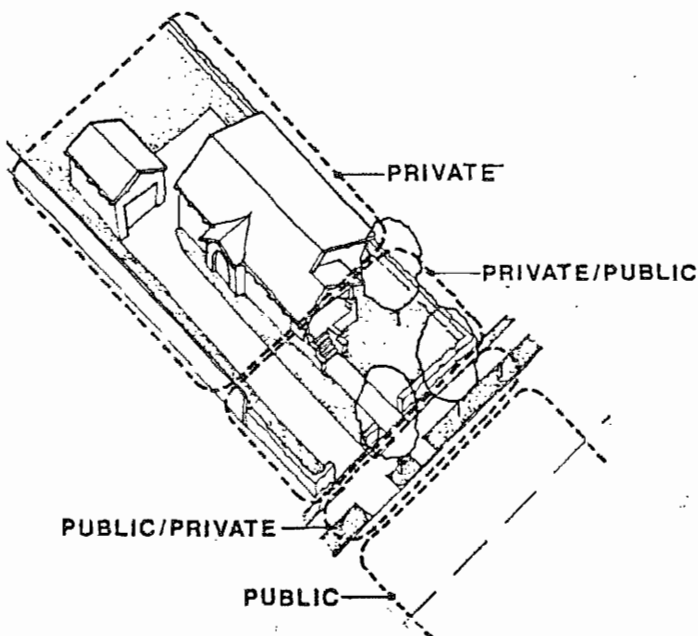
Currently, the City Attorney's Office is researching the fair market value of the respective billboards in order to determine the removal criteria. Once a "sunset" period is established for removing the billboards, it is possible to buy out the remaining lease time. This could be an expensive endeavor, however, funding resources such as CDBG could be used to buy out the remaining lease. Donations from sources such as the San Diego Community Foundation are also available for efforts associated with civic beautification.



COMMUNITY EYESORE

## 6 IMPROVEMENT RECOMMENDATIONS

Historically, in neighborhood revitalization projects it is often assumed entirely that the success of the project rests with the success of the public improvements. While it is true that public improvements can be a catalyst in creating renewed interest, and that they can be used to initiate the theme and quality standard throughout the neighborhood, it is not true that they alone will sustain a revitalized area. A neighborhood's appearance is influenced by public improvements, but it is predominantly affected by its buildings, both their character and condition, which is due to the direct involvement of property owners, residents and shop owners, see Figure 44.



**PUBLIC & PRIVATE ZONES**

**FIGURE 44**

It is because buildings influence appearance that private improvements are as valuable as public improvements. It will, therefore, be important to unify the character and upgrade the condition of the existing buildings throughout the district. Unfortunately, the scope of this document does not allow for recommendations for specific solutions to individual buildings. This responsibility will fall primarily in the hands of the new Golden Hill Community Coordinator and the property owners themselves. What follows are general recommendations for residential and commercial properties. It should be expanded as specific properties demand.

### RESIDENTIAL RECOMMENDATIONS

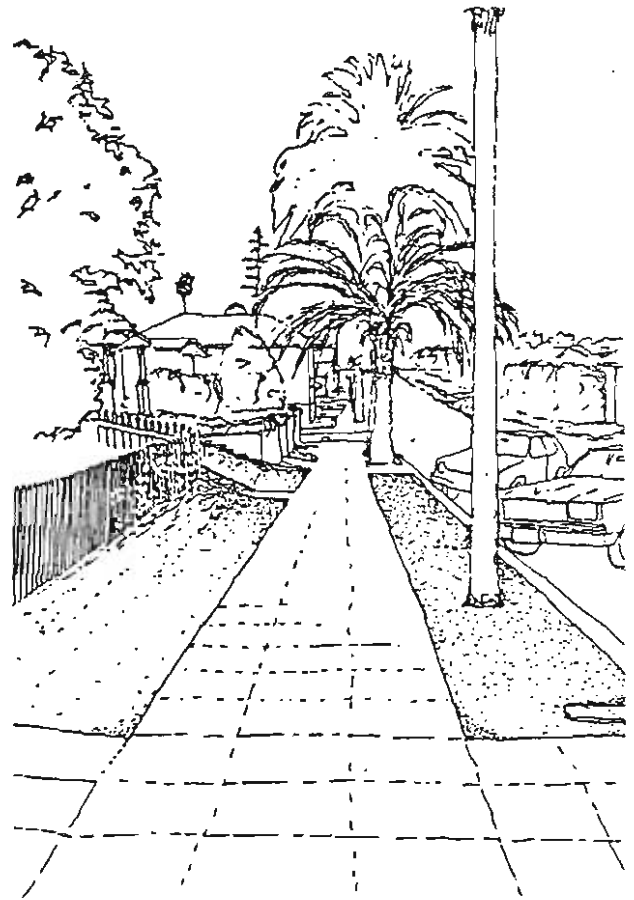
Within the Greater Golden Hill Precise Plan area is the Golden Hill Planned District. According to the Golden Hill Planned District document, it "is intended to preserve and enhance the cultural aesthetic, and economic value of Golden Hill". To this end, a Planned District Ordinance (P.D.O.) has been approved, with associated Design Criteria and Guidelines, for the area.

This district is located in the southwestern portion of the planning area and does not include the Fern Street and 30th Street study area.

At some time in the future the City should extend the P.D.O. boundaries to include the Fern Street and 30th Street study area. Until this occurs, the design criteria and guidelines of the P.D.O. should be followed for both rehabilitation and new construction, pages 47-57.

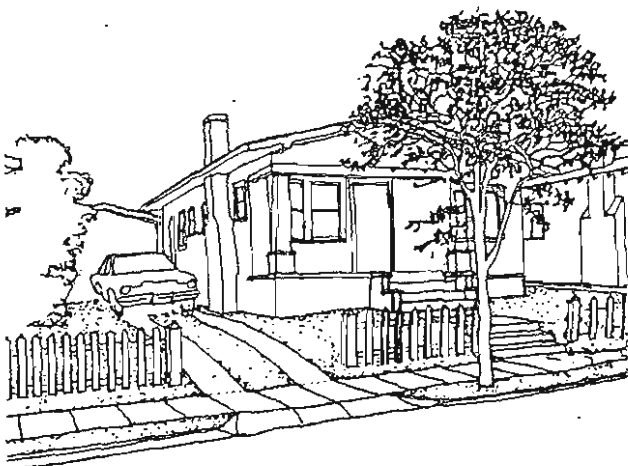
It should also be noted that the U.S. Department of the Interior has developed "Standards for Rehabilitation" which should be consulted prior to starting new work on existing buildings. This document is included in the P.D.O. as Appendix A, pages 63-76.

In conjunction with improvements to buildings, considerations for the semi-public space is important to the overall image of the neighborhood. Several methods of private involvement in this public act are illustrated in Figures 45, 46, and 47.



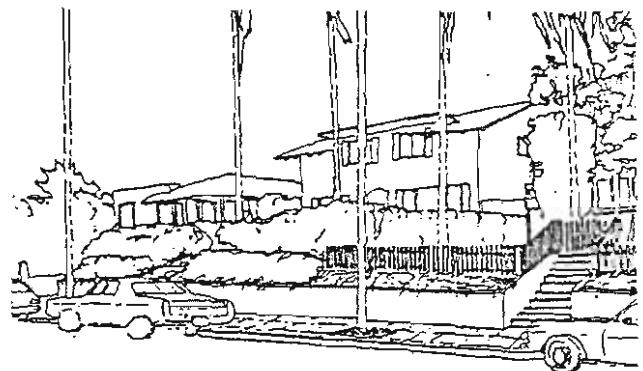
PUBLIC/PRIVATE ZONE

FIGURE 45



PRIVATE/PUBLIC ZONE

FIGURE 46



ZONE INTERACTION

FIGURE 47

## COMMERCIAL RECOMMENDATIONS

The design of commercial buildings and districts in the early part of this century are notable and appreciable today for several key characteristics. Most of the commercial buildings in the area today were built at a time when craftsmanship, quality materials, human scale, and pedestrian comfort and convenience were the dominant themes. A return to these priorities, which can also be seen in other early century San Diego communities such as Kensington and Uptown, can make this area the delightful neighborhood center it once was.

The most essential ingredient for commercial revitalization is already in place, that is, an existing sound (though neglected) building stock. The other ingredients needed are associated public improvements, as outlined in other sections of this study, sympathetic zoning laws, general guidelines for building improvements, and financial incentives for public improvements.

The commercial building stock is concentrated in two areas, as discussed previously, which are referred to as Grape Street Square and Beech Street Center. Each area is scaled to serve its own base of surrounding residents. For public improvements, these two areas receive more detailed references in the section following.

In 1986 the City Council adopted the "Pedestrian Commercial Overlay Zone" for "any commercially zoned area of the City which is designated for pedestrian-oriented uses". As quoted from the ordinance, it is meant to accomplish the following goals:

1. Implement pedestrian oriented commercial districts as proposed in adopted Community Plans.
2. Preserve and encourage a compact and continuous pedestrian environment in selected commercial areas by creating street frontage conditions conducive to a rich diverse and pleasurable walking experience.
3. Establish criteria and standards for the location and development of new buildings, and for the location of parking and vehicular access in a way that minimizes disruption of pedestrian continuity.

This legislation has been adopted to two areas in the Uptown community, and, like Uptown, the few "modern" buildings which have been built in this area have ignored the existing pedestrian character of the neighborhood.

It is, therefore, strongly recommended by this study that the City Council also adopt the pedestrian commercial overlay zone for the commercial zones

in the Fern Street and 30th Street study area.

Further deterioration of these special commercial districts must be curtailed. For existing commercial building improvements we are recommending general guidelines in the areas of building rehabilitation, maintenance, and signage. By making small improvements in these three areas a large change will be perceived in the upgrade of the commercial districts, Figure 48.

**REHABILITATION**

Rehabilitation of existing buildings can mean accurate historical restoration, a major



**COMMERCIAL DISTRICT UPGRADE**

FIGURE 48

undertaking at its extreme. For this neighborhood, however, we are not recommending this degree of reworking. Much of the building stock has been stripped, altered or severely neglected so historically accurate restoration would be both difficult and expensive. Storefronts can easily be improved, in a more conservative vein, by minimizing the less attractive features and adding simple, inexpensive elements to emphasize positive features.

New paint

Utilizing the buildings existing characteristics, applying a new coat of paint and highlighting architectural details with contrasting colors can have significant visual affect. Removing a "modern" sign or earlier, trendy, or unnecessary remodeling elements can create a whole new image at minimal costs.

Awnings

Fabric awnings, a popular facade element at the turn of the century, can be added for either shading or as a signage backdrop. In a simple fashion they add color, texture, and scale to a storefront making it visually appealing to the customer and affordable by the owner.

Cleanup/Maintenance

Building maintenance is the second significant category of commercial building improve-



ments. As can be witnessed in Golden Hill, all materials will wear out given time. The rate and degree of deterioration can, however, be controlled to a great extent with a maintenance program. And, from a cost standpoint, it is less expensive to maintain a building consistently than to allow it to deteriorate to a point where major investment is required.

Building owners who have sound buildings now, or go to the effort of rehabilitation, should continue to keep in mind that the condition of a building seems to represent the attitude of a business toward itself and its customers. A shabby storefront insinuates shabby service, a bad situation for a neighborhood store where personal service is one of its greatest strengths.

## SIGNAGE

Signage is also a major factor of the image of a commercial district. Because these neighborhood commercial centers exist to provide goods and services, almost every merchant, office, and public facility requires some type of sign to announce their function. As an integral part of these services, signs play an important role in the revitalization of downtown neighborhoods.

### Scale

The primary purpose of a sign

on a neighborhood facility is to let a new customer know where a business is located. It should also provide the potential customer with an idea of what products are being sold and reflect the image of the business. A successful sign in Golden Hill will recognize the scale of its surroundings and be oriented towards the pedestrian customer, being smaller and slower-paced. Large, flashy signs designed at scales and speeds for a highway commercial strip will overwhelm this neighborhood; the high expense will not be justifiable when compared to its effectiveness. To be effective, a sign should exhibit all of the following characteristics: legibility, clarity of wording, placement, attraction, and durability.

It will be important for the individual shop owners to consult with the Community Coordinator when considering new signage. Without coordination, a district's signage can become diverse, chaotic, confusing, and eventually ineffective. This negative impact could happen rather rapidly when one considers the inherent variety which occurs with the variety of business uses coupled with concerns for signage costs, placement, materials, illumination, color, lettering style, and size. On a more positive note, however, we must keep in mind that a coordinated sign program can give Grape Street Square and Beech Street Center a fresh new look, and a visible and relatively easily achievable record of accomplishment.

## FUNDING AND IMPLEMENTATION

Public financial assistance for business or storefront improvement within the Fern Street and 30th Street study area does not presently exist. Financial assistance is available from private sources such as commercial banks and savings and loan associations.

These traditional sources for financing commercial improvements often lack the beneficial terms (interest rate, money down, long-term repayment schedule) normally associated with public agencies. A wealth of programs, however, become available to local merchants and property owners once a Business Improvement District (B.I.D.) or Local Development Corporation (L.D.C.) is formed to administer programs within the study area.

For example, within the Mid-City commercial area, two financing programs have been established to assist area merchants. The objective of these programs is to encourage the participation of property and business owners in the revitalization effort with the overall goal of improving the area's business climate.

A loan program is available in several commercial areas within Mid-City to provide affordable property improvement financing. Administered by a Bank of America branch bank, the loans may be used to make facade and certain interior improvements.

Depending upon the proposed use of funds, loans may be made to either a business or a property owner, and may or may not be in conformance with the bank's standard lending criteria.

The second financing program within the Mid-City commercial area is a rebate program. The rebate program reimburses participants for a portion of the cost of facade improvements. Participants will be reimbursed up to \$5,000 or one-half of the cost incurred, whichever is less. The rebate applies only to improvements which are accepted in accordance with the design guidelines established for Mid-City.

The rebate program is similar to a revolving fund. Basically, a revolving fund is a pool of money made available to finance renovation or preservation projects that conventional financing sources will not (or cannot) assist. The source of the pool for a revolving fund may be entirely public funds, entirely private funds, or a combination thereof. Many cities use Community Development Block Grant funds to establish loan pools.

For further information about establishing loan programs and their applicability to the study area, contact the Economic Development Division, 236-6847.

Assistance is also offered through the Small Business Administration (S.B.A.). Bus-

iness financing is available through the 7(a) 502 and 503 programs. The 7(a) loans are direct loans, immediate participation loans and loan guarantees for small businesses. Loans can be used to finance construction, rehabilitation, equipment, and working capital. For further information about S.B.A. programs contact the San Diego County Certified Development Corporation, 234-8811.



## BEECH STREET CENTER

As residential neighborhoods developed in Golden Hill, small commercial districts also developed to support the neighborhood's daily needs. Food markets, drug stores, dry cleaners, and the like, developed at convenient locations throughout the area. Most homes are within a five minute walk to one of these neighborhood centers. They were also originally located near trolley stops.

The intersection of Beech Street and 30th Street is just such a location, though much of the scale and charm of its original pedestrian environment has been compromised by the automobile.

Large bright display posters, high power street lamps, and large unbroken expanses of asphalt and concrete, significantly alienate the pedestrian at this once vital urban crossroads. When the neighborhood first developed, the trolley, coming from nearby downtown, would travel east on Beech Street, then stop and turn north to go up 30th Street. To local residents working downtown, shopping could be a convenient part of the daily routine.

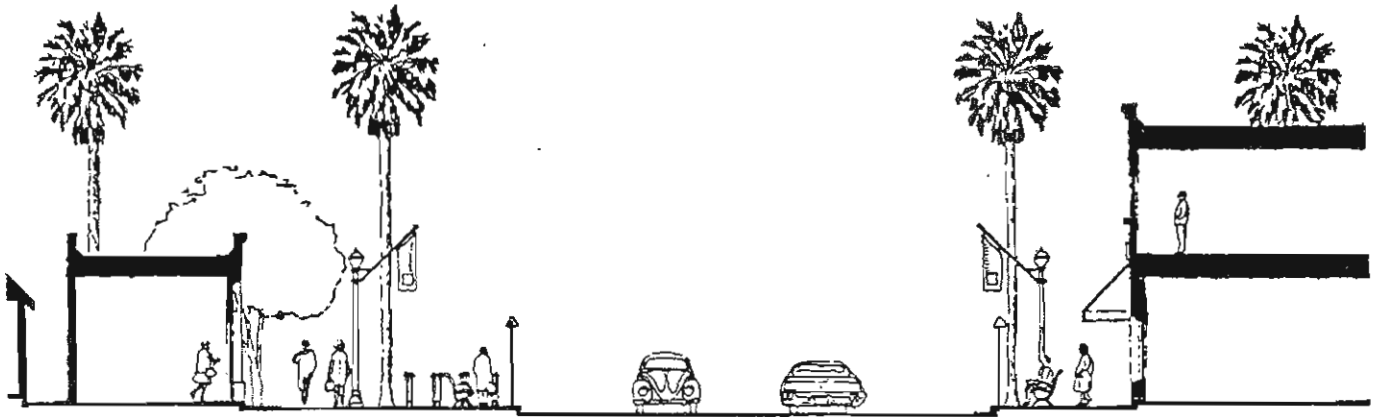
Our proposal for "Beech Street Center" is to revitalize this traditional neighborhood activity. The strategic location

of the Center is as relevant today as it was in the 1920's, and the buildings, though typically not well kept, are of sound and habitable stock (see Action Item #6). The primary tasks, then, are to repedestrianize the public spaces and to make daily shopping as easy for motorists today as it was for the trolley rider yesterday.

Convenience and comfort are two key concepts in developing pedestrian spaces. To make Beech Street Center more convenient, we are recommending the relocation of the two Beech Street bus stops from Fern Street to 30th Streets (See Action Item #2). This will increase activity and accessibility to the commercial district. To increase comfort we are recommending several streetscape improvements, including new street trees, sidewalk repair and unification, and period light fixtures. We are also suggesting several pieces of street furniture to complement pedestrian activity. The location of these pieces can be seen in the Beech Street Center Plan, Figure 50 (See also Action Item #3).

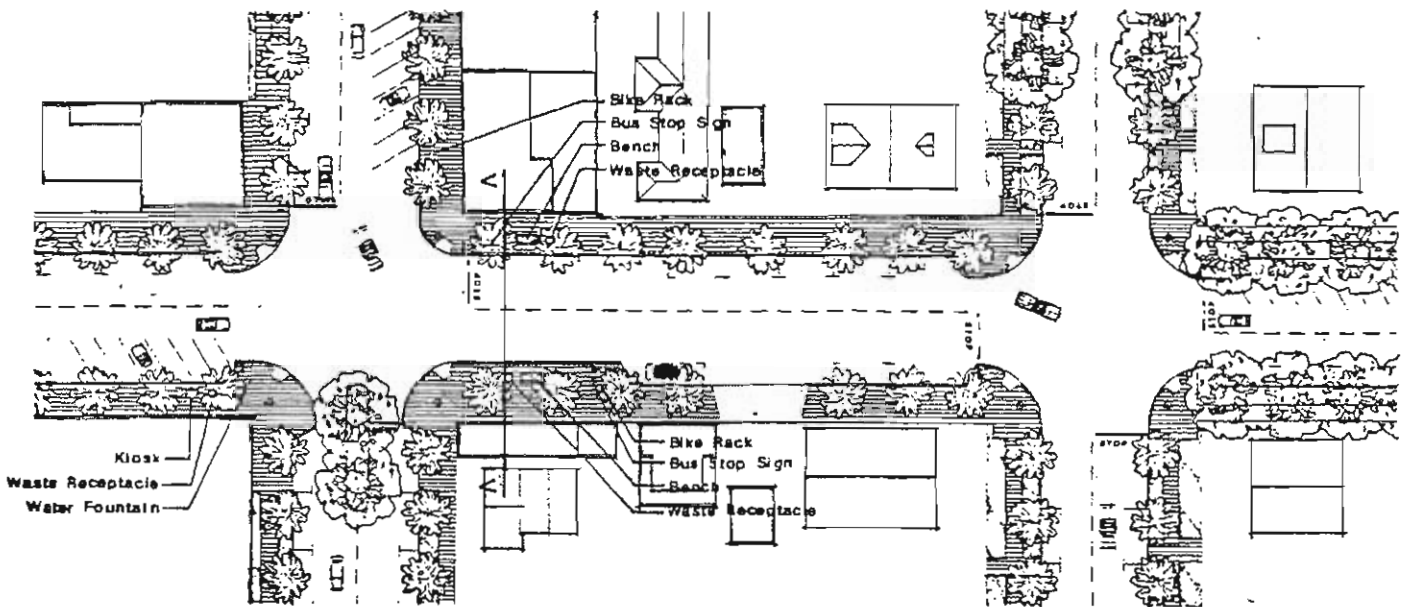
To accommodate the automobile we are recommending increased parking opportunities along Beech Street, west of 30th Street, and on 30th Street, north of Beech Street. This increased parking can be provided with relative ease by switching from parallel to angled parking in the areas shown in the plan and in Action Item #1.

By providing these improvements, in conjunction with an organized merchants association, the surrounding homes will have a pleasant and useable neighborhood focal point, Beech Street Center.



BEECH STREET SECTION

FIGURE 49



BEECH STREET CENTER

FIGURE 50

## 8 GRAPE STREET SQUARE

The neighborhood commercial needs of areas like those surrounding Beech Street Center satisfies the local residents needs on a daily basis, but less frequent and greater commercial needs occur which can also be provided at the neighborhood level. The commercial services which have developed in the Grape Street area satisfy this larger need. It provides a central location and greater services for a larger area than that served by Beech Street Center. And, like Beech Street, Grape Street commercial district developed based on an association with the trolley. In this case it was a significant stop near Ivy Street which actually bisects the block.

Besides providing similar services as Beech Street Center, the Grape Street commercial district also provides restaurants, a supermarket, a furniture store, liquor stores, bars, automobile repair, neighborhood organizations, and other collectors of commercial and social activity. This area signifies an important neighborhood service area and is attested to by several fine examples of historic commercial structures, as well as the currently very active Big Kitchen Restaurant.

The redevelopment of this commercial area into Grape Street Square, like Beech Street Cen-

ter, is as relevant today as it was in the 1920's. It, also like Beech Street, now shows the ranges of time, neglect, and insensitive modifications to accommodate the automobile.

The proposal for Grape Street Square is to revitalize this neighborhood focal point and to create a public gathering space worthy of the commercial and social needs of the study area.

The possibilities of rehabilitation of buildings (see Action Item #6) and the streetscapes (see Action Item #3) are very strong, because the primary tasks are to, again like Beech Street Center, repedestrianize the public spaces and to comfortably accommodate the automobile, and to create a clearly defined town square. The concepts of convenience and comfort are also important at Grape Street Square. There currently exists a northbound bus stop at Grape Street. For convenience we are recommending that a southbound stop also be located there (see Action Item #2). In regard to increased comfort we are recommending several streetscape improvements, again like Beech Street, but with a couple of essential additions. The first is the widening of the sidewalk on the south side of Grape Street to allow more pedestrian activity including space for outdoor dining in front of The Big Kitchen Restaurant (see Action Item #1). We are also recommending brick pavers in the sidewalks to enhance the pedes-

trian experience. The bollards and chains are located to enhance pedestrian safety by limiting pedestrian crossing and creating distinct pedestrian/auto zones near the busy and confusing intersection of Grape Street and 30th Street.

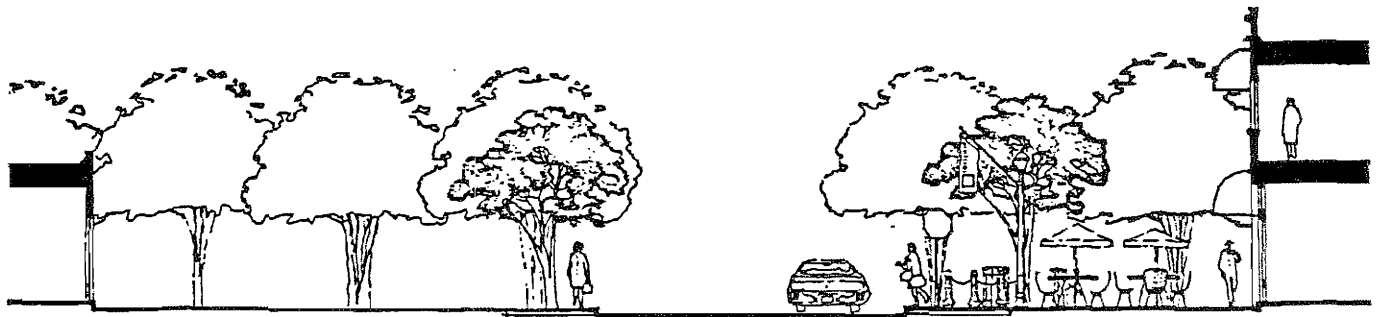
The town square is oriented by placing the streetscape elements, as previously described, in strategic locations to focus pedestrian activity in a certain location. For pedestrian safety this location is away from the traffic confusion at Grape Street and Fern Street. The spatial quality of the square is enhanced by specimen trees planted in a pattern that creates a greenroom in front of the Big Kitchen.

The planting pattern is developed in conjunction with new curb alignments at the intersections which provide more pedestrian scale, slower traffic and the possibility of block closure to automobiles (see also Action Item #1).

This location has been a popular spot in the past for neighborhood and civic gatherings and, with these new amenities, should provide even more opportunity for these types of events.

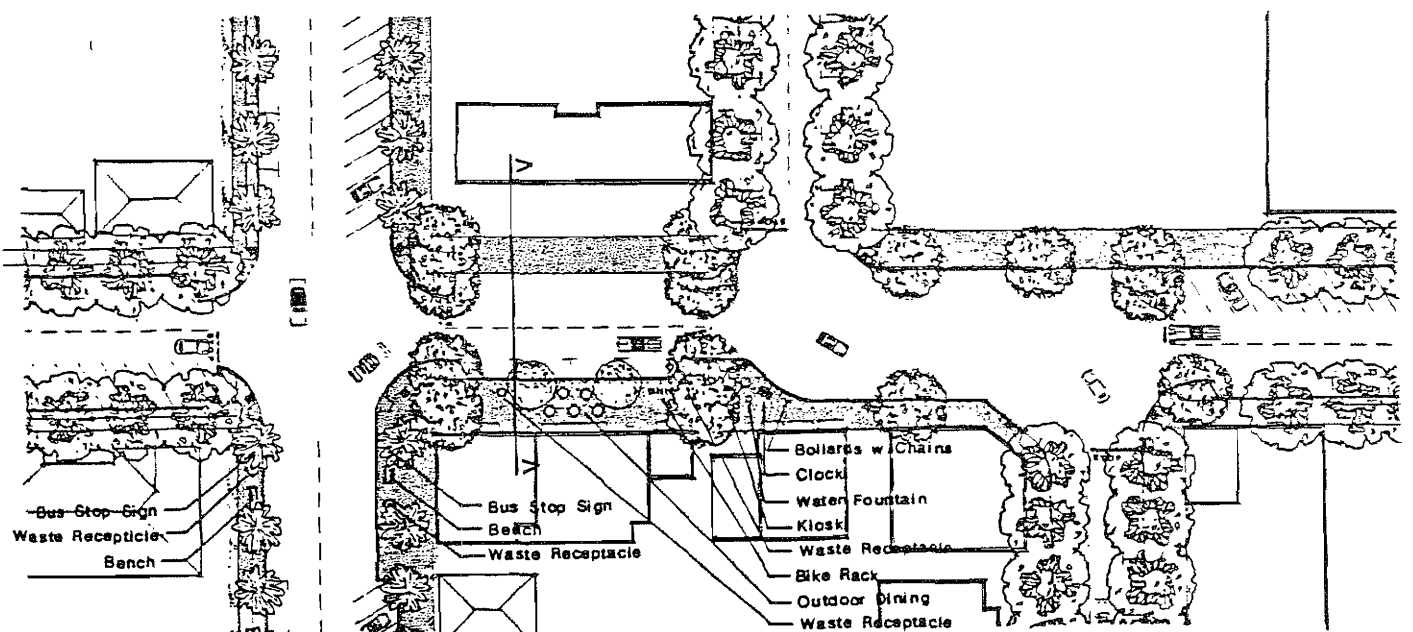
For the motorist there will be increased parking opportunities along Fern Street. The proposals for parking changes can occur with relative ease. Details can be found in Action Item #1.

When developed, it is easy to see that Grape Street can be Town Square of the neighborhood. As the rest of the Grape Street commercial district is redeveloped, Grape Street Square will be the heart that pumps life into the surrounding neighborhood. This is why a clock has been placed in the center of Grape Street Square. It is a classic symbol of a civic center; it keeps the record of the progress of the life of the neighborhood.



GRAPE STREET SECTION

FIGURE 51



GRAPE STREET SQUARE

FIGURE 52



## C. SCHEDULE OF PUBLIC IMPROVEMENTS

The schedule of public improvements recommends short and mid term (one to three years) public improvement projects that will act as catalysts to further private rehabilitation and improvement efforts. The con-

centration of public investments will begin to create a new image of Fern Street and 30th Street and provide evidence of public confidence in the future rehabilitation of the study area.

FIRST YEAR - 1987

PROJECT DESCRIPTION	ESTIMATED COST	POTENTIAL FUNDING SOURCES	IMPLEMENTATION PROCEDURES
* Complete construction documents for Phase One improvements - Fern Street Grape Street Square and Beech Street Center.	\$97,000.	CDBG Fund #18522, Department 5863, Organization 6312, J.O. 006312.	<ol style="list-style-type: none"> <li>1. Engineer and Dev. (E&amp;D) issue Request for Proposals.</li> <li>2. Select consultant</li> <li>3. Selected consultant complete construction documents.</li> <li>4. Plans reviewed by E&amp;D for city design compliance and future bidding.</li> </ol>
* Remove on-street parking from the west side of Fern Street between Grape Street (Fern Central) Restripe Fern Central to accomodate new conditions.	--	Streets Division Operating Budget	<ol style="list-style-type: none"> <li>1. E&amp;D review plans/proposals.</li> <li>2. Adjacent property owners noticed for approval.</li> <li>3. City Council review and approval.</li> <li>4. E&amp;D issue work order.</li> <li>5. Stripe for new parking conditions.</li> </ol>
* Review all-way stop proposals and one-way street designations and make necessary installations.	--	Streets Division Operating Budget	<ol style="list-style-type: none"> <li>1. E&amp;D review for conformance with Council policy 200-8.</li> <li>2. City Council hearing if necessary.</li> <li>3. E&amp;D issue work order.</li> <li>4. Install signs.</li> </ol>
* Review Study area parking plan and stripe for recommended diagonal parking stalls approval 30th Street.	--	Current CDBG fund allocation and/or Streets Division Operating Budget.	<ol style="list-style-type: none"> <li>1. E&amp;D review plans/proposals.</li> <li>2. Adjacent property owners noticed for approval.</li> <li>3. City Council review and approval.</li> <li>4. E&amp;D issue work order.</li> <li>5. Stripe for new parking conditions.</li> </ol>

## SECOND YEAR - 1988

PROJECT DESCRIPTION	ESTIMATE COST	POTENTIAL FUNDING SOURCES	IMPLEMENTATION PROCEDURES
* Initiate procedures for establishing an maintenance district for Phase One improvements.	--	Initiating department.	See appendix for a complete discussion concerning implementation procedures.
* Incorporate transit recommendations within Phase One streetscape improvements - realign bus route - relocate and improve existing bus stops.	--	San Diego Transit funds and CDBG.	1. San Diego Transit review improvement plan transit proposals. 2. Relocate bus stops and route per improvement plan recommendations. 3. Incorporate new bus stop paving design and seating as part of phase one improvements (see 3rd and 4th year programs).
* Finalize study area parking plan, stripe for recommended diagonal parking stalls on east-west streets.	--	Streets Division Operating Budget	1. E&D review plans/proposals. 2. Adjacent property owners noticed for approval. 3. City Council review and approval. 4. E&D issue work order. 5. Stripe for new parking conditions.
* Initiate review of utility undergrounding program for study area.	--	SDG&E	1. City Managers office to direct E&D to form an underground utility district for the study area and schedule for construction in 1989.
* Begin Phase One streetscape improvement (Grape Street Square/Beech Street Center).	\$284,845	CDBG	1. E&D coordinated and schedule phase one construction program. 2. E&D issue phase one construction documents for bidding. 3. Select contractor. 4. City Council approve contract. 5. Begin work.

## THIRD YEAR - 1989

PROJECT DESCRIPTION	ESTIMATED COST	POTENTIAL FUNDING SOURCES	IMPLEMENTATION PROCEDURES
* Begin construction process for undergrounding overhead utilities.	\$2,300,000	P.U.C. Case 8209 Funds	<ol style="list-style-type: none"> <li>1. SDG&amp;E to complete design/construction drawings for utility undergrounding.</li> <li>2. SDG&amp;E issue Request for Proposal (RFP) review bids.</li> <li>3. Select contractor.</li> <li>4. Begin construction.</li> </ol>
* Complete Phase One streetscape improvements in concert with utility undergrounding - sidewalk/curb demolition - new sidewalk improvements - street trees - additional street lighting - street furniture at Grape Street Square and Beech Street Center.	\$587,620.	CDBG	<ol style="list-style-type: none"> <li>1. E&amp;D coordinate and schedule phase one construction program.</li> <li>2. E&amp;D issue phase one construction documents for bidding.</li> <li>3. Select contractor.</li> <li>4. City Council approve contract.</li> <li>5. Begin work.</li> </ol>
- Resurfacing of Fern Street.	\$109,200 - 236,600	City Capital Outlay	
* Complete construction documents for Second Phase improvements - 30th Street between Juniper and Beech as well as east-west streets between 30th and Fern.	\$75,000.	CDBG	<ol style="list-style-type: none"> <li>1. Engineering and Development (E&amp;D) issue Request for Proposals.</li> <li>2. Select consultant.</li> <li>3. Selected consultant complete construction documents.</li> <li>4. Plans reviewed by E&amp;D for city design compliance and future bidding.</li> </ol>
* Initiate procedures for establishing a maintenance district for Phase Two improvements.	--	Initiating Department	See appendix for a complete discussion concerning implementation procedures.

## FOURTH YEAR - 1990

PROJECT DESCRIPTION	ESTIMATED COST	POTENTIAL FUNDING SOURCES	IMPLEMENTATION PROCEDURES
* Complete Second Phase streetscape improvements - sidewalk/curb demolition - new sidewalk improvements - street trees - additional street lighting	\$583,470.	CDBG	1. E&D coordinate and schedule phase one construction program. 2. E&D issue phase one construction documents for bidding. 3. Select contractor. 4. City Council approve contract. 5. Begin work.

# COST BREAKDOWN

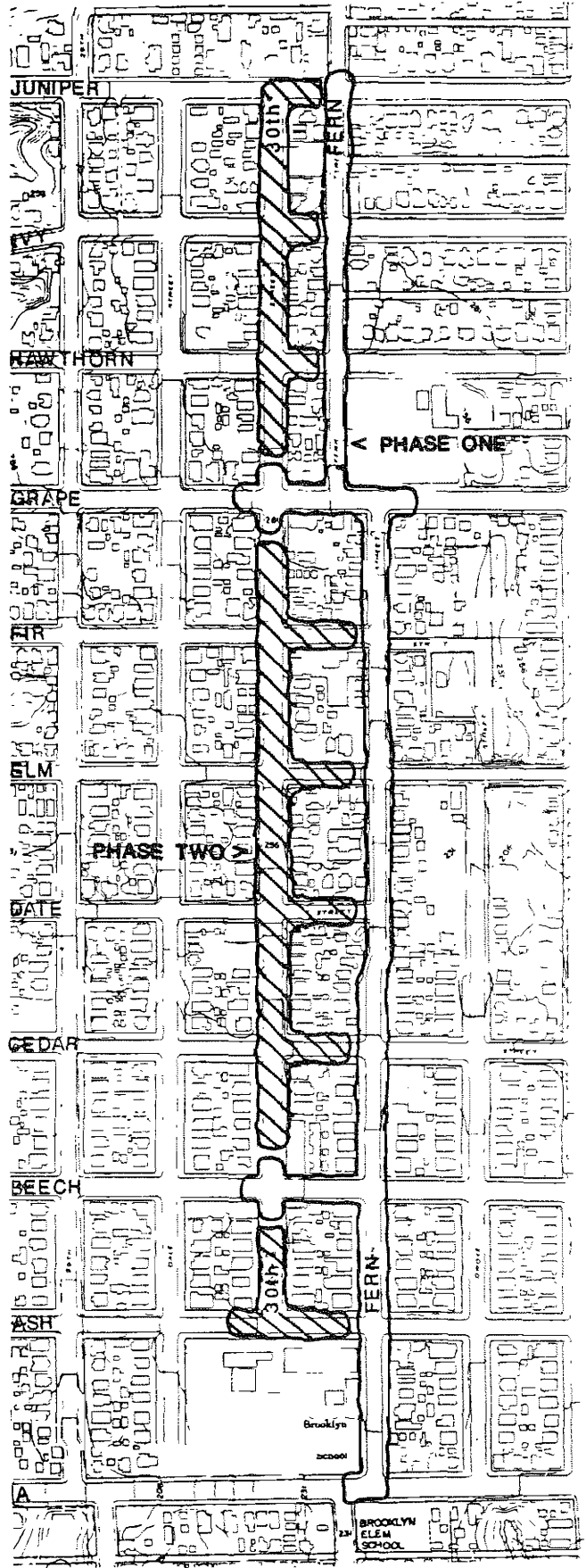
## PHASE ONE IMPROVEMENTS

<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
<u>Fern Street between Juniper Street and 'A' Street</u>			
Remove existing curb, gutter and sidewalk.	6,000 L.F.	11.00	66,000
New curbs and gutters.	6,000 L.F.	13.50	81,000
New sidewalks/paving per Fig. 29.	48,000 S.F.	3.25	156,000
Street Trees			
Broadleaf	220	225.00	49,500
Palms (12' trunk)	60	420.00	25,200
Street Lighting			
Type "C"	40	4,000.00	160,000
Irrigation	---	L.S.	21,500
<u>Grape Street Square</u>			
Remove existing curb, gutter and sidewalk.	800 L.F.	11.00	8,800
New curb and	1,200 L.F.	13.50	16,200
Street Trees			
Broadleaf	30	225.00	6,750
Irrigation		L.S.	5,500
Sidewalks/Paving Per Fig. 34	8,000 S.F.	5.00	40,000
Lighting			
Type "C"	9	4,000.00	36,000
Street Furniture			
Bench	2	1,150.00	2,300
Waste Recepticle	2	400.00	800
Water Fountain	1	1,500.00	1,500
Bike Rack (6 stalls)	1	600.00	600
Bollards w/chains	40	200.00	8,000
Kiosk	1	3,000.00	3,000
Clock	1	20,000.00	20,000

<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
<u>Beech Street Center</u>			
Remove existing curb, gutter and sidewalk	800 L.F.	11.00	8,800
New curb and gutter	1,200 L.F.	13.50	16,200
Street Trees			
Broadleaved	8	225.00	1,800
Palm	30	420.00	12,600
Irrigation	---	L.S.	5,500
Sidewalks/Paving			
Per Fig. 33	8,000 S.F.	2.00	16,000
Lighting			
Type "C"	10	4,000.00	40,000
Street Furniture			
Bench	2	1,150.00	2,300
Waste Recepticle	3	400.00	1,200
Water Fountain	1	1,500.00	1,500
Bike Rack (6 stalls)	1	600.00	600
Kiosk	1	3,000.00	3,000
10% Contingency	---	---	79,315
		Total-Phase One Improvements	\$872,465
<u>Additional required street improvement cost</u>			
Remove asphalt street section from curb to curb - Fern Street	---	L.S.	21,200 - 48,000
Adjust existing utility boxes manholes and gate valves in street - Fern Street	---	L.S.	8,000
New Asphalt street surfacing - Fern Street	---	L.S.	80,000 - 180,500
		Total	\$109,200 - \$236,600

## PHASE TWO IMPROVEMENTS

<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
<u>30th Street, Bus stops and all East-West Streets between Juniper Street and 'A' Street</u>			
Street Trees			
Broadleaf	386	225.00	\$86,850
Palm	196	420.00	\$82,320
Irrigation	---	L.S.	\$57,000
Sidewalk/Paving per Fig. 29	43,000 S.F.	6.30	\$88,000
Lighting Type "C"	31	4,000.00	\$124,000
Street Furniture (9 Bus Stops)			
Bench	9	1,150.00	\$10,350
Waste Recepticle	9	400.00.	\$36,000
Bus Stop Pop Outs/	---	L.S.	\$12,350
New Curb/Gutters	---	L.S.	\$33,600
10% Contingency	---	----	\$53,000
Total-Phase Two Improvements			\$583,470



PHASING MAP  
FIGURE 53







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## A. ADMINISTRATIVE IMPROVEMENTS

### ASSESSMENT DISTRICT BUSINESS IMPROVEMENT DISTRICT

Reproduced from, LIGHTING AND LANDSCAPE DISTRICTS  
AND BUSINESS IMPROVEMENT DISTRICTS  
IN THE CITY OF SAN DIEGO, Author, Jack Brandais

## Introduction

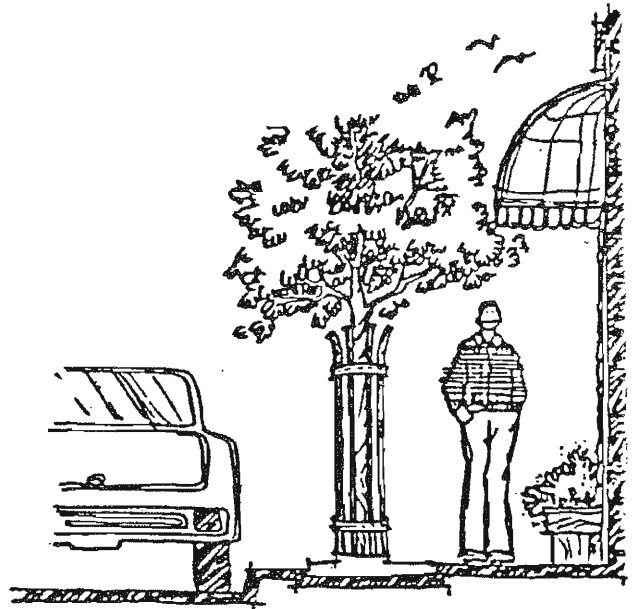
Part of the beauty of San Diego is the diversity of its component parts -- the communities. Mission Beach is different than Tierrasanta, which differs from South Bay Terraces and Rancho Bernardo. Part of what gives these communities their unique character are their business districts, street landscaping and open space.

Preservation of this uniqueness is what these districts are all about. Distinctive olive trees on medians are kept up in Rancho Bernardo, groves of eucalyptus trees and ponds are maintained in Scripps Ranch and a business area is helped to thrive in Hillcrest, all through use of special districts.

Although this booklet will discuss both Lighting and Landscaping Maintenance Districts and Business Improvement Districts, the two are unique. As used by the City of San Diego, maintenance districts for lighting, street landscaping and open space are a valuable tool that allows individual neighborhoods or communities to "keep things up" and maintain a unique character. In addition, new landscaping and lighting can be installed through use of a district.

Business Improvement Districts allow a community commercial area to band together for joint advertising, to hold events or increase parking, all to attract more customers to the area. Physical improvements such as sidewalks, benches or signs can also be made and maintained by the district.

This booklet will give the reader an outline of the laws governing the districts within the City of San Diego. Also summarized are each of the 18 Landscape and Lighting Districts, as well as the five Business Improvement Districts. More detailed information on any of the procedures described can be found in the documents listed in the bibliography. This manual is meant only to provide an outline in what the landscape and business improvement districts are, as well as how they are being used in the City.



**Lighting and Landscape Districts**

Bay Terraces Landscape Maintenance District  
Campus Point Landscape Maintenance District  
Carmel Mountain Ranch Landscape Maintenance District  
Coronado View Landscape and Lighting Maintenance District  
Downtown Street Tree Maintenance District  
Eastgate Technology Park Landscape Maintenance District  
Gaslamp Quarter Lighting and Landscape Maintenance District  
Lopez Canyon Stormwater

Retention Basin Maintenance District

Mira Mesa Landscape Maintenance District  
Mission Boulevard Landscape Maintenance District  
North City West Landscaping

and Lighting Maintenance District

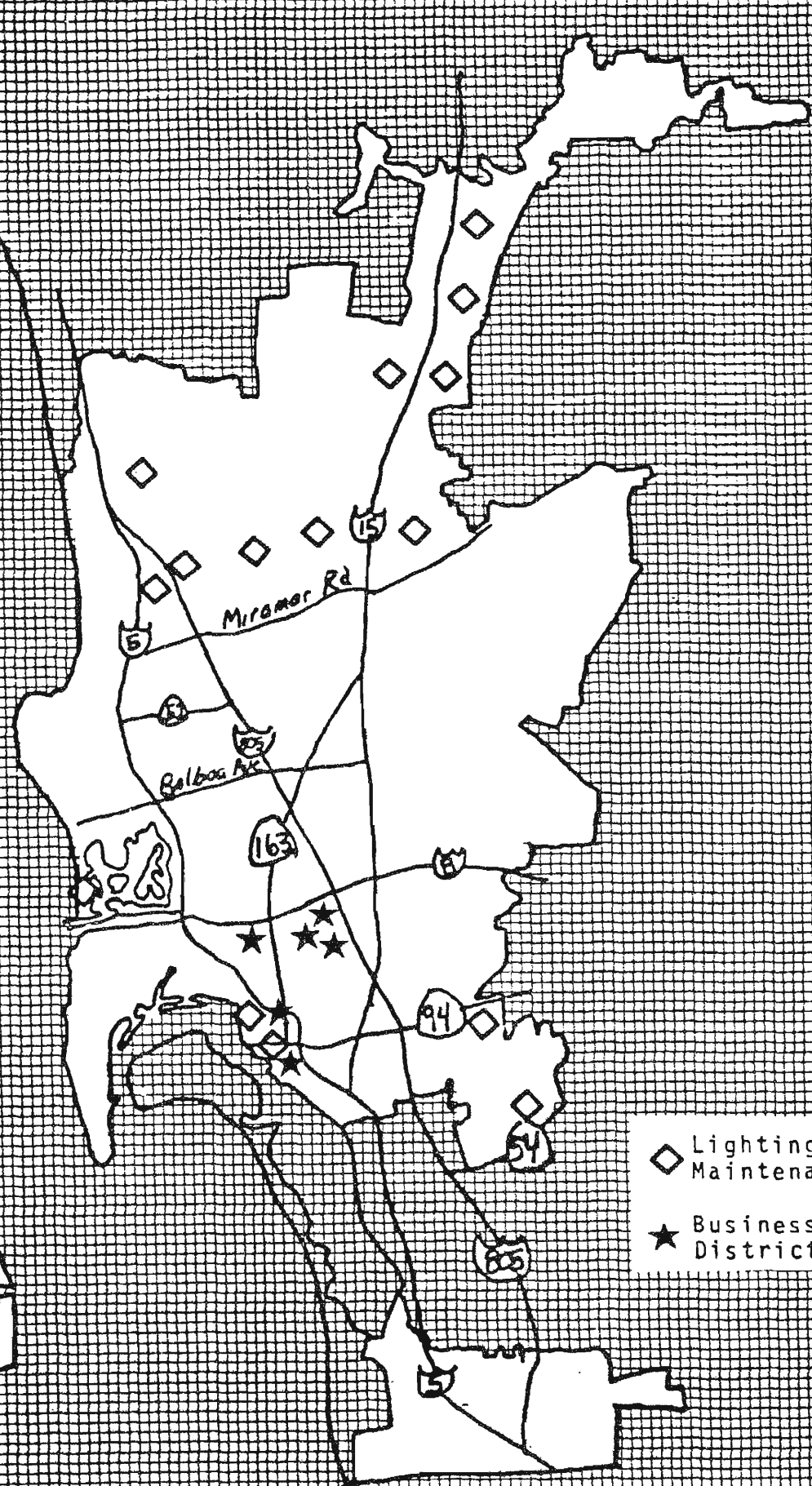
Park Village Landscape Maintenance District  
Penasquitos East Landscape Maintenance District  
Rancho Bernardo Landscape Maintenance District  
Sabre Springs Landscape and Lighting Maintenance District  
San Diego Street Lighting Maintenance District No. 1  
Scripps-Miramar Ranch Landscape

and Lighting Maintenance District

Tierrasanta Landscape and Lighting Maintenance District

**Business Improvement Districts**

Adams Avenue Business Improvement District  
Downtown Improvement Area  
El Cajon Boulevard Business Improvement District  
Gaslamp Quarter Business Improvement District  
Hillcrest Business Improvement District  
North Park Business Improvement District



- ◇ Lighting and Landscaping Maintenance Districts
- ★ Business Improvement Districts

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**Part I:  
Lighting  
and  
Landscape  
Districts**

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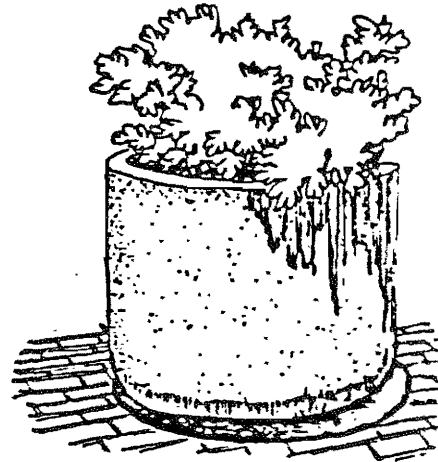
### Why Form Lighting and Landscape Districts?

Unique landscaping and lighting are two of the things that give a neighborhood its character, in fact this can even be the symbol of the community: Scripps Ranch has its eucalyptus trees, the Gaslamp Quarter has its unique street lights. Preservation of this character often times requires more funds for maintenance than the full City budget can absorb, so the City has encouraged the establishment of Lighting and Landscape Maintenance Districts.

These districts allow neighborhoods to go beyond the city's minimum maintenance of open space and street medians to provide appropriate levels of upkeep. Additional items such as "streetscapes" (landscaped areas from the curb to a private property line, including hillsides), along with additional and uniquely styled street lights can also be maintained by the district.

Landscaping: Prior to the early 1970s, the city had a program of landscaping street medians on major thoroughfares, landscaping which is still maintained by the City. Plants requiring a low level of maintenance were installed so City crews only make infrequent trips to trim trees, fertilize and weed. Open space areas not covered under districts receive annual visits for weeding and other upkeep. This work is paid for out of the Environmental Growth Fund, which is made up of

money received from San Diego Gas and Electric Company's franchise fee.



In general, "streetscapes" are not maintained under the city's street median maintenance program. These areas are located in the street right-of-way between the curb and private property line, usually where the property fronts on an interior street. For example, Clairemont Mesa Boulevard in Tierrasanta has an extensive streetscape system maintained by the local Lighting and Landscaping District. Plantings in the streetscape carry on the theme of the median, with trees and shrubs creating a park-like atmosphere. Streetscapes can also include hillsides within the right-of-way, such as in the Coronado View district. There, a slope alongside 60th Street was planted by a developer and is now maintained by the district.

Street Lights: Similarly, street lights are provided for safety at needed locations such as intersections and are paid for by the City's share of the state gasoline tax. Any additional street lights (officially called "ornamental lighting") are the responsibility of individual districts.



New communities such as North City West have had districts set up before the development was finalized, a step which eases formation. Some newer community plans even call for such districts to be established. The Sabre Springs plan, for example, calls for "...assessments through a community-wide open space maintenance district..." In addition, districts have been used in new business developments. Campus Point Technology Park uses a district to maintain open space, while Eastgate has large lawn areas, trees and flowers along its main street, as well as open space, all of which will be maintained through special districts.

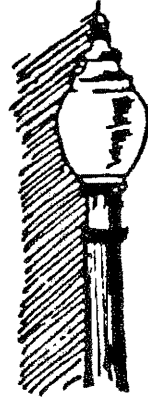
Within districts, special zones can be set up. For example, if one area has adjacent open space that is used only by its residents, those properties can be assessed separately.

**Financing:** The City makes contributions to both street median landscaping and open space maintenance. The 1985-86 contribution from the fund for open space maintenance was \$25 per acre, for a city-wide total of \$26,675; medians received just over eight cents per square foot, totaling \$35,478.

Districts can also be used to finance major improvements. Current City ordinances allow financing of items such as new landscaping or additional street lights over a period of five years, with the City loaning the district the money, or the district may accumulate funds until sufficient money is saved to finance the improvements. A 1984 amendment to the state law governing districts allows for issuance of bonds under provisions of the state Improvement Bond Act of 1915 to finance improvements; this provision has not yet been employed by the City of San Diego.

## How Are Districts Formed?

**Law:** Assessment districts are authorized under the State of California Landscape and Lighting Maintenance District Act of 1972 and the City of San Diego Maintenance District Procedural Ordinance 1975. Under the provisions of these laws, the City can assess properties and provide maintenance or improvements to the area based on the amount of benefit the property will receive from the improvements.



**District's Beginnings:** The City Council establishes assessment districts. Action can be initiated two ways: Upon a recommendation by the City Manager or by the property owners in the affected area. If the property owners ask for the district, a petition must be submitted containing signatures of support from owners of at least 67 percent of the property in the area.

**Report:** The City Council then orders that a boundary map of the area be prepared, a name selected and a report be made by the City Engineer. In San Diego, this work is carried on by the Assessment District Section of the Engineering and Development Department, headed up by the City Street Superintendent.

The Engineer's report surveys the type of work the assessment will cover, needed construction, maintenance cost projections, amount to be assessed and individual assessments.

**Map:** As part of his report, the Engineer must map each parcel in the assessment district and give it a number. This can involve as few as one map, such as with the City's Campus Point Landscape Maintenance District, or in the case of the Rancho Bernardo Landscape Maintenance district, 160 individual maps are used. Each individual ownership and lot division must be noted, including individual units within condominium projects. In the case of the developing area, what today takes up only a few maps (such as one of the newest districts, North City West), may eventually fill more than Rancho Bernardo.

**Assessment Spread:** The Engineer also proposes a method of "spreading," or dividing the total assessment among the participating parcels. This determines how large (or small) each assessment will be. Perhaps the most typical method used establishes a single family residence as one unit, with all other types of properties as fractions or combinations of the single unit. Another common style is to assess based on the amount of street frontage on a particular property. Other methods include the size of the lot plus the number of stories in the building, as in the Gaslamp Quarter, or the lot size and development potential, as in Mission Beach.

**Council Proceedings:** After completion of the Engineer's report, the matter returns to the City Council. The council must then pass a Resolution of Intention, which states it intends to form the district, describes the maintenance or improvements, approves the Engineer's report and sets a date for public hearing.

The hearing is usually set in the month of July for two reasons. By City ordinance, the hearing must be within the fiscal year the money is going to be spent; the City's fiscal year runs from July 1 to June 30. In addition, because most assessments are collected by the county Tax Collector, hearings are usually held in July because the Collector's deadline (by state law) is August 10. If the City collects the assessments itself, the hearing can be set at any time, as then the City decides when to bill the property owners. A later section will discuss why the Tax Collector is used for the bulk of the collections.

**Notice:** After passage of the Resolution of Intention, written notice is sent to all property owners in the area. The notice includes a description of the area's boundary (usually a small map), the scope of the work to be done and an estimated amount of the property's assessment. Those who wish to protest must file a written protest with the City Clerk. If at the public hearing, the council decides to expand the district area, it must notify those who are being added before final approval can be given. If areas are excluded from the district by the council, no notice is needed.

Notice must be given at least 10 days before the hearing.

**Public Hearing:** The City Council's public hearing on the district will decide if the district is to be established, its boundaries, scope of the work, total assessment and each property's share. The council can take any action it decides is necessary, including changing any of the above. If it decides to expand the area, owners of the newly included parcels must be properly noticed before the district can be formed.

**Handling Protests:** Protests must be filed in writing before the conclusion of the public hearing, and must contain either the assessor's parcel number or street address of the parcel as identification. Protests can only be made by the property owner, not by tenants, and can be withdrawn at any time.

If owners of more than half of the property file protests, the Council has two choices: it can decide not to form the district or overrule the protests. A vote to overrule a majority protest requires a four-fifths "yes" vote by the Council. If protesting properties total less than half the entire area, a simple Council majority suffices for approval.

**Improvements Ordered:** If the council decides to overrule the protests, if any, it orders that the assessment be made and the maintenance or improvements begin.

## Money Matters

Collection: Assessments are normally collected by the county Tax Collector as part of the property tax bill. The Engineer prepares a list of properties to be assessed; it must be received by the county Tax Collector no later than August 10, as required by state law. The Tax Collector does not charge for this service. Sending out Landscape and Maintenance District bills in this manner makes the assessment part of the annual tax bill — if everything isn't paid, the Tax Collector can foreclose on the delinquent property. Because property owners have the option of making two payments, all assessments must be even-penny amounts.

The City can also make collections on its own, with the City Treasurer issuing bills at other times of the year. Policy in that department is to send out bills one to two weeks after assessment rolls are received from the City Engineer. During that period, the Treasurer double checks the assessments and processes the bills. Bills are due 15 days after mailing and become delinquent 60 days later. Overdue bills start on the process toward the city's ultimate enforcement tool, foreclosure.

The City must incur all costs if billing is done by the Treasurer. In addition, costly foreclosure proceedings could be initiated for very small amounts; for example, the 1985-86 charge in the Mira Mesa Landscape District was just \$2.

Government Property: The City pays the assessment on its properties with non-park uses located in assessment districts, such as libraries and fire stations. Although most open space is City owned, the City does not pay assessments on this property, nor does it pay on dedicated park land or City reservoirs.

Properties owned by other governmental entities (public schools, county, state, federal, etc.) are assessed and can be billed for the district. But, because the City doesn't have the authority to enforce collection on these lots (through foreclosure) they usually don't pay. The San Diego Unified School District pays the share on its undeveloped school sites, but it is the exception, rather than the rule. So, to avoid forcing the district to assume additional costs, the City pays the assessments for the other governments.



### Renewal of District

The district must be renewed each year by council action, a process identical in many ways to the original establishment.

Engineer's Report: Gives proposed expenses and amount to be assessed for the year. Also notes changes in district services and proposed improvements for the coming year.

The report includes information provided by the City department that actually oversees the districts. For example, the City Park and Recreation Department manages a majority of the Lighting and Landscaping districts; work in the districts is contracted out to private firms in most cases. Park and Recreation's report to the Engineer includes budget projections and any new projects proposed for the new year.

Map: Assessment district maps must be updated every year. Such changes include lot splits, subdivisions and conversion of single-ownership apartments into condominium projects.

Notification: After the district has been established, City ordinance lists three cases when notice of a public hearing is sent to property owners:

1. An increase in assessment because of a change of the zone and/or formula for setting each owner's bill.
2. Lot splits with an ownership change of one of the parcels. (Just to avoid any problems, the Engineer mails to all owners affected by a lot split, not just new owners.)
3. If there is a change in the scope of the work in the district, such as more frequent litter pickup.

Under these noticing rules, when districts in new communities start to collect for the first time, all property owners affected are notified of the public hearing and proposed assessment.

Public Hearings: This follows the same procedures as establishment of the district. Protests again need to come in a written form, and the City Council may override any protests.

Annexation: If new areas are to be added to the district, the procedure is identical to establishment, except the actions affect only the area to be annexed. For example, the map need only cover the new area, and only property owners in the annexation have to be sent a notice.

A similar case is that of combining two districts into one. In this instance the assets, liabilities and assessment potential of the individual districts pass to the combined district.

**Detachment:** Certain areas may be removed from the district if it is determined that they no longer receive benefits and never will, or if they will make up a new assessment district. No notice is needed for the detachment; the Council simply passes a resolution defining the area detached from the district at the time of annual review; the updated map shows the new, smaller district. If the area removed is put into another district or a new one is created, that action requires noticing of the affected area.

**Dissolution:** If a majority of the community decides it no longer wants the district or the improvements maintained by the district, the district can be disbanded. The first step is to gather enough signatures to constitute a majority protest. This requires that owners of more than half of the total area in the district must protest, in writing, that the district not be renewed. The City Council would then have the choice at the public hearing of dissolving the district or overruling the protests and continuing the district. To dissolve the district, a resolution of intention is needed, but notice is not required. Any leftover funds become part of the city's general fund.

This action can only be taken when the district is renewed in July.

If a landscape district is dropped, funds may be needed to make sure the area needs only a minimum of maintenance. For example, in medians, plantings may be removed and the area paved over. Expenses such as these may require a final assessment before the district is dissolved.



## Operation

Who Maintains the Districts? Most Lighting and Landscaping Districts are maintained under the supervision of the City's Park and Recreation Department, although the City Council can designate a neighborhood entity as administrator of the district. Upkeep of landscaping is usually contracted out to a private company that performs the necessary mowing, clipping, weeding and other services. Contracts are issued through competitive bidding and can be for a maximum of one year, although annual renewal options can be included.

Supervision of Contracts: Park and Recreation Department (or the responsible city department) officials inspect each district at least twice a month. Larger districts may have more frequent visits, sometimes on a daily basis; in these cases a staff position is included in the district's budget. Inspectors also check for items such as vandalism or traffic accidents involving plantings or lights to determine if work in addition to routine maintenance is needed.

If an incorporated group in the district is designated as the supervisory agency, that group would monitor the contract.

Community Input: Many of the districts have a recognized board that advises Park and Recreation officials on conditions in the district and annually makes suggestions on improvements or changes in the level of service; many of these groups are local town councils. It is here that the Park and Recreation Department learns of the demands the community will place on its district, such as more frequent weeding or trash pickup on medians, or additional street lights.

Each fall, well in advance of the district's budget hearings, Park and Recreation works up the proposed annual budget for the district based on the level of service requested by the citizen group. After review by the Financial Management Department, the proposed budget is then forwarded on to the City Engineer for inclusion in his report to the City Council.

In addition, community members also assist Park and Recreation in inspection of maintained areas, either by making suggestions through local boards or directly to the department.

A proposal to allow local advisory boards to formally review and administer contracts is has been passed by the City Council. The Council, if it desires, can designate an "incorporated representative agency" of assessment district property owners to "participate in the review of proposed plans and specifications for maintenance district contracts and in the administration of such contracts," according to the ordinance. This local group will also set up the specifications and conduct bidding for the contract. Payment to the contractor will be made by the Treasurer when a notice of completion is received from the maintenance coordinator.

Should 25 percent of the property's owners ask it, under the ordinance, they could appeal the awarding of the contract to the low bidder to the City Council. The Council could then select a higher bidder, but it must state specific reasons for choosing the more expensive contract.

Accepting New Landscaping: In new tracts, developers may plant landscaping in medians, streetscapes or other areas destined to be maintained by a Lighting and Landscaping District. These improvements fall under the "two-year rule." Following the installation of landscaping, the improvements must first be accepted by the City, meaning they meet all City standards and are areas the City can maintain. The developer must then maintain the area at his expense for two years, to ensure that all plantings will survive. The district then takes over the area at the completion of the two-year "shakedown."



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**Part II  
Business  
Improvement  
Districts**

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### Why are Business Improvement Districts Formed?

Formation of a district is a way for business owners in an area to help themselves by developing more parking or holding activities designed to bring more customers through the doors. The fees, collected by the City through business licenses, are returned to the area and usually administered by a designated local board, giving the business owners a say on how their money is spent (although the City Council has the ultimate authority to decide).

Much like a shopping center creates an ambiance through its design and marketing, these districts serve to mold together a collection of diverse, separately owned businesses and properties into one identifiable area. Districts have sponsored weekend events, created brochures and business directories, and advertised in local publications, all to draw in customers. The districts also provide funding for their coordinating board, which with increased revenue are better able to work as an advocate for the area.

Other examples of uses for district revenues include improvement of the overall appearance of the business area by installing and/or maintaining landscaping, painting street lights or anything else that falls under the guidelines of improving the business area. Parking and traffic circulation studies, shuttle bus service and parking facilities can also be paid for by districts.

These districts perform functions the City wouldn't usually undertake. The City may sponsor a concert in a park, but the coordinated, annual festivities and advertising controlled by the businesses in the district is something only a Business Improvement District can do.

Based on the state's Parking and Business Improvement Area Law of 1979, the districts assess businesses through business license fees, with the designated organization spending the money on any of the following, after approval by the City Council:

- °Increasing parking through analysis and other methods to increase the number of available spaces, or by developing parking lots or buildings;
- °Decoration of public areas (such as holiday ornaments);
- °Promoting public events, such as street fairs;
- °Providing music in public areas;
- °Advertising of the retail stores in the area.

### How Are the Districts Formed?

**Law:** Business Improvement Districts are covered under the State of California Parking and Business Improvement Area Law of 1979 and the City of San Diego Council Policy 900-07, adopted in March 1986. Under the provisions of these acts, additional assessments can be placed on business license fees in a specific area to support activities benefiting that area.

Council Policy dictates that the districts can include only commercially zoned land. Multi-family residences (such as apartments) can be assessed, and businesses with Home Occupancy Permits (businesses run out of residences) can be excluded, if they lie within the boundaries.

**The Local Group:** Although the City Council has all authority over Business Improvement Districts, these have generally been neighborhood-based projects designed for maximum local input and control. The districts have generally evolved from merchants' associations wanting to help the business area by funding an organized improvement effort.

Because of this neighborhood basis, City staff has concentrated on allowing as many decisions as possible to remain local. One of the first steps a local group must take is to ask the City how to form a district. Officials of the association would contact the City Manager, who would determine which department will work with the group. To date, two City departments have worked to put together these districts: the Economic Development division of the Property Department, and Planning. Which department has handled formation of these districts has depended on the one that is more involved in the area. For instance, the Planning Department worked on the Gaslamp Quarter district, as Planning was the lead agency in that area's revitalization. The Mid-City districts have been formed through Property, which is directing the revitalization program in that area.

City staff assists with formation of these districts, but it is the local merchants' association that must sell the district to the businesses in the area. The sponsoring association usually has several open meetings with the business community as the district is being formed. As of this writing, the City does not charge the district for its administrative expenses at this stage or during operation.

**Petitions, Resolutions:** For action to be taken by the City Council, a petition signed by at least 20 percent (a majority is preferred) of the business owners in support must be submitted. At the same time the petition is submitted, other documents are also completed. Generally, the City supplies the format for the legal resolution of intention, operating ordinance and contract. The merchants' association then fills out the specifics, such as fee categories, district boundaries, work program and budget.

Public Hearings: Once these steps have been completed, the matter goes to the City Council. First the Transportation and Land Use Committee hears the matter, along with any other appropriate committee. The full City Council must pass a Resolution of Intention. This resolution does not form the district, only makes it official that the Council intends to form a district. The resolution sets the time and date for the public hearing, establishes the boundaries of the district and describes the classification of fees.

Unless there is a change in the fees or their structure, or the boundaries of the district are altered, no further public hearing is needed for the district. The only action required annually by the City Council is review of the budget and approval of the contract with the local operating group.

Fees: The district is supported by charges added to business license fees and are based on the amount of benefit each type of business will receive from the district's activities.

Fees can only be increased or the structure changed through a new City Council public hearing. If there is no public hearing to increase or change the fees, they will remain the same for the life of the district.

Each district tailors the benefit categories depending on the type of business, whereas zones are based on location. Although labels on categories can vary from district to district, the following patterns are usually used. Businesses in Zone "A" are charged more than Zone "B" because "A" businesses are in the area of most activity by the district. Category "A" businesses are usually those such as retail stores and restaurants that directly benefit from having more customers attracted to the area.

Category "B" businesses are those businesses that benefit from a general enhancement of the area, but don't depend as much on foot traffic, such as manufacturers, storage facilities, laboratories and secretarial services. These businesses benefit from the general enhancement or business activity improved by the district work, but are not significantly improved because of the district work.

The resolution establishing the district will specifically identify which types of businesses qualify for each category, so when the City Treasurer bills the businesses for the district, they will receive the correct bill.

The district's proposed budget, included in the Resolution of Intention, outlines the specific program the district will undertake, such as promotions, security, advertising, business recruiting, or new signs, plus administrative costs.

Notice: Each business that will be assessed by the proposed district must be sent a notice. The notice includes a copy of the resolution of intention, as well as the time and place of the public hearing. Notice must be given at least 10 days prior to the hearing date.

Protests: Protests must be filed in writing with the City Clerk. Protests are counted up based on the amount of total assessments, rather than simply the number of businesses protesting. For example, if one business is to pay half the total assessments for the district and it protests, then 50 percent of the proposed district is counted as being in protest. For example, if half the businesses in the district protest, but together they pay only 25 percent of the assessments, they are counted as one-quarter of the district being in protest. If a majority protests, state law dictates that the district cannot be formed.

Approval: If a majority is in favor, the City Council can approve the district and order that the collections be made. At the same hearing, the Council also designates the agency that will receive the funds and make the improvements.

### Operation

Once the district is established, the City's role becomes limited to collecting the fees and reviewing the activities of the operating group.

Collection: The City Treasurer collects business license fees each month; each business renews annually in the month it first received its license. As such, funds for Business Improvement Districts are not received in one lump sum, but trickle in through the year. Some months may have more renewals than others, making the cash flow even more erratic. For example, a large portion of the licenses in the North Park district are renewed in June and July; between the district's first collection in October and June, only half of the renewals were due. Districts build up reserves to allow operation through the lean times.

Budget: Each December, the operating group submits its budget to the City's Financial Management Department for review before it is submitted to the City Council for approval. The budgets usually list the categories within the work program and corresponding amounts the operating group plans to spend the money on, allowing maximum flexibility during the year. Generally, most of the funds collected are spent on project costs.

The City advances the districts funds to cover two months worth of expenses. Operating groups pay bills out of this advance, then submit receipts to the City monthly for reimbursement, thereby replenishing their checking account.

These districts operate on balanced budgets; they cannot spend more than has been collected. If they exceed this spending limit, Financial Management can hold up reimbursement until sufficient funds are available.

Work Performed: Categories of work performed by districts have been interpreted to include a variety of items, including improvements to the area in general. This includes sidewalk cleaning, landscaping and, in the case of the Hillcrest district, lighting and upkeep of the landmark Hillcrest sign, suspended over the corner of Fifth and University avenues. Operating groups can themselves contract out this work, but all contracts must first be approved the the City Manager.

Collection Chart: The following chart appears in each of the pages describing the individual districts. Each district has determined the degree of benefit each type of businesses receives from the district. The charges for these categories are listed under Categories A and B. Zone refers to the geographical location of the business, whether it is in the primary area of the district that receives the most benefit, or

in a secondary benefit zone. Locations of the zones are identified in each district description.

For example:

<u>Category</u>	<u>A</u>	<u>B</u>
Zone 1	\$70	60
2	50	40





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**B. PEDESTRIAN COMMERCIAL OVERLAY ZONE**

## PEDESTRIAN/COMMERCIAL OVERLAY ZONE

## SEC. 101.0458

## A. PURPOSE AND INTENT

This section is intended to provide the means to accomplish the following goals:

1. Implement pedestrian-oriented commercial districts as proposed in adopted community plans.
2. Preserve and encourage a compact and continuous pedestrian environment in selected commercial areas by creating street frontage conditions conducive to a rich, diverse and pleasurable walking experience.
3. Establish criteria and standards for the location and development of new buildings, and for the location of parking and vehicular access in a way that minimizes disruption of pedestrian continuity.

## B. APPLICATION OF THE OVERLAY ZONE

After a public hearing conducted pursuant to Chapter X, Article 1, Division 2 of the Municipal Code, and upon finding that the public health, safety, general welfare and sound zoning practice will be served thereby, the Pedestrian/Commercial Overlay Zone may be applied to any commercially zoned property in the City which is designated for pedestrian-oriented uses in adopted community plans. Only those property frontages abutting a pedestrian-oriented street are subject to the requirements of this Overlay Zone. The provisions of this ordinance apply to new construction, floor area additions at the ground level, and the provision of new parking facilities.

## C. ADMINISTRATION

The Building Inspection Department shall not issue any permit for new construction, floor area additions at the ground level, or provision of new parking facilities within the Pedestrian/Commercial Overlay Zone until compliance with said zone has been determined by The Planning Director or the designated representative. The applicant must provide a complete set of plans illustrating compliance with the regulations included herein.

## D. PERMITTED USES

Permitted uses shall be those provided by the regulations of the underlying zone.

## E. DEFINITIONS

115

1. Street Wall - Any wall abutting a dedicated public street(s) from which access to the subject property could be taken.
2. Blank Wall - Any wall or garage door not enhanced by architectural detailing, artwork, landscaping, windows, doors or similar features.
3. Transparency - Any clear or lightly tinted glass, of a light transmittance no less than 40 percent, used for windows, doors and display windows.
4. Pedestrian-Oriented Street - A public street which has or is proposed to have at least one of the following characteristics at the sidewalk level: a) a variety of land uses and activities, b) building sizes and architectural details that relate to human scale, c) private development integrated with the public sidewalk which creates a comfortable, pleasant and safe walking environment. The above characteristics along the street edges at the ground level makes a street pedestrian-oriented, even if it carries large volumes of traffic.

## F. DEVELOPMENT REGULATIONS

The development regulations shall be as required by the underlying zone and this overlay zone. In case of conflict, the standards of this overlay zone shall prevail.

### 1. Ground Level Development

- a. A new building wall at the ground level shall be extended for at least 65 percent of each street frontage abutting a pedestrian-oriented street, and shall be located at the property line or within five feet of the property line. (Ground floor additions to existing buildings which increase the gross floor area of a building by less than 20 percent or 5000 square feet need not comply with the 65 percent requirement.) If a sidewalk cafe, public plaza, retail courtyard or arcade is provided, the said required new wall may be setback up to 15 feet along the portion of said cafe, plaza, courtyard or arcade. The required building wall at the street level shall have a minimum height of 13 feet.
- b. At least 50 percent of the area of all new or redeveloped first story street walls must be devoted to pedestrian entrances, display windows or windows affording views into retail, office,

gallery or lobby space. Residential buildings are excluded.

- c. Blank walls shall be limited to segments of 15 feet in width except for garage doors which shall be limited to the width of the garage door plus five feet.
- d. The building wall subject to transparency requirements shall include the portion between three feet and ten feet above the sidewalk.
- e. All new developments abutting pedestrian-oriented streets shall provide at least one pedestrian entrance into or through the structure from each street frontage.

2. Parking/Vehicular Access

- a. All surface parking adjoining a pedestrian-oriented street shall be screened by a solid wall having a minimum continuous height of three feet. In addition, the wall must be separated from any adjacent public right-of-way by a minimum continuous width of two feet of landscaped area in compliance with the adopted City-wide landscaping regulations. (Temporary, at-grade parking during construction is exempted.)
- b. All above grade parking spaces visible from the public right-of-way must be architecturally screened or enclosed.
- c. On corner parcels, parking shall not be permitted to occupy the corner facing the street intersection.
- d. Vehicular access to premises shall be permitted only from the abutting alley on those blocks shown on Map "B". (This requirement may be selectively applied or waived at the time the Pedestrian/Commercial Overlay Zone is applied to a specific commercial district.)

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**C. GOLDEN HILL PLANNED DISTRICT**

# DESIGN CRITERIA AND GUIDELINES

The following design criteria shall apply in the Golden Hill Planned District.

## REHABILITATION

Rehabilitation shall herein be defined as the process of returning a property to a state of utility, through repair or alteration, which makes possible an efficient contemporary use while preserving those portions and features of the property which are significant to its historic, architectural, and cultural values.

Rehabilitation efforts in all subareas shall comply with the following:

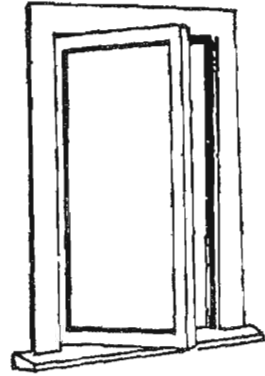
1. Every reasonable effort shall be made to provide a compatible use for a property which requires minimal alteration of the building, structure, or site and its environment, or to use a property for its originally intended purpose.
2. The distinguishing original qualities or character of a building, structure, or site and its environment shall not be destroyed. The removal or alteration of any historic material or distinctive architectural features shall be prohibited, except for reasons of repair or removal for safety.
3. All buildings, structures, and sites shall be recognized as products of their own time. Alterations that have no historical architectural relevance to the building shall be discouraged.
4. Distinctive stylistic features or examples of skilled craftsmanship which characterize a building, structure, or site shall be treated with sensitivity.
5. Deteriorated architectural features shall be repaired rather than replaced, wherever possible. In the event replacement is necessary, the new material shall match the material being replaced in composition, design, color, texture, and other visual qualities. Repair or replacement of missing architectural features shall be based on accurate duplications of features, substantiated by historic, physical, or pictorial evidence rather than on conjectural designs or the availability of different architectural elements from other buildings or structures.
6. The surface cleaning of structures shall be undertaken with the gentlest means possible. Sandblasting and other cleaning methods that will damage the historic building material shall not be undertaken.

7. Contemporary design for alterations and additions to existing properties will not be discouraged when such alterations and additions do not destroy significant historical, architectural or cultural material, and such design is compatible with the size, scale, color, material, and character of the property, neighborhood or environment.
8. Wherever possible, new additions or alterations to structures shall be done in such a manner that if such additions or alterations were to be removed in the future, the essential form and integrity of the structure would be unimpaired.
9. Architecturally important structural members shall be replaced only when necessary. Existing structural systems should be supplemented when damaged or inadequate.
10. Exterior Features:
  - a. Stucco should be repaired with a stucco mixture that duplicates the original as closely as possible in appearance and texture.
  - b. Masonry should be cleaned only when necessary to halt deterioration or to remove graffiti and stains, and always with the gentlest method possible, such as low pressure water and soft natural bristle brushes.
  - c. The original or early color and texture of masonry surfaces, including early signage, should be retained wherever possible. Brick or stone surfaces may have been painted or whitewashed for practical and aesthetic reasons.
  - d. The original wooden siding on a building or structure shall be retained whenever possible. Resurfacing with stucco or textured paint, or new materials such as artificial stone, brick veneer, asbestos or asphalt shingles, and plastic or aluminium siding is inappropriate and potentially damaging to the structure and is discouraged.
  - e. The original roofing material and shape shall be retained whenever possible. Deteriorated roof coverings should be replaced with new material which matches the old in composition, size, shape, color, and texture.
  - f. The architectural features that give the roof its character (such as dormer windows, cupolas, cornices, brackets, chimneys, cresting, etc.) shall be preserved or replaced.

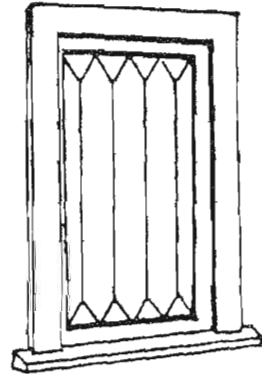
- g. Existing window and door openings including window sash, glass, lintels, sills, architraves, shutters, doors, pediments, hoods, steps, and all hardware shall be retained. The type of woodframe window (double-hung, casement, etc.) should also be retained.

## TYPES OF WOODFRAME WINDOWS

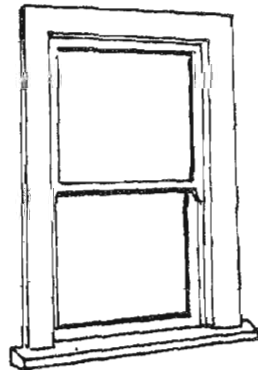
CASEMENT



FIXED



DOUBLE-HUNG



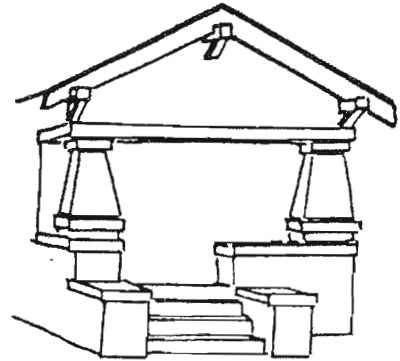


- h. Porches, railings, columns and steps that are appropriate to the building and its development should be retained. Porches or additions reflecting later architectural styles are often important to the building's historical integrity and should be retained.

Example: California Bungalow

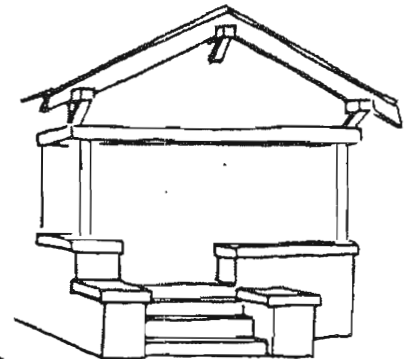
ORIGINAL ELEPHANTINE  
COLUMNS

Substantial and solid-looking  
like the house itself.

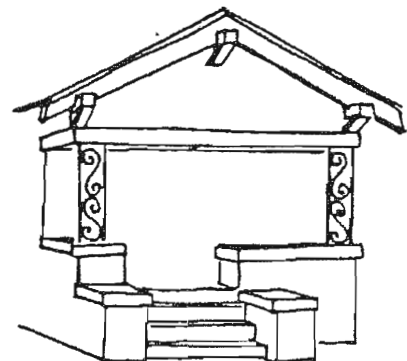


STEEL PIPE COLUMNS

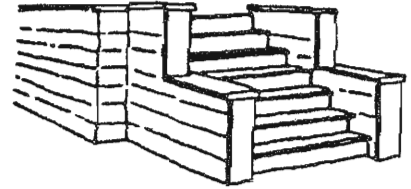
Although structurally adequate,  
they don't look sturdy enough to  
hold up the porch roof.



WROUGHT IRON STYLE COLUMNS  
Curvy and lacy in texture, they  
are very out-of-place on a  
bungalow porch.

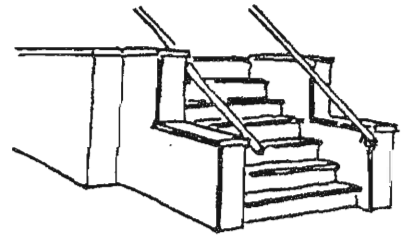


Examples: Appropriate designs for steps and handrails.

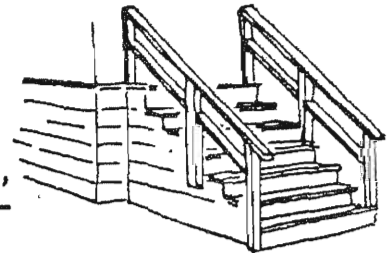


**ORIGINAL DESIGN**

Visually & physically solid uses a more appropriate material (wood), proportions echo those of house.

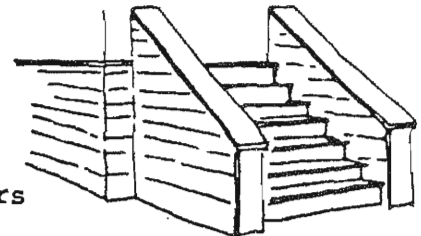


Examples: Inappropriate step and handrail design.



**RANCH STYLE RAILING**

Problems: "Back stairs" character, insubstantial quality, no relationship to house design.



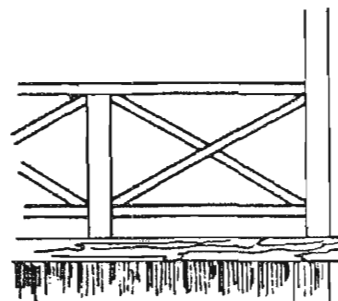
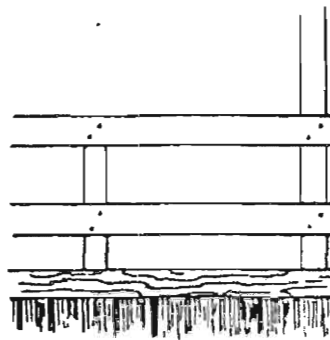
**ANGLED SOLID RAILING**

Problems: Doesn't fit square features of style, makes stairs seem narrow & enclosed.

Examples: Inappropriate designs for porch railings.

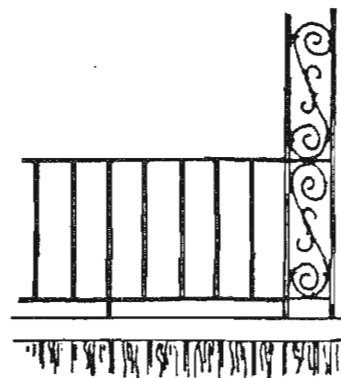
#### RANCH STYLE RAILINGS

Problem: No relationship to the square, solid features of a bungalow house design.



#### WROUGHT IRON COLUMN & RAILING

Problems: Too curvy and lacy in texture, doesn't fit the square features of style, incompatible material.

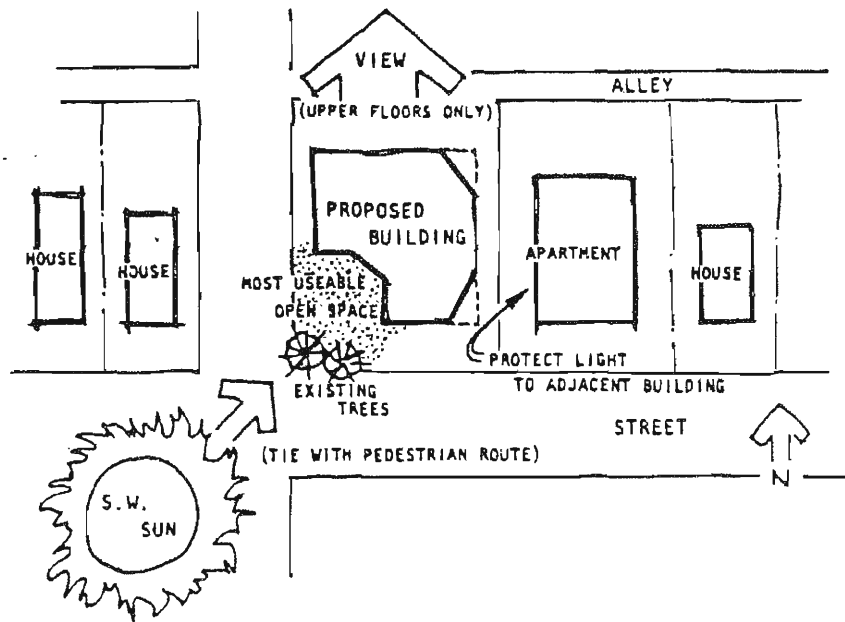


## NEW CONSTRUCTION

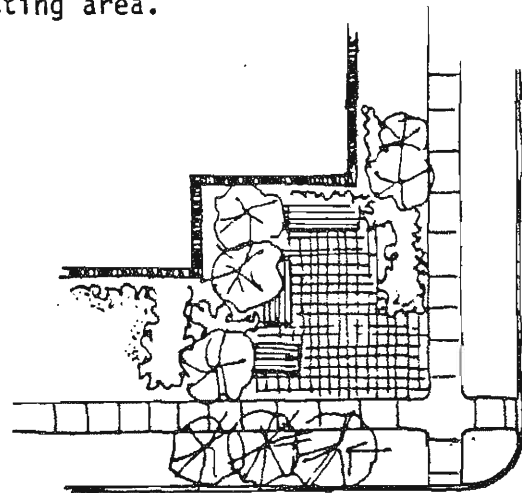
New construction in all subareas should comply with the following:

### 1. Site Conditions

- a. The proposed building should be located on the site to take advantage of sunlight, views, existing trees and landscaping and to minimize overshadowing and view blockage. On a sloping site, buildings and open areas may be terraced.



- b. Corners present the greatest potential sun, public open space and views as well as being a meeting of paths. Buildings may be setback to allow a landscaped sitting area.

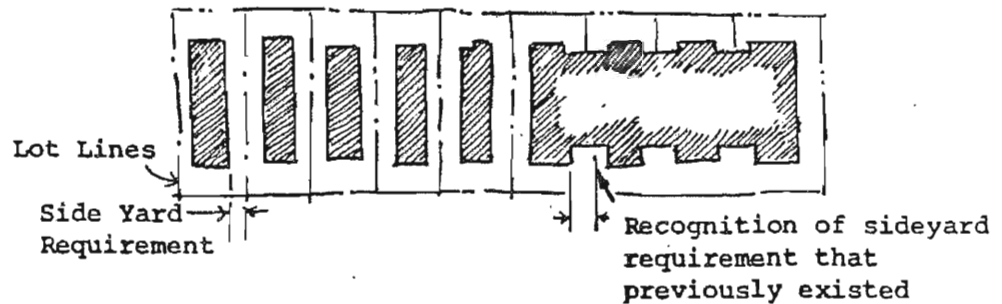


- c. When two or more lots are joined to create a single large lot, or when smaller buildings are replaced with one large structure, the design of the new building should reflect the previous sideyard requirements.



Existing & new structures built within existing lot lines appear to be harmonious.

When many smaller lots are joined to make one large lot, the result is often a structure that in no way resembles the other buildings in the neighborhood.



New building now resembles the other buildings in the neighborhood.

- d. The flat roofs of terraced buildings should be made accessible and landscaped so that they provide useable open space.

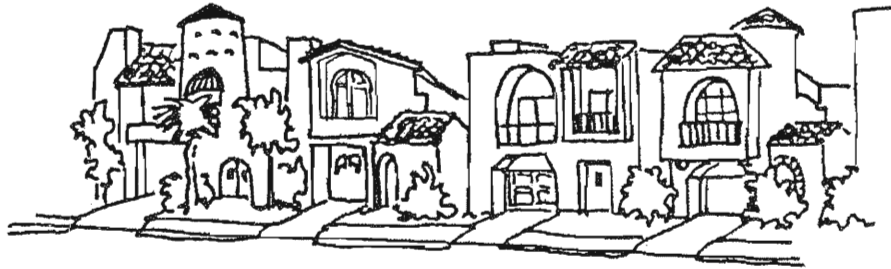
## 2. Color

Earth tones should predominate, including browns, yellows, grays, and greens. The natural colors of the exterior building materials are also acceptable. Colors such as white, black, blues, and reds should be used as accents.

## 3. Materials and Textures

Primary materials shall be wood, stucco, brick, and stone. Canvas should be used for awnings and canopies. The facade of the building at pedestrian level shall provide a relationship between the building and the street. Blank walls are considered to be detrimental to this relationship.

Use of similar surface materials assists in fitting a new building into a block.



A shingled building will not fit in well with predominately painted stucco buildings.



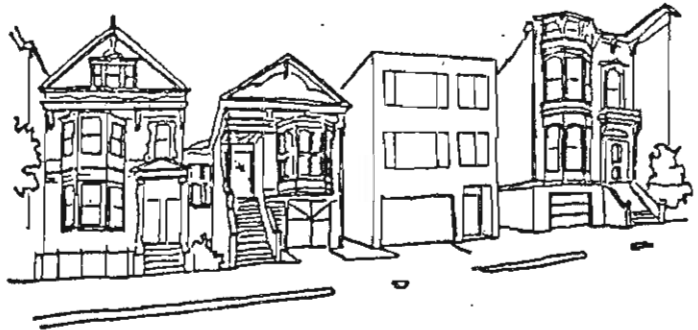
Plywood usually blends poorly with both stucco and traditional siding.

#### 4. Directional Expression

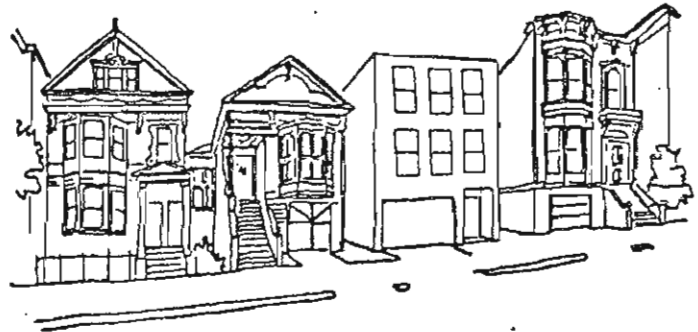
Structural shape, placement of openings, and architectural details give a predominately vertical, horizontal, or nondirectional character to a building's facade. The directional expression of the majority of the buildings on a block shall be continued by any new buildings or additions constructed. If no clear visual pattern exists, directional design options shall be open.



Buildings with horizontally shaped windows are incompatible with adjacent structures whose windows have a vertical orientation.



A simple change in proportions to correct this incongruity is less disruptive.



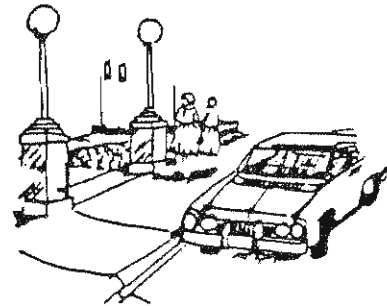
## 5. Entrances

Transitional spaces (that portion of the front or side yard between the public sidewalk and the entrance to a private building) should be delineated to help promote the street-orientation of a building and to maintain the pedestrian scale of the facade/entrance. Design elements such as low fences, shrubs, steps, changes in ground level, changes in paving texture, light standards, etc., may be used to define these spaces.

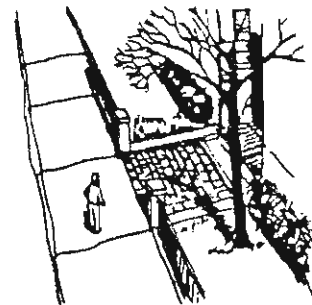
(a) Change in level



(b) Lights and standards used to define transitional space.



(c) low walls, posts, and change in texture used to define transition.





## HISTORIC DISTRICT-SUBAREA TWO

### 1. Rehabilitation

In addition to the requirements for rehabilitation in all subareas, in Subarea Two, all rehabilitation work done on the contributing structures of the Historic District shall comply with the provisions of the Secretary of the Interior's Standards for Rehabilitation (see Appendix A).

### 2. New Construction

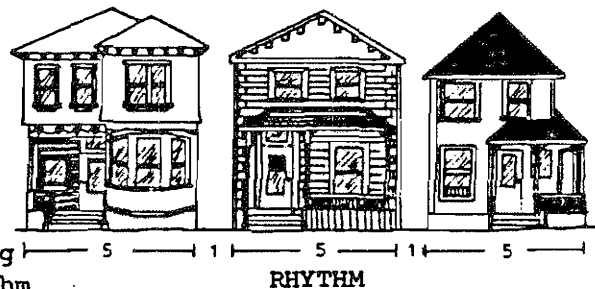
In addition to the requirements for new construction in all subareas, all new construction shall comply with the following:

- a. Scale and Proportion - New buildings shall be constructed to within ten percent of the average height and ratio of height-to-width of adjacent structures.



- b. Rhythm and Spacing - New construction shall also conform to the rhythm of spacing of buildings already existing on the street. This spacing will help to maintain an element of harmony within the Historic District.

RHYTHM OF SPACING OF BUILDINGS ON STREET—moving past a sequence of buildings, one experiences a rhythm of recurrent building masses to spaces between them. This rhythm is necessary to create an added element of harmony in a neighborhood's architecture.

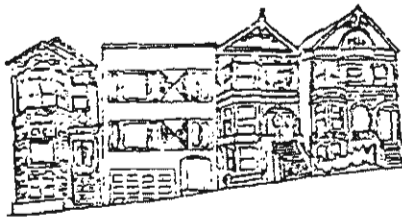


- c. Materials and Textures - The texture of the facade of a new building shall be compatible with the predominant textures (rough or smooth) of the other structures on the block.

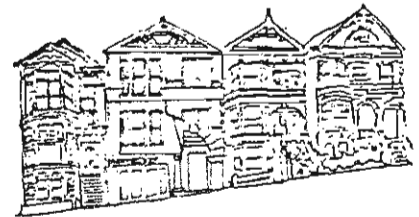
Smooth stucco finish.



- d. Detail - New buildings shall incorporate architectural detailing and design elements which are similar to the existing structures in the Historic District. Detailing may include trim, cornices, railings, awnings, shape and placement of doors and windows, etc. Any architectural detailing used should be compatible to the style of the new building or structure. New buildings should be compatible with older, historic structures, but should not attempt to replicate exactly the existing buildings



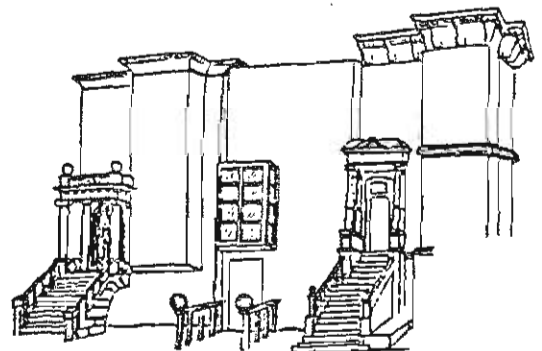
NOT THIS



THIS

The incorporation of similar architectural detailing can transform a stark focal point into a complementary addition.

The pattern can be maintained by enlarging the form of the entryway to include the second floor window above, creating a counterpart to the raised entries.



- e. Roofs - Roofing materials and colors shall complement the materials, colors, and textures of the new building. The roofline of the new building shall respect the continuity of established roofline patterns within the Historic District.

Incompatible rooflines can disrupt the continuity of a streetscape.



NOT THIS

A more compatible roofline can enhance the streetscape and complement the historic identity of the area.



THIS

## D. "WHO TO CALL"

Abandoned Vehicles - Investigator	262-1300
Abandoned Vehicles - Off-Street	236-6751
Abandoned Vehicles - On-Street	236-6566
Arson Investigation	236-6815
Benches (Private)	236-7845/236-6000
Bike Routes	236-5495
Building Relocation & Demolition Info.	236-5545
Bus Benches	236-5794
Buses (San Diego Transit)	233-3004
Business Licenses	236-6173
Business Licenses (Police Regulated)	236-6119
Community Plans - Planning	236-6417
County Government Reference Library	236-2760
Contracts (On File)	236-6420
Crime Prevention	236-6821
Crosswalks - Maintenance	236-5656
Crosswalks - New	236-6040
Cultural & Art Activities	236-5984
Curb Maintenance - City	236-5656
Curb Maintenance - County	565-5251
Dead Animals - City Streets	236-5660
Drainage - City Property	236-5656
Drainage - Private Property	236-6940
Driveway Permits	236-6000
Dumping - Illegal Dump Sites (City)	236-6751
Economic Development Project - Economic Grants	236-6039
Electrical Maintenance - Parks and Recreation Lighting (Outdoors), Primary Electrical (Outside Meter) Traffic Signals & Streetlights	236-5505
Encroachment Permits	236-7845/236-6000
Exhibits & Special Events Permits	236-6495
Fire Hazard Complaints	236-6495
Fire Hydrants	236-5600
Garbage Information	236-5660
Gas & Electric Co. - Emergency	234-6234
Gas & Electric Co. - General	232-4252
Gas & Electric Co. - Turn on and off	239-7511
Housing Authority	236-5648
Housing Rehabilitation	236-5907
Illegal Dumping	236-6751
Information (City Info. Center)	236-5555
Land Development - Assessment Districts	236-6010
Land Development - Utilities Undergrounding	236-5559
Landlord - Tenant Disputes	263-7761
Legal Aid	239-9611

Litter - City	262-1300/236-6751
Litter Control - Street Cleaning	236-5656
Litter Control - Vacant Lots	236-6751
Maintenance Districts	236-6896
New Construction - Inspection & Plan Check	236-6495
Noise Complaints - Bldg. Inspection	236-6088
Open Space - Operation and Maintenance	236-5544
Ordinances	236-6420
Park & Recreation Information	236-5740
Park Development	236-6059
Parking Meter Maintenance	236-7389
Permits - Private Construction - Building	236-6270
Permits - Private Construction - Electrical	236-6971
Permits - Private Construction - Plmb. & Mech.	236-6971
Permits - Private Construction - Signs	236-6027
Police - Central	236-6044
Police - General Information	236-6731
Police - Public Information Officer	236-6424
Pollution - Air	565-5901
Pollution - Litter	236-6751
Pollution - Noise	236-6088
Pollution - Water (Drinking)	236-5600
Pollution - Water (Other)	286-5114
Product & Materials Approvals	236-5891
Public Information Office	236-5554
Public Library Information (Central)	236-5800
Real Estate Information (City-owned)	236-6020
Records - As Built Records	236-6867
Records - Building Construction	236-6934
Records - City Clerk	236-6420
Records - Utilities Maps & Records	236-5650
Records - Zoning	236-6485
Recycling Centers - San Diego Ecology Center	235-0066
Refuse Collection - General	236-5660
Refuse Collection - Special Collection	232-0180
Rezoning - Planning	236-6460
Senior Citizen Information	236-5765
Sewer Repair	236-5600
Sidewalk Maintenance - City	236-6000/236-5656
Sign Permits (Building Inspection)	236-6027
Storm Drains - City	236-5656
Street Benches (Private)	236-7845/236-6000
Street Cleaning	236-5656
Streetlight Maintenance - City	236-5310/236-5505
Streetlight Maintenance - County	565-5233
Street Name Signs - City	236-5656
Street Repairs - City	236-5656
Street Repairs - County	565-5251
Street Signs (Illuminated)	236-5505

Street Sweeping - City	236-7063/236-5656
Street Sweeping - County	565-5251
Street Trash Receptacles Location	232-0180
Street Tree Planting	236-5744
Third District (Gloria D. McColl)	236-6633
Traffic Control Requirements	
Crosswalks, Signals, Signs	236-6040
Traffic Signal Maintenance - City	236-5505
Traffic Signal Maintenance - County	236-6514/236-5505
Traffic Signal Maintenance - Caltrans	237-6842
Traffic Signs/Markings	236-7014
Transportation & Traffic:	
Bicycle Route Analysis	236-5495
Drainage Design	236-6940
Local Street Design	236-6941
Major Street Design	236-6037
Signal & Light Design	236-5417
Street & Alley Design	236-6942
Traffic Safety	236-5762
Traffic Signal Location	236-6064
Trash - Customer Service	236-5660
Trash - General Information	236-6711
Trash Receptacle Location	232-0180
Tree Maint., Removal, Trimming - City Prop.	236-5744
Underground Service Alert of So. Calif.	1-800-422-4133
Underground Locating - Utilities (Gas & Electric)	
Underground Locating - Streetlights and Traffic Signals (City)	236-5505
Water & Sewer - Emergencies	236-5600
Water & Sewer - Turn-on, Shut-off	236-6380
Weeds	236-6495
Zoning Information - Planning	236-6490

