

Bahia Resort Hotel
Landscape Concept Plan



Certified Mission Bay Master Plan

Specific language in Mission Bay Park Master Plan Update

Bahia Resort Redevelopment Plan

<p>"Existing commercial leases should be intensified to the greatest extent possible. Bahia Hotel: 600 room resort hotel"</p>	<p>Bahia Hotel is intensified up to Master Plan guidelines of 600 guest rooms</p>	
<p>"A 10' wide continuous pedestrian and bicycle access around Bahia Point shall be made part of any redevelopment effort of the Bahia Hotel. To mitigate the loss of any lawn area at Bahia Point, a minimum 20' grass strip shall replace the beach along the length of Ventura Cove for approximately 400"</p>	<p>Plan adds a 10' wide pedestrian and bicycle walkway around Bahia Point and extends 400' across Bonita Cove parking area. 20' grass strip will be added to Ventura Cove Beach area</p>	
<p>"In accordance with the objective of intensifying existing leaseholds, the Bahia Hotel Lease, at the lessee's option, should be expanded towards the point of the peninsula...and shifted east in some areas"</p>	<p>Adds 1 acre to the lease at the end of Bahia Point and moves lease boundary to the East</p>	
<p>"In accordance with the objective of intensifying existing leaseholds, the Bahia Hotel Lease, at the lessee's option, should be expanded towards the point of the peninsula...and shifted east in some areas"</p>	<p>Adds 1 acre to the lease at the end of Bahia Point and moves lease boundary to the East</p>	
<p>"Onsite parking for all employees and guests within the hotels leasehold shall be provided."</p>	<p>Hotel Employee and Guest parking is removed from Gleason Road. 100% of Guest and Employee parking is contained within the Bahia leasehold.</p>	
<p>"An approximate 50 X 100 lawn area for Bocce Ball and other recreational uses shall be added north of the entrance to the Ventura cove parking lot adjacent to the beach"</p>	<p>Adds Public facilities such as restrooms, Bocchy Ball Courts, Kayak and Paddleboard lockers and over 400 ft. of new picnic lawn area</p>	
<p>"No new wet slips are recommended with the following exceptions: The current wet slip expansions proposed by the Bahia Hotel (41) slips (2.0 acres)</p>	<p>Adds 41, marina slips to existing Bahia Marina. Site is 2 acres.</p>	
<p>...These are limited expansions that do not impact the recreational or navigational use of their immediate water areas."</p>		

MISSION BAY PARK MASTER PLAN UPDATE

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City of San Diego

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The Community Planning Committees

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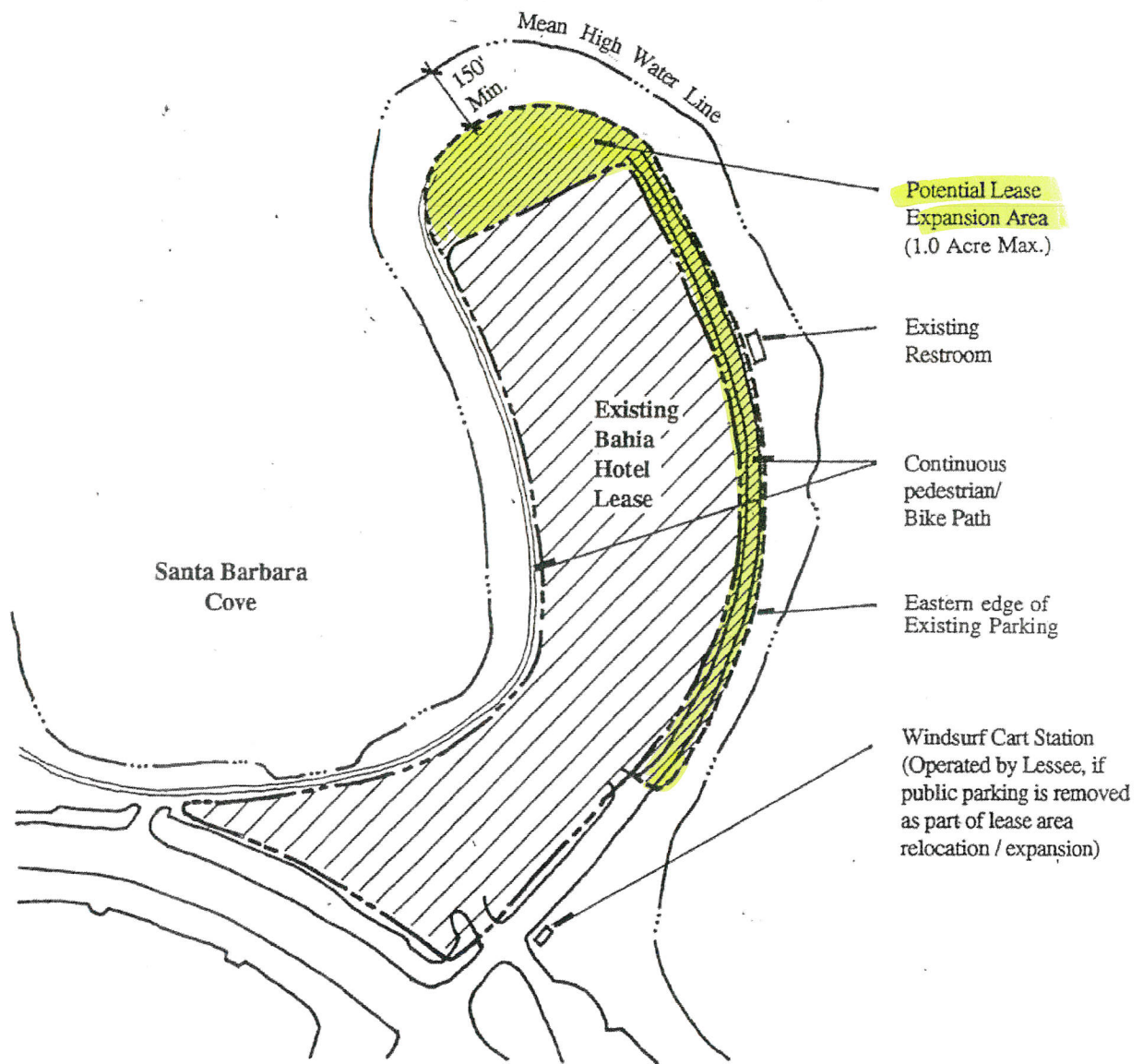
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17. **Bahia Hotel: 600-room resort hotel.** In accordance with the objective of intensifying existing leaseholds, the Bahia Hotel lease, at the lessee's option, should be expanded towards the point of the peninsula, no further than the south curb of the north parking area, and shifted eastward in some areas. Such an expansion and shift could potentially permit the addition of 120 hotel rooms to the complex, above and beyond the current 484-room redevelopment plans. The following criteria should guide the precise redevelopment plan for Bahia Point:

- The demand to maintain public parking shall be a priority of any redevelopment plan. Any net loss of public parking resulting from a lease expansion and/or relocation shall be mitigated by increasing parking lot capacity at Bonita Cove, Ventura Cove and if necessary, other areas in the western half of Mission Bay.
- On site parking for all hotel employees and guests within the hotel's leasehold shall be provided.
- Nothing in this plan shall be construed to allow development or the closure of public rights-of-way in a manner inconsistent with statutory or constitutional law.
- Access needs for small water craft users and the use of traditional picnic areas along the eastern shoreline shall be preserved as part of the specific redevelopment plan.
- An adequate public use zone should be maintained in accordance with the Design Guidelines taking into account the narrowness of the peninsula.
- A 10-foot wide continuous pedestrian and bicycle access around Bahia Point shall be made part of any redevelopment effort of the Bahia Hotel in accordance with the Design Guidelines.
- A minimum 20-foot grass strip along the eastern side of the peninsula shall remain.
- To mitigate the loss of any lawn area at Bahia Point, a minimum 20-foot wide grass strip shall replace beach along the length of Ventura Cove, adjacent to the parking lot, for approximately 400 feet.



Bahia Point Development Area

figure 12

In addition, an approximate 50-foot by 100-foot lawn area for bocce ball and other recreational uses shall be added north of the entrance to the Ventura Cove parking lot, adjacent to the beach.

- A seasonal accessible-walkway-for-all shall be installed at Ventura Cove to the beach and the Bahia Hotel's expansion plan shall comply with the Americans with Disabilities Act.
- Any other public facilities, including all public parking removed from Bahia Point, shall be fully mitigated in the vicinity of Bahia Point at the time of, or prior to, redevelopment.

18. De Anza Cove (Special Study Area): This area is planned as a Special Study Area (SSA) potentially involving any one or all of the following uses: guest housing, regional parkland, beach, boating concessions, wetland, wetland-related hydraulic improvements, paths and trails. Recommendation 25 describes in more detail the intent of this SSA and its development criteria.

19. Sunset Point Lease Expansion: In keeping with the objective of intensifying existing commercial areas, the Plan proposes the potential expansion of the Dana Inn by approximately 2.5-acres. It is estimated that 80 additional hotel rooms can be developed in this area. The expansion area should stretch from the northern boundary of the current leasehold towards Sunset Point, and observe the following development criteria:

- Development proposals should enhance pedestrian, bicycle, emergency and maintenance circulation around Sunset Point in accordance with the Design Guidelines.
- All required private parking should be provided within the leasehold area.
- Development intensification should minimize the impact to Sunset Point Park users. The waterfront areas of the Point should remain accessible to the public as required by the Design Guidelines.

Table 2

LAND LEASE CHANGES

Leases Lost	Acres	Leases Gained	Acres
Campland on the Bay	24.13	De Anza SSA	60.0 ⁽¹⁾
De Anza Trailer Resort	69.83	Sunset Point	2.5
Ski Club (Present Location)	4.0	Dana Landing	1.0
		Bahia Hotel	1.0
		South Shores "Best Use" Parcel	16.5
		Marina Village/ Pacific Rim Marine Enterprises, Inc. Potential Lease Expansion	10.0
		Ski Club (or Other Operation)	4.0
		Fiesta Island Primitive Camping	18.0 ⁽²⁾
Total (Acres)	97.96	Total (Acres)	113.0

Net Dedicated Lease Gain = 15.04

Current Lease Total = 404.42 Acres

Proposed Maximum Lease Total = 419.46

⁽¹⁾ Maximum available for commercial development

⁽²⁾ Lease area could be non-profit

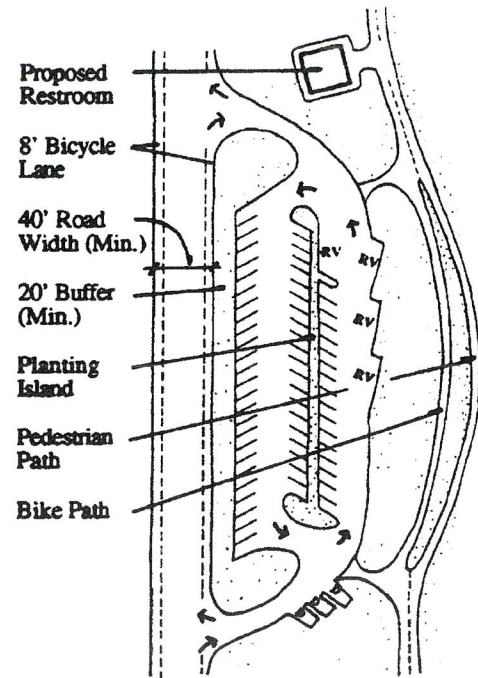
- About two-thirds, or 120 spaces, of the existing De Anza boat ramp trailer spaces should be maintained for day-use RVs (the ramp is being abandoned as part of the Water Use recommendations). The remaining spaces should be re-striped to serve full-size automobiles. The trailer spaces should be grouped in the south end of the parking lot to minimize the obstruction of water views from I-5.

88. Curbside Parking: In the interest of emergency access, pedestrian safety, Park surveillance, visual access to the water, convenience and safety of touring cyclists, and the operational efficiency of a potential future tram service, curbside parking on the Park roadways should be prohibited.

EXCEPTION: On East Mission Bay Drive, the removal of curbside parking should be subject to the following conditions:

- Priority given to the removal of vehicles from the eastern curb of the road
- Operation of a tram service along East Mission Bay Drive
- Replacement of the lost parking on the overflow lot, which can accommodate up to about 2,900 spaces, 360 more than is minimally required
- Consideration of the expansion of the Pacific Passage parking lot off East Mission Bay Drive and south of the Hilton Hotel to make up part of the loss in parking convenience

89. Drop-off and Loading: Curbside pull-outs should be provided at regular intervals on the water-side of the Park road to facilitate the loading and unloading of passengers and picnic ware. Permanent parking should be prohibited in these spaces.



RV: Recreational Vehicle Parking (5% of Parking Spaces)

D: Parking for Persons with Disabilities

Typical Parking Lot

- A pedestrian and bicycle bridge over Rose Creek, designed also to accommodate maintenance and emergency equipment. This bridge would allow Park users to conveniently circle the northern edge of the Park.
- A raised path, or boardwalk, under the Ingraham Street Bridge at Crown Point Shores. The path would permit uninterrupted movement from Fiesta Bay to Sail Bay.
- Widening of the East Mission Bay Drive Bridge. The combined path is currently inadequate at this location. A widened bridge or separate path along its west side is recommended.

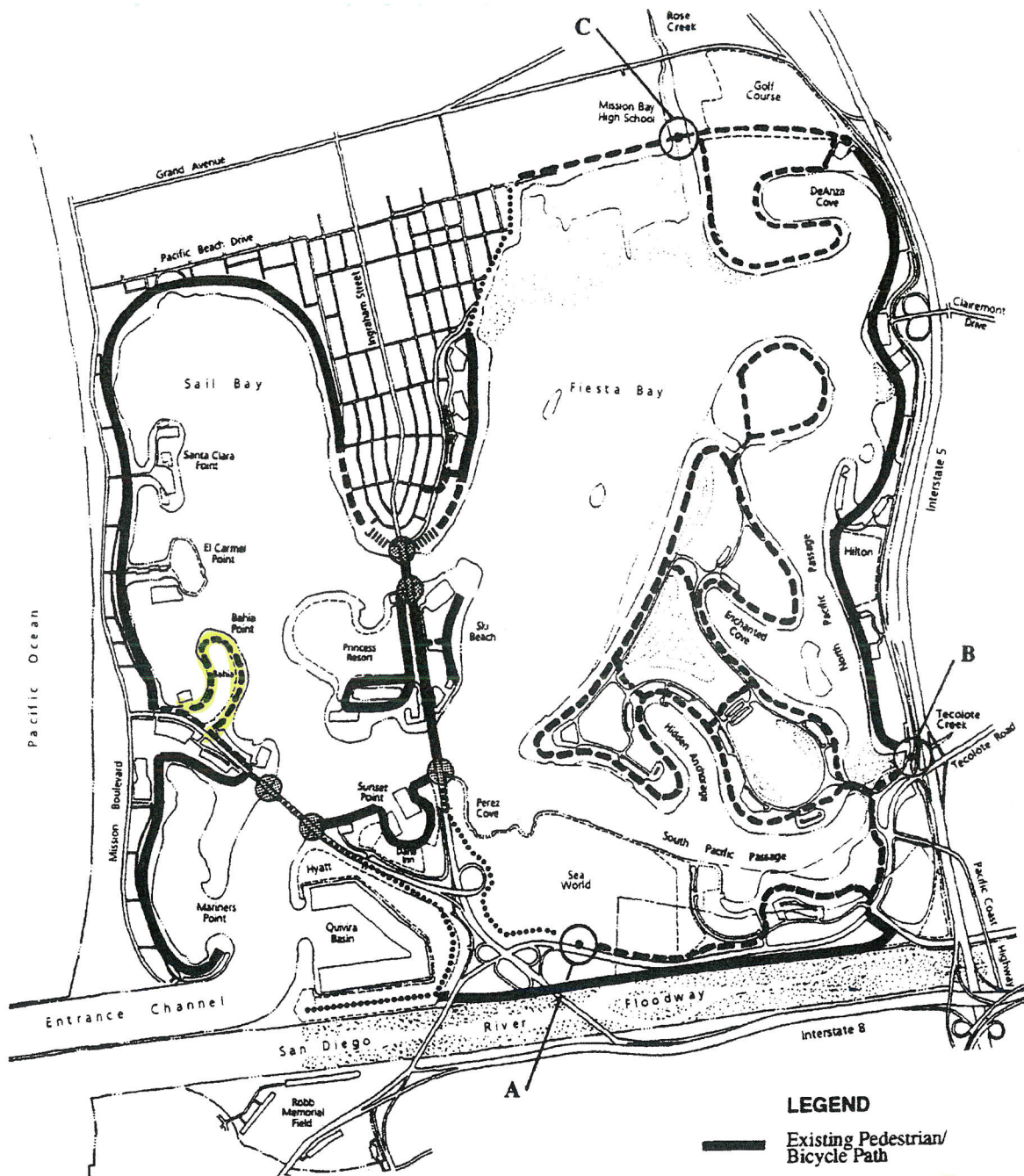
In addition to the above key linkage improvements, a continuous pedestrian and bicycle path should be pursued around Bahia Point. To this end, a shift in the Bahia Hotel lease area should be considered in accordance with Recommendation 17.

102. High-Speed Bicycle Path: To accommodate the higher speeds of touring cyclists and skaters, dedicated bicycle lanes should be provided on the Park roads to the extent possible.

If curbside parking is removed from East Mission Bay Drive, the parking lanes should be converted to bicycle lanes (this also facilitates emergency vehicle access). Alternatively, a dedicated bicycle path could be provided between the Park road and the boundary with I-5.

Extending a dedicated bike lane along the eastern edge of the Park next to the overflow parking lot, and bridging the path over Friars road, linking it to the San Diego River pathway should be considered. This improvement would create a nearly uninterrupted high-speed bikeway between De Anza Cove and Hospitality Point.

103. Regional Linkages: The Park should be viewed as a key destination of the regional system of recreational paths. To this end, studies should be conducted to determine the feasibility of connecting the Park's bikeways and pedestrian paths to the regional network, particularly along Rose Creek Canyon to San Clemente Canyon and across I-5 to Clairemont Boulevard. Coordination with Metropolitan Transit Development Board (MTDB) should be exercised to ensure the optimum pedestrian and bicycle access to the Park (possibly over I-5 from future planned light rail station).



Note: Refer to "Optional South Fiesta Island Development Plan" on page 130



Pedestrian / Bicycle Path Improvement

figure 30

- A Overpass on Sea World Exit Road
- B Widened Pedestrian/Bike Path @ Existing Bridge
- C New Bridge

LEGEND

- Existing Pedestrian/Bicycle Path
- Proposed Pedestrian/Bicycle Path
- Roadside Bicycle Lane
- Boardwalk (Under Bridge)
- Existing Under-Bridge Connection
- Special Improvement

WET SLIPS AND ANCHORAGE

Several areas of the Park serve as mooring basins for over-night or longer term anchorage. In addition, 1,983 wet slips, existing and planned, serve as permanent berths for a variety of watercraft. Most of these slips are located in Quivira Basin and Dana Landing. There is wide demand for more marinas in the region. However, in Mission Bay Park this demand must be weighed against the recreational and navigational value of the limited water areas.

Recommendations

47. Additional Wet Slips: The recreational and navigational uses of the Bay waters are valued substantially more than the dedication of water areas for wet slips and anchorage. Accordingly, no new slip or mooring areas are recommended, with the following exceptions:

- **Current wet slip expansions proposed by the Bahia Hotel (41 slips), the Princess Resort (58 slips), and the Mission Bay Yacht Club (27 slips) should proceed.** These are limited expansions that do not impact the recreational or navigational use of their immediate water areas. The new slips proposed by the Princess Resort would be within the current leasehold area.
- In the South Shores embayment, up to 24 wet slips may be provided for day-use only, as part of new docks for the Ski Club. This facility, operated as an option by the Ski Club or other independent operator, would allow boaters to access a potential chandlery and restaurant on the north side of the embayment.

52. Ski Club Relocation: Because of increasing sedimentation in Rose Creek, the Ski Club should be relocated to the South Shores embayment. This location is in close proximity to Hidden Anchorage in Fiesta Island, where the water skiers practice and compete.

53. Optional Day-Use Slips: At the option of the Ski Club, 24 day-use slips could potentially be developed in the South Shores Embayment. This facility would add about 1-acre to the Ski Club water lease area.

Table 3

WATER LEASE CHANGES

Leases Lost	Acres	Leases Gained	Acres
Campland on the Bay (West of Rose Creek)	5.76	Campland on the Bay (East of Rose Creek)	5.76
		Mission Bay Yacht Club	0.6
		Bahia Hotel	2.0
		South Shores Day-Use Slips	1.0 ⁽¹⁾
Total (Acres)	5.76	Total (Acres)	9.4

Net Dedicated Lease Gain = 3.6 Acres

Current Lease Total = 83.74 Acres

Proposed Maximum Lease Total = 87.34 Acres

⁽¹⁾ This is a potential use.