

## **URBAN DESIGN**

### **Existing Conditions**

The Peninsula community is a highly urbanized residential area surrounded by water and recreational resources. The Peninsula community is unique due to a number of physical factors. Elements which contribute to the Peninsula's appearance include:

- A coastline consisting of bluffs, rocky and sandy beaches, and the bay.
- Numerous hillsides and canyons which act as natural boundaries forming distinctive neighborhoods.
- Extensive areas of large trees and natural vegetation.
- Well-defined neighborhoods with a variety of well preserved architectural styles and housing types.
- A number of historically significant buildings and resources.

Certain areas within the Peninsula are characterized by visual clutter due to a proliferation of signs, overhead utilities, billboards and poorly maintained buildings. Specifically, the Roseville, Central Peninsula, Loma Palisades and south-central commercial districts lack aesthetic quality due to the absence of design standards and minimal landscaping. Major streets which include Rosecrans, Nimitz, Scott, Shelter Island Drive, West Point Loma and Voltaire also lack landscaping and sign control.

Generally none of the main entrances to the community (Nimitz Boulevard, North Harbor Drive or Rosecrans) are visually distinctive; although, recently the Harbor Drive entrance has been improved with a large attractive entry sign.

The Shelter Island area, which is under the jurisdiction of the San Diego Unified Port District, has a distinct design character due to strict design, sign and landscaping controls. The character of this area has been described as “south seas nautical.” The landscaping is primarily tropical and the architecture borrows heavily from traditional Polynesian architecture. The size, materials and colors of signs on Shelter Island are strictly controlled. The unified architectural themes and sign controls on Shelter Island contrast sharply with the visual clutter and wide variety of architectural styles and materials in the adjacent Roseville commercial district.

The topography of the Peninsula, including unimproved sections and endings, provides views of the San Diego Bay and along the north coast to Pacific Beach.

A number of view corridors throughout the Peninsula area provide vistas of the San Diego Bay, the downtown, Coronado, Mission Bay and Pacific Beach. These vistas occur primarily from existing roadways which include: Catalina Boulevard (within the Point Loma Naval

Complex), Shelter Island Drive, Rosecrans, Talbot, Canon, Garrison, Chatsworth, West Point Loma Boulevard, Famosa Boulevard, Santa Barbara, Point Loma Avenue and Sunset Cliffs Boulevard. Some views are partially blocked by existing development. In addition, the Sunset Cliffs Shoreline Park, from the Point Loma Naval Complex to Adair Street, provides an unobstructed view of the ocean.

## **Objectives**

- Maintain and complement the existing scale and character of the residential areas of Peninsula.
- Upgrade the physical appearance of the commercial area in Peninsula.
- Protect and enhance those natural and man-made features of the Peninsula community which make this area unique to the San Diego region.
- Enhance the community's image through special treatment of the major entry points into the community.
- Preserve and enhance significant views of the bay and ocean.

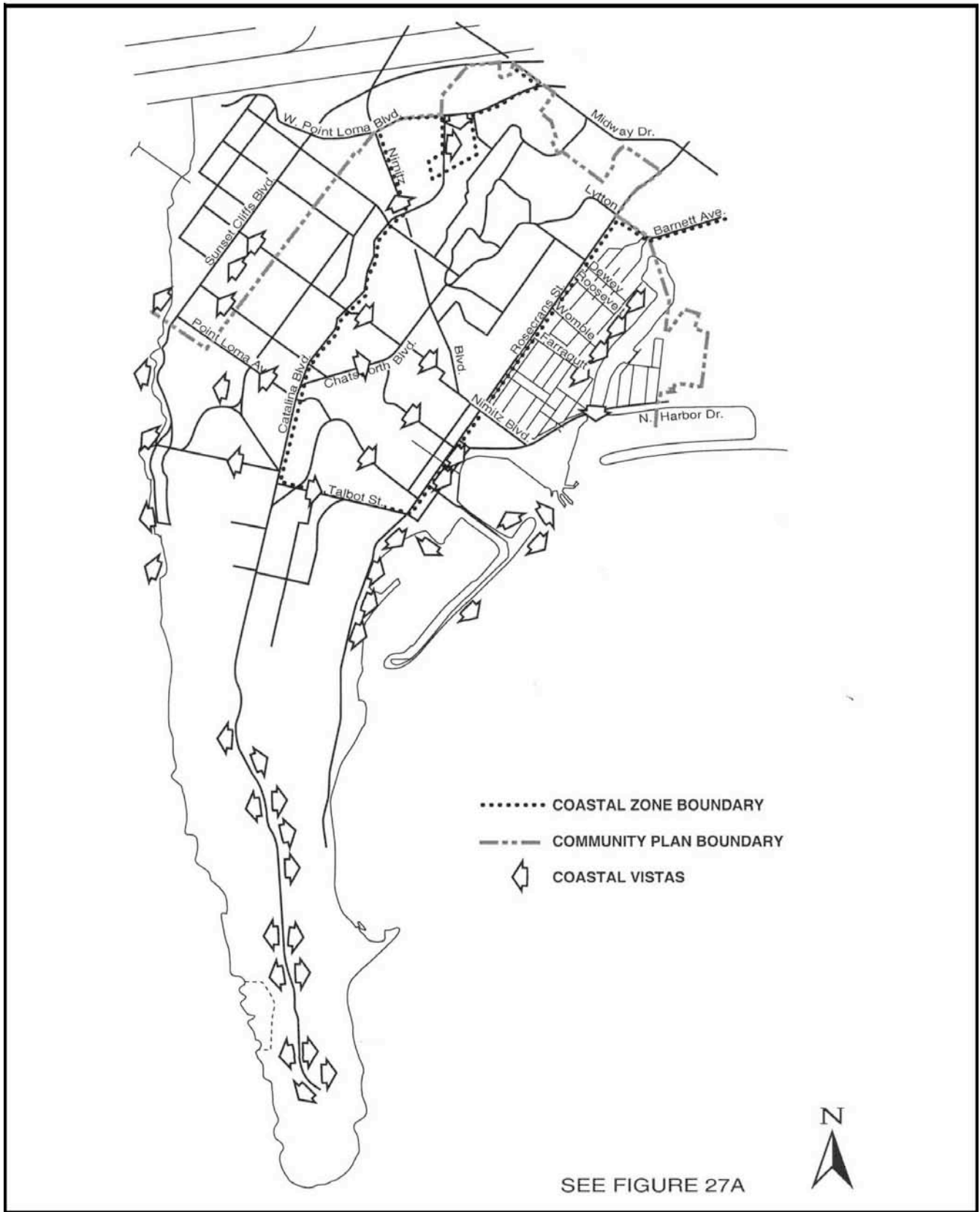
## **Recommendations - Urban Design Guidelines**

The following Urban Design guidelines have been developed for general application in residential and commercial development within the community.

### Residential Guidelines

#### 1. Natural Environment (views)

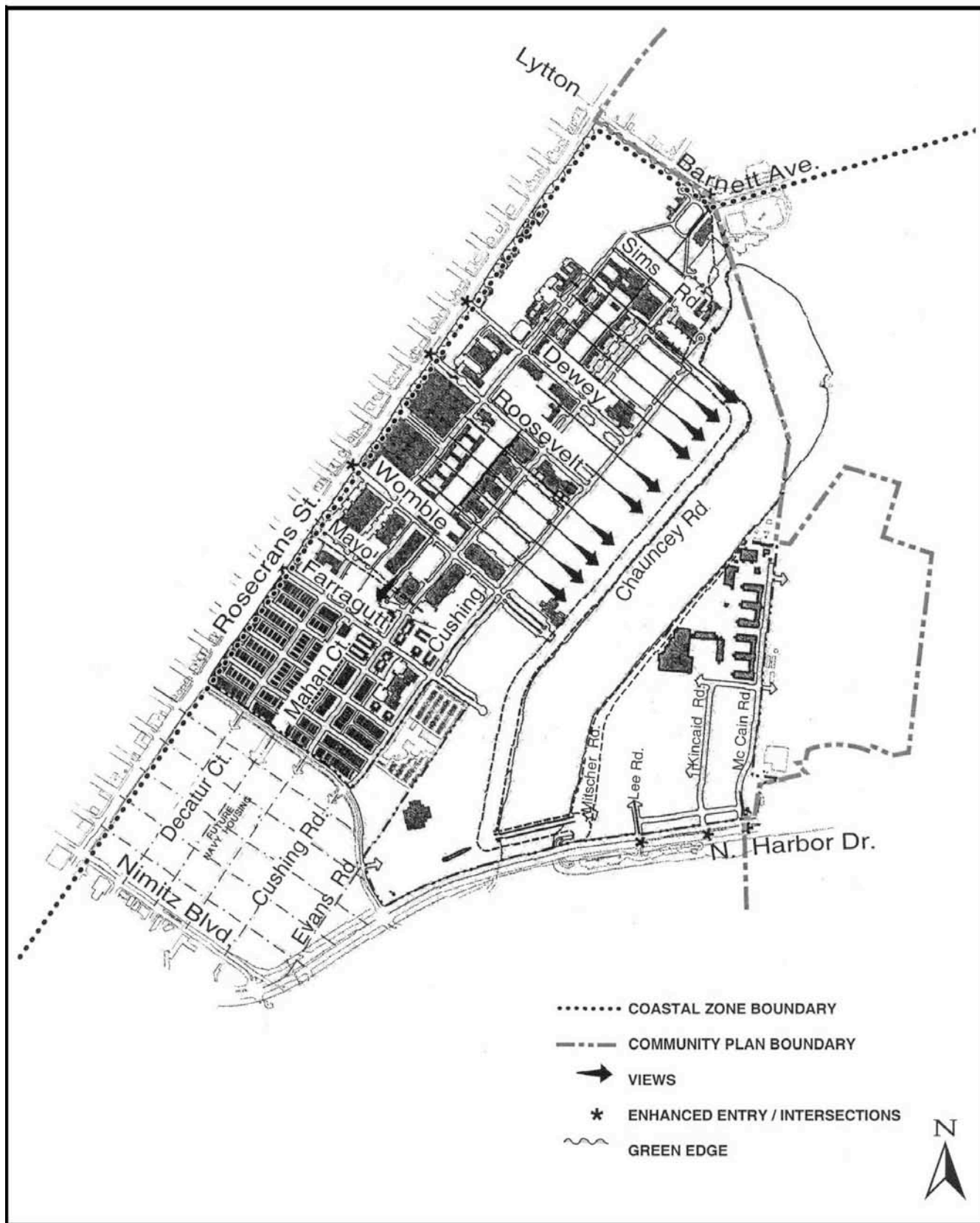
- Structures should be designed to protect views of Peninsula's natural scenic amenities, especially the ocean shoreline, and San Diego Bay.
- View corridors, by utilizing side yard setbacks, should be encouraged along the ocean and bay shoreline and bluff-top areas in order to avoid a continuous walled effect along the shoreline. Narrow corridors create visual interest and allow for sea breezes.
- Setbacks and view corridors should be kept clear of obstacles which may interfere with visual access.
- Where existing streets serve as public access and view corridors of the ocean and bay, development on corner lots requires special design considerations. In order to maximize public views, new development should be set back from the corner or terraced away from the street.



**Coastal Views**  
**Peninsula Community Plan**

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**FIGURE 27**



**Views**  
**Peninsula Community Plan**

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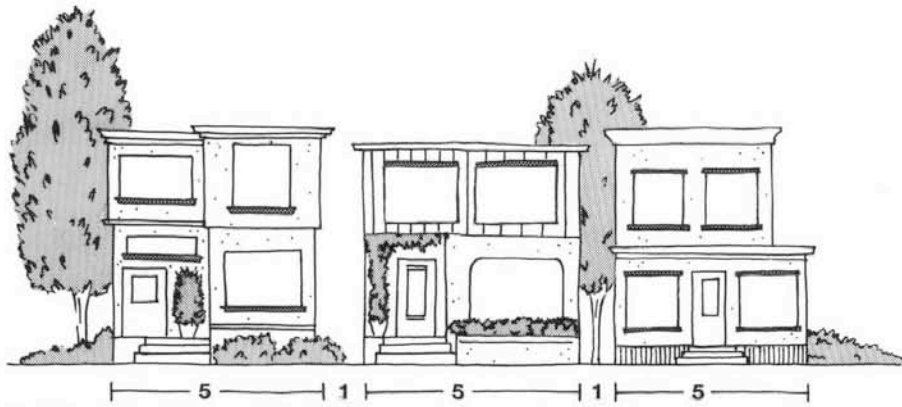
**FIGURE 27a**

## 2. Building Scale

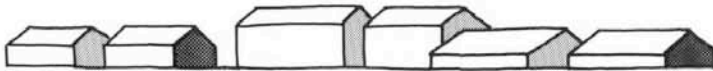
Building scale is a quality which describes the relationship of buildings to each other and to human dimensions.

- New development should be consistent with the scale and character of the existing development of the surrounding areas. The fitting in of new development is, in a broad sense a matter of scale. It requires a careful assessment of each building site in terms of the size and texture of its surroundings, and a very conscious attempt to achieve balance and compatibility in design between old and new buildings.
- Harmony should be promoted in the visual relationships and transitions between new and older buildings. New buildings should be sympathetic to the scale, form and texture of surrounding development. Where new buildings are larger than existing structures, large surfaces should be articulated and textured to reduce their apparent size and to reflect the pattern of the surrounding development. In order to achieve this, multifamily buildings should be designed and evaluated in the context of surrounding development. Plans and elevations should consider adjacent development.
- Abrupt differences in scale (building height) between new development and neighboring development should be avoided. Gradual transitions in scale are preferred.
- Building bulk should be controlled through the use of vertical and horizontal offsets and other architectural features (balconies, porches, bay windows) which serve to break up building facades. A reduction in the front yard setback for a portion of the structure would serve as an incentive for vertical offsets.
- The roofline of new structures should compliment the dominant rooflines of the neighboring buildings. Three-story structures, "boxlike" in design (flat roofs), should be avoided. An incentive should be offered in the form of a reduced interior yard for three-story structures which observe an additional setback above 20 feet. This would encourage rooflines that would reduce building scale and bulk.
- Taller portions (third stories) of buildings should be set back from the required front setbacks. This would control building scale by emphasizing one- and two-story facades along street frontages.
- Larger structures resulting from the consolidation of two or more lots should be designed to reduce actual or apparent building bulk. This can be achieved by pitched-roof design, separating large surface matter through architectural techniques such as vertical and horizontal offsets, setbacks and changes in exterior treatment.
- Flat roof surfaces should be considered for use as terraces, with limited landscaping if it is structurally and economically feasible.





RHYTHM OF SPACING OF BUILDINGS ON THE STREET:  
MOVING PAST A SEQUENCE OF BUILDINGS, ONE EXPERIENCES A RHYTHM OF RECURRENT BUILDING MASSES TO SPACES BETWEEN THEM. THIS RHYTHM IS NECESSARY TO CREATE AN ADDED ELEMENT OF HARMONY IN A NEIGHBORHOOD'S ARCHITECTURE.



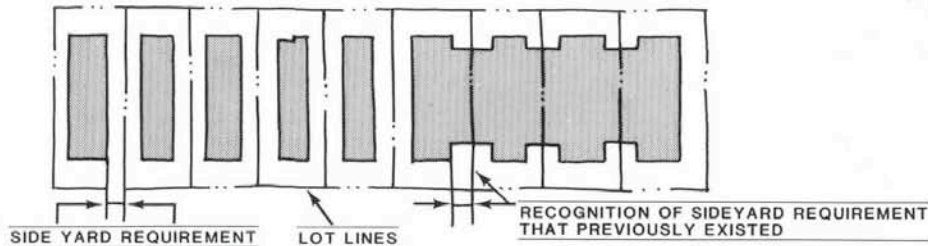
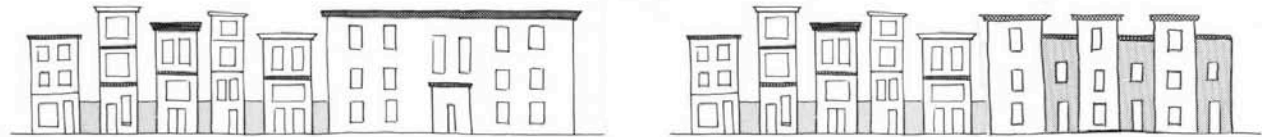
UNITY IS PROVIDED BY ORIENTATION OF HOUSES TO EACH OTHER AND THE USE OF RELATED SHAPES.



A CLASH OF SHAPE AND SCALE INVOKE CHAOS AND RESTLESSNESS.

MOST CONCERN ABOUT HEIGHT HAS TO DO WITH ITS APPARENT VISUAL IMPACT ON ITS SURROUNDINGS. DUE TO A VARIETY OF REASONS BUILDINGS OF RECENT YEARS HAVE TENDED TO BE LARGER AND BULKIER WITH PARTICULAR ARCHITECTURAL EMPHASIS BEING PLACED ON ACCENTUATING HEIGHT. THE RESULT HAS BEEN LARGER, MORE IMPRESSIVE STRUCTURES THAT HAVE TENDED TO RELATE POORLY TO THEIR ADJACENT NEIGHBORS.

EXISTING AND NEW STRUCTURES BUILT WITHIN EXISTING LOT LINES APPEAR TO BE HARMONIOUS. WHEN MANY SMALLER LOTS ARE JOINED TO MAKE ONE LARGE LOT, THE RESULT IS OFTEN A STRUCTURE THAT IN NO WAY RESEMBLES THE OTHER BUILDINGS IN THE NEIGHBORHOOD.



## Height, Scale, Bulk and Rhythm

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FIGURE 28

### 3. Architectural Detailing

- A building with a roof form or profile similar to surrounding buildings strengthens the visual identity of the structures and contributes to a street's visual harmony. A building that does not share roof form or profile with adjoining structures is particularly disruptive to a neighborhood street.
- Building facades range from the very flat with no expression of interior volumes to the highly articulated. Any new facade should support and enhance a block and design unity and sense of character.
- Features such as side notches, partial facade setbacks, entry porches, bay windows and other small scale geometric forms set up strong visual rhythms. Where these elements are a part of a block character, new development should incorporate them to preserve block unity and to fit in harmoniously.
- Facade decoration affects the impression of massiveness as well as the overall pattern of light and shade. New development should compliment the existing facades of the neighboring buildings.
- Entryways, window proportions, facade texture (surface materials) and finish are significant factors in how well a building contributes and relates to surrounding buildings. All of these elements should be taken into consideration to provide consistency between new existing structures.
- Structures should conform to the pattern and rhythm of spacing of buildings already existing within the block.

### 4. Parking

- The automobile or parking facilities should not be a dominant element of a neighborhood character. On-site parking should be screened or located in areas not highly visible from the street.
- On-site parking should be underground or located in the rear of buildings and accessed from the rear alley whenever feasible.
- Curb cuts and front driveways should be minimized. Driveways should be restricted in width within required front yard setbacks. Pavement patterns at driveway entrances should also be encouraged.
- Large surfaced parking areas should be broken up with landscaped islands and screened from view by landscaping. This can be accomplished through the use of trees, shrubs, mounding or walls appropriate to the character of the area. Large parking areas should also include patterned pavings as a means to enhance surface areas.



USE OF SIMILAR SURFACE MATERIALS ASSISTS FITTING A NEW BUILDING INTO A BLOCK.



TYPICALLY, A SHINGLED BUILDING WILL NOT FIT IN AS WELL WITH PREDOMINANTLY PAINTED STUCCO BUILDINGS UNLESS THE BUILDING HEIGHT, DETAILING AND SCALE ARE COMPATIBLE WITH THE SURROUNDINGS.



## Building Materials and Textures

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FIGURE 29



## 5. Streetscape

- Streets should be designed and developed to be pleasant places to walk down as well as drive upon. The arrangement of houses should create a pleasant streetscape. Attention to building siting, paving, landscaping and tree planting should all be designed to enhance the visual effect.
- The facades of buildings at pedestrian levels should provide a close relationship between the building and street. Blank walls should be considered detrimental to this relationship.
- Sidewalk and eye level elevations should be made up of glass, soft surfaces, extension of living areas (balconies, decks, patios) and appropriate landscaping.
- Fencing or landscaping that is provided as either a screen for open space or for private yards should avoid "blocking off" the building from the sidewalk.
- Mature street trees should be preserved and replaced when removal is unavoidable. Street trees, particularly when an individual species dominates, become an important element of a neighborhood and streetscape character. Trees should be upright, medium height and have good light penetration.
- In a few specific areas individual species of trees have become important community character elements. Examples are the Canary Island Date Palms on Catalina south of Talbot, California Pepper trees on San Fernando, Washingtonia Palms and Monterey Cypress on West Point Loma Boulevard and Jacaranda trees on Rosecrans in the Roseville commercial district. Any tree or trees removed from these areas should be replaced on a one for one basis with a specimen tree of the same species. Mature trees should be replaced with a minimum 36-inch box tree.
- Street widening should be discouraged in order to maintain community scale, utilizing instead traffic engineering techniques to expand street capacity.
- Streets which are unimproved due to geographic or geologic constraints should be considered as pedestrian links between neighborhoods.
- Provide additional parking on Voltaire Street through the acquisition of parking lots or through on-street parking. New parking lots should be adequately screened to buffer parking fronting onto the commercial street as well as residential streets.
- Landscaping
  - Landscaping should be considered a major element of the character of established neighborhoods. A consistent and attractive neighborhood landscaping theme should be retained in new development.
  - Landscaping should be used to add texture to blank walls, soften edges and provide a sense of pedestrian scale.

- Existing trees should be preserved where possible. New development should be sited and designed to mitigate any harmful impacts to major trees or any significant mature vegetation which is a major asset to Peninsula's residential neighborhoods. Where removal is unavoidable, replacement landscaping should be provided on-site.
- In areas of little or no activity, ground covers or lawns should be planted as alternative to paving.
- At key locations, specimen trees can become community focal points. Such trees should be designated as community resources and protected.

## 7. Hillside Development

Steep sloped areas which retain natural topography and native vegetation should remain in their natural state. Hillside areas of the Peninsula community which meet the City's adopted guidelines for application of the Hillside Review (HR) Overlay Zone should be placed within the HR Overlay Zone. These areas are indicated on **Figure 26**. Placing these areas in the HR Overlay Zone will help to preserve the natural topography and native vegetation in the few remaining natural areas within Peninsula.

The following criteria should be used as a guide for new development in the hilly sections of Peninsula. These criteria are particularly appropriate and critically important on properties with slopes exceeding 25 percent.

- Design structures to adapt to hillside conditions and not require standard prepared pads. This includes a minimization of earthmoving, creative architectural solutions in land preparation and selection of appropriate foundation types.
- The structural quality of the soil should determine the type of construction. This includes respecting the site conditions of steepness, soil characteristics and hydrology to ensure hillside stability both during and after construction.
- The natural surface drainage system should be maintained. The way in which changes to the natural landform or its surface conditions affects the natural drainage system should be determined prior to land preparation. This will help eliminate unforeseen problems of erosion, landslides or damage to existing plant and animal life.
- Structures should be clustered to emphasize the existing topography and conserve existing resources. Clustering permits higher densities while maintaining appropriate scale and identifiable open space.
- The amount of surfaced ground cover should be limited. The design of such site surfaces as structure foundations, driveways, patios, sidewalks, and roads should support and not impact the natural system of drainage. Clustering techniques and special ground cover respond to this requirement.

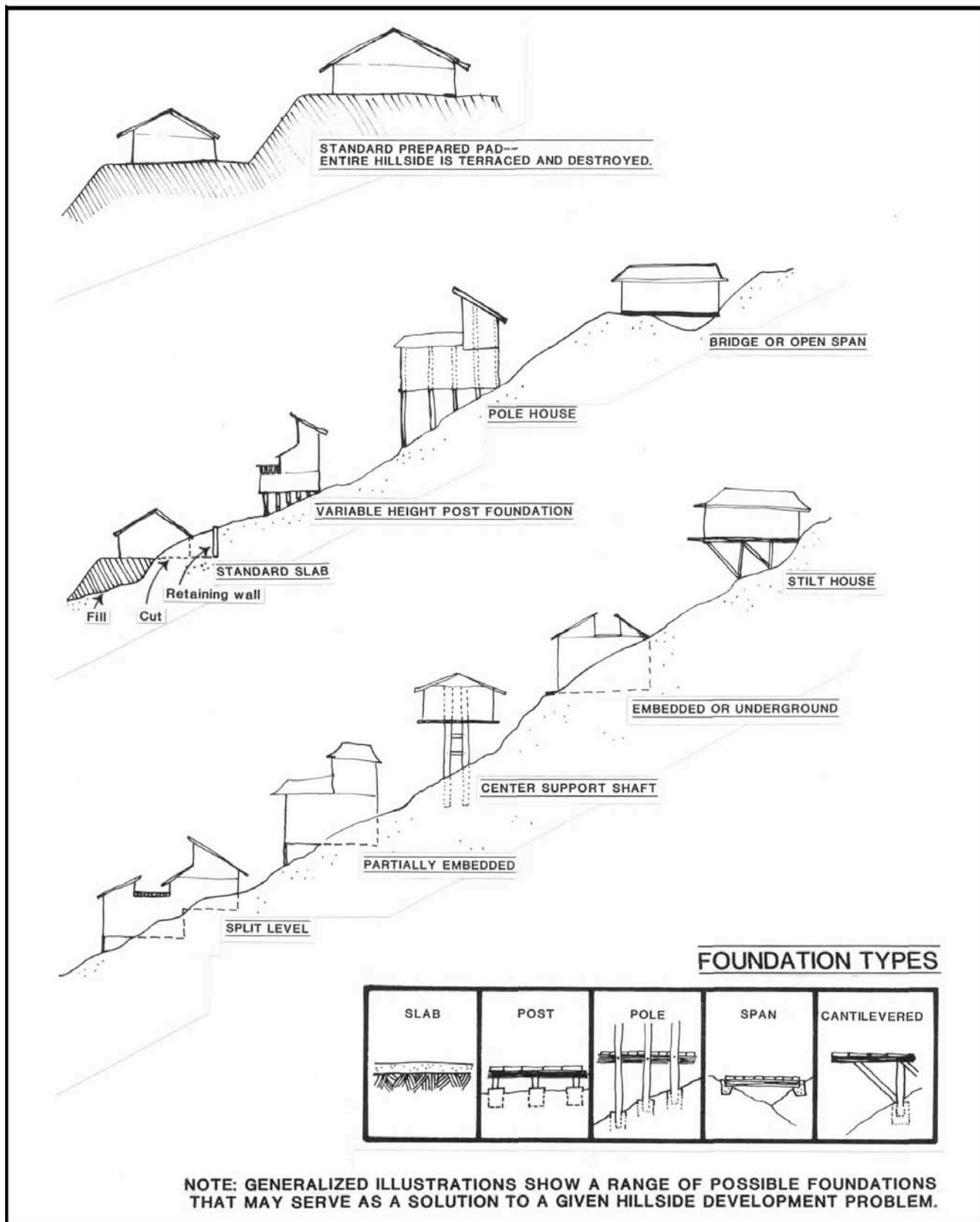
- Design of development should relate to existing topography and landscape features. The incorporation of existing features into project design can perpetuate or create unique environmental conditions.
- Meet large parking requirements with multiple small parking areas and screen them with planting berms and terraces. This allows better adaptation to terrain for the road system while providing needed parking and through traffic capacity.
- The scale and character of the buildings should be compatible with the typical irregular form of the hillside terrain. The use of irregular architectural edges to interlock structures with the hillside vegetation is often an effective technique.
- Existing plant cover and/or tree pattern should be considered when landscaping a site. Existing eroded hillsides should be planted with native materials and graded slopes should be planted in a similar fashion.
- The treatment of rooftops should be varied. Rooftops should be carefully designed to compliment rather than obliterate hillside forms. Large continuous and unbroken roof areas should be avoided since they will result in high visual prominence that detracts from the visual enjoyment of natural forms.

## Commercial Guidelines

### 1. Building Scale

Building scale is a quality which describes the relationships of buildings to each other and to human dimensions. Development within Peninsula cannot exceed 30 feet in height. However, the majority of existing commercial development within the community is old and characterized by small, low-scale, one-story structures. New buildings with a height of 30 feet could be out of scale with this established character. It is, therefore, important that new development compliment the scale of existing development to avoid abrupt and negative changes to the established development character.

- New buildings should be compatible with the scale and character of the surrounding development.
- Abrupt differences in scale between large commercial buildings and adjacent residential areas should be avoided. Gradual transitions in scale are preferred.
- Extreme contrasts in color, shape and organization of architectural elements will cause new buildings to stand out in excess of their public importance. Where visually strong buildings clash with their surroundings, the character of the area will be adversely affected.
- New buildings should be designed to compliment the scale, form and proportion of older development. However, duplication should be avoided.



## Foundation Types for Hillside Development

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FIGURE 30

- Larger structures should be designed to reduce actual or apparent bulk. This can be achieved by pitched roof designs, separating large surface masses through changes in exterior treatment and various other architectural techniques. Landscaping can also be used to add texture to blank walls, soften edges and provide a sense of pedestrian scale.
- Roof spaces should be made available for people activities whenever possible. The flat roofs of terraced buildings are ideal for this purpose.
- Visually distracting roof appendages such as stairway towers or ventilation equipment should be screened from public view.
- Originality and diversity in architectural design should be encouraged. However, the overall streetscape pattern should be considered.

## 2. Pedestrian Activity

### Paths

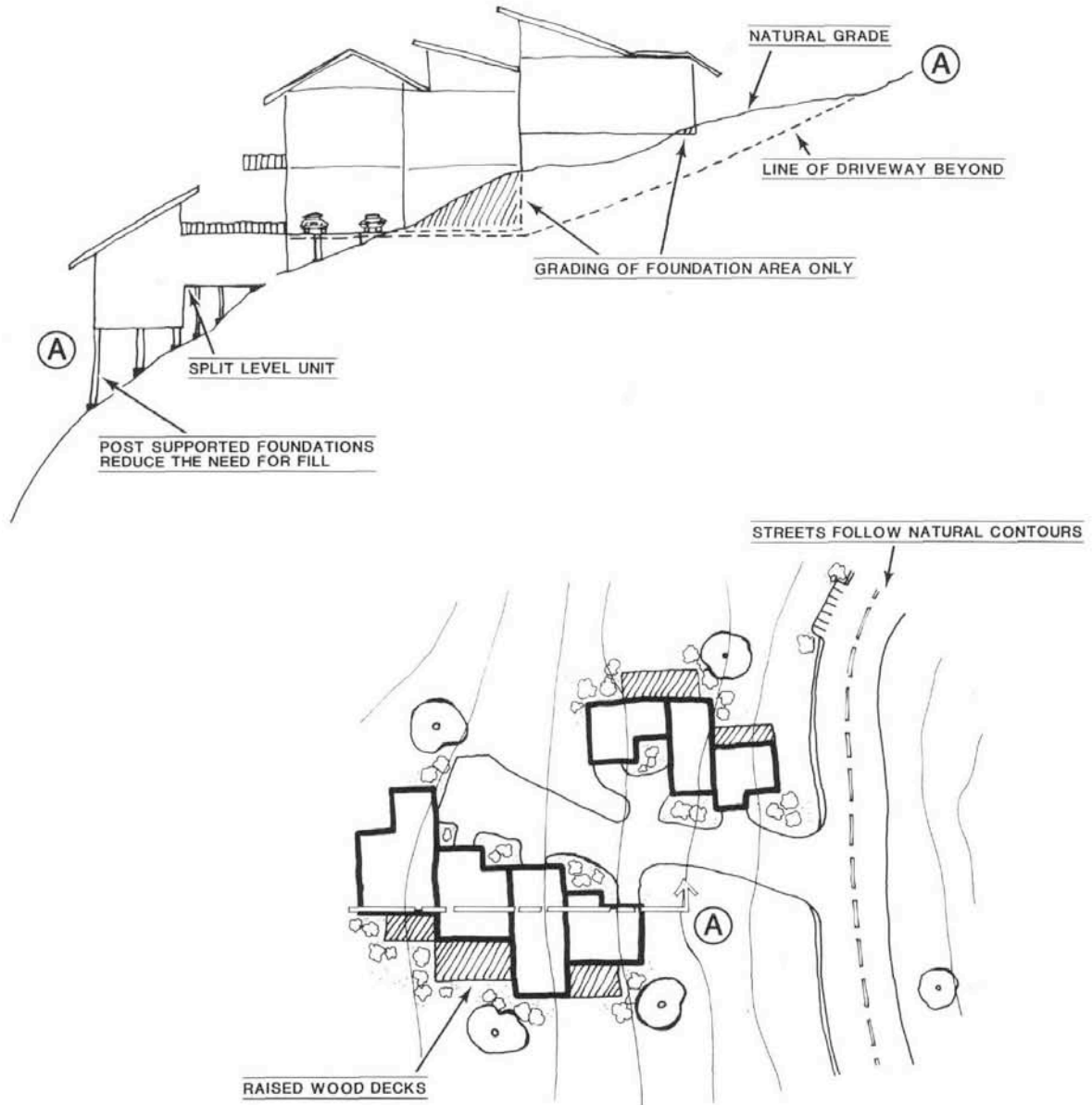
Paths are collectors of pedestrians and connectors of places.

- Streets and paths often connect the same places but the separation of the two systems should occur where possible.
- Trees, lighting bollards or sidewalk textures should be encouraged along major streets to separate pedestrian paths from traffic conflicts.
- Recreational values of paths should be increased by providing rest stops and focal points of activity. The use of interest nodes; a fountain, chain of shops, or plaza space, at reasonable walking distance can help entice pedestrian movement from one point to another.
- Pedestrian paths that provide series of views and a consistent variety of scenes should be encouraged. The discovery of interesting places along pedestrian paths contributes to the experience of serendipity and surprise.
- Storefront diversity adds to pedestrian accessibility, variety and choice along commercial paths.
- Clusters of shops around interior courtyards, or pedestrian malls extend the commercial frontage of the area and increase the diversity of the street.
- Window displays stimulate interest along paths and encourage pedestrians to stop and browse.



REDUCES CUT AND FILL REQUIREMENTS, WATERING REQUIREMENTS AND THE AREA OF IMPERVIOUS GROUND COVER. THIS RESULTS IN LESS INTERFERENCE WITH THE NATURAL DRAINAGE SYSTEM.

NOTE: POLE OR POST SUPPORTED FOUNDATIONS WILL FURTHER REDUCE THE AREA OF IMPERVIOUS GROUND COVER.



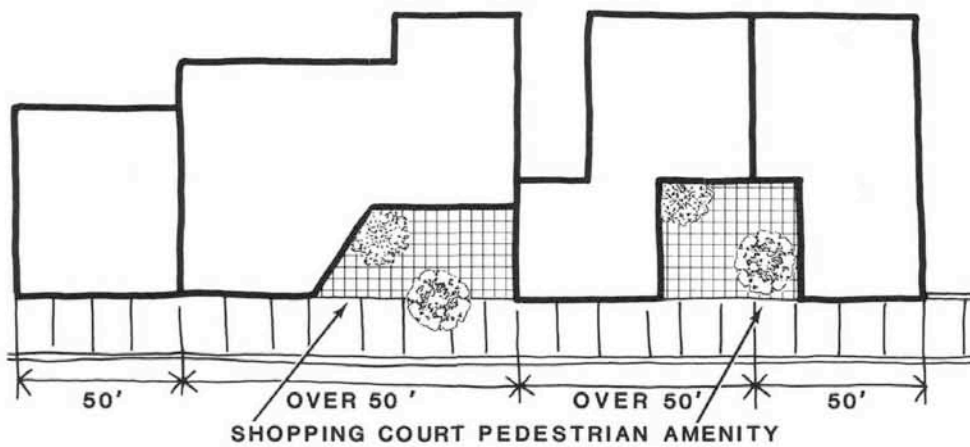
## Cluster Design Peninsula Community Plan

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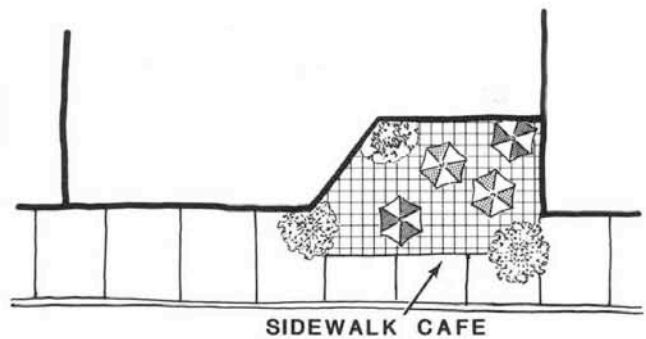
FIGURE 31



AWNINGS AND LANDSCAPING ARE BENEFICIAL TO THE PEDESTRIAN AND SERVE TO IMPROVE PEDESTRIAN ACTIVITY.



SHOPPING COURTYARDS, SIDEWALK CAFES AND DIVERSITY IN SHOPS AND FACADES CREATE A STRONG PEDESTRIAN ORIENTATION.



## Pedestrian Amenities in Commercial Areas

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FIGURE 32

- Various elements of climate control improve the pathway for the pedestrian and add color, visual variety and liveliness to the streetscape. Such elements include coverings, arbors, canopies, awnings, colonnades and arcades. Street trees providing broad canopies can also be used to create a sense of pedestrian scale adjacent to large buildings.
- Richly detailed facades, windows, entrances, stairways, retaining walls and other features are appreciated by the pedestrian and impart a sense of human scale.
- Various textures and paving treatments can be used to define and separate pedestrian paths. The coarseness of surface textures should not be so great as to create difficulty in walking or wheelchair movement.
- Curb cuts and driveway access across paths create conflicts between auto and pedestrian use. Where such conflicts cannot be avoided, the visual continuity of the pedestrian path can be maintained by continuing the pavement pattern across the driveway space. On commercial property, speed bumps and stop signs should be used to reduce hazards at blind auto egress points.
- Pavement textures can also be used at street crossings to clearly mark and improve the appearance of the intersection.

### Pedestrian Places

Pedestrian places are extensions of personal space. A place is formed when people establish boundaries and find recurring uses for a space. Activities are the focus of a space.

- Buildings should not dominate surrounding spaces but should step back or form enclosures in scale with pedestrians.
- Changes of level are an effective way to create interest and define pedestrian space.
  - Raised places give a sense of overlook and advantage to its occupants.
  - Lowered spaces create a sense of intimacy and enclosure.
  - Extreme changes in level impair the visual connection between the occupant and passerby, reducing interest for both.
- Spaces should have furnishings that enable multiple use by all age groups.
- The location of benches and their relationship to one another is of prime importance.
  - Benches should be arranged to promote contact and form interesting spaces to watch pedestrian activity.

- Views from fixed benches should not be obscured by walls, railing, poles, landscaping or other unnecessary obstructions.
- Alley ways are potential areas of activity and interest. The intimate scale and informality may offer a welcome contrast to the wider nearby streets.
- Planting and paving treatment in the alleys, coupled with active use in the adjacent buildings form in effect a commercial promenade.

### Continuity

- Uses requiring large frontages, such as banks, office buildings, parking lots and supermarkets can disrupt the continuity of a complex of small shops. Large development should be designed to maintain this continuity by creating additional pedestrian and landscaped areas along the street, providing display windows and breaking up large facades with architectural features.
- Residential units should be encouraged on the upper floors of commercial developments. The mixing of residential and commercial facilities provides a more efficient use of parking facilities and reduces dependence on the automobile.

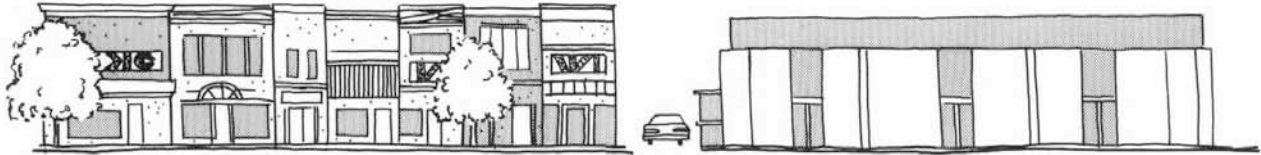
## 3. The Streetscape

### Landscaping

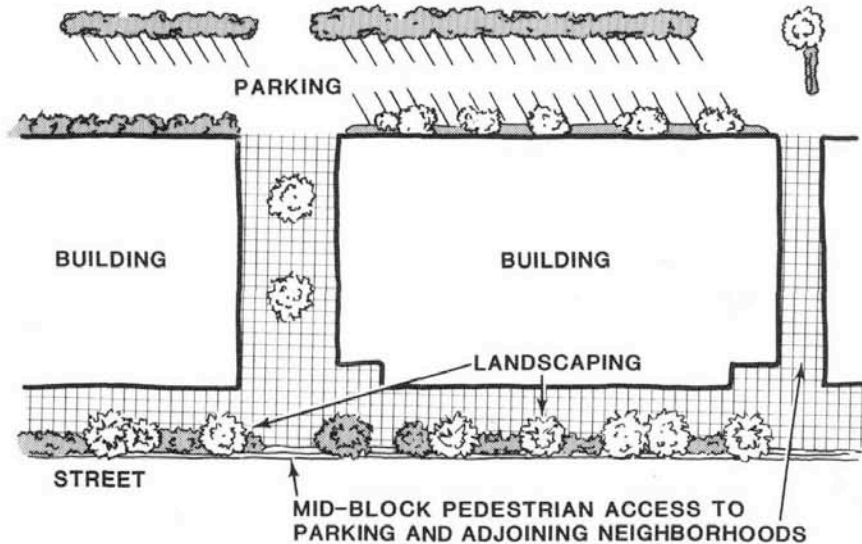
The use of appropriate landscaping materials and careful consideration of siting factors help to maintain a sense of nature in the urban environment. Landscaping exerts a major influence on the character of individual streets.

- Extensive tree plantings enhance the visual quality of public streets and provide a strong element of continuity as future development occurs.
- Landscaping themes for all major commercial streets in Peninsula should be studied and specific plans should be developed. Priority should be given to improving landscaping in the commercial areas as part of individual permit processing. Such plans should be reviewed by the community planning group before any implementing actions are taken.
- Existing street trees should be preserved. Where removal is unavoidable, replacement should be required nearby or at another appropriate location.
- Where feasible, tree massing should be carried into adjacent spaces both public and private.
- Shrub buffers should be provided and/or maintained on busy streets to protect pedestrians.
- In areas of little or no activity ground covers or lawns should be planted as an alternative to paving.

CONTINUITY AND COMPACTION ARE A CONTINUATION OF OUR CONCERN FOR PEDESTRIAN SCALE. COMPACTION IS NECESSARY IN ORDER TO PLACE THE ESSENTIAL ELEMENTS CLOSE ENOUGH TO ONE ANOTHER TO INDUCE PEDESTRIAN MOVEMENT. CONTINUITY HAS TO DO WITH THE QUALITY OF COMPACTION--THAT THERE BE A FAIRLY CONTINUOUS STREAM OF ITEMS OF PEDESTRIAN INTEREST.



THE VITALITY OF SHOPPING DISTRICTS IS DEPENDENT UPON CONTINUITY. LARGE SCALE TENANTS SUCH AS BANKS, THEATRES AND SUPERMARKETS CAN OFTEN SEVER THIS CONTINUITY BEYOND HOPE. THE CONTINUITY OF STORE WINDOWS GIVES THE PEDESTRIAN NEW THINGS TO LOOK AT EVERY FEW FEET--INSERT A BANK AND THERE IS ONE OR TWO HUNDRED FEET OF NO MERCHANDISE AND FEW PEOPLE. IT IS NOT THE "BANKNESS" OF THE BANK THAT MAKES IT INCOMPATIBLE, BUT RATHER ITS LONG EXPANSE OF INACTIVE STREET FRONTAGE.



## Continuity

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FIGURE 33



## Pedestrian Access

Although pedestrian activity should be concentrated along the streets, mid-block pedestrian paths (across the alleys) should be provided whenever feasible to link the activity centers of the main commercial street to adjacent large development across the alley areas.

## Parking

While the automobile will continue to play a role in the circulation system of Peninsula, it is necessary to ensure that primary consideration be given to the needs of the pedestrian.

- Surfaced parking lots in front of building facades or on adjacent lots are visually disruptive, especially in areas of heavy pedestrian activity. Curb cuts and drive-throughs are not appropriate on pedestrian-oriented streets, such as along Rosecrans between Emerson and Talbot.
- Wherever feasible, parking should be located at the rear of buildings with ingress and egress from the alley. Also, parking areas should be linked through various properties in order to provide better and more efficient parking areas.
- Efforts to consolidate parking through the use of common alley spaces, landscaped parking lots or parking garages should be encouraged.
- Where front parking is unavoidable, surfaced parking areas should be screened from view. This can be accomplished through the use of landscaping features appropriate to the character of the area.
- Surfaced parking areas should be broken up with landscaping islands. Pavement patterns should also be encouraged to enhance visual interest and give multiple functions to the parking lot. Multiple functions of parking lots could provide for their use as urban plazas, exhibit areas or the visual enhancement of the space.
- Where surfaced parking area is inadequate, underground parking or parking structures may provide a viable alternative. Underground parking and parking structures should be considered as viable alternatives where the required parking would result in large paved no space for landscaping amenities.

## Signage

- Sign standards should be established for all commercial areas of the Peninsula community regulating the height, bulk and design of all such signs. Rooftop signs, tall freestanding signs, off-premise signs and billboards should not be permitted. All existing or new signs that do not conform to such standards should be abated within a reasonable period.

- Business signs should vary according to function and location. Dignified and well maintained signs, designed with respect for the scale and character of the street, enhance the overall quality of commercial areas.
- Low-profile ground signs, wall signs, and projecting signs in scale with pedestrians are preferred.
- Symbols and maps should be used wherever practical to identify transit routes, pathway networks, important facilities and visitor information.
- Attractive kiosks and small bulletin boards, which give current information and identify coming events, should be located and maintained at key pedestrian nodes.
- Decorative graphics can be used to enhance the appearance of blank walls, provided they are tastefully done and in harmony with the character of the street.

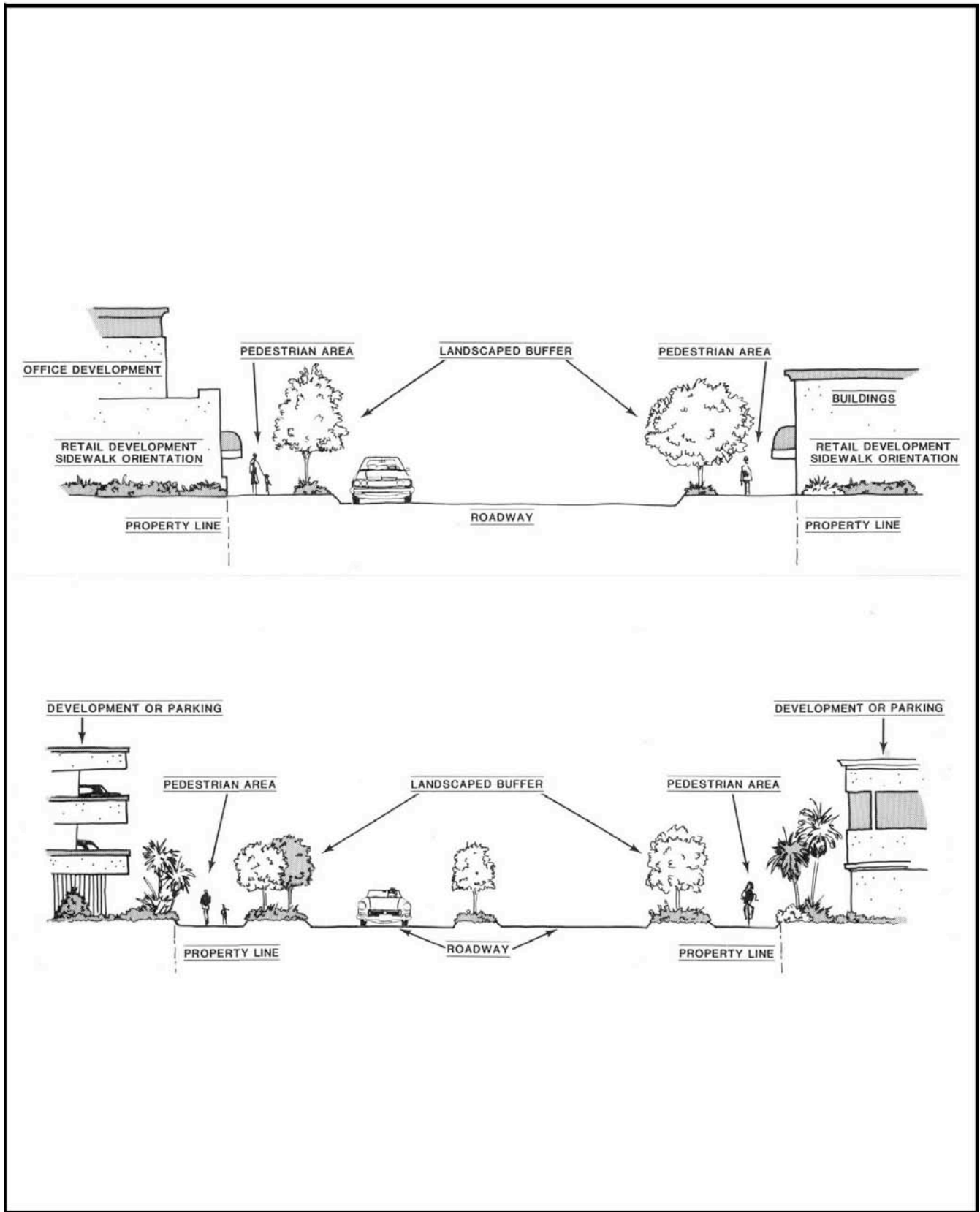
### Lighting

- Light quality should be geared to the specific use of the areas, spaces, and forms to be illuminated.
- Lighting should reflect the relative importance of pedestrian spaces by degrees of illumination at night and by the repetition, and scale of lighting standards during the day.
- Lamps and standards should be for people not cars. New lamps and standards should continue the existing design.
  - The height of standards should vary between ten and fifteen feet.
  - Lamps should be placed beside walks, not above the roadway.
  - Bollard-type lighting is suited for pedestrian paths.
- Spot lighting may be used to illuminate important design elements such as trees, furniture, graphics, and public spaces, but harsh or garish lighting should be avoided.

### **Area-Specific Recommendations**

- Redevelop NTC in a manner that is compatible with the adjacent residential community, provides public open space and recreational opportunities, respects the heritage of the base, and follows the established circulation systems. Development should occur in accordance with the NTC Precise Plan.
- The City should communicate its aesthetic and view preservation concerns to the Navy in order to maximize the visual aesthetics of the Peninsula. The Naval Training Center as viewed from Nimitz Boulevard and North Harbor Drive, is an example of an area that could benefit from landscaping and design improvements.

- Linkages connecting the Roseville district on either side of Rosecrans should be developed in order to increase continuity and pedestrian scale in this area. A design theme, emphasizing the area's marine orientation, should be established along with such features as coordinated street furniture and decorative paving (e.g., sidewalks, crosswalks).
- Redevelopment and infill development in that portion of the Roseville commercial district adjacent to Shelter Island should relate to and expand upon the nautical and South Seas architectural, signage and tropical landscaping themes of Shelter Island.
- The pedestrian character of the Roseville commercial area should be enhanced in any new development occurring in this area. This can be accomplished by locating new buildings adjacent to sidewalks and locating surface parking areas to the side or rear of buildings. Surface parking lots in front of building facades should be well buffered by landscaping.
- Canopies and arcades should be used at intervals to provide a pedestrian scale. Large frontages such as banks and office buildings should be broken up by use of vertical offset windows, changes in exterior treatment and other architectural treatments. Only low profile signs should be used in this area. Benches, kiosks and other street furniture which provide pedestrian amenities should be included on all new developments. Additional plantings of jacaranda trees on Rosecrans should be encouraged. An additional tree variety should be planted on Cannon Street and other side streets in the Roseville Commercial Core.
- City Council should consider allocating funding for development of a planned district ordinance in the Roseville commercial district. A planned district ordinance could require new or remodeled commercial developments to conform to urban design standards specifically tailored to foster compatible infill development and an enhanced commercial district environment in Roseville.
- Improvements to the Sunset Cliffs Shoreline Park and street endings in the La Playa/Kellogg Beach area should encourage the development of vista points. The Coastal Conservancy should be considered in this regard.
- Unimproved street ending should be improved as pedestrian linkages and visual corridors. Specific areas of emphasis should include the Roseville area east of Scott and the La Playa/Kellogg Beach area. Any improvements to street endings which occur on tidelands must conform to the Port Master Plan.
- Infill development in the Loma Portal and Roseville neighborhoods should be compatible with the Spanish Colonial Mediterranean revival and Modern architectural styles and pastel colors which dominate this neighborhood.



**Streetscape Sections**  
**Peninsula Community Plan**

CITY OF SAN DIEGO PLANNING DEPARTMENT

**FIGURE 34**

- New development in the very low-density "wooded area" and La Playa neighborhoods should preserve all trees with a caliper of eight inches or greater and other significant vegetation other than that lying directly on the building pad of the proposed development. Where feasible trees on the building pad should be transplanted elsewhere on site.
- Nimitz Boulevard should be upgraded to provide an additional positive entryway into Peninsula. Funding could come from individual property owners along the Boulevard, civic groups and City sources if available. Landscaping the median and sides of Nimitz should be installed where adequate right-of-way exists. Bicycle and pedestrian paths should be separated from auto traffic where feasible. A community identification sign should be erected at the north end of Nimitz where it enters Peninsula.
- The following street tree plans have been developed for the Peninsula Community. Please refer to the Street Tree Districts for guidance in the particular areas throughout the community. For tree listings along major corridors, please refer to the street tree corridors chart below. For guidance on tree selections in the Roseville area, please refer to page 123.



**STREET TREE DISTRICTS**  
(Refer to Figure 34a)

Residential Tree Districts					Tree Form	Type	Notes
A	B	C	D	E	TREE: Common Name/ <i>Botanical Name</i>		
<b>SMALL CANOPY TREE FORM</b>							
	Π				Purple-leaf Plum/ <i>Prunus cerasifera</i>	D/F	2,3
	Π	Π			“St. Mary’s” Magnolia/ <i>Magnolia grandiflora</i>	E/F	
	Π		Π		Gold Medallion/ <i>Cassia leptophylla</i>	E/F	2,3
			Π		Hong Kong Orchid/ <i>Bauhinia blakeana</i>	D/F	2,3
	Π			Π	Bronze Loquat/ <i>Eriobotrya deflexa</i>	E/F	2,3
	Π	Π			New Zealand Christmas/ <i>Metrosideros excelsus</i>	E/F	2,3
			Π		Bradford Pear/ <i>Pyrus calleryana</i>	D	2,3
	Π	Π			Coral Gum/ <i>Eucalyptus torquata</i>	E	2
<b>LARGE CANOPY TREE FORM</b>							
			Π	Π	Fruitless Olive/ <i>Olea europaea</i>	E	
				Π	London Plane/ <i>Platanus acerifolia</i>	D	
	Π			Π	Red Cap Gum/ <i>Eucalyptus erythrocorys</i>	E	
	Π				Chinese Flame/ <i>Koelreuteria bipinnata</i>	D/F	
			Π	Π	Red Gum/ <i>Eucalyptus ficifolia</i>	E/F	
		Π			Cape Chestnut/ <i>Calodendrum capense</i>	E	
	Π	Π			Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	
	Π				African Fern Pine/ <i>Podocarpus gracilior</i>	E	
				Π	Holly Oak/ <i>Quercus ilex</i>	E	
				Π	Cork Oak/ <i>Quercus suber</i>	E	
	Π	Π			Tipu Tree/ <i>Tipuana tipu</i>	D/F	
<b>UPRIGHT CANOPY TREE FORM</b>							
	Π		Π	Π	Canary Island Pine/ <i>Pinus canariensis</i>	E	
	Π		Π	Π	Paperbark/ <i>Melaleuca quinquenervia</i>	E	
	Π	Π			Sweet Shade/ <i>Hymenosporum flavum</i>	E/F	
			Π	Π	Red Ironbark/ <i>Eucalyptus sideroxylon</i>	E	
	Π	Π			Australian Willow/ <i>Geijera parvifolia</i>	E	3
	Π			Π	Brisbane Box/ <i>Tristania conferta</i>	E	
<b>PALM TREE FORM</b>							
	Π	Π	Π	Π	Queen Palm/ <i>Syagrus romanzoffianum</i>	Feather	2
	Π	Π	Π	Π	King Palm/ <i>Archontophoenix cunninghamiana</i>	Q	2
	Π	Π	Π	Π	Mexican Fan Palm/ <i>Washingtonia robusta</i>	M	2

## **STREET TREE DISTRICT KEY**

### Peninsula Street Tree Districts

- Roseville
- Loma Portal, Palisades, Alta
- La Playa
- Pt. Loma Highlands, Sunset Cliffs, Fleetridge
- Woodlands

### Legend

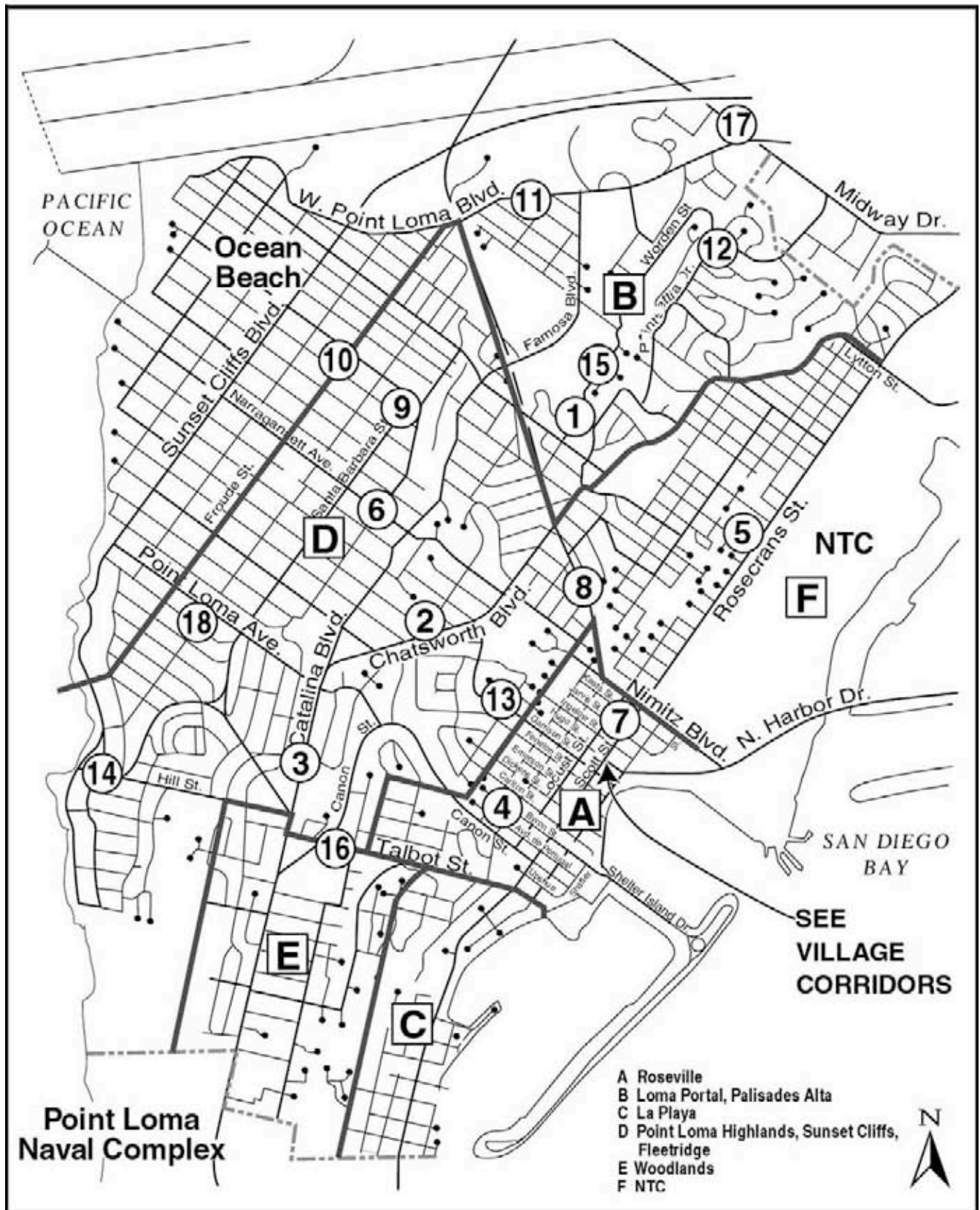
- D. Deciduous
- E. Evergreen
- F. Flowering
- Q. Feather Palm
- M. Fan Palm

### Notes

1. Requires special approval.
2. Acceptable tree in public view corridor.
3. Acceptable tree under power lines.

### General Criteria

- On all streets where curb to property line is less than 10 feet, street trees should typically be located on private property.
- Size of all required trees to be minimum 24-inch box. Palms to be a minimum of 8 ft. Brown Trunk Heights (B.T.H.).
- All street tree selections shall be consistent with the City of San Diego tree selection guide relative to parkway width, root barriers, overhead utility wires, special installation, etc.
- All street tree planting installations require a “No Fee Street Tree Permit,” and approval by the City of San Diego’s Urban Forester. Tree watering to establish tree is the responsibility of the property owner. Tree pruning after establishment of the tree is the responsibility of the owner.
- All plant material shall be installed per the standards of the *Landscape Technical Manual*
- Coordinate trash receptacles, benches, and planters at bus stops and along bus routes.



**Street Tree Plan**  
**Peninsula Community Plan**

CITY OF SAN DIEGO PLANNING DEPARTMENT

FIGURE 34a

**STREET TREE CORRIDORS**  
(Refer to Figure 34a)

**CORRIDOR**

Segment/Nodes	TREE: Common Name/Botanical Name	Type	Form	Note
<b>1 VOLTAIRE STREET</b>				
Mendocino Boulevard to Chatsworth Boulevard	Raywood Ash/ <i>Fraxinus Oxycarpa</i> Hong Kong Orchid/ <i>Bauhina Blakeana</i>	D/F D/F	LC SC	3
<b>2 CHATSWORTH BOULEVARD/LYTTON STREET</b>				
Rosecrans Street to Catalina Boulevard	Tabebuia/ <i>Tabebuia impetiginosa</i>	D/F	SC	
<b>Nodes:</b>				
Lytton Street at Rosecrans Street	Gold Medallion/ <i>Cassia leptophylla</i>	E/F	SC	3
Chatsworth Boulevard at Voltaire Street	Gold Medallion/ <i>Cassia leptophylla</i>	E/F	SC	3
Chatsworth Boulevard at Nimitz Boulevard	Gold Medallion/ <i>Cassia leptophylla</i>	E/F	SC	3
Chatsworth Boulevard at Catalina Boulevard	Gold Medallion/ <i>Cassia leptophylla</i>	E/F	SC	3
Chatsworth Boulevard at Narragansett Avenue	Gold Medallion/ <i>Cassia leptophylla</i>	E/F	SC	3
<b>3 CATALINA BOULEVARD/FAMOSA BOULEVARD</b>				
Voltaire Street to Pt. Loma Avenue	Tabebuia/ <i>tabebuia impetiginosa</i>	D/F	SC	
<b>Nodes:</b>				
Catalina Boulevard at Chatsworth Boulevard	Gold Medallion/ <i>Cassia leptophylla</i>	E/F	SC	3
Catalina Boulevard at Narragansett Avenue	Gold Medallion/ <i>Cassia leptophylla</i>	E/F	SC	3
Catalina Boulevard at Pt. Loma Avenue	Gold Medallion/ <i>Cassia leptophylla</i>	E/F	SC	3
Catalina Boulevard at Talbot Street	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	
Catalina Boulevard at Santa Barbara Street/Hill Street	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	

**STREET TREE CORRIDORS (continued)**  
(Refer to Figure 34a)

**CORRIDOR**

Segment/Nodes	TREE: Common Name/Botanical Name	Type	Form	Note
<b>4 CANNON STREET</b>				
Pt. Loma Avenue to Evergreen Street	London Plane Tree or Sycamore/ <i>Platanus acerifolia/racemosa</i>	D	UC	
Evergreen Street Rosecrans Street	Gold Medallion/ <i>Cassia leptophylla</i>	E/F	SC	3
<b>Node:</b>				
Cannon Street at Talbot Street	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	4
<b>5 ROSECRANS STREET</b>				
Evergreen Street/Sellers Drive to Kellogg Street	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	
<b>Nodes:</b>				
Rosecrans Street at Lytton Street	Gold Medallion/ <i>Cassia leptophylla</i>	E/F	SC	3,5
Rosecrans Street at Nimitz Boulevard	Jacaranda/ <i>Jacaranda mimosifolia</i>	E/F	SC	3
Rosecrans Street at Canon Street	Gold Medallion/ <i>Cassia leptophylla</i>	E/F	SC	3,6
Rosecrans Street at Talbot Street	Gold Medallion/ <i>Cassia leptophylla</i>	E/F	SC	3,4
<b>6 NARRAGANSETT AVENUE/LOWELL STREET/MACAULAY STREET</b>				
Froude Street to Macaulay Street	Queen Palm/ <i>Syagrus romanzoffianum</i> <i>Tabebuia/Tabebuia impetiginosa</i>	Q E/F	P SC	2
<b>Nodes:</b>				
Narragansett Avenue at Santa Barbara	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	
Narragansett Avenue at Catalina Boulevard	Gold Medallion/ <i>Cassia leptophylla</i>	E/F	SC	3
Narragansett Avenue at Chatsworth Boulevard	Gold Medallion/ <i>Cassia leptophylla</i>	E/F	SC	3
<b>7 LOCUST STREET</b>				
Canon Street to Nimitz Boulevard	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	
<b>Node:</b>				
Locust Street at Canon Street	Gold Medallion/ <i>Cassia leptophylla</i>	D/F	SC	3



**STREET TREE CORRIDORS (continued)**  
(Refer to Figure 34a)

**CORRIDOR**

Segment/Nodes	TREE: Common Name/Botanical Name	Type	Form	Note
<b>8 NIMITZ BOULEVARD</b>				
West Point Loma Boulevard to Harbor Drive	Jacaranda/Jacaranda mimosifolia	D/F	LC	
<b>Nodes:</b>				
Nimitz Boulevard at West Point Loma Boulevard	Coral Trees/Erythrina caffra	D/F	LC	
Nimitz Boulevard at Lowell Street	Gold Medallion/Cassia leptophylla	E/F	SC	3
Nimitz Boulevard at Rosecrans Street	Gold Medallion/Cassia leptophylla	E/F	SC	3
Nimitz Boulevard at Harbor Drive	Gold Medallion/Cassia leptophylla	E/F	SC	3
<b>9 SANTA BARBARA STREET</b>				
Cape May Avenue to Catalina Boulevard	Queen Palm/Syagrus romanzoffianum	Q	P	2
	Gold Medallion/Cassia leptophylla	E/F	SC	3
<b>Nodes:</b>				
Santa Barbara Street at Narragansett Avenue	Coral Trees/Erythrina caffra	D/F	LC	
Santa Barbara Street at Catalina Boulevard/Hill Street	Gold Medallion/Cassia leptophylla	D/F	LC	3
<b>10 FROUDE STREET</b>				
Sunset Cliffs Boulevard to West Point Loma Boulevard	Queen Palm/Syagrus romanzoffianum	Q	P	2
	Gold Medallion/Cassia leptophylla	E/F	SC	3
<b>11 WEST POINT LOMA BOULEVARD</b>				
Nimitz Boulevard to Midway Drive	London Plane Tree or Sycamore/ <i>Platanus acerifolia</i>	D	LC	5
<b>Nodes:</b>				
West Point Loma Boulevard at Nimitz Boulevard	Coral Trees/Erythrina caffra	D/F	LC	6
West Point Loma Boulevard at Midway Drive	Coral Trees/Erythrina caffra	D/F	LC	5,6
<b>12 POINSETTIA DRIVE</b>				
Leland Street to Alcott Street	Gold Medallion/Cassia leptophylla	E/F	SC	3
<b>Node:</b>				
Poinsettia Drive at Leland Street	Jacaranda/Jacaranda mimosifolia	D/F	LC	

**STREET TREE CORRIDORS (continued)**  
(Refer to Figure 34a)

**CORRIDOR**

<b>Segment/Nodes</b>	<b>TREE: Common Name/Botanical Name</b>	<b>Type</b>	<b>Form</b>	<b>Note</b>
<b>13 GARRISON STREET</b>				
Chatsworth Boulevard to Rosecrans Street	Tabebuia/ <i>Tabebuia impetiginosa</i>	D/F	SC	
Garrison Street at Locust Street	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	
Garrison Street at Rosecrans Street	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	
<b>14 HILL STREET</b>				
Catalina Boulevard to Sunset Cliffs Boulevard	Queen Palm/ <i>Syagrus romanzoffianum</i> Tabebuia/ <i>Tabebuia impetiginosa</i>	Q E/F	P SC	2
<b>Node:</b>				
Hill Street at Santa Barbara Street and Catalina Boulevard	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	
<b>15 WORDEN STREET</b>				
Voltaire Street to Leland Street	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	
<b>16 TALBOT STREET</b>				
Catalina Boulevard to Rosecrans Street	Tabebuia/ <i>tabebuia impetiginosa</i> Queen Palm/ <i>Syagrus romanzoffianum</i>	E/F Q	LC P	
<b>Nodes:</b>				
Talbot Street at Catalina Boulevard	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	
Talbot Street at Cannon Street	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	
Talbot Street at Rosecrans Street	Gold Medallion/ <i>Cassia leptophylla</i>	D/F	SC	3
<b>17 MIDWAY DRIVE</b>				
<b>Node:</b>				
Midway Drive at West Point Loma Boulevard	Coral Trees/ <i>Erythrina caffra</i>	D/F	LC	5
<b>18 POINT LOMA AVENUE</b>				
Canon Street to Froude Street	Tabebuia/ <i>tabebuia impetiginosa</i>	D/F	SC	
<b>Node:</b>				
Point Loma Avenue at Catalina Boulevard	Gold Medallion/ <i>Cassis leptophylla</i>	D/F	SC	3

## VILLAGE CORRIDORS

Segment/Nodes	TREE: Common Name/Botanical Name	Type	Form	Note
<b>AVENIDA DE PORTUGAL</b>				
Rosecrans Street to Bay	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	8
<b>CANON STREET</b>				
Rosecrans Street to Bay	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	8
<b>CARLETON STREET</b>				
Locust Street to Bay	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	8
<b>DICKENS STREET</b>				
Locust Street to Bay	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	8
<b>Node:</b>				
Dickens at Scott Street	Fern Pine/ <i>Podocarpus gracilior</i>	E	LC	
<b>EMERSON STREET, FENELON STREET, GARRISON STREET, INGELOW STREET, JARVIS STREET, KEATS STREET</b>				
Locust Street to Bay	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	7,8
<b>NIMITZ BOULEVARD</b>				
Lowell Street to Rosecrans	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	6,7,8
North side of Nimitz Boulevard. from Rosecrans Street to North Harbor Drive	Canary Island Pine/ <i>Pinus canariensis</i>	C	UC	6,7
South side of Nimitz Boulevard. from Rosecrans Street to North Harbor Drive	Jacaranda/ <i>Jacaranda mimosifolia</i>	E/F	LC	7,8
<b>NORTH HARBOR DRIVE</b>				
Rosecrans Street to Nimitz Boulevard	Existing Monterey Pines to be replaced with King Palms / <i>Archontophoenix</i> <i>cunninghamiana</i> Coral Tree/ <i>erythrina</i> <i>corraloides</i> in Median	Q E/F	P LC	6 6 6
<b>ROSECRANS STREET</b>				
Nimitz Boulevard to Talbot Street	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	7
<b>SCOTT STREET</b>				
Talbot Street to Emerson Street and Dickens Street to Nimitz Boulevard	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	7,8
Emerson Street to Dickens Street	Fern Pine/ <i>Podocarpus gracilior</i>	E	LC	1,6,7
<b>Node:</b>				
Scott Street at Dickens Street	Fern Pine/ <i>Podocarpus gracilior</i>	E	LC	1,6,7

## VILLAGE CORRIDORS (continued)

Segment/Nodes	TREE: Common Name/Botanical Name	Type	Form	Note
<b>SHAFTER STREET</b>				
Upshur Street to Carleton Street	Jacaranda/ <i>Jacaranda mimosifolia</i>	D	LC	7,8
<b>SHELTER ISLAND</b>				
Rosecrans Street to Anchorage Lane	Queen Palm/ <i>Syagrus romanzoffianum</i>			
<b>TALBOT STREET</b>				
Rosecrans Street to Anchorage Lane	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	7,8
<b>UPSHUR STREET</b>				
Rosecrans Street to Shafter Street	Jacaranda/ <i>Jacaranda mimosifolia</i>	D/F	LC	7, 8

## STREET TREE CORRIDOR KEY

### Types

D Deciduous  
 E Evergreen  
 F Flowering  
 M Fan Palms  
 Q Feather Palms  
 C Conifer

### Form

SC Small Canopy  
 LC Large Canopy  
 UC Upright Canopy  
 P Palm Species

### Notes

1. Requires special approval.
2. Acceptable tree in public view corridor.
3. Acceptable tree under power lines.
4. Refer to the Village Corridors.
5. Refer to the Midway Community Plan.
6. Established Street Tree.
7. Root Pruning required on New Trees.
8. Where Jacaranda cannot be planted, use Gold Medallion.

### General Criteria

- On all streets where existing curb to property line is less than ten feet, street trees should typically be located on private property.
- Size of all required trees to be minimum 24-inch box. Palms to be a minimum of eight ft. Brown Trunk Height (B.T.H.).
- All street tree selections shall be consistent with the City of San Diego tree selection guide relative to parkway width, root barriers, overhead utility wires, special installation, etc.
- All street tree planting installations require a “No Fee Street Tree Permit,” and approval by the City of San Diego’s Urban Forester. Tree watering to establish tree is the responsibility of the property owner. Tree pruning after establishment of the tree is the responsibility of the City.
- All plant material shall be installed per the standards of the *Landscape Technical Manual*.
- Coordinate trash receptacles, benches, and above-grade planters at bus stops and along bus routes.
- Special landscape treatment areas occur at nodes encompassing major intersections, important civic and commercial locations and entryways.

## **Voltaire Street CPIOZ Guidelines**

The Voltaire Street commercial district is characterized by small-scale development that is in need of façade, landscape and parking improvements. Residential development interrupts and surrounds the commercial area, which consists of retail stores built up to the property line and automobile oriented commercial uses (fast food restaurants, food markets, gas station and car wash) surrounded by parking lots and driveways. There are also a few professional office buildings. Despite the regular stream of traffic that moves along Voltaire Street, existing single-story shops provide a pedestrian orientation on some blocks and help to enhance the identity of the street.

The commercially-designated land along Voltaire Street is shown on **Figure 35a** and extends west from Chatsworth Boulevard to the area surrounding the intersection of Mendocino Boulevard, Voltaire Street and Venice Street. The CPIOZ, Type B, shall be applied to this commercial corridor. The purpose of this overlay zone is to enhance the quality of development and strengthen the identity of Voltaire Street by providing additional guidelines not specified by the underlying, predominantly CC, zoning. As described below, the issues to be addressed for the required CPIOZ Type B permit include signage, building placement, pedestrian and neighborhood orientation, landscaping, streetscape improvements, exterior building materials, vehicular access and parking.

### Signage

Signs are important elements of the streetscape and should convey a sense of permanence to symbolize each merchant's commitment to the community. Therefore, the use of plastic materials on signage for the Voltaire CPIOZ area shall be restricted to interior mechanisms. The utilization of plastic facing on signage shall be prohibited. The use of free standing and pole signs shall not be allowed except where it is demonstrated that such signs will enhance the identity of the commercial area (i.e., by being integrated into a project's overall design concept). In addition, the sign regulations of the coastal zone shall be adhered to throughout the Voltaire commercial area.

### Building Placement and Pedestrian Orientation

Buildings shall be oriented to the street, where physically feasible, in order to promote pedestrian accessibility and activity. The building walls facing streets should have a minimum continuous height of 13 feet. Single-story structures should not exceed 18 feet in height, two-story development should be limited to 24 feet, and three-story development must observe the 30-foot statutory height limit mandated by Proposition D. It is required that new developments provide at least one pedestrian entrance into or through the structure from each street frontage. A minimum of 40 percent of the area of all first-story commercial street walls facing Voltaire Street shall be transparent with visibility into a commercial use area. Non-reflective windows and openings shall be utilized. Building facades should be articulated with expression lines between floors, punched windows, window awnings and use of high quality, long-lasting materials such as stone, masonry or wood on the ground floor. Building walls adjacent to the residential development shall be in scale with the surrounding development.

On larger lots, new development should incorporate courtyards or street plaza spaces. A five-foot setback from the property line is recommended for all new development within the CC zone to allow for larger pedestrian spaces and landscaping along Voltaire Street. Special street lights such as the “double acorn” style historic light should be incorporated along the street (see illustration). All refuse storage facilities and rooftop mechanical equipment shall be concealed and screened from public view.

### Landscape and Streetscape Improvements

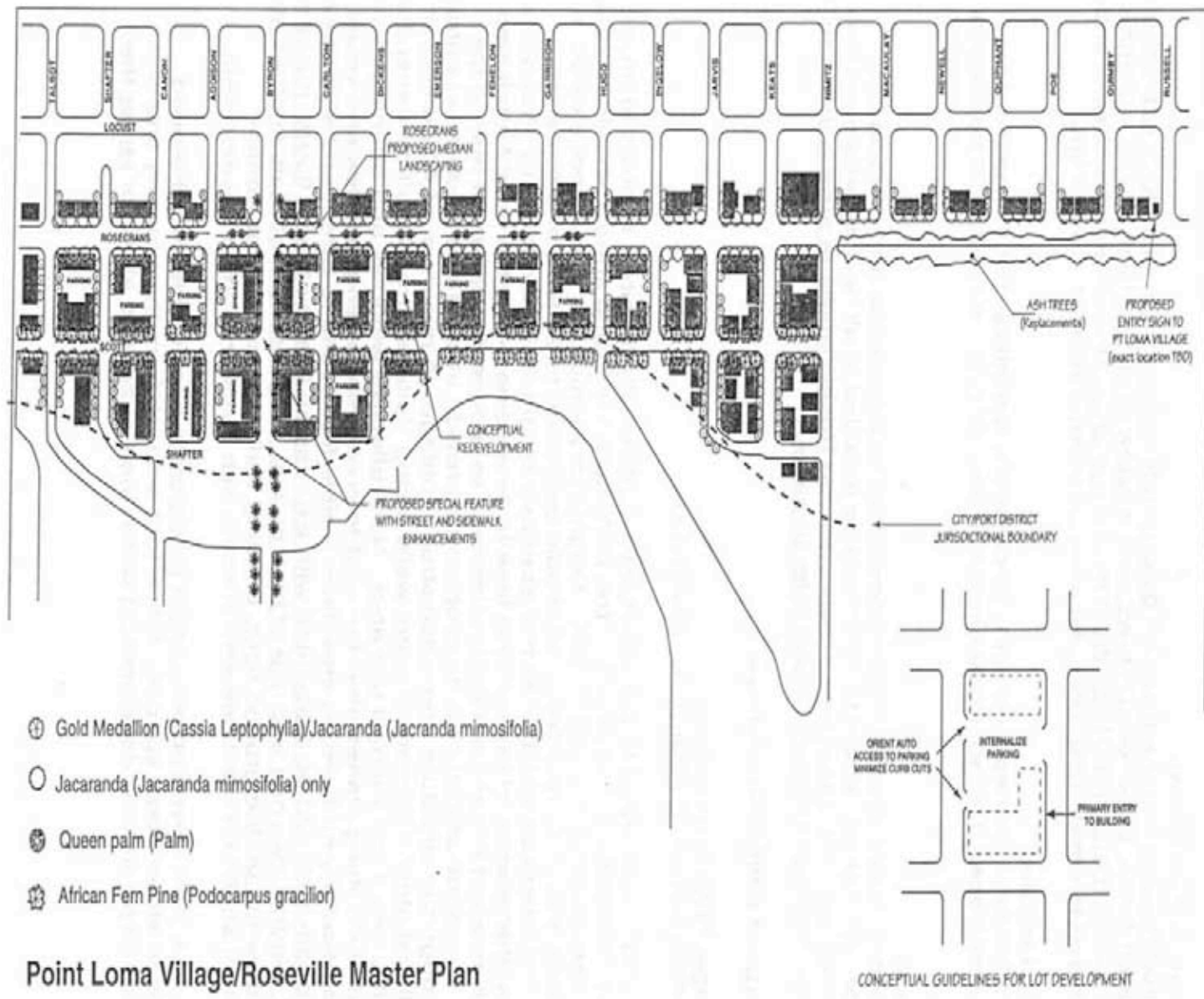
On Voltaire Street, from Mendocino eastward to Chatsworth, a tree theme consisting of *Fraxinus oxycarpa* and *Bauhinia blakeana* is proposed for the commercial area along Voltaire in the public right-of-way. Palm trees may be planted in addition to the *Fraxinus oxycarpa* and *Bauhinia blakeana* to increase the continuity of the street and the connection to Ocean Beach. The new tree types are intended to establish a greater sense of community identity and differentiate Voltaire from other neighborhoods. These trees should be considered as a separate and supplemental requirement beyond the standard requirements of the City’s landscape ordinance. As such they shall not be used to fulfill the landscape requirements set forth in that ordinance. The street tree program for the Voltaire Street is included in the community-wide program. Please refer to **Figure 35a**.

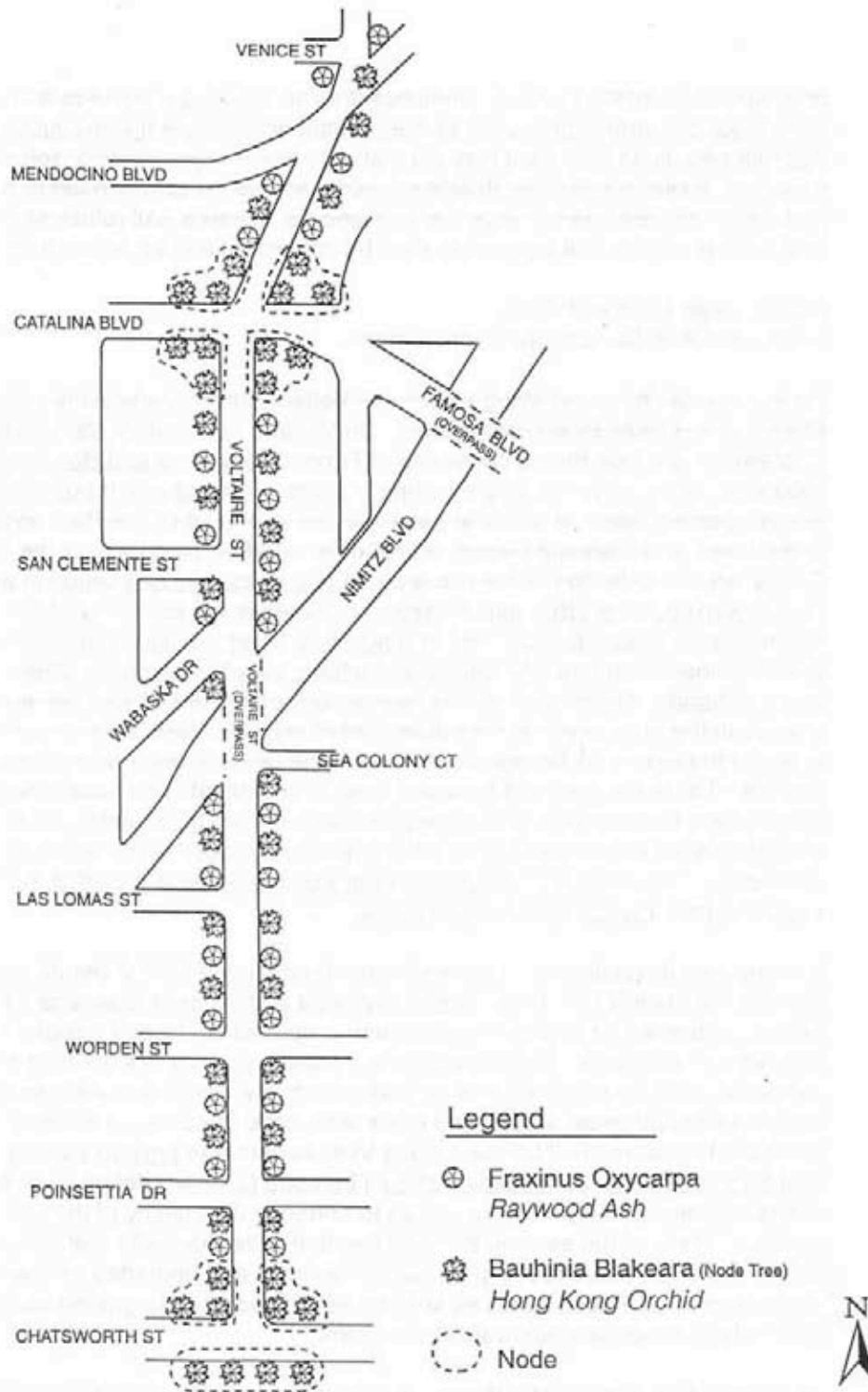
For any new development or redevelopment requiring a CPIOZ permit, existing trees that are compatible with the *Fraxinus oxycarpa* and *Bauhinia blakeana* should be preserved and incorporated into proposed landscape designs to enhance the character of the street. As redevelopment occurs, parking lots abutting streets and sidewalks shall be screened and upgraded with low, decorative walls or berms that are accompanied by trees, shrubs and other landscape features. In addition, street furniture shall be used along Voltaire Street to provide seating at bus stops and for pedestrians. Landscaping should be used to clearly define store front entrance areas and pedestrian paths, as well as to enhance the identity of the commercial corridor. Many of the existing four- and five-foot wide sidewalks that are cluttered with telephone poles and street signs shall be replaced and upgraded with wider sidewalks where possible. Public agencies shall be requested to underground utility lines and eliminate unnecessary sidewalk obstructions.

In addition to giving the streetscape a unified character through the landscaping, the street furniture should also be coordinated. Street furniture should be organized along the street to complement the activities and pedestrian flows and should not be obstructive to pedestrian paths. Street furniture should be grouped between street trees and not hinder the rhythm of tree planting. Well placed, these elements complement the overall character. Benches should be located parallel to the street, along the curb, facing the shops. The placement of benches should leave a clear path for the pedestrians along the sidewalk. Benches should be designed to be aesthetically compatible with the character of Voltaire Street and should not have advertising on them (see illustration).

Retail signs should fit into the overall scale of the street and not be overbearing, loud or overstated. Posters in windows should be equally sensitive to the scale of the street.



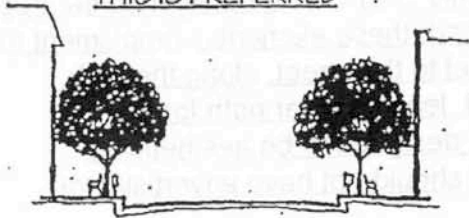




**Voltaire Street Tree Plan**  
**North Bay Revitalization**

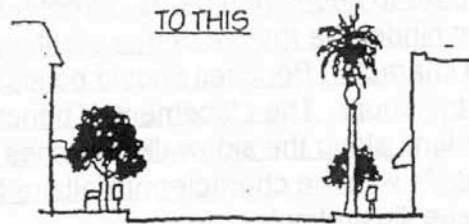
CITY OF SAN DIEGO PLANNING DEPARTMENT

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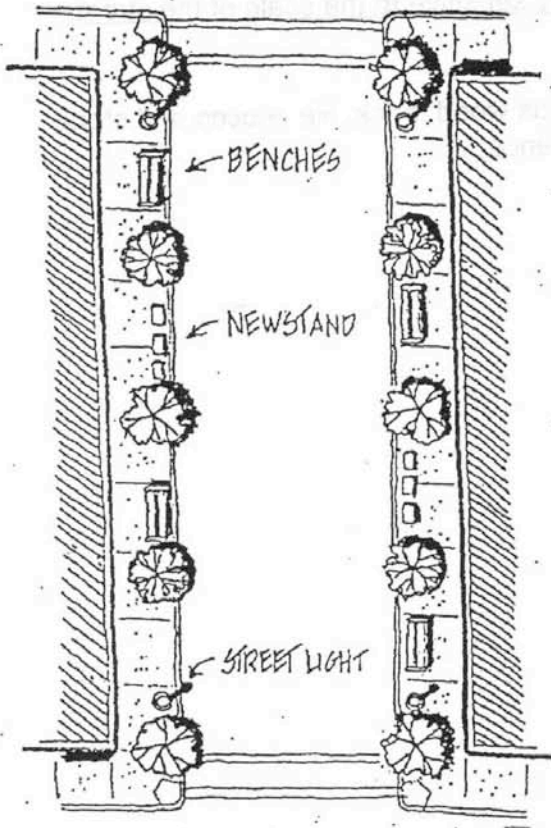


STREET TREES WITH WIDE CANOPY PROVIDE SHADING AND FRAME THE STREET

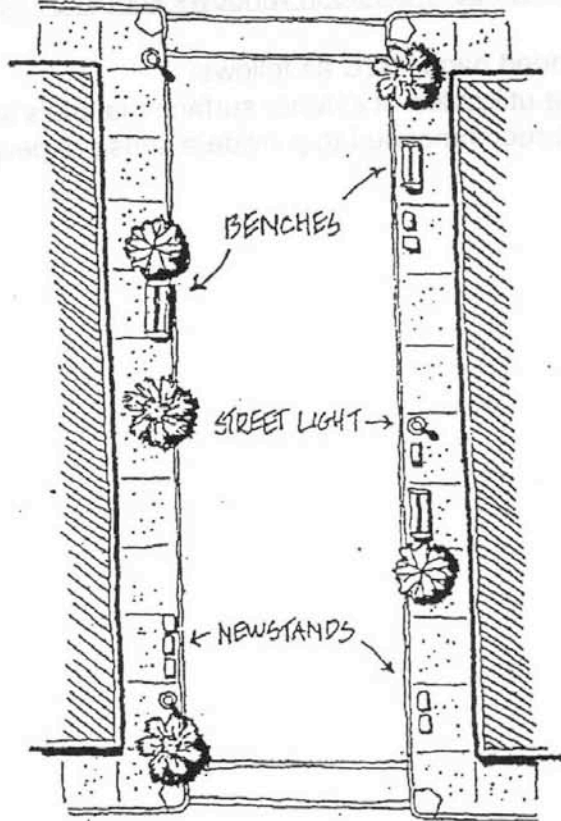
TO THIS



RANDOMLY PLACED LANDSCAPING AND STREET FURNITURE ADDS TO A CHAOTIC IMAGE



UNIFORM LANDSCAPING PROVIDES RHYTHM



NON-UNIFORM LANDSCAPING BREAKS RHYTHM



### Building Materials For Exterior Surfaces

The utilization of exterior surface materials such as wood, brick, and stone are recommended to provide a sense of permanence, traditionalism and quality for all projects along Voltaire Street. New or remodeled commercial buildings should be compatible with the overall architecture and color scheme or surrounding development. Use of predominantly glass construction on exterior building walls is not appropriate for the area east of Worden Street, which is within proximity to the Colonial Revival style buildings such as the Presbyterian Church on the corner of Udall Street and Poinsettia Drive.

### Parking And Vehicular Access

For projects abutting an alley, parking facilities should access the alley in order to reduce pedestrian/automobile conflicts along Voltaire Street. Various traffic engineering techniques described in applicable city regulations should be used where appropriate to reduce pedestrian/auto conflicts. Where feasible, existing driveways shall be closed and relocated to side streets and alleys in order to reduce the number of curb cuts and promote smooth traffic flow along Voltaire Street. Where site constraints are such that parking can only be accessed from Voltaire Street, the parking areas shall be buffered and screened by landscaping. On the few remaining large parcels within the CPIOZ area, enclosed parking is recommended as an alternative to surface parking when redevelopment occurs.

### **Roseville CPIOZ Guidelines**

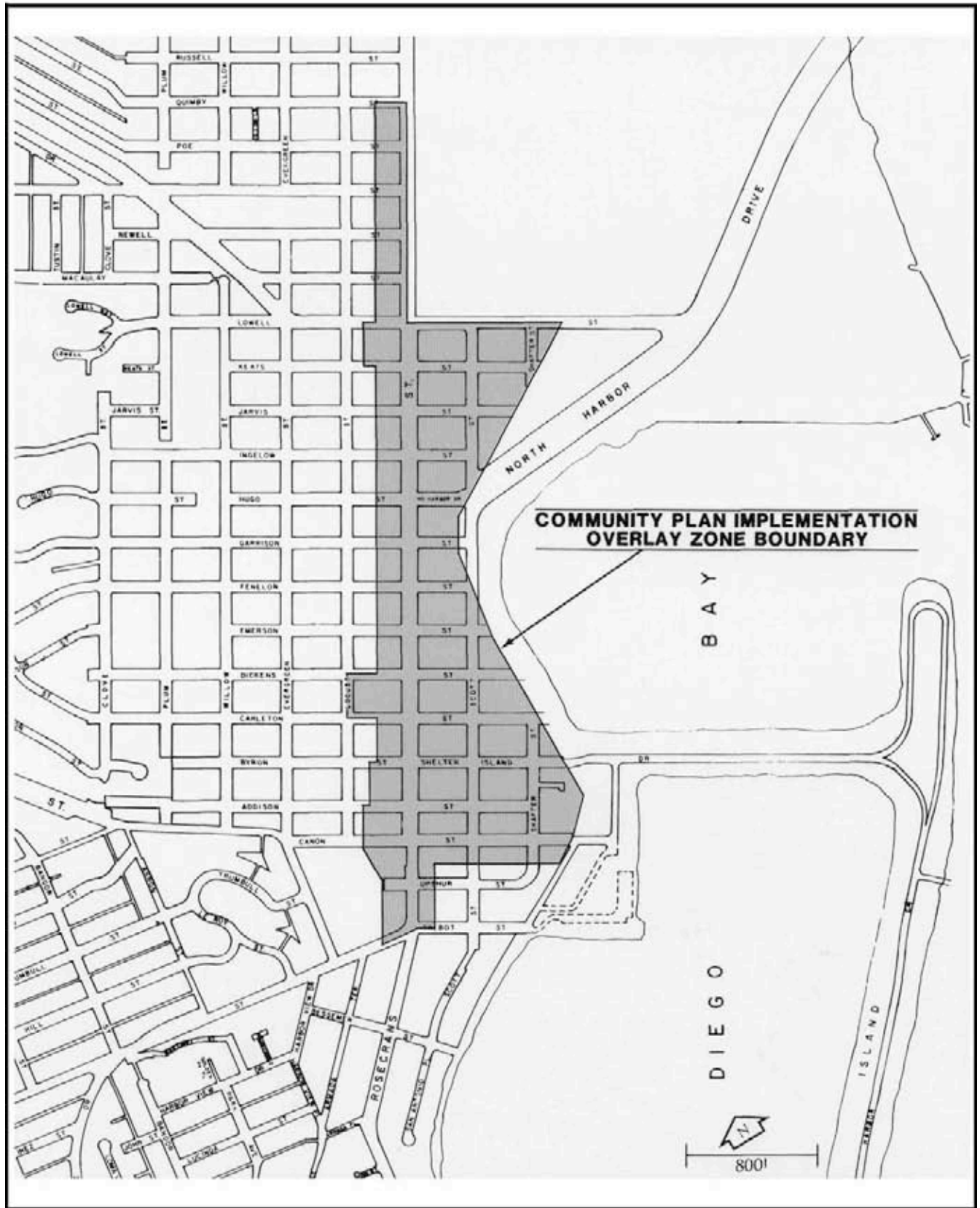
The Roseville commercial district, which encompasses all commercially-zoned land within City jurisdiction in the area bounded by Quimby, Locust and Talbot Streets and San Diego Bay, has a unique location, character and history. This area has served as a center for the whaling, fishing, and hide industries in the past and today has become a major yachting, boat building, and sports fishing center. Architecture and urban design in Roseville has been influenced by its location adjacent to San Diego Bay and Shelter Island as well as the ethnic background of the early Portuguese settlers. Shelter Island, which is under the jurisdiction of the San Diego Port District, has a distinct character dominated by South Seas style architecture, landscaping and signage.

In order to foster new development compatible with the historical development pattern in Roseville and to enhance the character and atmosphere of the commercial district, discretionary review of development is necessary. The CPIOZ, Type B, should be applied to the district. The boundaries for the Roseville CPIOZ are identified in **Figure 35**. As detailed below, the issues to be addressed for a Type B permit should include signage, building placement and pedestrian orientation, architectural features and landscaping.

### Signage

The sign regulations in effect within the coastal zone should be adhered to throughout the area.





**Roseville Commercial Area Community Plan  
Implementation Overlay Zone (CPIOZ) Boundaries  
Peninsula Community Plan**

CITY OF SAN DIEGO PLANNING DEPARTMENT

FIGURE 35

## Building Placement and Pedestrian Orientation

The intent of the following guidelines regarding building placement and pedestrian orientation is to achieve the basic objectives stated above. Where physical conditions dictate that strict compliance is not practical, alternative proposals which achieve the basic objectives may be considered. In addition, variations from these guidelines may be considered if superior solutions can be achieved.

Placement of buildings should serve to enhance pedestrian activity. Parking should be located at the rear or side of residential and commercial buildings. Parking in front of building facades or visible from the public right-of-way should be buffered and de-emphasized by landscaping pursuant to the Citywide Landscape Ordinance. (Temporary, at-grade parking during construction is exempted.) All above-grade parking structures visible from the public right-of-way should be architecturally screened and enclose some provisions for landscaping.

Primary vehicular access should be from side streets rather than principal streets. Curb cuts should be kept to a minimum.

At least 40 percent of the area of all new or redeveloped first-story street walls in commercial buildings should be devoted to pedestrian entrances, display windows or windows affording views into retail, office, gallery or lobby space. This percentage for buildings having four street walls may, however, be reduced to an average of 30 percent of the total area of first-story street walls. The building wall subject to these transparency guidelines should include the portion between three feet and eight feet above the sidewalk.

Building facades should be articulated by a variety of measures to include expression lines between floors, punched windows, and window awnings. High quality materials should be used on buildings throughout the village area. Corner lots should have an average setback of two feet from the property line and interior lots should have average setback of three feet from the property line. The setback should be the greater of the above standards or of the requirements of the underlying zone.

## Architectural Features

Use of architectural themes incorporating elements reminiscent of the nautical, maritime, fishing, Portuguese heritage, or pertinent historical heritage are desired. Use of canopies, canvas elements, nautical flag systems, bollards and nautical sculpture is encouraged. Use of wood is encouraged. Colors should be sympathetic to and harmonious with the architectural theme selected. Tilt-up concrete curtain wall and prefab structures are discouraged.

Projects should include amenities such as outdoor patios, street furniture (benches, kiosks, outdoor clocks, sculpture, fountains, etc.), flags, canvas awnings and boat related features.

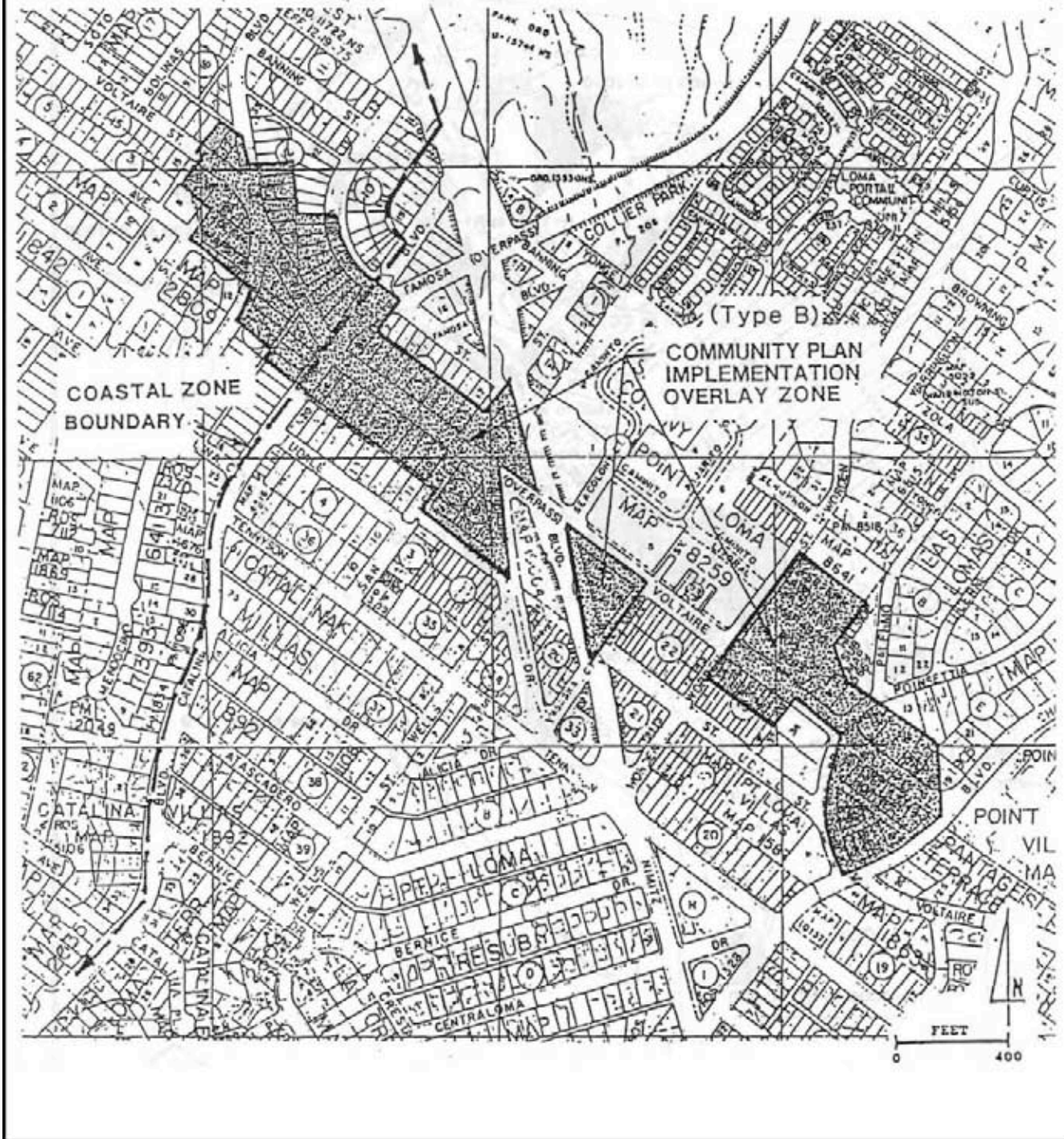
## Landscaping

Improvements should conform to the community design theme and existing City regulations. Appropriate street trees should be included in all new development and major remodeling. The street tree program for the Roseville/village area is included in the community-wide program. Please refer to page 122.





# PENINSULA COMMUNITY PLAN



## Voltaire Street CPIOZ Boundaries Peninsula Community Plan