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## **HOUSING**

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### **GOAL**

Provide a diversity of housing options encouraging the construction and preservation of moderate- and higher-cost housing.

### **EXISTING CONDITIONS**

The critical question in Greater North Park is how and where additional residential units will be accommodated. If residential development were to continue to occur as it has in the past several decades, then additional disruption of single-family and low-density neighborhoods can be anticipated. However, if residential infill were to occur, primarily within the commercial and transit corridors, then the single-family and low-density neighborhoods could be preserved and rehabilitated.

There is also need for residential revitalization because the housing stock has aged and because the lure of new, larger suburban homes has attracted the more affluent buyers, leaving the smaller, older housing in Greater North Park to lower-income people, many of whom do not have the resources to upgrade their property. With the lack of significant undeveloped acreage in this community, the preponderance of changes in housing will result from the replacement of existing housing with new housing probably at higher densities, and from revitalization of housing through rehabilitation and/or conversion of existing neighborhoods. Because of these facts, the thrust of this element will be principally directed to revitalization of housing.

### **Housing Type and Tenure**

The housing stock in Greater North Park has grown slowly but steadily over the last 20 years and reached approximately 22,400 units in 1985. The housing can be characterized as older but generally sound, with the majority of the housing being multifamily units. Many of the detached houses built before 1940 are now being replaced with multifamily units. The southeasterly portion of the community remains mostly single-family and contains the most recent development.

Overall, 44 percent of the units are single-family and 56 percent are multifamily. Most of the multifamily units are rental apartments at densities ranging up to 100 units per acre and more, while two high-rise elderly housing projects average over 200 units per acre. Rental units comprise 62 percent of all households compared to 51 percent citywide.

Multifamily development, occurring over the last several years, has been predominantly smaller projects of less than ten units confined to parcels of 7,000 square feet or less. These projects have observed only the bare minimum requirements as to landscaping and off-street parking requirements. While there have been a number of higher-quality multifamily developments, the standard six- to nine-plex apartment development with its 25-foot curb cuts and minimal landscaping has been the typical multifamily product in the community.

During the 1960s, the number of owner-occupied housing units in Greater North Park fell from 45 percent to 35 percent, over which time several thousand apartment units were constructed. Since 1970, the ratio of owner-occupied units increased slightly, as apartment construction has slowed and condominium construction has emerged.

### **Existing Zoning**

Existing residential zoning in Greater North Park would permit approximately 44,000 dwelling units. Not only does existing zoning permit residential density in excess of what can be reasonably anticipated for the community plan area, but the existing zoning patterns permit multifamily development in areas that are predominantly single-family or low-density residential areas.

### **Cost of Housing**

The average value of homes is somewhat lower than the City average. The median value of ownership units was set at \$73,400 versus \$90,700 for the City as a whole in 1980, according to census respondents. The median rent in 1980 was \$225 as compared to \$249 for the City. The highest values and rents appear in the southerly portion of the community, where the homes are larger, newer and predominantly single-family.

### **Conditions of Housing**

Approximately 95 percent of the dwelling units in Greater North Park were identified as being in sound condition in the 1975 Special Census, while less than one percent were considered dilapidated. However, over ten percent of the dwelling units were not considered sound in two areas; south of Upas Street between 28<sup>th</sup> Street and 32<sup>nd</sup> Street, and east of Arizona Street between Madison Avenue and El Cajon Boulevard.

### **Vacancy Rate**

The vacancy rate for Greater North Park in 1985 was 4.4 percent compared to 5.0 percent for the City.

### **Population Characteristics**

The population of Greater North Park has grown very little in the last 20 years, reaching 37,292 in 1980. The declining number of persons in each household has nearly offset the added housing units. The average household size is now less than 1.9 persons per household, compared to 2.5 persons per household citywide. This statistic reflects the large number of elderly households with no children at home; 21 percent of the Greater North Park population is over 65 years compared to ten percent of the City, and 30 percent of the householders are retired. Only 14 percent of the population is under 18 years old, while 21 percent of the City population falls into this category.

## **Ethnic Composition**

The ethnic composition shows a higher than average percentage of whites at 87 percent versus 76 percent whites citywide. The only ethnic minority group with a higher concentration than the City is Vietnamese, but only 1.3 percent of the population falls into that group.

## **Income**

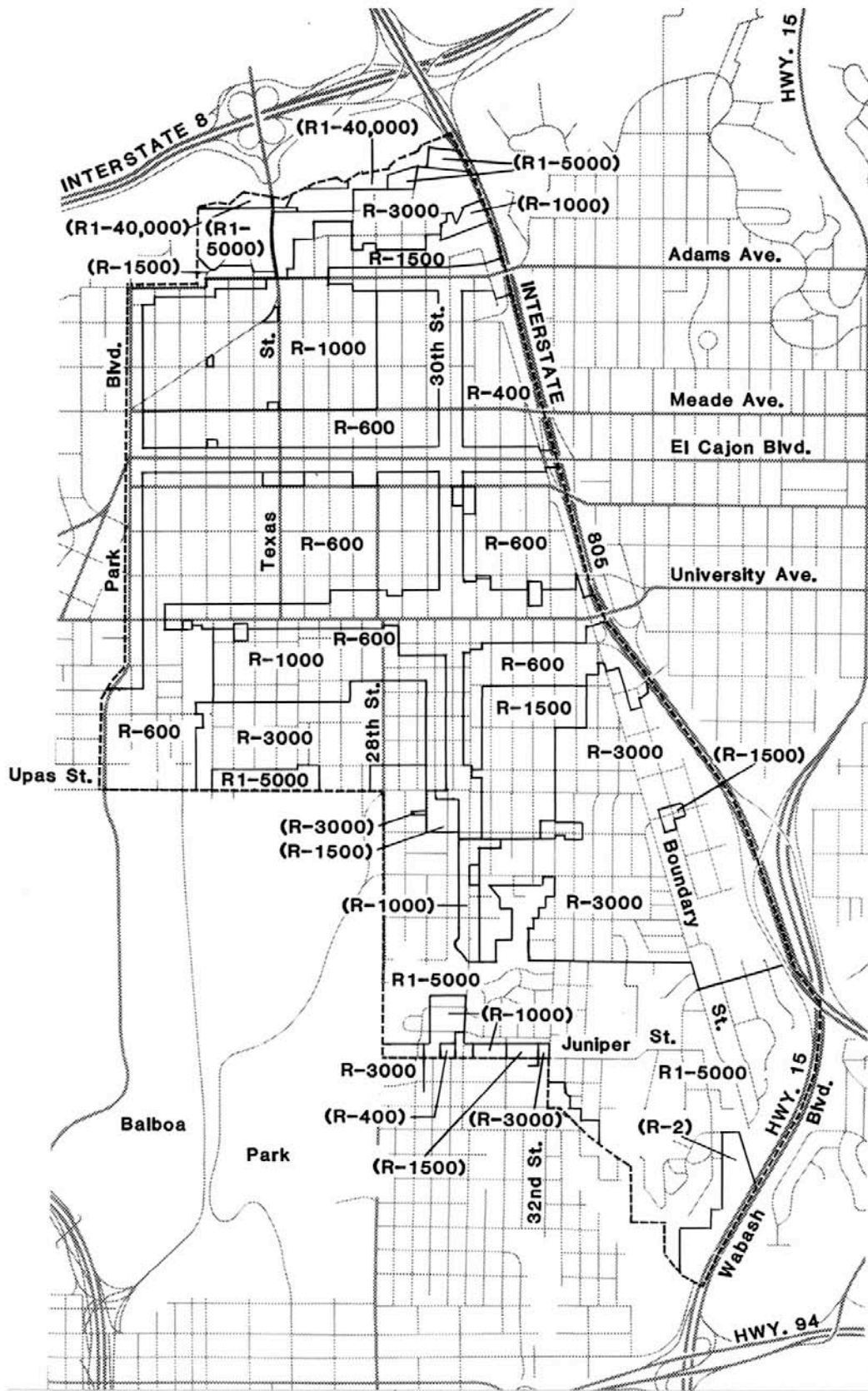
As of 1980, the median annual household income was \$11,432, about one-third lower than the City average. Within Greater North Park, the highest average incomes are found in the southwest sector of the community, where income actually exceeded the City average. The lowest average incomes are found in the northwest corner of the community. Lower incomes are typically seen in areas with a concentration of elderly persons.

## **Projections**

The adjusted Series VI regional growth forecast projects a gradual increase in the number of housing units from the October 1, 1985 estimate of approximately 22,400 to the year 2000 figure of 22,600. It is obvious that this projection is no longer accurate, primarily due to the surge of multifamily development in recent years. Therefore, based upon assumptions established for the traffic forecast study for the community, it is estimated that approximately 7,000 new housing units will be provided over the next 20 years for a total of approximately 30,000. The current (October 1, 1985) estimated population for Greater North Park is 40,800. The 20-year population in projecting for the community based upon the number of projected housing units and a family size of 1.85 is 55,500.

## **OBJECTIVES**

- Maintain the low-density character of predominantly single-family areas, outside the designated higher-density areas primarily located along El Cajon Boulevard and University Avenue, and encourage rehabilitation where appropriate.
- Require high-quality development at medium- to high-densities, centrally located within the community, to form an attractive and vital central area focusing on El Cajon Boulevard and University Avenue.
- Encourage mixed-use development that incorporates housing with commercial and office uses within selected commercial nodes.
- Ensure that new residential development is provided in accordance with the performance standards established in this Plan.
- Provide adequate off-street parking.



**Existing Zoning – Residential (Prior to Adoption of Community Plan)**

Greater North Park Community Plan

**5**

FIGURE

## RESIDENTIAL DENSITY RECOMMENDATIONS

The Greater North Park Community Plan seeks to provide a diversity of residential densities and housing types within the community. In order to accomplish this goal, two primary objectives have been established: First, to provide for the preservation and rehabilitation of single-family and lower-density neighborhoods; and, second, to establish higher-density residential areas in close proximity to the transit corridors. By concentrating the high-density areas in the central portion of the community in close proximity to major public transit corridors and to the community’s primary commercial areas, it is anticipated that a strong, vibrant, redeveloped high-intensity urban core will result. Preservation of the single-family and low-density areas in the northern section of the community and throughout the southern section of the community will help provide a variety of housing stock within the community.

Between the high-intensity core and the single-family and low-density areas are transition areas. These areas not only provide a transition between the high- and low-density areas but also add to the variety of housing stock through their predominately low-medium and medium-density residential designations.

Therefore, the residential density map (see **Figure 6**) is a reflection of a high-intensity residential core with densities becoming lower as the distances from the high-intensity core increase.

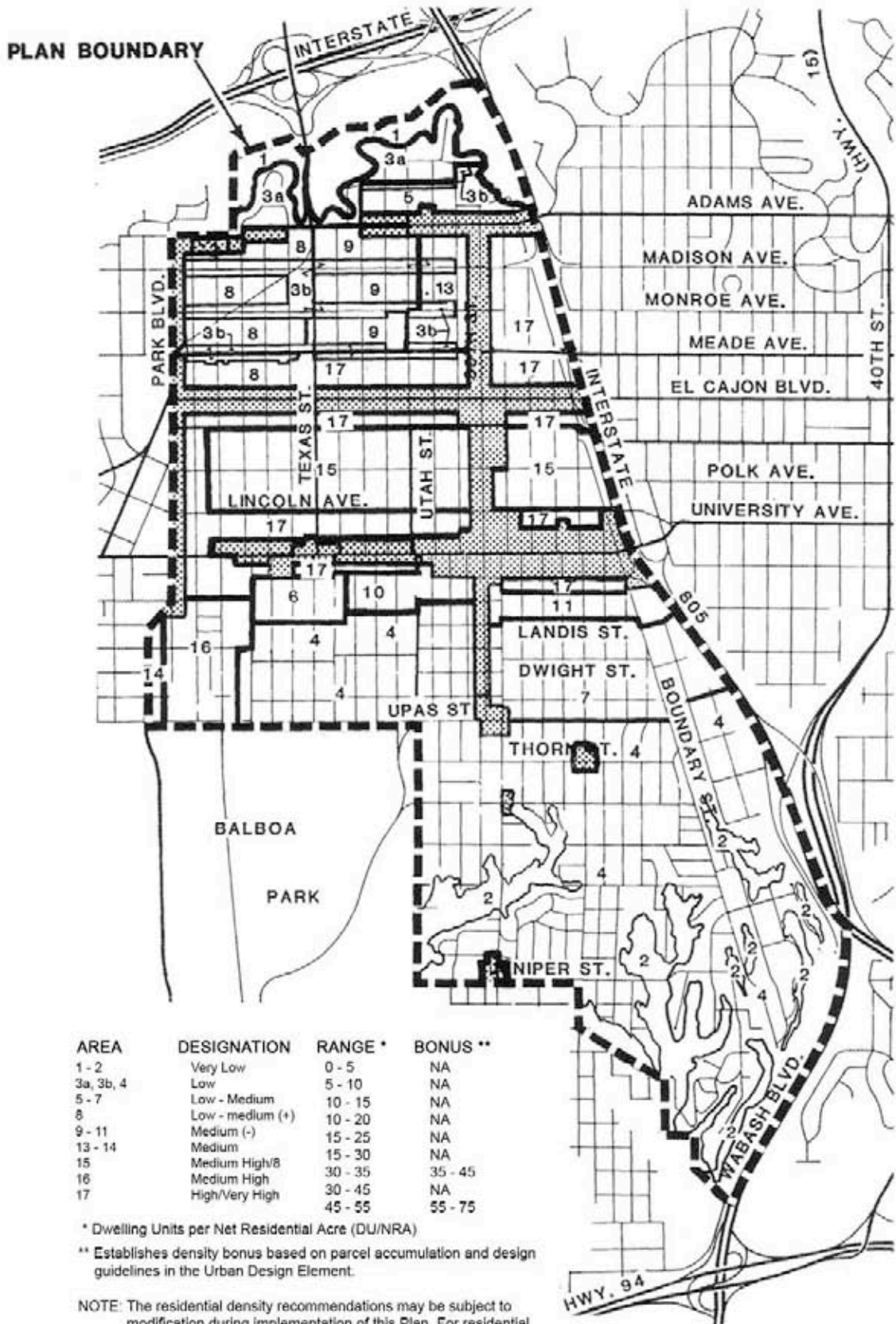
The following recommended residential densities in **Table 2** are applicable within the residential areas illustrated in **Figure 6**. A planned district or tailored zoning should be utilized to permit the residential densities established within selected areas. The guidelines set forth in the **Urban Design Element** should be utilized in formulating the tailored zoning for the selected areas. Recommended density ranges are established based upon dwelling units per net residential acre (DU/NRA). Existing residential zoning is indicated in **Figure 5**.

**TABLE 2**  
**RESIDENTIAL DENSITY RECOMMENDATIONS**

Area	Designation	Range*	Bonus**
1 - 2	Very Low	0 - 5	NA
3 - 4	Low	5 - 10	NA
5 - 7	Low-Medium	10 - 15	NA
8	Low-Medium (+)	10 - 20	NA
9 - 11	Medium (-)	15 - 25	NA
13 - 14	Medium	15 - 30	NA
15	Medium High/Bonus	30 - 35	35 - 45
16	Medium High	30 - 45	NA
17	High/Very High	45 - 55	55 - 75

\* Dwelling units per net residential acre.

\*\* Establishes density bonus based upon parcel accumulation and design guidelines in the **Urban Design Element**.



AREA	DESIGNATION	RANGE *	BONUS **
1 - 2	Very Low	0 - 5	NA
3a, 3b, 4	Low	5 - 10	NA
5 - 7	Low - Medium	10 - 15	NA
8	Low - medium (+)	10 - 20	NA
9 - 11	Medium (-)	15 - 25	NA
13 - 14	Medium	15 - 30	NA
15	Medium High/8	30 - 35	35 - 45
16	Medium High	30 - 45	NA
17	High/Very High	45 - 55	55 - 75

\* Dwelling Units per Net Residential Acre (DU/NRA)

\*\* Establishes density bonus based on parcel accumulation and design guidelines in the Urban Design Element.

NOTE: The residential density recommendations may be subject to modification during implementation of this Plan. For residential densities permitted in commercial areas, see the Commercial Element.



**Residential Densities**  
**Greater North Park Community Plan**

**6**  
**FIGURE**

## RESIDENTIAL AREA DENSITY ASSIGNMENTS

The residential areas are described, together with their density assignments, in the following narrative. The boundaries, as described for each area, are only for general locational purposes. For specific boundary descriptions of the individual areas, reference should be made to **Figure 6**.

### Very Low-Density (0-5 DU/NRA)

- Area 1: That area lying north of Adams Avenue which is in the Hillside Review (HR) Overlay District and is designated as open space. This area is also part of the south slopes of Mission Valley and it provides a buffer between the residential areas along the edge of the mesa and the intensive commercial development in Mission Valley. These areas should only be allowed to develop under the provisions of a Planned Residential Development (PRD) permit and should not exceed a density of one dwelling unit per 40,000 square feet (see **Figure 6**).
- Area 2: Those areas of the community lying south of Redwood Street which are in canyons and hillsides. These areas are in the Hillside Review (HR) Overlay District and are designated as open space. The 34<sup>th</sup> Street Canyon provides a natural open space boundary between Greater North Park and the Greater Golden Hill Precise Plan area while all the canyon areas in this portion of the community provide a feeling of openness. They should be allowed to develop only under the provisions of a Planned Residential Development (PRD) permit and should not exceed a density of one dwelling unit per 40,000 square feet (see **Figure 6**).

### Low-Density (5-10 DU/NRA)

- Area 3a: This is the single-family area north of Adams Avenue. The predominant type of development in this area is the single-family dwelling, although there has been some multifamily development intrusion. This area provides an enclave of single-family residential development in the northern portion of the community and adds to the residential diversity of the community. Panorama Drive is an example of a quiet residential enclave existing in close proximity to higher-density areas (see **Figure 6**).
- Area 3b: This area consists of the lots fronting on the east-west streets of Meade, Madison and Monroe Avenues; lots on Shirley Ann Place; and lots fronting on Collier Avenue, Kansas Street and Copley Avenue. Although many of the parcels are substandard in size, it is intended that these areas be preserved as low-density in scale and character (see **Figure 6**).
- Area 4: Those areas of predominantly single-family development in the southern portions of the community. These areas feature quality single-family neighborhoods in the blocks adjacent to Balboa Park and around the canyon areas in the southern portions of the community. In addition, numerous examples of the classic single-family California bungalow can be found in these areas. It is intended that these areas be preserved as the community's primary single-family residential area and that, where necessary, rehabilitation of deteriorated residential units be undertaken (see **Figure 6**).

- Areas 1-4: For the single-family residential Areas 1 through 4, the clustering of dwelling units through a Planned Residential Development permit should only be approved if the proposed project creates dwelling units which strongly resemble the scale and character of the surrounding development. It is acknowledged that this approach may often result in substantially fewer dwelling units than the maximum allowed by the underlying R-1 (single-family) zones.

#### **Low-Medium Density (10-15 DU/NRA)**

- Area 5: This area is north of Adams Avenue and east of Hamilton Street. It has a range of housing types, with the multifamily structure being the dominant type. It provides a transition between the single-family areas to the north and the west and the commercial center around Adams Avenue and 30<sup>th</sup> Street (see **Figure 6**).
- Area 6: This area is located north of Landis Street and west of 28<sup>th</sup> Street, and south of Wightman Street and west of the alley between Arizona and Arnold Streets and is primarily a duplex area with some single-family and higher-density development. This area serves as a transitional area between the higher-density areas to the north and the single-family neighborhoods adjacent to Balboa Park. This area could be a recipient area for single-family bungalow structures being moved to this area as second units from higher-density areas rather than being demolished (see **Figure 6**).
- Area 7: This is a large area south of Landis Street and primarily east of 30<sup>th</sup> Street which is intended as a duplex area. This area features a variety of housing types with the duplex being a typical form of housing in this area. This area also serves as a transitional area between higher-density and commercial development to the north and the community's primary single-family areas to the south. This is an ideal area for move-on bungalow structures from higher-density areas (see **Figure 6**).

#### **Low-Medium (+) Density (10-20 DU/NRA)**

- Area 8: That area north of Meade Avenue and west of Texas Street which is a mixture of single-family, duplex and multifamily development with single-family accounting for a significant portion of the existing housing stock. The smaller lot patterns along the east-west streets provide assurance that some single-family development will remain. In addition, this density will permit infill on the interior lots in the form of one or two dwelling units per parcel (see **Figure 6**).
- Areas 5, 6, 7, 8: Parcel consolidation should be prohibited in residential Areas 5, 6, 7 and 8 which are designated low-medium density at 10-15 and 10-20 dwelling units per acre.

#### **Medium (-) Density (15-25 DU/NRA)**

- Area 9: That area north of Meade and generally east of Texas which primarily consists of multifamily development with very little remaining lower-density development. This area is suitable for infill development at a density that will not give the appearance of higher-density development (see **Figure 6**).



- Area 10: That area south of Wightman Street and North Park Way and west of 30<sup>th</sup> Street and east of the alley between Arizona and Arnold Streets which is intended to serve as a transition area between the higher-density areas along University Avenue and the lower-density areas to the south. This area is a mix of higher-density development and duplexes with some remaining single-family dwellings (see **Figure 6**).
- Area 11: That area south of North Park Way and east of 30<sup>th</sup> Street which is also intended to serve as a transition area between the higher-density areas to the north and the lower-density areas to the south. It is presently a mixture of multifamily and duplex development with some scattered remaining single-family dwellings (see **Figure 6**).

#### **Medium-Density (15-30 DU/NRA)**

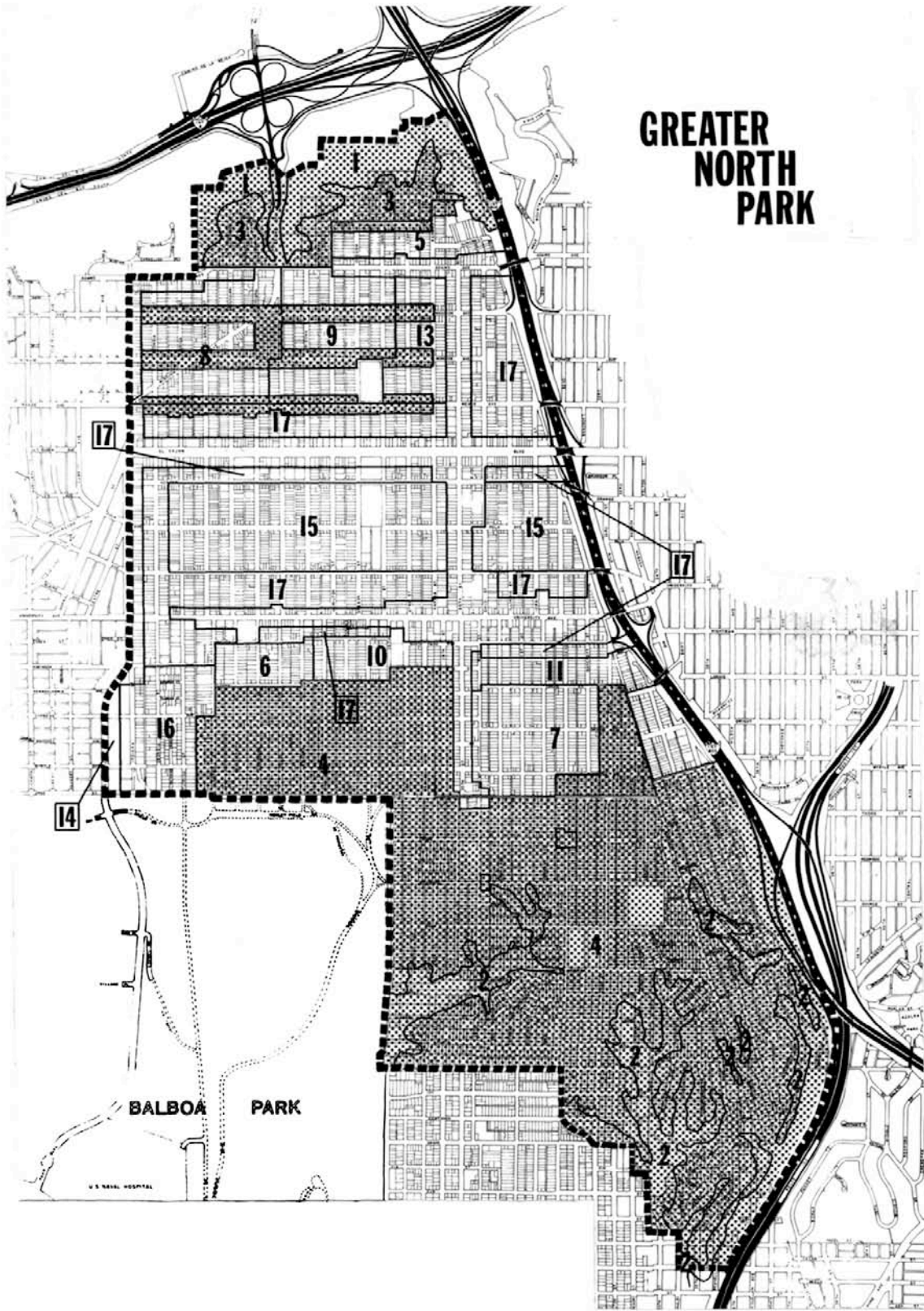
- Area 13: That area between Idaho and 30<sup>th</sup> Streets north of Meade Avenue primarily consists primarily of multifamily development with little remaining lower-density development. This area serves as a transition from the 30<sup>th</sup> Street commercial area and the lower-density areas to the west. Infill in this area will also provide support to the commercial area along 30<sup>th</sup> Street and to the Adams Avenue and 30<sup>th</sup> Street commercial center (see **Figure 6**).
- Area 14: This area consists of those residentially designated blocks abutting Park Boulevard on the east side between Robinson Avenue and Upas Street. There are a large number of structures in this area which are visually pleasing from a historical or architectural point of view. It is possible that some of these structures could be utilized as “bed and breakfast” inns and serve a visitor clientele. In addition, this portion of Park Boulevard provides a pleasant transition to and from Balboa Park. The residential density recommended for this area is intended to maintain the existing character and density of the area (see **Figure 6**).

#### **Medium-High/Bonus Density (30-35 DU/NRA) with a 35-45 DU/NRA Bonus**

- Area 15: This area lies between Howard and Lincoln Avenues and extends virtually the entire width of the community. It is characterized by a mixture of lower- and medium-density development with a scattering of single-family development. This area, while not abutting the major transit corridors, is within walking distance of them and the commercial areas along El Cajon Boulevard, 30<sup>th</sup> Street and University Avenue. As a result of these factors, this area is recommended for medium-high density residential development with a density bonus being obtainable for parcel accumulation and compliance with the design guidelines of the **Urban Design Element** (see **Figure 6**).

#### **Medium-High Density (30-45 DU/NRA)**

- Area 16: This area has a mixture of residential types with multifamily being the predominant type. There are examples of older quality development which lend a qualitative visual character to the area. This area also focuses on Florida Street which is a north-south linkage between Balboa Park and the Trolley Barn park site on Adams



 Protected Single-Family Neighborhoods



**Single-Family Classification Map**  
 Greater North Park Community Plan

**6a**  
 FIGURE

Avenue. The medium high-density recommended for this area is in keeping with existing quality development and will provide for infill development of similar density (see **Figure 6**).

### **High/Very High-Density (45-55 DU/NRA with a 55-75 DU/NRA Bonus)**

- **Area 17:** This area actually consists of several areas, all of which are located in close proximity to the major transportation corridors of El Cajon Boulevard, University Avenue and Park Boulevard. Also included is the area abutting I-805 between Adams Avenue and El Cajon Boulevard. These areas are assigned the highest residential densities for designated residential areas due to their easy walking distances to public transit routes which run along the aforementioned streets. These areas are also located in close proximity to many of the community's commercial areas and, by permitting higher densities in these residential areas, support in the form of increased patronage is provided to the commercial areas. In addition, the western sector of University Avenue which is currently a mixture of older deteriorated residential development and strip commercial uses. Redevelopment of this area should be encouraged by designating the area high-density residential. Because of the proximity of these areas to transit corridors and commercial centers and in order to encourage quality residential development in these areas, they are recommended for high-density residential development with the opportunity for obtaining a bonus to very high-density through parcel accumulation and compliance with the design guidelines of the **Urban Design Element** (see **Figure 6**).

Along the south side of Meade Avenue, south of residential Area 8, tailored zoning should be developed in such a manner as to ease the transition between the differing densities of Areas 8 and 17 to the extent possible. Similar tailored zoning should also be investigated where there are similar differences or transitions in densities.

### **SENIOR HOUSING**

Current regulations provide for a Conditional Use Permit (CUP) to be granted for senior housing, which allows both increased density and decreased parking for such projects. These concessions are based on the smaller family size and lesser number of cars in senior households.

Greater North Park is an attractive area for this type of project. Bus service is available, as are neighborhood markets and other shopping. Balboa Park is nearby, while the North Park Recreation Center also offers programs catering to senior citizens. In addition, higher-density zoning is available and land prices are relatively reasonable. Due to these characteristics, a number of senior projects have located in the Greater North Park area.

Concentrating senior housing projects in one area could cause adverse impacts such as out-of-scale development. Therefore, care should be taken in the granting of increased density or the permitting of decreased parking. The impact of such concessions on parks, emergency services, neighborhood character and on-street parking in particular should be studied.

## **IMPLEMENTATION PROGRAM**

1. Establish a higher-density residential core centered around the public transit corridors of El Cajon Boulevard and University Avenue.
2. Preserve and rehabilitate plan designated single-family and low-density neighborhoods.
3. Develop, as an implementation program, tailored zoning for all or portions of the community. The standards and development regulations should be predicated upon the goals and objectives of the **Urban Design Element** and the implementation studies leading to implementing legislation.

Refinements and modifications to the boundaries and recommended densities for the various residential areas may be incorporated into the implementing legislation. In addition, the achievability of the recommended densities may be predicated upon the design standards, development regulations and other regulations of the implementing legislation.

### **The Future Single-Family Neighborhood Character - Single-Family Protection**

The existing low- and very low-density residential areas are characterized by lots developed with a single-family detached house or a duplex which is compatible with the scale of surrounding single-family houses. These areas are zoned for single-family development (R1-5000) and should be protected as single-family neighborhoods in the future. Therefore, requests for rezonings or other discretionary actions in these areas, which could result in construction of any type of residential structure other than a traditional single-family house with one unit per lot, should be denied.

### **Recommendations**

- Retain the existing residential zones in their present configurations, except those for the areas called out on the Single-Family Neighborhood Classification Map (**Figure 6a**).
- Designate the areas called out on the Single-Family Neighborhood Classification Map (**Figure 6a**) map as Low-Residential Use (5-10 DU per acre).
- Rezone the areas called out on the Single-Family Neighborhood Classification Map (**Figure 6a**), as protected single-family, from MR-3000 and MR-1000 to R1-5000. This area is developed primarily as single-family and should remain as an integral part of the single-family character of the neighborhood.

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## COMMERCIAL

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### GOAL

Provide appropriately located, attractive commercial and office facilities offering a wide variety of goods and services.

### EXISTING CONDITIONS

The Greater North Park plan area has two major commercial areas, one a commercial hub (30<sup>th</sup> and University) and the other a commercial strip (El Cajon Boulevard), and a number of minor commercial strips, which are really dispersed neighborhood commercial centers.

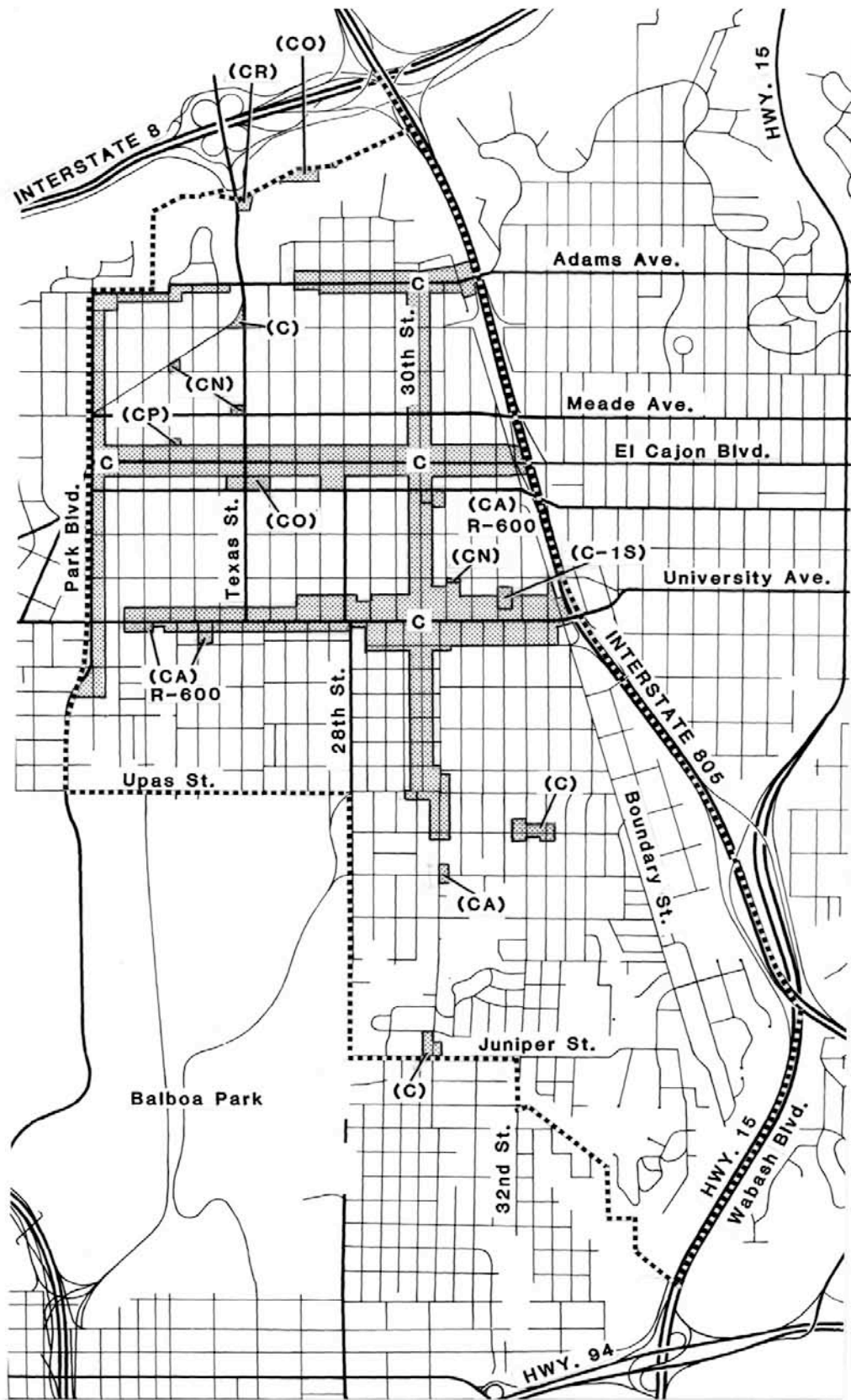
El Cajon Boulevard was once the major highway into San Diego from the east and the major commercial artery in San Diego. However, the construction of I-8 in Mission Valley, the advent of the mobile consumer and the coming of regional shopping centers have changed the boulevard from a primary to a secondary market.

The North Park commercial center (30<sup>th</sup> and University), once a vital center of commerce and community activity in San Diego, has suffered a decline in recent decades. With the advent of two major shopping centers in Mission Valley in the 1960s and the more recent opening of Horton Plaza in downtown San Diego, North Park can no longer envision itself as the major regional center it was through the 1950s. It has become a community-serving commercial center with some very limited regional aspects.

Greater North Park has an excess of commercial zoning which has transformed its major streets into underutilized commercial strips. The community is in need of a consolidation and restructuring of its commercial base (see **Figure 8**). Existing commercial zoning is indicated on **Figure 7**.

The Economic Analysis of the Mid-City community completed in 1983, was undertaken as an initial step in the economic revitalization of the communities of Greater North Park and Mid-City. This study serves as a supportive document to the Greater North Park Community Plan. The study reviews existing conditions within the study area and provides a demographic analysis of the area. The recommendations of the study include:

- Encourage contained business districts, primarily at major intersections, as opposed to continuous commercial strips.
- Encourage the physical upgrading of the business strips.
- Encourage higher-density residential development.
- Provide additional parking.
- Encourage businesses tailored to a younger market.
- Encourage more restaurants, entertainment, and specialty shops.
- Encourage coordinated marketing efforts for Mid-City businesses and for each business district.



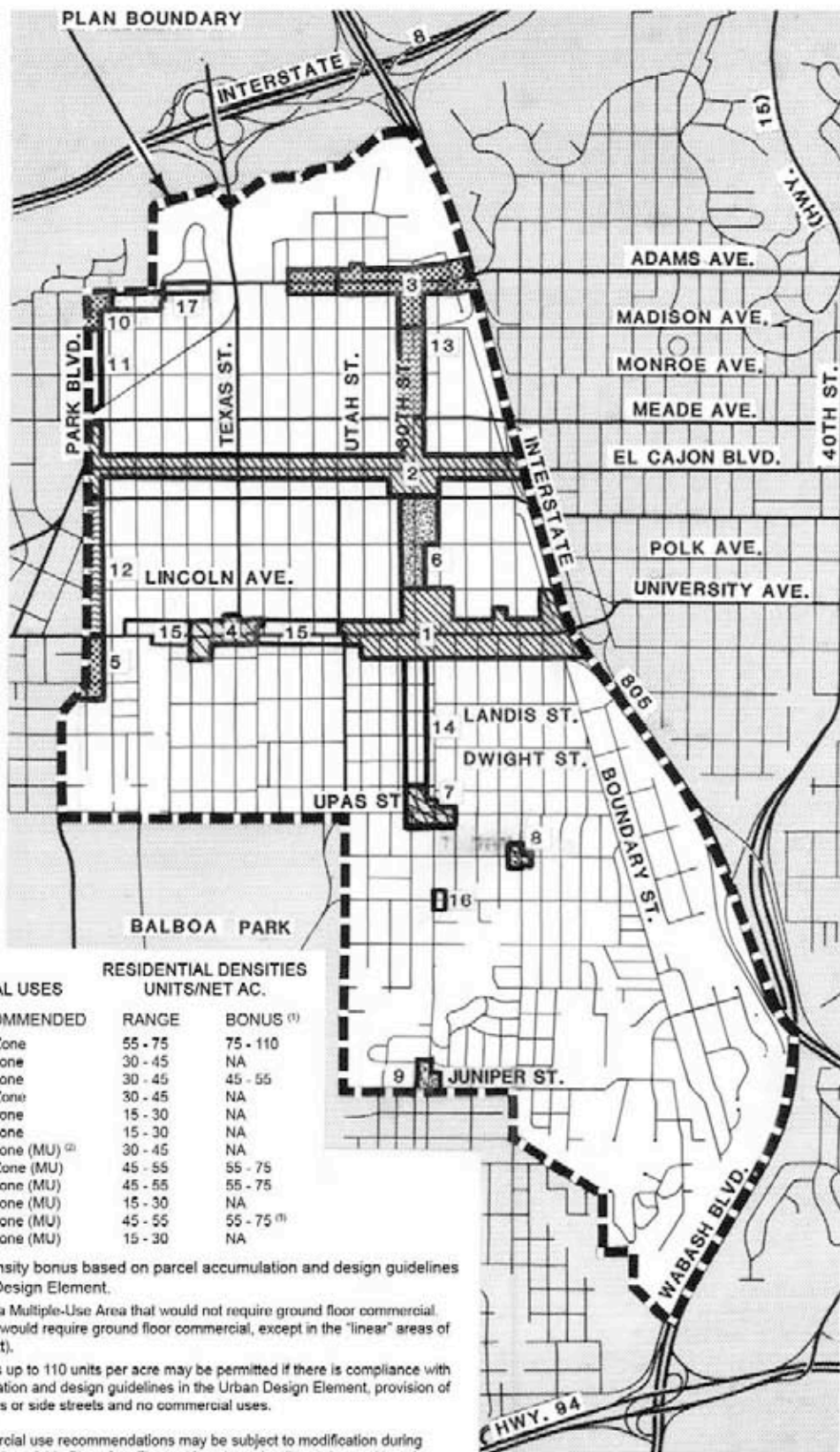
Existing Zoning – Commercial (Prior to Adoption of Community Plan)



The above recommendations from the Economic Analysis of the Mid-City community provides a basis upon which to base desirable commercial objectives for the Greater North Park community. These objectives are set forth below.

## **OBJECTIVES**

- Revitalize the central business district at University Avenue and 30<sup>th</sup> Street and other selected business districts.
- Enhance the level and quality of business activity in North Park by encouraging concentration of retail commercial uses in nodes and reducing strip commercial activities.
- Improve the appearance of commercial development through establishment of overall design standards.
- Provide adequate parking for commercial areas, encouraging off-street parking.
- Encourage mixed-use development to include retail facilities, offices and housing at medium- and high-densities within selected commercial nodes.
- Enhance pedestrian activity in the central business core by improving the pedestrian environment.
- Maintain existing convenience (mom and pop) stores, provided that the uses are compatible with surrounding neighborhoods.
- Encourage new development and redevelopment for purposes of increasing employment opportunities within the community.
- Continue the ongoing implementation of the programs for Adams Avenue, El Cajon Boulevard and University Avenue which include land use and urban design analysis, economic analysis and marketing survey.
- Increase business district parking through the private acquisition of property for parking reservoirs behind existing businesses, and the creative redesign of existing on-street parking.
- Where appropriate, preclude residential development from preempting commercial development.
- Establish residential densities for the individual commercial and multiuse areas.
- Rehabilitate and redevelop certain underutilized strip commercial areas into multiuse areas.
- Provide for neighborhood and specialty commercial services to the residential development along and abutting these multiuse areas.
- Provide additional opportunities for residential development within the community.



**COMMERCIAL USES**

AREA	RECOMMENDED	RANGE	BONUS <sup>(1)</sup>
1, 2	C-1 Zone	55 - 75	75 - 110
3, 10	CC Zone	30 - 45	NA
4	CN Zone	30 - 45	45 - 55
5	C-1 Zone	30 - 45	NA
6, 9	CC Zone	15 - 30	NA
7, 8, 16	CN Zone	15 - 30	NA
11	CC Zone (MU) <sup>(2)</sup>	30 - 45	NA
12	C-1 Zone (MU)	45 - 55	55 - 75
13	CC Zone (MU)	45 - 55	55 - 75
14	CN Zone (MU)	15 - 30	NA
15	CC Zone (MU)	45 - 55	55 - 75 <sup>(3)</sup>
17	CN Zone (MU)	15 - 30	NA

- (1) Establishes density bonus based on parcel accumulation and design guidelines in the Urban Design Element.
- (2) "MU" indicates a Multiple-Use Area that would not require ground floor commercial. All other areas would require ground floor commercial, except in the "linear" areas of Area 2 (see text).
- (3) A density bonus up to 110 units per acre may be permitted if there is compliance with parcel accumulation and design guidelines in the Urban Design Element, provision of access via alleys or side streets and no commercial uses.

NOTE: The commercial use recommendations may be subject to modification during implementation of this Plan. See Figure 10 and text for discussion and location of commercial nodes.

**Commercial Area Designations**  
**Greater North Park Community Plan**

**8**  
**FIGURE**





## COMMERCIAL AREA RECOMMENDATIONS

In order to provide appropriately located, attractive retail and office facilities offering a wide variety of goods and services this Plan recommends the consolidation and intensification of commercial activities.

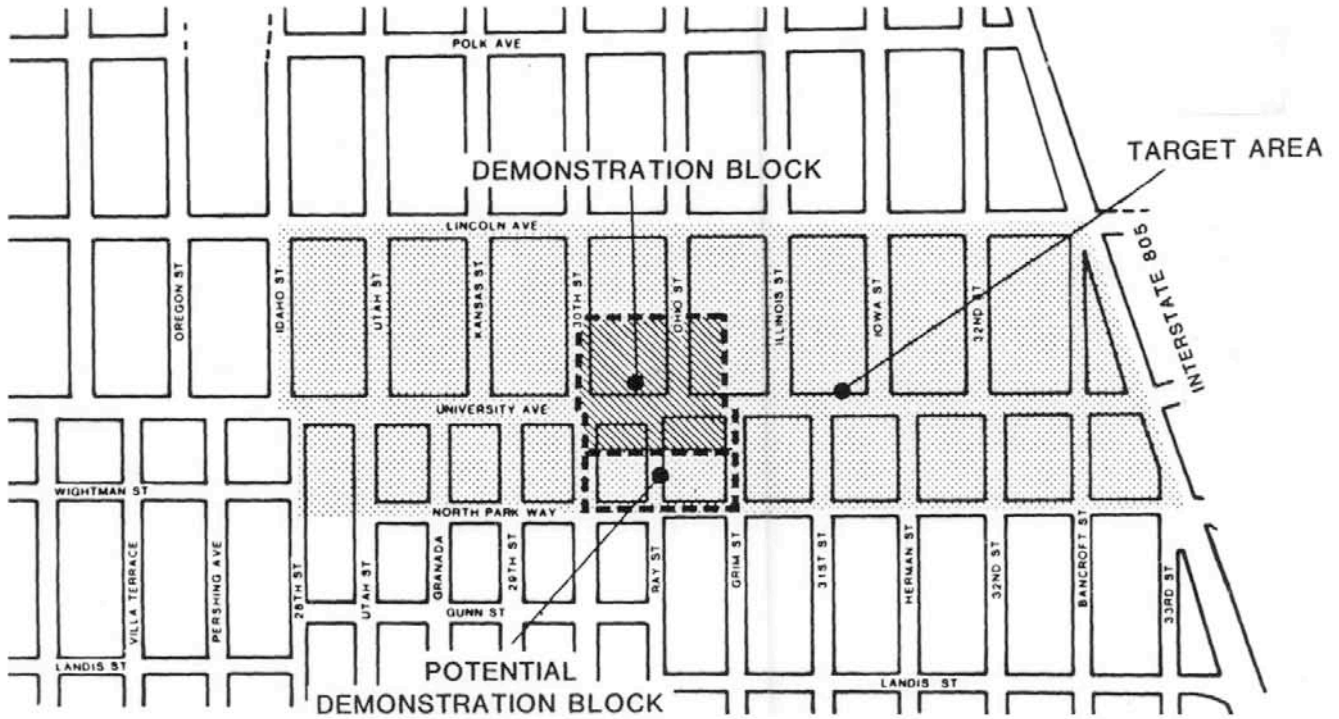
Heightened levels of commercial activity should be promoted at University Avenue and 30<sup>th</sup> Street, which is the commercial core or “downtown” of Greater North Park. El Cajon Boulevard should continue to be emphasized as a classic commercial strip with additional emphasis being given to auto-oriented uses. Excessive and unproductive strip commercial areas should be contracted and redefined as either neighborhood or specialty commercial nodes. Portions of the former strip commercial areas should become multiuse areas featuring higher-density residential development and specialty commercial uses serving that residential development as well as other residential areas within walking distance. **Table 3** provides the recommended commercial area designations and **Figure 8** illustrates these recommendations. **Figure 7** illustrates existing commercial zoning.

**TABLE 3**  
**RECOMMENDED COMMERCIAL AREA DESIGNATIONS**

Area	Commercial Uses	Residential Densities (Units/Net Ac.)	
	Recommended	Range	Bonus (1)
1, 2	C-1 Zone	55-75	75-110
3, 10	CC Zone	30-45	NA
4	CN Zone	30-45	45-55
5	C-1 Zone C	30-45	NA
6, 9	CC Zone C	15-30	NA
7, 8, 16	CN Zone	15-30	NA
11	CC Zone (MU) (2)	30-45	NA
12	C-1 Zone (MU)	45-55	55-75
13	CC Zone (MU)	45-55	55-75
14	CN Zone (MU)	15-30	NA
15	CC Zone (MU)	45-55	55-75 (3)
17	CN Zone (MU)	15-30	NA

- (1) Establishes density bonus based on parcel accumulation and design guidelines in the Urban Design Element.
- (2) “MU” indicates a Multiuse Area that would not require ground floor commercial. All other areas would require ground floor commercial, except in the “linear” areas of Area 2 (see text).
- (3) A density bonus up to 110 units per acre may be permitted if there is compliance with parcel accumulation and design guidelines in the Urban Design Element, provision of access via alleys or side streets and no commercial uses.

Both **Table 3** and **Figure 8** indicate that Park Boulevard and 30<sup>th</sup> Street are each divided into several different commercial or multiuse areas. These distinctions have been made in order to relate the various areas to adjacent residential areas in order to provide for the commercial needs of these areas.



North Park Commercial Area Design Study  
 Greater North Park Community Plan

**9**  
 FIGURE

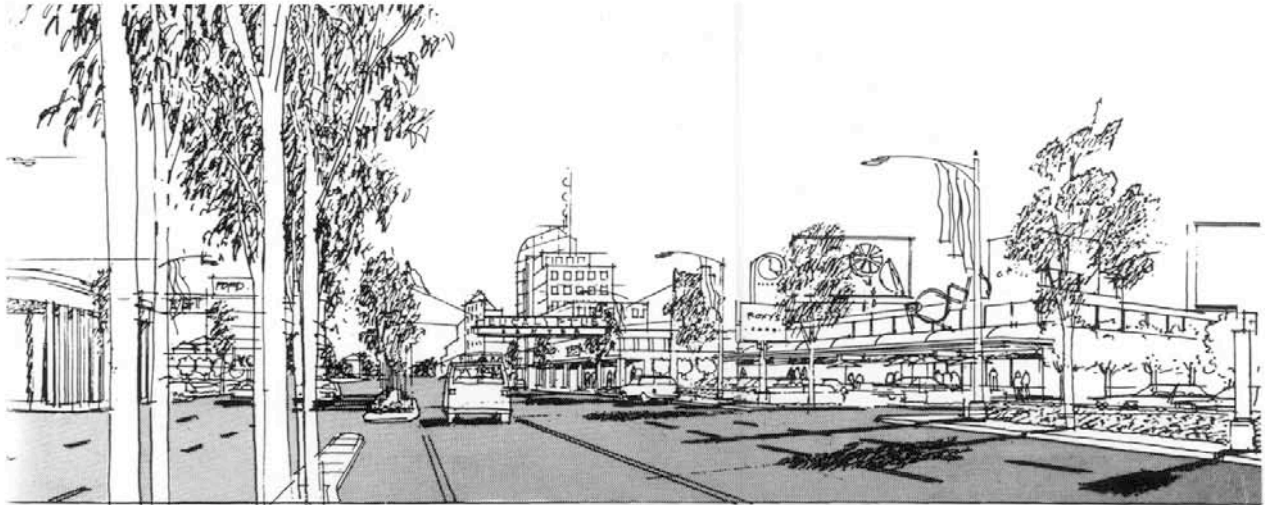
### **30<sup>th</sup> and University Commercial Area (Area 1)**

The 30<sup>th</sup> and University community commercial area is the community's commercial core and is centered at 30<sup>th</sup> Street and University Avenue. It's bounded generally by Lincoln Avenue on the north, I-805 on the east, North Park Way on the south and by Utah Street and Idaho Street on the west. This area is shown on **Figure 8**. This area is the subject of the recently published North Park Design Study. The design study provides the opportunity for taking the necessary first step in upgrading and reorienting the commercial center. The design study provides standards and guidelines for the physical and visual rehabilitation of the center, including; facade treatment, circulation, access and parking improvements, pedestrian circulation and streetscape beautification. The North Park Design Study should be utilized as a supportive document to the Greater North Park Community Plan and a summary of its guidelines is set forth in the **Urban Design Element** of this Plan. The recommendations of the design study include:

- Develop the North Park commercial area as a satellite to downtown and Mission Valley, including hotel, entertainment and office space to complement retail uses.
- Encourage design unification.
- Encourage a large variety of retail activity.
- Increase parking availability.
- Encourage pedestrian street activity through pedestrian-oriented business activity and public events.
- Develop a coordinated program for street improvements including trees, landscape islands and unified paving.
- Promote development incentives.
- Encourage mixed-use facilities.

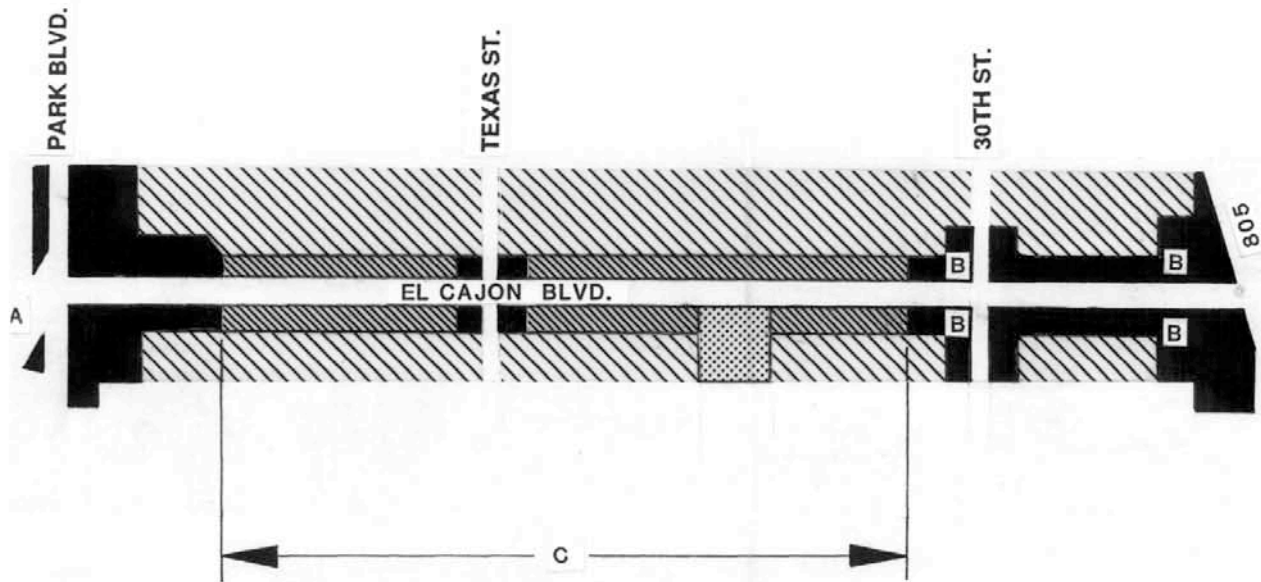
This area has been determined to be a target area for the City's involvement and commitment to upgrading the area and encouraging private improvements and investment. A demonstration block including both sides of University Avenue between 30<sup>th</sup> and Grim Streets has been selected for public improvements such as street beautification. Future potential demonstration blocks, depending upon the availability of funding, should be located in the immediate vicinity of 30<sup>th</sup> Street and University Avenue. These areas are indicated on **Figure 9**.

Zoning implementation for this area should be tailored zoning legislation in the form of a planned district. Permitted uses should be those of the C-1 Zone which permit a full range of consumer goods and services and limited wholesaling and warehousing. Residential development in this area should be permitted up to a density of 75 dwelling units per net residential acre with a bonus to 110 dwelling units per net residential acre for parcel accumulation and compliance with the design guidelines set forth in the **Urban Design Element**.



PROPOSED IMPROVEMENTS

Source: Land Studio, Rob Quigley, AIA, Kathleen McCormick



**PROPOSED DESIGN AREAS**

- |   |                   |  |                                    |
|---|-------------------|--|------------------------------------|
| A | Western Gateway   |  | Standard Commercial                |
| B | Eucalyptus Center |  | 'R' Zone Transition                |
| C | Linear Strip      |  | Community Oriented Public Facility |
|   | Mixed Use (Nodes) |  |                                    |



**El Cajon Boulevard Design Study (Illustrative Only)**

Greater North Park Community Plan

**10**  
FIGURE

The higher densities are recommended in order to provide increased residential development in close proximity to both the commercial core and to public transit corridors. Also offered, is the opportunity for multiuse development with commercial uses being required on the ground floor and residential use being permitted above the ground floor.

In addition, the community center needs ancillary activities such as outdoor cafes, regular community events, restaurants, theaters and other gathering places. These activities can extend commercial hours into the evening and make for a more viable commercial center.

Also, a public transit point should be established in the vicinity of 30<sup>th</sup> Street and University Avenue in order to provide greater access to the commercial center from throughout the Greater North Park community and from adjacent communities, thus providing additional support for the center. A transit point is a transit stop with either a high number of daily boardings or is served by two or more routes at an intersection crossed by one or more other routes. In addition, public transit offers opportunities for those either lacking, or preferring not to use personal transportation, the opportunity to meet their needs by having public transit access to the commercial center. Finally, secure bicycle parking facilities should be located in conjunction with the transit point as well as in other appropriate locations within the commercial center.

In providing a pedestrian environment, the building frontages on 30<sup>th</sup> Street and University Avenue should be devoted to retail uses. Office uses should be restricted to side streets and upper stories. Office uses should be secondary to the primary functions of the center which is the provision of retail goods and services. Office uses are support services and should be within easy walking distance of the retail area but should not intrude into it and preempt retail space.

In order to facilitate the rehabilitation of the 30<sup>th</sup> and University community commercial center and implement goals and objectives of this element, it will be necessary to initiate a rezoning program in order to consolidate the commercial center.

### **El Cajon Boulevard Commercial Area (Area 2)**

El Cajon Boulevard is among the longest continuous commercial strips in San Diego County. The area is generally bounded by Park Boulevard on the west, I-805 on the east and by the parallel east/west alleys immediately to the north and south of the boulevard. This area is shown on **Figure 8**. It is actually a continuation of Washington Street in Mission Hills on the west to La Mesa on the east, a distance of about eight miles. That portion within the planning area is about 1.2 miles long, continuously zoned commercial (“C”) throughout its length. It formerly served as the main east-west highway prior to the completion of I-8 in the late 1950s. This street has diminished as a commercial attraction since the completion of the freeways and the development of regional shopping centers in Mission Valley. The boulevard is characterized by auto-oriented commercial facilities, but has experienced a decline in both the quality and quantity of commercial establishments as a result of the impact of the shopping centers.

Since El Cajon Boulevard is a major east-west transportation corridor, new development and redevelopment should occur in a manner that will eliminate or reduce reliance on mid-block driveways which create conflicts with traffic flows. The design study recommends a number of techniques for avoiding or minimizing this situation. It is anticipated that future development along the boulevard will remain auto-oriented thereby creating a need for design and development regulations which avoid conflicts with traffic flows on the boulevard.

The design study establishes focused areas of concentrated development intensity along the “strip” in the form of “gateways” and “district centers” which are higher-intensity “nodes.” The Western Gateway for the boulevard is at Park Boulevard while 30<sup>th</sup> Street and I-805 is a district center called the “Eucalyptus Center.” Those portions of El Cajon Boulevard not within either a gateway or a district center are referred to as linear areas which will feature lower development intensities and may also be multiuse areas which may not require ground floor commercial uses. Since the design study is specific in its recommendations for the various designated areas, it will be necessary to establish creative commercial zoning legislation and initiate a rezoning program for the El Cajon Boulevard strip commercial area (see **Figure 10**).

The El Cajon Boulevard Design Study should be utilized as a supportive document to the Greater North Park Community Plan and a summary of its guidelines is set forth in the **Urban Design Element** of this Plan. The recommendations of the design study include:

- The boulevard has regional significance and should have a strong image with noticeable gateways at either end.
- Centers of high intensity should be created at major cross streets.
- High-density residential use is encouraged as infill mixed with existing commercial uses, between the high-intensity “nodes.” These should be shaped by standardized setbacks, parking and design guidelines.
- It is important that rehabilitation of existing structures emphasize the overall image. Individual detailing is secondary to the major image themes.
- Street improvements should emphasize the gateways, district identification signs, neighborhood identification features, major landscaping statements and coordination of color.

El Cajon Boulevard should retain its orientation to automobile-related businesses, but the visual appearance of the “strip” should be upgraded through the use of urban design standards and guidelines recommended by the Design Study for the Commercial Revitalization of El Cajon Boulevard.

In addition, El Cajon Boulevard, because of its high level of accessibility, also provides an opportunity for office development which would be complementary to the auto-oriented uses and could become part of the multiuse development recommended by the design study for the gateways and district centers.

Zoning implementation for this area will be tailored zoning legislation in the form of a planned district. Permitted uses should be those of the C-1 Zone which permits a full range of consumer goods and services and limited wholesaling and warehousing.

Residential development in this area should be permitted up to a density of 75 dwelling units per net residential acre with a bonus to 110 dwelling units per net residential acre for parcel consolidation and compliance with the design guidelines in the **Urban Design Element**. The higher residential densities are recommended for El Cajon Boulevard in order to provide increased residential development along this major commercial strip which is a major public transit corridor. These higher residential densities also provide the opportunity for multiuse development along the boulevard.

### **Adams Avenue Neighborhood/Specialty Commercial Center (Area 3)**

Adams Avenue, from Hamilton Street to I-805, has the potential to function both as a neighborhood commercial center and as a unique specialty center, focusing primarily on the antique market. This area is shown on **Figure 8**. In addition, this area could become a very attractive commercial area by featuring restaurants, outdoor cafes, and a limited range of specialty shops catering to patrons of the antique market. As a neighborhood commercial area, the necessary retail goods and services typically provided by such an area should be available to local residents as well as those patronizing the specialty retail elements of the area.

Like most of the other commercial areas in Greater North Park, this area is zoned with the “C” Zone, which is a generalized commercial zone that is not suited to promoting the type of specialty/neighborhood commercial center to which this area is suited. Therefore, revisions to the commercial zoning should be implemented in order for this area to realize its potential as a unique specialty area featuring the antique trade. Zoning implementation for this area will be tailored zoning legislation in the form of a planned district. The **Urban Design Element** provides design guidelines which should be utilized in formulating tailored zoning legislation for this area. Permitted uses should be those of the CC Zone which permits community commercial activities, business and professional offices, convenience goods and services, and commercial recreational activities. Residential development in this area should be permitted up to a density of 45 dwelling units per net residential acre. This residential density will permit infill residential development above the ground floor and will provide increased patronage for this commercial area.

### **University Avenue Neighborhood Commercial Center (Area 4)**

University Avenue, between Mississippi Street and Arizona Street, should function as a neighborhood commercial center serving the western portion of the community south of El Cajon Boulevard. This area is shown on **Figure 8**. Like other commercial areas in the community, it will benefit from a rezoning program, placing it in a commercial zone that permits appropriate commercial uses and requires off-street parking, landscaping and design or development standards.

Zoning implementation for this area will be tailored zoning legislation in the form of a planned district requiring off-street parking, landscaping and design standards. These regulations should be formulated based upon the guidelines of the **Urban Design Element**. Permitted uses should be those of the CN Zone which permits neighborhood commercial activities, business and professional offices and convenience goods and services. Residential development in this area should be permitted up to a density of 45 dwelling units per net residential acre with a bonus to 55 dwelling units per acre for parcel consolidation and compliance with the design guidelines of the **Urban Design Element**. This area will provide for the needs of the surrounding residential neighborhoods. The recommended residential densities including the bonus density, offer the opportunity for multiuse development with commercial uses being required on the ground floor.

### **Park Boulevard Community Commercial Area (Area 5)**

Park Boulevard, which forms the western boundary of the Greater North Park community plan area, features a community commercial area from University Avenue to just below Robinson Avenue. This area is shown on **Figure 8**. Since Park Boulevard is a major entryway into Balboa Park and a shared community plan boundary with the Uptown community, it is imperative that the community planning programs for the two communities coordinate on planning and proposals for this major street.

Zoning implementation for this area should be tailored zoning legislation in the form of a planned district. Development and design standards should be based upon the guidelines of the **Urban Design Element**. Park Boulevard could benefit from being placed in a Business Improvement District which would serve as the basis for upgrading the commercial areas along this street. Permitted uses should be those of the C-1 Zone which permits a full range of consumer goods and services and limited wholesaling and warehousing. Residential development in this area should be permitted up to a density of 45 dwelling units per net residential acre. This commercial center serves a large senior citizen population which resides in the adjacent residential areas. The residential density recommended for this area provides for multiuse development with commercial uses required on the ground floor.

### **Thirtieth Street Specialty Commercial Strip (Area 6)**

Thirtieth Street, from University Avenue to Howard Avenue, is both zoned and developed commercially and should continue to fulfill a commercial function. This area is shown on **Figure 8**. The block between University Avenue and Lincoln Avenue is part of the North Park Design Study pertaining to the 30<sup>th</sup> and University community commercial center and should develop or redevelop within the perimeters of that center. The two blocks between Lincoln and Howard Avenues should serve as a specialty commercial area with an orientation towards restaurant and office uses.

Zoning implementation for this area will be tailored zoning in the form of a planned district. Permitted uses will be those of the CC Zone. Residential development in this area should be permitted to a density of 30 dwelling units per net residential acre. This residential density is recommended in order to provide some residential mix which would complement the primary commercial use along 30<sup>th</sup> Street. Commercial use should be required on the ground floor for all development.

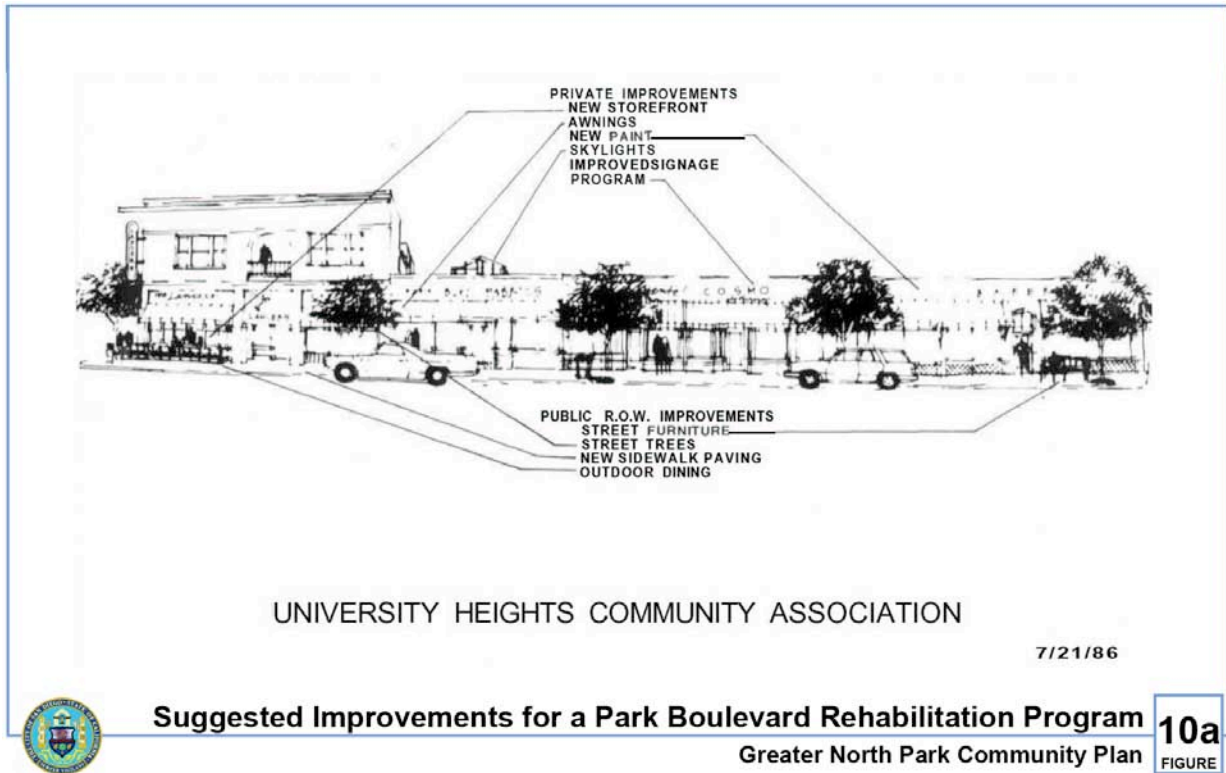


### **Other Neighborhood Commercial Centers (Areas 7, 8, 9 and 16)**

There are four other commercial areas in Greater North Park which should serve as neighborhood commercial centers primarily intended for the convenience of residents of the surrounding areas. These four areas are delineated as follows: The intersection of 30<sup>th</sup> and Upas Street (Area 7); the area centered on the intersection of Thorn and 32<sup>nd</sup> Streets (Area 8); the area along 30<sup>th</sup> Street immediately north of Juniper Street (Area 9) which is part of a neighborhood commercial center lying immediately to the south within the Greater Golden Hill Precise Plan area; and the southeast and northeast corners of the intersection of 30<sup>th</sup> and Redwood Streets (Area 16). Area 7 is recommended for tailored zoning in the form of a planned district. Permitted uses should be those of the CN Zone. Area 8 is recommended for CN zoning to reduce impacts on adjacent low-density residential zoning. Area 9 is recommended for CC zoning in order to be consistent with the commercial zoning and the land use designation of the Greater Golden Hill Precise Plan. The boundaries of these areas are shown on **Figure 8**. Area 16 is recommended for CN zoning in order to reduce impacts on the surrounding low-density residential neighborhood. Residential development in these areas should be permitted to a density of 30 dwelling units per net residential acre. Since these areas are recommended for standard commercial zoning the selected residential density for these areas is consistent with the residential densities permitted in the CN and CC zones.

### **Park Boulevard/Adams Avenue Neighborhood Commercial Area (Area 10)**

Park Boulevard, between Adams Avenue and Madison Avenue and Adams Avenue between Park Boulevard and Georgia Street, provides an opportunity for the upgrading of an existing strip commercial area into a vital compact neighborhood commercial center providing goods and services to the residents of the University Heights portions of the Greater North Park and Uptown communities. This area is shown on **Figure 8**. This commercial area is located in the University Heights neighborhood and is divided between two community plan areas, Greater North Park and Uptown on the east and west sides of Park Boulevard respectively. The area has existing problems which are common to both sides of Park Boulevard. These problems include; vacant storefronts, properties which have been allowed to deteriorate and are in need of rehabilitation, and a need to enhance and upgrade Park Boulevard into a visually pleasing street. The Park Boulevard Revitalization Committee of the University Heights Community Association has prepared a graphic illustration indicating how a revitalized Park Boulevard could appear. This illustration is shown on **Figure 10A**. Zoning implementation for this area should be tailored zoning in the form of a planned district. Development and design standards should be based upon the **Urban Design Element**. In addition, the Trolley Barn Park site, located north of Adams Avenue at the terminus of Florida Street in the Uptown community plan area, has a historical relationship to this area. This relationship should be taken into consideration when zoning regulations are formulated for this area. In addition, the former Mission Cliffs Park, which was a northern terminus of the trolley, also has a historical relationship to this area. The still-existing gateway and wall are the only remaining reminders of this park. Permitted uses should be those of the CC Zone which permits community commercial activities, business and professional offices, convenience goods and services and commercial recreational activities. Residential development in this area should be permitted to a density of 45 dwelling units per net residential acre. This residential density will provide an opportunity



for multiuse development with commercial uses being required on the ground floor. An adjacent area along Adams Avenue to Louisiana Street (Area 17 discussed below) should be permitted to redevelop in either commercial or residential uses, similar to the existing CN Zone.

## MULTIUSE AREAS

Within the Greater North Park community there exists the opportunity to upgrade and redevelop those portions of existing strip commercial development which are underutilized, deteriorated or are currently a mix of commercial and residential development. These areas have potential as multiuse areas which, while providing residential development opportunities, can also provide needed commercial goods and services to residents of these areas. It is recommended that commercial establishments be generally limited to the ground floor of new developments, but for these areas, commercial development should not be required on the ground floor.

Development regulations for these areas will be based upon the **Urban Design Element** and the El Cajon Boulevard Design Study and the North Park Design Study. Building height limitations, based upon relationships with adjacent residential areas should be considered for most or all of these areas.

### **Park Boulevard—Madison Avenue to Meade Avenue (Area 11)**

This portion of Park Boulevard between Madison and Meade Avenues is intended to be a mixture of neighborhood commercial uses and residential development. Permitted commercial uses should be those of the CC Zone which permits neighborhood commercial activities, business and professional offices, convenience goods and services and commercial recreational activities. Residential development should be permitted to a density of 45 dwelling units per net residential acre.

### **Park Boulevard—Howard Avenue to University Avenue (Area 12)**

This portion of Park Boulevard between Howard Avenue and University Avenue is intended to be a mixture of neighborhood commercial uses, office and institutional uses and residential development. Permitted commercial areas should be those of the C-1 Zone, which permits a full range of consumer goods and services and limited wholesaling and warehousing. Residential development should be permitted to a density of 55 dwelling units per net residential acre with a bonus to 75 dwelling units per net residential acre for parcel consolidation and compliance with the design guidelines of the **Urban Design Element**.

### **Thirtieth Street—Madison Avenue to Meade Avenue (Area 13)**

This portion of 30<sup>th</sup> Street between Madison and Meade Avenues is intended to be a mixture of commercial office use and residential development. Permitted uses should be those of the CC Zone which permits community commercial activities, business and professional offices, convenience goods and services and commercial recreational activities. Residential development should be permitted up to a density of 55 dwelling units per net residential acre with a bonus to 75 dwelling units per net residential acre for parcel consolidation and compliance with design guidelines of the **Urban Design Element**.

### **Thirtieth Street—North Park Way to Myrtle Street (Area 14)**

This portion of 30<sup>th</sup> Street is intended to be a mixture of neighborhood commercial uses and residential development. Permitted commercial uses should be those of the CN Zone which permits neighborhood commercial activities, business and professional offices and convenience goods and services. Residential development should be permitted up to a density of 30 dwelling units per net residential acre.

### **University Avenue—28<sup>th</sup> and Idaho Streets to Alley west of Florida Street (Area 15)**

This portion of University Avenue (exclusive of Area 4 between Mississippi and Arizona Streets) between 28<sup>th</sup> and Idaho Streets on the east and the alley west of Florida Street on the west is recommended to be a mixture of neighborhood commercial uses and residential development. Permitted commercial uses should be those of the CC Zone which permits community commercial activities, business and professional offices, convenience pools and services and commercial recreation activities. Area 15 development regulations should be developed which result in low commercial intensities relative to the central business district. Residential development should be permitted up to a density of 55 dwelling units per net residential acre with a density bonus possible to 75 dwelling units per net residential acre if parcel consolidation occurs, if there is compliance with the design guidelines of the **Urban Design Element**, and if access can be safely provided by existing or new alleys or side streets. No additional direct access to University Avenue should be encouraged for residential uses. In addition, new zoning regulations should limit the amount of commercial development in this area by limiting the amount of floor area permitted for commercial uses under new development or redevelopment proposals.

An additional density bonus of up to 110 dwelling units per acre may be permitted if there is compliance with parcel accumulation standards, provision of adequate access to parcels via alleys or side streets, compliance with urban design guidelines and the development is exclusively residential. Amenities should also be provided in the form of common areas and balconies.

### **Adams Avenue—Georgia Street to Louisiana Street (Area 17)**

This portion of the south side of Adams Avenue between Georgia Street and Louisiana Street is intended to be a mixture of neighborhood commercial uses and residential development. Permitted commercial uses should be those of the CN Zone which permits neighborhood commercial activities, business and professional offices and convenience goods and services. Residential development should be permitted up to a density of 30 dwelling units per acre. Because this area is surrounded by single-family and lower-density residential areas, particular care should be taken in formulating implementing design and development guidelines for this area. In addition, the Trolley Barn Park site, located north of Adams Avenue at the terminus of Florida Street in the Uptown community plan area, has a historical relationship to this area. This relationship should be taken into consideration when zoning regulations are formulated for this area.

### **IMPLEMENTATION PROGRAM**

1. Develop, as an implementation program, tailored zoning (Planned Districts) for all designated commercial and multiuse areas in the community. The standards and development regulations should be predicated upon the goals and objectives of the **Urban Design Element** and the implementation studies leading to implementing legislation. The tailored zoning should include, but not be limited to, the following factors: the commercial areas should be separated into nodes and linear strips with separate design and development standards for individual node and linear strip areas and commercial uses should be mandatory on the ground floor of development within designated commercial areas, but should only be optional for multiuse areas.

In order to ensure consolidation and intensification of commercial activities as recommended by this community plan the implementing legislation should limit the amount of floor area available for commercial uses in certain commercially designated areas. These areas include those areas designated as multiuse areas and those portions of El Cajon Boulevard not designated as higher-intensity “nodes” by the El Cajon Boulevard Design Study. Incentives for multiuse projects could also be provided in the multiuse areas by permitting more commercial floor area (a higher FAR) if the project contains both residential and commercial uses. The design study establishes specific “gateways” and “district centers” as higher-intensity “nodes.” It is primarily within these high-intensity nodes that commercial intensification is encouraged, and the permitted floor area and other development criteria of the zones should reflect this concept. It is particularly important to consolidate and intensify commercial development within the central business district centered around 30<sup>th</sup> Street and University Avenue in order to provide the community a central focal point and a strong and viable commercial core.

Refinements and modifications to boundaries, permitted uses and residential densities of the various commercial and multiuse areas may be incorporated into the implementing legislation. In addition, the achievability of recommended residential densities may be predicated upon the design standards, development regulations and other regulations of the implementing legislation.

2. Establish permitted residential densities for the individual commercial and multiuse areas.
3. Unless designated for commercial or multiuse by this document, existing commercially zoned areas should be rezoned to appropriate residential zoning as specified within this document.
4. Establish a Business Improvement District for the commercial and multiuse portions of Park Boulevard.
5. Provide for the visual enhancement and revitalization of Park Boulevard between Adams Avenue and Meade Avenue through the utilization of design and development regulations established as part of the implementation program.
6. Utilize the existing Urban Design studies and the Economic Analysis study as guidelines for the formulation of development regulations.

## **CURRENT AND PROPOSED IMPLEMENTATION PROGRAMS**

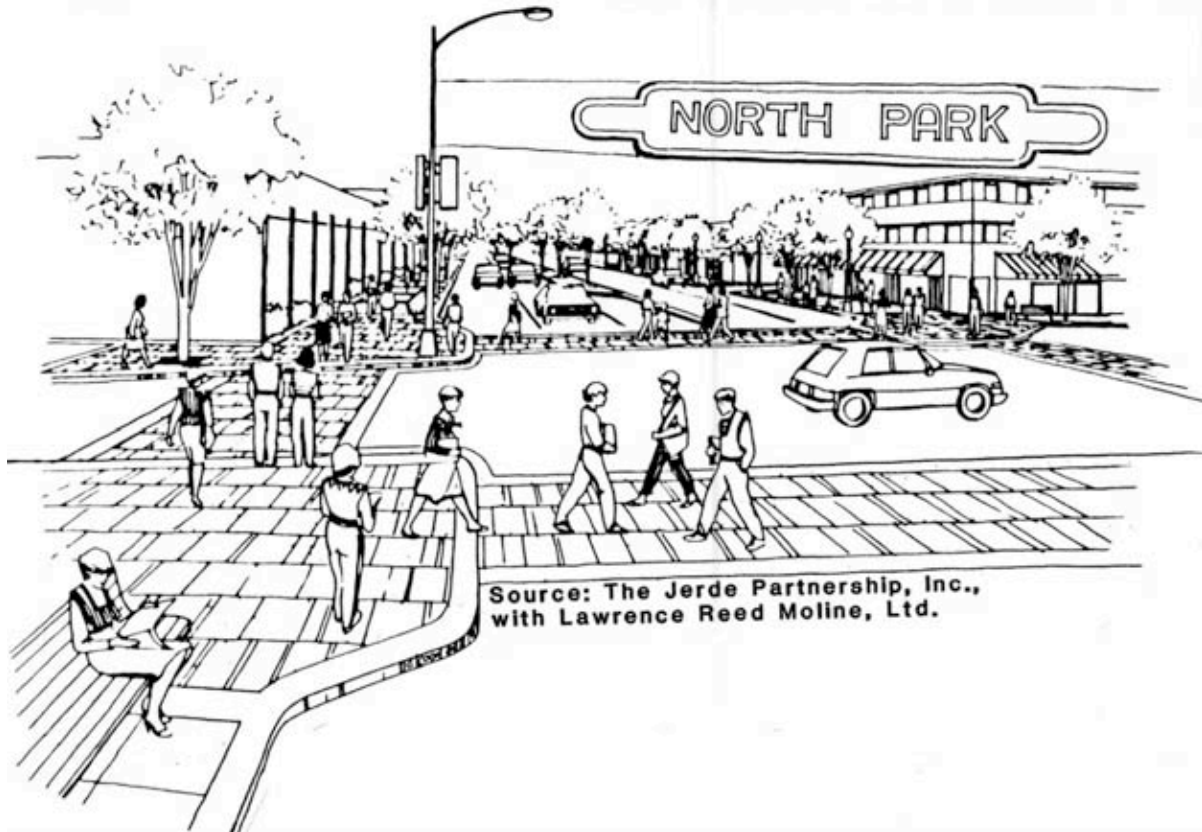
A series of ongoing studies and programs have been initiated by the City of San Diego in order to upgrade and enhance the business establishment in Greater North Park. These studies and programs, which are administered by the Economic Development Division of the Property Department, are described in the following narrative. Most of these studies and programs include both the Greater North Park and the Mid-City communities.

**BUSINESS SURVEY** - This study was completed in 1983, and consists of a computerized survey of Mid-City/Greater North Park business owners regarding their community concerns and business plans.

**ECONOMIC ANALYSIS** - This study, which was completed in 1983 for the City of San Diego, provides information to plan and implement commercial revitalization and other economic development efforts based on the commercial strengths and weaknesses of the Mid-City/Greater North Park community and its potential for commercial revitalization.

The study reviews the Mid-City/Greater North Park community as a part of the greater San Diego Metropolitan market and also focuses on the economic conditions and trends within the Mid-City/Greater North Park marketplace.

**DESIGN STUDY FOR THE COMMERCIAL REVITALIZATION OF EL CAJON BOULEVARD** - Provides recommendations to facilitate revitalization efforts for public/private improvements to be implemented in order to create a sense of identity for the boulevard.



**NORTH PARK DESIGN STUDY** - Includes a concept design for public improvements to be implemented for the Greater North Park central business district, as well as design guidelines for specific private improvements.

**PARKING STUDY** - This study is to be completed in 1986 and will assess the parking supply and demand in the Mid-City and Greater North Park areas and provide specific solutions to parking needs within targeted areas.

### **Public Improvements**

**NORTH PARK** - Public improvements include a pedestrian plaza on Ray Street, upgraded sidewalks and park benches on University Avenue between 30<sup>th</sup> and Grim. Completion date - Summer 1987.

**EL CAJON BOULEVARD** - Public improvements include a Gateway sign at El Cajon Boulevard and Park Boulevard. This sign is designed to enhance community identity for the six-mile long commercial strip. Completion date - end of 1986.

### **Loan Program**

Low-interest loans are available to business and property owners for facade and visible interior improvements. Eligible areas include:

- North Park - University Avenue between Idaho and Iowa. Thirtieth Street between Lincoln and University Avenue.
- Normal Heights - Adams Avenue between Kansas Street and Ohio Street.

### **Rebate Program**

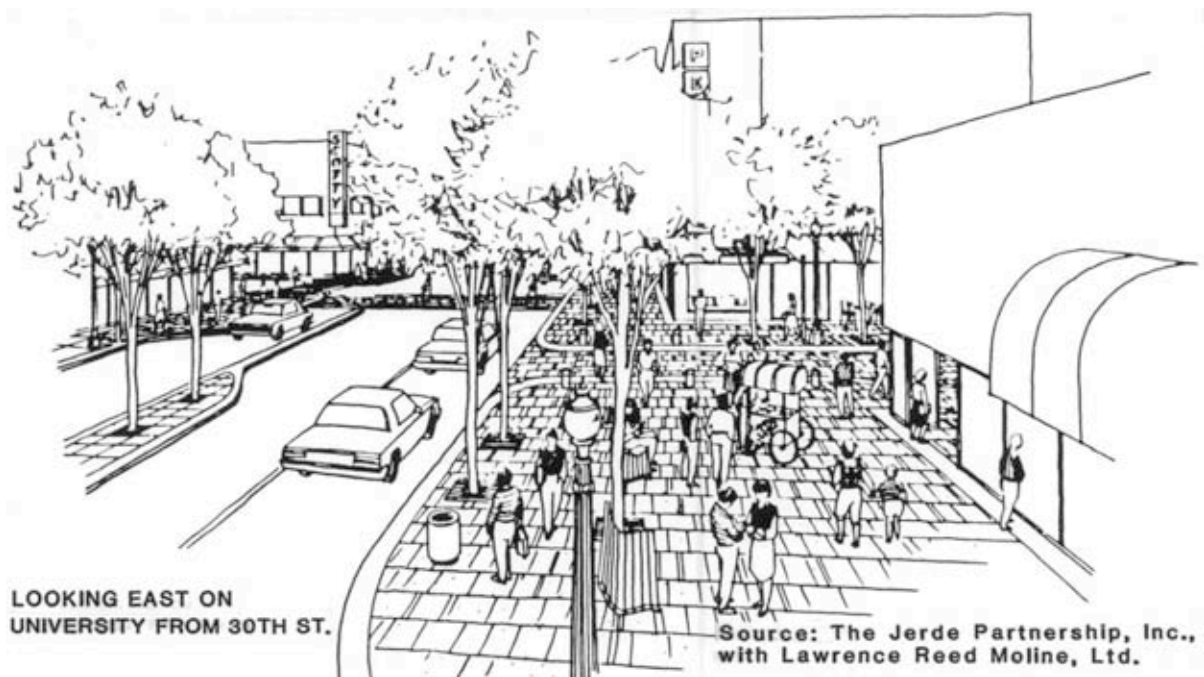
The Rebate Program provides rebates for exterior improvements of the project cost up to \$5,000. Eligible areas include:

- El Cajon Boulevard - Park Boulevard to I-805.
- University Avenue - Idaho to I-805.
- Adams Avenue - Texas to I-805 (goes beyond I-805, but out of the Greater North Park Community Plan boundaries).

Design Guidelines must be followed for eligibility for both loan and rebate program.

Other potential areas which should be considered for participation in the Rebate Program include:

- Park Boulevard - Robinson Avenue to Adams Avenue.
- Adams Avenue - Park Boulevard to Louisiana Street.
- Thirtieth Street - Adams Avenue to Upas Street.
- University Avenue - Idaho Street to alley west of Florida Street.



In giving consideration to potential areas for participation in the rebate and other revitalization programs, priority should be given to those areas designated commercial by the community plan over those areas designated multiuse in order to provide additional incentives to consolidate and intensify commercial development within the community. These programs should assist in rehabilitating the visual and pedestrian environment of these areas proposed for intensified commercial development, thus encouraging increased patronage by the residents of the community.

### **Business Improvement District**

A Business Improvement District (BID) is a program authorized by State Law (AB 1693) which allows an assessment to be placed on businesses, in addition to their business license fee. This money can only be used within the district boundaries and can only be used for the following:

- a. The acquisition, construction or maintenance of parking facilities for the benefit of the area.
- b. Decoration of any public place in the area.
- c. Promotion of public events that are to take place on or in public places in the area.
- d. Furnishing music in any public place in the area.
- e. The general promotion of retail trade activities in the area.

Each district can set its own priorities within the allowed activities or uses.

There currently are three Business Improvement Districts within the Greater North Park community. They are North Park central business district, El Cajon Boulevard and Adams Avenue.

The North Park BID encompasses all the business license holders on University Avenue between Idaho and I-805 and one block north and south of University Avenue.

The El Cajon BID consists of the businesses on El Cajon Boulevard between Park Boulevard and I-805 and one block north and south of El Cajon Boulevard.

The Adams Avenue BID goes beyond the Greater North Park community boundaries. It covers all businesses along Adams Avenue between Texas and 40<sup>th</sup> including one block north and south of Adams Avenue.



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## **TRANSPORTATION AND CIRCULATION**

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### **GOAL**

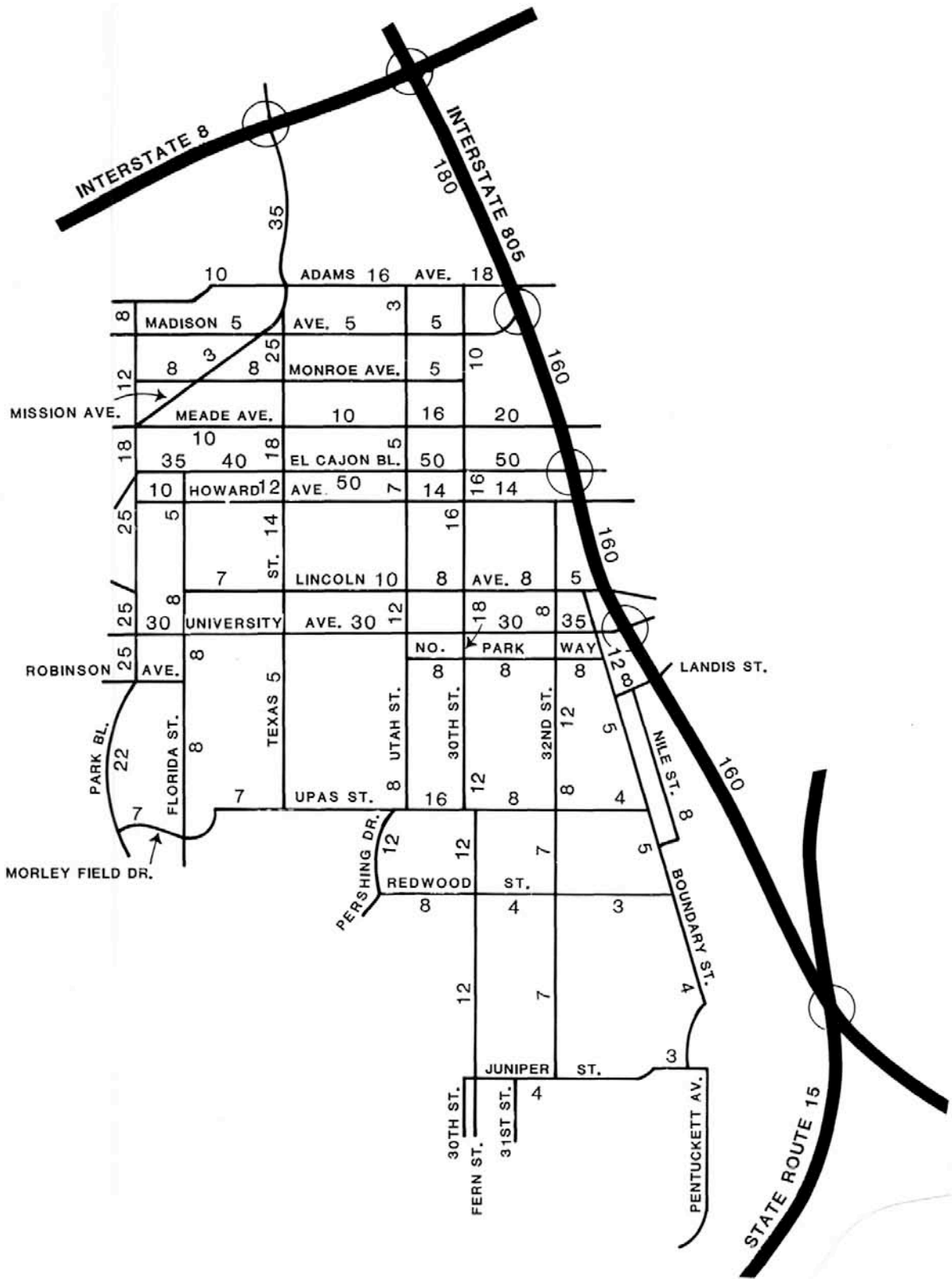
Provide a safe and efficient transportation system that maximizes access for residents and visitors to the community, links the community to major activity centers, and minimizes adverse environmental effects.

### **EXISTING CONDITIONS**

The existing street system serving Greater North Park is basically a gridiron pattern which was originally laid out after the turn of the century. Greater North Park is served by two major streets, El Cajon Boulevard and University Avenue, which provide east-west access to the Uptown community on the west and to the Mid-City and State College communities on the east. Adams Avenue also provides a connection to the east, linking the Greater North Park neighborhoods of Normal Heights and University Heights with the Mid-City neighborhoods of Normal Heights (which was split by I-805), Kensington and Talmadge. The major north-south streets in the community are 30<sup>th</sup> Street, which provides a link with the Greater Golden Hill community and Centre City; Texas Street, which provides access to Mission Valley and into Balboa Park; and Park Boulevard, which is adjacent to Uptown and provides access to Balboa Park and to Centre City. Other surface streets of importance are two east-west streets, Meade Avenue and Lincoln Avenue and two north-south streets, Utah Street and 32<sup>nd</sup> Street.

In addition to the surface street system, Greater North Park has access to the regional freeway system. There is direct access to I-805 via El Cajon Boulevard and University Avenue. Interstate 805 provides access to southbound SR-15 and to SR-94. State Route 94 can also be accessed through Greater Golden Hill. Interstate 8 in Mission Valley is accessible by way of Texas Street while SR-163 to the west is accessed through the Uptown community via El Cajon Boulevard and Washington Street and also by I-805 by way of the northbound on-ramp from Madison Avenue in the Mid-City community.

Greater North Park is presently served by public transit in the form of bus service on a number of routes, both local and express. The service, while adequate, needs to be upgraded in frequency of service and, possibly, by the addition of new routes. Greater North Park, at one time, was the beneficiary of an electric street rail system that linked the community with Hillcrest, Mission Hills, downtown San Diego, Golden Hill, Normal Heights, Kensington and East San Diego. Adams Avenue and University Avenue accommodated the east-west trolley tracks while Park Boulevard and 30<sup>th</sup> Street accommodated the north-south trolley tracks.



12 2005 Average weekday trips (thousands)

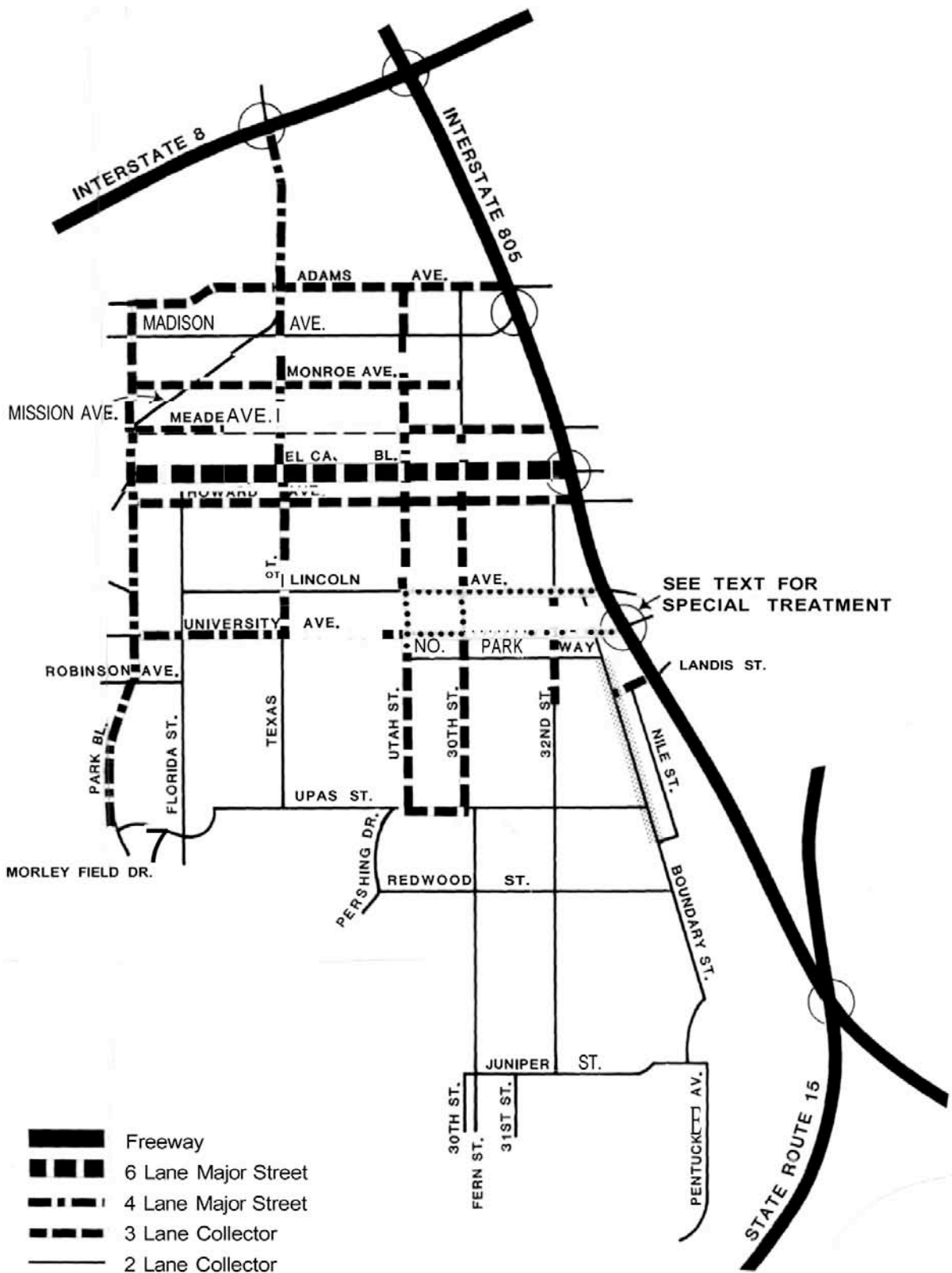









**Future Traffic Volumes**  
Greater North Park Community Plan

**11**  
FIGURE

## **OBJECTIVES**

- Protect residential areas from through traffic by encouraging through traffic to use freeways and major streets, while discouraging through traffic on local streets in the community.
- Improve the street system as necessary to accommodate growth in locally-generated traffic while minimizing adverse effects on existing residential, business or open space uses.
- Reduce vehicular traffic in Greater North Park by encouraging the use of alternative modes of transportation, including public transit, bicycles and pedestrian travel.
- Provide a high level of public transportation service linking Greater North Park with Mission Valley and Downtown.
- Provide adequate off-street parking in residential and commercial areas.
- Maintain the pedestrian interface between Balboa Park and the community, ensuring that vehicular access to Balboa Park does not use local streets in Greater North Park as through travel routes.
- Visually enhance transportation corridors to improve community image and identification.
- Evaluate the feasibility of providing fixed rail transit service to the community.
- Establish a transit point at University Avenue and 30<sup>th</sup> Street in order to provide support to the central business district.
- Enhance existing urban level bus service by increasing the frequency of service, adding express service and improving transit stops, thereby establishing a higher level of service within the community and providing strong public transit links with adjacent communities.
- Utilize DART (Direct Access to Regional Transit) within various areas of the community where fixed route transit services are not readily available in order to bring service closer to the door of the aged, the infirm and other transit-dependent groups.
- Develop a system of bikeways to connect the various neighborhoods within the community and to connect with major activity centers in San Diego.
- Install secure bicycle parking facilities at major activity centers, including shopping centers, employment centers, parks and schools.



-  Freeway
-  6 Lane Major Street
-  4 Lane Major Street
-  3 Lane Collector
-  2 Lane Collector
-  1 Lane Collector
-  2 Way Couplet System



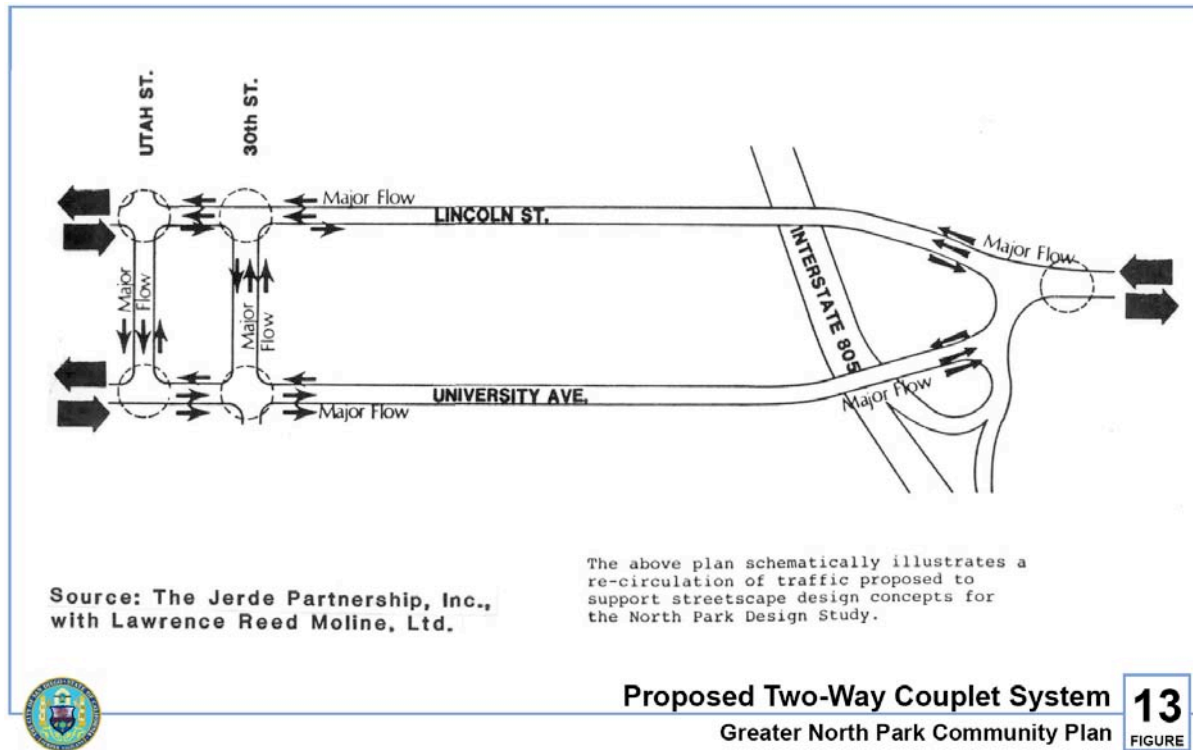
**Future Street Classifications**  
 Greater North Park Community Plan

**12**  
 FIGURE



## STREET SYSTEM

The Traffic Forecast Study for Greater North Park has given the 20-year projections for surface street volumes in the community (see **Figure 11**). In order to accommodate the increased volumes, a series of recommended improvements to the street system have been formulated. These improvements will provide an upgraded street system as illustrated in **Figure 12**. The recommended improvements are as follows:



## PHYSICAL IMPROVEMENTS

### El Cajon Boulevard

El Cajon Boulevard, between Park Boulevard and I-805, should have left-turn pockets retained at appropriate intersections. However, any new construction of left-turn pockets should be considered in conjunction with improved landscaping in the median strips and a need for safe and convenient pedestrian crossings. If there are conflicts with median landscaping and pedestrian crossings, then a reduction in the number of existing left-turn pockets should be considered.

### University Avenue

University Avenue, between Utah Street and Boundary Street, should be improved as part of a two-way couplet system with Lincoln Avenue. The two-way couplet circulation system is recommended for the central business district by the North Park Design Study. This system would pair University Avenue and Lincoln Avenue into a two-way couplet system with

University Avenue carrying two lanes eastbound and one lane westbound, and Lincoln Avenue carrying two lanes westbound and one lane eastbound between Utah Street and Boundary Street. This system would also include the two-way couplet pair of 30<sup>th</sup> Street carrying two lanes northbound and one lane southbound, and Utah Street carrying two lanes southbound and one lane northbound.

This system may require operational improvements in order to be functionally incorporated into the overall Greater North Park circulation system (see **Figure 13**). Physical improvements for vehicular circulation should not include increasing the existing curb-to-curb width or reducing existing sidewalk widths. However, a minimum of 52 feet curb-to-curb is required on University Avenue to accommodate the two-way couplet system, except between Ray and 28<sup>th</sup> Street, where a minimum of 72 feet curb-to-curb is needed. Minor sidewalk widening and other public improvements, such as landscaping, could be considered in the section between Ray and 28<sup>th</sup> Street as long as the 72-foot curb-to-curb width is maintained to allow left-turn lanes, four travel lanes and on-street (parallel) parking.

Should the two-way couplet system described above fail to function at some future time due to unanticipated growth or other changes in the region, then the option of going to four lanes on University Avenue should be considered. The four-lane system should only be implemented if the two-way couplet has clearly been shown to be inadequate. The provision of four travel lanes on University Avenue would require the removal of most of the existing on-street parking, which could hinder the revitalization efforts for the corridor.

Between Utah and Florida Streets, University Avenue should have a 60-foot curb-to-curb width in order to accommodate four traffic lanes. The existing width of the street between Florida Street and Park Boulevard is adequate to meet future needs. Finally, between Bancroft and Boundary Streets, it will be necessary to widen University Avenue an additional ten feet for right-turn monuments from eastbound University Avenue to southbound Boundary Street.

In addition, the University Avenue bridge over I-805 should be widened by twelve feet.

### **Texas Street**

Texas Street, between Madison Avenue and Camino del Rio South, should be widened from three lanes to four lanes to provide an additional northbound traffic lane within the existing right-of-way.

In addition, Texas Street between El Cajon Boulevard and Madison should be widened to four lanes with a 60-foot curb-to-curb width within the existing 80-foot right-of-way. Left-turn movements would be controlled through operational improvements which would be determined during the street-widening program. This improvement would still permit on-street parking on Texas Street, except where left-turn lanes are needed.

## **32<sup>nd</sup> Street**

Thirty-second Street, from Landis Street to University Avenue, should be widened from 45 feet to 52 feet to provide three lanes of traffic with parking allowed on both sides of the street. Additional right-of-way will be needed for this widening.

## **Boundary Street**

Boundary, between University Avenue and North Park Way should be widened by 12 feet on the west side to a four-lane collector street, if CALTRANS widens the southbound I-805 on-ramp at North Park Way to two lanes. A traffic signal should be installed at Boundary Street and North Park Way in conjunction with this work.

## **OPERATIONAL IMPROVEMENTS**

The following streets should be restriped as three-lane collector streets when individually warranted by future traffic volumes:

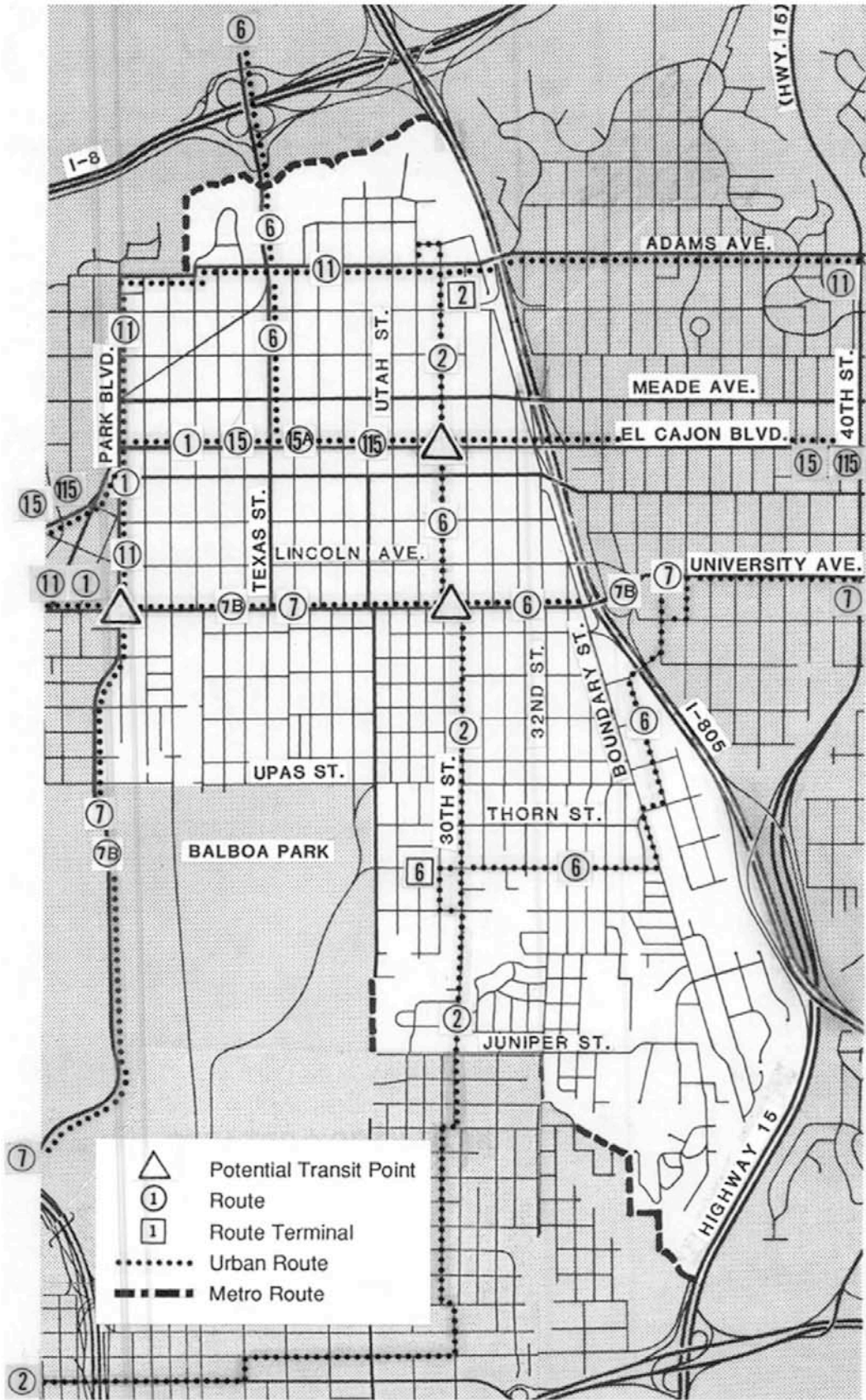
1. Adams Avenue, from Park Boulevard to Texas Street.
2. Upas Street, from Pershing Drive to 30<sup>th</sup> Street (west intersection).
3. Landis Street, from Boundary Street to Swift Avenue.
4. Park Boulevard, from Meade Avenue to Adams Avenue.
5. Utah Street, from Upas Street and Adams Avenue.
6. 32<sup>nd</sup> Street, from Lincoln Avenue to University Avenue.

## **University Avenue**

Future traffic volumes on University Avenue across I-805 may necessitate some operational measures to provide indirect vehicular access to northbound and southbound traffic on I-805. These measures could involve prohibiting left turns on University Avenue, rerouting traffic and designating some streets one-way to provide access to I-805.

## **PUBLIC TRANSIT**

Greater North Park, in its relationship with surrounding communities, can be the beneficiary of a comprehensive public transit system. Being essentially a residential community, Greater North Park can provide the opportunity to live within commuting distance of the employment centers of Centre City and Mission Valley. In addition, the public transit system can support the community's commercial establishment by providing a primary means of access to those residents of the community who do not have personal transportation but still need to have access to goods and services.



**Bus Routes**  
 Greater North Park Community Plan **14**  
 FIGURE



The present public transit system should be upgraded to provide greater frequency of service, additional routes, more direct access to employment centers and to adjacent communities, and include, if feasible, a fixed rail transit system.

The major bus routes serving Greater North Park are all operated by the San Diego Transit Corporation and are illustrated in **Figure 14**.

The Metropolitan Transit Development Board has developed the Metropolitan San Diego Short Range Transit Plan which is the basis for transit services in the San Diego Region. This plan is updated on an annual basis in order to meet changing needs within the region. Therefore, the public transit needs of Greater North Park can be evaluated on any annual basis. The key aspects of the plan are:

- Transit centers and transit points at strategic points throughout the area to provide connections between transit services; and
- Three levels of transit service (METRO, URBAN and LOCAL/FEEDER), to provide the most efficient, cost-effective service.

METRO routes are for regional or long-distance travel utilizing freeways or light rail wherever possible, with stops only at transit centers and major residential and activity centers. URBAN routes are moderate or long distance, connecting communities via transit centers and transfer points. LOCAL/FEEDER routes are short distance intra-community bus and van services serving neighborhood destinations and transit centers in the immediate area.

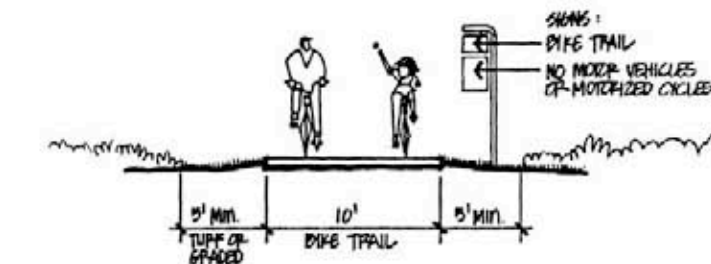
The Short Range Regional Transit Plan is the five-year plan for transit service for the metropolitan area. A high priority is placed on the implementation of regionally significant transit routes, two of which currently traverse the Greater North Park area (Routes 7 and 15).

El Cajon Boulevard should be developed as a public transit spine, featuring upgraded bus service connecting Greater North Park to Mid-City, Uptown and Centre City.

While a fixed rail transit system is considered infeasible for North Park, physical and operation improvements to the bus system can provide the same benefits at a lower cost. If future evaluation of a fixed rail transit system is undertaken, it should only be upon determination that all feasible improvements to the bus system have been accomplished and that an additional level of public service is needed in the community.

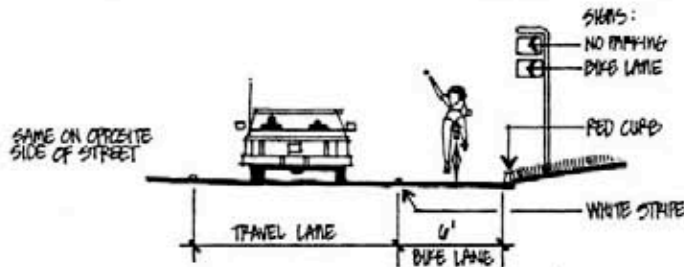
Within Greater North Park, the intersection of 30<sup>th</sup> Street and University Avenue should become the focal point of the public transit system. A transit point should be established either at or in the vicinity of this intersection. This transit point will provide access to Centre City, Mission Valley, to the East Line of the San Diego Trolley at 32<sup>nd</sup> Street and Commercial Street and to adjacent communities and other important activity centers via existing bus routes. This transit point could also become the focal point of a possible future feeder bus system or intra-community shuttle bus system which would provide access to the community's retail and social center for those who lack personal transportation.

Bicycle Path - A completely separate right-of-way for the exclusive use of bicycles. (Class I)



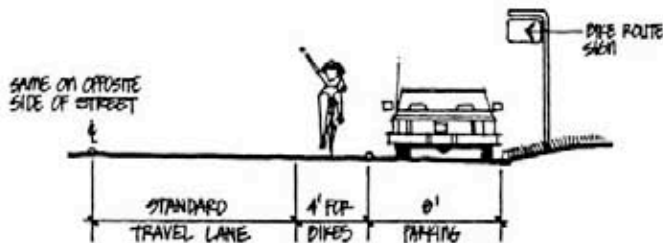
**CLASS I** open space

Bicycle Lane - A restricted right-of-way located on the paved road surface alongside the traffic lane nearest the curb, and identified by special signs, land striping, and other pavement markings. (Class II)



**CLASS II** major street

Bicycle Route - A shared right-of-way designated by signs only, with bicycle traffic sharing the roadway with pedestrian and motor vehicles. (Class III)



**CLASS III** neighborhood street

\* The dimensions illustrated on this page are subject to change.



A transit point is a transit stop with either a high number of boardings and transfers or is a transit stop which serves two or more bus routes at an intersection crossed by one or more other routes. A transit point is typically equipped with a shelter, adequate seating, posted timetables and route maps and trash receptacles. Optional facilities include a posted system map and a telephone.

In addition to the recommended transit point at 30<sup>th</sup> Street and University Avenue, the intersections of El Cajon Boulevard and 30<sup>th</sup> Street and Park Boulevard and University Avenue meet the minimum standards for transit point status and should receive the same transit point improvements as 30<sup>th</sup> Street and University Avenue.

An existing DART (Direct Access to Regional Transit) system is presently functioning in the southeastern portion of the community and interfaces with Route 6. The DART system is designed to provide access to public transit lines for areas with low population densities or topographic problems. Service is provided on a demand basis through contractual agreements between the transit company and taxi companies. For instance, pick-up service is provided between transit stops and private homes by the taxi companies.

## **BIKEWAYS**

There are no bike lanes in the Greater North Park community. The only designated bike route runs the length of Howard Avenue from Park Boulevard to I-805, where it crosses into Mid-City and links up with that community's only bike route extending along Orange Avenue to 54<sup>th</sup> Street.

Bikeways are classified into three general categories based on the degree or extent of their improvements (see **Figure 15**).

Ideally, Greater North Park should have an extensive bikeway system since the relatively level topography of the community lends itself to the use of the bicycle as an alternative mode of transportation. A bikeway system should not only provide access throughout the community, but should provide access to and from Balboa Park and adjacent communities. Given Greater North Park's urban environment and proximity to employment centers and other activity centers, it is logical that the bicycle will ultimately become an important alternative means of personal transportation.

Whenever possible, bicycle lockers or areas of restricted access should be provided for employees who commute to work by bicycle. In addition, bicycle racks should be provided for customers who travel by bicycle. These bicycle racks should be placed in visible locations near store entrances, but should not impede pedestrian circulation and should be of a secure and stable design. Bicycle parking signs might be used to identify bicycle parking areas.

Streets that should be included in a bikeway system include all or portions of Howard Avenue (existing route), Adams Avenue, Landis Street, Morley Field Drive, Upas Street, Thorn Street, Juniper Street, Park Boulevard, Louisiana Street, Texas Street, 28<sup>th</sup> Street, Utah Street, Boundary Street and Nile Street. In addition, the two-way couplet of University Avenue and Lincoln Avenue serving the Central Business District should include bike lanes



- ..... Class II
- - - - - Class III
- Class III (Existing)



**Bicycle Routes**  
**Greater North Park Community Plan** **16**  
 FIGURE

in order to provide alternative means of access to the business area. The proposed bikeway system is shown on **Figure 16**. In developing the bikeway system, consideration should be given to the safety of bicyclists and any negative impacts on vehicular travel lanes and on-street parking as well as to any safety problems which may exist for bicyclists.

Bicycle routes should be adequately identified by proper signage. Destination plates should be added to selected bicycle route signs for the purpose of identifying the routes to major activity centers and to secure parking facilities in these activity centers.

## **PEDESTRIAN CIRCULATION**

Greater North Park, because of its generally level topography, is a community in which walking should be encouraged. Given its public transit system, its readily accessible commercial areas, Balboa Park and other community activity centers, pedestrian access becomes an important alternative form of transportation.

Walking is a form of transportation which must be provided for, especially in neighborhoods for short trips to local commercial and public facilities and in business areas where many shoppers congregate. Sidewalks, malls and similar spaces provide not only for pedestrian movement but also for children's play, socializing among residents, window shopping, and sitting and watching. Congestion occurs on sidewalks in high activity areas, just as it does on streets. The inadequacy of pedestrian space creates inconveniences for those trying to pass through and those shopping or stopping to talk or look or rest.

In many high-activity areas, the sidewalks are narrower than required for pedestrians. Where pedestrian traffic is high and through vehicular traffic is light or can be moved to alternate routes or reduced by transit improvements, some street space should be converted into wider sidewalks, landscaped strips, and sitting areas. In high-density residential areas with little open space, wider sidewalks and small plazas should be created to provide more usable space as well as to discourage through traffic.

Pedestrian walkways should be sharply delineated from traffic areas, and set apart where possible to provide a separate circulation system. Separation should include landscaping and other barriers, and walkways should pass through the interiors of blocks wherever practical in commercial areas. Walkways in commercial areas that cross streets should also have pavement markings and good sight distances for motorists and pedestrians.

Driveways across sidewalks should be kept to a practical minimum, with control maintained over the number and width of curb cuts. Barriers should be installed along parking lots to avoid encroachments on sidewalks, with adequate sight distances maintained at driveways. Truck loading should occur on private property rather than in roadways or on sidewalks.

Where streets are designed for high volumes or relatively fast movements of vehicles, adequate provision must be made for safe and convenient pedestrian crossings.

In a community like Greater North Park, with its gridiron street system and generally level topography, local residents will typically discover their own favorite routes, varying them as destinations, purpose of trip, desire for variety, availability of traffic signals become personal factors. Therefore, the sidewalks of the entire street system lend themselves to providing

pedestrian access to Balboa Park, commercial and other public activity areas and to public transit corridors. In effect, the street system becomes a community-wide pedestrian circulation system.

## **PARKING**

Greater North Park, being an older community, experienced the bulk of its development during the first half of this century. Most of that development provided little or no off-street parking. This is seen today in the commercial areas where redevelopment has not occurred or where, because of zoning requirements in force at the time, sufficient off-street parking was not provided as part of redevelopment. As people have become more reliant upon the automobile, this lack of adequate parking in commercial areas has tended to hurt the individual businesses, especially in light of competition from shopping centers with more than adequate parking.

In order to rectify the existing parking problem, new increased parking requirements will be necessary. As described in the Commercial Element, Business Improvement Districts (BIDs) have been approved for El Cajon Boulevard, a portion of Adams Avenue and the 30<sup>th</sup> and University commercial center. These BIDs provide for the acquisition, construction or maintenance of parking facilities. If necessary, new parking requirements for new development or redevelopment will be formulated and applied to the commercial areas in Greater North Park.

In those residential areas where higher-density residential redevelopment has occurred, a combination of excessive curb cuts and inadequate parking requirements for new development has resulted in the reduced availability of on-street curb side parking, upon which early development is dependent, thereby creating an overall parking shortage.

In order not to further compound the parking problem in the multifamily residential areas, new curb cuts should be restricted or prohibited in order to preserve existing on-street parking for that older development which is dependent upon it.

## **IMPLEMENTATION PROGRAM**

1. Establish, within the Capital Improvements Program and a long-range financing plan, a program for prioritizing and financing the circulation system and bikeways system.
2. Provide input to the annual update of the Metropolitan San Diego Short Range Transit Plan in order to ensure that the public transit needs of the community are met.
3. Implement the two-way couplet system for the central business district as recommended by the North Park Design Study.

Refinements and modifications to the recommended circulation system, the bikeway system and other aspects of this element, may result from the formulation of implementing legislation. In addition, the achievement of recommended development intensities may be predicated upon the ability to adequately finance the public improvements called for in this element.

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## COMMUNITY FACILITIES

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### GOAL

Establish and maintain a high level of public facilities and services to meet the needs of the community.

### OBJECTIVES

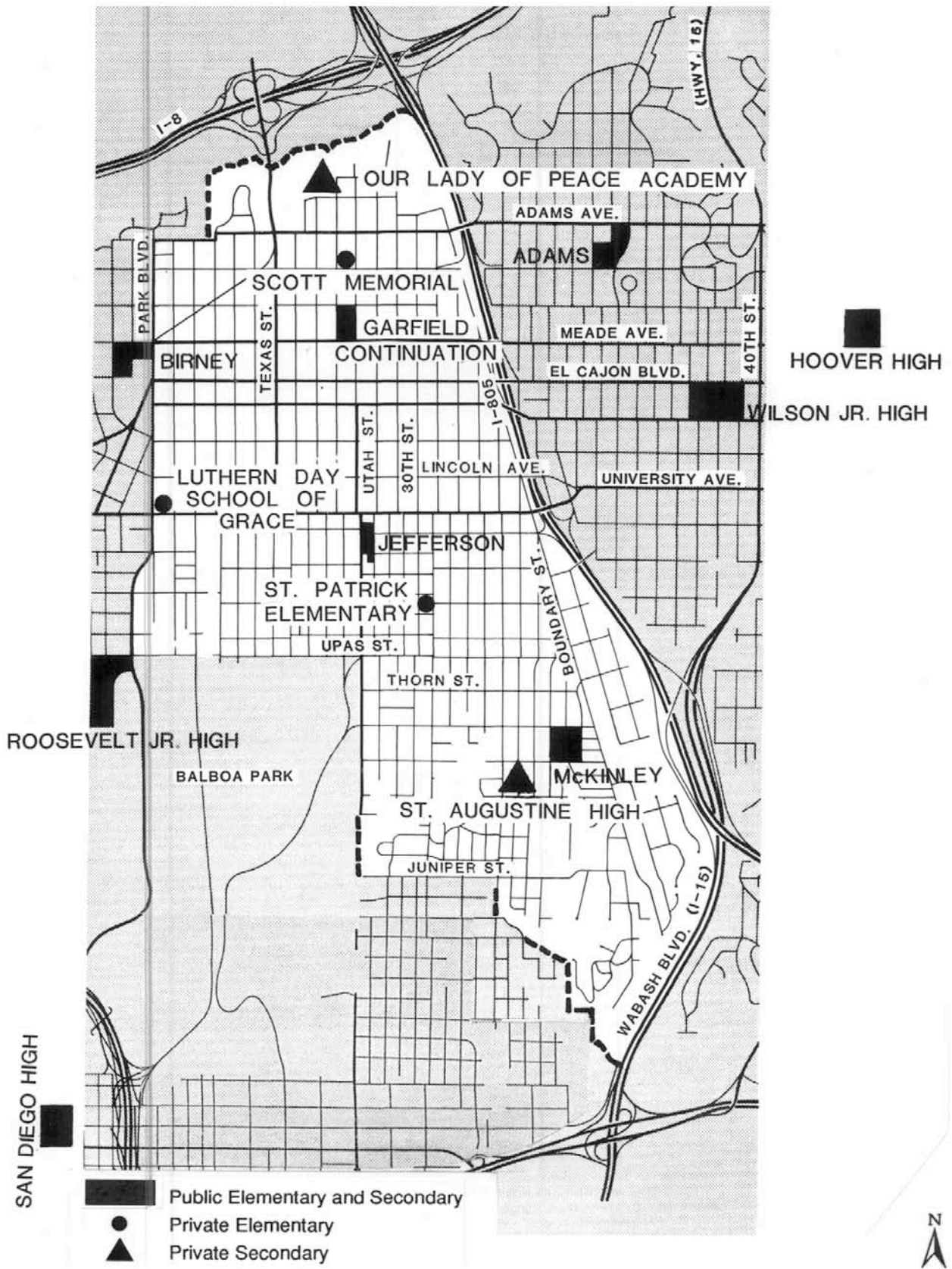
- Provide educational facilities, law enforcement, fire protection, libraries and public utilities in accordance with City standards.
- Provide childcare services to serve families in North Park.
- Program the systematic improvement and gradual replacement of water and sewer facilities.
- Program the undergrounding of telephone and electric power lines.
- Maximize the use of existing facilities for community activities.
- Provide improved street lighting at appropriate locations.
- Encourage a full range of health care facilities within the community.

### EXISTING CONDITIONS AND RECOMMENDATIONS

#### Schools

Public education is provided by the San Diego Unified School District. Elementary schools serving the area include Jefferson and McKinley, both located within the community, and Birney, located to the west of the community (see **Figure 17**). Junior high school students attend Roosevelt and Wilson, both located outside the community. High school students attend San Diego and Hoover, also located outside the community (see **Figure 17**). Garfield Independent Learning Center, located within the community, is a continuation school for secondary students from throughout the school district.

Of the elementary schools serving the community, only McKinley's enrollment is currently exceeding its operating capacity. This situation will be corrected by the addition of portable classrooms. However, enrollments have increased recently and are projected to continue to increase in the next five years as a result of several factors, including the recent influx of young families with children moving into the area and expected housing growth. This means that Birney and Jefferson may also eventually exceed their operating capacities (see **Table 4**). All public elementary schools serving the area have insufficient usable land area, according to current General Plan standards, which establish ten net usable acres as the standard for elementary schools.



Schools **17**  
Greater North Park Community Plan **FIGURE**



All secondary schools serving the area are operating under capacity (see **Table 4**). Several parochial schools are also located in the area. Elementary schools include Saint Patrick's School and Lutheran Day School of Grace. Secondary schools are Saint Augustine High School, Scott Memorial and Our Lady of Peace Academy (see **Figure 17**).

### **School Recommendations:**

As can be seen from the above discussion, there are a number of problems associated with existing school facilities in Greater North Park. The elementary schools are either over operating capacities or are projected to be over operating capacities in the future. Possible alternatives to accommodate expected elementary school enrollment include the relocation of special education or other uses occupying classrooms, the addition of portable classrooms, or the adjustment of school boundaries. Operating capacities can be increased through the use of portable classrooms which is the traditional method of adjusting to enrollment capacity problems. However, portable classrooms utilize, in many instances, playground space, reducing recreational acreage available both to students and to residents of the surrounding communities.

Street closings can be one method of adding needed land area, but this is a limited approach at best. One potential street closing is Gunn Street between 28<sup>th</sup> and Idaho Streets. Possible partial street closings include Oregon and Idaho Streets between Monroe and Meade Avenues (abutting the Garfield Continuation School) and between Howard and Lincoln Avenues (abutting the North Park Recreation Center). These streets (Oregon and Idaho Streets) could be narrowed and made one-way streets providing use of public right-of-way in school and recreational use. A more costly solution, which is being utilized in the Mid-City community, is the purchase of abutting properties by the City for joint school and public recreational usage. In any event, efforts should be made to provide full-time use of school facilities, including full community use during non-school hours for educational, recreational and cultural needs.

As it has done in other communities, the San Diego Unified School District could also establish a planning process involving design professionals, school district staff and community members for the purpose of identifying problems and needs and alternative solutions. These solutions could include the following:

1. Improve the aesthetic educational environment at each of the elementary schools. Improvements needed may differ from school to school and include interior and exterior painting, landscaping, and turfing.
2. Improve the existing permanent facilities at some sites. Consideration could be given to the expansion of permanent facilities at some of the elementary schools to accommodate the large and growing enrollment and reduce the high percentage of portable classrooms. New facilities could be integrated architecturally with existing buildings. Underground buildings with play areas on top could be considered for some schools. In addition, multistory buildings could be considered for some sites in order to conserve play areas.

3. The school district could work with the City to acquire additional land to expand existing sites. In some cases, the expanded sites could be used for educational facilities and in other instances, joint use with City parks could be accomplished.
4. There could be explorations of the possibility of using new buildings jointly for commercial and educational use. The joint use of buildings would provide revenue to the district and give the district a means for reducing further the number of portable classrooms at each school.
5. Consideration could be given to develop ways to deal more aesthetically with portable classrooms. This could be accomplished through a combination of landscaping and arrangement of the portables.

These concepts could ultimately be developed into long-range physical master plans for the individual school sites. The development of these master plans could be accomplished as follows:

- Master planning could be carried out for each school by a committee composed of school staff, school district central office staff, community representatives and consultants. These committees could define the educational and physical needs for each school. After these needs were defined, an architectural firm could be obtained to prepare individual physical master plans for each school site.
- Each master plan could be developed based upon necessary review and evaluation of existing conditions, buildings that could be retained and new buildings that could be constructed. In addition, playgrounds, landscaping, parking and traffic circulation could be considered. Cost estimates for the implementation of the master plan could also be developed.
- All elements of the master plan could be reviewed by the planning committee which could establish priorities for the implementation of the master plan. The master plan for each individual school site would then be presented to the board of education for its approval.

Additionally, the San Diego Unified School District should consider the following site-specific solutions:

1. The Garfield Continuation School was previously an elementary school and could, if conditions warrant, revert back to that use. However, the question of relocation of existing educational services on the site would have to be answered. An additional problem is that most of the elementary school buildings were demolished or extensively remodeled to provide for a secondary school facility.
2. In the event that the functions of the existing Educational Center on Normal Street are moved to another location, consideration will have to be given to the reuse of the site. Although the existing facility is located in the Uptown community plan area, reuse of the site for other than educational purposes could have impacts on Greater North Park. If the Educational Center is relocated, consideration should be given to using the site to meet the educational needs of the surrounding communities. The main structure itself is of historical significance and consideration should be given to its preservation.

The San Diego Unified School District is presently developing a Long Range Facilities Master Plan (LRFMP) which is intended to determine future facilities needs to the year 2000, educational programs for the same period of time, facilities utilization policies, and availability of financial resources.

**TABLE 4  
PUBLIC SCHOOL ENROLLMENT**

School	Actual Enrollments		Projected Enrollments*			Capacities 1985 – 1986	
	October 1985		1986	1989	1995	Operating Capacity	Total Capacity
<u>Elementary</u>							
Birney	617	(K-6)	626	661	700	654	810
Jefferson	560	(K-6)	586	658	675	570	750
McKinley	520	(K-6)	534	535	525	570	630
<u>Junior High</u>							
Roosevelt	1,075	(7-9)	1,227	1,350	1,550	1,274	1,770
Wilson	1,496	(6-8)	1,528	1,429	1,700	1,723	2,052
<u>Senior High</u>							
Hoover	1,886	9-12	1,950	2,006	2,180	2,047	2,424
San Diego	1,406	10-12	1,383	1,475	1,700	1,742	2,262

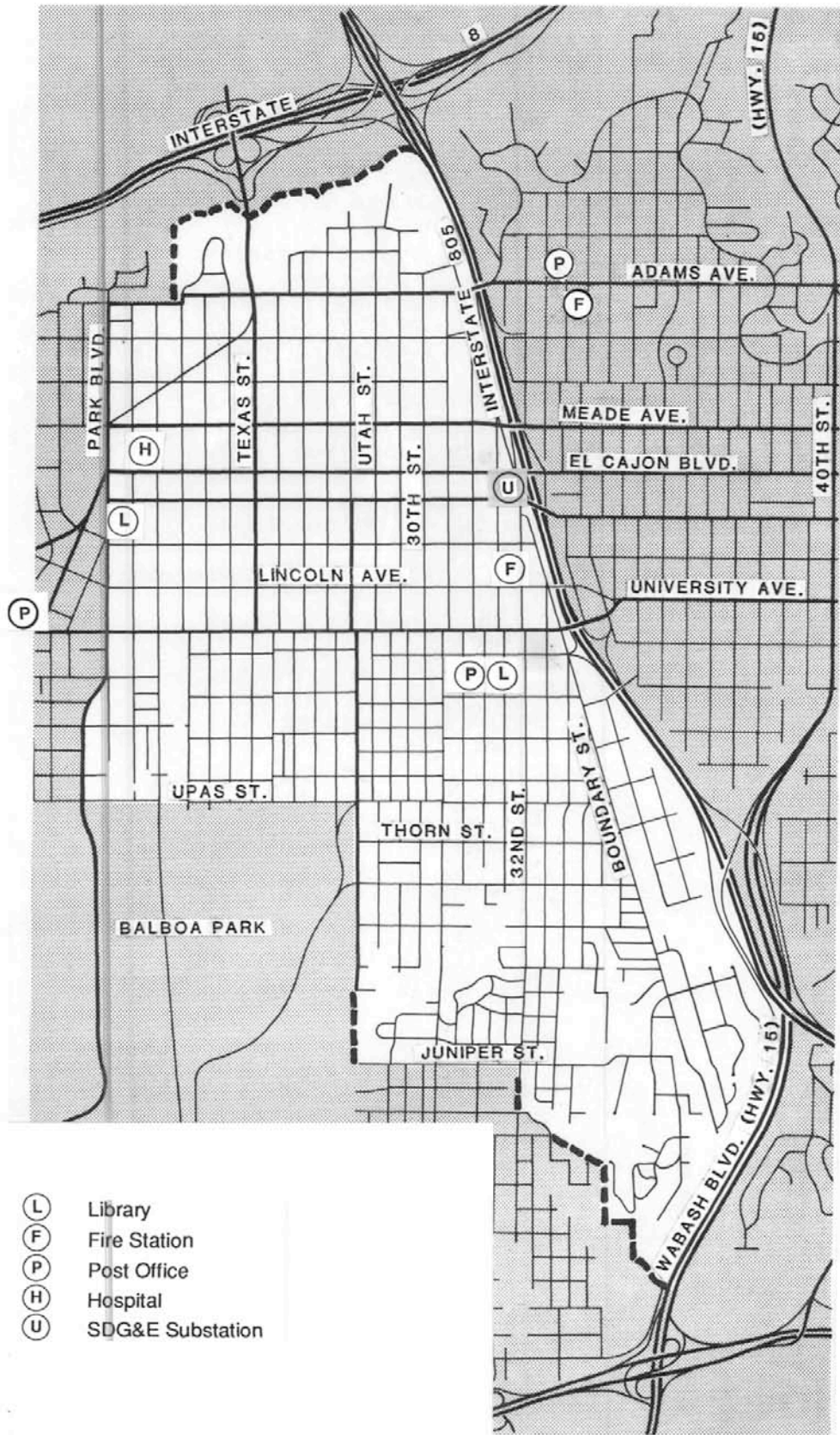
\*All projected enrollments represent preliminary data which have not been subjected to the regular formal procedures undertaken during each year's official enrollment preparation.

### Police

The area north of Upas Street is served by the Western Area Substation, located in the Morena area northwest of Mission Valley. The southerly portion of the community is served by the Central area substation, located downtown. The Community Relations Office serving the community is located in Linda Vista.

Consideration should be given to establishing a Community Relations Office in Greater North Park. An excellent location would be the vicinity of 30<sup>th</sup> Street and University Avenue, which is the hub of the community. It is both the commercial core and the potential public transit center of the community.

Like most of San Diego, Greater North Park has experienced increases in both violent and nonviolent crimes. In recent years, residential burglaries in increasing frequency have stimulated the establishment of numerous Neighborhood Watch programs. The police department considers these programs to be the most effective means of reducing crime in any given area. This Plan's **Urban Design Element** also addresses some features of development which can improve natural surveillance of properties, including lighting, the careful use of walls and landscaping.



- (L) Library
- (F) Fire Station
- (P) Post Office
- (H) Hospital
- (U) SDG&E Substation



**Community Facilities**  
 Greater North Park Community Plan

**18**  
 FIGURE

## **Fire**

Fire protection for the community is provided primarily by four fire stations (see **Figure 18**). All of the stations have average response times of less than six minutes for their engine companies. The six-minute response time is the Fire Department's guideline for responses for residential areas. It is anticipated that the six-minute response time will still be viable in the face of density increases within the community since response times are a function of station location and not development intensity.

Station 14 is located within Greater North Park at 32<sup>nd</sup> Street and Lincoln Avenue and provides fire protection for the majority of the community. This station had a 3.9-minute average response time in 1985. This station has one engine company (four firefighters). It is scheduled for reconstruction in 1988 and 1989. Upon completion, it will house one division chief, one engine company (four firefighters) and one aerial ladder truck company (four firefighters).

Station 18 in Normal Heights services the northern end of the community. It had an average response time of 5.1 minutes in 1985. It is scheduled for reconstruction in Fiscal Years 1986 and 1987. Once completed, it will house an engine company (four firefighters), a paramedic unit (two paramedics) and a hazardous material response team (three personnel).

Station 11 in Golden Hill serves the southern end of the community. This station has one engine company (four firefighters) and a truck company (four firefighters). In 1985, the station had an average response time of 3.8 minutes for the engine company and 5.5 minutes for the truck company. It is scheduled for reconstruction in Fiscal Years 1990 and 1991. Upon completion, it will continue to house one engine company and a truck company.

Station 5 in Hillcrest serves the western portion of the community. The station has an engine company (four firefighters) and a truck company (four firefighters). In 1985, the station had an average response time of 4.1 minutes for the engine company and 6.6 minutes for the truck company.

## **Paramedics**

Paramedic service is provided throughout the City of San Diego through a contractual arrangement with a private ambulance provider. Greater North Park is currently serviced by paramedic units based at the company administrative offices at 47<sup>th</sup> Street and El Cajon Boulevard, at Mercy Hospital in the Hillcrest area and Physicians and Surgeons Hospital in the Southeast area.

## **Hospitals**

Hillside Hospital, which has emergency facilities, is located within the community, on El Cajon Boulevard, just east of Park Boulevard. Mercy Hospital and University Hospital (University of California Medical Center) are located to the west in the Uptown community (see **Figure 18**).

## **Post Office**

A branch post office is located in close proximity to the 30<sup>th</sup> and University commercial center at the corner of Grim Avenue and North Park Way (see **Figure 18**). However, the post office is limited in size and should either be expanded or relocated to a site where a larger facility could be provided. If the post office is to be retained on the existing site, any expansion might be coordinated with the expansion of the North Park Library, with emphasis being placed on enhancing pedestrian circulation between the post office, the library and University Avenue (the Central Business District).

## **Libraries**

Library service is provided by the North Park Library, located at 31<sup>st</sup> Street and North Park Way, and by the University Heights Library at Park Boulevard and Howard Avenue. These libraries offer 31,900 volumes and 22,000 volumes respectively. Both branches are open six days a week. The University Heights branch has 3,749 square feet in usable floor area. The North Park branch is scheduled for future expansion from 3,560 square feet to 8,000 square feet (see **Figure 18**).

Branch libraries are intended to serve about 30,000 residents and should have a maximum service area of a radius of two miles. The two branch libraries generally meet these criteria. In addition, they are ideally located to accommodate the areas of greatest projected growth in Greater North Park. Branch libraries should have an eventual capacity of 4.4 volumes per square foot of floor area. Both branches currently exceed this standard.

## **Water and Sewer Service**

The capacity of water mains and sewer lines is considered generally adequate throughout the community. However, the advanced age of the systems necessitates frequent repairs and replacements. The City of San Diego has a sewer and water main replacement program. These programs are funded annually on a citywide basis with four, five and six million dollars in fiscal years 1986, 1987 and thereafter, respectively, for sewer main replacement; and five and six million dollars in fiscal year 1986 and thereafter, respectively, for water main replacement.

Monies in these two programs are being used to replace sewer and water mains with higher than the citywide average water break/sewer stoppage frequencies and to provide adequate capacities to meet demand according to zoning and the community plan. In replacing and upgrading water and sewer lines, the population densities permitted by the community plan or by existing zoning, whichever is higher, is a determining factor. As a general rule, the ability of the water and sewer systems to serve the community will exceed the ability of the street system to accommodate vehicular traffic generated by the community. In addition, the primary factor in determining water system capacities is fire fighting flow demand which typically exceeds peak hour use demand.

On a citywide basis, replacement priorities are also based upon the history of sewer stoppages and spills and low water pressure, with areas having the worst problems being given top priority. Typically, this means that older communities will receive more emphasis on upgrading and replacement because the age of their facilities causes those facilities to be more prone to breakdown. Also, in older communities, the age of the service pipes rather than the size is the problem because older pipes have a reduced capacity due to interior deterioration.

### **Gas, Electricity and Telephone**

Gas and electricity service are provided by San Diego Gas and Electric Company. Telephone service is provided by Pacific Telephone Company. These services are considered satisfactory at this time.

The undergrounding of overhead distribution utility wires on four major streets is scheduled in the City's Capital Improvements Program. These include all of Adams Avenue and University Avenue, and both Park Boulevard and 30<sup>th</sup> Street north of University Avenue. Phased construction is scheduled for 1983 through 1988, and is funded by San Diego Gas & Electric Company.

### **Childcare Center**

A community childcare center should be provided within the community. Such a facility could be publicly owned, but operated by a private operator. The Garfield School site, if it becomes available, would be an appropriate location.

### **IMPLEMENTATION PROGRAM**

1. Provide ongoing community input to the San Diego Unified School District regarding any necessary upgrading and expansion of existing educational facilities.
2. Upgrade or replace obsolete or inadequate community facilities as programmed in the Capital Improvement Program.

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## PARK AND RECREATION

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### GOAL

Ensure adequate park and recreational facilities and activities easily accessible to all portions of the community.

### EXISTING CONDITIONS

The General Plan speaks to older communities such as Greater North Park as follows:

“In older, already developed parts of the City, where recreation space is difficult to acquire, efforts should be directed toward providing staff and facilities which compensate for deficiencies in acreage. Land, equipment, and supervision in varying proportions can still add up to recreational opportunity and service to the residents. If acreage is reduced, facility investment and leadership should be correspondingly increased.”

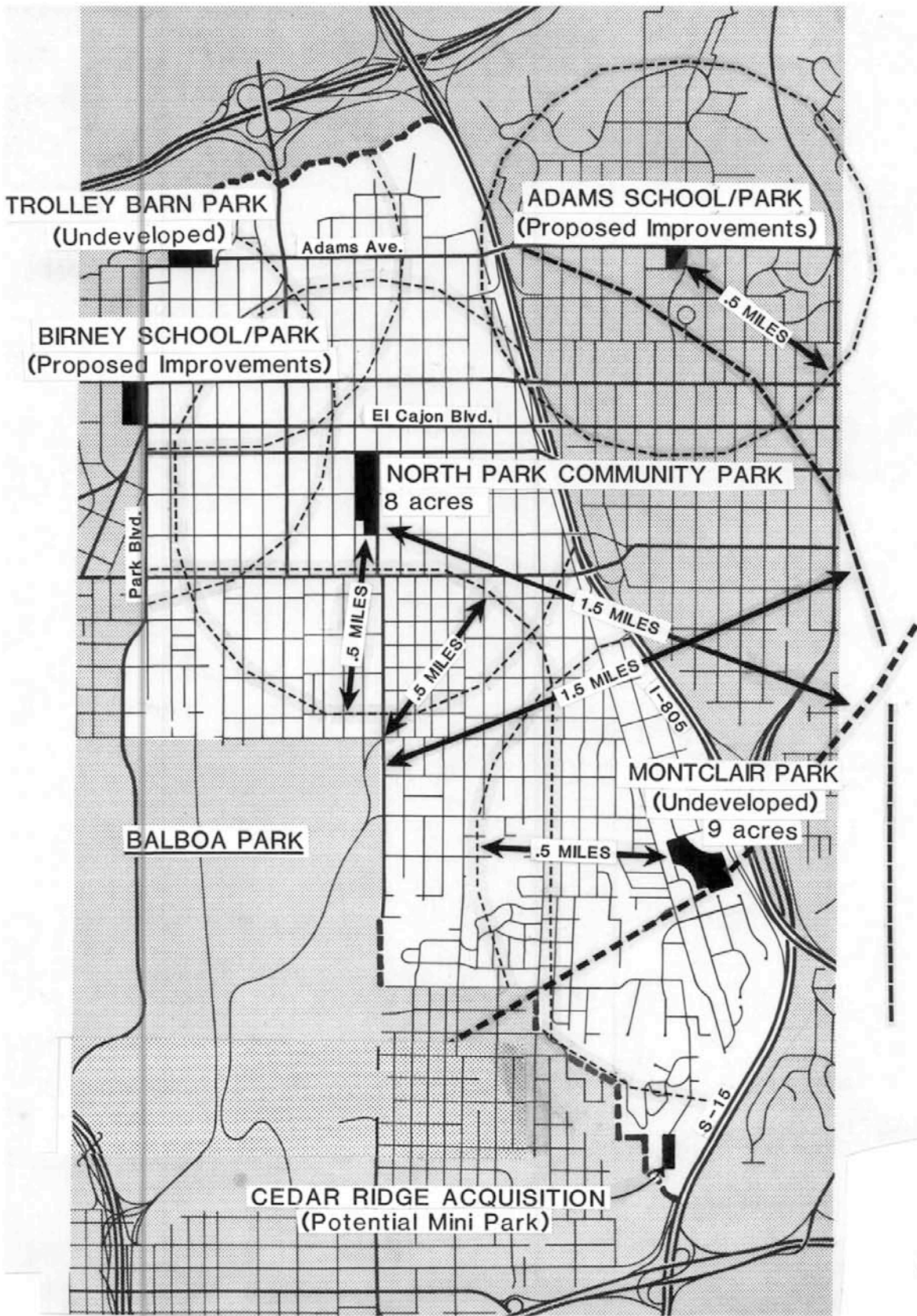
General Plan park criteria are difficult to achieve due to the lack of available vacant land and the prohibitive cost of buying and clearing developed land in locations where the need is the greatest. In addition, acquisition of developed land for park expansion purposes generally means the removal of housing stock and the displacement of the residents of that housing stock.

North Park Recreation Center is the only developed community or neighborhood park in Greater North Park. It is an eight-acre community park providing lighted baseball fields and tennis courts, shuffleboard, picnic and play areas and a recreation building (see **Figure 19**).

Montclair Park is an undeveloped neighborhood park of approximately nine acres, including “paper” streets, located south of Quince Street and next to I-805. Development of this park is planned for 1990. However, due to the community’s lack of adequate parks, consideration should be given to accelerating the development of this park. Paper streets within the park site should be vacated as part of the park development program (see **Figure 19**). The park should be developed as a passive neighborhood park featuring play areas, picnic facilities, landscaping and lawn areas.

Balboa Park is a regional facility which also provides recreational opportunities to neighboring communities. Portions of the park which border Greater North Park offer tennis courts, shuffleboard, a bicycle track, golf course and other forms of active and passive recreation. Most of these facilities are found at the Morley Field recreation area in the northeast corner of the park. The Birney Elementary School Park, located on Park Boulevard at Meade Avenue in the Uptown community, also serves the Greater North Park community. It is scheduled for improvements in fiscal year 1992 with the provision of a turf athletic area, reconstruction of the existing lighted athletic field with attendant facilities and the construction of a comfort station.





Existing and Proposed Park  
Greater North Park Community Plan

**19**  
FIGURE



While also not within the Greater North Park community, the old Trolley Barn site on Adams Avenue has been acquired by the City and is scheduled for development as a neighborhood park in fiscal year 1988. This facility would serve the residents of the northwest portion of Greater North Park (see **Figure 19**). Development of this park could be coordinated with the Park and Adams neighborhood commercial center by means of a common visual theme involving landscaping, ornamental lighting and clearly defined pedestrian connections.

## **OBJECTIVES**

- Provide, to the extent feasible given existing constraints, a system of parks and recreational facilities within the community consistent with General Plan standards.
- Continue enhancement of the North Park Recreation Center as the central facility and focal point serving Greater North Park's active recreation needs.
- Enhance public space adjacent to community facilities, such as post office, libraries, schools and churches, to serve as a focus for passive recreation.
- Establish smaller neighborhood parks and mini-parks throughout the community, especially in areas more distant from larger park facilities.
- Encourage development of public park areas in commercial districts, particularly in areas with high pedestrian activity, to provide visual relief and complementary activity areas.
- Require the provision of private recreational facilities in conjunction with new larger residential projects.
- Provide or maintain adequate access to Balboa Park and its facilities for Greater North Park residents.
- Minimize any adverse impacts from regional visitors to Balboa Park on the Greater North Park community.

## **RECOMMENDATIONS**

Normally, Greater North Park, exclusive of Balboa Park, would be considered to be lacking in adequate park and recreation facilities. Based upon its projected population, the community should have at least two community parks of 20 usable acres each and nine neighborhood parks of ten acres each. These figures are based upon standards estimated by the General Plan of the City of San Diego.

However, the existence and proximity of Balboa Park must be taken into consideration since most of Greater North Park is within the one and one-half mile radius established by the General Plan as the standard for community parks. Therefore, due to the proximity to Balboa Park, the community should not be considered to be deficient in community parks and only deficient in neighborhood parks from an acreage standpoint, since most of the community is within one-half-mile walking radius of an existing or proposed park. Based upon General Plan standards, Greater North Park currently has a 21-acre deficiency in park acreage. After

the completion of the Cedar Ridge mini-park, the community will still be deficient by 17-18 acres. In addition, the community’s sole community park (North Park Recreation Center) is 12 acres short of the 20-acre standard for community parks.

There are a number of possibilities which should be considered in seeking improvement to the current park and recreation situation in the community.

Consideration might be given to utilizing all or portions of the Garfield School site as a combined neighborhood park and childcare center. In order to achieve this objective, cooperation of the San Diego Unified School District would be necessary since displacement and relocation of existing educational services would result. In addition, it may become necessary, at some point in the future, for the site to revert back to use as an elementary school. However, in that instance, joint school/park use of the four-acre site should be explored.

The City has recently acquired the Cedar Ridge property at the foot of Pentuckett Avenue. The upland or mesa portion of this property could be developed as a small, passive mini-park of approximately three or four acres.

There should be ongoing cooperation between the City and the School District in efforts to expand existing sites and to maximize the use of existing facilities. Street closings, where feasible, are one method of expanding existing facilities.

Property acquisition, including possible land trades, are another possibility of expanding existing sites. In addition, the sale of other City-owned properties in the community which are not needed or are not suitable for public uses, should be explored as a method of obtaining funds for acquiring properties abutting existing park facilities. **Table 5** lists park facilities within and abutting Greater North Park.

**TABLE 5  
PARK FACILITIES**

<b>Name</b>	<b>Type</b>	<b>Size</b>
Balboa	Regional	—
North Park Recreation Center	Community	8 acres
Montclair	Neighborhood	9 acres
Cedar Ridge	Mini	3-4 acres
Trolley Barn*	Neighborhood	
Adams Avenue*	Community	
Birney School*	Community	

\* Parks located outside the community

Balboa Park, although primarily thought of as a regional facility, does provide neighborhood- and community-oriented recreational opportunities. A Balboa Park Development and Management Plan has been prepared and is currently undergoing environmental review.

Aspects of that plan which will impact Greater North Park include the addition of a new municipal gymnasium at Morley Field. More significant are the recommendations to extend Upas Street across SR-163 and Florida Canyon and to extend 28<sup>th</sup> Street across Switzer Canyon.

“Certain adverse visual effects would be associated with both the 28<sup>th</sup> Street and Upas Street extensions. These effects would be the result of the extension of 28<sup>th</sup> Street across Switzer Canyon and the extension of Upas Street across SR-163 and Florida Canyon. These extensions would require construction of three bridges with extensive grading and landform alteration. These bridges would significantly alter the existing visual environment in the surrounding areas. The 28<sup>th</sup> Street extension would reduce the value of Switzer Canyon as a visual amenity for homes along the canyon edge.” (Preliminary Draft Environmental Impact Report — EQD No. 84-0595).

### **IMPLEMENTATION PROGRAM**

1. Establish and maintain an ongoing cooperative program between the City and the School District to maximize the use of existing recreational facilities and to maximize efforts to expand these facilities.
2. Establish financing programs necessary to upgrade and expand existing recreational facilities.
3. If acreage deficiencies of the community’s park system cannot be specified, then efforts should be made towards providing additional staff and facilities which make up for those deficiencies.
4. Development of the Trolley Barn park site should be coordinated with the rehabilitation of the Park and Adams commercial center.
5. Due to the community’s current deficiency in park acreage, consideration should be given to accelerating the development of Montclair Park.

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## **OPEN SPACE**

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### **GOAL**

Provide an open space system which preserves existing canyons and hillsides throughout the community and reintroduces open space areas throughout the community as redevelopment occurs.

### **EXISTING CONDITIONS**

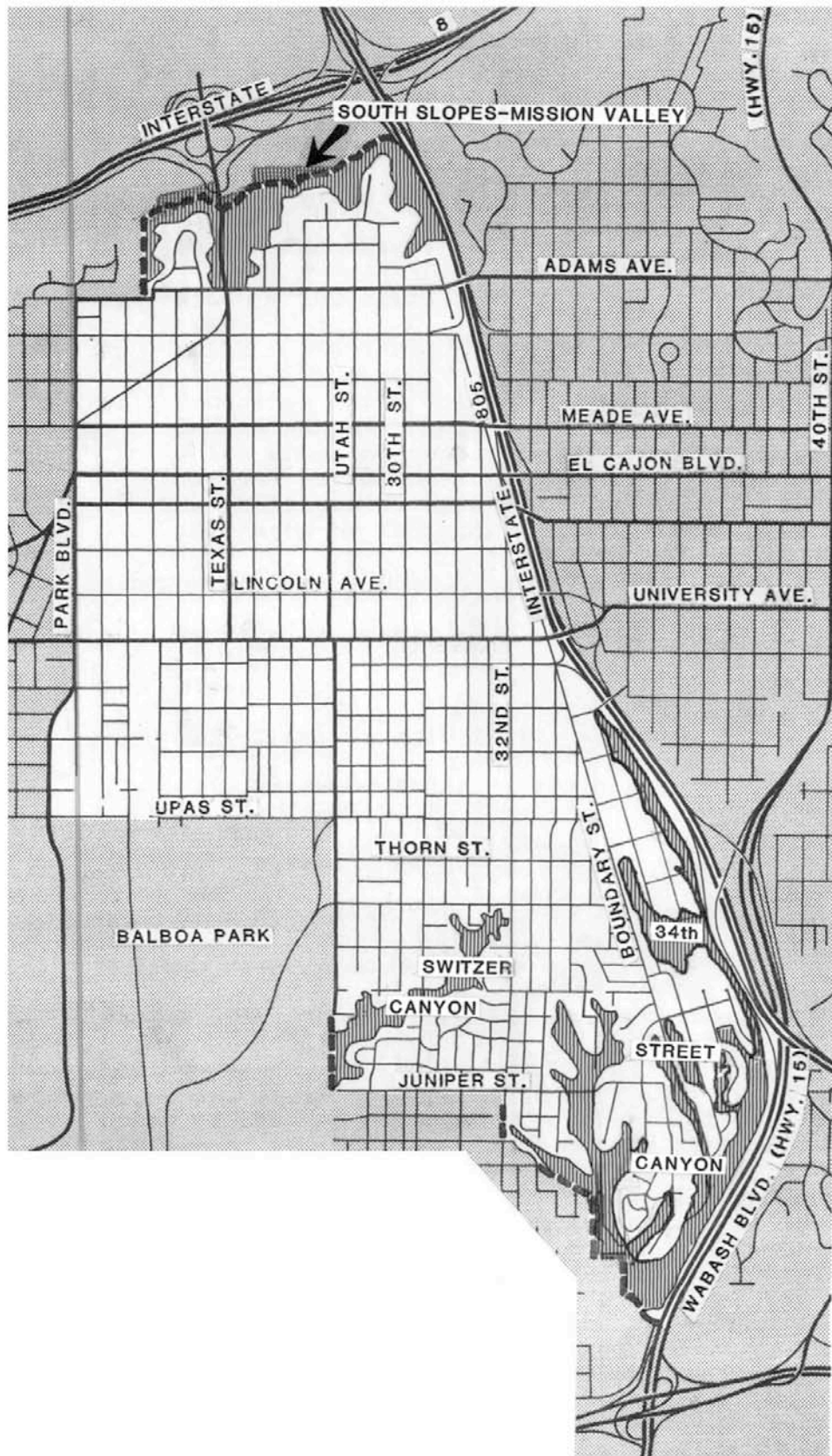
Open space in the Greater North Park community consists of hillsides and canyons. The neighborhoods of University Heights and Normal Heights are bounded to a certain extent by the south slopes of Mission Valley, which are currently designated as open space. Switzer Canyon, in the Burlingame and South Park areas, and the 34<sup>th</sup> Street Canyon, in the southeast section of the community near Greater Golden Hill, complete the open space system. Minor open space acquisitions have been accomplished on the Mission Valley slopes; major acquisitions are in process in the 34<sup>th</sup> Street Canyon; and the acquisition of Switzer Canyon is nearly complete (see **Figure 20**).

There are two forms of open space in Greater North Park. One is generally in slopes remaining after subdivision and development of land where the slopes are mostly undevelopable and owned by individual property owners. It is expected that they will continue to be privately maintained and protected. The other form consists of several large canyons which are unsubdivided or in large lots in single ownerships. These canyons are a notable part of San Diego's beauty and heritage. Every effort must be made to preserve and protect the integrity of these canyons and systems of canyons. They are among the few remaining wildlife habitats within the urbanized area of the City. Access for study and passive recreation should be allowed, but care must be taken to allow minimal disruption of the integrity of the canyons.

The purpose of this open space element is to ensure that open space conservation policies for the natural open space areas are well coordinated with adjacent land development. It is also the purpose of this element to ensure that the environmental and visual impact of upland development, as it relates to natural open space and public view presentation, is adequately addressed.

### **OBJECTIVES**

- Preserve remaining undeveloped canyons and hillsides as important features of visual open space and community definition.
- Utilize publicly-controlled open space for passive recreation where feasible.
- Develop private, usable open space throughout the community by incorporating landscaped areas into new residential and commercial projects.



Natural and Undeveloped Open Space  
 Greater North Park Community Plan

**20**  
 FIGURE



- Acquire open space through open space easements and dedications or other mechanisms as part of project approvals.
- Coordinate open space conservation policies with adjacent land development.
- Establish requirements as part of development approvals for the rehabilitation of disturbed on-site open space.

### **Hillside Areas**

Hillsides which have significant open space value are the highest priority in terms of preservation. They include any slope of 25 percent gradient or greater and the canyon bottoms. Only very low-residential development density should be allowed on these sites, not to exceed approximately one dwelling unit per acre (R1-40000). **Figure 6**, in the **Housing Element**, should be consulted as to those areas recommended for R1-40000 zoning.

Minimal disturbance of the natural terrain and vegetation should be permitted within the undeveloped portion of this zone, unless required due to the necessity to stabilize other areas of the site. Grading shall be avoided by using appropriate building types, such as split level or custom houses, to avoid the typical grading needed for flat slab construction. Building height shall not exceed 30 feet.

Any graded areas in these areas shall be revegetated with native vegetation to minimize erosion and soil instability and to enhance the undisturbed open space area. However, the use of native fire resistant and non-invasive species should also be encouraged directly adjacent to development.

Grading shall be avoided by using appropriate building types, such as split level or custom houses, thus avoiding the typical grading needed for flat slab construction.

Clustering of development is a technique that can be utilized to preclude or limit intrusions into sensitive areas. However, any project featuring attached dwelling units should be designed to be similar in scale and character with the surrounding neighborhood.

Remaining undeveloped portions of the hillside areas should be preserved through open space easements, open space lot designations, or non-building area easements.

### **Open Space Fire Prevention**

The potential for brush fires peaks from May through October. Little rainfall, combined with summer heat and Santa Ana winds, can create an explosive condition.

The following measures should be implemented by property owners adjacent to open space areas.

### **Short Term**

1. Thin out existing woody or dry vegetation.
  - a. Raise tree branch height - lower shrubs underneath trees.
  - b. Remove branches near structures.
  - c. Thin out shrubs on slopes - remove near trees.
  - d. Remove highly flammable plants.
2. Install irrigation at top of slope to establish a buffer.
3. Plant low-growing drought-tolerant fire retardant plants at top of slope.

### **Mid-Term**

1. Extend buffer and continue thinning.

### **Long-Term**

1. Irrigate top of slope buffer and keep vegetation below 24 inches in height.
2. Continually thin and clear slopes of dry plant material and debris.
3. Maintain and control vegetation near structures.
4. Plant fire retardant or resistant trees and plants in yard areas.

### **IMPLEMENTATION PROGRAM**

1. Continue ongoing open space acquisition program in accordance with the criteria established by the Park and Recreation department.
2. Formulate implementing legislation regulating development in hillside and related areas.
3. Initiate a rezoning program within all designated open space areas utilizing the following guidelines:
  - a. Multifamily zoned properties should be rezoned to an appropriate single-family zone;
  - b. Large parcels should be rezoned to appropriate lower-density single-family zones; and,
  - c. City-owned open space should be rezoned to appropriate open space zones.  
(Note: Reference should be made to the **Housing Element** for specific rezoning recommendations.)



4. For purposes of determining allowable density, any portion of a proposed Planned Residential Development which is designated open space and is in the Hillside Review (HR) Overlay District should be restricted to one dwelling unit per acre.
5. The development in natural canyon areas shall be limited to residential, or other low-intensity open space type uses. This should encourage open space preservation and allow property owners to have some reasonable development rights in the event the City or other government agency is unable to purchase the property.
6. All projects located within a hillside and canyon open space area should be subject to development review for the purpose of preserving the area's habitat and natural characteristics. Project review should be based on each project site's major features.

Refinements and modifications to recommended development regulations for open space and hillside areas may be incorporated into implementing legislation. In addition, it may be necessary to adjust land use and density boundaries in order to properly apply implementing zoning legislation. Finally, the achievability of recommended development intensities and residential densities may be predicated upon the design standards and development regulations of the implementing legislation.

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## **CONSERVATION**

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### **GOAL**

Provide a clean and healthy environment in which to live.

### **OBJECTIVES**

- Minimize and avoid adverse noise impacts by planning for the appropriate placement of high noise generating land uses and by mitigating existing noise impacts, where feasible.
- Encourage water conservation through development and landscaping guidelines.
- Conserve energy by utilizing alternative energy sources and energy-efficient building and site design principles.

### **DISCUSSION**

Conservation and protection of natural resources is becoming an increasingly important aspect of daily life in every community. Air, water, land and energy are resources which must be conserved or protected. Conservation is the planned management, preservation and wise utilization of natural resources. Its purpose is to prevent the wasteful exploitation or destruction of the community's natural resources and adoption of policies for their preservation, development and wise use.

#### **Air Quality**

Monitoring of air quality at the Island Avenue and El Cajon Air Monitoring Stations (the two closest stations) for the years 1977, 1978 and 1979 (and 1980 for Island Avenue) indicate that the California standards for ozone, hydrocarbons and particulates were exceeded while the California standards for nitrogen dioxide and sulfur dioxide were achieved during the most recent monitoring year. The standard for carbon monoxide was exceeded at both air monitoring stations in 1979, but in 1980, the standard was achieved at Island Avenue.

The Park North-East community planning area is located in the San Diego Air Basin/San Diego County which has been classified as a non-attainment area for the pollutants of ozone and particulates; the county is an attainment area for nitrogen dioxide, carbon monoxide and sulfur dioxide. The most significant source of air pollution in the San Diego Air Basin is automobile emissions. There are no known stationary sources in Greater North Park that significantly impact air quality.

#### **Noise**

Air and ground transportation are the predominant noise sources in the Greater North Park community planning area. Traffic volumes on all existing freeways, prime arterials, major streets and many collector streets within the Park North-East generate average noise levels of

65 decibels and greater on adjacent properties. Noise contours for the year ending September 30, 1981 indicate that only the southwestern tip of the community was impacted by average noise levels of 65 and greater because of aircraft approaching Lindbergh Field.

According to the San Diego Plan for Air Transportation prepared for SANDAG, a “comparison of the 1980 and 1985 contours shows that the contour areas are reduced in later years, reflecting changes in aircraft types that will serve Lindbergh Field in the future.” More specifically, “based on estimated fleet replacement rates and manufacturers’ estimates, it is assumed that all carrier aircraft operating from Lindbergh Field will meet Federal Aviation Regulation (FAR Part 36) noise standards by 1985.” As a consequence, the aircraft projected 60 CNEL noise contour for 1995 falls outside the Park North-East community.

## **Energy**

There is general agreement that existing ways of life, urban patterns, transportation facilities, buildings and equipment all reflect a past when energy was abundant and cheap. Many other countries, with living standards equal to ours, use less than half the energy per capita that is consumed in the United States. Apart from savings in transportation, the next most fertile area for improving efficiency is building and development design and land use patterns. It is indisputable that sprawled low-density urban development increases travel distances, street and highway requirements, public utility extensions and public service costs (fire, police, schools) – all of which translate directly into increased energy use. Grouped structures and higher-density development have recognized energy savings. Subdivisions in areas that are hot in summer and cold in winter, or in areas where auto dependence is mandatory, or where cultural and commercial and recreational and employment facilities are lacking, can only result in increased energy use – not only in initial development but also in yearly operation and in the more nebulous energy costs that traffic congestion, waste water and public services demand.

In addition to the location of development, its design can be oriented toward better use of energy. Narrow streets reduce construction energy and materials, and reflected summer heat. Deciduous street trees allow summer shade and winter sun on buildings and streets, and make walking and bicycling more attractive. More extensive walks and bicycle paths reduce auto use. Smaller minimum lot sizes reduce travel, utility and service distances.

Important energy savings can also be realized through energy-conserving site planning and building design techniques and principles. Flexibility in required setbacks allows building to be oriented to maximize sun access and wind for natural heating and cooling factors. Designs that consider micro-climates, building efficiency, summer shade and winter exposure of windows and the energy implications of colors and materials can reduce total energy operating needs by as much as 50 percent.

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## CULTURAL AND HERITAGE RESOURCES

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### GOAL

Preserve the cultural and heritage resources of Greater North Park.

### EXISTING CONDITIONS

To date, no group or individual has undertaken a comprehensive survey of the cultural and heritage resources of Greater North Park. While the community is known to contain a number of Gill houses, early bungalow courts and excellent examples of California bungalow style houses, the full extent to which these and other cultural and heritage resources exist in Greater North Park is unknown. In addition, Greater North Park is a repository of Art Deco architecture and, together with the Uptown community plan area, contains the largest concentration of Egyptian Revival architecture in the country. **Figure 21** is a map indicating some known potential historic sites and districts.

### OBJECTIVES

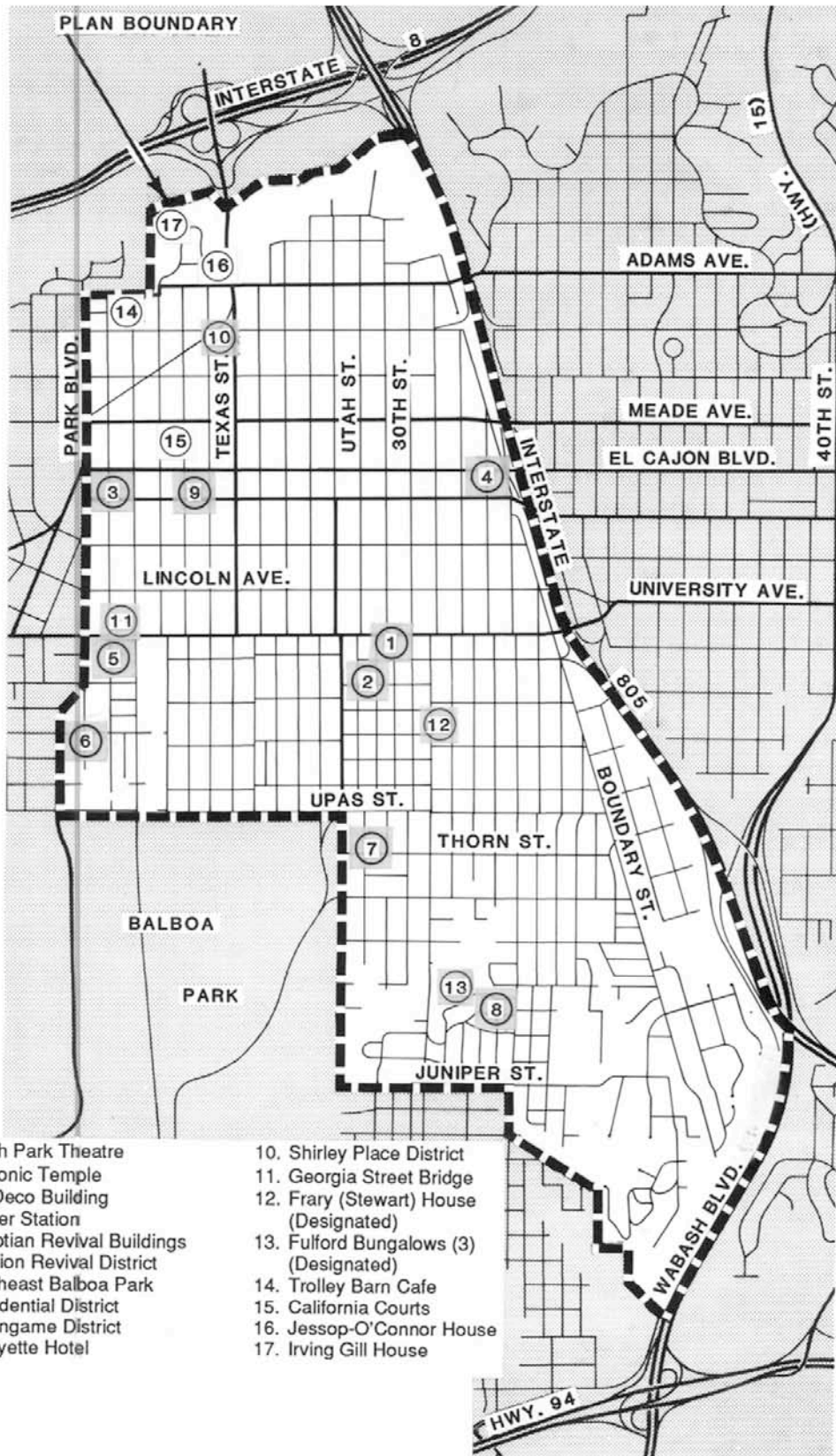
- Undertake a comprehensive historical and architectural survey of the cultural and heritage resources of Greater North Park.
- Establish a list of buildings and neighborhoods for historic designation.
- Establish a program for the identification of potential funding for historic preservation.

It is possible that portions of Greater North Park could be considered for historic district status. Until a comprehensive historical and architectural survey can be undertaken of the community by appropriate organizations or individuals competent in those fields, the full extent of historical and architectural sites in the community will not be known.

The 1981 Economic Recovery Tax Act provides significant tax incentives (investment tax credits) which can be used by the owners of commercial property, which is either non-historic or 30 or 40 years old, or is historic and at least 50 years old. The use of this tax incentive would assist in the revitalization of older commercial areas as well as older apartment buildings.

Any proposed use of federal money for housing rehabilitation, park development, or commercial revitalization should require a review of the project's affect on historic resources. Since no survey has been conducted to identify historic resources, analysis of proposed projects would result in a piecemeal approach to discovery of the community's cultural resources. Therefore, a comprehensive survey or survey program would help to identify sites in advance of specific project planning and assist the community and the City in planning for and programming revitalization projects.

Areas within Greater North Park which should be given consideration for preservation and inclusion in historic districts include, but should not be limited to: the Burlingame neighborhood; Park Boulevard south of Robinson Avenue; and the single-family neighborhood around the northeast sector of Balboa Park.



- |   |  |
|---|--|
| 1. North Park Theatre                         | 10. Shirley Place District             |
| 2. Masonic Temple                             | 11. Georgia Street Bridge              |
| 3. Art Deco Building                          | 12. Frary (Stewart) House (Designated) |
| 4. Power Station                              | 13. Fulford Bungalows (3) (Designated) |
| 5. Egyptian Revival Buildings                 | 14. Trolley Barn Cafe                  |
| 6. Mission Revival District                   | 15. California Courts                  |
| 7. Northeast Balboa Park Residential District | 16. Jessop-O'Connor House              |
| 8. Burlingame District                        | 17. Irving Gill House                  |
| 9. Lafayette Hotel                            |  |



Potential Historic Sites and Areas  
Greater North Park Community Plan

**21**  
FIGURE

## **IMPLEMENTATION PROGRAM**

1. Establish a comprehensive survey program of historically and architecturally significant buildings and neighborhoods.
2. Identify potential funding for historic preservation.
3. Encourage the utilization of the 1981 Economic Recovery Tax Act in the revitalization of older commercial areas.
4. Establish a review process for projects using federal money for housing rehabilitation, park development, or commercial revitalization in order to determine the impacts of individual projects on historic resources.

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## URBAN DESIGN

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### GOAL

Enhance the unique character and community image of Greater North Park.

### EXISTING CONDITIONS

Urban Design is not a physical entity in and of itself which can be isolated and discussed separately from the other elements and functions which make up this community plan. Urban Design deals with all the factors and issues examined elsewhere in the Plan, yet is also concerned with more detailed features such as architectural or site design. This Plan recommends that the scope of community planning should also consider urban design in the continuing development and improvement of the Greater North Park community.

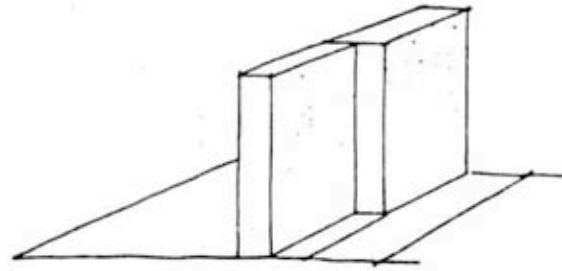
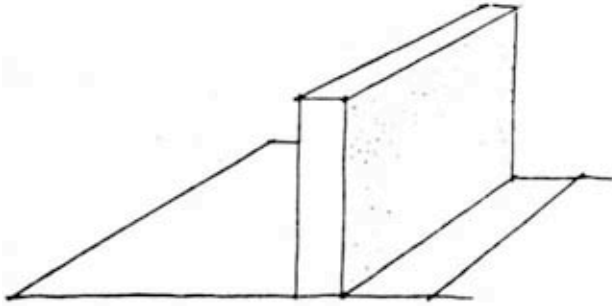
Within the City of San Diego generally, and the Greater North Park community in particular, exist many older neighborhoods, which have evolved memorable individual characters of their own, a character which is modest and small in scale. Greater North Park has many of these neighborhoods and their qualities should be preserved.

Greater North Park is a community of many images. It provides an image of single-family neighborhoods featuring classic California bungalows; it is a community of intense apartment construction intruding into traditional low-density residential areas; it has a definable central business district which is unique in auto-oriented Southern California; and, it is a community of landmarks, the water tower which can be seen from great distances, the Lafayette Hotel with its classic appearance and the North Park Theatre and other visually pleasing buildings.

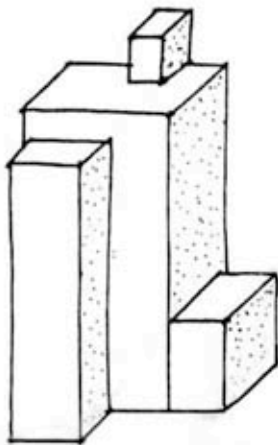
Many Greater North Park neighborhoods still have an intimate scale, with mature trees and quiet streets. Many single-family homes have survived and present a residential style well worth conserving.

### OBJECTIVES

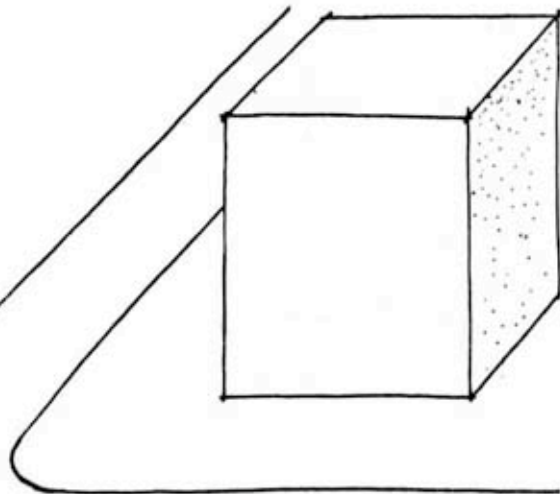
- Ensure that development in the community conforms with the Greater North Park Community Plan **Urban Design Element**.
- Preserve the architectural variety and residential character of Greater North Park.
- Maintain the existing open space edges of the community and develop projects which create a sense of arrival at major community gateways.
- Develop a varied urban character within the community.
- Ensure that new buildings are in character and scale with their neighborhoods.



Example: A wall effect can be avoided by establishing a maximum dimension of wall planes in relation to the lot line.



Example: Differences in the heights and depths of various portions of the building divide the mass into distinct elements, and create a transition between low- and mid-rise development.



Example: **Uniform** heights and depths bring attention to the building's total size.



- Enhance the appearance of major streets through the design of new development, public improvements and landscaping.
- Preserve existing street trees and increase the quality of landscaping in public right-of-way and front yard areas.
- Eliminate visual clutter, including nonconforming signs and overhead utility lines.
- Preserve and restore unique or historic structures within the community.
- Maintain the visual interface between Balboa Park and the community.
- Create neighborhood identities through coordinated street plantings.
- Establish development and design guidelines which will assist in reducing crime in the community.

## **URBAN DESIGN GUIDELINES**

### **Natural Environment-Public Views**

- Structures should be designed to protect public views of Greater North Park's natural scenic amenities, especially Mission Valley, Balboa Park and the Switzer and 32<sup>nd</sup> Street canyons.
- Where existing streets serve as public access and public view corridors of the valley, park and canyons, development on corner lots requires special design considerations. In order to maximize public views, new development should be set back from the corner or terraced away from the street.

### **Building Scale**

Building scale is a quality that describes the relationship of buildings to each other and to human dimensions. Much of the residential development in North Park is older and single-story, consisting of single-family bungalows, and low-scale multifamily development including bungalow courts. However, over recent years, higher-density development, out of scale with the older development has been introduced into the community causing impacts on the visual environment. It is important that new residential development be designed to blend into the scale and visual environment of the community.

The majority of existing commercial development within the community is old and characterized by small, low-scale, one- and two-story structures. New buildings of greater heights could be out of scale with this established character. It is therefore important that new development complement the scale of existing development to avoid abrupt and negative changes to the established development character. In some instances, land use designations will permit higher densities than is the norm. It is intended that new development, while not being forced to comply with the scale of existing development where the community plan designates higher development intensities, should not ignore existing scale and should provide good design relationships with adjacent development.

- New development should be consistent with the scale and character of the existing development of the surrounding areas. The fitting in of new development is, in a broad sense, a matter of scale. It requires a careful assessment of each building site in terms of the size and texture of its surroundings, and every conscious attempt to achieve balance and compatibility in design between old and new buildings.
  - Relate the height of new buildings to the height of existing development. Exceptionally tall buildings immediately adjacent to low buildings can create problems such as excessive shadows, undesirable wind tunnels and lack of privacy.
  - Abrupt differences in scale between large commercial buildings and adjacent residential areas should be avoided. Gradual transitions in scale are preferred.
  - Extreme contrasts in color, shape and organization of architectural elements will cause new buildings to stand out in excess of their public importance. Where visually strong buildings clash with their surroundings, the character of the area will be adversely affected.
- Harmony should be promoted in the visual relationships and transitions between new and older buildings. New buildings should be made sympathetic to the scale, form and texture of surrounding development. Where new buildings reach exceptional height and bulk in comparison to existing structures, large surfaces should be articulated and textured to reduce their apparent size and to reflect the pattern of the surrounding development. Multifamily buildings should be designed and evaluated in the context of surrounding development.
  - Building bulk should be controlled through the use of vertical and horizontal offsets and other architectural features (balconies, porches, bay windows) which serve to break up building facades.
  - Building surfaces should be articulated and textured to reduce their apparent size.
  - Buildings can be textured by using different materials, colors, or facades to produce separate elements.
  - Treat upper floors so that they actually, or seem to, set back and recede. Structures of over two stories in particular should incorporate this treatment.
  - Smaller architectural elements: windows, cornices and other details should not be much larger than they would be on a single-family house.
  - Taller portions (multiple stories) of buildings should be set back from the required front setbacks. This would control building scale by emphasizing one- and two-story facades along street frontages.
  - Landscaping can also be used to add texture to blank walls, soften edges and provide a sense of pedestrian scale.

- Features such as entryways, side notches, partial facade setbacks, entry porches, bay windows, window proportions and other small-scale geometric forms set up strong visual rhythms. Where these elements are a part of a block's character, new development should incorporate them to preserve block unity and to fit in harmoniously.
- Structures should conform to the spacing pattern and rhythm of spacing of buildings already existing within the block.
- The clustering of dwelling units in the single-family designated areas (residential areas 1 through 4) through a Planned Residential Development permit should only be approved if the proposed projects create dwelling units which strongly resemble the scale and character of the surrounding development. It is acknowledged that this approach may often result in substantially fewer dwelling units than the maximum allowed by the underlying R-1 (single-family) zones.
- Parcel accumulation will be permitted, and even encouraged, in some residential areas of Greater North Park in order to provide for increased residential densities. However, care must be taken in the formulation of the regulations permitting parcel accumulation density increases (bonuses) so as not to permit development that is out of scale with the surrounding neighborhood. New development should blend into the visual environment of the neighborhood unless extensive redevelopment is anticipated to occur within the neighborhood.
  - Where parcel accumulation takes place, consideration should be given to adjacent parcels which have lower scale development of a long-term nature. For instance, a three-story apartment building with sideyard walls featuring an unbroken plane can visually overwhelm a neighboring bungalow or single-story apartment development. This impact can be avoided by the use of regulations which require adequate setbacks, diagonal building planes and roof articulation.
- Parcel consolidation should be prohibited in residential Areas 5, 6, 7 and 8 to preclude large clustered developments which are not compatible with the scale and character of these areas.

### **Architectural Detailing**

- Flat roof surfaces should be considered for use as terraces, with limited landscaping if it is structurally and economically feasible.
- Visually distracting roof appendages such as stairway towers or ventilation equipment should be screened from public view.
- Buildings with a roof form or profile similar to surrounding buildings should be required to strengthen the visual identity of the structures and contribute to a street's visual harmony.

- Commercial buildings should present open facades to the street. This adds to the pedestrian interest along the street and also provides a security factor based on visual openness between the street and the building. This is particularly important for an area like the 30<sup>th</sup> and University central business district has a pedestrian orientation, which should be reemphasized and enhanced.

## **Parking**

- The automobile or parking facilities should not be a dominant element of the neighborhood character. On-site parking should be screened or located in areas not highly visible from the street.
  - On-site parking should be underground or located in the rear of buildings and accessed from the rear alley whenever feasible. Also, parking spaces should be linked through various properties in order to provide better and more efficient parking areas. Efforts to consolidate parking through the use of common alley spaces, landscaped parking lots or parking garages should be encouraged.
  - Curb cuts and front driveways should be minimized. Driveways should be restricted in width within required front yard setbacks. Curb cuts and drive-throughs are not appropriate on pedestrian-oriented streets such as portions of University Avenue.
- Large surfaced parking areas should be broken up with landscaped islands and screened from view by landscaping. This can be accomplished through the use of trees, shrubs, mounding or walls appropriate to the character of the area. Large parking areas should also include patterned pavings as a means to enhance surface areas.
- The width of garages should be adequate for maneuvering. Two-car garages should be designated to accommodate standard-size cars.
- In many instances, required parking spaces are utilized to accommodate trash receptacles. Provisions should be established which will preclude this practice and will provide for refuse collection areas in a manner that will both retain required parking and cut down on litter on the streets and alleys.

## **Streetscape and Parkways**

- At key locations, specimen trees can become community focal points. Such trees should be designated as community resources and protected.
  - New development should be sited and designed to mitigate any harmful impacts to major trees or other significant resources.
- Where feasible, tree massing should be carried into adjacent public and private spaces.
- Shrub buffers should be provided and/or maintained on busy streets to protect pedestrians.

- Streets should be designed and developed to be pleasant places to walk down as well as drive upon. The arrangement of houses should create a pleasant streetscape. Attention to building siting, paving, landscaping and tree plantings should all be designed to enhance the visual effect.
  - Excessive use of concrete in front yards shall be eliminated.
  - The citywide landscaping ordinance should be utilized to enhance the streetscape by increasing the amount of landscaping in front yard areas.
  - Landscaping should be used to establish a linkage between the street and the buildings along the street. It should also be used to “tie the street together.” This can be done through the use of tall street trees that lend identity or “an image” to the street and also help to soften parking areas. Different species of trees can be planted along individual streets in order to create individual and identifying images for different streets.
- The facades of buildings at pedestrian levels should provide a close relationship between the building and street. Blank walls are considered detrimental to this relationship.
- The existing condition of many commercial streets is the result of many years of neglect and a failure to upgrade the street environment as the need arose.
  - There is noticeable sign clutter. This situation could be improved through enforcement of existing regulations.
  - Many of the streets have cluttered sidewalks. This is usually the result of a combination of narrow sidewalks and poorly located street furniture. A combination of sidewalk widening and more appropriate locational criteria for street furniture should be able to resolve this problem.
  - Most of the commercial streets have little or no landscaping. The addition of landscaping, particularly trees, can enhance the visual environment, provide shade and soften the effect of the pavement inherent in a commercial area.
- In commercial areas, the linkage between the street can best be established by keeping the buildings close to the street in order to enhance the pedestrian environment of the street. This adds interest to the street by permitting “window shopping” and encouraging merchants to do on-premises advertising through the use of window displays. This is particularly important in the central business district where the pedestrian environment should be enhanced. Therefore, mini-shopping centers and other forms of commercial development which place parking areas along the sidewalk and feature buildings with deep setbacks should be discouraged.
- Trees, lighting bollards or sidewalk textures should be encouraged along major streets to separate pedestrian paths from traffic conflicts.

- Recreational values of paths should be increased by providing rest stops and focal points of activity. The use of interest nodes; a fountain, chain of shops, or plaza space, at reasonable walking distance, can help entice pedestrian movement from one point to another.
- Pedestrian paths that provide a series of views and a consistent variety of scenes should be encouraged. The discovery of interesting places along pedestrian paths contributes to the existence of serendipity and surprise.
  - Storefront diversity adds to pedestrian accessibility, variety and choice along commercial paths.
  - Clusters of shops around interior courtyards, or pedestrian malls extend the commercial frontage of the area and increase the diversity of the street.
  - Window displays stimulate interest along paths and encourage pedestrians to stop and browse.
- Various elements of climate control improve the pathway for the pedestrian and add color, visual variety and liveliness to the streetscape. Such elements include coverings, arbors, canopies, awnings, colonnades and arcades. Street trees providing broad canopies can also be used to create a sense of pedestrian scale adjacent to large buildings.
- Richly detailed facades, windows, entrances, stairways, retaining walls and other features are appreciated by the pedestrian and impart a sense of human scale.
- Various textures and paving treatments can be used to define and separate pedestrian paths. The coarseness of surface textures should not be so great as to create difficulty in walking.
  - Curb cuts and driveway access paths create conflicts between auto and pedestrian use. Where such conflicts cannot be avoided, the visual continuity of the pedestrian path can be maintained by continuing the pavement pattern across the driveway space. On commercial property, speed bumps and stop signs should be used to reduce hazards at blind auto egress points.
  - Pavement textures can also be used at street crossings to clearly mark and improve the appearance of the intersection. However, care should be taken to ensure that pavement textures indicating pedestrian crossings are only used in situations where proper traffic controls are in place so as not to create unsafe pedestrian crossings.
  - Although pedestrian activity should be concentrated along the streets, mid-block pedestrian paths (along the alleys) should be provided whenever feasible to link the activity centers of the main commercial street to adjacent large development across the alley areas.
- Fencing or landscaping that is provided as either a screen for open space or for private yards should avoid “blocking off” the building from the sidewalk.

## Landscaping

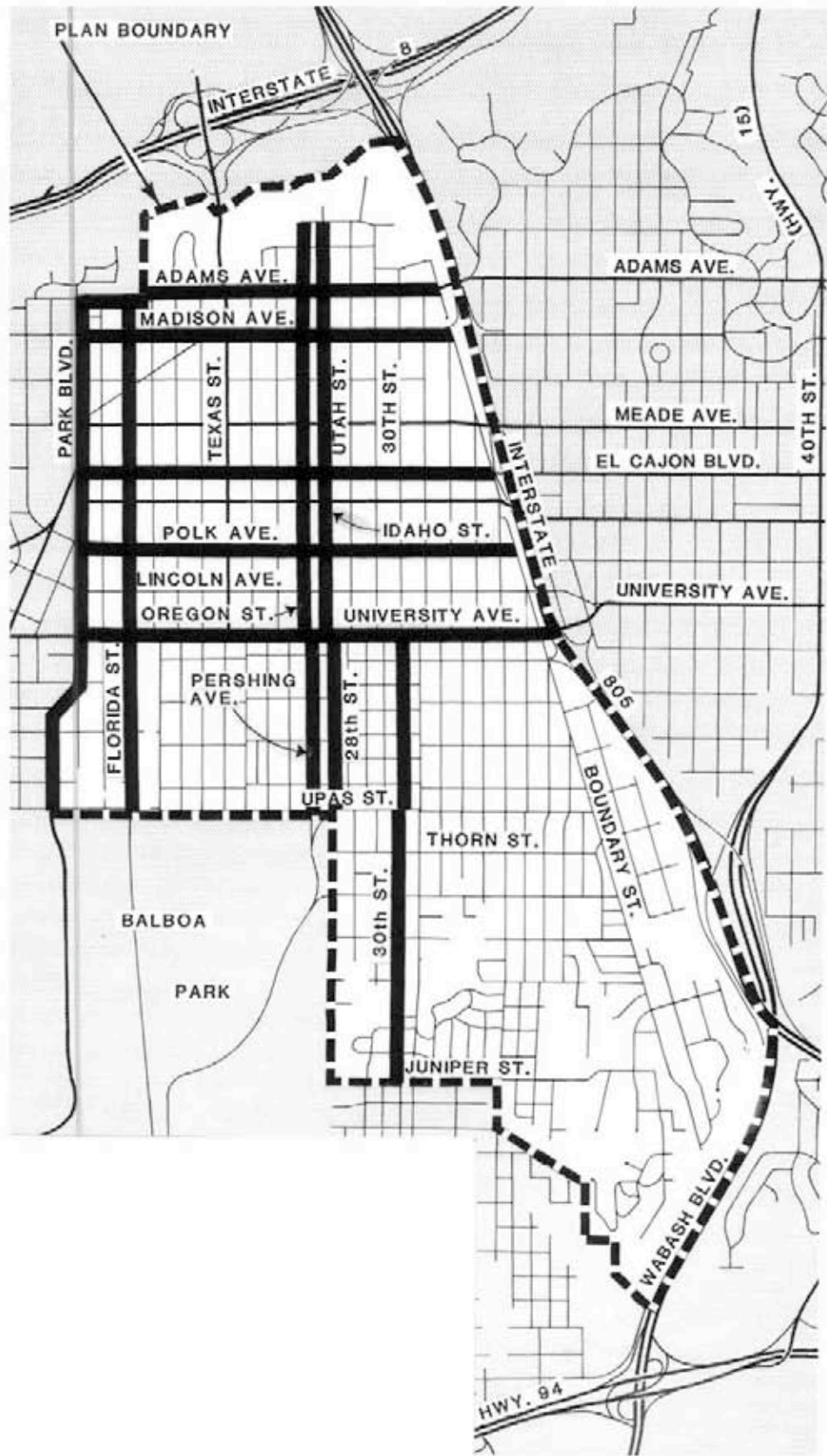
The use of appropriate landscaping materials and careful consideration of siting factors help to maintain a sense of nature in the urban environment. Landscaping exerts a major influence on the character of individual streets.

- Extensive tree plantings should be provided or preserved to enhance the visual quality of public streets and provide a strong element of continuity as future development occurs.
- Landscaping themes for all major streets in the community should be studied, and specific plans should be developed. Priority should be given to improving landscaping in the commercial areas as part of individual permit processing. Such plans should be reviewed by the community planning group before any implementing actions are taken.
- In areas of little or no activity, groundcovers or lawns should be planted as an alternative to paving.
- At key locations, specimen trees can become community focal points. Such trees should be designated as community resources and protected.
  - New development should be sited and designed to mitigate any harmful impacts to major trees or other significant resources.
- Where feasible, tree massing should be carried into adjacent public and private spaces.
- Shrub buffers should be provided and/or maintained on busy streets to protect pedestrians.
- Mature street trees should be preserved and replaced when removal is unavoidable. Street trees, particularly when an individual species dominates, become an important element of a neighborhood and streetscape character.

Standards requiring minimum percentages of landscaping and limiting the amount of concrete areas, particularly in residential areas, should be established and incorporated into implementing legislation.

Landscaping programs should be established along selected streets throughout the community. These “parkways” have been selected because they either enhance the visual appearance of major important thoroughfares which enhance the visual image of the community or because they provide pleasant landscaped linkages between public places.

Park Boulevard, El Cajon Boulevard, University Avenue, Adams Avenue and portions of 30<sup>th</sup> Street are streets which can enhance the visual appearance of the community through landscaping programs. El Cajon Boulevard already presents a “landscaped appearance” and efforts should be made to enhance it, especially within the median. In order to properly enhance El Cajon Boulevard’s medians through landscaping, the landscaping program should be designed in conjunction with the provision of left turn pockets for vehicular traffic.



**Proposed Landscaped Parkways**  
 Greater North Park Community Plan

**22**  
 FIGURE





These pockets should be placed only at selected intersections so as to permit maximum landscaping opportunities in the medians. Adams Avenue is an important link through the community between University Heights and Normal Heights. Park Boulevard is a major entry into Balboa Park. Thirtieth Street, south of University Avenue, is the only southern route into and out of the community.

In the latter category, Florida Street is a direct link between Balboa Park and the future Trolley Barn Park north of Adams Avenue. Oregon Street/Pershing Drive and Idaho Street/28<sup>th</sup> Street connect Balboa Park with the south rim of Mission Valley via the North Park Recreation Center. Polk Street provides east-west access to the recreation center from the eastern and western edges of the community. These parkways are shown in **Figure 22**.

In order to ensure the visual enhancement of these parkways, special design and development regulations will be formulated as part of the legislative implementation program.

### **Places**

Pedestrian places are extensions of personal space. A place is formed when people establish boundaries and find recurring uses for a space. Activities are the focus of a space.

- Buildings should not dominate surrounding spaces but should step back or form enclosures in scale with pedestrians.
- Changes of level are an effective way to create interest and define pedestrian space.
  - Raised places give a sense of overlook and advantage to its occupants.
  - Lowered spaces create a sense of intimacy and enclosure.
  - Extreme changes in level impair the visual connection between the occupant and passerby, reducing interest for both.
- Spaces should have furnishings that enable multiple use by all age groups.
- The location of benches and their relationship to one another is of prime importance.
  - Benches should be arranged to promote contact and form interesting spaces to watch pedestrian activity.
  - Views from fixed benches should not be obscured by walls, railing, poles, landscaping or other unnecessary obstructions.

### **Transition Areas**

Along the south sides of Meade Avenue, south of residential Area 8, tailored zoning should be developed in such a manner, as to ease the transition between the differing densities of Areas 8 and 17 to the extent possible. Similar tailored zoning should also be investigated where there are similar differences or transitions in densities.

## **Continuity**

- Uses requiring large frontages, such as banks, office buildings, parking lots and supermarkets can disrupt the continuity of a complex of small shops. Large development should be designed to maintain this continuity by creating additional pedestrian and landscaped areas along the street, providing display windows and breaking up large facades with architectural features.
- Residential units should be encouraged on the upper floors of commercial developments. The mixing of residential and commercial facilities provides a more efficient use of parking facilities and reduces dependence on the automobile.
- Many blocks in Greater North Park do not have alleys. This provides an opportunity for development to cross common rear property lines. This, in effect, provides two front yards. However, because of the need to provide off-street parking, one front yard can become a vehicular parking area having adverse impacts on abutting front yards. Development standards precluding this from occurring should be established.
- In many older areas of Greater North Park, front yard setbacks are greater than the minimum front yard setbacks presently required by zoning regulations. Consideration might be given, particularly in lower-density residential areas, to utilizing the older established setbacks as the required setbacks.

## **Density Bonus Areas**

In certain specified areas, residential development may be increased from the established base density (maximum permitted dwelling units per net residential acre) to a specified bonus density if certain bonus density criteria are met. The above guidelines are recommended minimum standards; the requirements for a specific project may vary based upon individual site and neighborhood characteristics.

Minimum parcel accumulation area, which is the basic qualifier necessary to obtain density bonuses must be formulated during the implementation phase of the planning program. Refinements and additions to the above recommended guidelines may also be incorporated into the implementing legislation.

## **Lighting**

- Light quality should be geared to the specific use of the areas, spaces and forms to be illuminated.
- Lighting should reflect the relative importance of pedestrian spaces by degrees of illumination at night and by the repetition and scale of lighting standards during the day.
- Lamps and standards should be for people, and not cars, wherever possible.
  - The height of standards should vary between ten and 15 feet.
  - Lamps should be placed beside walks, not above the roadway.
  - Bollard-type lighting is suited for pedestrian paths.

- In instances where desired lighting standards are not in conformance with the standards established in Council Policy 600-4, it may be necessary to establish a maintenance district for any added costs of providing the special street lighting.

## **BALBOA PARK**

Balboa Park lies adjacent to Centre City, Greater Golden Hill, Uptown and the community of Greater North Park which it borders on the south and the west.

The park is divided in four major segments by three deep canyons, Cabrillo, Florida and Switzer. In some cases the mesas are so widely separated from one another that it is difficult to associate one mesa with another as being in the same park. This distinct topographic cleavage has been successfully overcome in the western sector by the construction of four bridges over the Cabrillo Canyon. No such connection has been made over Florida and Switzer Canyons.

The north-south division of the park, the Cabrillo and Florida Canyons, has influenced the basic development pattern of the park. The western one-fifth has been extensively landscaped and is devoted primarily to picnicking and various forms of passive recreation including facilities for older persons. The two-fifths of the area in the center of the park, bounded by the Cabrillo and Florida Canyons, contains the major developed areas; the Prado area along Laurel Street is mostly a museum use, the Palisades area south of the Prado contains buildings used in both the 1915 and 1935 Expositions and the Balboa Park Bowl is primarily an indoor recreational use. The San Diego Zoo, the Veterans Building, the San Diego High School, the Roosevelt Junior High School, the Boy and Girl Scout Camp areas and the Naval Hospital are also found in the center of the park. The schools and the hospital lands have been legally removed from the park.

The easternmost two-thirds of Balboa Park has been further divided by Switzer Canyon, which runs in a northeast-southeast direction. The triangular area south of the canyon contains a nine-hole golf course, the Golden Hill picnic area and a community recreation center. North of Switzer Canyon is an 18-hole golf course. In the extreme northeast portion of the park is Morley Field area, a partially developed active recreation area.

Because of Balboa Park's physical relationship to the Greater North Park community, it is important to ensure that the design and development of abutting areas are consistent with the regional resource and design qualities of the park. For this purpose, the following development guidelines are recommended:

1. Development adjoining the parks should maintain and enhance public vistas to the parks, incorporate landscaping motifs and materials consistent with the parks, and incorporate development densities that are consistent with the landform and that preserve the parks and topography. Development should maintain and enhance the traditional character at the perimeter of the parks.
2. Development should maintain the low-density residential character, reflective of the existing development in the area which is primarily single-family. Future development should be consistent with these densities, lot patterns, front yard landscaped areas, street trees and existing early architectural style.

3. Development should maintain an open space character with vegetated courtyards and setbacks. Landscaped/vegetated areas and street trees, within the development site, should relate to the typical vegetation and tree species in the adjacent park area. Also the existing street tree treatment should be continued.

## **SECURITY AGAINST CRIME**

The concept of territoriality and defensible space should be considered in designing public and private improvements. This is accomplished without the building of fortresses, and is successful when a potential offender perceives that he is intruding on the domain of another, and that he will be noticed if he intrudes. He is then more likely to be deterred from criminal behavior. The Neighborhood Watch Program successfully uses this premise.

The physical design of a development project can provide surveillance opportunities of the neighborhood by its residents or of the commercial center by merchants, employees and customers. Greater security against crime is also accomplished by improving the capacity of people to live, work and shop while at the same time casually and continually surveying their environments, both indoors and outdoors.

Therefore, design features of a project, together with the features of the natural environment, should be considered in the review of development proposals and in the preparation of development regulations.

## **COMMERCIAL AREA DESIGN STUDIES**

### **Introduction**

The Mid-City Commercial Revitalization Program was undertaken in the early 1980s in an effort to revitalize the commercial areas in the Mid-City and Greater North Park communities. This program has provided two design studies pertinent to Greater North Park: the North Park Design Study for the community's central business district centered around 30<sup>th</sup> Street and University Avenue; and, the Design Study for the Commercial Revitalization of El Cajon Boulevard which includes that segment of the boulevard between Park Boulevard and I-805 within the Greater North Park community.

These design studies establish design guidelines for the revitalization of these two important commercial areas. It is intended that these studies serve as supplements to the Greater North Park Community Plan and, to the extent feasible, serve as a basis for the formulation of development and design regulations for these areas. The objectives of the two studies are listed below in order to provide a summary of the design guidelines set forth in those studies.

### **North Park Commercial Center Design Study**

The design study for the North Park (30<sup>th</sup> & University Avenue) Commercial Center is an extensive survey and analysis of the area complete with design objectives. It is intended that this study be utilized with this community plan document as a supplement to the Plan. It is therefore summarized below.

### Pedestrian Circulation - Design Objectives

1. Improve quality and appearance of sidewalks, crosswalks, shelters, bus stops, benches and other pedestrian amenities.
2. Improve alleys and rear entries for pedestrian use.
3. Consider a pedestrian mall or plaza.
4. Consider a small-scale public transportation system for North Park.
5. Provide bicycle racks in or near the commercial area.

### Vehicular Circulation - Design Objectives

1. Maintain traffic speed controls through the commercial areas.
2. Consider alternatives to University Avenue for through east-west traffic.
3. Provide ideas for a small-scale public transportation system.
4. Consider a parking structure or removal of buildings to create more parking in the area.
5. Enhance and improve streets, sidewalks, benches and other amenities which will encourage more pedestrian, bicycle and public transportation use.

### Street Furniture - Design Objectives

1. Select attractively designed benches, trash containers and bicycle racks that complement the existing architecture.
2. Street furniture should be comfortable and convenient for the elderly and handicapped.
3. Remove billboards and establish uniform design standards for signs.

### Landscaping - Design Objectives

1. Select appropriate size, form, color and texture of street trees and shrubs that will complement and enhance the existing buildings.
2. Place trees and shrubs appropriately.
3. Select plant materials with low water and maintenance requirements.

### Colors and Materials - Design Objectives

1. Select a color palette for building in North Park.
2. Building materials should be compatible with the architectural styles of the buildings.

### Character - Design Objectives

1. Preserve, restore and enhance historic buildings.
2. Encourage architectural detailing on new buildings that is compatible with the historic character of the commercial area.
3. Suggest design features which will unify the commercial area.

### Image - Design Objectives

1. Restore deteriorating buildings.
2. Visually unify and simplify the avenue through uniform signage and building colors.
3. Introduce landscaping to enhance the appearance of the area.

### Building Use - Design Objectives

1. Provide ideas for building facades and street improvements which enhance community shops and services.
2. Provide ideas for pedestrian amenities which encourage residents and non-residents to shop in the neighborhood.
3. Suggest design features that will make shopping more convenient in the area. This may involve improving access, parking or public transportation.

### **El Cajon Boulevard Design Study**

El Cajon Boulevard is the subject of The Design Study for the Commercial Revitalization of El Cajon Boulevard. While this study includes the entire length of El Cajon Boulevard from Park Boulevard to the city of La Mesa, it does provide guidelines for that segment of El Cajon Boulevard traversing Greater North Park. The design study establishes a series of general design guidelines intended to assist in achieving the study's major objective which is business revitalization. These guidelines are summarized as follows:

1. Regional Commercial Role
  - a. Develop the boulevard into a major "Spine." Relate to the Greater North Park market, draw from outside.
  - b. Focus commercial/community centers at nodes.
  - c. Build in resident market with new housing.
  - d. Recognize the historic and continued demand for service- and auto-oriented business.
  - e. Create special districts.

## 2. Attractive Image

- a. Develop the image of the “Spine.”
- b. Use centers of activity at nodes to create specialized image districts to reduce anonymity.
- c. Capitalize on strip nostalgia. Use lights, signs and width to distinguish the boulevard from other streets.
- d. Change the boulevard name to increase community identification.
- e. Develop an urban, high-activity, high-energy boulevard that is fun.
- f. Use minimum floor area ratio calculations and architectural reviews at district centers.
- g. Establish a coordinated streetscape program for street improvements to reduce visual chaos.
- h. Create a more defined physical structure of the boulevard through setback control, screening, sign placement.

## 3. Early Benefits for Full Length

- a. Use design elements such as gateways and median trees at centers to affect the image of the entire boulevard but which do not require six miles of improvements.
- b. Develop low cost ideas that businesses and City can implement relatively quickly.
  - Painting street furniture
  - Painting private and public sign frames
  - Color coding store fronts
  - Color theming district centers

## 4. Cost Efficient Plan

- a. Use public improvements as highly visible seeds for private improvement.
- b. Initiate major improvements through private development which respond to and work with the market.
- c. Establish a workable, realistic image that capitalizes on existing good qualities - do not start over.

5. Respond to the Community

- a. Reinforce Greater North Park identification with district centers that reflect adjacent neighborhoods.
- b. Use increasing residential densities along the boulevard to benefit commercial and to hold down densities in existing residential areas. Decrease density in neighborhoods.
- c. Encourage and accommodate rather than destroy business.
- d. Capitalize on ethnic restaurants and specialty businesses that reflect changes in demographics.
- e. Develop social and police services for the community.

6. Encourage Participation

- a. Establish zoning that allows small-scale development.
- b. Develop FIX UP ideas that work immediately within existing business climate.
- c. Facilitate loan incentives, loans for FIX UP and new development.
- d. Recognize effort and accomplishment.
- e. Establish an effective system of promotion, marketing and organization of individual businesses.
  - BIDs
  - Events

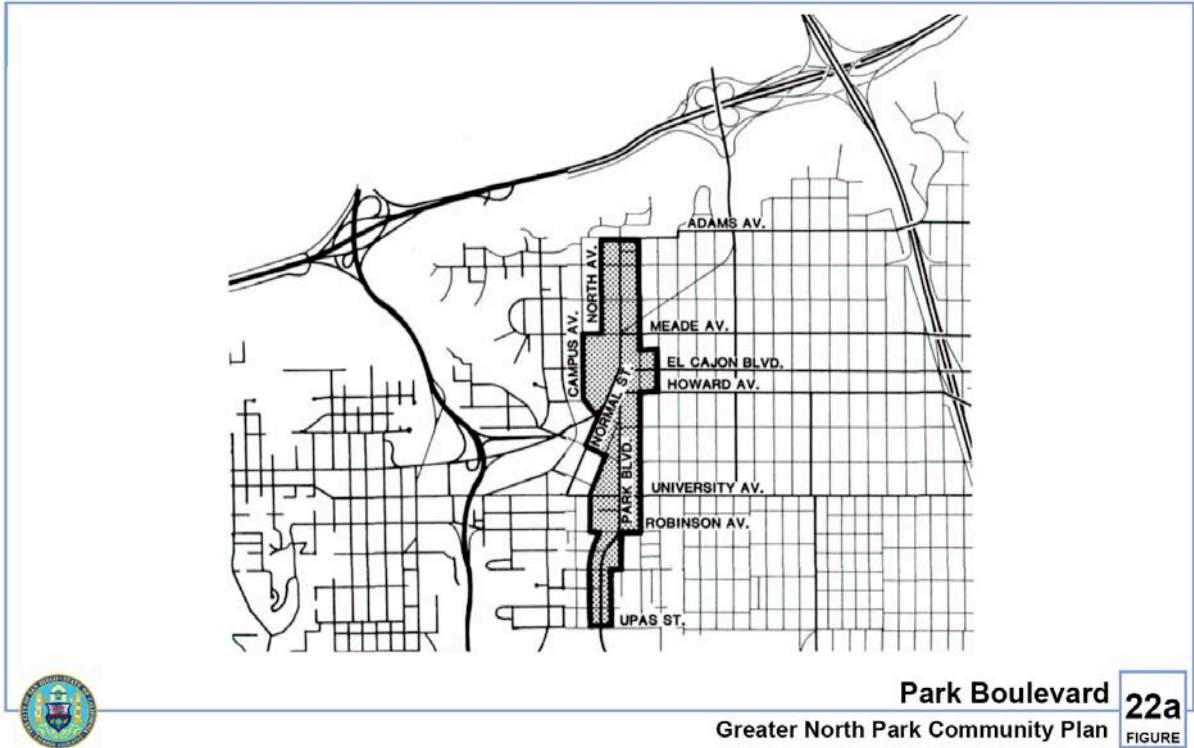
7. Pedestrian Environment

- a. Develop a simple, consistent parking system in front of buildings in linear areas between centers.
- b. Provide parking behind buildings and in common lots or garages at district centers. Place building at right-of-way line in district centers.
- c. Provide arcades, furniture, shelters, trees.

8. Major Arterial Multi-Modal Circulation

- a. Maintain width of street and widen in the most congested areas.
- b. Add light rail transit, trolley or continuous bus service to minimize congestion.
- c. Reduce number of left turn areas.
- d. Coordinate streetlights.
- e. Reduce curb cuts on the boulevard or trade curbside parking for off-street parking.





**Park Boulevard**  
Greater North Park Community Plan **22a**  
FIGURE

**Park Boulevard**

Park Boulevard is an important north-south street which is the common boundary between the Greater North Park and Uptown communities. It is a street of diverse land uses ranging from higher quality residential development south of Robinson Street to areas in need of rehabilitation north of El Cajon Boulevard. The development of Park Boulevard should be coordinated between the communities of Greater North Park and Uptown. The Park Boulevard area is illustrated in **Figure 22A**.

Park Boulevard should become a landscaped parkway linking Balboa Park and the Trolley Barn Park, with trees being placed along the curb lines of both streets. The existing median strip between El Cajon Boulevard and University Avenue should also be enhanced by the planting of additional trees. The Park and Recreation department, which regulates tree planting in the public right-of-ways (streets), should be consulted in order to ensure that trees are selected which will thrive and will present a desirable visual appearance and will not cause damage to sidewalks and curbs. A well-planned landscaping program will add to and improve the pedestrian environment of the boulevard and will also serve to soften the appearance of many of the larger existing buildings.

The preservation and rehabilitation of existing historical and architecturally significant buildings along this street is also recommended, with an emphasis on the Egyptian Revival buildings located within the commercial area between University and Robinson Avenues and the residential area located south of Robinson Avenue. This latter area provides a visually pleasing entry into and exit out of Balboa Park with its two parallel rows of older, and usually ornate, two-story residential buildings. These buildings should be preserved, and further intrusions of high-rise buildings should be prevented.

The two primary commercial areas between University and Robinson Avenues and between Adams and Madison Avenues should be upgraded and made more appealing to surrounding residential areas through the provision of desired goods and services, an improved pedestrian environment, and an improved visual appearance. The area between University and Robinson Avenues is the focal point of a significant senior citizen population including four senior residential towers and should provide the goods and services needed by these senior citizens. The Capri Theater, currently vacant, is located in this area and should be rehabilitated to its original Egyptian Revival appearance. The area between Adams and Madison Avenues, including Adams Avenue between Park Boulevard and Georgia Street, has historically been the commercial center for the University Heights area. However, in recent times, this commercial center has deteriorated and no longer provides an adequate range of desired goods and services. Given its proximity to the Trolley Barn Park site, this area might be rehabilitated using its physical relationship to the past as a redevelopment theme. This area lends itself to becoming a unique commercial center, providing not only essential and convenience goods and services to the surrounding residential area but also providing small specialty restaurants and shops which would attract citywide support.

The Design Study for the Commercial Revitalization of El Cajon Boulevard designates the Park Boulevard and El Cajon Boulevard intersection as the western gateway to El Cajon Boulevard and the intersection is also a primary focal point on Park Boulevard. Therefore, as called for by the El Cajon Boulevard design study, this intersection should become a high-intensity node with appropriate design standards regulating new development.

The Mid-City Revitalization Program includes the commercially zoned properties on both sides of Park Boulevard. This program is administered by the Economic Development Division of the Property Department which is currently studying the Park Boulevard business district in an effort to formulate appropriate implementation programs. The Economic Development Division can also provide measures to support and leverage business reinvestment in the area. Proposed measures include staff support for the formation of a business improvement district, design and implementation of a public improvement project and the creation of a financial assistance program to assist businesses in the rehabilitation of their storefronts. The implementation of any adopted measures is contingent upon the availability of Community Development Block Grant funds. The programs are described in the Commercial Element of this community plan.

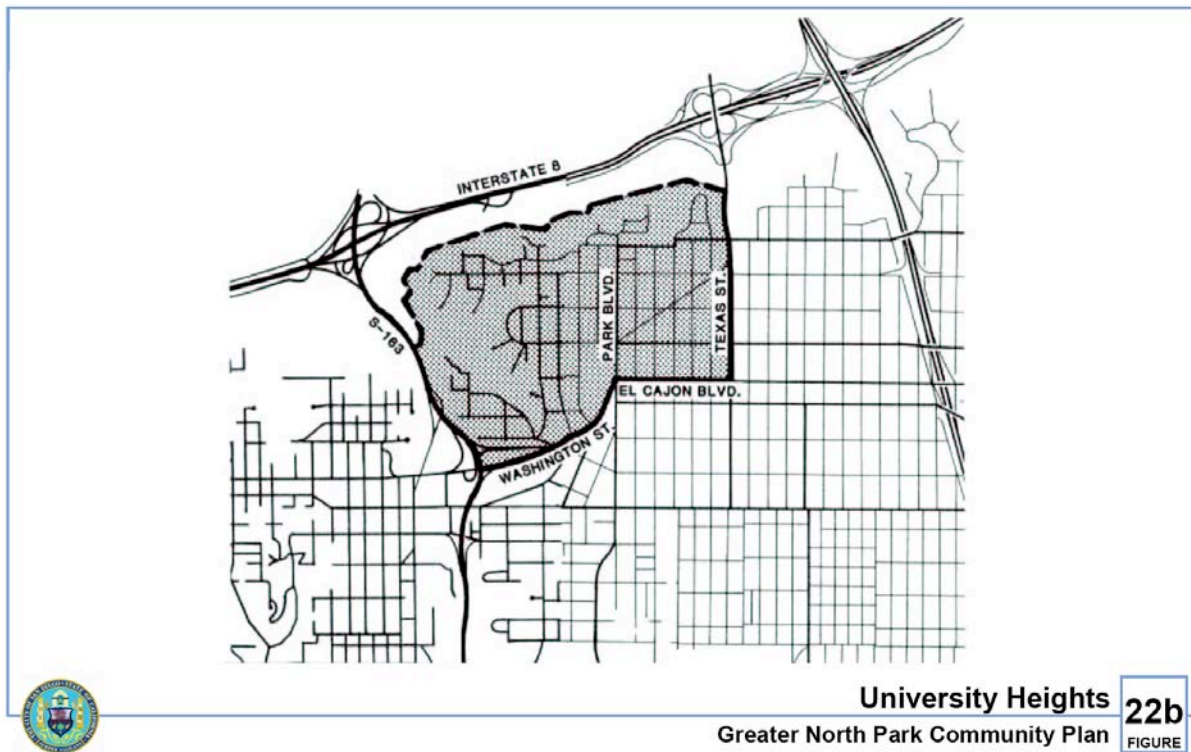
### **The University Heights Neighborhood**

University Heights has traditionally been considered to be a distinct and definable neighborhood centered around the Park Boulevard and Adams Avenue commercial area. That portion of University Heights within the Greater North Park community plan area might be considered to be predominantly north of El Cajon Boulevard and west of Texas Street. The portion of University Heights west of Park Boulevard is in the Uptown Community Plan and is generally located north of Washington Street and east of SR-163. The University Heights area is shown on **Figure 22B**.

For that portion of the University Heights neighborhood which lies within the Greater North Park community planning area, the residential designations are for very-low and low-density or single-family dwellings north of Adams Avenue and for generally low-medium density of 10 to 20 dwelling units per acre south of Adams Avenue. These residential density designations are intended to ensure the retention of the low-density and low-medium density character of the neighborhood while still permitting some infill development.

Commercial development will be limited to Park Boulevard and Adams Avenue with Park Boulevard north of Madison Avenue and Adams Avenue west of Georgia Street being the focal point of the University Heights commercial area. The community plan recommends that a strong relationship be developed between this commercial area and the Trolley Barn Park site immediately north of Adams Avenue.

The **Urban Design Element**, specifically Urban Design Area 1, lists a series of guidelines intended to provide a basis for the tailored zoning regulations. Included are the establishment of a design motif for the Park and Adams commercial node based upon its relationship with the Trolley Barn Park site; retention of the small lot, single-family character along Mission Avenue; establishment of a landscaping program for specified streets; and, retention of the Shirley Ann Place enclave with its small lots and small bungalows.



## URBAN DESIGN AREAS

An urban design review of the Greater North Park community north of Upas Street has established a series of guidelines which should be taken into consideration during the formation of design and development regulations for the community. It should be recognized that during the implementation phase of the planning process these guidelines may be modified and refined and that additional criteria and guidelines may be established. Five urban design areas are discussed below.



### Urban Design Area 1

- A. Enhance and upgrade the Park and Adams commercial node. It should have a design motif based upon its relationship with the nearby Trolley Barn Park site and existing historical features.
- B. Encourage multiuse along this section of Park Boulevard, including higher residential density development and office use.
- C. Retain the existing single-family, large lot character of the Panorama Drive neighborhood. Retain existing palm trees in this area.

- D. Keep higher-density residential development to the west of the topographical break along Florida Street.
- E. The single-family character created by the small lots along Mission Avenue should be retained. Opportunities for landscaped islands in Mission Avenue should be utilized.
- F. Enhance Florida Street as a landscaped parkway between Balboa Park and the Trolley Barn Park site.
- G. Establish a landscaping program for specified streets, which could include landscaped medians, if feasible.
- H. Retain the Shirley Place enclave in its present state featuring small lots and small bungalows.
- I. The Park Boulevard Revitalization Committee of the University Heights Community Association is undertaking a survey of Park Boulevard between Adams Avenue and El Cajon Boulevard. This survey is the first step in the preparation of an urban design study for Park Boulevard. The recommendations of this urban design study should be utilized to the extent possible in the formulation of development regulations and incentives.



## Urban Design Area 2

- A. Preserve and enhance the Adams Avenue “Antique Row” and commercial mode. Commercial development should be kept east of Idaho Street. Establish a design character for this commercial node.
- B. Enhance the entryways to the community.
- C. Maintain the lower-density residential character of the neighborhood north of Adams Avenue.
- D. Encourage multiple use along 30<sup>th</sup> Street including higher density residential development and office use.
- E. Establish a landscaping program for specified east-west streets.
- F. Encourage the private redevelopment of the residential area east of 30<sup>th</sup> Street.



### Urban Design Area 3

#### Urban Design Area 3 (West)

- A. Enhance Florida Street as a landscaped parkway between Balboa Park and the Trolley Barn Park site.
- B. El Cajon Boulevard at Park Boulevard is the major western gateway to Greater North Park. As such, it should be enhanced as a major commercial node and should be distinguished as a gateway through special signage and landscape treatments.
- C. El Cajon Boulevard at I-805 is the major eastern gateway to Greater North Park. It should be enhanced so as to signify its status as a major gateway.
- D. El Cajon Boulevard is a major commercial strip featuring auto-oriented uses. It should develop in accordance with the guidelines of the Design Study for the Commercial Revitalization of El Cajon Boulevard. In addition, the boulevard should feature enhanced landscaping in the medians.
- E. The intersection of 30<sup>th</sup> Street and El Cajon Boulevard is a major commercial node and should be developed as such.



Urban Design Area 3 (East)

- F. Thirtieth Street is an important commercial link between El Cajon Boulevard and University Avenue. It should have design and development standards which are consistent with those two commercial streets.
- G. Park Boulevard between Howard and Lincoln Avenues should be a mixed-use area and should have design and development standards which reflect its status as a major entryway into Balboa Park.
- H. The topographical break west of Florida Street provides easterly view opportunities for development in this area. Design and development standards for this area should take this factor into consideration.





#### Urban Design Area 4

- A. University Avenue at Park Boulevard is a major gateway to the community. It is in close proximity to the Georgia Street bridge. These factors indicate that appropriate design standards enhancing this gateway should be utilized for this area together with special signage, landscaping or other public improvements.
- B. The commercial area between University and Robinson Avenues features a number of Egyptian Revival Buildings. Development regulations oriented towards preserving these buildings should be utilized for this area.
- C. Park Boulevard south of Robinson Avenue is a high-quality residential area featuring many architecturally significant buildings. It is also a major entryway into Balboa Park. Development regulations aimed at preserving this area should be established.
- D. Enhance Florida Street as a landscaped parkway between Balboa Park and the Trolley Barn Park site.
- E. University Avenue between Park Boulevard and Idaho Street should be redeveloped as a higher-density residential area with the exception of the Texas Street area which should be a small neighborhood commercial center. Design and development regulations enhancing the University Avenue area should be established, recognizing that the Georgia Street bridge over University Avenue and the high-rise residential towers on Park Boulevard, are nearby.

- F. The area west of Florida Street has significant quality development. Development regulations encouraging the expansion of this development should be established. In addition, the view opportunities found in this area should be enhanced by new development.
- G. The area south of Landis Street is a stable lower-density residential area with numerous California bungalows. The existing character should be preserved.



### Urban Design Area 5

- A. Thirtieth Street and University Avenue is a major commercial node and the community's central business district. It should be upgraded and enhanced in accordance with the recommendation of the North Park Design Study.
- B. University Avenue at I-805 is a major gateway to the community and should be enhanced.
- C. The node at University Avenue and 32<sup>nd</sup> Street should be strengthened possibly with additional tall buildings emphasizing it as a gateway to the community.
- D. Give consideration to possible street closings in order to create opportunities for pedestrian plazas and pedestrian circulation.
- E. Investigate the opportunities for parking structures serving the commercial area.
- F. Enhance 28<sup>th</sup> Street as a link between the commercial area and Balboa Park.
- G. Retain the area west of 30<sup>th</sup> Street and south of Dwight; and on Felton Street and 33<sup>rd</sup> Street south of Landis Street as single-family.
- H. Higher-density residential development should be permitted in close proximity to the commercial area.

## **IMPLEMENTATION PROGRAM - URBAN DESIGN**

### **Development and Design Proposals**

1. The appearance and compatibility of new development should be considered in reviewing proposals. The proper treatment of bulk and facades is particularly important. For example, large blank walls should be avoided; variations in height and depth of wall surfaces and architectural detailing should be provided on all sides of structures. New buildings should blend into the visual character of the neighborhood.
2. Higher-density development should be encouraged to utilize larger sites. Projects with more than 50 feet of frontage can begin to incorporate more usable open space, imaginative site design, recreational facilities, secured parking and other amenities not possible in a smaller project. Such projects should be carefully designed, however, so as not to overwhelm existing development.
3. Usable open areas should be provided. Even in high-density development, open areas can be provided in the form of balconies, patios and courtyards. Rooftops are a largely untapped resource for open uses in multifamily development. Recent revisions to the multifamily zones encourage both the use of rooftops and the variation in wall surfaces described above.
4. Housing designed for senior citizens should accommodate their needs. Smaller household sizes and a lower incidence of car ownership may allow these projects to be built at a higher density and with less off-street parking than would otherwise be permitted. But these projects must be located with convenient access to transit, shopping and, if possible, senior services. Care should also be taken not to overly concentrate these uses in one area, or to locate them in areas with a shortage of available on-street parking. They should provide an attractive environment for their residents, who spend more time at home than working families. Usable outdoor areas such as patios, balconies and courtyards should be provided, as should laundry facilities. Ample landscaping should be provided to soften the effect of the added density, and to provide shade and visual screening.
5. Adequate landscaping should be provided to help soften higher-density development and to provide shade. Trees and shrubs should be of adequate size when planted. Native and drought-resistant plant materials are encouraged; large turf areas should be avoided.
6. Structures should be designed to utilize shade and breezes. Solar heating and cooling and improved insulation techniques should be utilized.
7. Hillside development should cause minimal disturbance to the topography, complementing the land's natural character. Minimize the need for level areas such as streets, parking lots and yard areas. Required level areas should be in small increments. Development should be clustered, but the resultant structures should not be out of scale with the neighborhood. The use of specialized construction to minimize grading should be considered.

8. Hillside borders Mission Valley and the canyons in the southern portion of the community should be treated with an extreme level of sensitivity. Development should be restricted to the top of the mesa or other areas where virtually no manufactured slopes are visible. An adequate, undisturbed buffer should remain between the mesa and the valley floor.
9. Where manufactured slopes are necessary, they should be made to blend with the natural contours. Natural appearing groundcover should be provided on all created slopes.
10. Public access to canyons and views should be provided at suitable locations.

### Specific Recommendations

1. Special development regulations should be formulated for the R-3000 zone in order to retain the single-family character of areas within that zone. Second units, including move-ons from other areas of the community, should be located in the rear portion of lots with existing single-family dwellings “up front.” Development regulations should ensure that second units maintain the visual character of existing units. Prohibitions against parcel accumulation should be considered in order to maintain neighborhood scale and preclude intrusions by apartment-like structures.
2. New landscaping and parking regulations and standards should be formulated in a coordinated manner so that they complement each other and enhance the visual appearance of the community.
3. Where parcel accumulation is permitted for purposes of increasing permitted residential densities, special design and development regulations should be formulated so as to preclude massive or tall buildings which are out of scale with surrounding development.
4. Special design and development regulations should be formulated for commercial areas emphasizing the unique individual character of each area.
5. Urban design regulations for the entire community should be specifically written and included in the implementing of zoning legislation for the community.
6. The North Park Water Tower is a visual landmark not only visible from within the Greater North Park Community, but also from surrounding communities. The Greater North Park Community Planning Committee and the North Park Community Association have selected a community-identifying logo which could be painted on the water tower as a symbol of community pride.
7. The central business district, at one time, featured a “North Park” sign over the right-of-way at 30<sup>th</sup> Street and University Avenue. As part of the revitalization program for the central business district, consideration might be given to restoration of the sign to its former place of prominence.

8. The “Design Study for the Commercial Revitalization of El Cajon Boulevard” recommends that a “gateway” sign be placed on El Cajon Boulevard at Park Boulevard identifying El Cajon Boulevard as the major east-west thoroughfare between Park Boulevard and La Mesa.

Refinements and modifications to the recommended development intensities for both residential and commercial development, as well as for permitted uses, may be incorporated into the implementing legislation. In addition, it may be necessary to adjust land use and density boundaries in order to properly apply implementing zoning legislation. Finally, the achievability of recommended development intensities and residential densities may be predicated upon the design standards and development regulations of the implementing legislation.