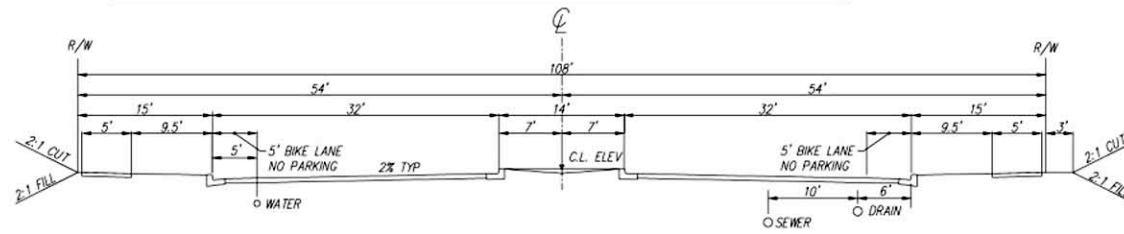
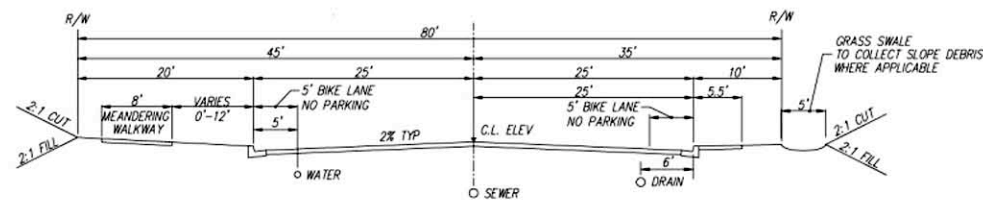


LEGEND

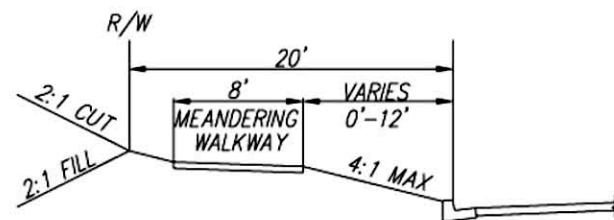
- █ RANCHO ENCANTADA PARKWAY WEST OF PA1 / PA1A ENTRIES
- █ RANCHO ENCANTADA PARKWAY FROM "B" ST. TO "EE/Q" ST.
- █ RANCHO ENCANTADA PARKWAY ALTERNATIVE PARKWAY
- █ RANCHO ENCANTADA PARKWAY FROM "E" STREET TO "U" STREET
- █ RANCHO ENCANTADA PARKWAY FROM RANCHO ENCANTADA / MONTECITO BOUNDARY TO "B" STREET
- █ RANCHO ENCANTADA PARKWAY EAST OF P.A. 12 AND STREET "B"



**RANCHO ENCANTADA PARKWAY (STREET "A")
MODIFIED FOUR LANE URBAN COLLECTOR W/RAISED MEDIAN**

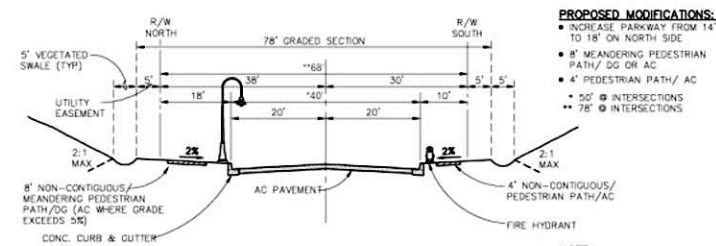


**RANCHO ENCANTADA PARKWAY (STREET "A")
MODIFIED TWO LANE COLLECTOR W/TWO WAY LEFT LANE
35 MPH DESIGN SPEED
ENHANCED PARKWAY NORTH SIDE**



**STREET "A" ALTERNATIVE PARKWAY
FOR MEANDERING WALKWAY**

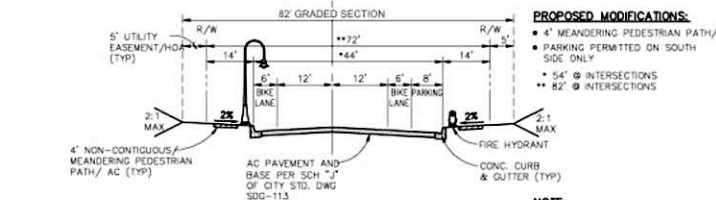
MONTECITO



**TYPICAL SECTION
TWO-LANE COLLECTOR STREET (MOD.)
NO FRONTING PROPERTY
"A" ST FROM THE WESTERLY BOUNDARY TO "B" ST
NOT TO SCALE**

- PROPOSED MODIFICATIONS:**
- INCREASE PARKWAY FROM 14' TO 18' ON NORTH SIDE
 - 8' MEANDERING PEDESTRIAN PATH/ DG OR AC
 - 4' PEDESTRIAN PATH/ AC
 - 50' ∅ INTERSECTIONS
 - 78' ∅ INTERSECTIONS

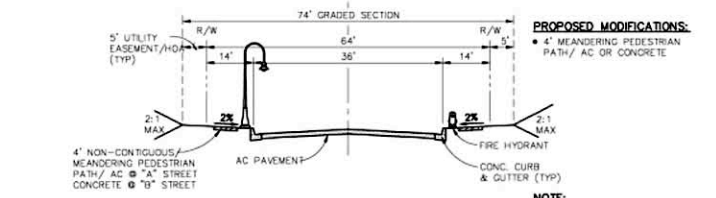
NOTE:
• 40 MPH DESIGN SPEED



**TYPICAL SECTION
TWO-LANE COLLECTOR STREET (MOD.)
"A" ST FROM "B" ST TO "EE"/"Q" ST
NOT TO SCALE**

- PROPOSED MODIFICATIONS:**
- 4' MEANDERING PEDESTRIAN PATH/AC
 - PARKING PERMITTED ON SOUTH SIDE ONLY
 - 54' ∅ INTERSECTIONS
 - 82' ∅ INTERSECTIONS

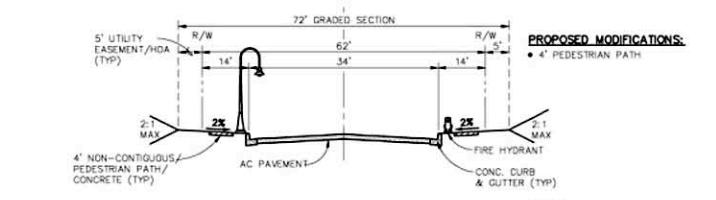
NOTE:
• 30 MPH DESIGN SPEED



**TYPICAL SECTION
TWO-LANE SUB-COLLECTOR STREET (MOD.)
"A" ST FROM "EE"/"Q" ST TO EAST END OF "A" ST, AND "B" ST
NOT TO SCALE**

- PROPOSED MODIFICATIONS:**
- 4' MEANDERING PEDESTRIAN PATH/ AC OR CONCRETE

NOTE:
• 30 MPH DESIGN SPEED



**TYPICAL SECTION
RESIDENTIAL LOCAL STREET (MOD.)
"C" ST TO "NN" CT
NOT TO SCALE**

- PROPOSED MODIFICATIONS:**
- 4' PEDESTRIAN PATH

NOTE:
• 25 MPH DESIGN SPEED

NOT IN LEGEND

SYCAMORE ESTATES

SOURCE: NOLTE BEYOND ENGINEERING (MONTECITO)
SOURCE: RBF AND ASSOCIATES (SYCAMORE)



CIRCULATION

COMMUNITY LINKAGES

The Rancho Encantada Precise Plan area is served primarily by Pomerado Road and Beeler Canyon Road. Pomerado Road abuts the Precise Plan boundary to the west and Beeler Canyon Road abuts the Precise Plan boundary to the north, although within some areas, Beeler Canyon Road is within the Precise Plan area. Major roads that link the Precise Plan with the region and the surrounding community include Scripps Poway Parkway, Creek Road, Pomerado Road, Spring Canyon Road, State Route 67 (SR-67) and Interstate 15 (I-15). Interstate 15 is located approximately two miles to the west of the Precise Plan area. State Route 125 (SR-125) is planned to cross the eastern portion of the Rancho Encantada site, but its alignment is speculative.

The Precise Plan area will be linked internally by a series of residential collector roads and local residential streets. The Montecito and Sycamore Estates portions of the Precise Plan will be linked by Rancho Encantada Parkway. A trail system also will be accommodated within the Precise Plan. This trail system will connect with existing trail heads within the Precise Plan area and to the open space area to the east, including trails within the adjacent Gooden Ranch and Sycamore Canyon County Open Space Preserve.

NEIGHBORHOOD STREET SYSTEM

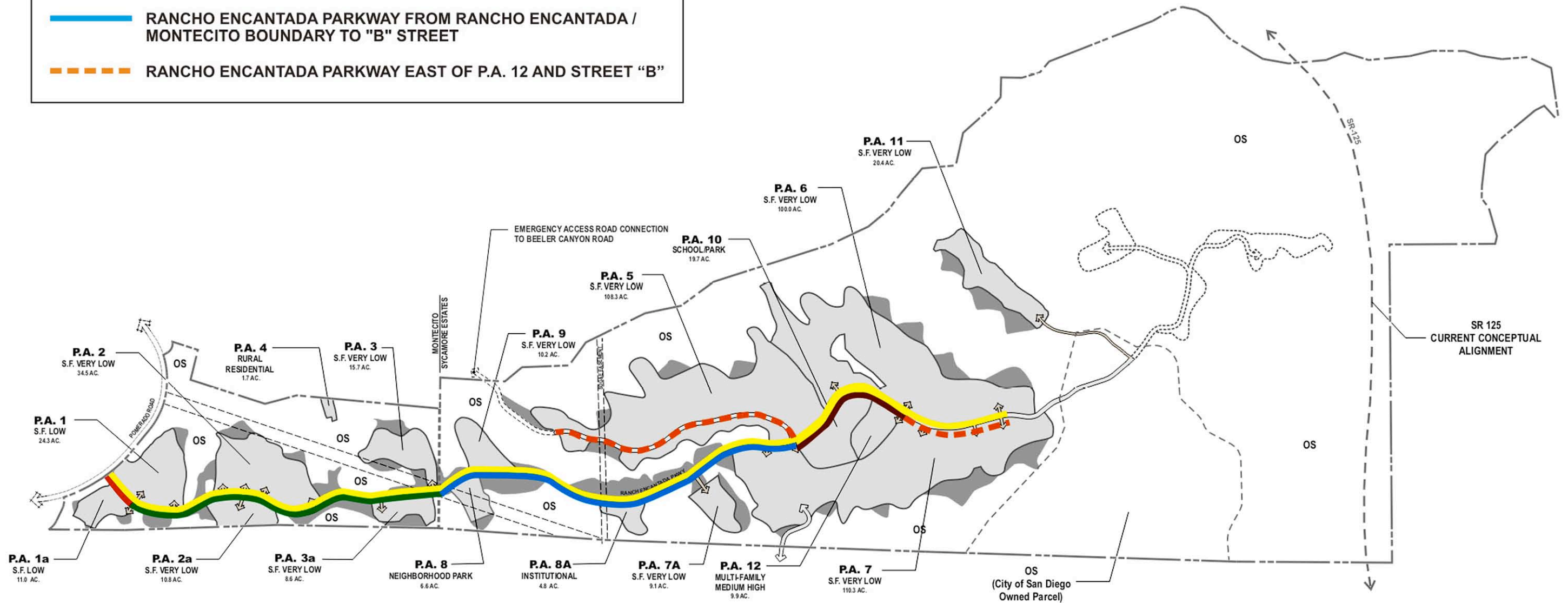
Street Classifications and Design

Two existing roadways abut the Precise Plan: Pomerado Road and Beeler Canyon Road. Pomerado Road is located adjacent to the western boundary of the Precise Plan area and will provide primary access to the site. This roadway connects to I-15 at Miramar Road and travels northeast to Spring Canyon Road as a two-lane street. The street turns to the right at Spring Canyon Road and is classified and built as a four-lane street for approximately 500 feet, to accommodate the large turning volumes at this intersection. It then continues as a two-lane street past the Precise Plan to the south of Creek Road. Beeler Canyon Road is located adjacent to the northern boundary of the Precise Plan area and will connect to a gated emergency-only access entry to the site. The emergency-only access entry will occur within the Sycamore Estates portion of the project site. Beeler Canyon Road eventually connects with Pomerado Road to the west.

Access to the individual planning areas of Montecito (Planning Areas 1-4) and Sycamore Estates (Planning Areas 5-12) will be provided by Rancho Encantada Parkway and other public or private local roads, constructed to the satisfaction of the City Engineer. All public streets within the Precise Plan area will be conveyed to the City via easements, not in fee title. Street design shall conform to the City of San Diego Street Design Manual or to the satisfaction of the City Engineer and the City Fire Marshall. In addition, on-street parking on all internal roadways shall be provided according to City regulations. All streets within Rancho Encantada are anticipated to be public; however, private streets and gated entries are

LEGEND

- RANCHO ENCANTADA PARKWAY WEST OF PA1 / PA1A ENTRIES
- RANCHO ENCANTADA PARKWAY FROM "B" ST. TO "EE/Q" ST.
- RANCHO ENCANTADA PARKWAY ALTERNATIVE PARKWAY
- RANCHO ENCANTADA PARKWAY FROM "E" STREET TO "U" STREET
- RANCHO ENCANTADA PARKWAY FROM RANCHO ENCANTADA / MONTECITO BOUNDARY TO "B" STREET
- - - RANCHO ENCANTADA PARKWAY EAST OF P.A. 12 AND STREET "B"



permitted but should be limited to the smaller planning areas accessed from Rancho Encantada Parkway. In addition, an emergency-only access gate will be provided on Sycamore Estates' Street "B" to limit access to Beeler Canyon Road. If gated entries are provided, they shall conform to Council Policy 600-42 (Limited and Controlled Access Development). Right-of-way specifications for local roads will be determined during the Planned Residential Development (PRD) permit process or the tentative subdivision map process for individual planning areas. For a discussion of the design treatment of Precise Plan entrances and streetscapes, refer to **Section 5, Design Element, Landscape Design**. Roadway cross-sections are shown on **Figures 10 and 10a**.

Rancho Encantada Parkway

Rancho Encantada Parkway will be the primary east/west roadway within the Precise Plan. This roadway will have a varied street section. It will connect with Pomerado Road at the site's western boundary and will begin as a four-lane road for a distance of approximately 800 feet. Rancho Encantada Parkway will transition into a modified four-lane urban collector from Pomerado Road to the Planning Area 1 entry in the Montecito portion of the Precise Plan and will continue as a modified two-lane collector through Sycamore Estates. Residential lot driveways shall not be accessed directly from Rancho Encantada Parkway, unless approved by the City Engineer.

Local Residential Streets

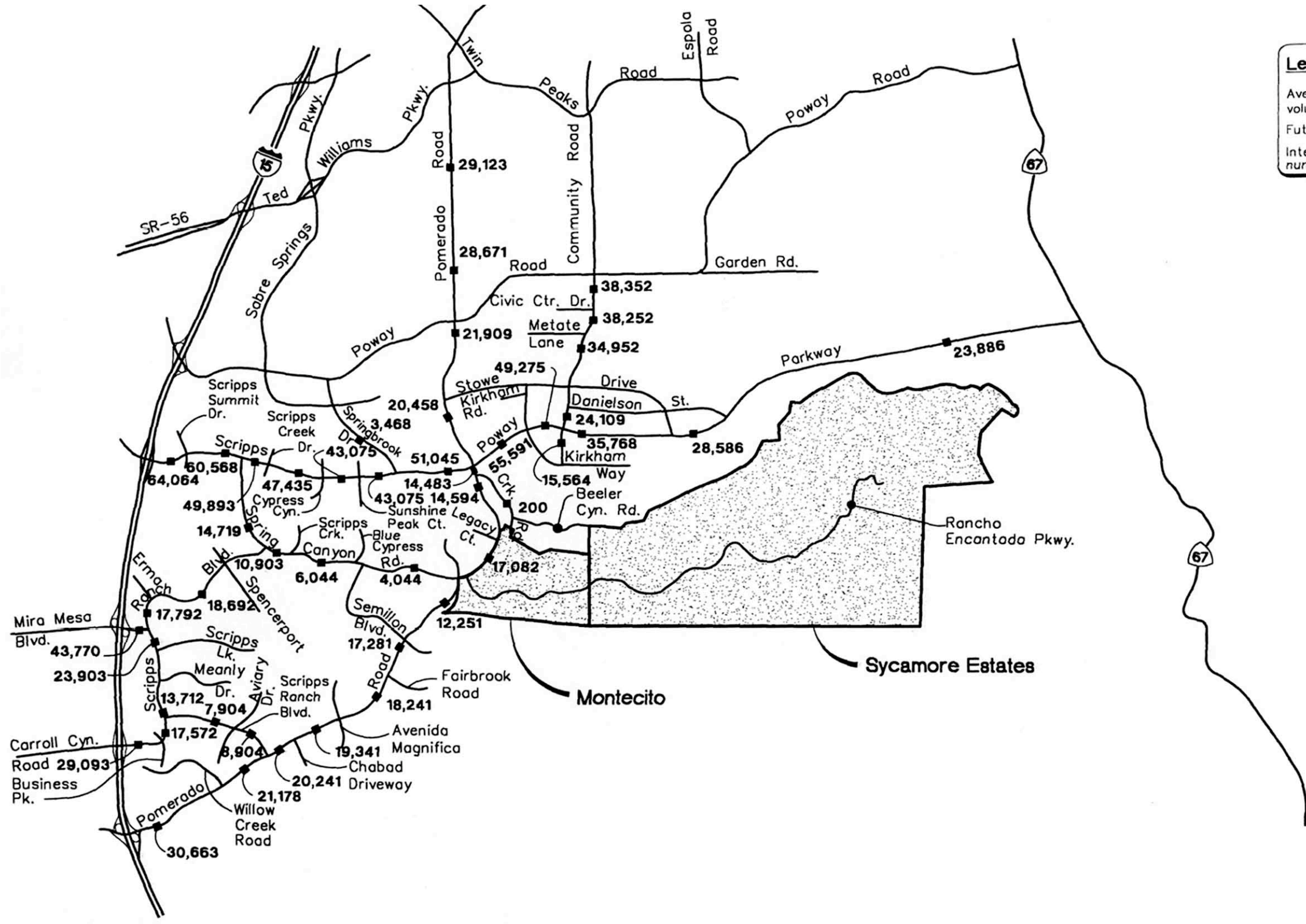
Local residential streets are planned to serve the individual planning areas of Rancho Encantada. The adopted street design requirements set forth by the City of San Diego will be adhered to in the design of the Rancho Encantada local street system (see **Figures 10 and 10a**).

Area Coordination of Roadways

Rancho Encantada Parkway is designed as the Precise Plan's main access road, and as such, coordination must occur between the Montecito and Sycamore Estates development areas. Coordination during the PRD permit and tentative subdivision map stages of Precise Plan implementation should encompass street alignment; roadway cross-sections including bicycle and pedestrian paths; grades and grading; design treatments; and traffic controls as appropriate. If the Sycamore Estates portion of the Precise Plan develops prior to development of the Montecito portion, it may be necessary to construct Rancho Encantada Parkway across the Montecito property to gain appropriate access. All development is subject to reimbursement.

Sound Attenuation Considerations

Vehicular traffic on Pomerado Road and/or Rancho Encantada Parkway may create noise that exceeds the City's criteria of 65 CNEL (exterior) and 45 CNEL (interior) for maximum acceptable noise levels for residential homes. Mitigation measures such as increased building setbacks, sound attenuation walls, berms, or a combination thereof may be required in areas that are subject to noise levels in excess of City standards. An acoustical analysis shall be required for all future tentative maps identified in the accompanying Environmental Impact



Legend

- Average daily traffic volume ■ X,XXX
- Future roadway -----
- Intersection identification number



Report as having the potential for exposure to adverse noise levels. Any mitigation recommendations contained in the acoustical analysis shall be required as conditions of approval for all tentative maps and PRD permits so affected.

ALTERNATIVE TRANSPORTATION FACILITIES

Mass Transit

The Metropolitan Transit Development Board (MTDB) states that there is currently no bus service along Pomerado Road, but future service along Pomerado Road is possible.

Bicycle Circulation

Bicycle lanes will be provided within the Rancho Encantada Precise Plan area and will connect to the bikeway network in neighboring communities. On-street, Class II bicycle lanes in and adjacent to Rancho Encantada will follow Pomerado Road and Rancho Encantada Parkway within the Precise Plan. Class II bicycle lanes will be marked in conformance with the City of San Diego striping and width requirements along these roadways. Bicycle travel on local residential streets will be accommodated by Class III bicycle routes, which consist of a shared right-of-way designated by signs only, with bicycle traffic sharing the roadway with motor vehicles. Bicycle parking facilities consist of bicycle racks and are anticipated at the public school/park site. Bicycle racks should not require the use of chains or cables to secure them as chains and cables are easily cut by thieves using bolt cutters. It is recommended that bicycle rack design encourage the use of U-shaped high security locks to lock bicycles.

Pedestrian Circulation

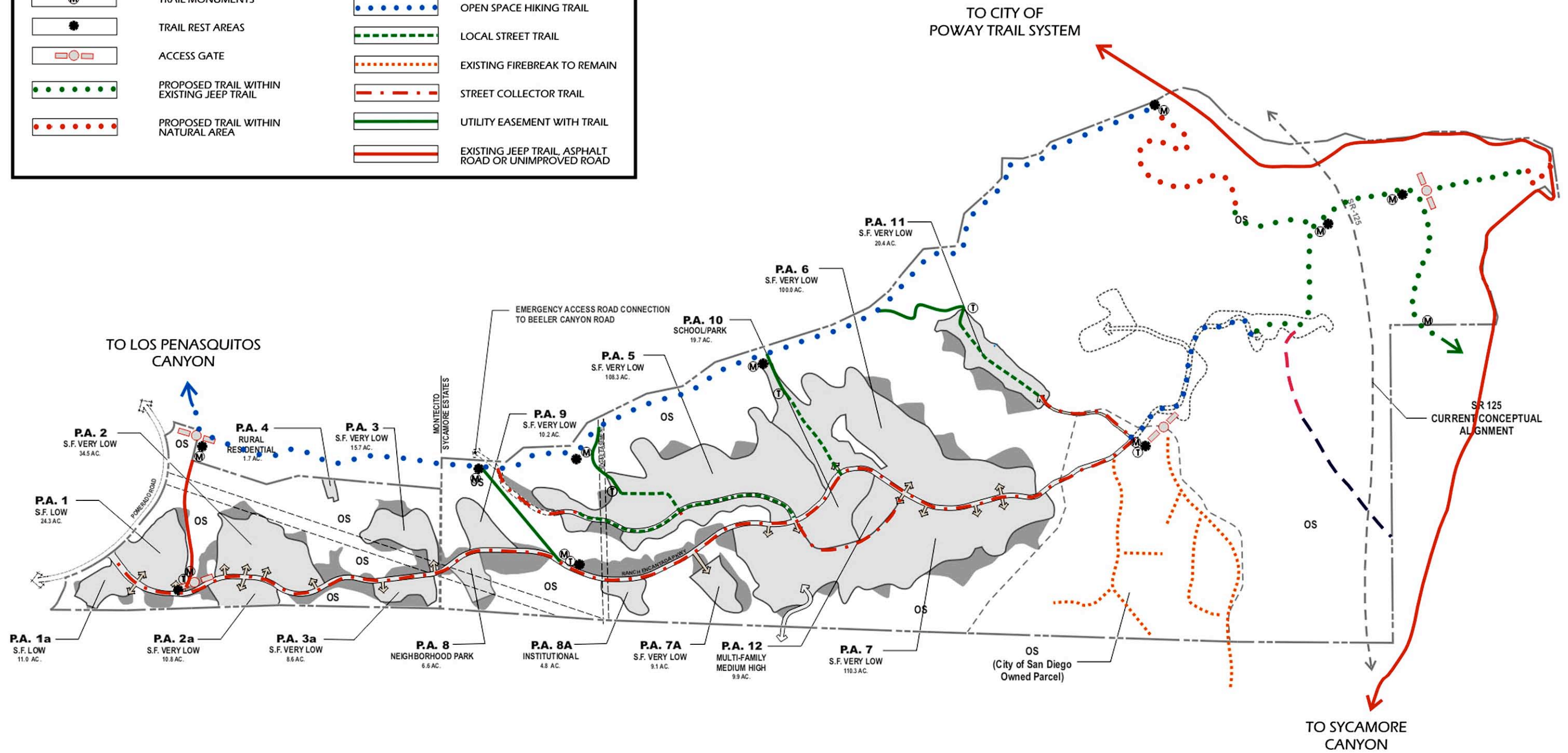
Pedestrian circulation within the Precise Plan area will be accommodated by sidewalks on local residential streets and by trails. The sidewalks along the local residential streets will be designed to the City of San Diego requirements. A meandering non-contiguous sidewalk, also referred to as a streetscape trail, is planned along the northern side of Rancho Encantada Parkway in both the Montecito and Sycamore Estates sub-project sites. This sidewalk is intended to separate pedestrians from vehicular traffic. Pedestrian trails will be designed to provide pleasant walking experiences and linkages between all proposed residences and the school and park site.

Master Trail System

Although the Precise Plan does not propose active recreational uses within open space areas, pedestrian trails are permitted uses in all areas designated as open space, subject to the requirements and restrictions of the City's MSCP. If trails are provided through MHPA open space, the following standards shall apply:

LEGEND

	TRAIL LINKS TO REGIONAL TRAIL HEADS		PROPOSED TRAIL WITHIN EXISTING SDGE ACCESS
	COMMUNITY TRAIL HEADS		POTENTIAL TRAIL WITHIN FORMER FIREBREAK/JEEP TRAIL
	TRAIL MONUMENTS		OPEN SPACE HIKING TRAIL
	TRAIL REST AREAS		LOCAL STREET TRAIL
	ACCESS GATE		EXISTING FIREBREAK TO REMAIN
	PROPOSED TRAIL WITHIN EXISTING JEEP TRAIL		STREET COLLECTOR TRAIL
	PROPOSED TRAIL WITHIN NATURAL AREA		UTILITY EASEMENT WITH TRAIL
			EXISTING JEEP TRAIL, ASPHALT ROAD OR UNIMPROVED ROAD



- a. Provide sufficient signage to clearly identify public access to the MHPA.
- b. Locate trails, view overlooks, and staging areas in publicly-owned areas and in the least sensitive areas of the MHPA. Locate trails along the edges of urban development and follow existing dirt roads/trails and utility easements as much as possible.
- c. Trails should not be paved, and trail widths should be minimized, except for the Streetscape Trail shown on **Figure 11**.
- d. Existing jeep trails and firebreaks that are proposed to be included in the trail system will be converted to trails. Existing jeep trails that are not identified as trails within this Precise Plan area shall be naturally revegetated and will not be considered trails. Portions of existing firebreaks not identified as trails by this Precise Plan shall be artificially revegetated and will not be considered trails. Existing firebreaks located on the City-owned parcel will not be revegetated.

In addition, compliance with the MSCP General Management Directives for trail design and maintenance will be followed where required. See **Figure 11, Master Trail Plan**, for the general location of trails within the Precise Plan area.