

NOTES:

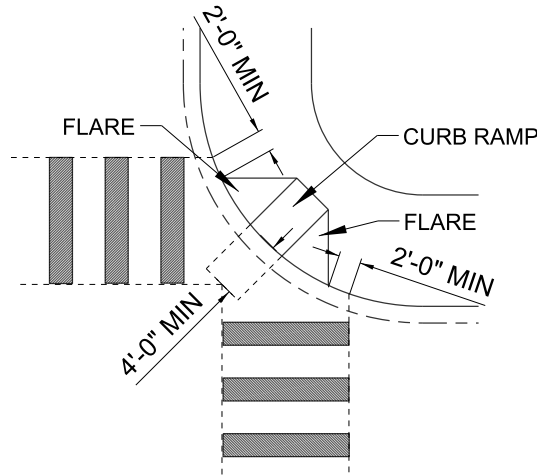
1. A CURB RAMP IS REQUIRED FOR EACH SIDEWALK CROSSING. IF A PEDESTRIAN CAN CROSS IN TWO DIRECTIONS FROM AN INTERSECTION CORNER, TWO CURB RAMPS SHALL BE PROVIDED. IF A PEDESTRIAN CAN ONLY CROSS IN ONE DIRECTION FROM AN INTERSECTION CORNER, ONLY A SINGLE CURB RAMP SHALL BE PROVIDED.
2. IN AN IDEAL CONDITION WHERE THE CURB RETURN AND INTERSECTION ARE LEVEL AND THE CURB HEIGHT IS 6":
 - A. IF THE RIGHT-OF-WAY (ROW) LIMITS ARE 10'-0" OR MORE (MEASURED FACE OF CURB TO PROPERTY LINE), A TYPE A OR TYPE B CURB RAMP SHALL BE USED.
 - B. IF THE ROW LIMITS ARE LESS THAN 10'-0" BUT NOT LESS THAN 8'-0", A TYPE C2 CURB RAMP SHALL BE USED.
 - C. IF THE ROW LIMITS ARE LESS THAN 8'-0", A TYPE C1 CURB RAMP SHALL BE USED.
 - D. A TYPE D CURB RAMP SHALL BE USED AT ALLEY CORNERS.
 - E. A TYPE E CURB RAMP (BLENDED TRANSITION) SHALL BE USED FOR MULTI-DIRECTIONAL ACCESS AS AN ALTERNATIVE IF DUAL RAMPS CANNOT BE INSTALLED.
3. OPPOSING CURB RAMPS AT A SINGLE CROSSING SHALL LINE UP. ALIGN THE CURB RAMP WITH THE CROSSWALK SO THERE IS A STRAIGHT PATH OF TRAVEL FROM THE TOP OF THE RAMP TO THE CURB RAMP ON THE OTHER SIDE, TO THE MAXIMUM EXTENT FEASIBLE.
4. PULL BOXES, MANHOLES, VAULTS, AND OTHER UTILITIES SHALL BE RELOCATED OR INCORPORATED ONTO THE CURB RAMP AREA PROVIDED THAT THE ACCESS COVER IS STABLE, FIRM, SLIP RESISTANT, AND FLUSH OR ADJUSTED TO GRADE. COORDINATE THE WORK WITH THE ENGINEER
5. UTILITY POLES MAY BE INCORPORATED INTO THE FLARES OF THE CURB RAMP PROVIDED THAT THE REQUIRED ACCESSIBLE ROUTE WIDTH IS COMPLIANT.
6. RETAINING CURBS OR WALLS:
 - A. INSTALL A RETAINING CURB/WALL AT THE BACK OF THE CURB RAMP IF NOT ABLE TO GRADE EXISTING SLOPE AT A 3:1 RATIO OR FLATTER WITHIN THE ROW.
 - B. THE FRONT OF THE RETAINING CURB/WALL SHALL BE IN LINE WITH THE BACK OF THE SIDEWALK. TAPER THE BACK OF THE SIDEWALK WITHIN THE 4'-0" TRANSITION AREA OR TO THE NEXT JOINT, WHICHEVER IS GREATER.
 - C. IN LIEU OF A FLARE, A RETAINING CURB WILL BE ALLOWED IF IT IS IMPRACTICAL TO REMOVE THE EXISTING OBSTRUCTION, SUCH AS A CURB INLET, UTILITY INFRASTRUCTURE, A SIGNIFICANT OR REGISTERED TREE, OR THE TREE CANNOT BE TRANSPLANTED.
 - D. IF PEDESTRIANS ARE ABLE TO ACCESS THE PAVED AREA DIRECTLY BEHIND THE RETAINING CURB, INSTALL A PROTECTIVE RAILING PER **SDG-140**.
7. GRADE BREAKS AT THE TOP AND BOTTOM OF THE RAMPS AND CURB RAMPS SHALL BE PERPENDICULAR TO THE DIRECTION OF THE RAMP RUN. GRADE BREAKS SHALL NOT BE PERMITTED ON THE SURFACE OF RAMP RUNS AND TURNING SPACES. SURFACE SLOPES THAT MEET AT GRADE BREAKS SHALL BE FLUSH TO DELINEATE THE CURB RAMP.
8. PROVIDE A 1/2" DEEP TOOLED JOINT WITH 1/8" RADIUS EDGES AS SHOWN ON DRAWINGS.
9. INSTALL A 1/4" EXPANSION JOINT FILLER BETWEEN THE NEW CURB RAMP GUTTER AND THE EXISTING SIDEWALK.
10. PONDING IS NOT ALLOWED WITHIN THE CURB RAMP LIMITS, AND THE DRAINAGE PATTERN SHALL NOT BE ALTERED.
11. THE ADJUSTMENT OF THE CROSS SLOPE AT THE RAMP OPENING SHALL NOT CAUSE GUTTER TRICKLE FLOW TO SPILL ONTO TRAVELLED LANES OR PONDING ANYWHERE.
12. TRANSITIONS FROM RAMPS TO WALKS AND SIDEWALK GUTTER OR STREET SURFACE SHALL BE FLUSH AND AT THE SAME LEVEL. PAVEMENT AT THE STREET SURFACE SHALL BE MILLED TO ACHIEVE FLUSH CONDITION.

SHEET 1 OF 3

REVISION	BY	APPROVED	DATE	CITY OF SAN DIEGO – STANDARD DRAWING	RECOMMENDED BY THE CITY OF SAN DIEGO STANDARDS COMMITTEE
ORIGINAL	SS	A. OSKOUJ	12/03		
UPDATED	MM	J.NAGELVOORT	03/18	GENERAL CURB RAMP NOTES	 COORDINATOR R.C.E. 56523
UPDATED	FC	J.NAGELVOORT	06/18		
UPDATED	FC	J.NAGELVOORT	09/18		
UPDATED	HN	J.NAGELVOORT	04/19		
UPDATED	FC	J.NAGELVOORT	09/20		
					DRAWING NUMBER SDG-131

NOTES:

13. THE REMOVAL OF EXISTING PAVEMENT, CONCRETE CURB, GUTTER, SIDEWALK, AND EXISTING CURB RAMP FOR THE INSTALLATION OF A NEW CURB RAMP SHALL COMPLY WITH **SDG-156**.
14. DIAGONAL CURB RAMPS WITH FLARED SIDES SHALL HAVE A SEGMENT OF CURB 2'-0" LONG MINIMUM LOCATED ON EACH SIDE OF THE CURB RAMP AND WITHIN THE MARKED CROSSING. SEE SINGLE CURB RAMP CORNER DETAIL BELOW.



SINGLE CURB RAMP CORNER

15. PERPENDICULAR CURB RAMPS (TYPE A, TYPE B, AND TYPE C2) SHALL HAVE A 4'-0" MINIMUM CLEAR SPACE BEYOND THE BOTTOM OF THE GRADE BREAK WITHIN THE PEDESTRIAN STREET CROSSING AND WHOLLY OUTSIDE THE TRAFFIC LANES OF THE ROADWAY (VEHICULAR AND BIKE LANES). SEE SINGLE CURB RAMP CORNER DETAIL IN NOTE 14.
16. CURB RAMP AND FORM WORK SLOPES SHALL BE CHECKED WITH A DIGITAL LEVEL OF AN APPROPRIATE LENGTH. NO PORTION OF A RAMP RUN SHALL EXCEED THE MAXIMUM SLOPE REQUIREMENT.
17. CONTRACTOR SHALL PROVIDE AN EMBEDDED STAMP ON TOP OF THE CURB AT THE OPENING OF THE CURB RAMP. THE STAMP SHALL BE 1/4" DEEP AND 3/4" TO 1" HIGH, AND SHALL ONLY INCLUDE THE NAME OF THE CONTRACTOR AND THE YEAR OF INSTALLATION. THE STAMP SHALL BE COMPLETELY FLUSH WITH THE CURB SURFACE.
18. THE SLOPE OF THE RAMP SHALL BE UNIFORM ALONG EACH RAMP RUN.
19. THE CROSS SLOPE OF THE RAMP SHALL BE MEASURED PERPENDICULARLY TO THE PATH OR DIRECTION OF TRAVEL.
20. ANY DEVIATION FROM THESE PROVISIONS REQUIRES PRIOR APPROVAL FROM THE ENGINEER.
21. CONCRETE SHALL BE 520-C-2500.
22. THE EXISTING CONCRETE SPANDREL OF A CROSS GUTTER SHALL BE REMOVED AND REPLACED IN ITS ENTIRETY AS PART OF THE CURB RAMP INSTALLATION.
23. THE EXISTING CONCRETE ALLEY APRON SHALL BE REMOVED AND REPLACED IN ITS ENTIRETY AS PART OF THE CURB RAMP INSTALLATION.
24. A TRANSITION AREA IS REQUIRED IF THE CURB RAMP IMPROVEMENTS TIE INTO AN EXISTING SIDEWALK CROSS SLOPE EXCEEDING 2%. FOR CURB RAMPS TYPE C AND D, EXTEND THE TRANSITION AREA 4'-0" LONG MINIMUM WITH A 1.5% MAXIMUM CROSS SLOPE.

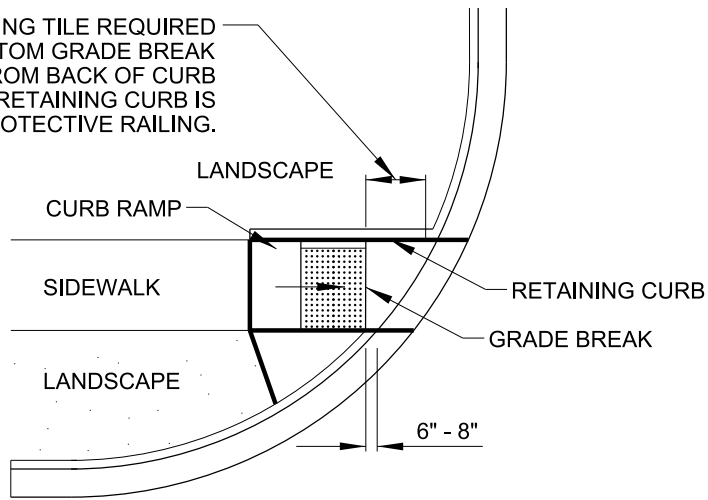
REVISION	BY	APPROVED	DATE	CITY OF SAN DIEGO – STANDARD DRAWING	RECOMMENDED BY THE CITY OF SAN DIEGO STANDARDS COMMITTEE
ORIGINAL	SS	A. OSKOUI	12/03		
UPDATED	MM	J.NAGELVOORT	03/18	GENERAL CURB RAMP NOTES	<i>Chung</i> 9/22/20 COORDINATOR R.C.E. 56523 DATE
UPDATED	FC	J.NAGELVOORT	06/18		
UPDATED	FC	J.NAGELVOORT	09/18		DRAWING
UPDATED	HN	J.NAGELVOORT	04/19		NUMBER
UPDATED	FC	J.NAGELVOORT	09/20		SDG-131

NOTES:

25. DETECTABLE WARNING TILE SHALL BE LOCATED SO THE EDGE NEAREST THE CURB IS 6" MINIMUM AND 8" MAXIMUM FROM THE LINE AT THE FACE OF THE CURB.

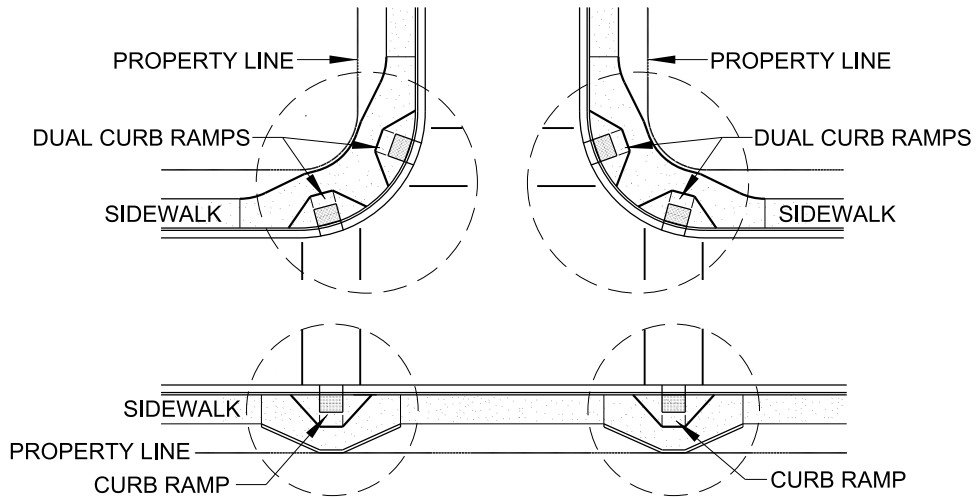
SEE DETAIL BELOW FOR EXCEPTION.

NO DETECTABLE WARNING TILE REQUIRED WHEN BOTH ENDS OF THE BOTTOM GRADE BREAK ARE LESS THAN 5'-0" FROM BACK OF CURB AND ACCESS ACROSS THE RETAINING CURB IS PREVENTED BY LANDSCAPE OR PROTECTIVE RAILING.



DETAIL - NOTE 25
NOT TO SCALE

26. CURB RAMPS SHALL BE INSTALLED AT ALL PEDESTRIAN CROSSINGS AT INTERSECTIONS, INCLUDING AT T-INTERSECTIONS. SEE DETAIL BELOW.



DETAIL - NOTE 26
NOT TO SCALE

REVISION	BY	APPROVED	DATE
ORIGINAL	SS	A. OSKOUI	1203
UPDATED	MM	J.NAGELVOORT	03'18
UPDATED	FC	J.NAGELVOORT	06'18
UPDATED	FC	J.NAGELVOORT	0918
UPDATED	HN	J.NAGELVOORT	0419
UPDATED	FC	J.NAGELVOORT	0920

CITY OF SAN DIEGO – STANDARD DRAWING

GENERAL CURB RAMP NOTES

RECOMMENDED BY THE CITY OF SAN DIEGO STANDARDS COMMITTEE

Chungee 9/22/20
COORDINATOR R.C.E. 56523 DATE

DRAWING NUMBER **SDG-131**