

**FOURTH ADDENDUM  
TO THE  
FINAL ENVIRONMENTAL IMPACT REPORT (FEIR) FOR THE  
SAN DIEGO DOWNTOWN COMMUNITY PLAN, CENTRE CITY PLANNED  
DISTRICT ORDINANCE, AND REDEVELOPMENT PLAN FOR THE  
CENTRE CITY PROJECT AREA (STATE CLEARINGHOUSE NUMBER  
2003041001, REVISED MARCH 2006)**

**FOR THE PROPOSED  
SAN DIEGO CIVIC CENTER COMPLEX PROJECT**

**June 2010**

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## 1.0 INTRODUCTION

This Addendum to the San Diego Downtown Community Plan Final Environmental Impact Report (FEIR) SCH No. 2003041001, prepared for the Redevelopment Agency of the City of San Diego, has been prepared in accordance with the California Environmental Quality Act (CEQA) Guidelines Section 15164. This Addendum updates the FEIR which was certified on February 28, 2006. The FEIR is available to the public for review at the offices of the Centre City Development Corporation (CCDC) located at 401 B Street, Suite 400, San Diego, CA 92101. This document was circulated for a public review period beginning May 05, 2010 and ending May 19, 2010. No public comments were received during the public review period.

### 1.1 Purpose

The purpose of this Addendum is to evaluate the potential environmental effects from implementation of the San Diego Civic Center Complex project (the “project” or “proposed project”), which comprises a new civic center and mixed-use complex. The proposed project presents new information that was not known at the time of the San Diego Downtown Community Plan FEIR was prepared. Section 15164(a) of the California Environmental Quality Act (“CEQA”) Guidelines provides that the lead agency shall prepare an addendum to a previously certified environmental impact report (“EIR”) if some changes or additions are necessary but none of the conditions described in CEQA Guidelines Sections 15162-15163 calling for preparation of a Supplemental or Subsequent EIR have occurred. This Addendum conclusively demonstrates that none of the conditions described in CEQA Guidelines Section 15162-15163 have occurred.

### 1.2 Background

On February 28, 2006, the San Diego City Council adopted the Downtown Community Plan, the 10th Amendment to the Redevelopment Plan for the Centre City Redevelopment Project, and amendments to the Centre City Planned District Ordinance (PDO), the framework for downtown land development. At the time of adoption, it was anticipated that it would be necessary to amend these documents to make a variety of refinements based on the lessons learned in implementation of the new programs and policies. As such, these documents were amended in July of 2007 per the 11th Amendment to the Redevelopment Plan for the Centre City Redevelopment Project and amendments to the Downtown Community Plan and Centre City PDO. The Downtown Community Plan and Centre City and Marina PDOs were further amended in April 2010 to enhance historic preservation policies, provide consistent parking regulations, and to allow additional land uses in the Residential Emphasis land use districts.

### 1.3 CEQA Requirements

Pursuant to Section 15164(a) of the CEQA Guidelines, the lead agency shall prepare an Addendum to a previously certified EIR “if some changes or additions are necessary, but none of the conditions described in Sections 15162-15163 calling for preparation of

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a subsequent or supplemental EIR have occurred". These sections of the CEQA Guidelines would require a Subsequent or Supplemental EIR if any of the following conditions apply:

- Substantial changes are proposed in the project which will require major revisions of the previous EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;
- Substantial changes occur with respect to the circumstances under which the project is undertaken which will require major revisions of the previous EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or
- New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the previous EIR was certified as complete, shows any of the following:
  - The project will have one or more significant effects not discussed in the previous EIR;
  - Significant effects previously examined will be substantially more severe than shown in the previous EIR;
  - Mitigation measures or alternatives previously found not to be feasible would in fact be feasible, and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or
  - Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative.

In the event that none of the aforementioned conditions are met, Section 15164(a) states that a Subsequent or Supplemental EIR is not required. Rather, an agency can:

- Decide that no further environmental documentation is necessary; or
- Require that an addendum be prepared.

Based on the results of the Environmental Secondary Study Checklist prepared for the proposed project, none of the situations described in CEQA Sections 15162-15163 apply. Therefore, the decision was made to prepare an Addendum (see further discussion in Section 1.6).

#### **1.4 Previous Environmental Documents Incorporated by Reference**

Consistent with Section 15150 of the CEQA Guidelines, the following documents were used in the Preparation of this Addendum and are incorporated herein by reference:

Final Environmental Impact Report (FEIR) for the San Diego Downtown Community Plan, Centre City Planned District Ordinance, and 10th Amendment to the Redevelopment Plan for the Centre City Project (State Clearinghouse Number 2003041001, certified by the Redevelopment

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Agency (Resolution No. R-04001) and the City Council (Resolution No. R-301265) on March 14, 2006.

Addendum to the FEIR for the 11<sup>th</sup> Amendment to the Redevelopment Plan for the Centre City Redevelopment Project, Amendments to the San Diego Downtown Community Plan, Centre City Planned District Ordinance, Marina Planned District Ordinance, and Mitigation, Monitoring and Reporting Program of the FEIR for the San Diego Downtown Community Plan, Centre City Planned District Ordinance, and the Redevelopment Plan for the Centre City Redevelopment Project certified by the Redevelopment Agency by Resolution R-04193 and by the City Council by R-302932 on July 31, 2007.

Second Addendum to the FEIR for the proposed amendments to the San Diego Downtown Community Plan, Centre City Planned District Ordinance, Marina Planned District Ordinance, and Mitigation Monitoring and Reporting Program certified by the Redevelopment Agency by Resolution R-04508 on April 21, 2010.

Third Addendum to the FEIR for the Residential Emphasis District Amendments to the Centre City Planned District Ordinance certified by the Redevelopment Agency by Resolution R-04510 on April 21, 2010.

## 1.5 Project Description

### Project Location:

The proposed project is located within the San Diego Civic Center in the Core Redevelopment District of the Expansion Sub Area of the Centre City Redevelopment Project, downtown San Diego (**Figure 1**). The Centre City Redevelopment planning area includes approximately 1,500 acres of the metropolitan core of San Diego, bounded by Interstate 5 on the north and east and San Diego Bay on the south and southwest. Centre City is located 15 miles north of the United States International Border with Mexico.

### Project Setting:

The FEIR for the San Diego Downtown Community Plan, Centre City Planned District Ordinance, and Redevelopment Plan for the Centre City Project Area describes the existing setting of Centre City including the Core Redevelopment District. This description is hereby incorporated by reference. Located in the highly urbanized Centre City environment, the project site would occupy the southwestern segment of the four blocks bounded by Third Avenue to the east, First Avenue to the west, A Street to the north, and C Street to the south. The project site is currently occupied by a two-story, 158,000 square-foot meeting and conference venue, known as the San Diego Community Concourse, and a 13-story, 189,000 square-foot office building, known as the City Administration Building. The Community Concourse and City Administration Building were constructed in the mid-1960s and are known to contain hazardous materials, including asbestos-containing materials. Both facilities are proposed by the project to be remediated of any hazardous materials and demolished in compliance

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with existing laws and regulations including rules of the San Diego Air Pollution Control District (SDAPCD). After remediation and demolition of the Community Concourse building, the site would be excavated and a new City Hall would be constructed in its place. The City Administration building would then be remediated and demolished and replaced by new plaza area.

Directly north of the project site is an 11-story above-grade parking structure, known as the Evans Jones Parkade (Parkade). The Parkade provides weekday employee and public parking for the adjacent City office facilities and is planned to remain in place to provide parking for the proposed project. The Parkade is currently attached to the Community Concourse by a second level terrace walkway and removal of this building would include removal of this walkway and minor repairs on the Parkade’s southern façade.

As shown in **Figure 2**, the project vicinity includes a high concentration of multi-story office buildings. The project site is surrounded by older single- and multi- story commercial buildings to the south along C Street. The San Diego Metropolitan Transit System (MTS) Orange Line trolley runs along C Street from One America Plaza, through the Civic Center to San Diego City College. One- to three-story office buildings are present to the north along A Street, and open public parking lots are located north of A Street, west of First Avenue, and east of Third Avenue. The City Operations Building is located west of the project site, adjacent the Evan Jones Parkade. Two older, multi-story commercial buildings are also located west of the project site, and at the southwest corner of A Street and Third Avenue is a 14-story commercial office building. East of the project site, along Third Avenue, is the Civic Center Plaza building and the San Diego Civic Theatre.

**Proposed Project:**

The City of San Diego proposes a redevelopment project to provide a new City Hall facility. The new City Hall is proposed to consolidate nearly all current downtown City employees, currently spread among various buildings through lease agreements, into one new facility. The project includes the development of a 19-story (approximately 300 feet tall) 551,794 gross square foot (gsf) office tower and 16,087 gsf of ground floor retail space. Adjacent to the tower is an approximate one-acre public plaza comprised of hardscape and landscape elements. The plaza is currently estimated to contain approximately 4,000 square feet of landscaped areas, including planters along B and C streets. **Figure 3** depicts the site plan for the project. **Figure 4** shows the ground floor uses of the office tower consisting of the lobby, retail space, loading area, and the City Hall’s “One Stop Customer Service Center” which would facilitate and support the public’s interaction with the City. **Figure 5** illustrates the second floor of the office tower including the council chambers, conference rooms, committee rooms, break rooms, restrooms, and foyer. **Figure 6** represents the third floor of the office tower including the district council suites and reception area. The third through 19<sup>th</sup> floor would be used entirely for office space. **Figure 7** shows how the office space would potentially be laid out on the tenth floor.

Located below grade, under the office tower and plaza, the project proposes to include two levels of underground parking containing approximately 461 vehicles, but

may be increased to approximately 840 stalls through the use of mechanical or robotic parking.

**Figures 8** through **11** depict the building elevations. The proposed City Hall building is designed with an aluminum frame glass curtain wall, which is highlighted with touches of stainless steel metal panels. The curtain wall is comprised of 40% clear vision glass, 30% spandrel, and 20% fritted vision glass. The building base would be a combination of concrete and stone with polished and honed finishes. The Council Chambers is proposed to contain wood exterior cladding and colored glass accents. As a project feature, local materials would be used to minimize the adverse effects associated with the harvest, manufacture, transportation and installation of materials.

The proposed project would include design features to reduce energy use. Metal sunshades with photovoltaic cells would be incorporated into the south, east, and west facades with low-e coatings to be used on all glazing at the east, south and west to reduce solar heat gain from the direct sunlight. The faceted building façade allows for glazing tuned to the particular needs related to each orientation to optimize energy performance. East and west facades would employ an approach of reduced amounts of glazing and vertical external shading devices. These western shading devices would incorporate solar panels where appropriate. Due to the south facing building facades exposure to higher sun angles, horizontal external shading devices would be incorporated to eliminate most of the direct sunlight prior to entering the building. Such exterior shading devices on these building facades would allow diffused light into the building to maximize visual and thermal comfort.

Sustainability elements are included with the objective of achieving LEED Gold or Platinum certification. The design of the Civic Center Complex places the building to the north and east on the block, thereby creating open spaces to optimize access to southern exposure, light and warmth, for both the building and the open spaces. The building floor plates are kept narrow with the intent of increasing occupant access to daylight and views.

Project-related energy targets include:

- Performing 38% better than LEED Silver minimum efficiency – according to LEED NC v2.2
- Having 75% of domestic hot water demand to be met with the solar thermal system
- Offsetting electricity consumption through the incorporation of solar power (photovoltaics)
- Extending the viability of the downtown district’s chilled water loop for use by the rest of the community by tapping cooling energy in off-peak night hours
- Including additional LEED credits available under the proposed changes for LEED 2009. The project expects to achieve the full number of energy credits available in the rating system.

Strategies also include maximizing use of district resources such as thermal storage, hydronic heating and cooling, solar thermal panels to supply domestic hot water, lighting sensors that operate lighting and solar shading, natural ventilation, as well as recapture of wastewater for reuse within the development and surrounding sites. Water

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strategies include a goal of net-zero water use through implementation of water conservation measures and cost-effective use of alternative water resources. This includes reclaiming wastewater to be reused in non-potable applications; meeting and exceeding the City of San Diego's 2003 sustainable buildings policy through the use of cost-effective technologies.

In addition, the San Diego Civic Center Complex proposes a partnership with NRG Energy Inc. (NRG), the downtown district chilled water system operator. The two fold plan is; 1) to connect to the existing NRG chilled water network adjacent to the development site, and 2) provide a satellite NRG chilled / hot water plant to be located on the premises.

The project is located in the Public/Civic land use designation, which provides a center for government, civic, cultural, educational, and other public uses. The project is designed per the Centre City PDO requirements, which allow for a maximum Base Floor Area Ratio (FAR) of 10.0 (Minimum 6.0). The project proposes a FAR of 9.6.

#### **Mitigation Monitoring and Reporting Program:**

As described in the Environmental Secondary Study Checklist and summarized in attached **Appendix A**, the following mitigation measures included in the Mitigation Monitoring and Reporting Program (MMRP) found in volume 1.B.2 of the FEIR will be implemented by the proposed project:

**AQ-B.1-1; HIST-A.1-1; HIST-A.1-3; HIST-B.1-1; PAL-A.1-1**

#### **1.6 Summary of Environmental Impacts and Explanation of the Decision not to prepare a Supplemental or Subsequent EIR**

Based on the analysis in the Environmental Secondary Study Checklist (see Section 2.0) prepared as part of this Addendum, the proposed project would not result in any new significant impacts not discussed in the FEIR, or result in any substantial increases in the severity of impacts identified by the FEIR. In addition, no new information of substantial importance has become available since the FEIR was prepared regarding new significant impacts, or feasibility of mitigation measures or alternatives. Therefore, none of the situations described in CEQA Sections 15162-15163 apply.

#### **1.7 Conclusion**

In summary, the analysis concludes that none of the conditions described in Sections 15162-15163 of the CEQA Guidelines have occurred; therefore, preparation of a Subsequent or Supplemental EIR is not appropriate. Thus, this Addendum to the Downtown Community Plan FEIR has been prepared in accordance with Section 15164 of the CEQA Guidelines. The proposed project does not introduce new significant environmental effects, exacerbate previously identified significant effects, make previously infeasible mitigation measures or alternatives feasible, or require adoption of additional mitigation measures or alternatives to reduce a newly identified significant impact.

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The Centre City Development Corporation (CCDC), the implementing body for the Redevelopment Agency of the City of San Diego, administered the preparation of this Addendum.

  
\_\_\_\_\_  
Signature of Lead Agency Representative

6/16/10  
\_\_\_\_\_  
Date

  
\_\_\_\_\_  
Signature of Preparer

June 16, 2010  
\_\_\_\_\_  
Date

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## 2.0 ENVIRONMENTAL SECONDARY STUDY CHECKLIST

This environmental checklist evaluates the potential environmental effects of the proposed project consistent with the significance thresholds and analysis methods contained in the FEIR for the San Diego Downtown Community Plan, Centre City Planned District Ordinance (PDO), and Redevelopment Plan for the Centre City Project Area. The following table indicates how the impacts of the proposed activity relate to the conclusions of the FEIR. As a result, the impacts are classified into one of the following categories:

- Significant and Not Mitigated (SNM)
- Significant but Mitigated (SM)
- Not Significant (NS)

In accordance with Sections 15168 and 15180 of the CEQA Guidelines, the potential impacts associated with future development within the Centre City Redevelopment Project are addressed in the Final Environmental Impact Report (FEIR) prepared for the San Diego Downtown Community Plan, Centre City Planned District Ordinance and 10th Amendment to the Redevelopment Plan for the Centre City Redevelopment Project, which was certified on March 14, 2006 and the subsequent Addenda certified in 2007 and 2010 cited earlier. These previous documents address the potential effects of future development within the Centre City Redevelopment Project based on buildout forecasts projected from the land use designations, density bonus, and other policies and regulations governing development intensity and density.

Based on this analysis, the FEIR and Addenda concluded that development would result in significant impacts related to the following issues (mitigation and type of impact shown in parentheses):

### Significant but Mitigated Impacts:

- Air Quality: Construction Emissions (AQ-B.1) (D)
- Paleontology: Impacts to Significant Paleontological Resources (PAL-A.1) (D/C)

Consistent with the FEIR analysis, the following issue areas have been identified as Significant and Not Mitigated even with inclusion of the proposed mitigation measures, where feasible:

### Significant and Not Mitigated Impacts:

- Air Quality: Mobile Source Emissions (AQ-A.1) (C)
- Historical Resources: Architectural (HIST-A.1) (D/C)
- Historical Resources: Archeological (HIST-B.1) (D/C)
- Water Quality: Urban Runoff (WQ-A.1) (C)

- Land Use: Physical Changes Related to Transient Activity (LU-B.6) (C)
- Noise: Exterior Traffic Level Increase on Grid Streets (NOI-A.1) (C)
- Noise: Exterior Traffic Noise in Residential Development (NOI-C.1) (C)
- Traffic: Impact on Surrounding Streets (TRF-A.1) (C)
- Traffic: Impact on Freeway Ramps and Segments (TRF-A.2) (C)
- Parking: Excessive Parking Demand (TRF-D.1) (C)

In certifying the FEIR and approving the San Diego Downtown Community Plan, Planned District Ordinance and 10th Amendment to the Redevelopment Plan, the San Diego City Council and Redevelopment Agency adopted a Statement of Overriding Considerations which determined that the unmitigated impacts were acceptable in light of economic, legal, social, technological or other factors including the following.

Overriding Considerations:

- Implement Downtown's Role As Primary Urban Center
- Relieve Growth Pressure On Outlying Communities
- Organize Balanced Mix Of Uses Around Neighborhood Centers
- Maximize Employment
- Capitalize On Transit Opportunities

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>2.1 AESTHETICS/VISUAL QUALITY</b>						
<p>(a) Substantially disturb a scenic resource, vista or view from a public viewing area, including a State scenic highway or view corridor designated by the San Diego Downtown Community Plan? <i>Views of scenic resources such as San Diego Bay, San Diego-Coronado Bay Bridge, Point Loma, Coronado, Petco Park and the downtown skyline are afforded by the public viewing areas within and around the downtown and along view corridor streets within the planning area. Additionally, Highway 163 is a State Scenic Highway entering downtown at 10th Avenue.</i></p> <p><i>The proposed project would be a 19-story (approximately 300-foot tall) high-rise (The Uniform Building Code designates buildings higher than 75 feet as high-rise) building in the Civic/Core District. The project would not be located on a street designated as a view corridor by the San Diego Downtown Community Plan. Therefore the architectural features of the proposed project would not substantially disturb views of the San Diego Bay, San Diego-Coronado Bay Bridge, Point Loma, Coronado, Petco Park and the downtown skyline from public viewing areas. Moreover, Highway 163 is not in close proximity to the proposed project and therefore would not impact this scenic resource. Therefore, significant impacts associated with these resources would not occur.</i></p>					X	X

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
(b) Substantially incompatible with the bulk, scale, color and/or design of surrounding development? <i>The bulk, scale, and design of the proposed project would be compatible with the existing and planned development of the surrounding area (Civic/Core District). As downtown's business district, there is an existing high density of high-rise and large-scale buildings. As stated in the FEIR, further development of such structures would not be out of context in this neighborhood. Therefore, project-level and cumulative impacts associated with this issue would not occur.</i>					X	X
(c) Substantially affect daytime or nighttime views in the area due to lighting? <i>The proposed project would not involve a substantial amount of exterior lighting or include materials that would generate substantial glare. The City's Light Pollution Law (Municipal Code Section 101.1300 et seq.) protects nighttime views (e.g., astronomical activities) and light-sensitive land uses from excessive light generation from development in the downtown area. Therefore, the proposed project's required conformance with these regulations would ensure that direct and cumulative impacts associated with this issue are not significant.</i>					X	X
<b>2.2 AGRICULTURAL RESOURCES</b>						
(a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland) to non-agricultural use? <i>Centre City is an urban downtown environment that does not contain land designated as prime</i>					X	X

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<i>agricultural soils by the Soils Conservation Service, nor does it contain prime farmlands designated by the California Department of Conservation. Therefore, no impact to agricultural resources would occur.</i>						
(b) Conflict with existing zoning for agricultural use, or a Williamson Act contract? <i>The area does not contain, nor is it near land zoned for agricultural use or land subject to a Williamson Act Contract pursuant to Section 512101 of the California Government Code. Therefore, impacts resulting from conflicts with existing zoning for agricultural use or a Williamson Act Contract would not occur.</i>					X	X
<b>2.3 AIR QUALITY</b>						
(a) Conflict with or obstruct implementation of an applicable air quality plan, including the County's Regional Air Quality Strategies or the State Implementation Plan? <i>The proposed project is consistent with the Public/Civic land use designation of the San Diego Downtown Community Plan and Centre City PDO, the land use policies and regulations of which are in accordance with those of the Regional Air Quality Strategy (RAQS). Thus, the proposed project would not conflict with, but would help implement, the RAQS with its compact, high intensity land use. No impact to the applicable air quality plan would occur.</i>					X	X
(b) Expose sensitive receptors to substantial air contaminants including, but not limited to, criteria pollutants, smoke, soot, grime, toxic fumes and substances, particulate matter, or any other emissions that may endanger			X			X

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<p>human health? <i>The proposed project could involve the exposure of sensitive receptors to substantial air contaminants during short-term construction activities and over the long-term operation of the project. The potential for short-term, temporary impacts to sensitive receptors during construction activities would be mitigated to below a level of significance through compliance with the City's mandatory standard dust control measures and the dust control and construction equipment emission reduction measures required by FEIR Mitigation Measure AQ-B.1-1 (see Appendix A).</i></p> <p><i>The proposed project could involve the exposure of sensitive receptors to air contaminants over the long-term operation of the project, such as carbon monoxide exposure (commonly referred to as CO "hot spots") due to traffic congestion near the project site. However, the FEIR concluded that development within the downtown would not expose sensitive receptors to significant levels of any of the substantial air contaminants. Since the land use designation of the proposed development does not differ from the land use designation assumed in the FEIR analysis, the project would not expose sensitive receptors to substantial air contaminants beyond the level assumed by the FEIR. Additionally, the proposed project is not located close enough to any industrial activities to be impacted by any emissions potentially associated with such activities. Therefore, impacts associated with this issue would not</i></p>						

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<i>be significant. Project impacts associated with the generation of substantial air contaminants are discussed below in 3.c.</i>						
<p>(c) Generate substantial air contaminants including, but not limited to, criteria pollutants, smoke, soot, grime, toxic fumes and substances, particulate matter, or any other emissions that may endanger human health? <i>Implementation of the proposed project could result in potentially adverse air quality impacts related to the following air emission generators: construction and mobile-sources. Site preparation activities and construction of the proposed project would involve short-term, potentially adverse impacts associated with the creation of dust and the generation of construction equipment emissions. The clearing, grading, excavation and construction activities associated with the proposed project would result in dust and equipment emissions that, when considered together, could endanger human health. Implementation of FEIR Mitigation Measure AQ-B.1-1 (see Appendix A) would reduce dust and construction equipment emissions generated during construction of the proposed project to a level below significance.</i></p> <p><i>The air emissions generated by automobile trips associated with the proposed project would not exceed air quality significance standards established by the San Diego Air Pollution Control District. However, the project's mobile source emissions, in combination with dust generated during the construction of the project, would contribute</i></p>		X	X			

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<i>to the significant and unmitigated cumulative impact to air quality identified in the FEIR. The proposed project does not propose any uses that would significantly increase stationary-source emissions in the downtown planning area; therefore, impacts from stationary sources would be not significant.</i>						
<b>2.4 BIOLOGICAL RESOURCES</b>						
(a) Substantially effect, either directly or through habitat modifications, any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by local, state or federal agencies? <i>Due to the highly urbanized nature of the downtown area, there are no sensitive plant or animal species, habitats, or wildlife migration corridors within the area. In addition, the ornamental trees and landscaping included in the proposed project are considered of no significant value to the native wildlife in their proposed location. Therefore, no impact associated with this issue could occur.</i>					X	X
(b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, and regulations by local, state or federal agencies? <i>As identified in the FEIR, the San Diego Downtown Community Plan area is not within a subregion of the San Diego County Multiple Species Conservation Program (MSCP). Therefore, impacts associated with substantial adverse effects on riparian habitat or other sensitive natural</i>					X	X

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<i>communities identified in local or regional plans, policies, and regulations by local, state or federal agencies would not occur.</i>						
<b>2.5 GEOLOGY AND SOILS</b>						
<p>(a) Substantial health and safety risk associated with seismic or geologic hazards? <i>The proposed project site is in a seismically active region. There are no known active or potentially active faults located on the project site. However, the project site is located within a block of the San Diego Fault and within the Rose Canyon Fault Zone, which is designated as an Earthquake Fault Zone by the California Department of Mines and Geology. A seismic event on this fault could cause significant ground-shaking on the proposed project site.</i></p> <p><i>Although the potential for geologic hazards (landslides, liquefaction, slope failure, and seismically-induced settlement) is considered low due to the site's moderate to non-expansive geologic structure, such hazards could nevertheless occur. Conformance with, and implementation of, all seismic-safety development requirements, including all applicable requirements of the Alquist-Priolo Zone Act, the seismic design requirements of the International Building Code (IBC), the City of San Diego Notification of Geologic Hazard procedures, and all other applicable requirements would ensure that the potential impacts associated with seismic and geologic hazards are not significant.</i></p>					X	X

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>2.6 GREENHOUSE GAS EMISSIONS</b>						
(a)Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment? <i>California's Assembly Bill (AB) 32, the Global Warming Solutions Act of 2006, codified the State's greenhouse gas (GHG) emissions target by requiring the State's GHG emissions to be reduced to 1990 levels by 2020. To achieve these GHG reductions outlined in AB 32, there will have to be widespread reductions of GHG emissions across the California economy. Some of the reductions will come in the form of changes in vehicle emissions and mileage, changes in the sources of electricity, and increases in energy efficiency by existing facilities as well as other measures. The remainder of the necessary GHG reductions will come from requiring new facility development to have lower carbon intensity than "Business-as-Usual" (BAU), or existing, conditions. In addition, State Senate Bill (SB) 97 directed the Office of Planning and Research (OPR) to adopt CEQA Guidelines concerning the effects and mitigation of GHG emissions. The new CEQA Guidelines became effective in March 2010. The new CEQA Guidelines require either a quantitative or qualitative discussion of the amount of GHG emissions that would result from the project, determination if those emissions would result in a significant impact on the environment, and identification of feasible mitigation measures to reduce GHG emissions if a significant impact is found.</i>					X	X

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<p><i>According to the new CEQA Guidelines, the lead agency may provide a quantitative or qualitative means of analysis when analyzing GHG emissions. In this case, the significance of the proposed Project can be evaluated in terms of how the project would increase or reduce GHG emissions compared to the existing environmental setting as well as if the project would result in emissions consistent with the broad economy-wide goals reflected in AB 32.</i></p> <p><i>The existing setting is that the City employees currently occupy the 347,000 square foot (SF) Civic Center Complex as well as other leased space near the project site. Existing GHG emissions include emissions associated with employee and public commutes to these locations, business travel between the different offices, the building energy emissions associated with electricity and natural gas consumption, the waste generated in these offices that is landfilled (resulting in methane gas emissions), the water consumed in these offices (and the energy emissions associated with water transport), and the wastewater from these offices treated at local wastewater treatment facilities (and the associated electricity and methane emissions).</i></p> <p><i>With the construction of the new Civic Center, City functions would be consolidated compared to the existing setting. The leased commercial office space would then be available for other commercial use. In the short run, some of the leased space may</i></p>						

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<p><i>remain vacant; during this interim period, the project will not result in an increase in GHG emissions and may actually reduce GHG emissions relative to the baseline conditions. In the long run, the previously occupied commercial office space will be occupied by commercial uses and much of the profile of baseline GHG emissions will return as it relates to those uses. Thus, in the long-run, when examining the City of San Diego as a whole, the GHG emissions of the Civic Center will result in an increase of GHG emissions as it will facilitate growth in leased commercial use over time.</i></p> <p><i>Construction and operation of the redeveloped and expanded San Diego Civic Center Complex would result in GHG emissions. The redeveloped and expanded Civic Center Complex would consolidate nearly all current downtown City employees into one facility. Project construction and operation would include numerous sustainability features, including recycling of construction debris, the use of local and resource-efficient materials, building and water efficiency measures, on-site renewable energy, and on-site wastewater treatment for landscaping water use, among others. Taken together, these measures would help the facility achieve LEED Gold or Platinum certification. The building, while being expanded, would reduce per-capita consumption of resources related to City functions, as the new facility will be far more efficient than the dispersed current facilities utilized by City departments. The project</i></p>						

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<p><i>itself would not create more City employees, but would instead transfer employees that currently occupy office space in other downtown buildings. In addition, as a LEED Gold or LEED Platinum municipal building, the Project would set precedence for resource efficiency, not only for City operations but for all future downtown development.</i></p> <p><i>The proposed project would result in approximately 12,610 metric tons of carbon dioxide equivalent (MTCO<sub>2e</sub>) per year, which is approximately 38% less than the BAU scenario (20,341 MTCO<sub>2e</sub>) (see Appendix B). As noted above, relative to baseline, the project will in time result in an increase in GHG emissions as the formerly occupied commercial space is occupied by new commercial/office uses and the baseline GHG emissions of the formerly leased space returns. Thus, the “new” emissions of the new Civic Center Complex must be evaluated for whether they are consistent with overall AB 32 goals of increasing efficiency over time. AB 32, at a state level, calls for a reduction of emissions in 2020 by approximately 29 percent compared to a “Business as Usual” case. The Civic Center Complex, through the design features noted above, would substantially improve on the 29 percent reduction level, as mentioned above, by reducing electricity and natural gas use, by consolidating functions into a single building (avoiding travel between office locations), by being located in the heart of downtown San Diego with probably the greatest access to transit in all of San Diego County, by</i></p>						

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<p><i>saving on water, by using renewable materials in construction and by recycling construction debris.</i></p> <p><i>Thus, by design the proposed project would be consistent with the goals of recent statewide GHG legislation, which aims to reduce statewide GHG emissions. For example, the project incorporates project features included in Senate Bill (SB) 375, which aims to reduce GHG emissions by promoting resource-efficient infill development near existing mass transit facilities. In addition, the project would aid in the achievement of the goals of AB 32, in that the project would increase the overall efficiency of GHG emissions at the project site. GHG emissions and their impact on global climate change are discussed in the context of projected California emissions in the absence of any GHG reducing activities (BAU) and in the context of already adopted state measures that will act to reduce GHG emissions in the future. As such, although the project would increase emissions compared to baseline, it can be concluded that the project would result in a decrease in emissions compared to BAU conditions at a level exceeding that necessary on state level to meet AB 32 goals and would thus not result in significant GHG emissions. Therefore, this impact is considered less than significant.</i></p>						

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
(b) Conflict with any applicable plan, policy, or regulation of an agency adopted for the purpose of reducing the emissions of greenhouse gas? <i>As stated under 2.6(a), operation of the proposed Project would be consistent with statewide plans, policies, and regulations adopted for the purpose of reducing GHG emissions. The resource-efficient, infill, and transit-proximity aspects of the project would be consistent with the language and direction of SB 375 and AB 32. In addition, the project would be consistent with current City of San Diego programs and policies, including the Sustainability Community Program and Climate Protection Action Plan, which aim to reduce GHG emission from City operations through building efficiency retrofits, incorporation of solar energy systems, and water conservation. As such, the project would not conflict with any State and local plan, policy or regulation, adopted for the purpose of reducing GHG emissions. Therefore, this impact is considered less than significant.</i>					X	X
<b>2.7 HAZARDS AND HAZARDOUS MATERIALS</b>						
(a) Substantial health and safety risk related to onsite hazardous materials? <i>The FEIR states that contact with, or exposure to, hazardous building materials, soil and ground water contaminated with hazardous materials, or other hazardous materials could adversely affect human health and safety during short-term construction or long-term operation of a development. The site is currently occupied by two buildings, both constructed</i>					X	X

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<p><i>in the mid-1960s and known to contain hazardous materials including asbestos-containing materials. The buildings also contain large basement areas, used for storage and facility maintenance, and the central plant which provides power, heating and cooling to the entire complex. The buildings are to be remediated of any hazardous materials and demolished. The proposed project is subject to federal, state, and local agency regulations for the handling of hazardous building materials and waste. Compliance with all applicable requirements of the County of San Diego Department of Environmental Health and federal, state, and local regulations for the handling of hazardous building materials and wastes would ensure that potential health and safety impacts caused by exposure to onsite hazardous materials are not significant during short-term, construction activities. In addition, herbicides and fertilizers associated with the landscaping of the project could pose a significant health risk over the long-term operation of the project. However, the proposed project's adherence to existing mandatory federal, state, and local regulations controlling these materials would ensure that long-term health and safety impacts associated with onsite hazardous materials over the long-term operation of the project are not significant.</i></p>						
(b) Be located on or within 2,000 feet of a site that is included on a list of hazardous materials sites compiled pursuant to Government Code § 65962.5 and, as a result,					X	X

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
would it create a significant hazard to the public or the environment? <i>The proposed project is not located on the State of California Hazardous Waste and Substances Sites List. However, as indicated by the Phase I Environmental Site Assessment report prepared for the site by SCS Engineers in 2010 (Appendix C), a number of facilities store or use hazardous materials, generate hazardous wastes, or have leaking underground storage tanks within the site vicinity. Due to the nature of historic and current land uses located throughout the downtown planning area, there is a high potential for encountering hazardous material sites identified on registers compiled pursuant to Government Code § 65962.5. However, the FEIR states that significant impacts to human health and the environment regarding hazardous waste sites would be avoided through compliance with mandatory federal, state, and local regulations and no mitigation measures would be required.</i>						
(c) Substantial safety risk to operations at San Diego International Airport? <i>The proposed project is within the boundaries of the Airport Influence Area of the Airport Land Use Compatibility Plan (ALUCP) for San Diego International Airport (SDIA). The project is subject to FAA determination of no hazard to air navigation prior to issuance of any development permit. Therefore, impacts associated with this issue are not anticipated to occur.</i>					X	X

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
(d) Substantially impair implementation of an adopted emergency response plan or emergency evacuation plan? <i>The project does not propose any features that would affect an emergency response or evacuation plan. Therefore, no impact associated with this issue is anticipated.</i>					X	X
<b>2.8 HISTORICAL RESOURCES</b>						
(a) <i>Substantially impact a significant historical resource, as defined in § 15064.5? A Historical Resources Technical Report was prepared by ICF International to evaluate a grouping of resources in and around the project area for National Register of Historic Places (NRHP), California Register of Historical Resources (CRHR), and San Diego Historical Landmark listing. Four buildings were evaluated as part of the San Diego Community Concourse including the City of San Diego Administration Building (City Hall), San Diego Convention and Performing Arts Center (itself commonly called the San Diego Concourse), the San Diego Civic Theatre, and the Community Concourse Parking Garage (commonly known as the Evan V. Jones Parkade). In addition, two nearby resources that were constructed after the Community Concourse were also evaluated. These include the City Operations Building and the Bow Wave Fountain, located just above the original Community Concourse plaza. (For more detailed description, refer to Appendix D, Historical Resources Technical Report).</i>  <i>As documented in the historical technical</i>	X	X				

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<p><i>report prepared for this project, the San Diego Community Concourse, which includes the four buildings as listed above, are all historical resources for CEQA purposes. For their collective contribution in spearheading a revitalization of downtown San Diego and for their association with the group San Diegans, Inc., the buildings and features constructed as part of the Community Concourse appear to be eligible under San Diego Historical Landmark Criterion A ("Exemplifies or reflects special elements of the city's, community's, or neighborhood's historical, archeological, cultural, social, economic, political, aesthetic, engineering, landscaping, or architectural development") and city Criterion B ("Is identified with persons or events significant in local, state, or national history"). The Community Concourse as a complex possesses a period of significance of 1964 to 1965.</i></p> <p><i>Within the Community Concourse, the San Diego Civic Theatre and Community Concourse Parking Garage appear to be eligible under city Criterion C ("Embodies distinctive characteristics of a style, type, period, or method of construction or is a valuable example of the use of indigenous materials or craftsmanship"). Additionally, the garage is eligible under city Criterion D ("Is representative of the notable work of a master builder, designer, architect, engineer, landscape architect, interior designer, artist, or craftsman"). For possessing a variety of design features and elements that are</i></p>						

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<p><i>sensitively handled and uncommon on the vernacular parking structure building type, the resource appears to be CRHR eligible under Criterion 3 (“The resource embodies the distinctive characteristics of a type, period, region, or method of construction or represents the work of an important creative individual or possesses high artistic values”).</i></p> <p><i>The proposed project would entail demolishing two buildings within the Community Concourse boundary, the City Administration Building and the San Diego Convention and Performing Arts Center. Demolition of contributing elements of the historical resource would result in a significant effect under Section 15064.5 (b) (2)(C) of the State CEQA Guidelines.</i></p> <p><i>The Community Concourse Parking Garage would not be demolished; however, the garage would be altered on the south elevation’s second level terrace walkway. The garage was determined to be eligible individually as a San Diego Historical Landmark and CRHR eligible and therefore is considered a historical resource for the purposes of CEQA. Alteration of a historical resource in a way that materially affects the physical characteristics that convey its significance in an adverse manner could be a significant effect under Section 15064.5 (b) (2)(A) of the State CEQA Guidelines.</i></p> <p><i>The Bow Wave Fountain would not be demolished and will be preserved in place. The fountain was determined to be eligible individually as a San Diego Historical</i></p>						

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<p><i>Landmark and CRHR eligible and therefore is considered a historical resource for the purposes of CEQA. The proposed project would not cause a significant effect to the fountain because it would not be demolished or materially altered in an adverse manner.</i></p> <p><i>Because the two buildings within the Community Concourse boundary, the City Administration Building and the San Diego Convention and Performing Arts Center, are scheduled for demolition, and the parking garage would be altered on the south elevation's second level terrace walkway, implementation of Mitigation Measures HIST-A.1-1 and HIST-A.1-3, as applicable to Local Criteria A and B structures (City Administrative Building and San Diego Convention and Performing Arts Center respectively) and as applicable to Local Criteria C and CRHR eligible under Criterion 3 (Community Concourse Parking Garage) is required.</i></p> <p><i>Consistent with the conclusions of the FEIR, implementation of this mitigation measure and any conditions of approval stemming from them (as may be ultimately approved through applicable City review procedures in accordance with Chapters 11-14 of the City's Land Development Code) would not be sufficient to reduce the identified impacts associated with the loss of these Local Register historic resources in conjunction with an unknown number of similar resources within the Downtown area to below a level of significance. Therefore, consistent with the analysis of the FEIR, the proposed project</i></p>						

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<p>would contribute to significant impacts associated with this issue.</p> <p>The City Council adopted a Statement of Overriding Considerations for this potential significant impact identified in the FEIR, thereby acknowledging that the benefits of implementing the Downtown Community Plan outweigh the potential for impacts resulting from such actions (refer to P.9 of this Addendum). Because of the adoption of Overriding Considerations for this impact, there is no further environmental review required for the proposed demolition of the City Administration Building and the San Diego Convention and Performing Arts Center if the applicable City procedures are followed as outlined in Chapters 11-14 of the City's Land Development Code and conditions are applied to the project as outlined in Mitigation Measures HIST-A.1-1 and HIST-A.1-3 (see Appendix A).</p>						
<p>(b) Substantially impact a significant archaeological resource pursuant to § 15064.5, including the disturbance of human remains interred outside of formal cemeteries? <i>The likelihood of encountering archaeological resources is greatest for projects that include grading and/or excavation of areas on which past grading and/or excavation activities have been minimal (e.g., surface parking lots). Since archaeological resources have been found within inches of the ground surface in the downtown planning area, even minimal grading activities can impact these resources. In addition, the likelihood of</i></p>	X	X				

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<p><i>encountering subsurface human remains during construction and excavation activities, although considered low in the downtown area, is possible. Thus, the excavation, demolition, and surface clearance activities associated with development of the proposed project (two levels underground) could have potentially adverse impacts to archaeological resources, including buried human remains. Implementation of FEIR Mitigation Measure HIST-B.1-1, (see Appendix A) would minimize, but not fully mitigate, these potential impacts. Since the potential for archaeological resources and human remains on the proposed project site cannot be confirmed until grading is conducted, the exact nature and extent of impacts associated with the proposed project cannot be predicted. Consequently, the required mitigation may or may not be sufficient to reduce these direct project-level impacts to below a level of significance. Therefore, project-level impacts associated with this issue remain potentially significant and not fully mitigated, and consistent with the analysis of the FEIR. Furthermore, project-level significant impacts to important archaeological resources would contribute to the potentially significant and unmitigated cumulative impacts identified in the FEIR.</i></p>						
<p>(c) Substantially impact a unique paleontological resource or site or unique geologic feature? <i>The proposed project site is underlain by the Bay Point Formation, which has high paleontological resource</i></p>			X	X		

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<p><i>potential. The FEIR concludes that development would have potentially adverse impacts to paleontological resources if grading and/or excavation activities are conducted beyond a depth of 1-3 feet. The project's proposal for two levels of subterranean parking would involve excavation beyond the FEIR standard, resulting in potentially significant impacts to paleontological resources. However, implementation of FEIR Mitigation Measure PAL-A.1-1 (see Appendix A) would ensure that the proposed project's potentially direct impacts to paleontological resources are not significant. Furthermore, the project would not impact any resources outside of the project site. The mitigation measures for direct impacts fully mitigate for paleontological impacts, therefore, the project's contribution to cumulative impacts to paleontological resources would be significant but mitigated because the same measures that mitigate direct impacts would also mitigate for any cumulative impacts.</i></p>						
<b>2.9 HYDROLOGY AND WATER QUALITY</b>						
<p>(a) Substantially degrade groundwater or surface water quality? <i>The project proposes soil excavation at a depth that may surpass known groundwater levels, which would indicate that groundwater dewatering might be required. Compliance with the requirements of either (1) the San Diego Regional Water Quality Control Board under a National Pollution Discharge Elimination system general permit for construction dewatering (if dewatering is discharged to</i></p>		<b>X</b>			<b>X</b>	

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<p><i>surface waters), or (2) the City of San Diego Public Utilities, Water and Wastewater Department (if dewatering is discharged into the City's sanitary sewer system under the Industrial Waste Pretreatment Program), and (3) the mandatory requirements controlling the treatment and disposal of contaminated dewatered groundwater would ensure that potential impacts associated with construction dewatering and the handling of contaminated groundwater are not significant. In addition, Best Management Practices (BMPs) required as part of the local Storm Water Pollution Prevention Plan (SWPPP) would ensure that short-term water quality impacts during construction are not significant. The proposed project would result in hard structure areas and other impervious surfaces that would generate urban runoff with the potential to degrade groundwater or surface water quality. However, implementation of BMPs required by the local Standard Urban Stormwater Mitigation Program (SUSMP) and Stormwater Standards would reduce the project's long-term impacts. Thus, adherence to the state and local water quality controls would ensure that direct impacts to groundwater and surface water quality would not be significant.</i></p> <p><i>Despite not resulting in direct impacts to water quality, the FEIR found that the urban runoff generated by the cumulative development in the downtown would contribute to the existing significant cumulative impact to the water quality of</i></p>						

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<i>San Diego Bay. No mitigation other than adherence to existing regulations has been identified in the FEIR to feasibly reduce this cumulative impact to below a level of significance. Consistent with the FEIR, the project's contribution to the cumulative water quality impact will remain significant and unmitigated.</i>						
(b) Substantially increase impervious surfaces and associated runoff flow rates or volumes? <i>The proposed project site is currently developed and covered with impervious surfaces. Implementation of the proposed project would result in impervious surfaces similar to those that exist onsite. Therefore, the proposed project would not substantially increase the runoff volume entering the storm drain system. Therefore, impacts associated with this issue are not significant. (Impacts associated with the quality of urban runoff are analyzed in Section 8.a.)</i>					X	X
(c) Substantially impede or redirect flows within a 100-year flood hazard area? <i>The project site is not located within a 100-year floodplain. Similarly, the proposed project would not affect offsite flood hazard areas, as no 100-year floodplains are located downstream. Therefore, impacts associated with these issues are not significant.</i>					X	X
(d) Substantially increase erosion and sedimentation? <i>The project site is currently developed with impervious surfaces. The hydrology of the proposed site would not be substantially altered by implementation of the proposed project as the site would maintain a</i>					X	X

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<i>similar quantity of impervious surfaces and, therefore, the proposed project would not substantially increase the long-term potential for erosion and sedimentation. However, the potential for erosion and sedimentation could increase during the short-term during site preparation, excavation, and other construction activities. The proposed project's compliance with regulations mandating the preparation and implementation of a SWPPP would ensure that impacts associated with erosion and sedimentation are not significant.</i>						
<b>2.10 LAND USE AND PLANNING</b>						
(a) Physically divide an established community? <i>The proposed project does not propose any features or structures that would physically divide an establishment community. Impacts associated with this issue would not occur.</i>					X	X
(b) Substantially conflict with the City's General Plan and Progress Guide, Downtown Community Plan or other applicable land use plan, policy, or regulation? <i>The project site is located within the Civic/Core District of the Centre City Planned District under the San Diego Downtown Community Plan. The project site is within the Centre City PDO designated Public/Civic Land Use District. The Public/Civic Use Land Use District provides a center for government, civic, cultural, educational, and other public uses.</i>  <i>The Centre City PDO permits a base minimum Floor Area Ratio (FAR) of 6.0 on the project site and a maximum FAR of 10.0. In conformance with PDO requirements, the</i>					X	X

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<p><i>project proposes a FAR of 9.6.</i></p> <p><i>As discussed in 7.c, the proposed project is within the jurisdiction of the Airport Land Use Compatibility Plan (ALUCP) for San Diego International Airport (SDIA) and is subject to FAA determination of no hazard to air navigation prior to issuance of any development permit. In addition, the proposed project would not conflict with other applicable land use plans, policies, or regulations. The proposed project complies with the goals and requirements of the San Diego Downtown Community Plan, and meets all applicable standards of the PDO. Therefore, no significant direct or cumulative impacts associated with an adopted land use plan would occur.</i></p>						
<p>(c) Substantial incompatibility with surrounding land uses? <i>Sources of land use incompatibility include lighting, shading, industrial activities, and noise. The proposed project would not result in, or be subject to, adverse impacts due to substantially incompatible land uses. Compliance with the City's Light Pollution Ordinance would ensure that land use incompatibility impacts related to the proposed project's emitting of, and exposure to, lighting are not significant. In addition, the FEIR concludes that existing mandatory regulations addressing land use compatibility with industrial activities would ensure that people served by the proposed project are not subject to potential land use incompatibilities (potential land use incompatibilities resulting from hazardous materials and air emissions are evaluated</i></p>					X	X

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<i>elsewhere in this Addendum). Similarly, the project site is not directly adjacent to any major planned neighborhood parks that could be significantly impacted by shading from the project. Potentially significant impacts associated with the project's incompatibility with traffic noise on adjacent grid streets are discussed in Sections 11.b and 11.c. No impacts associated with incompatibility with surrounding land use would occur.</i>						
(d) Substantially impact surrounding communities due to sanitation and litter problems generated by transients displaced by downtown development? <i>Although not expected to be a substantial direct impact of the project because substantial numbers of transients are not known to congregate onsite, the project, in tandem with other downtown redevelopment activities, would have a significant cumulative impact on surrounding communities resulting from sanitation problems and litter generation by transients who are displaced from downtown into surrounding canyons and vacant land as discussed in the FEIR. Continued support of Homeless Outreach Teams (HOTs) and similar transient outreach efforts will reduce, but not fully mitigate, the adverse impacts to surrounding neighborhoods caused by the transient relocation. Therefore, the proposed project would result in cumulatively significant and not fully mitigated impacts to surrounding neighborhoods.</i>		<b>X</b>			<b>X</b>	

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>2.11 MINERAL RESOURCES</b>						
(a) Substantially reduce the availability of important mineral resources? <i>The FEIR states that the viable extraction of mineral resources is limited in the Centre City due to its urbanized nature and the fact that the area is not designated as having high mineral resource potential. Therefore, no impact associated with this issue would occur.</i>					X	X
<b>2.12 NOISE</b>						
(a) Substantial noise generation? <i>The proposed project would not result in substantial noise generation from any stationary sources over the long-term. Short-term construction noise impacts would be avoided by adherence to construction noise limitations imposed by the City's Noise Abatement and Control Ordinance. In addition, the proposed project is consistent with the land use designation for this site in the Downtown Community Plan. Therefore, as significant noise impacts were not identified in the Downtown Community Plan, the proposed project is not expected to result in substantial noise increases. Thus, no significant impact related to noise generation would be associated with the proposed project. However, the project would, in combination with other development in the downtown, contribute to the cumulatively significant traffic noise increases on nine street segments. This impact is consistent with the analysis of the FEIR and considered cumulatively significant and not mitigated.</i>		X			X	

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<p>(b) Substantial exposure of required outdoor residential open spaces or public parks and plazas to noise levels (e.g. exposure to levels exceeding 65 dBA CNEL)? <i>The proposed redevelopment is a public/civic use project by the PDO, and the common outdoor space proposed by the project is not required by the PDO. Therefore, substantial exposure of required residential open spaces to noise levels exceeding the 65 dB (A) CNEL standard would not occur; project-level and cumulative impacts associated with this issue are not significant.</i></p> <p><i>As detailed in response 11 (a) above, the proposed project would not result in direct significant noise impacts due to generation of vehicular traffic. Thus, direct significant noise impacts to outdoor spaces at adjacent existing or future residential units would not occur. However, the project would, in combination with other development in the downtown, contribute to cumulatively significant traffic noise increases. This cumulative noise effect could adversely affect residential outdoor spaces. This impact is consistent with the analysis of the FEIR and considered cumulatively significant and not mitigated.</i></p>		<b>X</b>			<b>X</b>	
<p>(c) Substantial interior noise within habitable rooms (e.g. levels in excess of 45 dBA CNEL)? <i>Project impacts associated with substantial interior noise within habitable rooms would not occur since the proposed project does not include habitable rooms.</i></p>					<b>X</b>	<b>X</b>

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>2.13 POPULATION AND HOUSING</b>						
(a) Substantially induce population growth in an area? <i>The proposed project is consistent in land use with the San Diego Downtown Community Plan. Adverse physical changes associated with the population growth generated by the proposed project would be consistent with the assumptions analyzed throughout the FEIR. Therefore, project-level and cumulative impacts associated with this issue are not significant.</i>					X	X
(b) Substantial displacement of existing housing units or people? <i>No housing units currently exist on the project site. Therefore, project-level and cumulative impacts associated with this issue would not occur.</i>					X	X
<b>2.14 PUBLIC SERVICES AND UTILITIES</b>						
(a) Substantial adverse physical impacts associated with the provision of new schools? <i>The FEIR concludes that the additional student population anticipated at build out of the downtown area would require the construction of at least one additional school. In and of itself, the proposed project, an official government building, would not generate students and would not warrant construction of a new school facility. However, the project would contribute, in combination with other development in downtown to the need for at least one additional school in downtown, consistent with the analysis of the FEIR. Nevertheless, as indicated in the FEIR, the specific future location of a new school is unknown at present time. Pursuant to</i>					X	X

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<i>Section 15145 of the California Environmental Quality Act (CEQA), analysis of the physical changes in the downtown planning area, which may occur from future construction of schools, would be speculative and no further analysis of their impacts is required. However, construction of new schools would be subject to CEQA. Environmental documentation prepared pursuant to CEQA would identify potentially significant impacts and appropriate mitigation measures.</i>						
(b) Substantial adverse physical impacts associated with the provision of new libraries? <i>The FEIR concludes that, cumulatively, development in the downtown would generate the need for a new Main Library and possibly several smaller libraries within the downtown. In and of itself, the proposed project would not generate additional demand necessitating the construction of new library facilities. However, the proposed project would contribute to the cumulative need for new library facilities in the downtown identified in the FEIR. Nevertheless, the specific future location of these facilities (except the Main Library) is unknown at present time. Pursuant to Section 15145 of the California Environmental Quality Act (CEQA), analysis of the physical changes in the downtown planning area, which may occur from future construction of these public facilities, would be speculative and no further analysis of their impacts is required (The environmental impacts of the Main Library were analyzed in a Mitigated Negative Declaration prepared</i>					X	X

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<i>by the City of San Diego). Environmental documentation prepared pursuant to CEQA would identify potentially significant impacts and appropriate mitigation measures.</i>						
(c) Substantial adverse physical impacts associated with the provision of new fire protection/emergency facilities? <i>The FEIR does not conclude that the cumulative development of the downtown would generate additional demand necessitating the construction of new fire protection/emergency facilities. Since the land use designation of the proposed development is consistent with the land use designation assumed in the FEIR analysis, the project would not generate a level of demand for fire protection/emergency facilities beyond the level assumed by the FEIR. However, the FEIR reports that the San Diego Fire Department is in need of securing sites for two new fire stations in the downtown area. Pursuant to Section 15145 of the California Environmental Quality Act (CEQA), analysis of the physical changes in the downtown planning area that may occur from future construction of potential fire station facilities would be speculative and no further analysis of the impact is required. However, one site has been identified at the southeast corner of Pacific Highway and Cedar Street, and a Secondary Study has been prepared for the Fire Station No. 2 project. The construction of additional facilities would be subject to CEQA. Environmental documentation prepared pursuant to CEQA would identify</i>					X	X

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<i>significant impacts and appropriate mitigation measures.</i>						
(d) Substantial adverse physical impacts associated with the provision of new law enforcement facilities? <i>The FEIR analyzes impacts to law enforcement service resulting from the cumulative development of the downtown and concludes the construction of new law enforcement facilities would not be required. Since the land use designation of the proposed development is consistent with the land use designation assumed in the FEIR analysis, the project would not generate a level of demand for law enforcement facilities beyond the level assumed by the FEIR. However, the need for a new facility could be identified in the future. Pursuant to Section 15145 of the California Environmental Quality Act (CEQA), analysis of the physical changes in the downtown planning area that may occur from the future construction of law enforcement facilities would be speculative and no future analysis of their impacts would be required. However, construction of new law enforcement facilities would be subject to CEQA. Environmental documentation prepared pursuant to CEQA would identify potentially significant impacts and appropriate mitigation measures.</i>					X	X
(e) Substantial adverse physical impacts associated with the provision of new water transmission or treatment facilities? <i>A Water Supply Assessment prepared by the City of San Diego Public Utilities, Water and Wastewater Department on May 3, 2010</i>					X	X

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<p><i>states that the project can be served by existing supplies and facilities under the 2005 Urban Water Management Plan (UWMP) for the City of San Diego based on a Water Demand Study (included as Appendix E). The Water Demand Study shows water use for the project by calculating existing demand, proposed demand, and proposed demand less the existing demand with Leadership in Energy and Environmental Design (LEED) demand reductions included. Existing demand in gallons per day (gpd) for the existing use on the project site is calculated to be approximately 15,456 or (0.015) million gallons per day (mgd). Proposed gpd calculated for the project resulted in approximately 31,945 or (0.032) million gallons per day. However, this increase does not take into account LEED water efficiencies which the proposed facilities would use to conserve water. The LEED Reduction Factors include 50% for landscaping such as drip irrigation, 40% for waterless urinals, dual flush toilets and low flow faucets, and 25% for gray water use on landscaping. However, feasibility of the project implementing the use of gray water is unknown at this time. Implementation of these LEED conservation measures would reduce water use of the proposed project to 13,000 gpd or (0.013) mgd, which would result in an increase of water use on the site of 0.004 mgd. In addition, there may be opportunity of additional savings through the use of cooling tower water and agreements with NRG Energy Inc. who provide cooling tower water services. Therefore, potential</i></p>						

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<p><i>direct impacts would not be significant.</i></p> <p><i>Buildout of the 2006 Downtown Community Plan would generate 4.2% more water demand than planned for in the adopted 2005 UWMP. The proposed project would contribute to this cumulative water supply impact. However, the San Diego County Water Authority has clearly stated in its 2005 UWMP that additional supplies are available through its Metropolitan Water District of Southern California (MWD) purchasing agreements should any of the member agencies demand water in excess of planned supplies. Additionally, the project water demand (an increase of only 0.004 mgd) is not a considerable contribution to the cumulative impact. Therefore, the project's contribution to this cumulative impact would not be significant.</i></p>						
<p>(f) Substantial adverse physical impacts associated with the provision of new storm water facilities? <i>The FEIR concludes that the cumulative development of the downtown would not impact the existing downtown storm drain system. Since implementation of the proposed project would result in an amount of impervious surfaces similar to the existing use of the site, the amount of runoff volume entering the storm drain system would not create demand for new storm water facilities. Direct and cumulative impacts associated with this issue are not considered significant.</i></p>					X	X
<p>(g) Substantial adverse physical impacts associated with the provision of new</p>					X	X

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
wastewater transmission or treatment facilities? <i>The FEIR concludes that new wastewater treatment facilities would not be required to address the cumulative development of the downtown. In addition, sewer improvements that may be needed to serve the proposed project are categorically exempt from environmental review under CEQA as stated in the FEIR. Therefore, impacts associated with this issue would not be significant.</i>						
(h) Substantial adverse physical impacts associated with the provision of new landfill facilities? <i>The FEIR concludes that cumulative development within the downtown would increase the amount of solid waste to the Miramar Landfill and contribute to the eventual need for an alternative landfill. Although the proposed project would generate a higher level of solid waste than the existing use of the site, implementation of a mandatory Waste Management Plan and compliance with the applicable provisions of the San Diego Municipal Code would ensure that both short-term and long-term project-level impacts are not significant. However, the project would contribute, in combination with other development activities in downtown, to the cumulative increase in the generation of solid waste sent to Miramar Landfill and the eventual need for a new landfill as identified in the FEIR. The location and size of a new landfill is unknown at this time. Pursuant to Section 15145 of the California Environmental Quality Act (CEQA),</i>					X	X

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<i>analysis from the physical changes that may occur from future construction of landfills would be speculative and no further analysis of their impacts is required. However, construction or expansion of a landfill would be subject to CEQA. Environmental documentation prepared pursuant to CEQA would identify potentially significant impacts of the proposed project and appropriate mitigation measures. Therefore, cumulative impacts of the proposed project are also considered not significant.</i>						
<b>2.15 PARKS AND RECREATIONAL FACILITIES</b>						
(a) Substantial increase in the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated? <i>The FEIR discusses impacts to parks and recreational facilities and the maintenance thereof and concludes that buildout of the Downtown Community Plan would not result in significant impacts associated with this issue. Since the land use designation of the proposed development does not differ from the land use designation assumed in the FEIR analysis, the project would not generate a level of demand for parks and recreational facilities beyond the level assumed by the FEIR. Therefore, substantial deterioration of existing neighborhood or regional parks would not occur or be substantially accelerated as a result of the proposed project. No significant impacts with this issue would occur.</i>					X	X

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>2.16 TRANSPORTATION/TRAFFIC</b>						
<p>(a) Cause the LOS on a roadway segment or intersection to drop below LOS E? <i>Based on Centre City Cumulative Traffic Generation Rates contained in the May 2003 San Diego Municipal Code Trip Generation Manual, the worst-case scenario for automobile trips by the project is 2,338 Average Daily Trips (ADT) based on a trip generation rate of 10 ADT per 1,000 square feet of government office (total of 2,048 ADT) and 18 ADT per 1,000 square feet of retail space (total of 290 ADT) for the proposed project (Fehr and Peers Traffic Memorandum, Appendix F). Anticipated traffic generation does not exceed the 2,400 ADT threshold used for determining the need for a traffic study established in the FEIR, therefore the project is not considered a large project that would generate significant automobile trips.</i></p> <p><i>With buildout of the Downtown Community Plan, a total of 62 intersections are anticipated to operate at LOS F. The proposed project's direct impacts on downtown roadway segments or intersections would not be significant; however, the traffic generated by the proposed project would, in combination with the traffic generated by other downtown development, contribute to the significant cumulative traffic impacts projected in the FEIR to occur on a number of downtown roadway segments and intersections, and streets within neighborhoods surrounding the Plan area at buildout of the downtown. The FEIR includes mitigation measures to address</i></p>		<b>X</b>			<b>X</b>	

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<i>these impacts, but the identified measures may or may not be able to fully mitigate these cumulative impacts due to constraints imposed by bicycle and pedestrian activities and the land uses adjacent to affected roadways. These mitigation measures are not the responsibility of the proposed project, and are therefore not included in Appendix A. Therefore, consistent with the analysis of the FEIR, the proposed project would contribute to significant cumulative impacts associated with this issue.</i>						
(b) Cause the LOS on a freeway segment to drop below LOS E or cause a ramp delay in excess of 15 minutes? <i>The FEIR concludes that development within the downtown will result in significant cumulative impacts to freeway segments and ramps serving the downtown planning area. Since the land use designation of the proposed development is consistent with the land use designation assumed in the FEIR analysis, the proposed development would contribute on a cumulative-level to the substandard LOS F identified in the FEIR on all freeway segments in the downtown area and several ramps serving the downtown. TRF-A.2.1-1 would reduce these impacts to the extent feasible, but not to below the level of significance. This mitigation measure is not the responsibility of the proposed project, and therefore is not included in Appendix A. The FEIR concludes that the uncertainty associated with implementing freeway improvements and limitations in increasing ramp capacity limits the feasibility of fully</i>		<b>X</b>			<b>X</b>	

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<i>mitigating impacts to these facilities. Thus, the proposed project's cumulative-level impacts to freeways would remain significant and unavoidable, consistent with the analysis of the FEIR. The proposed project would not have a direct impact on freeway segments and ramps.</i>						
(c) Create an average demand for parking that would exceed the average available supply? <i>The proposed project is considered mixed-use (office and retail) per the Centre City PDO. The Centre City PDO requires a minimum of 1.5 off-street parking spaces per 1,000 square feet and is exempt from retail parking requirements since the retail space would total less than 30,000 square feet. The project would result in the need for a minimum of 828 parking spaces. The Evan Jones Parkade, located adjacent to the project site, is an approximate 1,100 stall above-grade parking structure that currently provides parking for the existing City buildings proposed by the project for demolition. This parking structure would continue to provide parking for the new City Hall facility. In addition, implementation of the project would include two levels of subterranean parking providing approximately 461 parking spaces that may be increased through the use of mechanical or robotic parking to approximately 840. Therefore, with parking provided by the proposed underground parking garage and the Evan Jones Parkade, the proposed project would not result in a significant direct impact on downtown parking.</i>		<b>X</b>			<b>X</b>	

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<p><i>However, demand generated by cumulative downtown development would exceed the amount of parking provided by such development in accordance with the PDO. Implementation of FEIR Mitigation Measure TRF-D.1-1 would reduce, but not fully mitigate, the significant cumulative impact of excessive parking demand (this mitigation measure is not the responsibility of the proposed project, and therefore is not included in Appendix A). Therefore, the proposed project would contribute to the cumulatively significant and not mitigated shortfall in parking supply anticipated to occur throughout the downtown by the FEIR.</i></p>						
<p>(d) Substantially discourage the use of alternative modes of transportation or cause transit service capacity to be exceeded? <i>The proposed project does not include any features that would discourage the use of alternatives modes of transportation. In addition, the project site is located adjacent to an existing light-rail trolley station on C Street and there is regular bus service adjacent to the project site on A Street and First Avenue, and elsewhere in the Civic/Core District. The project's proximity to several existing and planned community serving uses, including nearby shopping and recreational activities, also encourages walking. Additionally, as stated in the FEIR, SANDAG has indicated that transit facilities should be sufficient to serve the downtown population without exceeding capacity.</i></p>					X	X

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>2.17 MANDATORY FINDINGS OF SIGNIFICANCE</b>						
(a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory? <i>As indicated in the FEIR, due to the highly urbanized nature of the downtown area, no sensitive plant or animal species, habitats, or wildlife migration corridors are located in the Centre City area. However, the project does have potential to eliminate important examples of major periods of California history or prehistory at the project level. No other aspects of the project would substantially degrade the environment. Cumulative impacts described in the subsection 16.b below.</i>	<b>X</b>	<b>X</b>				
(b) Does the project have impacts that are individually limited, but cumulatively considerable? (“Cumulatively considerable” means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)? <i>As acknowledged in the FEIR, implementation of the Downtown Community Plan, PDO, and Redevelopment Plan will result in cumulative impacts associated with: air quality, historical resources, paleontological</i>		<b>X</b>				

Issues and Supporting Information	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<i>resources, physical changes associated with transient activities, noise, traffic, parking, and water quality. This project will contribute to those impacts. Implementation of the mitigation measures identified in the FEIR would reduce some significant impacts; however, the impacts would remain significant and unavoidable. Cumulative impacts would not be greater than those identified in the FEIR.</i>						
(c) Does the project have environmental effects that will cause substantial adverse effects on human beings, either directly or indirectly? <i>As described elsewhere in this study, the proposed project would result in significant and unmitigated impacts. Those impacts associated with air and noise could have substantial adverse effects on human beings. However, these impacts would be no greater than those assumed in the FEIR. Implementation of the mitigation measures identified in the FEIR would mitigate many, but not all, of the significant impacts.</i>	<b>X</b>	<b>X</b>				



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Figure 1  
Location Map  
June 2010



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Source: ZGF



Figure 2  
Vicinity Map  
June 2010



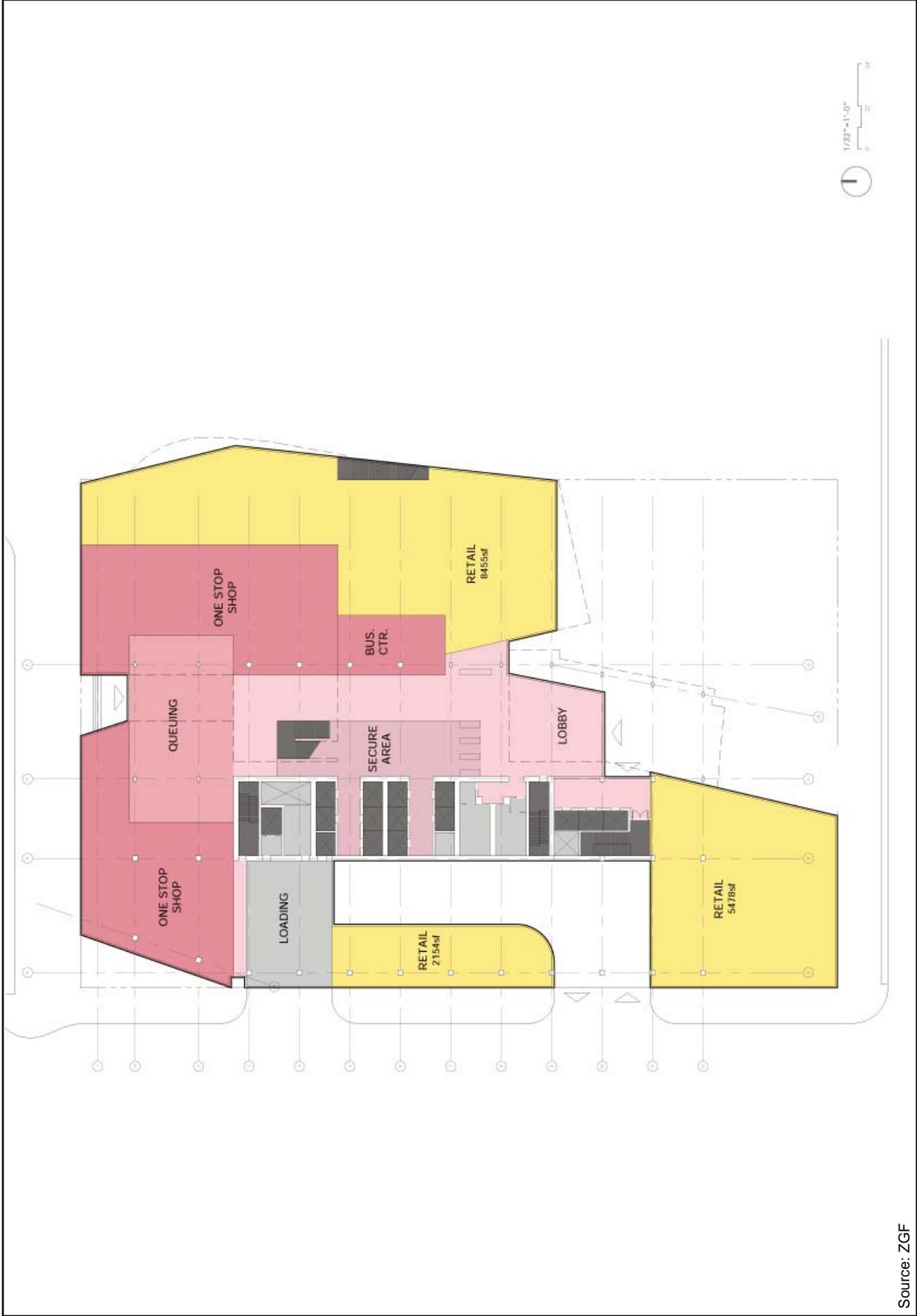
Source: ZGF



NO SCALE

Civic Center Complex Project  
CCDC Addendum to FEIR

Figure 3  
Site Plan  
June 2010



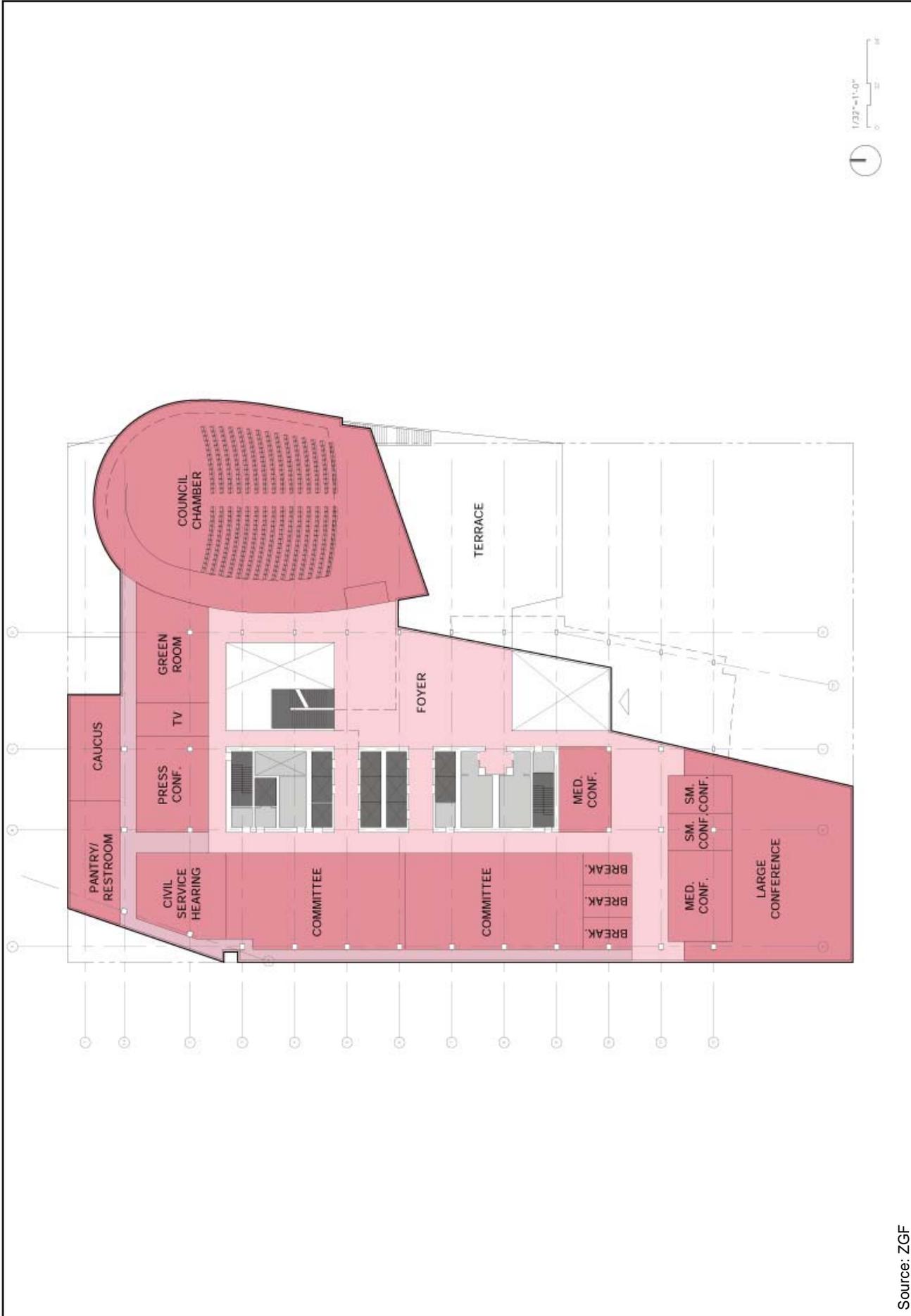
Source: ZGF



NO SCALE

Civic Center Complex Project  
 CCDC Addendum to FEIR

Figure 4  
 First Floor Plan  
 June 2010



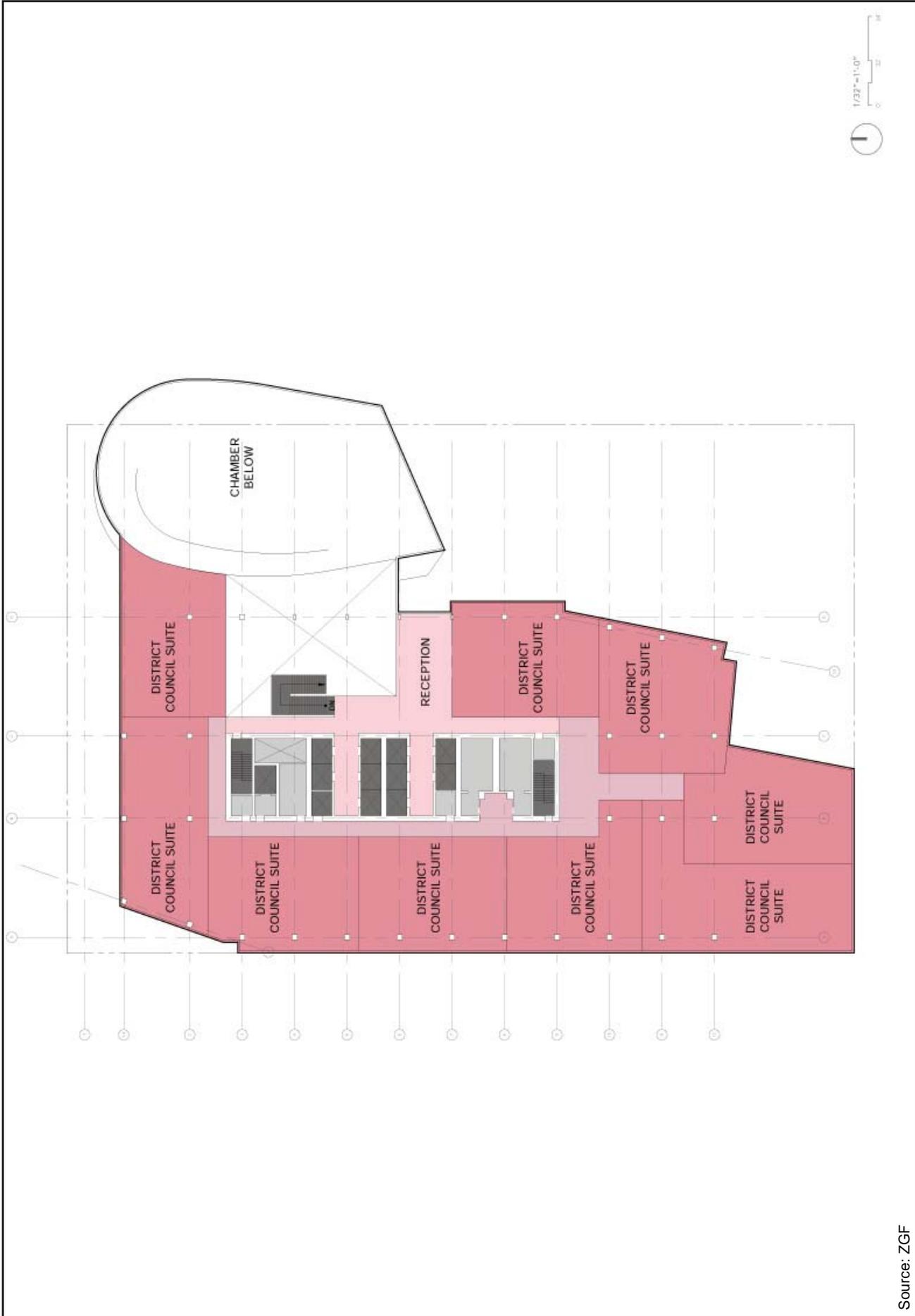
Source: ZGF



NO SCALE

Civic Center Complex Project  
CCDC Addendum to FEIR

Figure 5  
Second Floor Plan  
June 2010



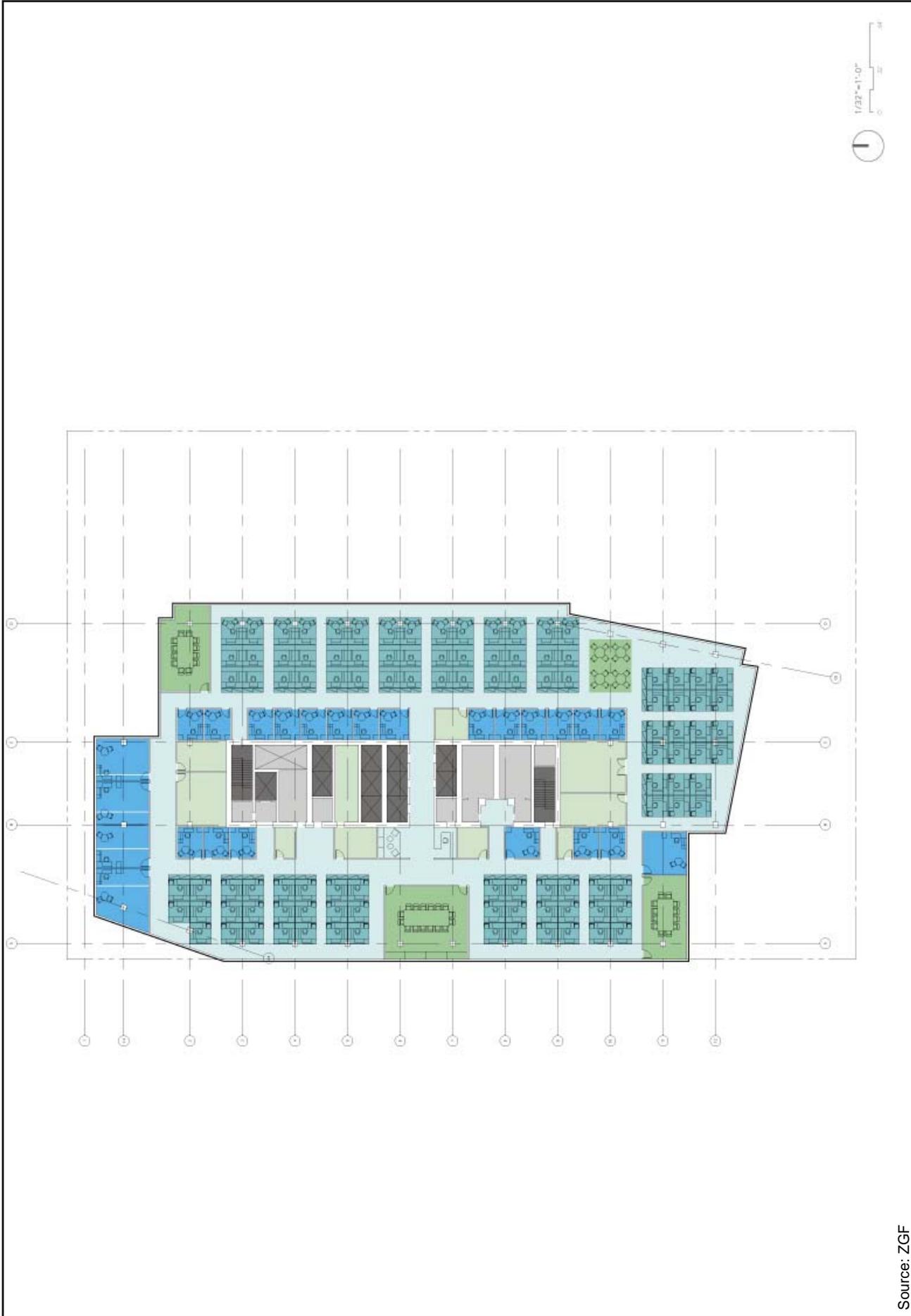
Source: ZGF



NO SCALE

Civic Center Complex Project  
CCDC Addendum to FEIR

Figure 6  
Third Floor Plan  
June 2010



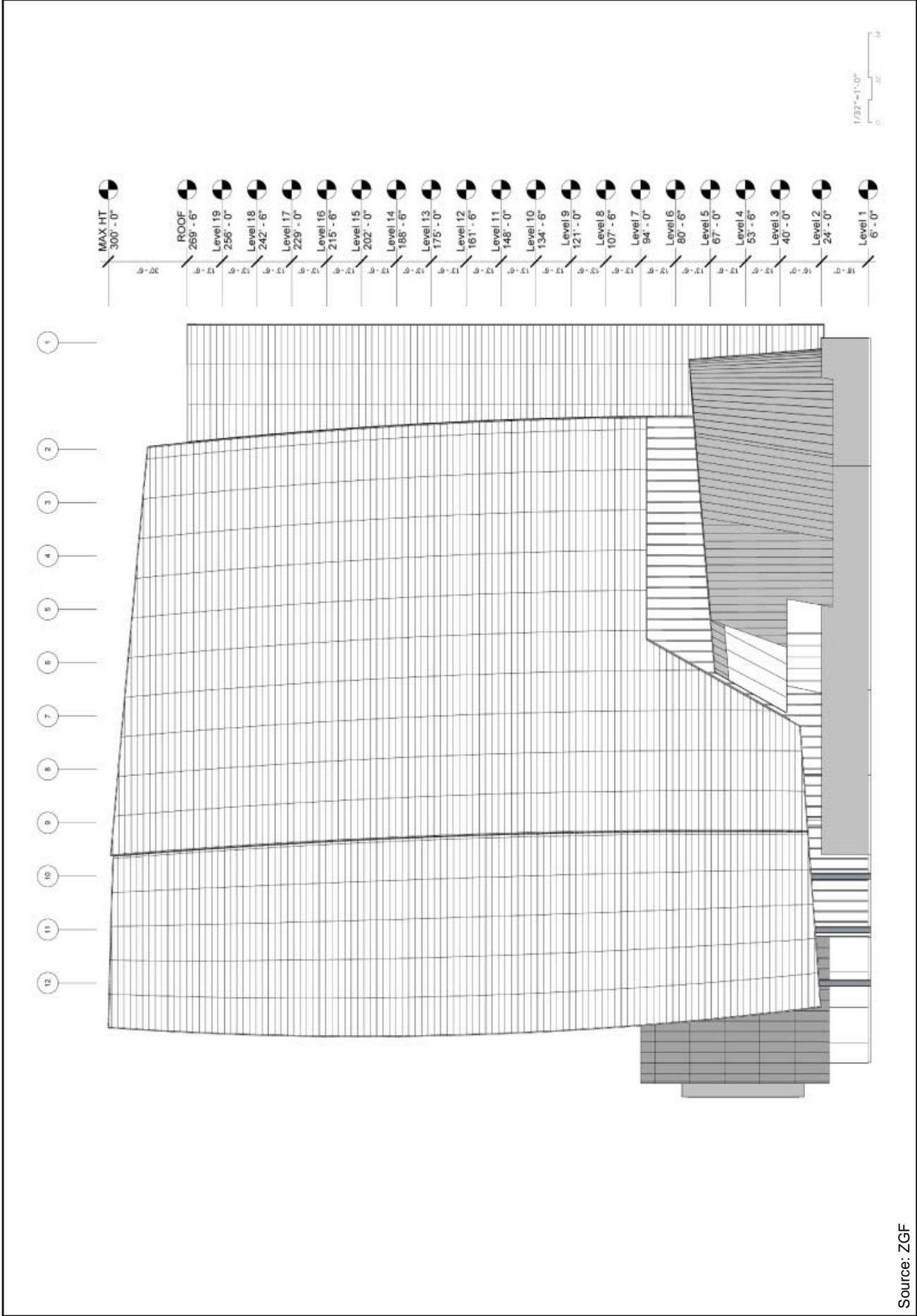
Source: ZGF



NO SCALE

Civic Center Complex Project  
CCDC Addendum to FEIR

Figure 7  
Tenth Floor Plan  
June 2010



Source: ZGF



NO SCALE

Civic Center Complex Project  
CCDC Addendum to FEIR

Figure 8  
East Elevation  
June 2010

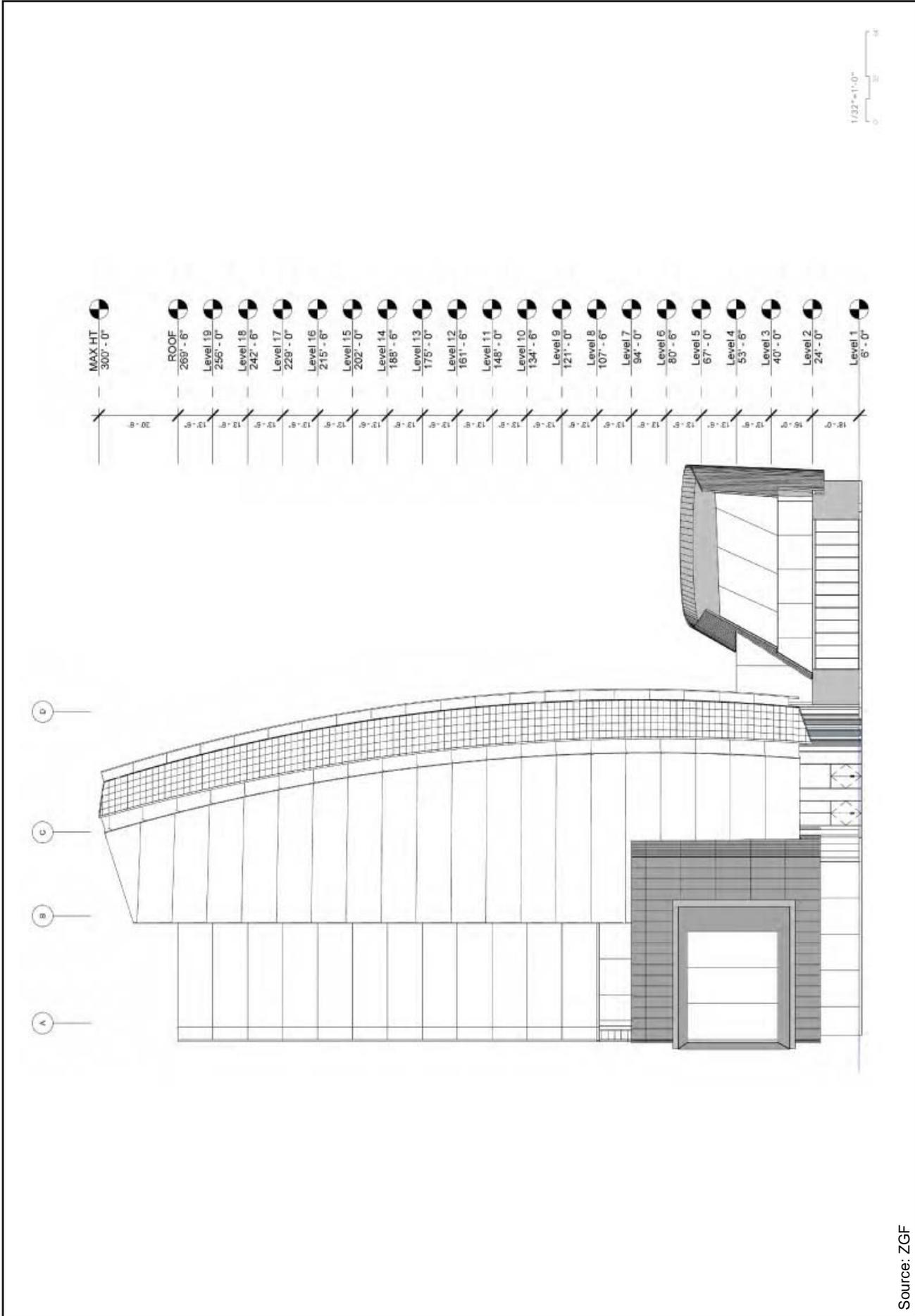


Figure 9  
South Elevation  
June 2010

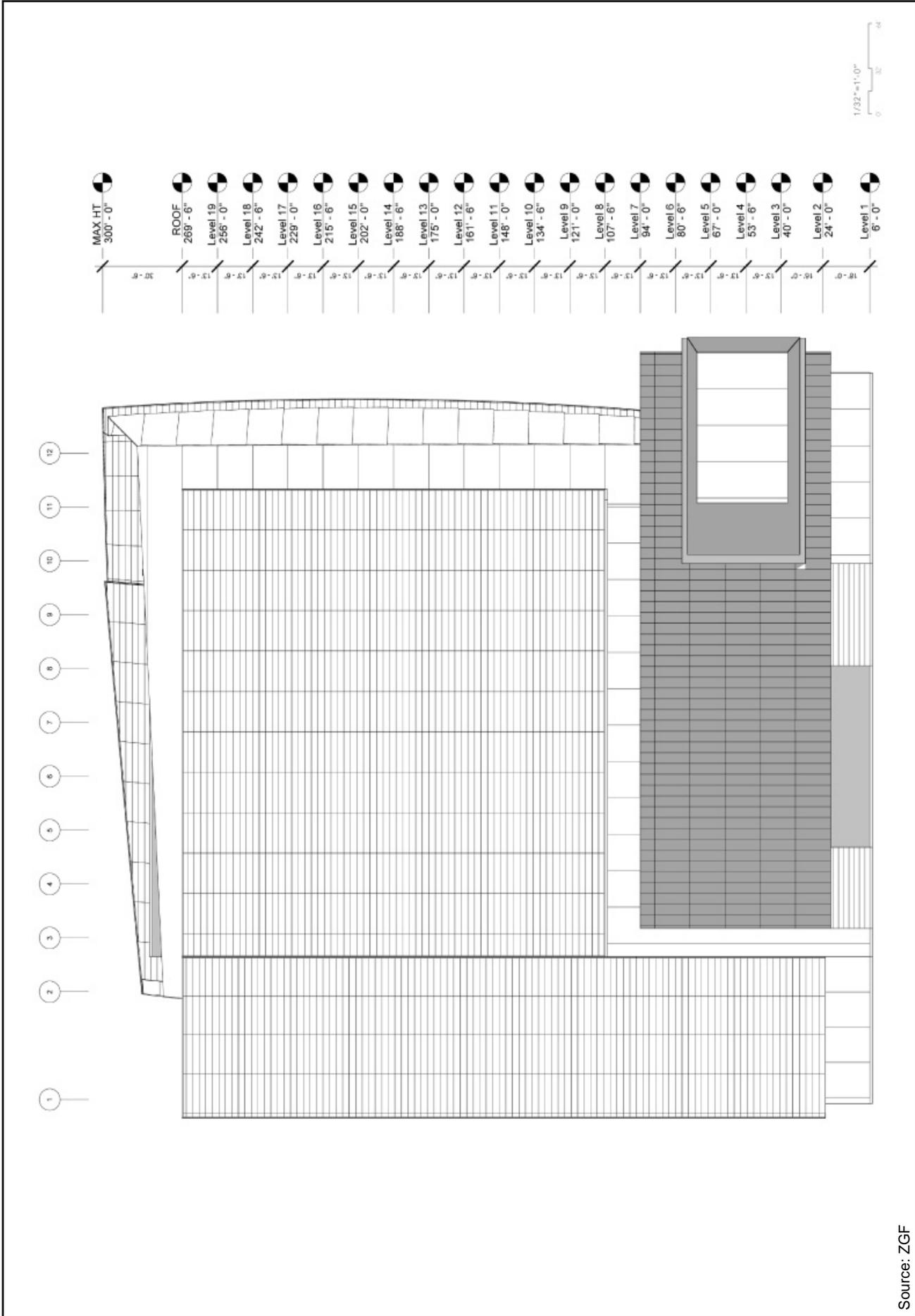


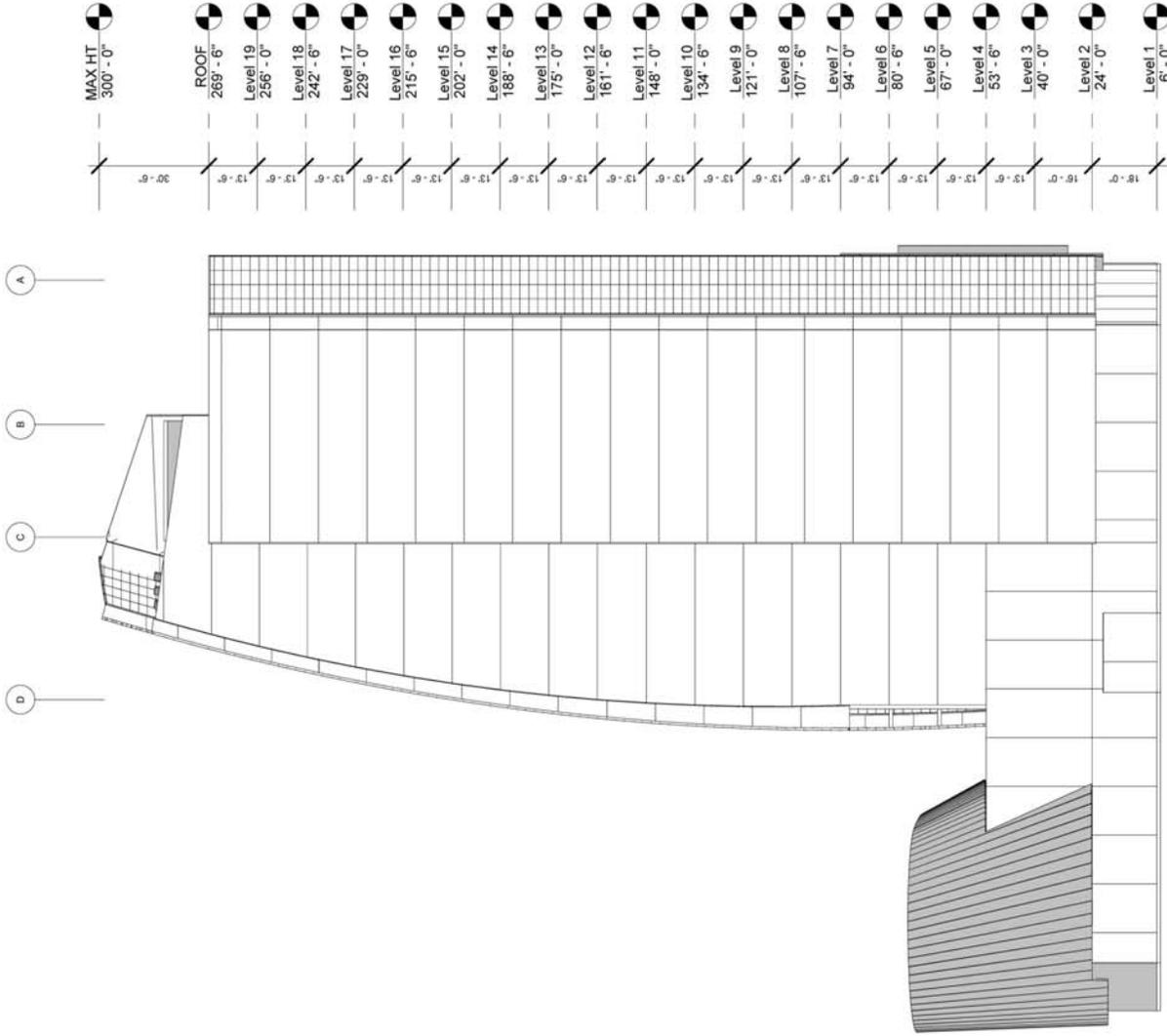
Figure 10  
West Elevation  
June 2010

Source: ZGF



NO SCALE

Civic Center Complex Project  
CCDC Addendum to FEIR



Source: ZGF



NO SCALE

Civic Center Complex Project  
CCDC Addendum to FEIR

Figure 11  
North Elevation  
June 2010

SIGNIFICANT IMPACT(S)	MITIGATION MEASURE(S)	IMPLEMENTATION TIME FRAME	IMPLEMENTATION RESPONSIBILITY	VERIFICATION RESPONSIBILITY
<b>AIR QUALITY (AQ)</b>				
<p><b>Impact AQ-B.1:</b>  <b>Dust and construction equipment engine emissions generated during grading and demolition would impact local and regional air quality. (Direct and Cumulative)</b></p>	<p><i>Mitigation Measure AQ-B.1-1:</i> Prior to approval of a Grading or Demolition Permit, the City shall confirm that the following conditions have been applied, as appropriate:</p> <ol style="list-style-type: none"> <li>1. Exposed soil areas shall be watered twice per day. On windy days or when fugitive dust can be observed leaving the development site, additional applications of water shall be applied as necessary to prevent visible dust plumes from leaving the development site. When wind velocities are forecast to exceed 25 miles per hour, all ground disturbing activities shall be halted until winds that are forecast to abate below this threshold.</li> <li>2. Dust suppression techniques shall be implemented including, but not limited to, the following:               <ol style="list-style-type: none"> <li>a. Portions of the construction site to remain inactive longer than a period of three months shall be seeded and watered until grass cover is grown or otherwise stabilized in a manner acceptable to the CCDC.</li> <li>b. On-site access points shall be paved as soon as feasible or watered periodically or otherwise stabilized.</li> <li>c. Material transported offsite shall be either sufficiently watered or securely covered to prevent excessive amounts of dust.</li> <li>d. The area disturbed by clearing, grading, earthmoving, or excavation operations shall be minimized at all times.</li> </ol> </li> <li>3. Vehicles on the construction site shall travel at speeds less than 15 miles per hour.</li> <li>4. Material stockpiles subject to wind erosion during construction activities, which will not be utilized within three days, shall be covered with plastic, an alternative cover deemed equivalent to plastic, or sprayed with a nontoxic chemical stabilizer.</li> <li>5. Where vehicles leave the construction site and enter adjacent public streets, the streets shall be swept daily or washed down at the end of the work day to remove soil tracked onto the paved surface. Any visible track-out extending for more than fifty (50) feet from the access point</li> </ol>	<p>Prior to Demolition or Grading Permit (Design)</p>	<p>Developer</p>	<p>City</p>

SIGNIFICANT IMPACT(S)	MITIGATION MEASURE(S)	IMPLEMENTATION TIME FRAME	IMPLEMENTATION RESPONSIBILITY	VERIFICATION RESPONSIBILITY
	<p>shall be swept or washed within thirty (30) minutes of deposition.</p> <ol style="list-style-type: none"> <li>6. All diesel-powered vehicles and equipment shall be properly operated and maintained.</li> <li>7. All diesel-powered vehicles and gasoline-powered equipment shall be turned off when not in use for more than five minutes, as required by state law.</li> <li>8. The construction contractor shall utilize electric or natural gas-powered equipment in lieu of gasoline or diesel-powered engines, where feasible.</li> <li>9. As much as possible, the construction contractor shall time the construction activities so as not to interfere with peak hour traffic. In order to minimize obstruction of through traffic lanes adjacent to the site, a flag-person shall be retained to maintain safety adjacent to existing roadways, if necessary.</li> <li>10. The construction contractor shall support and encourage ridesharing and transit incentives for the construction crew.</li> <li>11. Low VOC coatings shall be used as required by SDAPCD Rule 67. Spray equipment with high transfer efficiency, such as the high volume-low pressure (HPLV) spray method, or manual coatings application such as paint brush hand roller, trowel, spatula, dauber, rag, or sponge, shall be used to reduce VOC emissions, where feasible.</li> <li>12. If construction equipment powered by alternative fuel sources (LPG/CNG) is available at comparable cost, the developer shall specify that such equipment be used during all construction activities on the development site.</li> <li>13. The developer shall require the use of particulate filters on diesel construction equipment if use of such filters is demonstrated to be cost-competitive for use on this development.</li> <li>14. During demolition activities, safety measures as required by City/County/State for removal of toxic or hazardous materials shall be utilized.</li> <li>15. Rubble piles shall be maintained in a damp state to minimize dust generation.</li> <li>16. During finish work, low-VOC paints and efficient transfer systems shall be utilized, to the extent possible.</li> <li>17. If alternative-fueled and/or particulate filter-equipped construction</li> </ol>			

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	equipment is not feasible, construction equipment shall use the newest, least-polluting equipment, whenever possible.			
<b>HISTORICAL RESOURCES (HIST)</b>				
<p><b>Impact HIST-A.1:</b>  <b>Future development in downtown could impact architectural resources. (Direct and Cumulative)</b></p>	<p><i>Mitigation Measure HIST-A.1-1:</i> For construction or development permits that may impact potentially historical resources which are 45 years of age or older and which have not been evaluated for local, state and federal historic significance, a site specific survey shall be required in accordance with the Historical Resources Regulations in the Land Development Code. Based on the survey and the best information available, City Staff to the Historical Resources Board (HRB) shall determine whether historical resources exist, whether potential historical resource(s) is/are eligible for designation as designated historical resource(s) by the HRB, and the precise location of the resource(s). The identified historical resource(s) may be nominated for HRB designation as a result of the survey pursuant to Chapter 12, Article 3, Division 2, Designation of Historical Resource procedures, of the Land Development Code.</p> <p>All applications for construction and development permits where historical resources are present on the site shall be evaluated by City Staff to the HRB pursuant to Chapter 14, Article 3, Division 2, Historical Resources Regulations of the Land Development Code.</p> <ul style="list-style-type: none"> <li>• <b>National Register-Listed/Eligible, California Register-Listed/Eligible Resources:</b> Resources listed in or formally determined eligible for the National Register or California Register and resources identified as contributing within a National or California Register District, shall be retained onsite and any improvements, renovation, rehabilitation and/or adaptive reuse of the property shall ensure its preservation and be consistent with the Secretary of the Interior’s Standards for the Treatment of Historic Properties (1995) and the associated Guidelines.</li> <li>• <b>San Diego Register-Listed Resources:</b> Resources listed in the San Diego Register of historical Resources, or determined to be a contributor to a San Diego Register District, shall, whenever possible, be retained on-site. Partial retention, relocation, or demolition of a resource shall only be permitted according to Chapter 14, Article 3, Division 2, Historical Resources Regulations of the Land Development Code.</li> </ul>	<p>Prior to Development Permit (Design)</p> <p>Prior to Demolition, Grading, and/or Building Permit (Design)</p> <p>Prior to Certificate of Occupancy (Implementation)</p>	<p>Developer</p>	<p>CCDC/City</p>

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	<p><b>Mitigation Measure HIST-A.1-3:</b> If a designated or potential historical resource (“historical resource”) as defined in the Land Development Code would be demolished, the following measure shall be implemented in accordance with Chapter 14, Article 3, Division 2, Historical Resources Regulations of the Land Development Code.</p> <p><b>I. Prior to Issuance of a Demolition Permit</b></p> <p>A. A Documentation Program (DP) shall be submitted to City Staff to the Historic Resources Board (HRB) (“City Staff”) for review and approval and shall include the following:</p> <ol style="list-style-type: none"> <li>1. Photo Documentation               <ol style="list-style-type: none"> <li>(a) Documentation shall include professional quality photo documentation of the structure prior to demolition with 35mm black and white photographs, 4x6 standard format, taken of all four elevations and close-ups of select architectural elements, such as, but not limited to, roof/wall junctions, window treatments, decorative hardware. Photographs shall be of archival quality and easily reproducible.</li> <li>(b) Xerox copies or CD of the photographs shall be submitted for archival storage with the City of San Diego Historical Resources Board and the CCDC Project file. One set of original photographs and negatives shall be submitted for archival storage with the California Room of the City of San Diego Public Library, the San Diego Historical Society and/or other relative historical society or group(s).</li> </ol> </li> <li>2. Required drawings               <ol style="list-style-type: none"> <li>(a) Measured drawings of the building’s exterior elevations depicting existing conditions or other relevant features shall be produced from recorded, accurate measurements. If portions of the building are not accessible for measurement, or cannot be reproduced from historic sources, they should not be drawn, but clearly labeled as not accessible. Drawings produced in ink on translucent material or archivally stable material (blue-line drawings are acceptable). Standard drawing sizes are 19" x 24" or 24" x 36", standard scale is 1/4" = 1 foot.</li> <li>(b) One set of measured drawings shall be submitted for</li> </ol> </li> </ol>			

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	<p>archival storage with the City of San Diego Historical Resources Board, the CCDC Project file, the South Coastal Information Center, the California Room of the City of San Diego Public Library, the San Diego Historical Society and/or other historical society or group(s).</p> <p>B. Prior to the first Precon Meeting, City Staff shall verify that the DP has been approved.</p> <p>C. In addition to the Documentation Program, the Applicant shall comply with any other conditions contained in the Site Development Permit pursuant to Chapter 14, Article 3, Division 2, Historical Resources Regulations of the Land Development Code.</p>			
<p><b>Impact HIST-B.1:</b>  <b>Development in downtown could impact significant buried archaeological resources. (Direct and Cumulative)</b></p>	<p><i>Mitigation Measure HIST-B.1-1:</i> If the potential exists for direct and/or indirect impacts to significant buried archaeological resources, the following measures shall be implemented in coordination with a Development Services Department designee and/or City Staff to the Historic Resources Board (HRB) ("City Staff") in accordance with Chapter 14, Article 3, Division 2, Historical Resources Regulations of the Land Development Code. Prior to issuance of any permit that could directly affect an archaeological resource, City Staff shall assure that all elements of the MMRP are performed in accordance with all applicable City regulations and guidelines by an Archaeologist meeting the qualifications specified in Appendix B of the San Diego Land Development Code, Historical Resources Guidelines. City Staff shall also require that the following steps be taken to determine: (1) the presence of archaeological resources and (2) the appropriate mitigation for any significant resources which may be impacted by a development activity. Sites may include residential and commercial properties, privies, trash pits, building foundations, and industrial features representing the contributions of people from diverse socio-economic and ethnic backgrounds. Sites may also include resources associated with pre-historic Native American activities. Archeological resources which also meet the definition of historical resources or unique archaeological resources under CEQA or the SDMC shall be treated in accordance with the following evaluation procedures and applicable mitigation program:</p> <p>Step 1-Initial Evaluation</p> <p>An initial evaluation for the potential of significant subsurface archaeological resources shall be prepared to the satisfaction of City Staff as part of an</p>	<p>Prior to Demolition or Grading Permit (Design)</p> <p>Prior to Certificate of Occupancy (Implementation)</p>	<p>Developer</p>	<p>CCDC</p>

SIGNIFICANT IMPACT(S)	MITIGATION MEASURE(S)	IMPLEMENTATION TIME FRAME	IMPLEMENTATION RESPONSIBILITY	VERIFICATION RESPONSIBILITY
	<p>Environmental Secondary Study for any activity which involves excavation or building demolition. The initial evaluation shall be guided by an appropriate level research design in accordance with the City’s Land Development Code, Historical Resources Guidelines. The person completing the initial review shall meet the qualification requirements as set forth in the Historical Resources Guidelines and shall be approved by City Staff. The initial evaluation shall consist , at a minimum, of a review of the following historical sources: The 1876 Bird’s Eye View of San Diego, all Sanborn Fire Insurance Company maps, appropriate City directories and maps that identify historical properties or archaeological sites, and a records search at the South Coastal Information Center for archaeological resources located within the property boundaries. Historical and existing land uses shall also be reviewed to assess the potential presence of significant prehistoric and historic archaeological resources. The person completing the initial review shall also consult with and consider input from local individuals and groups with expertise in the historical resources of the San Diego area. These experts may include the University of California, San Diego State University, San Diego Museum of Man, Save Our Heritage Organization (SOHO), local historical and archaeological groups, the Native American Heritage Commission (NAHC), designated community planning groups, and other individuals or groups that may have specific knowledge of the area. Consultation with these or other individuals and groups shall occur as early as possible in the evaluation process.</p> <p>When the initial evaluation indicates that important archaeological sites may be present on a project site but their presence cannot be confirmed prior to construction or demolition due to obstructions or spatially limited testing and data recovery, the applicant shall prepare and implement an archaeological monitoring program as a condition of development approval to the satisfaction of City Staff. If the NAHC Sacred Lands File search is positive for Native American resources within the project site, then additional evaluation must include participation of a local Native American consultant in accordance with CEQA Sections 15064.5(d), 15126.4(b)(3) and Public Resources Code Section 21083.2.</p> <p>No further action is required if the initial evaluation demonstrates there is no potential for subsurface resources. The results of this research shall be summarized in the Secondary Study.</p> <p>Step 2-Testing</p>			

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	<p>A testing program is required if the initial evaluation demonstrates that there is a potential for subsurface resources. The testing program shall be conducted during the hazardous materials remediation or following the removal of any structure or surface covering which may be underlain by potential resources. The removal of these structures shall be conducted in a manner which minimizes disturbance of underlying soil. This shall entail a separate phase of investigations from any mitigation monitoring during construction.</p> <p>The testing program shall be performed by a qualified Historical Archaeologist meeting the qualifications specified in Appendix B of the San Diego Land Development Code, Historical Resources Guidelines. The Historical Archaeologist must be approved by City Staff prior to commencement. Before commencing the testing, a treatment plan shall be submitted for City Staff approval that reviews the initial evaluation results and includes a research design. The research design shall be prepared in accordance with the City’s Historical Resources Guidelines and include a discussion of field methods, research questions against which discoveries shall be evaluated for significance, collection strategy, laboratory and analytical approaches, and curation arrangements. All tasks shall be in conformity with best practices in the field of historic urban archaeology. A recommended approach for historic urban sites is at a minimum fills and debris along interior lot lines or other areas indicated on Sanborn maps.</p> <p>Security measures such as a locked fence or surveillance shall be taken to prevent looting or vandalism of archaeological resources as soon as demolition is complete or paved surfaces are removed. These measures shall be maintained during archaeological field investigations. It is recommended that exposed features be covered with steel plates or fill dirt when not being investigated.</p> <p>The results of the testing phase shall be submitted in writing to City Staff and shall include the research design, testing results, significance evaluation, and recommendations for further treatment. Final determination of significance shall be made in consultation with City Staff , and with the Native American community, if the finds are prehistoric. If no significant resources are found and site conditions are such that there is no potential for further discoveries, then no further action is required. If no significant resources are found but results of the initial evaluation and testing phase indicates there is still a</p>			

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	<p>potential for resources to be present in portions of the property that could not be tested, then mitigation monitoring is required and shall be conducted in accordance with the provisions set forth in Step 4 - Monitoring. If significant resources are discovered during the testing program, then data recovery in accordance with Step 3 shall be undertaken prior to construction. If the existence or probable likelihood of Native American human remains or associated grave goods area discovered through the testing program, the Qualified Archaeologist shall stop work in the area, notify the City Building Inspector, City staff, and immediately implement the procedures set forth in CEQA Guidelines Section 15064.5 and the California Public Resources Code (PRC) Section 5097.98 for discovery of human remains. This procedure is further detailed in the Mitigation, Monitoring and Reporting Program (Step 4). City Staff must concur with evaluation results before the next steps can proceed.</p> <p>Step 3-Data Recovery</p> <p>For any site determined to be significant, a Research Design and Data Recovery Program (RDDR) shall be prepared in accordance with the City's Historical Resources Guidelines, approved by City Staff, and carried out to mitigate impacts before any activity is conducted which could potentially disturb significant resources. The archaeologist shall notify City Staff of the date upon which data recovery will commence ten (10) working days in advance.</p> <p>All cultural materials collected shall be cleaned, catalogued and permanently curated with an appropriate institution. Native American burial resources shall be treated in the manner agreed to by the Native American representative or be reinterred on the site in an area not subject to further disturbance in accordance with CEQA section 15164.5 and the Public Resources Code section 5097.98. All artifacts shall be analyzed to identify function and chronology as they relate to the history of the area. Faunal material shall be identified as to species and specialty studies shall be completed, as appropriate. All newly discovered archaeological sites shall be recorded with the South Coastal Information Center at San Diego State University. Any human bones and associated grave goods of Native American origin encountered during Step 2-Testing, shall, upon consultation, be turned over to the appropriate Native American representative(s) for treatment in accordance with state regulations as further outlined under Step 4-Monitoring (Section IV. Discovery of Human Remains).</p>			

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	<p>A draft Data Recovery Report shall be submitted to City Staff within twelve months of the commencement of the data recovery. Data Recovery Reports shall describe the research design or questions, historic context of the finds, field results, analysis of artifacts, and conclusions. Appropriate figures, maps and tables shall accompany the text. The report shall also include a catalogue of all finds and a description of curation arrangements at an approved facility, and a general statement indicating the disposition of any human remains encountered during the data recovery effort (please note that the location of reinternment and/or repatriation is confidential and not subject to public disclosure in accordance with state law). Finalization of draft reports shall be subject to City Staff review.</p> <p>Step 4 – Monitoring</p> <p>If no significant resources are encountered, but results of the initial evaluation and testing phase indicates there is still a potential for resources to be present in portions of the property that could not be tested, then mitigation monitoring is required and shall be conducted in accordance with the following provisions and components:</p> <p><b>I. Prior to Permit Issuance</b></p> <p>A. Construction Plan Check</p> <ol style="list-style-type: none"> <li>1. Prior to Notice to Proceed (NTP) for any construction permits, including but not limited to, the first Grading Permit, Demolition Permits and Building Permits, but prior to the first Precon Meeting, whichever is applicable, City Staff shall verify that the requirements for Archaeological Monitoring and Native American monitoring, where the project may impact Native American resources, have been noted on the appropriate construction documents.</li> </ol> <p>B. Letters of Qualification have been submitted to City Staff</p> <ol style="list-style-type: none"> <li>1. The applicant shall submit a letter of verification to City Staff identifying the Principal Investigator (PI) for the project and the names of all persons involved in the archaeological monitoring program, as defined in the City of San Diego Historical Resources Guidelines (HRG). If applicable, individuals involved in the archaeological monitoring program must have completed the 40-hour HAZWOPER training with certification documentation.</li> </ol>			

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	<p>2. City Staff will provide a letter to the applicant confirming that the qualifications of the PI and all persons involved in the archaeological monitoring of the project meet the qualifications established in the HRG.</p> <p>3. Prior to the start of work, the applicant must obtain written approval from City Staff for any personnel changes associated with the monitoring program.</p> <p><b>II. Prior to Start of Construction</b></p> <p>A. Verification of Records Search</p> <ol style="list-style-type: none"> <li>1. The PI shall provide verification to City Staff that a site-specific records search (1/4 mile radius) has been completed. Verification includes, but is not limited to a copy of a confirmation letter from South Coastal Information Center, or, if the search was in-house, a letter of verification from the PI stating that the search was completed.</li> <li>2. The letter shall introduce any pertinent information concerning expectations and probabilities of discovery during trenching and/or grading activities.</li> <li>3. The PI may submit a detailed letter to City Staff requesting a reduction to the ¼ mile radius.</li> </ol> <p>B. PI Shall Attend Precon Meetings</p> <ol style="list-style-type: none"> <li>1. Prior to beginning any work that requires monitoring, the Applicant shall arrange a Precon Meeting that shall include the PI, Native American consultant/monitor (where Native American resources may be impacted), Construction Manager (CM) and/or Grading Contractor, Resident Engineer (RE), the Native American representative(s) (where Native American resources may be impacted), Building Inspector (BI), if appropriate, and City Staff. The qualified Archaeologist and the Native American consultant/monitor shall attend any grading/excavation related Precon Meetings to make comments and/or suggestions concerning the Archaeological Monitoring program with the Construction Manager and/or Grading Contractor.                     <ol style="list-style-type: none"> <li>(a) If the PI is unable to attend the Precon Meeting, the Applicant shall schedule a focused Precon Meeting with City Staff, the PI, RE, CM or BI, if appropriate, prior to the start of any work that requires monitoring.</li> </ol> </li> <li>2. Archaeological Monitoring Plan (AMP)</li> </ol>			

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	<p>(a) Prior to the start of any work that requires monitoring, the PI shall submit an Archaeological Monitoring Plan (with verification that the AMP has been reviewed and approved by the Native American consultant/monitor when NA resources may be impacted) which describes how the monitoring would be accomplished for approval by City Staff and the Native American monitor. The AMP shall include an Archaeological Monitoring Exhibit (AME) based on the appropriate construction documents (reduced to 11x17) to City Staff identifying the areas to be monitored including the delineation of grading/excavation limits.</p> <p>(b) The AME shall be based on the results of a site-specific records search as well as information regarding existing known soil conditions (native or formation).</p> <p>(c) Prior to the start of any work, the PI shall also submit a construction schedule to City Staff through the RE indicating when and where monitoring will occur.</p> <p>(d) The PI may submit a detailed letter to City Staff prior to the start of work or during construction requesting a modification to the monitoring program. This request shall be based on relevant information such as review of final construction documents which indicate site conditions such as depth of excavation and/or site graded to bedrock, etc., which may reduce or increase the potential for resources to be present.</p> <p><b>III. During Construction</b></p> <p>A. Monitor(s) Shall be Present During Grading/Excavation/Trenching</p> <ol style="list-style-type: none"> <li>1. The Archaeological monitor shall be present full-time during all soil disturbing and grading/excavation /trenching activities which could result in impacts to archaeological resources as identified on the AME. The Construction Manager is responsible for notifying the RE, PI, and City Staff of changes to any construction activities.</li> <li>2. The Native American consultant/monitor shall determine the extent of their presence during soil disturbing and grading/excavation/trenching activities based on the AME, and provide that information to the PI and City Staff. If prehistoric resources are encountered during the Native</li> </ol>			

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	<p>American consultant/monitor's absence, work shall stop and the Discovery Notification Processes detailed in Sections III.B-C, and IVA-D, shall commence.</p> <ol style="list-style-type: none"> <li>3. The archeological and Native American consultant/monitor shall document field activity via the Consultant Site Visit Record (CSV). The CSV's shall be faxed by the CM to the RE the first day of monitoring, the last day of monitoring, monthly (Notification of Monitoring Completion), and in the case of ANY discoveries. The RE shall forward copies to City Staff.</li> <li>4. The PI may submit a detailed letter to City Staff during construction requesting a modification to the monitoring program when a field condition such as modern disturbance post-dating the previous grading/trenching activities, presence of fossil formations, or when native soils are encountered that may reduce or increase the potential for resources to be present.</li> </ol> <p>B. Discovery Notification Process</p> <ol style="list-style-type: none"> <li>1. In the event of a discovery, the Archaeological Monitor shall direct the contractor to temporarily divert all soil disturbing activities, including but not limited to, digging, trenching, excavating, or grading activities in the area of discovery and in the area reasonably suspected to overlay adjacent resources and immediately notify the RE or BI, as appropriate.</li> <li>2. The Monitor shall immediately notify the PI (unless Monitor is the PI) of the discovery.</li> <li>3. The PI shall immediately notify City Staff by phone of the discovery, and shall also submit written documentation to City Staff within 24 hours by fax or email with photos of the resource in context, if possible.</li> <li>4. No soil shall be exported off-site until a determination can be made regarding the significance of the resource specifically if Native American resources are encountered.</li> </ol> <p>C. Determination of Significance</p> <ol style="list-style-type: none"> <li>1. The PI and Native American consultant/monitor, where Native American resources are discovered, shall evaluate the significance of the resource. If Human Remains are involved,</li> </ol>			

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	<p>follow protocol in Section IV below.</p> <ul style="list-style-type: none"> <li>(a) The PI shall immediately notify City Staff by phone to discuss significance determination and shall also submit a letter to City Staff indicating whether additional mitigation is required.</li> <li>(b) If the resource is significant, the PI shall submit an Archaeological Data Recovery Program (ADRP) which has been reviewed by the Native American consultant/monitor when applicable, and obtain written approval from City Staff and the Native American representative(s), if applicable. Impacts to significant resources must be mitigated before ground disturbing activities in the area of discovery will be allowed to resume.</li> <li>(c) If the resource is not significant, the PI shall submit a letter to City Staff indicating that artifacts will be collected, curated, and documented in the Final Monitoring Report. The letter shall also indicate that that no further work is required.</li> </ul> <p><b>IV. Discovery of Human Remains</b></p> <p>If human remains are discovered, work shall halt in that area and no soil shall be exported off-site until a determination can be made regarding the provenance of the human remains; and the following procedures set forth in CEQA Section 15064.5(e), the California Public Resources Code (Sec. 5097.98) and State Health and Safety Code (Sec. 7050.5) shall be undertaken:</p> <ul style="list-style-type: none"> <li>A. Notification             <ul style="list-style-type: none"> <li>1. Archaeological Monitor shall notify the RE or BI as appropriate, City Staff, and the PI, if the Monitor is not qualified as a PI. City Staff will notify the appropriate Senior Planner in the Environmental Analysis Section (EAS) of the Development Services Department to assist with the discovery process.</li> <li>2. The PI shall notify the Medical Examiner after consultation with the RE, either in person or via telephone.</li> </ul> </li> <li>B. Isolate discovery site             <ul style="list-style-type: none"> <li>1. Work shall be directed away from the location of the discovery and any nearby area reasonably suspected to overlay adjacent human remains until a determination can be</li> </ul> </li> </ul>			

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	<p>made by the Medical Examiner in consultation with the PI concerning the provenance of the remains.</p> <ol style="list-style-type: none"> <li>2. The Medical Examiner, in consultation with the PI, will determine the need for a field examination to determine the provenance.</li> <li>3. If a field examination is not warranted, the Medical Examiner will determine with input from the PI, if the remains are or are most likely to be of Native American origin.</li> </ol> <p>C. If Human Remains are determined to be Native American</p> <ol style="list-style-type: none"> <li>1. The Medical Examiner will notify the Native American Heritage Commission (NAHC) within 24 hours. By law, <b>ONLY</b> the Medical Examiner can make this call.</li> <li>2. NAHC will immediately identify the person or persons determined to be the Most Likely Descendent (MLD) and provide contact information..</li> <li>3. The MLD will contact the PI within 24 hours or sooner after the Medical Examiner has completed coordination, to begin the consultation process in accordance with CEQA Section 15064.5(e) and the California Public Resources and Health &amp; Safety Codes.</li> <li>4. The MLD will have 48 hours to make recommendations to the property owner or representative, for the treatment or disposition with proper dignity, of the human remains and associated grave goods.</li> <li>5. Disposition of Native American Human Remains will be determined between the MLD and the PI, and if:             <ol style="list-style-type: none"> <li>(a) The NAHC is unable to identify the MLD, OR the MLD failed to make a recommendation within 48 hours after being notified by the Commission; OR;</li> <li>(b) The landowner or authorized representative rejects the recommendation of the MLD and mediation in accordance with PRC 5097.94 (k) by the NAHC fails to provide measures acceptable to the landowner, THEN,</li> <li>(c) In order to protect these sites, the Landowner shall do one or more of the following:                 <ol style="list-style-type: none"> <li>(1) Record the site with the NAHC;</li> <li>(2) Record an open space or conservation easement on the site;</li> <li>(3) Record a document with the County.</li> </ol> </li> </ol> </li> </ol>			

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	<p>6. Upon the discovery of multiple Native American human remains during a ground disturbing land development activity, the landowner may agree that additional conferral with descendants is necessary to consider culturally appropriate treatment of multiple Native American human remains. Culturally appropriate treatment of such a discovery may be ascertained from review of the site utilizing cultural and archaeological standards. Where the parties are unable to agree on the appropriate treatment measures the human remains and buried with Native American human remains shall be reinterred with appropriate dignity, pursuant to Section 5.c., above.</p> <p>D. If Human Remains are not Native American</p> <ol style="list-style-type: none"> <li>1. The PI shall contact the Medical Examiner and notify them of the historic era context of the burial.</li> <li>2. The Medical Examiner will determine the appropriate course of action with the PI and City staff (PRC 5097.98).</li> <li>3. If the remains are of historic origin, they shall be appropriately removed and conveyed to the San Diego Museum of Man for analysis. The decision for internment of the human remains shall be made in consultation with City Staff, the applicant/landowner and the San Diego Museum of Man.</li> </ol> <p><b>V. Night and/or Weekend Work</b></p> <p>A. If night and/or work is included in the contract</p> <ol style="list-style-type: none"> <li>1. When night and/or weekend work is included in the contract package, the extent and timing shall be presented and discussed at the Precon Meeting.</li> <li>2. The following procedures shall be followed.                     <ol style="list-style-type: none"> <li>(a) No Discoveries In the event that no discoveries were encountered during night and/or weekend work, the PI shall record the information on the CSV and submit to City Staff via fax by 8 am of the next business day.</li> <li>(b) Discoveries All discoveries shall be processed and documented using the existing procedures detailed in Sections III - During Construction, and IV – Discovery of Human Remains. Discovery of human remains shall always be treated as a</li> </ol> </li> </ol>			

SIGNIFICANT IMPACT(S)	MITIGATION MEASURE(S)	IMPLEMENTATION TIME FRAME	IMPLEMENTATION RESPONSIBILITY	VERIFICATION RESPONSIBILITY
	<p>significant discovery.</p> <p>(c) Potentially Significant Discoveries If the PI determines that a potentially significant discovery has been made, the procedures detailed under Section III - During Construction and IV-Discovery of Human Remains shall be followed.</p> <p>(d) The PI shall immediately contact City Staff, or by 8 am of the next business day to report and discuss the findings as indicated in Section III-B, unless other specific arrangements have been made.</p> <p>B. If night and/or weekend work becomes necessary during the course of construction</p> <ol style="list-style-type: none"> <li>1. The Construction Manager shall notify the RE, or BI, as appropriate, a minimum of 24 hours before the work is to begin.</li> <li>2. The RE, or BI, as appropriate, shall notify City Staff immediately.</li> </ol> <p>C. All other procedures described above shall apply, as appropriate.</p> <p><b>VI. Post Construction</b></p> <p>A. Submittal of Draft Monitoring Report</p> <ol style="list-style-type: none"> <li>1. The PI shall submit two copies of the Draft Monitoring Report (even if negative) prepared in accordance with the Historical Resources Guidelines and Appendices which describes the results, analysis, and conclusions of all phases of the Archaeological Monitoring Program (with appropriate graphics) to City Staff, for review and approval within 90 days following the completion of monitoring.                     <ol style="list-style-type: none"> <li>(a) For significant archaeological resources encountered during monitoring, the Archaeological Data Recovery Program shall be included in the Draft Monitoring Report.</li> <li>(b) Recording sites with State of California Department of Parks and Recreation The PI shall be responsible for recording (on the appropriate State of California Department of Park and Recreation forms-DPR 523 A/B) any significant or potentially significant resources encountered during the Archaeological Monitoring Program in accordance with the City's Historical Resources Guidelines, and submittal</li> </ol> </li> </ol>			

SIGNIFICANT IMPACT(S)	MITIGATION MEASURE(S)	IMPLEMENTATION TIME FRAME	IMPLEMENTATION RESPONSIBILITY	VERIFICATION RESPONSIBILITY
	<p>of such forms to the South Coastal Information Center with the Final Monitoring Report.</p> <ol style="list-style-type: none"> <li>2. City Staff shall return the Draft Monitoring Report to the PI for revision or, for preparation of the Final Report.</li> <li>3. The PI shall submit revised Draft Monitoring Report to City Staff for approval.</li> <li>4. City Staff shall provide written verification to the PI of the approved report.</li> <li>5. City Staff shall notify the RE or BI, as appropriate, of receipt of all Draft Monitoring Report submittals and approvals.</li> </ol> <p>B. Handling of Artifacts and Submittal of Collections Management Plan, if applicable</p> <ol style="list-style-type: none"> <li>1. The PI shall be responsible for ensuring that all cultural remains collected are cleaned and catalogued.</li> <li>2. The PI shall be responsible for ensuring that all artifacts are analyzed to identify function and chronology as they relate to the history of the area; that faunal material is identified as to species; and that specialty studies are completed, as appropriate.</li> <li>3. The PI shall submit a Collections Management Plan to City Staff for review and approval for any project which results in a substantial collection of historical artifacts.</li> </ol> <p>C. Curation of artifacts: Accession Agreement and Acceptance Verification</p> <ol style="list-style-type: none"> <li>1. The PI shall be responsible for ensuring that all artifacts associated with the survey, testing and/or data recovery for this project are permanently curated with an appropriate institution. This shall be completed in consultation with City Staff and the Native American representative, as applicable.</li> <li>2. The PI shall include the Acceptance Verification from the curation institution in the Final Monitoring Report submitted to the RE or BI and City Staff.</li> <li>3. When applicable to the situation, the PI shall include written verification from the Native American consultant/monitor indicating that Native American resources were treated in accordance with state law and/or applicable agreements. If the resources were reinterred, verification shall be provided to show what protective measures were taken to ensure no</li> </ol>			

SIGNIFICANT IMPACT(S)	MITIGATION MEASURE(S)	IMPLEMENTATION TIME FRAME	IMPLEMENTATION RESPONSIBILITY	VERIFICATION RESPONSIBILITY
	<p>further disturbance in accordance with section IV – Discovery of Human Remains, subsection 5.(d).</p> <p>D. Final Monitoring Report(s)</p> <ol style="list-style-type: none"> <li>1. The PI shall submit one copy of the approved Final Monitoring Report to the RE or BI as appropriate, and one copy to City Staff (even if negative), within 90 days after notification from City Staff that the draft report has been approved.</li> <li>2. The RE shall, in no case, issue the Notice of Completion until receiving a copy of the approved Final Monitoring Report from- City Staff which includes the Acceptance Verification from the curation institution.</li> </ol>			
<b>PALEONTOLOGICAL RESOURCES (PAL)</b>				
<p><b>Impact PAL-A.1:</b> Excavation in geologic formations with a moderate to high potential for paleontological resources could have an significant impact on these resources, if present. (Direct and Cumulative)</p>	<p><i>Mitigation Measure PAL-A.1-1:</i> In the event the Secondary Study indicates the potential for significant paleontological resources, the following measures shall be implemented as determined appropriate by CCDC.</p> <p><b>I. Prior to Permit Issuance</b></p> <p>A. Construction Plan Check</p> <ol style="list-style-type: none"> <li>1. Prior to Notice to Proceed (NTP) for any construction permits, including but not limited to, the first Grading Permit, Demolition Permits and Building Permits, but prior to the first preconstruction meeting, whichever is applicable, Centre City Development Corporation (CCDC) shall verify that the requirements for Paleontological Monitoring have been noted on the appropriate construction documents.</li> </ol> <p>B. Letters of Qualification have been submitted to CCDC</p> <ol style="list-style-type: none"> <li>1. The applicant shall submit a letter of verification to CCDC identifying the Principal Investigator (PI) for the project and the names of all persons involved in the paleontological monitoring program, as defined in the City of San Diego Paleontology Guidelines.</li> <li>2. CCDC will provide a letter to the applicant confirming the qualifications of the PI and all persons involved in the paleontological monitoring of the project.</li> <li>3. Prior to the start of work, the applicant shall obtain approval from CCDC for any personnel changes associated with the monitoring</li> </ol>	<p>Prior to Demolition, Grading or Building Permit (Design)</p> <p>Prior to Certificate of Occupancy (Implementation)</p>	<p>Developer</p>	<p>CCDC/City</p>

SIGNIFICANT IMPACT(S)	MITIGATION MEASURE(S)	IMPLEMENTATION TIME FRAME	IMPLEMENTATION RESPONSIBILITY	VERIFICATION RESPONSIBILITY
	<p>program.</p> <p><b>II. Prior to Start of Construction</b></p> <p>A. Verification of Records Search</p> <ol style="list-style-type: none"> <li>1. The PI shall provide verification to CCDC that a site-specific records search has been completed. Verification includes, but is not limited to a copy of a confirmation letter from San Diego Natural History Museum, other institution or, if the search was in-house, a letter of verification from the PI stating that the search was completed.</li> <li>2. The letter shall introduce any pertinent information concerning expectations and probabilities of discovery during trenching and/or grading activities.</li> </ol> <p>B. PI Shall Attend Precon Meetings</p> <ol style="list-style-type: none"> <li>1. Prior to beginning any work that requires monitoring, the Applicant shall arrange a Precon Meeting that shall include the PI, Construction Manager (CM) and/or Grading Contractor, Resident Engineer (RE), Building Inspector (BI), if appropriate, and CCDC. The qualified paleontologist shall attend any grading/excavation related Precon Meetings to make comments and/or suggestions concerning the Paleontological Monitoring program with the Construction Manager and/or Grading Contractor.             <ol style="list-style-type: none"> <li>a. If the PI is unable to attend the Precon Meeting, the Applicant shall schedule a focused Precon Meeting with CCDC, the PI, RE, CM or BI, if appropriate, prior to the start of any work that requires monitoring.</li> </ol> </li> <li>2. Identify Areas to be Monitored             <ol style="list-style-type: none"> <li>a. Prior to the start of any work that requires monitoring, the PI shall submit a Paleontological Monitoring Exhibit (PME) based on the appropriate construction documents (reduced to 11x17) to CCDC identifying the areas to be monitored including the delineation of grading/excavation limits. The PME shall be based on the results of a site specific records search as well as information regarding existing known soil conditions (native or formation).</li> </ol> </li> <li>3. When Monitoring Will Occur             <ol style="list-style-type: none"> <li>a. Prior to the start of any work, the PI shall also submit a construction schedule to CCDC through the RE indicating</li> </ol> </li> </ol>			

SIGNIFICANT IMPACT(S)	MITIGATION MEASURE(S)	IMPLEMENTATION TIME FRAME	IMPLEMENTATION RESPONSIBILITY	VERIFICATION RESPONSIBILITY
	<p>when and where monitoring will occur.</p> <p>b. The PI may submit a detailed letter to CCDC prior to the start of work or during construction requesting a modification to the monitoring program. This request shall be based on relevant information such as review of final construction documents which indicate conditions such as depth of excavation and/or site graded to bedrock, presence or absence of fossil resources, etc., which may reduce or increase the potential for resources to be present.</p> <p><b>III. During Construction</b></p> <p>A. Monitor Shall be Present During Grading/Excavation/Trenching</p> <ol style="list-style-type: none"> <li>1. The monitor shall be present full-time during grading/excavation/trenching activities as identified on the PME that could result in impacts to formations with high and moderate resource sensitivity. The Construction Manager is responsible for notifying the RE, PI, and CCDC of changes to any construction activities.</li> <li>2. The monitor shall document field activity via the Consultant Site Visit Record (CSV). The CSV's shall be faxed by the CM to the RE the first day of monitoring, the last day of monitoring, monthly (Notification of Monitoring Completion), and in the case of any discoveries. The RE shall forward copies to CCDC.</li> <li>3. The PI may submit a detailed letter to CCDC during construction requesting a modification to the monitoring program when a field condition such as trenching activities that do not encounter formational soils as previously assumed, and/or when unique/unusual fossils are encountered, which may reduce or increase the potential for resources to be present.</li> </ol> <p>B. Discovery Notification Process</p> <ol style="list-style-type: none"> <li>1. In the event of a discovery, the Paleontological Monitor shall direct the contractor to temporarily divert trenching activities in the area of discovery and immediately notify the RE or BI, as appropriate.</li> <li>2. The Monitor shall immediately notify the PI (unless Monitor is the PI) of the discovery.</li> <li>3. The PI shall immediately notify CCDC by phone of the discovery, and shall also submit written documentation to CCDC within 24 hours by fax or email with photos of the resource in context, if possible.</li> </ol>			

SIGNIFICANT IMPACT(S)	MITIGATION MEASURE(S)	IMPLEMENTATION TIME FRAME	IMPLEMENTATION RESPONSIBILITY	VERIFICATION RESPONSIBILITY
	<p>C. Determination of Significance</p> <ol style="list-style-type: none"> <li>1. The PI shall evaluate the significance of the resource.                             <ol style="list-style-type: none"> <li>a. The PI shall immediately notify CCDC by phone to discuss significance determination and shall also submit a letter to CCDC indicating whether additional mitigation is required. The determination of significance for fossil discoveries shall be at the discretion of the PI.</li> <li>b. If the resource is significant, the PI shall submit a Paleontological Recovery Program (PRP) and obtain written approval from CCDC. Impacts to significant resources must be mitigated before ground disturbing activities in the area of discovery will be allowed to resume.</li> <li>c. If resource is not significant (e.g., small pieces of broken common shell fragments or other scattered common fossils) the PI shall notify the RE, or BI as appropriate, that a non-significant discovery has been made. The Paleontologist shall continue to monitor the area without notification to CCDC unless a significant resource is encountered.</li> <li>d. The PI shall submit a letter to CCDC indicating that fossil resources will be collected, curated, and documented in the Final Monitoring Report. The letter shall also indicate that no further work is required.</li> </ol> </li> </ol> <p><b>IV. Night Work</b></p> <ol style="list-style-type: none"> <li>A. If night work is included in the contract                             <ol style="list-style-type: none"> <li>1. When night work is included in the contract package, the extent and timing shall be presented and discussed at the pre-con meeting.</li> <li>2. The following procedures shall be followed.                                     <ol style="list-style-type: none"> <li>a. No Discoveries   <ol style="list-style-type: none"> <li>(1) In the event that no discoveries were encountered during night work, The PI shall record the information on the CSVR and submit to CCDC via fax by 9am the following morning, if possible.</li> </ol> </li> <li>b. Discoveries   <ol style="list-style-type: none"> <li>(1) All discoveries shall be processed and documented using the existing procedures detailed in Sections III - During Construction.</li> </ol> </li> <li>c. Potentially Significant Discoveries   <ol style="list-style-type: none"> <li>(1) If the PI determines that a potentially significant</li> </ol> </li> </ol> </li> </ol> </li> </ol>			

SIGNIFICANT IMPACT(S)	MITIGATION MEASURE(S)	IMPLEMENTATION TIME FRAME	IMPLEMENTATION RESPONSIBILITY	VERIFICATION RESPONSIBILITY
	<p>discovery has been made, the procedures detailed under Section III - During Construction shall be followed.</p> <ul style="list-style-type: none"> <li>d. The PI shall immediately contact CCDC, or by 8AM the following morning to report and discuss the findings as indicated in Section III-B, unless other specific arrangements have been made.</li> </ul> <p>B. If night work becomes necessary during the course of construction</p> <ul style="list-style-type: none"> <li>1. The Construction Manager shall notify the RE, or BI, as appropriate, a minimum of 24 hours before the work is to begin.</li> <li>2. The RE, or BI, as appropriate, shall notify CCDC immediately.</li> </ul> <p>C. All other procedures described above shall apply, as appropriate.</p> <p><b>VI. Post Construction</b></p> <p>A. Submittal of Draft Monitoring Report</p> <ul style="list-style-type: none"> <li>1. The PI shall submit two copies of the Draft Monitoring Report (even if negative) which describes the results, analysis, and conclusions of all phases of the Paleontological Monitoring Program (with appropriate graphics) to CCDC for review and approval within 90 days following the completion of monitoring,             <ul style="list-style-type: none"> <li>a. For significant paleontological resources encountered during monitoring, the Paleontological Recovery Program shall be included in the Draft Monitoring Report.</li> <li>b. Recording Sites with the San Diego Natural History Museum                 <ul style="list-style-type: none"> <li>(1) The PI shall be responsible for recording (on the appropriate forms) any significant or potentially significant fossil resources encountered during the Paleontological Monitoring Program in accordance with the City's Paleontological Guidelines, and submittal of such forms to the San Diego Natural History Museum with the Final Monitoring Report.</li> </ul> </li> </ul> </li> <li>2. CCDC shall return the Draft Monitoring Report to the PI for revision or, for preparation of the Final Report.</li> <li>3. The PI shall submit revised Draft Monitoring Report to CCDC for approval.</li> <li>4. CCDC shall provide written verification to the PI of the approved report.</li> <li>5. CCDC shall notify the RE or BI, as appropriate, of receipt of all Draft Monitoring Report submittals and approvals.</li> </ul>			

SIGNIFICANT IMPACT(S)	MITIGATION MEASURE(S)	IMPLEMENTATION TIME FRAME	IMPLEMENTATION RESPONSIBILITY	VERIFICATION RESPONSIBILITY
	<p>B. Handling of Fossil Remains</p> <ol style="list-style-type: none"> <li>1. The PI shall be responsible for ensuring that all fossil remains collected are cleaned and catalogued.</li> <li>2. The PI shall be responsible for ensuring that all fossil remains are analyzed to identify function and chronology as they relate to the geologic history of the area; that faunal material is identified as to species; and that specialty studies are completed, as appropriate</li> </ol> <p>C. Curation of fossil remains: Deed of Gift and Acceptance Verification</p> <ol style="list-style-type: none"> <li>1. The PI shall be responsible for ensuring that all fossil remains associated with the monitoring for this project are permanently curated with an appropriate institution.</li> <li>2. The PI shall include the Acceptance Verification from the curation institution in the Final Monitoring Report submitted to the RE or BI and CCDC.</li> </ol> <p>D. Final Monitoring Report(s)</p> <ol style="list-style-type: none"> <li>1. The PI shall submit two copies of the Final Monitoring Report to CCDC (even if negative), within 90 days after notification from CCDC that the draft report has been approved.</li> <li>2. The RE shall, in no case, issue the Notice of Completion until receiving a copy of the approved Final Monitoring Report from CCDC which includes the Acceptance Verification from the curation institution.</li> </ol>			



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<b>AIR QUALITY (AQ)</b>				
<p><b>Impact AQ-B.1:</b>  <b>Dust and construction equipment engine emissions generated during grading and demolition would impact local and regional air quality. (Direct)</b></p>	<p><i>Mitigation Measure AQ-B.1-1:</i> Prior to approval of a Grading or Demolition Permit, the City shall confirm that the following conditions have been applied, as appropriate:</p> <ol style="list-style-type: none"> <li>1. Exposed soil areas shall be watered twice per day. On windy days or when fugitive dust can be observed leaving the development site, additional applications of water shall be applied as necessary to prevent visible dust plumes from leaving the development site. When wind velocities are forecast to exceed 25 miles per hour, all ground disturbing activities shall be halted until winds that are forecast to abate below this threshold.</li> <li>2. Dust suppression techniques shall be implemented including, but not limited to, the following:               <ol style="list-style-type: none"> <li>a. Portions of the construction site to remain inactive longer than a period of three months shall be seeded and watered until grass cover is grown or otherwise stabilized in a manner acceptable to the CCDC.</li> <li>b. On-site access points shall be paved as soon as feasible or watered periodically or otherwise stabilized.</li> <li>c. Material transported offsite shall be either sufficiently watered or securely covered to prevent excessive amounts of dust.</li> <li>d. The area disturbed by clearing, grading, earthmoving, or excavation operations shall be minimized at all times.</li> </ol> </li> <li>3. Vehicles on the construction site shall travel at speeds less than 15 miles per hour.</li> <li>4. Material stockpiles subject to wind erosion during construction activities, which will not be utilized within three days, shall be covered with plastic, an alternative cover deemed equivalent to plastic, or sprayed with a nontoxic chemical stabilizer.</li> <li>5. Where vehicles leave the construction site and enter adjacent public streets, the streets shall be swept daily or washed down at the end of the work day to remove soil tracked onto the paved surface. Any visible</li> </ol>	<p>Prior to Demolition or Grading Permit (Design)</p>	<p>Developer</p>	<p>City</p>

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	<p>track-out extending for more than fifty (50) feet from the access point shall be swept or washed within thirty (30) minutes of deposition.</p> <ol style="list-style-type: none"> <li>6. All diesel-powered vehicles and equipment shall be properly operated and maintained.</li> <li>7. All diesel-powered vehicles and gasoline-powered equipment shall be turned off when not in use for more than five minutes, as required by state law.</li> <li>8. The construction contractor shall utilize electric or natural gas-powered equipment in lieu of gasoline or diesel-powered engines, where feasible.</li> <li>9. As much as possible, the construction contractor shall time the construction activities so as not to interfere with peak hour traffic. In order to minimize obstruction of through traffic lanes adjacent to the site, a flag-person shall be retained to maintain safety adjacent to existing roadways, if necessary.</li> <li>10. The construction contractor shall support and encourage ridesharing and transit incentives for the construction crew.</li> <li>11. Low VOC coatings shall be used as required by SDAPCD Rule 67. Spray equipment with high transfer efficiency, such as the high volume-low pressure (HPLV) spray method, or manual coatings application such as paint brush hand roller, trowel, spatula, dauber, rag, or sponge, shall be used to reduce VOC emissions, where feasible.</li> <li>12. If construction equipment powered by alternative fuel sources (LPG/CNG) is available at comparable cost, the developer shall specify that such equipment be used during all construction activities on the development site.</li> <li>13. The developer shall require the use of particulate filters on diesel construction equipment if use of such filters is demonstrated to be cost-competitive for use on this development.</li> <li>14. During demolition activities, safety measures as required by City/County/State for removal of toxic or hazardous materials shall be utilized.</li> <li>15. Rubble piles shall be maintained in a damp state to minimize dust generation.</li> <li>16. During finish work, low-VOC paints and efficient transfer systems shall be utilized, to the extent possible.</li> </ol>			

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	17. If alternative-fueled and/or particulate filter-equipped construction equipment is not feasible, construction equipment shall use the newest, least-polluting equipment, whenever possible.			
<b>HISTORICAL RESOURCES (HIST)</b>				
<p><b>Impact HIST-A.1:</b>  <b>Future development in downtown could impact architectural resources. (Direct and Cumulative)</b></p>	<p><i>Mitigation Measure HIST-A.1-1:</i> For historic resources which are 45 years of age or older and which have not been evaluated for local, state and federal historic significance, CCDC shall consult with HRB to determine whether the resources is significant pursuant to CEQA.</p> <p>For resources that have been formally determined to be significant under federal, state or local criteria, the following actions shall be carried out under direction of CCDC in consultation with HRB, as appropriate.</p> <ul style="list-style-type: none"> <li>• <b>National Register-Listed/Eligible, California Register-Listed/Eligible Resources:</b> Resources listed on or formally determined eligible for the National Register or California Register and structures identified as contributing structures within a National or California Register District, shall be retained onsite and any improvements, renovation, rehabilitation and/or adaptive reuse of the property shall ensure its preservation according to the Secretary of the Interior’s Standards for Rehabilitation of Historic Buildings and Guidelines for Rehabilitation of Historic Buildings.</li> <li>• <b>San Diego Register-Listed Resources:</b> Any development that proposes to remove or significantly alter one of these historical resources shall comply with Chapter 14, Article 3, Division 2 of the San Diego Municipal Code which regulates Historical Resources.</li> </ul>	<p>Prior to Development Permit (Design)</p> <p>Prior to Demolition, Grading, and/or Building Permit (Design)</p> <p>Prior to Certificate of Occupancy (Implementation)</p>	<p>Developer</p>	<p>CCDC/City</p>
<p><b>Impact HIST-B.1:</b>  <b>Development in downtown could impact significant buried archaeological resources. (Direct and Cumulative)</b></p>	<p><i>Mitigation Measure HIST-B.1-1:</i> If the potential exists for archaeological resources, the following measures shall be implemented.</p> <p><b>I. Prior to Permit Issuance</b></p> <p>A. Construction Plan Check</p> <p>1. Prior to Notice to Proceed (NTP) for any construction permits, including but not limited to, the first Grading Permit, Demolition Permits and Building Permits, but prior to the first preconstruction meeting, whichever is applicable, the Centre City Development Corporation (CCDC) shall verify that the requirements for Archaeological Monitoring and Native</p>	<p>Prior to Demolition or Grading Permit (Design)</p> <p>Prior to Certificate of Occupancy (Implementation)</p>	<p>Developer</p>	<p>CCDC</p>

SIGNIFICANT IMPACT(S)	MITIGATION MEASURE(S)	IMPLEMENTATION TIME FRAME	IMPLEMENTATION RESPONSIBILITY	VERIFICATION RESPONSIBILITY
	<p>American monitoring, if applicable, have been noted on the appropriate construction documents.</p> <p>B. Letters of Qualification have been submitted to CCDC</p> <ol style="list-style-type: none"> <li>1. The applicant shall submit a letter of verification to CCDC identifying the Principal Investigator (PI) for the project and the names of all persons involved in the archaeological monitoring program, as defined in the City of San Diego Historical Resources Guidelines (HRG). If applicable, individuals involved in the archaeological monitoring program must have completed the 40-hour HAZWOPER training with certification documentation.</li> <li>2. CCDC will provide a letter to the applicant confirming the qualifications of the PI and all persons involved in the archaeological monitoring of the project.</li> <li>3. Prior to the start of work, the applicant must obtain approval from CCDC for any personnel changes associated with the monitoring program.</li> </ol> <p><b>II. Prior to Start of Construction</b></p> <p>A. Verification of Records Search</p> <ol style="list-style-type: none"> <li>1. The PI shall provide verification to CCDC that a site-specific records search (1/4 mile radius) has been completed. Verification includes, but is not limited to a copy of a confirmation letter from South Coast Information Center, or, if the search was in-house, a letter of verification from the PI stating that the search was completed.</li> <li>2. The letter shall introduce any pertinent information concerning expectations and probabilities of discovery during trenching and/or grading activities.</li> <li>3. The PI may submit a detailed letter to CCDC requesting a reduction to the 1/4 mile radius.</li> </ol> <p>B. PI Shall Attend Precon Meetings</p> <ol style="list-style-type: none"> <li>1. Prior to beginning any work that requires monitoring, the Applicant shall arrange a Precon Meeting that shall include the PI, Construction Manager (CM) and/or Grading Contractor, Resident Engineer (RE), Building Inspector (BI), if appropriate, and CCDC. The qualified Archaeologist shall attend any grading/excavation related Precon Meetings to make comments and/or suggestions concerning the Archaeological Monitoring program with the Construction</li> </ol>			

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	<p>Manager and/or Grading Contractor.</p> <p>(a) If the PI is unable to attend the Precon Meeting, the Applicant shall schedule a focused Precon Meeting with CCDC, the PI, RE, CM or BI, if appropriate, prior to the start of any work that requires monitoring.</p> <p>2. Archaeological Monitoring Plan (AMP)</p> <p>(a) Prior to the start of any work that requires monitoring, the PI shall submit an Archaeological Monitoring Plan which describes how the monitoring would be accomplished for approval by CCDC. The AMP shall include an Archaeological Monitoring Exhibit (AME) based on the appropriate construction documents (reduced to 11x17) to CCDC identifying the areas to be monitored including the delineation of grading/excavation limits.</p> <p>(b) The AME shall be based on the results of a site-specific records search as well as information regarding existing known soil conditions (native or formation).</p> <p>(c) Prior to the start of any work, the PI shall also submit a construction schedule to CCDC through the RE indicating when and where monitoring will occur.</p> <p>(d) The PI may submit a detailed letter to CCDC prior to the start of work or during construction requesting a modification to the monitoring program. This request shall be based on relevant information such as review of final construction documents which indicate site conditions such as depth of excavation and/or site graded to bedrock, etc., which may reduce or increase the potential for resources to be present.</p> <p><b>III. During Construction</b></p> <p>A. Monitor Shall be Present During Grading/Excavation/Trenching</p> <p>1. The monitor shall be present full-time during soil remediation and grading/excavation/trenching activities which could result in impacts to archaeological resources as identified on the AME. The Construction Manager is responsible for notifying the RE, PI, and CCDC of changes to any construction activities.</p> <p>2. The monitor shall document field activity via the Consultant Site Visit Record (CSVR). The CSVR's shall be faxed by the</p>			

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	<p>CM to the RE the first day of monitoring, the last day of monitoring, monthly (Notification of Monitoring Completion), and in the case of ANY discoveries. The RE shall forward copies to CCDC.</p> <ol style="list-style-type: none"> <li>3. The PI may submit a detailed letter to CCDC during construction requesting a modification to the monitoring program when a field condition such as modern disturbance post-dating the previous grading/trenching activities, presence of fossil formations, or when native soils are encountered may reduce or increase the potential for resources to be present.</li> </ol> <p>B. Discovery Notification Process</p> <ol style="list-style-type: none"> <li>1. In the event of a discovery, the Archaeological Monitor shall direct the contractor to temporarily divert trenching activities in the area of discovery and immediately notify the RE or BI, as appropriate.</li> <li>2. The Monitor shall immediately notify the PI (unless Monitor is the PI) of the discovery.</li> <li>3. The PI shall immediately notify CCDC by phone of the discovery, and shall also submit written documentation to CCDC within 24 hours by fax or email with photos of the resource in context, if possible.</li> </ol> <p>C. Determination of Significance</p> <ol style="list-style-type: none"> <li>1. The PI and Native American representative, if applicable, shall evaluate the significance of the resource. If Human Remains are involved, follow protocol in Section IV below.             <ol style="list-style-type: none"> <li>(a) The PI shall immediately notify CCDC by phone to discuss significance determination and shall also submit a letter to CCDC indicating whether additional mitigation is required.</li> <li>(b) If the resource is significant, the PI shall submit an Archaeological Data Recovery Program (ADRP) and obtain written approval from CCDC. Impacts to significant resources must be mitigated before ground disturbing activities in the area of discovery will be allowed to resume.</li> <li>(c) If resource is not significant, the PI shall submit a letter to CCDC indicating that artifacts will be collected, curated, and documented in the Final Monitoring Report. The letter shall also indicate that that no further work is</li> </ol> </li> </ol>			

SIGNIFICANT IMPACT(S)	MITIGATION MEASURE(S)	IMPLEMENTATION TIME FRAME	IMPLEMENTATION RESPONSIBILITY	VERIFICATION RESPONSIBILITY
	<p style="text-align: center;">required.</p> <p><b>IV. Discovery of Human Remains</b>                      If human remains are discovered, work shall halt in that area and the following procedures set forth in the California Public Resources Code (Sec. 5097.98) and State Health and Safety Code (Sec. 7050.5) shall be undertaken:</p> <p>A. Notification</p> <ol style="list-style-type: none"> <li>1. Archaeological Monitor shall notify the RE or BI as appropriate, CCDC, and the PI, if the Monitor is not qualified as a PI.</li> <li>2. The PI shall notify the Medical Examiner after consultation with the RE, either in person or via telephone.</li> </ol> <p>B. Isolate discovery site</p> <ol style="list-style-type: none"> <li>1. Work shall be directed away from the location of the discovery and any nearby area reasonably suspected to overlay adjacent human remains until a determination can be made by the Medical Examiner in consultation with the PI concerning the provenience of the remains.</li> <li>2. The Medical Examiner, in consultation with the PI, shall determine the need for a field examination to determine the provenience.</li> <li>3. If a field examination is not warranted, the Medical Examiner shall determine with input from the PI, if the remains are or are most likely to be of Native American origin.</li> </ol> <p>C. If Human Remains are determined to be Native American</p> <ol style="list-style-type: none"> <li>1. The Medical Examiner shall notify the Native American Heritage Commission (NAHC). By law, only the Medical Examiner can make this call.</li> <li>2. The NAHC shall contact the PI within 24 hours or sooner, after Medical Examiner has completed coordination.</li> <li>3. NAHC shall identify the person or persons determined to be the Most Likely Descendent (MLD) and provide contact information..</li> <li>4. The PI shall coordinate with the MLD for additional consultation.</li> <li>5. Disposition of Native American Human Remains shall be determined between the MLD and the PI, if:                             <ol style="list-style-type: none"> <li>(a) The NAHC is unable to identify the MLD, OR the MLD failed to make a recommendation within 24 hours after</li> </ol> </li> </ol>			

SIGNIFICANT IMPACT(S)	MITIGATION MEASURE(S)	IMPLEMENTATION TIME FRAME	IMPLEMENTATION RESPONSIBILITY	VERIFICATION RESPONSIBILITY
	<p>being notified by the Commission; OR;</p> <p>(b) The landowner or authorized representative rejects the recommendation of the MLD and mediation in accordance with PRC 5097.94 (k) by the NAHC fails to provide measures acceptable to the landowner.</p> <p>D. If Human Remains are not Native American</p> <ol style="list-style-type: none"> <li>1. The PI shall contact the Medical Examiner and notify them of the historic era context of the burial.</li> <li>2. The Medical Examiner will determine the appropriate course of action with the PI and City staff (PRC 5097.98).</li> <li>3. If the remains are of historic origin, they shall be appropriately removed and conveyed to the Museum of Man for analysis. The decision for internment of the human remains shall be made in consultation with CCDC, the applicant/landowner and the Museum of Man.</li> </ol> <p>V. Night Work</p> <p>A. If night work is included in the contract</p> <ol style="list-style-type: none"> <li>1. When night work is included in the contract package, the extent and timing shall be presented and discussed at the precon meeting.</li> <li>2. The following procedures shall be followed.                     <ol style="list-style-type: none"> <li>(a) No Discoveries In the event that no discoveries were encountered during night work, the PI shall record the information on the CSVR and submit to CCDC via fax by 9am the following morning, if possible.</li> <li>(b) Discoveries All discoveries shall be processed and documented using the existing procedures detailed in Sections III - During Construction, and IV – Discovery of Human Remains.</li> <li>(c) Potentially Significant Discoveries If the PI determines that a potentially significant discovery has been made, the procedures detailed under Section III - During Construction shall be followed. The PI shall immediately contact CCDC, or by 8AM the following morning to report and discuss the findings as indicated in Section III-B, unless other specific arrangements have been made.</li> </ol> </li> </ol> <p>B. If night work becomes necessary during the course of construction</p>			

SIGNIFICANT IMPACT(S)	MITIGATION MEASURE(S)	IMPLEMENTATION TIME FRAME	IMPLEMENTATION RESPONSIBILITY	VERIFICATION RESPONSIBILITY
	<ol style="list-style-type: none"> <li>1. The Construction Manager shall notify the RE, or BI, as appropriate, a minimum of 24 hours before the work is to begin.</li> <li>2. The RE, or BI, as appropriate, shall notify CCDC immediately.</li> </ol> <p>C. All other procedures described above shall apply, as appropriate.</p> <p><b>VI. Post Construction</b></p> <p>A. Submittal of Draft Monitoring Report</p> <ol style="list-style-type: none"> <li>1. The PI shall submit two copies of the Draft Monitoring Report (even if negative) which describes the results, analysis, and conclusions of all phases of the Archaeological Monitoring Program (with appropriate graphics) to CCDC for review and approval within 90 days following the completion of monitoring.                             <ol style="list-style-type: none"> <li>(a) For significant archaeological resources encountered during monitoring, the Archaeological Data Recovery Program shall be included in the Draft Monitoring Report.</li> <li>(b) Recording sites with State of California Department of Parks and Recreation The PI shall be responsible for recording (on the appropriate State of California Department of Park and Recreation forms-DPR 523 A/B) any significant or potentially significant resources encountered during the Archaeological Monitoring Program in accordance with the City’s Historical Resources Guidelines, and submittal of such forms to the South Coastal Information Center with the Final Monitoring Report.</li> </ol> </li> <li>2. CCDC shall return the Draft Monitoring Report to the PI for revision or, for preparation of the Final Report.</li> <li>3. The PI shall submit revised Draft Monitoring Report to CCDC for approval.</li> <li>4. CCDC shall provide written verification to the PI of the approved report.</li> <li>5. CCDC shall notify the RE or BI, as appropriate, of receipt of all Draft Monitoring Report submittals and approvals.</li> </ol> <p>B. Handling of Artifacts and Submittal of Collections Management Plan, if applicable</p>			

SIGNIFICANT IMPACT(S)	MITIGATION MEASURE(S)	IMPLEMENTATION TIME FRAME	IMPLEMENTATION RESPONSIBILITY	VERIFICATION RESPONSIBILITY
	<ol style="list-style-type: none"> <li>1. The PI shall be responsible for ensuring that all cultural remains collected are cleaned and catalogued.</li> <li>2. The PI shall be responsible for ensuring that all artifacts are analyzed to identify function and chronology as they relate to the history of the area; that faunal material is identified as to species; and that specialty studies are completed, as appropriate.</li> <li>3. The PI shall submit a Collections Management Plan to CCDC for review and approval for any project which results in a substantial collection of historical artifacts.</li> </ol> <p>C. Curation of artifacts: Accession Agreement and Acceptance Verification</p> <ol style="list-style-type: none"> <li>1. The PI shall be responsible for ensuring that all artifacts associated with the survey, testing and/or data recovery for this project are permanently curated with an appropriate institution. This shall be completed in consultation with CCDC and the Native American representative, as applicable.</li> <li>2. The PI shall include the Acceptance Verification from the curation institution in the Final Monitoring Report submitted to the RE or BI and CCDC.</li> </ol> <p>D. Final Monitoring Report(s)</p> <ol style="list-style-type: none"> <li>1. The PI shall submit one copy of the approved Final Monitoring Report to the RE or BI as appropriate, and one copy to CCDC (even if negative), within 90 days after notification from CCDC that the draft report has been approved.</li> <li>2. The RE shall, in no case, issue the Notice of Completion until receiving a copy of the approved Final Monitoring Report from CCDC which includes the Acceptance Verification from the curation institution.</li> </ol>			
<b>PALEONTOLOGICAL RESOURCES (PAL)</b>				
<p><b>Impact PAL-A.1:</b> Excavation in geologic formations with a moderate to high potential for paleontological resources could have an significant impact on these resources, if present. (Direct and Cumulative)</p>	<p><i>Mitigation Measure PAL-A.1-1:</i> In the event the Secondary Study indicates the potential for significant paleontological resources, the following measures shall be implemented as determined appropriate by CCDC.</p> <p><b>I. Prior to Permit Issuance</b></p> <p>A. Construction Plan Check</p> <ol style="list-style-type: none"> <li>1. Prior to Notice to Proceed (NTP) for any construction permits,</li> </ol>	<p>Prior to Demolition, Grading or Building Permit (Design)</p> <p>Prior to Certificate of Occupancy (Implementation)</p>	<p>Developer</p>	<p>CCDC/City</p>

SIGNIFICANT IMPACT(S)	MITIGATION MEASURE(S)	IMPLEMENTATION TIME FRAME	IMPLEMENTATION RESPONSIBILITY	VERIFICATION RESPONSIBILITY
	<p>including but not limited to, the first Grading Permit, Demolition Permits and Building Permits, but prior to the first preconstruction meeting, whichever is applicable, Centre City Development Corporation (CCDC) shall verify that the requirements for Paleontological Monitoring have been noted on the appropriate construction documents.</p> <p>B. Letters of Qualification have been submitted to CCDC</p> <ol style="list-style-type: none"> <li>1. The applicant shall submit a letter of verification to CCDC identifying the Principal Investigator (PI) for the project and the names of all persons involved in the paleontological monitoring program, as defined in the City of San Diego Paleontology Guidelines.</li> <li>2. CCDC will provide a letter to the applicant confirming the qualifications of the PI and all persons involved in the paleontological monitoring of the project.</li> <li>3. Prior to the start of work, the applicant shall obtain approval from CCDC for any personnel changes associated with the monitoring program.</li> </ol> <p><b>II. Prior to Start of Construction</b></p> <p>A. Verification of Records Search</p> <ol style="list-style-type: none"> <li>1. The PI shall provide verification to CCDC that a site-specific records search has been completed. Verification includes, but is not limited to a copy of a confirmation letter from San Diego Natural History Museum, other institution or, if the search was in-house, a letter of verification from the PI stating that the search was completed.</li> <li>2. The letter shall introduce any pertinent information concerning expectations and probabilities of discovery during trenching and/or grading activities.</li> </ol> <p>B. PI Shall Attend Precon Meetings</p> <ol style="list-style-type: none"> <li>1. Prior to beginning any work that requires monitoring, the Applicant shall arrange a Precon Meeting that shall include the PI, Construction Manager (CM) and/or Grading Contractor, Resident Engineer (RE), Building Inspector (BI), if appropriate, and CCDC. The qualified paleontologist shall attend any grading/excavation related Precon Meetings to make comments and/or suggestions concerning the Paleontological Monitoring program with the Construction Manager and/or Grading</li> </ol>			

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	<p>Contractor.</p> <ol style="list-style-type: none"> <li>a. If the PI is unable to attend the Precon Meeting, the Applicant shall schedule a focused Precon Meeting with CCDC, the PI, RE, CM or BI, if appropriate, prior to the start of any work that requires monitoring.</li> </ol> <ol style="list-style-type: none"> <li>2. Identify Areas to be Monitored             <ol style="list-style-type: none"> <li>a. Prior to the start of any work that requires monitoring, the PI shall submit a Paleontological Monitoring Exhibit (PME) based on the appropriate construction documents (reduced to 11x17) to CCDC identifying the areas to be monitored including the delineation of grading/excavation limits. The PME shall be based on the results of a site specific records search as well as information regarding existing known soil conditions (native or formation).</li> </ol> </li> <li>3. When Monitoring Will Occur             <ol style="list-style-type: none"> <li>a. Prior to the start of any work, the PI shall also submit a construction schedule to CCDC through the RE indicating when and where monitoring will occur.</li> <li>b. The PI may submit a detailed letter to CCDC prior to the start of work or during construction requesting a modification to the monitoring program. This request shall be based on relevant information such as review of final construction documents which indicate conditions such as depth of excavation and/or site graded to bedrock, presence or absence of fossil resources, etc., which may reduce or increase the potential for resources to be present.</li> </ol> </li> </ol> <p><b>III. During Construction</b></p> <ol style="list-style-type: none"> <li>A. Monitor Shall be Present During Grading/Excavation/Trenching             <ol style="list-style-type: none"> <li>1. The monitor shall be present full-time during grading/excavation/trenching activities as identified on the PME that could result in impacts to formations with high and moderate resource sensitivity. The Construction Manager is responsible for notifying the RE, PI, and CCDC of changes to any construction activities.</li> <li>2. The monitor shall document field activity via the Consultant Site Visit Record (CSV). The CSV's shall be faxed by the CM to the RE the first day of monitoring, the last day of monitoring, monthly (Notification of Monitoring Completion), and in the case of any discoveries. The RE shall forward copies to CCDC.</li> </ol> </li> </ol>			

SIGNIFICANT IMPACT(S)	MITIGATION MEASURE(S)	IMPLEMENTATION TIME FRAME	IMPLEMENTATION RESPONSIBILITY	VERIFICATION RESPONSIBILITY
	<p>3. The PI may submit a detailed letter to CCDC during construction requesting a modification to the monitoring program when a field condition such as trenching activities that do not encounter formational soils as previously assumed, and/or when unique/unusual fossils are encountered, which may reduce or increase the potential for resources to be present.</p> <p>B. Discovery Notification Process</p> <ol style="list-style-type: none"> <li>1. In the event of a discovery, the Paleontological Monitor shall direct the contractor to temporarily divert trenching activities in the area of discovery and immediately notify the RE or BI, as appropriate.</li> <li>2. The Monitor shall immediately notify the PI (unless Monitor is the PI) of the discovery.</li> <li>3. The PI shall immediately notify CCDC by phone of the discovery, and shall also submit written documentation to CCDC within 24 hours by fax or email with photos of the resource in context, if possible.</li> </ol> <p>C. Determination of Significance</p> <ol style="list-style-type: none"> <li>1. The PI shall evaluate the significance of the resource.                     <ol style="list-style-type: none"> <li>a. The PI shall immediately notify CCDC by phone to discuss significance determination and shall also submit a letter to CCDC indicating whether additional mitigation is required. The determination of significance for fossil discoveries shall be at the discretion of the PI.</li> <li>b. If the resource is significant, the PI shall submit a Paleontological Recovery Program (PRP) and obtain written approval from CCDC. Impacts to significant resources must be mitigated before ground disturbing activities in the area of discovery will be allowed to resume.</li> <li>c. If resource is not significant (e.g., small pieces of broken common shell fragments or other scattered common fossils) the PI shall notify the RE, or BI as appropriate, that a non-significant discovery has been made. The Paleontologist shall continue to monitor the area without notification to CCDC unless a significant resource is encountered.</li> <li>d. The PI shall submit a letter to CCDC indicating that fossil resources will be collected, curated, and documented in the Final Monitoring Report. The letter shall also indicate that no further work is required.</li> </ol> </li> </ol>			

SIGNIFICANT IMPACT(S)	MITIGATION MEASURE(S)	IMPLEMENTATION TIME FRAME	IMPLEMENTATION RESPONSIBILITY	VERIFICATION RESPONSIBILITY
	<p><b>IV. Night Work</b></p> <p>A. If night work is included in the contract</p> <ol style="list-style-type: none"> <li>1. When night work is included in the contract package, the extent and timing shall be presented and discussed at the precon meeting.</li> <li>2. The following procedures shall be followed.                             <ol style="list-style-type: none"> <li>a. No Discoveries                                     <ol style="list-style-type: none"> <li>(1) In the event that no discoveries were encountered during night work, The PI shall record the information on the CSV and submit to CCDC via fax by 9am the following morning, if possible.</li> </ol> </li> <li>b. Discoveries                                     <ol style="list-style-type: none"> <li>(1) All discoveries shall be processed and documented using the existing procedures detailed in Sections III - During Construction.</li> </ol> </li> <li>c. Potentially Significant Discoveries                                     <ol style="list-style-type: none"> <li>(1) If the PI determines that a potentially significant discovery has been made, the procedures detailed under Section III - During Construction shall be followed.</li> </ol> </li> <li>d. The PI shall immediately contact CCDC, or by 8AM the following morning to report and discuss the findings as indicated in Section III-B, unless other specific arrangements have been made.</li> </ol> </li> </ol> <p>B. If night work becomes necessary during the course of construction</p> <ol style="list-style-type: none"> <li>1. The Construction Manager shall notify the RE, or BI, as appropriate, a minimum of 24 hours before the work is to begin.</li> <li>2. The RE, or BI, as appropriate, shall notify CCDC immediately.</li> </ol> <p>C. All other procedures described above shall apply, as appropriate.</p> <p><b>VI. Post Construction</b></p> <p>A. Submittal of Draft Monitoring Report</p> <ol style="list-style-type: none"> <li>1. The PI shall submit two copies of the Draft Monitoring Report (even if negative) which describes the results, analysis, and conclusions of all phases of the Paleontological Monitoring Program (with appropriate graphics) to CCDC for review and approval within 90 days following the completion of monitoring,                             <ol style="list-style-type: none"> <li>a. For significant paleontological resources encountered during monitoring, the Paleontological Recovery Program shall be included in the Draft Monitoring Report.</li> <li>b. Recording Sites with the San Diego Natural History</li> </ol> </li> </ol>			

SIGNIFICANT IMPACT(S)	MITIGATION MEASURE(S)	IMPLEMENTATION TIME FRAME	IMPLEMENTATION RESPONSIBILITY	VERIFICATION RESPONSIBILITY
	<p>Museum</p> <ol style="list-style-type: none"> <li>(1) The PI shall be responsible for recording (on the appropriate forms) any significant or potentially significant fossil resources encountered during the Paleontological Monitoring Program in accordance with the City’s Paleontological Guidelines, and submittal of such forms to the San Diego Natural History Museum with the Final Monitoring Report.</li> <li>2. CCDC shall return the Draft Monitoring Report to the PI for revision or, for preparation of the Final Report.</li> <li>3. The PI shall submit revised Draft Monitoring Report to CCDC for approval.</li> <li>4. CCDC shall provide written verification to the PI of the approved report.</li> <li>5. CCDC shall notify the RE or BI, as appropriate, of receipt of all Draft Monitoring Report submittals and approvals.</li> </ol> <p>B. Handling of Fossil Remains</p> <ol style="list-style-type: none"> <li>1. The PI shall be responsible for ensuring that all fossil remains collected are cleaned and catalogued.</li> <li>2. The PI shall be responsible for ensuring that all fossil remains are analyzed to identify function and chronology as they relate to the geologic history of the area; that faunal material is identified as to species; and that specialty studies are completed, as appropriate</li> </ol> <p>C. Curation of fossil remains: Deed of Gift and Acceptance Verification</p> <ol style="list-style-type: none"> <li>1. The PI shall be responsible for ensuring that all fossil remains associated with the monitoring for this project are permanently curated with an appropriate institution.</li> <li>2. The PI shall include the Acceptance Verification from the curation institution in the Final Monitoring Report submitted to the RE or BI and CCDC.</li> </ol> <p>D. Final Monitoring Report(s)</p> <ol style="list-style-type: none"> <li>1. The PI shall submit two copies of the Final Monitoring Report to CCDC (even if negative), within 90 days after notification from CCDC that the draft report has been approved.</li> <li>2. The RE shall, in no case, issue the Notice of Completion until receiving a copy of the approved Final Monitoring Report from CCDC which includes the Acceptance Verification from the curation institution.</li> </ol>			

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<b>SIGNIFICANT IMPACT(S)</b>	<b>MITIGATION MEASURE(S)</b>	<b>IMPLEMENTATION TIME FRAME</b>	<b>IMPLEMENTATION RESPONSIBILITY</b>	<b>VERIFICATION RESPONSIBILITY</b>

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# **Greenhouse Gas Analysis for the San Diego Civic Center Project**

**Prepared for:**

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# Acronyms and Abbreviations

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AB	Assembly Bill
BACMs	best available control measures
BAU	business as usual
CAA	federal Clean Air Act
CAFE	corporate average fuel economy
CalEPA	California Environmental Protection Agency
CAPCOA	California Air Pollution Control Officers Association
CARB	California Air Resources Board
CAT	Climate Action Team
CCAA	California Clean Air Act
CCR	California Code of Regulations
CEC	California Energy Commission
CEQA	California Environmental Quality Act
CCP	Cities for Climate Protection
CH <sub>4</sub>	methane
CO <sub>2</sub> e	carbon dioxide equivalent
CPUC	California Public Utilities Commission
EPA	U.S. Environmental Protection Agency
GHG	greenhouse gas
GWP	global warming potential
HC	hydrocarbons
HDV	Heavy Duty Vehicle
HFCs	hydrofluorocarbons
ICLEI	International Council for Local Environmental Initiatives
IOUs	investor-owned utilities

kWh	kilowatt-hours
LCFS	Low Carbon Fuel Standard
LEDs	light emitting diodes
LEED	Leadership in Energy and Environmental Design
LOS	level of service
MMT	million metric tons
mpg	miles per gallon
MPO	Metropolitan Planning Organization
MSW	municipal solid waste
MWh	megawatt-hours
N <sub>2</sub> O	nitrous oxide
O <sub>3</sub>	ozone
OAL	Office of Administrative Law
OPR	Office of Planning and Research
PFC	perfluorocarbons
ppm	parts per million
RPS	Renewable Portfolio Standard
RTP	Regional Transportation Plan
SANDAG	San Diego Association of Governments
SB	Senate Bill
SF <sub>6</sub>	sulfur hexafluoride
TRUs	Transport Refrigeration Units
VMT	vehicle miles traveled

## Executive Summary

The proposed project calls for the construction of a new city hall with ground-floor retail spaces. The site of the proposed project is currently occupied by the San Diego Convention and Performing Arts Center, a meeting and conference venue; the Community Concourse plaza; and the City of San Diego Administration Building that houses city administrative and legislative functions. The 158,000-square-foot meeting and conference venue and the 189,000-square-foot office building would be demolished and replaced with a 551,794 square-foot office tower and 16,087 square-feet of ground level retail. No changes would be made to the other structures on-site.

This report provides an analysis of potential greenhouse gas (GHG) impacts related to the proposed project as they related to the new CEQA Guidelines, which become effective in March 2010. The proposed project would not result in significant impact on the environment, as the new Civic Center would result in substantially fewer emissions than would likely occur if the project was not built. Specifically, the project:

- Would generate GHG emissions directly and indirectly, but those emissions would not have a significant impact on the environment because they would be substantially less than would likely occur if the project was not built.
- Would not conflict with any applicable plan, policy, or regulation of an agency adopted for the purpose of reducing GHG emissions.
- Would not expose property and persons to the physical effects of climate change, including but not limited to flooding, public health, wildfire risk, or other impacts resulting from climate change.

## Introduction

The new State CEQA Guidelines, which became effective in March 2010, require a discussion of the significance of impacts from greenhouse gas (GHG) emissions on the environment. Specifically, Section 15064.4(a) states that “a lead agency should make a good-faith effort, based on available information, to describe, calculate or estimate the amount of greenhouse gas emissions resulting from a project.”

The analysis below provides the context necessary to determine the potential impacts of GHG emissions from the proposed redevelopment of the San Diego Civic Center Project (proposed project). This analysis describes the potential impacts resulting from construction and operation of the project, including a description of the greenhouse effect and climate change, regulations applicable to GHG emissions and climate change, and a discussion of the global, national, statewide, and local GHG emission inventories. This analysis also includes an inventory of the project’s GHG emissions and a discussion of the project’s contribution to cumulative GHG emissions and climate change. The analysis also includes a discussion of the potential impacts of climate change on the project.

## Project Site Location

The project is located within the City of San Diego. The proposed project would occupy a three-square block within the Civic Core Redevelopment District of the Expansion Sub Area of the Centre City Redevelopment Project, downtown San Diego. The Centre City Redevelopment planning area includes approximately 1,500 acres of the metropolitan core of San Diego, bounded by Interstate 5 on the north and east and San Diego Bay on the south and southwest. The project site is located in the north-central portion of this planning area, west of the San Diego Civic Theatre and bounded by First and Third avenues, and A and C streets.

The area surrounding the project site is accessible via multiple modes of transportation. The area surrounding the project site is easily walkable and accessible via mass transit, as the C Street trolley station, which is served by both blue and orange trolley lines, is directly adjacent to the project site. In addition, the project site is surrounded by a mixture of land uses, including both residential and commercial.

## Development Description

The Civic Center Complex project proposes to replace the existing 158,000-square-foot meeting and conference venue and the 189,000-square-foot office building with a new 551,794 square-foot office tower and 16,087 square-feet of ground level retail. The new City Hall is proposed to consolidate nearly all current downtown City employees into one new facility.

## Project Design Features

The project would include a multitude of strategies that are consistent with the Leadership in Energy and Environmental Design (LEED) certification system. LEED is a voluntary certification program for buildings, and promotes sustainability in key areas, including sustainable sites, water efficiency, energy and atmosphere, indoor air quality, and others. The proposed project includes

various sustainability elements that would be implemented with the objective of achieving at a minimum LEED Gold and potentially LEED Platinum certification. Energy targets include:

- Performing 38% better than LEED Silver minimum efficiency;
- Having 75% of domestic hot water demand to be met with the solar thermal system;
- Offsetting electricity consumption through the incorporation of solar power (photovoltaics);
- Extending the viability of the downtown district's chilled water loop for use by the rest of the community by tapping cooling energy in off-peak night hours; and
- Including additional LEED credits available under the proposed changes for LEED 2009. The project expects to achieve the full number of energy credits available in the rating system.

The design of the Civic Center Complex places the building to the north and east on the block, thereby creating open spaces to optimize access to southern exposure, light and warmth, for both the building and the open spaces. The building floor plates are kept narrow with the intent of increasing occupant access to daylighting and views. Strategies also include maximizing use of district resources such as thermal storage, hydronic heating and cooling, solar thermal panels to supply domestic hot water, lighting sensors that operate lighting and solar shading, natural ventilation, as well as recapture of wastewater for reuse within the development and surrounding sites. Water strategies include a goal of net-zero water use through implementation of water conservation measures and cost-effective use of alternative water resources. This includes reclaiming wastewater to be reused in non-potable applications; meeting and exceeding the City of San Diego's 2003 sustainable buildings policy through the use of cost-effective technologies.

In addition, the Civic Center Complex proposes a partnership with NRG Energy Inc. (NRG), the downtown district chilled water system operator. The two fold plan is; 1) to connect to the existing NRG chilled water network adjacent to the development site, and 2) provide a satellite NRG chilled / hot water plant to be located on the premises (Gerdling Edlen 2008).

## Existing Conditions

### Greenhouse Effect and Climate Change

According to the United States Environmental Protection Agency (USEPA), a GHG is any gas that absorbs infrared radiation in the atmosphere. This absorption traps heat within the atmosphere, maintaining Earth's surface temperature at a level higher than would be the case in the absence of GHGs. GHGs include water vapor, carbon dioxide (CO<sub>2</sub>), methane (CH<sub>4</sub>), nitrous oxide (N<sub>2</sub>O), halogenated chlorofluorocarbons (HCFCs), ozone (O<sub>3</sub>), perfluorinated carbons (PFCs), and hydrofluorocarbons (HFCs). Naturally occurring GHGs include water vapor, CO<sub>2</sub>, CH<sub>4</sub>, N<sub>2</sub>O, and O<sub>3</sub>. Human activities add to the levels of most of these naturally occurring gases. The sources and sinks of each GHG are discussed later in this section.

Since the industrial revolution (approximately 1800), the concentration of CO<sub>2</sub> in the Earth's atmosphere has increased from 270 parts per million (ppm) to roughly 379 ppm. Atmospheric concentrations of CH<sub>4</sub> and N<sub>2</sub>O have similarly increased since the beginning of the industrial age

(IPCC 2007c). Over this same time period, global average surface temperature has increased by 0.6°C, global average sea level has increased by nearly 60 millimeters (mm), and northern hemisphere snow cover (data available since 1920) has decreased by nearly 3 million square kilometers (IPCC 2007c). These recently recorded changes can be attributed with a high degree of certainty to increased concentrations of GHGs in the atmosphere (IPCC 2007c). Sinks of CO<sub>2</sub><sup>1</sup> (which remove, rather than emit, CO<sub>2</sub>), include uptake by vegetation and dissolution into the ocean. Global GHG emissions greatly exceed the removal capacity of natural sinks. As a result, concentrations of GHGs in the atmosphere are increasing (California Energy Commission 2006).

Increasing levels of GHGs in the atmosphere result in an increase in the temperature of the Earth's lower atmosphere, a phenomenon which is commonly referred to as *global warming*. Warming of the Earth's lower atmosphere induces a suite of additional changes including changes in: global precipitation patterns; ocean circulation, temperature, and acidity; global mean sea level; species distribution and diversity; and the timing of biological processes. These large-scale changes are collectively referred to as *climate change*.

The Intergovernmental Panel on Climate Change (IPCC) has been established by the World Meteorological Organization and United Nations Environment Programme to assess scientific, technical, and socioeconomic information relevant to the understanding of climate change, its potential impacts, and options for adaptation and mitigation. As the leading authority on climate change science, their best estimates are that the average global temperature rise between 2000 and 2100 could range from 0.6°C (assuming no increase in GHG emissions above 2000 levels) to 4.0°C (assuming substantial increase in GHG emissions) (IPCC 2007b). Large increases in global temperatures could have massive deleterious impacts on the natural and human environments.

GHGs are global pollutants, unlike criteria air pollutants (such as ozone precursors) and toxic air contaminants (TACs). Criteria air pollutants, such as O<sub>3</sub> precursors and TACs, are pollutants solely of regional and local concern, and local concentrations respond to locally implemented control measures. The long atmospheric lifetimes of GHGs allow them to be transported long distances from sources and to become well-mixed, unlike criteria air pollutants, which typically exhibit strong concentration gradients away from point sources.

## Greenhouse Gases

The GHGs listed by the IPCC (CO<sub>2</sub>, CH<sub>4</sub>, N<sub>2</sub>O, HFCs, PFCs, and sulfur hexafluoride [SF<sub>6</sub>]) are documented in this section, in order of abundance in the atmosphere. California law and the State CEQA Guidelines contain a similar definition of GHGs (Health and Safety Code 38505(g); 14 CCR 15364.5). Water vapor, although the most abundant GHG, is not included in this list because natural concentrations and fluctuations far outweigh anthropogenic influences. The sources and sinks of each of these gases are also discussed in detail.

To simplify reporting and analysis of GHGs, methods have been set forth to describe emissions of GHGs in terms of a single gas, CO<sub>2</sub>. Generally, GHG emissions are quantified in terms of metric tons of carbon dioxide equivalents (CO<sub>2</sub>e) emitted per year. GHGs are compared in terms of their respective global warming potentials (GWP), that is, the warming capacity per molecule given an atmospheric lifetime of 100 years. The IPCC defines the GWP of various GHG emissions on a normalized scale that

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<sup>1</sup> A CO<sub>2</sub> sink is a resource that absorbs CO<sub>2</sub> from the atmosphere. The classic example of a sink is a forest in which vegetation absorbs CO<sub>2</sub> and produces oxygen through photosynthesis.

recasts all GHG emissions in terms of CO<sub>2</sub>e, which compares the gas in question to that of the same mass of CO<sub>2</sub> (CO<sub>2</sub> has a GWP of 1 by definition).

The atmospheric residence time of a gas, or lifetime, is equal to the total atmospheric abundance of the gas divided by its rate of removal. The atmospheric residence time of a gas is, in effect, a half-life measurement of how long a gas is expected to persist in the atmosphere when taking into account removal mechanisms such as chemical transformation and deposition. Table 1 lists the GWP of each GHG, its lifetime, and abundance in the atmosphere in parts per trillion (ppt), parts per billion (ppb) or parts per million (ppm).

**Table 1. Lifetimes, Global Warming Potentials, and Abundances of Several Significant Greenhouse Gases**

<b>Gas</b>	<b>Global Warming Potential (100 years)</b>	<b>Lifetime (years)</b>	<b>2005 Atmospheric Abundance (ppm)</b>
CO <sub>2</sub>	1	50–200	379
CH <sub>4</sub>	21	9–15	1.7
N <sub>2</sub> O	310	120	0.32
HFC-23	11,700	264	1.8 x 10 <sup>-5</sup>
HFC-134a	1,300	14.6	3.5x 10 <sup>-5</sup>
HFC-152a	140	1.5	3.9x10 <sup>-6</sup>
CF <sub>4</sub>	6,500	50,000	7.4x10 <sup>-5</sup>
C <sub>2</sub> F <sub>6</sub>	9,200	10,000	2.9x10 <sup>-6</sup>
SF <sub>6</sub>	23,900	3,200	5.6x10 <sup>-6</sup>

Notes: CF<sub>4</sub> and C<sub>2</sub>F<sub>6</sub> are PFCs. The GWP values presented above are based on the IPCC Second Assessment Report (SAR) and United Nations Framework Convention on Climate Change (UNFCCC) reporting guidelines (IPCC 1996; UNFCCC 2006). Although the IPCC Fourth Assessment Report (AR4) presents different GWP estimates, the current inventory standard relies on SAR GWPs to comply with reporting standards and consistency with regional and national inventories (EPA 2009).

Sources: IPCC 2007a.

## Carbon Dioxide

CO<sub>2</sub> is the most important anthropogenic GHG and accounts for more than 75% of all anthropogenic GHG emissions. Its long atmospheric lifetime (on the order of decades to centuries) ensures that atmospheric concentrations of CO<sub>2</sub> will remain elevated for decades after GHG mitigation efforts to reduce GHG concentrations are promulgated (IPCC 2007b).

Primary sources of anthropogenic CO<sub>2</sub> in the atmosphere are the burning of fossil fuels (including motor vehicles), gas flaring, cement production, and land use changes, including deforestation. CO<sub>2</sub> emissions due to the burning of fossil fuels represent nearly 60% of total GHG emissions worldwide, of which approximately 23% is from the transportation sector. In California the percentage of transportation-related CO<sub>2</sub> emissions is approximately 39%. CO<sub>2</sub> emissions resulting from deforestation are the second largest source of GHGs worldwide (17%) (CEC 2006).

## Methane

CH<sub>4</sub>, the main component of natural gas, is the second most abundant GHG and has a GWP of 21 (IPCC 1996). Anthropogenic emissions of CH<sub>4</sub> are the result of growing rice, raising cattle, combusting natural gas, and mining coal (NOAA 2005). Atmospheric CH<sub>4</sub> has increased from a pre-industrial concentration of 715 ppb to 1,775 ppb in 2005 (IPCC 2007a).

## Nitrous Oxide

N<sub>2</sub>O is a powerful GHG, with a GWP of 310. Anthropogenic sources of N<sub>2</sub>O include agricultural processes, nylon production, fuel-fired power plants, nitric acid production, and vehicle emissions. N<sub>2</sub>O also is used in rocket engines, racecars, and as an aerosol spray propellant. More than 70% of U.S. N<sub>2</sub>O emissions are related to agricultural soil management practices, particularly fertilizer application. N<sub>2</sub>O concentrations in the atmosphere have increased from pre-industrial levels of 270 ppb to 319 ppb in 2005, an 18% increase (IPCC 2007a).

## Hydrofluorocarbons

HFCs are human-made chemicals used in commercial, industrial, and consumer products and have high GWPs (EPA 2006). HFCs generally are used as substitutes for ozone-depleting substances (ODS) in automobile air conditioners and refrigerants. The most abundant HFCs are HFC-134a (35 ppt), HFC-23 (17.5 ppt), and HFC-152a (3.9 ppt). Concentrations of HFCs have risen from zero to current levels.

## Perfluorocarbons

The most abundant PFCs are CF<sub>4</sub> (PFC-14) and C<sub>2</sub>F<sub>6</sub> (PFC-116). These human-made chemicals are emitted largely from aluminum production and semiconductor manufacturing processes. PFCs are extremely stable compounds that are destroyed only by very high-energy ultraviolet rays, which results in the very long lifetimes of these chemicals, as shown in Table 1. Although not listed in Table 1 due to their low abundance, many other PFCs are common and considered in GHG inventories.

## Sulfur Hexafluoride

SF<sub>6</sub>, another human-made chemical, is used as an electrical insulating fluid for power distribution equipment, in the magnesium industry, and in semiconductor manufacturing; and also as a trace chemical for the study of oceanic and atmospheric processes (EPA 2006).

In 1998, atmospheric concentrations of SF<sub>6</sub> were 4.2 ppt and are steadily increasing in the atmosphere. SF<sub>6</sub> is the most powerful of the GHGs listed in IPCC studies, with a GWP of 23,900 (IPCC 1996).

## Background Greenhouse Gas Emissions

In the 2007 IPCC Synthesis Report, global anthropogenic GHG emissions were estimated to be 49,000 million metric tons of CO<sub>2</sub>e in 2004, which is 24% greater than 1990 emissions levels. CO<sub>2</sub> contributed to 76.7% of total emissions; CH<sub>4</sub> accounted for 14.3%; N<sub>2</sub>O contributed 7.9% of total emissions and fluorinated gases (HFCs, PFCs, and SF<sub>6</sub>) contributed to the remaining 1.1% of global emissions in 2004. Energy supply was the sector responsible for the greatest amount of GHG

emissions (25.9%), followed by industry (19.4%), forestry (17.4%), agriculture (13.5%), and transport (13.1%) (IPCC 2007b).

The U.S. Environmental Protection Agency (EPA) estimates that total U.S. GHG emissions for 2008 amounted to 6,956.8 million metric tons of CO<sub>2</sub>e, which is 14% greater than 1990 levels. However, GHG emissions declined from 2007 to 2008. U.S. GHG emissions were responsible for 14.4% of global GHG emissions in 2004 (Intergovernmental Panel on Climate Change 2007b; EPA 2009). The largest contributors to U.S. GHG emissions in 2008 were electricity generation (34.2%), transportation (27.9%), and the industrial sector (19.4%) (EPA 2009).

In December 2006, the CEC published the *Inventory of California Greenhouse Gas Emissions and Sinks: 1990 to 2004*. This report indicates that California is the second largest emitter of GHGs in the United States next to Texas. Worldwide, California is the 12<sup>th</sup> to 16<sup>th</sup> largest emitter of CO<sub>2</sub> (CEC 2006), and is responsible for approximately 2% of the world's CO<sub>2</sub> emissions (CEC 2006). When considering fossil fuel emissions at the individual person level, California is second lowest in the nation in per capita CO<sub>2</sub> emissions. Between 1990 and 2000, California's population grew by 4.1 million people, and during the 1990 to 2003 period, California's gross state product grew by 83% (in dollars, not adjusted for inflation). However, California's greenhouse gas emissions grew by only 12% between 1990 and 2003. The report concludes that California's ability to slow the rate of growth of greenhouse gas emissions is largely due to the success of its energy efficiency, renewable energy programs, and commitment to clean air and clean energy. In fact, the state's programs and commitments lowered its greenhouse gas emissions rate of growth by more than half of what it would have been otherwise (CEC 2006).

The CEC updated the GHG inventory in 2009 to include GHG emissions for the year 2006. Transportation is responsible for 39% of the state's GHG emissions, followed by electricity generation (22%), the industrial sector (21%), agriculture and forestry (5%), and other sources (12%) (CEC 2009a). Emissions of CO<sub>2</sub> and N<sub>2</sub>O are byproducts of fossil fuel combustion and other sources. Methane, a highly potent GHG, results from off-gassing associated with agricultural practices and landfills, among other sources. Sinks of CO<sub>2</sub> include uptake by vegetation and dissolution into the ocean. California GHG emissions in 2006 totaled approximately 479.8 million metric tons (MMT) CO<sub>2</sub>e, a decrease of approximately 2.55 MMT CO<sub>2</sub>e over 2004 GHG emissions.

## **City of San Diego Greenhouse Gas Inventory**

As shown in Table 2, the City of San Diego emitted approximately 15.5 million tons of GHGs in 1990 (City of San Diego 2005). Of this, only 0.2 million tons was the result of the City government's operations. The majority is generated from the community as a whole. These emissions levels were previously projected to result in an increase to 22.5 million tons per year by 2010. By adopting a goal of 15% reduction of baseline (1990) levels, the City hoped to reduce emissions to 13.2 million tons per year by 2010. There is no recent year inventory of City emissions.

The City has identified actions to achieve a 15% reduction goal compared to 1990 levels. These actions include transportation, energy efficiency and renewable energy, waste reduction and recycling, urban heat island policy, and environmentally preferable purchasing for City purchases. The Climate Protection Action Plan also directs the City to collaborate with other cities through ICLEI, increase outreach and education, and document and report progress to decision makers and the public (City of San Diego 2005).

**Table 2. City of San Diego Greenhouse Gas Emission Inventory and Reduction Goals.**

<b>Milestone</b>	<b>Tons of GHG/year</b>
1990 Baseline	15,547,000
2010 “No Action” Projection (BAU)	22,517,000
2010 Projection (Goal)	13,215,000
Difference Between BAU and Goal	9,302,000
Reduction Achieved from 1990 – 2003	3,814,000*
Remaining Reduction Needed by 2010	5,488,000
*The cumulative reductions from 1990-2003 have eliminated the listed tonnage of GHG and thus can be assumed to prevent this same amount from accumulating on a per year basis going forward. Source: City of San Diego 2005.	

## Climate Change Impacts on California

Increases in the globally averaged atmospheric concentration of GHGs will cause the lower atmosphere to warm, in turn inducing a myriad of changes to the global climate system. These large scale changes will have unique and potentially severe impacts in the western United States, California, and the region surrounding the city. Current research efforts coordinated through the California Air Resources Board (CARB), California Energy Commission (CEC), California EPA (Cal/EPA), the University of California (UC) system, and others are examining the specific changes to California’s climate that will occur as the Earth’s surface warms.

The best available climate models indicate that climate change could impact the natural environment in California in the following ways, among others. A growing body of scientific evidence is showing that many of these predicted changes are already underway (CNRA 2009, CEC 2009b).

- rising sea levels along the California coastline, particularly in San Francisco Bay and the San Joaquin Delta caused by ocean expansion and glacier melt;
- extreme-heat conditions, such as heat waves and very high temperatures, which could last longer and become more frequent;
- an increase in heat-related human deaths, infectious diseases, and a higher risk of respiratory problems caused by deteriorating air quality;
- reduced snow pack and stream flow in the Sierra Nevada mountains, affecting winter recreation and water supplies;
- potential increase in the severity and historical pattern of winter storms, affecting peak stream flows and flooding;
- changes in growing season conditions that could affect California agriculture, causing variations in crop quality and yield; and
- changes in distribution of plant and wildlife species brought about by changes in temperature, competition from colonizing species, changes in hydrologic cycles, changes in sea levels, and other climate-related effects.

These alterations of California's climate and ecosystems are occurring at a time when California's population is expected to increase from 34 million to 59 million by 2040 (CEC 2006). As such, the number of people potentially affected by climate change, as well as the amount of anthropogenic GHG emissions, is expected to significantly increase. Similar changes would occur in other parts of the world, with regional variations in vulnerabilities and affected resources.

## Applicable Laws and Regulations

### Federal

Although there is currently no federal overarching law or policy related to climate change or the regulation of GHGs, recent activity, suggests that regulation may be forthcoming. Foremost among recent developments has been the U.S. Supreme Court's decision in *Massachusetts v. EPA* and the USEPA's recent findings on GHGs which are described below. Despite these findings, the future of GHG regulations at the federal level is still uncertain. EPA regulation may be pre-empted by congressional action if a climate change/energy bill is passed by Congress. The following summarizes recent legal cases, legislation, and policies related to climate change and GHG regulation at the federal level.

#### Massachusetts et al. v. U.S. Environmental Protection Agency (2007)

Twelve U.S. states and cities including California, in conjunction with several environmental organizations, sued to force the EPA to regulate GHGs as a pollutant pursuant to the CAA in *Massachusetts et al. v. Environmental Protection Agency* 549 US 497 (2007). The court ruled that the plaintiffs had standing to sue, GHGs fit within the CAA's definition of a pollutant, and the EPA's reasons for not regulating GHGs were insufficiently grounded in the CAA.

#### Energy Independence and Security Act of 2007

The Energy Independence and Security Act of 2007 mandates a host of actions that would aid in the reduction of GHG emissions. These actions include (but are not limited to): fuel economy standard of 35 miles per gallon (mpg) by 2020; improved energy efficiency in lighting and appliances; and investments in efficiency and renewable energy use.

#### Update to Corporate Average Fuel Economy Standards (2009)

The new Corporate Average Fuel Economy (CAFE) standards incorporate stricter fuel economy standards promulgated by the State of California into one uniform standard. Additionally, automakers are required to cut GHG emissions in new vehicles by roughly 25% by 2016. Rule-making to adopt these new standards is still in process and thus they are not yet in effect. When the national program takes effect, California has committed to allowing automakers who show compliance with the national program to also be deemed in compliance with state requirements. (EPA 2010).

#### EPA Rule: Mandatory Reporting of GHGs (2009)

Under the rule, suppliers of fossil fuels or industrial GHGs, manufacturers of vehicles and engines, and facilities that emit 25,000 metric tons or more per year of GHGs are required to report annual

emissions to the EPA. The first annual reports for the largest emitting facilities, covering calendar year 2010, will be submitted to the EPA in 2011. The mandatory reporting rule does not limit GHG emissions but establishes a standard framework for emissions reporting and tracking of large emitters (EPA 2010).

## **EPA “Endangerment Finding” and “Cause or Contribute Finding” (2009)**

In its “Endangerment Finding,” the Administrator of the EPA found that GHG, as described above, in the atmosphere threaten the public health and welfare of current and future generations. The Administrator also found that the combined emissions of these well-mixed GHGs from new motor vehicles and new motor vehicle engines contribute to the GHG pollution that threatens public health and welfare. Although the Finding of Endangerment does not place requirements on industry, it is an important step in the EPA’s process to develop regulation. This action is a prerequisite to finalizing the EPA’s proposed GHG emission standards for light-duty vehicles, which were jointly proposed by EPA and the Department of Transportation’s National Highway Safety Administration on September 15, 2009 (EPA 2010).

In its “Cause or Contribute Finding” the Administrator found that the combined emissions of these well-mixed GHG from new motor vehicles and new motor vehicle engines contribute to the GHG pollution that threatens public health and welfare (EPA 2010).

## **California**

The State of California has adopted legislation, and regulatory agencies have enacted policies, addressing various aspects of climate change and GHG emissions mitigation. Much of this legislation and policy activity is not directed at citizens or jurisdictions but rather establishes a broad framework for the state’s long-term GHG mitigation and climate change adaptation program. The Governor has also issued several executive orders related to the state’s evolving climate change policy.

### **Assembly Bill 32 (2006)—The California Global Warming Solutions Act.**

AB 32 codified the state’s GHG emissions target by requiring that the state’s global warming emissions be reduced to 1990 levels by 2020. Since being adopted, the CARB, CEC, CPUC, and Building Standards Commission have been developing regulations that will help meet the goals of AB 32 and EO S-03-05. The Scoping Plan for AB32 identifies specific measures and actions to reduce GHG emissions to 1990 levels by 2020, and requires CARB and other state agencies to develop and enforce regulations and other initiatives for reducing GHGs (CARB 2008c).

### **Assembly Bill 1493—Pavley Rule**

Known as “Pavley I,” Assembly Bill (AB) 1493 standards are the nation’s first GHG standards for automobiles. AB 1493 requires CARB to adopt vehicle standards that will lower GHG emissions from new light duty autos to the maximum extent feasible beginning in 2009. Additional strengthening of the Pavley standards (Pavley II) has been proposed for vehicle model years 2017–2020. Together, the two standards are expected to increase average fuel economy to roughly 43 mpg by 2020 and reduce GHG emissions from the transportation sector in California by approximately 14%. In June 2009, the EPA granted California’s waiver request enabling the state to enforce its GHG emissions

standards for new motor vehicles beginning with the current model year. The new federal CAFE standards, described above, are the analogous national policy.

### **Executive Order S-03-05 (2005)**

Executive Order (EO) S-03-05 established the following GHG emission reduction targets for California's state agencies:

- by 2010, reduce GHG emissions to 2000 levels;
- by 2020, reduce GHG emissions to 1990 levels; and
- by 2050, reduce GHG emissions to 80% below 1990 levels.

Executive orders are binding only on state agencies. Accordingly, EO S-03-05 will guide state agencies' efforts to control and regulate GHG emissions but will have no direct binding effect on local efforts. The Secretary of Cal/EPA is required to report to the Governor and state legislature biannually on the impacts of global warming on California, mitigation and adaptation plans, and progress made toward reducing GHG emissions to meet the targets established in this executive order.

### **Senate Bills 1078/107 and Executive Order S-14-08—Renewable Portfolio Standard**

Senate Bills (SB) 1078 and 107, California's Renewable Portfolio Standard (RPS), obligates investor-owned utilities (IOUs), energy service providers (ESPs), and Community Choice Aggregations (CCAs) to procure an additional 1% of retail sales per year from eligible renewable sources until 20% is reached, no later than 2010. The California Public Utilities Commission (CPUC) and CEC are jointly responsible for implementing the program. EO S-14-08 set forth a longer range target of procuring 33% of retail sales by 2020.

### **Senate Bill 97, Chapter 185, Statutes of 2007**

SB 97 of 2007 requires that Office of Planning and Research (OPR) to prepare guidelines to submit to the California Resources Agency regarding feasible mitigation of GHG emissions or the effects of GHG emissions as required by CEQA. The Natural Resources Agency adopted Amendments to the CEQA Guidelines for GHG emissions on December 30, 2009. On February 16, 2010, the Office of Administrative Law approved the Amendments, and filed them with the Secretary of State for inclusion in the California Code of Regulations. The Amendments became effective on March 18, 2010 (CNRA 2010).

### **Executive Order S-01-07, Low Carbon Fuel Standard**

EO S-01-07 essentially mandates the following: (1) that a statewide goal be established to reduce the carbon intensity of California's transportation fuels by at least 10% by 2020, and (2) that a Low Carbon Fuel Standard (LCFS) for transportation fuels be established in California.

## **Senate Bill 375—Sustainable Communities Strategy, Chapter 728, Statutes of 2008**

SB 375 provides for a new planning process that coordinates land use planning, regional transportation plans, and funding priorities in order to help California meet the GHG reduction goals established in AB 32. SB 375 requires regional transportation plans, developed by metropolitan planning organizations (MPOs) relevant to the project area, including SANDAG, to incorporate a “sustainable communities strategy” (SCS) in their Regional Transportation Plans (RTPs). The goal of the SCS is to reduce regional VMT through land use planning and consequent transportation patterns. The CARB will set regional GHG reduction targets that will focus each SCS. The regional targets are scheduled to be released by the CARB in September 2010. SB 375 also includes provisions for streamlined CEQA review for some infill projects such as transit-oriented development. However, those provisions will not become effective until an SCS is adopted. SANDAG has not yet developed an SCS and is not expected to adopt an RTP incorporating an SCS until the next RTP update in 2011.

## **California Energy Efficiency Standards for Residential and Non-Residential Buildings (Title 24)**

Energy Conservation Standards for new residential and nonresidential buildings were adopted by the California Energy Resources Conservation and Development Commission in June 1977 and most recently revised in 2008 (24 CCR 6). Title 24 requires that building shells and building components be designed to conserve energy. The standards are updated periodically to allow consideration and possible incorporation of new energy efficiency technologies and methods. This program has been partially responsible for keeping California’s per capita energy use approximately constant over the past 30 years.

On July 17, 2008, the California Building Standards Commission adopted the nation’s first green building standards. The California Green Building Standards Code (proposed Part 11, Title 24) was adopted as part of the California Building Standards Code (24 CCR). Part 11 establishes voluntary standards that will become mandatory in the 2010 edition of the code, including planning and design for sustainable site development, energy efficiency (in excess of the California Energy Code requirements), water conservation, material conservation, and internal air contaminants.

## **Assembly Bill 939, Titles 14, 17, and 27, Chapter 1095, Statutes of 1989**

GHG emissions from landfills are regulated under AB 939, Titles 14 and 27. AB 939 mandated local jurisdictions to meet waste diversion goals of 25% by 1995 and 50% by 2000. In addition, AB 939 established an integrated statewide system for compliance and program implementation. Titles 14 and 27 contain detailed rules on daily operations, handling of specific waste types, monitoring, closure, and record-keeping.

At its June 25, 2009, public hearing, the CARB approved for adoption CCR, Title 17, article 4, sub-article 6, sections 95460 to 95476, Methane Emissions from Municipal Solid Waste Landfills. This regulation is a discrete early action GHG reduction measure, as described in the California Global Warming Solutions Act of 2006 (AB 32; Stats. 2006, chapter 488). It will reduce methane emissions from landfills primarily by requiring owners and operators of certain uncontrolled landfills to install gas collection and control systems, and by requiring existing and newly installed gas collection and control systems to operate optimally.

## **CARB Local Government Operations Protocol**

On September 25, 2008, the Local Government Operations Protocol (LGOP) was adopted by the CARB. The protocol, prepared by the CARB, California Climate Action Registry, ICLEI, and the Climate Registry, provides methods and techniques for the preparation of GHG emissions inventories for local government municipal operations. The adopted protocol does not contain recommendations for GHG reductions by local governments (CARB 2008d).

## **Local**

### **City of San Diego**

The City of San Diego has taken steps to address climate change impacts at a local level. In 2002, the City Council adopted the San Diego Sustainable Community Program. This program established the partnership with the Cities for Climate Protection (CCP) Campaign, which is a program administered by the International Council for Local Environmental Initiatives (ICLEI). The Sustainable Community Program, adopted in 2005, established a GHG reduction goal of 15 percent below 1990 levels by the year 2010. City has continued to reduce its share of GHG emissions through fuel efficiency, energy conservation, the use of renewable energy, and the use of methane gas (biogas) to generate electricity. In addition, the City's most recent General Plan includes various policies that address conservation with the goal of reducing GHG emissions by increased energy efficiency, increase the use of alternative forms of transportation, among others (City of San Diego 2008).

The City has also adopted a LEED Silver building standard for large municipal buildings and remodels. In addition, in April 2010, the City unveiled a new program, called "Centre City Green". This program includes new, incentive-based, "green" building measures that will help new and existing buildings to use less water and less energy, reduce automobile demand, and provide healthy indoor and outdoor spaces for people. Potential incentives include increased building density, faster permitting, development code variances, support of "green team" specialists, and public recognition. The program also includes new lighting strategies for the Centre City and a pilot program to create new "green" streets throughout downtown (CCDC 2010).

In addition, the City has adopted an interim threshold to determine whether a GHG analysis will be required for projects subject to CEQA analysis. The threshold is based on the analysis presented in the recent California Air Pollution Control Officers Association (CAPCOA) report entitled "CEQA & Climate Change". The 900 metric ton screening threshold was adopted for the interim, as it serves as a conservative threshold for requiring further analysis and mitigation. If proposed projects would exceed the 900 metric ton figure then a significant environmental impact may occur. To reduce potential impacts to below a level of significance, proposed projects must show a 30% reduction to the 2020 business-as-usual model, which is consistent with the broad economy-wide goals reflected in AB 32 (City of San Diego 2010).

## **Thresholds of Significance**

The California Resources Agency, with input from OPR and the public, recently adopted revisions to the State CEQA Guidelines that address GHG impacts in the context of CEQA documents. While the new State CEQA Guidelines, which became effective in March 2010, do not specifically establish significance thresholds, they do describe some of the factors that agencies should consider in

determining whether GHG impacts are significant. Specifically, a Lead Agency should consider the following factors, among others, when assessing the significance of GHG emissions:

- the extent to which a project may increase or reduce GHG emissions as compared to the existing environment;
- whether the project emissions exceed a threshold of significance that the lead agency determines applies to the project; and
- the extent to which the project complies with regulations or requirements adopted to implement a statewide, regional or local plan for GHG reduction or mitigation.

Based on the first of these three factors, and in the absence of regulatory standards for GHG emissions, lead agencies should undertake a project-by-project analysis to determine the GHG impacts of the project and whether such impacts are cumulatively considerable. Based on the second of the above factors, the project emissions should be compared to an established threshold of significance that the lead agency determines to be applicable to the project. On a state level, AB 32 identified that an acceptable level of GHG emissions in California in 2020 is 427 MMTCO<sub>2e</sub>, which is the same as the 1990 GHG emissions level. This target level is approximately 15% less than current GHG emissions and about 29% less than projected 2020 BAU conditions. As such, for the purpose of this evaluation, to reduce potential impacts to below a level of significance, proposed projects must show a 30% reduction to the 2020 business-as-usual model, which is consistent with the state-wide goals of AB 32.

Additionally, a lead agency should consider whether climate change would significantly impact the proposed project. In conducting such an evaluation, the agency should focus on the long-term impacts of the project that are more likely to experience the effects of climate change in the future.

In summary, the project would result in a cumulatively significant contribution to climate change if it would:

- Generate GHG emissions, either directly or indirectly, that may have a significant impact on the environment.
- Conflict with any applicable plan, policy, or regulation of an agency adopted for the purpose of reducing GHG emissions.
- Expose property and persons to the physical effects of climate change, including but not limited to flooding, public health, wildfire risk, or other impacts resulting from climate change.

## Methodology

Individual projects, when considered in isolation, are not responsible for anthropogenic climate change, since an individual project's emissions are insufficient to change the radiative balance of the atmosphere. However, even small emissions from a single source contribute to the global GHG emissions total. Because climate change is the result of GHG emissions, and GHGs are emitted by innumerable sources worldwide, global climate change is a significant cumulative impact of human development and activity. Thus, the discussion below references analysis of cumulative contributions to a significant global impact. In addition, the below analysis treats all project emissions as "new", if they represent an increase over the baseline of the existing use of the project site.

The OPR Technical Advisory states “lead agencies must describe the existing environmental conditions or setting, without the project, which normally constitutes the baseline physical conditions for determining whether a project’s impacts are significant” (OPR 2008). The existing setting is that City employees currently occupy 347,000 square feet (SF) of office space at the Civic Center Complex. Existing GHG emissions occur from emissions associated with employee and public commutes, business travel between the different office location within the City, the building energy emissions associated with electricity and natural gas consumption, the waste generated in these offices that is landfilled (resulting in methane gas emissions), the water consumed in these offices (and the energy emissions associated with water transport), and the wastewater from these offices treated at local wastewater treatment facilities (and the associated electricity and methane emissions).

Implementation of the proposed project would result in the existing 347,000 SF Civic Center being replaced by a larger, 551,794 SF, LEED-certified facility. This new facility would include various sustainability measures, as discussed above under the *Development Description*. Implementation of the proposed project would result in GHG emissions as a result of short-term construction and long-term operations. Long-term sources of GHG emissions would be similar to the existing setting, and would result from emissions associated with employee and public commutes, building energy use, solid waste and wastewater generated, and from water consumed in these offices.

In order to determine if a project’s emissions are cumulatively considerable, the emissions generated from the project were compared to a business-as-usual (BAU) scenario. As defined in the CAPCOA white paper, BAU is “the projection of GHG emissions at a future date based on current technologies and regulatory requirements in absence of other reductions”. In effect, BAU defines the CEQA future “No Project” scenario (CAPCOA 2008). With respects to this analysis, BAU is defined as the existing Civic Center Complex operating in the year 2020, which is the assumed buildout year of the proposed project. In addition, BAU assumes that more commercial/office space would be needed downtown to house future employees and retail space, equivalent to the difference between the existing and proposed land uses. Therefore, the current facility is 347,000 SF of office, and the new facility would be approximately 551,794 SF of office and 16,087 SF of retail space. Thus, a difference of approximately 204,794 SF of office and 16,087 SF of retail that would be needed in the future to house employees and retail. In addition, the BAU approach assumes that future commercial/office space would be in an average facility, and the average commercial/office space is assumed to be neither infill nor energy efficient beyond current regulations. This approach allows for an apples-to-apples approach, comparing the emissions from the proposed project to emissions that would potentially occur without the proposed project, assuming the same amount of office and retail space.

Emissions from each of the project scenarios (baseline, BAU, and with project) were calculated using the following methodology. The URBEMIS 2007 (version 9.2.4) model was used to calculate emissions from motor vehicle trips (CARB 2007). Daily trip rates for both the existing and with project scenarios were obtained from the traffic engineer (Fehr & Peers 2010).

Given the design features that are inherent to the project site, the following measures were applied in URBEMIS to reduce vehicle and vehicle miles traveled (VMT) from both the existing and with project scenarios to reflect the favorable downtown location relative to transit and alternate transportation opportunities:

- Baseline employment of 660 for the existing and 2,420 for the with-project scenario;

- Presence of local-serving retail;
- 150 daily rail/trolley stops per day (every 15 minutes for each the Blue and Orange lines, from 4:30am to midnight every day);
- 50 intersection per square mile; and
- 75 percent of sidewalks within ½ mile of the project site with sidewalks on both sides.

As mentioned in the preceding paragraphs, the BAU scenario assumes that future leased space would be occupied within the City. URBEMIS was used to calculate motor vehicle emissions for a standard office land use using the default URBEMIS default trip rate. The trip reductions displayed above were not applied to this “office” land use, as discussed above, as the standard office space could occur anywhere within the City of San Diego and is not assumed to necessarily be downtown or near transit.

GHG emissions generated from electricity consumption for each scenario were calculated according to the methodology in the CCAR General Reporting Protocol (2009). Electricity and natural gas consumption for the baseline and BAU cases were calculated based on consumption rates in presented in the Commercial Buildings Energy Consumption Survey (EIA 2008). To calculate emissions from energy consumption, total energy consumption was multiplied by emission factors presented in the CCAR General Reporting Protocol (2009) for California. Electricity and natural gas consumption for the new building were calculated by estimating the amount of reductions of energy use associated with the new building design.

GHG Emissions for each scenario were the sum of emissions emanating from motor vehicle travel, electricity consumption, and natural gas consumption. Emissions are presented in metric tons per year, consistent with statewide reporting requirements. Other potential sources of emissions (wastewater, water, and solid waste) were not included in this study because it was assumed that the consumption of water and generation of wastewater and solid waste would not change considerable between the BAU and with project scenarios. This is likely a conservative assumption in that the project proposes various water efficiency measures that would likely reduce emissions associated with water and wastewater transport over BAU, thereby further reducing the net-decrease in emissions associated with the project.

In determining the significance of the project, the net change in GHG emissions associated with the project were compared to BAU emissions, and the statewide goal of 30% below BAU threshold was used. In addition, to provide a proper context for the overall efficiency of the project, an efficiency metric was also applied to the project in terms of GHG emissions per City employee, and how these emissions compare to the overall efficiency goals of AB 32. Both CAPCOA and the BAAQMD use a metric known as the “service-population” efficiency threshold. The “service population” is defined as the total number of residents and employees. At a state level, considering the AB 32 reduction goal for 2020 and the projected population in 2020, the AB 32 goal is equivalent to approximately 4.6 MTCO<sub>2e</sub> per service population (CAPCOA 2008, BAAQMD 2009). With respects to the proposed project, if GHG emissions would be more efficient (i.e. less emissions per service population) than the statewide equivalent, then the project can be deemed to not result in GHG emissions that would have a significant on the environment. These results are presented below.

# Impact Analysis

***Would redevelopment of the City of San Diego Civic Center generate GHG emissions, either directly or indirectly, that may have a significant impact on the environment or conflict with the goals of AB 32?***

Implementation of the proposed project would consolidate employees into a highly- efficient LEED certified facility, leading to an overall reduction in GHG emissions compared to the BAU scenario. While the project would result in GHG emission that would exceed the baseline emissions, the baseline scenario accounts for a smaller Civic Center Complex that currently does not have adequate space for downtown City employees.

As shown in Table 3, implementation of the proposed project would result in emissions that are approximately 35% below BAU, which exceeds the 30% below BAU goal of AB 32. In addition, the proposed project would help to achieve the goals of AB 32 by increasing the overall efficiency of GHG emissions per job, resulting in approximately 3.0 MTCO<sub>2e</sub> per employee at project buildout. Thus, the project would result in emissions that are more efficient than both the BAU and baseline scenarios as well as the statewide goal of 4.6 MTCO<sub>2e</sub> per service population.

Based on these preliminary emission calculations, the impacts caused by emissions from the proposed project are considered to be less than significant and the cumulative contribution of the project to climate change would be less than significant and no mitigation would be required.

**Table 3. Estimate of Annual Greenhouse Gas Emissions (MTCO<sub>2e</sub>)**

<b>Sector</b>	<b>Existing (Baseline)</b>	<b>BAU 2020 (No-Project)</b>	<b>With Project 2020</b>
Mobile Source	2,923	7,120	4,892
Electricity	2,198	3,291	1,935
Natural Gas	550	842	511
<i>Total</i>	<i>5,671</i>	<i>11,230</i>	<i>7,321</i>
Number of Employees	660	2,420	2,420
MTCO <sub>2e</sub> per job	8.6	4.7	3.0
Note: URBEMIS and GHG emissions calculation worksheets are provided in the Appendix A and B to this report, respectively.			

In addition, the above analysis does not take into account already adopted state measures that would further reduce GHG emissions from motor vehicles and energy use, including AB 1493(Pavley), the LCFS, and RPS, among others. GHG emissions associated with the proposed project presented above do not include reductions as a result of these statewide measures because they were not necessary to show compliance with AB 32.

***Expose property and persons to the physical effects of climate change, including but not limited to flooding, public health, wildfire risk, or other impacts resulting from climate change.***

The new CEQA Guidelines also require a discussion of the potential increased exposure to the potential adverse effects of global warming as identified in AB 32. As noted earlier, climate change impacts in California include, but are not limited to: sea level rise; extreme heat events; increase in infectious diseases and respiratory illnesses; and reduced snowpack and water supplies.

The project site is not estimated to be inundated by a predicted rise of up to 1.4 meters in sea level rise by 2100 (California Climate Change Center 2009). The project is in the middle of the urban core and thus is not subject to immediate wildfire risks.

While regional water supplies are subject to potential future climate change effects that could affect both local and distant water supplies, the project includes various efficiency measures, which will help to reduce per-capita water demand, thus helping to alleviate demand for scarce statewide water resources.

There are a range of other potential effects of climate change to which the citizens of San Diego may be subject to including increased temperatures and heat stress days, for example, but the new Civic Center will not exacerbate those potential effects nor create a particular hazard to those potential effects. Thus, the project would not result in a significant exposure of property or persons to the potential effects of climate change. This impact is considered to be less than significant, and no mitigation would be required.

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Appendix A  
**URBEMIS2007 Modeling Output Sheets**



Urbemis 2007 Version 9.2.4

Combined Annual Emissions Reports (Tons/Year)

File Name: G:\San Diego\10\_Staff\Air Quality Staff\San Diego Civic Center\GHG Calcs\URBEMIS runs\baseline2010\_with reductions.urb924

Project Name: Civic Center - Baseline

Project Location: South Coast AQMD

On-Road Vehicle Emissions Based on: Version : Emfac2007 V2.3 Nov 1 2006

Off-Road Vehicle Emissions Based on: OFFROAD2007

Summary Report:

AREA SOURCE EMISSION ESTIMATES

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10</u>	<u>PM2.5</u>	<u>CO2</u>
TOTALS (tons/year, unmitigated)							

OPERATIONAL (VEHICLE) EMISSION ESTIMATES

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10</u>	<u>PM2.5</u>	<u>CO2</u>
TOTALS (tons/year, unmitigated)	4.08	4.56	37.95	0.03	5.98	1.17	3,483.82
TOTALS (tons/year, mitigated)	3.80	4.22	35.10	0.03	5.53	1.08	3,221.75
Percent Reduction	6.86	7.46	7.51	0.00	7.53	7.69	7.52

SUM OF AREA SOURCE AND OPERATIONAL EMISSION ESTIMATES

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10</u>	<u>PM2.5</u>	<u>CO2</u>
TOTALS (tons/year, unmitigated)	4.08	4.56	37.95	0.03	5.98	1.17	3,483.82

Both Area and Operational Mitigation must be turned on to get a combined mitigated total.

Operational Unmitigated Detail Report:

OPERATIONAL EMISSION ESTIMATES Annual Tons Per Year, Unmitigated

Source	ROG	NOX	CO	SO2	PM10	PM25	CO2
Government (civic center)	4.08	4.56	37.95	0.03	5.98	1.17	3,483.82
TOTALS (tons/year, unmitigated)	4.08	4.56	37.95	0.03	5.98	1.17	3,483.82

Operational Mitigated Detail Report:

OPERATIONAL EMISSION ESTIMATES Annual Tons Per Year, Mitigated

Source	ROG	NOX	CO	SO2	PM10	PM25	CO2
Government (civic center)	3.80	4.22	35.10	0.03	5.53	1.08	3,221.75
TOTALS (tons/year, mitigated)	3.80	4.22	35.10	0.03	5.53	1.08	3,221.75

Operational Settings:

Includes correction for passby trips

Does not include double counting adjustment for internal trips

Analysis Year: 2010 Season: Annual

Emfac: Version : Emfac2007 V2.3 Nov 1 2006

Summary of Land Uses

Land Use Type	Acreage	Trip Rate	Unit Type	No. Units	Total Trips	Total VMT
Government (civic center)		10.00	1000 sq ft	347.00	3,470.00	18,936.08
					3,470.00	18,936.08

Vehicle Fleet Mix

Vehicle Type	Percent Type	Non-Catalyst	Catalyst	Diesel
Light Auto	51.7	1.2	98.6	0.2
Light Truck < 3750 lbs	7.3	2.7	94.6	2.7
Light Truck 3751-5750 lbs	22.9	0.4	99.6	0.0
Med Truck 5751-8500 lbs	10.6	0.9	99.1	0.0
Lite-Heavy Truck 8501-10,000 lbs	1.6	0.0	81.2	18.8
Lite-Heavy Truck 10,001-14,000 lbs	0.5	0.0	60.0	40.0
Med-Heavy Truck 14,001-33,000 lbs	0.9	0.0	22.2	77.8
Heavy-Heavy Truck 33,001-60,000 lbs	0.5	0.0	0.0	100.0
Other Bus	0.1	0.0	0.0	100.0
Urban Bus	0.1	0.0	0.0	100.0
Motorcycle	2.8	67.9	32.1	0.0
School Bus	0.1	0.0	0.0	100.0
Motor Home	0.9	0.0	88.9	11.1

Travel Conditions

	Residential			Commuter	Commercial	
	Home-Work	Home-Shop	Home-Other		Non-Work	Customer
Urban Trip Length (miles)	12.7	7.0	9.5	13.3	7.4	8.9
Rural Trip Length (miles)	17.6	12.1	14.9	15.4	9.6	12.6
Trip speeds (mph)	30.0	30.0	30.0	30.0	30.0	30.0
% of Trips - Residential	32.9	18.0	49.1			

Travel Conditions

	Residential			Commercial		
	Home-Work	Home-Shop	Home-Other	Commute	Non-Work	Customer
% of Trips - Commercial (by land use)						
Government (civic center)				10.0	5.0	85.0

Urbemis 2007 Version 9.2.4

Combined Annual Emissions Reports (Tons/Year)

File Name: G:\San Diego\10\_Staff\Air Quality Staff\San Diego Civic Center\GHG Calcs\URBEMIS runs\Project\_with reductions.urb924

Project Name: Civic Center - Project

Project Location: South Coast AQMD

On-Road Vehicle Emissions Based on: Version : Emfac2007 V2.3 Nov 1 2006

Off-Road Vehicle Emissions Based on: OFFROAD2007

Summary Report:

OPERATIONAL (VEHICLE) EMISSION ESTIMATES

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10</u>	<u>PM2.5</u>	<u>CO2</u>
TOTALS (tons/year, unmitigated)	3.58	3.45	30.86	0.06	9.94	1.94	5,830.15
TOTALS (tons/year, mitigated)	3.34	3.20	28.54	0.05	9.20	1.79	5,391.57
Percent Reduction	6.70	7.25	7.52	16.67	7.44	7.73	7.52

SUM OF AREA SOURCE AND OPERATIONAL EMISSION ESTIMATES

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10</u>	<u>PM2.5</u>	<u>CO2</u>
TOTALS (tons/year, unmitigated)	3.58	3.45	30.86	0.06	9.94	1.94	5,830.15

Both Area and Operational Mitigation must be turned on to get a combined mitigated total.

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Operational Unmitigated Detail Report:

OPERATIONAL EMISSION ESTIMATES Annual Tons Per Year, Unmitigated

Source	ROG	NOX	CO	SO2	PM10	PM25	CO2
Strip mall	0.16	0.16	1.41	0.00	0.45	0.09	264.84
Government (civic center)	3.42	3.29	29.45	0.06	9.49	1.85	5,565.31
TOTALS (tons/year, unmitigated)	3.58	3.45	30.86	0.06	9.94	1.94	5,830.15

Operational Mitigated Detail Report:

OPERATIONAL EMISSION ESTIMATES Annual Tons Per Year, Mitigated

Source	ROG	NOX	CO	SO2	PM10	PM25	CO2
Strip mall	0.15	0.15	1.30	0.00	0.42	0.08	244.91
Government (civic center)	3.19	3.05	27.24	0.05	8.78	1.71	5,146.66
TOTALS (tons/year, mitigated)	3.34	3.20	28.54	0.05	9.20	1.79	5,391.57

Operational Settings:

- Includes correction for passby trips
- Does not include double counting adjustment for internal trips
- Analysis Year: 2020 Season: Annual
- Emfac: Version : Emfac2007 V2.3 Nov 1 2006

Summary of Land Uses

Land Use Type	Acreage	Trip Rate	Unit Type	No. Units	Total Trips	Total VMT
Strip mall		18.00	1000 sq ft	16.09	289.62	1,433.22
Government (civic center)		10.00	1000 sq ft	551.79	5,517.90	30,111.65
					5,807.52	31,544.87

Vehicle Fleet Mix

Vehicle Type	Percent Type	Non-Catalyst	Catalyst	Diesel
Light Auto	50.6	0.0	100.0	0.0
Light Truck < 3750 lbs	7.2	0.0	98.6	1.4
Light Truck 3751-5750 lbs	23.3	0.0	100.0	0.0
Med Truck 5751-8500 lbs	11.0	0.0	100.0	0.0
Lite-Heavy Truck 8501-10,000 lbs	1.7	0.0	82.4	17.6
Lite-Heavy Truck 10,001-14,000 lbs	0.5	0.0	60.0	40.0
Med-Heavy Truck 14,001-33,000 lbs	1.0	0.0	20.0	80.0
Heavy-Heavy Truck 33,001-60,000 lbs	0.6	0.0	0.0	100.0
Other Bus	0.1	0.0	0.0	100.0
Urban Bus	0.1	0.0	0.0	100.0
Motorcycle	2.9	41.4	58.6	0.0
School Bus	0.1	0.0	0.0	100.0
Motor Home	0.9	0.0	88.9	11.1

Travel Conditions

	Residential			Commuter	Commercial	
	Home-Work	Home-Shop	Home-Other		Non-Work	Customer
Urban Trip Length (miles)	12.7	7.0	9.5	13.3	7.4	8.9
Rural Trip Length (miles)	17.6	12.1	14.9	15.4	9.6	12.6
Trip speeds (mph)	30.0	30.0	30.0	30.0	30.0	30.0
% of Trips - Residential	32.9	18.0	49.1			

Travel Conditions

	Residential			Commercial		
	Home-Work	Home-Shop	Home-Other	Commute	Non-Work	Customer
% of Trips - Commercial (by land use)						
Strip mall				2.0	1.0	97.0
Government (civic center)				10.0	5.0	85.0

Urbemis 2007 Version 9.2.4

Combined Annual Emissions Reports (Tons/Year)

File Name: G:\San Diego\10\_Staff\Air Quality Staff\San Diego Civic Center\GHG Calcs\URBEMIS runs\Leased Space.urb924

Project Name: Civic Center - Leased Space

Project Location: South Coast AQMD

On-Road Vehicle Emissions Based on: Version : Emfac2007 V2.3 Nov 1 2006

Off-Road Vehicle Emissions Based on: OFFROAD2007

Summary Report:

AREA SOURCE EMISSION ESTIMATES

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10</u>	<u>PM2.5</u>	<u>CO2</u>
TOTALS (tons/year, unmitigated)	0.30	0.30	0.80	0.00	0.00	0.00	345.74

OPERATIONAL (VEHICLE) EMISSION ESTIMATES

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10</u>	<u>PM2.5</u>	<u>CO2</u>
TOTALS (tons/year, unmitigated)	2.27	2.61	23.73	0.05	7.91	1.54	4,627.45

SUM OF AREA SOURCE AND OPERATIONAL EMISSION ESTIMATES

	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10</u>	<u>PM2.5</u>	<u>CO2</u>
TOTALS (tons/year, unmitigated)	2.57	2.91	24.53	0.05	7.91	1.54	4,973.19

Area Source Unmitigated Detail Report:

AREA SOURCE EMISSION ESTIMATES Annual Tons Per Year, Unmitigated

<u>Source</u>	<u>ROG</u>	<u>NOx</u>	<u>CO</u>	<u>SO2</u>	<u>PM10</u>	<u>PM2.5</u>	<u>CO2</u>
Natural Gas	0.02	0.29	0.24	0.00	0.00	0.00	344.71
Hearth	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Landscape	0.04	0.01	0.56	0.00	0.00	0.00	1.03
Consumer Products	0.00						
Architectural Coatings	0.24						
<b>TOTALS (tons/year, unmitigated)</b>	<b>0.30</b>	<b>0.30</b>	<b>0.80</b>	<b>0.00</b>	<b>0.00</b>	<b>0.00</b>	<b>345.74</b>

Area Source Changes to Defaults

Operational Unmitigated Detail Report:

OPERATIONAL EMISSION ESTIMATES Annual Tons Per Year, Unmitigated

<u>Source</u>	<u>ROG</u>	<u>NOX</u>	<u>CO</u>	<u>SO2</u>	<u>PM10</u>	<u>PM25</u>	<u>CO2</u>
Strip mall	0.36	0.37	3.32	0.01	1.06	0.21	623.93
Office park	1.91	2.24	20.41	0.04	6.85	1.33	4,003.52
<b>TOTALS (tons/year, unmitigated)</b>	<b>2.27</b>	<b>2.61</b>	<b>23.73</b>	<b>0.05</b>	<b>7.91</b>	<b>1.54</b>	<b>4,627.45</b>

Operational Settings:

Includes correction for passby trips

Does not include double counting adjustment for internal trips

Analysis Year: 2020 Season: Annual

Summary of Land Uses

Land Use Type	Acreage	Trip Rate	Unit Type	No. Units	Total Trips	Total VMT
Strip mall		42.94	1000 sq ft	15.89	682.32	3,376.52
Office park		11.42	1000 sq ft	213.06	2,433.15	21,719.17
					3,115.47	25,095.69

Vehicle Fleet Mix

Vehicle Type	Percent Type	Non-Catalyst	Catalyst	Diesel
Light Auto	50.6	0.0	100.0	0.0
Light Truck < 3750 lbs	7.2	0.0	98.6	1.4
Light Truck 3751-5750 lbs	23.3	0.0	100.0	0.0
Med Truck 5751-8500 lbs	11.0	0.0	100.0	0.0
Lite-Heavy Truck 8501-10,000 lbs	1.7	0.0	82.4	17.6
Lite-Heavy Truck 10,001-14,000 lbs	0.5	0.0	60.0	40.0
Med-Heavy Truck 14,001-33,000 lbs	1.0	0.0	20.0	80.0
Heavy-Heavy Truck 33,001-60,000 lbs	0.6	0.0	0.0	100.0
Other Bus	0.1	0.0	0.0	100.0
Urban Bus	0.1	0.0	0.0	100.0
Motorcycle	2.9	41.4	58.6	0.0
School Bus	0.1	0.0	0.0	100.0
Motor Home	0.9	0.0	88.9	11.1

Travel Conditions

	Residential			Commercial		
	Home-Work	Home-Shop	Home-Other	Commute	Non-Work	Customer
Urban Trip Length (miles)	12.7	7.0	9.5	13.3	7.4	8.9
Rural Trip Length (miles)	17.6	12.1	14.9	15.4	9.6	12.6
Trip speeds (mph)	30.0	30.0	30.0	30.0	30.0	30.0
% of Trips - Residential	32.9	18.0	49.1			
% of Trips - Commercial (by land use)						
Strip mall				2.0	1.0	97.0
Office park				48.0	24.0	28.0

Appendix B  
**Energy Emissions Worksheet**



**San Diego Civic Center, Carbon Dioxide Equivalent Emissions from Building Energy Use**  
(Electricity Consumption and Natural Gas Use Only)

	<b>Units</b>	<b>Project</b>	<b>Existing (Baseline)</b>	<b>Leased Space</b>
Floor Space	Square feet (office)	560,006	347,000	213,006
Floor Space	Square feet (retail)	15,888	0	15,888
Employees	Employees	2,420	660	1,760
<b>Electricity Consumption</b>				
Electricity Demand (office)	Kwh/sf/year (1)	14.6	19.2	14.6
Electricity Demand (retail)	Kwh/sf/year (1)	12.9	12.9	12.9
Electricity Demand	Mwh/year	8,381	6,662	3,315
CO2 Emissions	lb/Mwh (2)	724.12	724.12	724.12
CO2 Emissions	metric tons/year	2752.8	2188.3	1088.8
NO2 Emissions	lb/Mwh (2)	0.0081	0.0081	0.0081
	metric tons			
NO2 Emissions	CO2 equivalent/year (3)	9.5	7.6	3.8
CH4 Emissions	lb/Mwh (2)	0.0302	0.0302	0.0302
	metric tons			
CH4 Emissions	CO2 equivalent (3)	2.4	1.9	1.0
	<b>metric tons</b>			
<b>Subtotal</b>	<b>CO2 equivalent/year</b>	<b>2,765</b>	<b>2,198</b>	<b>1,094</b>
	<i>Project 30% below T24</i>	1,935		
<b>Natural Gas Consumption</b>				
Natural Gas Demand (office)	Cubic feet/sf (1)	23	29	23
Natural Gas Demand (retail)	Cubic feet/sf (1)	27.7	27.7	27.7
Natural Gas Demand	Mmbtu/year	13,720	10,365	5,499
CO2 Emissions	kg/MMbtu (2)	53.06	53.06	53.06
CO2 Emissions	metric tons/year	728.0	550.0	291.8
NO2 Emissions	kg/MMbtu (2)	0.0001	0.0001	0.0001
	metric tons			
NO2 Emissions	CO2 equivalent/year (3)	0.4	0.0	0.0
CH4 Emissions	kg/MMbtu (2)	0.005	0.005	0.005
	metric tons			
CH4 Emissions	CO2 equivalent/year (3)	1.5	0.0	0.0
	<b>metric tons</b>			
<b>Subtotal</b>	<b>CO2 equivalent/year</b>	<b>730</b>	<b>550</b>	<b>292</b>
	<i>Project 30% below T24</i>	511		
<b>Building Total</b>				
<b>TOTAL</b>	<b>metric tons</b> <b>CO2 equivalent/year</b>	<b>3,495</b>	<b>2,748</b>	<b>1,385</b>
<b>TOTAL</b>	<b>metric tons</b> <b>CO2 equivalent/year/job</b>	<b>1.4</b>	<b>4.2</b>	<b>0.8</b>
<i>If Reduced by 30% from BAU</i>	<b>metric tons</b> <b>CO2 equivalent/year</b>	<b>2,446</b>	<b>1,923</b>	<b>970</b>
<i>If Reduced by 30% from BAU</i>	<b>metric tons</b> <b>CO2 equivalent/year/job</b>	<b>1.0</b>	<b>2.9</b>	<b>0.6</b>
CAPCOA	<b>Mass Threshold (metric tons</b> <b>CO2e)</b>	<b>900.0</b>	<b>900.0</b>	<b>900.0</b>
CAPCOA	<b>Service Population</b> <b>Threshold (metric tons</b> <b>CO2e/SP)</b>	<b>4.6</b>	<b>4.6</b>	<b>4.6</b>

(1) From U.S. Department of Energy, 2003 Commercial Buildings Energy Consumption Survey: Consumption and Expenditure Tables. December 2006. Electricity and natural gas consumption factor for office space. Likely overstates demand as building survey was of existing buildings and new building electricity and natural gas consumption likely to be less than existing average consumption.

(2) From California Climate Action Registry, General Climate Action Registry General Reporting Protocol: Repeating Entity-Wide GreenhouseGas Emissions, Version 3.1. January 2009.

(3) CO2 equivalent calculated from Global Warming Potentials of 310 for NO2 and 21 for CH4 from California Climate Action Registry, General Climate Action Registry general Reporting Protocol: Repeating Entity-Wide GreenhouseGas Emissions, Version 3.1. January 2009.

<b>Conversion factors</b>	
Lb to metric ton	0.000453592
cubic feet to MMBtu	0.00103



**Phase I Environmental Site Assessment**



**Assessor Parcel Number  
533-433-28  
(With the Exception of the Civic Theater Portion  
of the Parcel Adjacent to Third Avenue)  
1222 1<sup>st</sup> Avenue and 202 C Street  
San Diego, California**

Presented to:

**CENTRE CITY DEVELOPMENT CORPORATION**

401 B Street, Suite 400  
San Diego, California 92101  
(619) 235-2200

Presented by:

**SCS ENGINEERS**

8799 Balboa Avenue, Suite 290  
San Diego, California 92123  
(858) 571-5500

February 9, 2010

Project Number: 01206548.00

**Offices Nationwide  
[www.scsengineers.com](http://www.scsengineers.com)**



February 9, 2010

Project Number: 01206548.00

Mr. David Allsbrook, Vice President  
Centre City Development Corporation  
401 B Street, Suite 400  
San Diego, California 92101

**Subject: Phase I Environmental Site Assessment (Assessment)**

**Site: Assessor Parcel Number (APN) 533-433-28 (with the Exception of the Civic Theater Portion of the Parcel Adjacent to Third Avenue)  
1222 1<sup>st</sup> Avenue and 202 C Street  
San Diego, California**

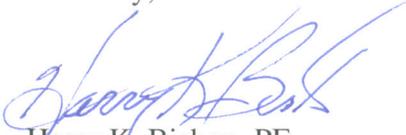
Dear Mr. Allsbrook:

SCS Engineers (SCS) is pleased to present this report (Report) of the Assessment of the above-described Site. This Report summarizes the results of the Assessment that was conducted in order to evaluate the Site's current environmental conditions. The work described in this Report was performed by SCS in general accordance with Exhibit 00 and the Centre City Development Corporation Agreement for Environmental Site Assessment Consulting Service with SCS Engineers (Contract) between SCS and Centre City Development Corporation (Client). The Exhibit and Contract were fully executed on January 8, 2010.

Because your full understanding of the Assessment is important to us, SCS recommends that you read the Report in its entirety. However, if time does not allow you a complete reading, summaries may be found in text boxes at the end of each section (pages 15, 16, 28, and 52 to 55), our conclusions and recommendations may be found on pages 55 to 58, and a summary of the conclusions and recommendations are also included in the Summary Table of Known and Suspected Recognized Environmental Conditions included after the figures. A glossary of terms commonly used in environmental assessment is also provided as an appendix to this Report.

SCS enjoyed working with you on this project. Providing economic environmental solutions to meet your needs is more than our goal—it is our mission and the measure of our success. If we may assist you in any way, now or in the future, please do not hesitate to call our office at (858) 571-5500.

Sincerely,



Harry K. Bishop, PE  
Project Professional  
**SCS ENGINEERS**



Daniel E. Johnson  
Vice President  
**SCS ENGINEERS**



Ryan T. Marcos, CAC  
Project Manager  
**SCS ENGINEERS**

HKB

Enclosures

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**Appendices**

## 1 BACKGROUND

SCS understands that the Site consists of approximately 8.18 acres land (less the Civic Theater portion) with a reported APN of 533-433-28 and reported addresses of 1222 1st Avenue and 202 C Street, San Diego, California. SCS understands that this Assessment is being requested in connection with the possible redevelopment of the Site, including offices, residential uses and retail. The building or excavation plans were not reviewed as part of our Assessment.

Please note that based on the convention promulgated in the city directories, addresses in this Report located west of 1<sup>st</sup> Avenue will be listed as West (e.g., West A Street). Addresses located east of 1<sup>st</sup> Avenue will be listed without a direction designation (e.g. A Street).

The Site reportedly consists of three city blocks and a portion of a fourth city block as follows:

Block	Description
A	Bounded to west and east by Front and 1 <sup>st</sup> Avenue (historically Streets) and to the north and south by West A and West B Street
B	Bounded to west and east by 1 <sup>st</sup> and 2 <sup>nd</sup> Avenue (historically Streets) and to the north and south by A and B Street. Please note that B Street between 1 <sup>st</sup> and 3 <sup>rd</sup> Avenue and 2 <sup>nd</sup> Avenue between A and C Street were closed as a part of the current Site development.
C	Bounded to west and east by 1 <sup>st</sup> and 2 <sup>nd</sup> Avenue (historically Streets) and to the north and south by B and C Street. As noted above, B Street between 1 <sup>st</sup> and 3 <sup>rd</sup> Avenue and 2 <sup>nd</sup> Avenue between A and C Street were closed as a part of the current Site development.
D	A portion of the block historically bounded to west and east by 2 <sup>nd</sup> and 3 <sup>rd</sup> Avenue (historically Streets) and to the north and south by the San Diego Civic Theater and C Street

The Site is reportedly developed with four buildings as follows:

Building/ Block	Description
A	City Operations Building (offices and fire station) (1222 1 <sup>st</sup> Avenue)
B	Parking garage
C	Plaza Hall (Golden Hall)
D	City Administration Building (offices) (202 C Street)

Please see the Historical City Directories Review section for a listing of interpreted current and historical Site addresses.

A review of an Environmental FirstSearch Report indicated that a number of facilities store or use hazardous materials, generate hazardous wastes, or have leaking underground storage tanks in the Site vicinity. Please note that relevant facilities were investigated as part of this Assessment.

## 2 STANDARDS BACKGROUND

This Assessment was conducted in general accordance with the following:

- Environmental Protection Agency (EPA), 40 CFR Part 312, Standards and Practices for All Appropriate Inquiries; Final Rule (AAI).
- American Society for Testing and Materials (ASTM) Standard Practice for Phase I Environmental Site Assessment Process E1527-05.
- The scope, conditions, and limitations of our Contract.

The Client understands that the above-referenced EPA and ASTM standards were not developed to identify all environmental risk to property. The standards were developed to allow a user (Client) to qualify for the innocent purchaser defense, bona fide prospective purchaser defense, and contiguous property owner defense to the Comprehensive Environmental Response, Compensation, and Liability Act of 1980 (CERCLA, a.k.a Superfund) liability. This Assessment is intended to constitute an appropriate inquiry into the previous ownership and uses of the property consistent with good commercial or customary practice, as part of the due diligence process required by CERCLA, the Superfund Amendments and Reauthorization Act of 1986 (SARA), and the Small Business Liability Relief and Brownfields Revitalization Act of 2002 (Acts).

Please note that this Assessment may initially qualify the Client for a CERCLA defense; however, after purchase, there may be “continuing obligations” that must be implemented in order to preserve this defense through the term of property ownership. There may be additional requirements under state law that also apply. The Client should contact qualified legal counsel regarding matters of liability, interpretation of the Acts, and potential continuing obligations. While it is outside the scope of this Assessment, SCS would be pleased to work with your legal counsel to develop and implement a strategy to preserve your CERCLA liability defenses through the term of your ownership.

This Assessment focused on potential sources of hazardous substances and petroleum products that could be considered a *recognized environmental condition*<sup>1,2</sup> and liability due to their presence in significant concentrations (e.g., above acceptable limits set by the federal, state, or local government) or due to the potential for exposure and risk due to contaminant migration and complete exposure pathways (e.g., soil vapor inhalation or groundwater ingestion). Materials that contain substances that are not currently deemed hazardous by the United States Environmental Protection Agency (U.S. EPA) or the California EPA (Cal-EPA) were not considered as part of this Assessment.

Unless specifically included in SCS’s scope of services, building materials such as asbestos, lead-based paint, lead in drinking water, urea formaldehyde, and pressure-treated lumber are not

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<sup>1</sup> *Recognized environmental conditions*, as defined by ASTM, include the presence or likely presence of hazardous substances or petroleum products on a property that indicate an existing release, a past release, or a material threat of release of any hazardous substances or petroleum products into structures on the property or into the ground, groundwater, or surface water on the property. However, the term is not intended to include *de minimis* conditions. A condition considered *de minimis* is not a recognized environmental condition.

<sup>2</sup> *De minimis condition*. An environmental condition that does not generally present a material risk of harm to the public health or the environment and that generally would not be subject to an enforcement action if brought to the attention of appropriate governmental agencies.

considered in this Report, nor are building issues such as fire safety, indoor air quality, mold, or similar matters. SCS did not evaluate the Site for compliance with land use, zoning, wetlands, or similar laws. This Assessment also excludes regulatory compliance, cultural and historic resources, industrial hygiene, health and safety, ecological resources, endangered species, and high-voltage power lines. This Report is not intended to be an environmental compliance audit.

Hazardous substances occurring naturally in plants, soils, and rocks (e.g., heavy metals, naturally occurring asbestos, or radon) are not typically considered in these investigations. Similarly, construction debris (e.g., discarded concrete, asphalt) is not considered, unless obvious indications suggest that hazardous substances are likely to be present in significant concentrations or likely to migrate.

An evaluation of business environmental risk associated with a parcel of commercial real estate may necessitate investigation beyond that included herein.

### 3 OBJECTIVE

The objective of the scope of services was to assess the likelihood<sup>3</sup> that recognized environmental conditions are present at the Site as a result of the current or historical Site land use or from a known and reported off-Site source.

### 4 SCOPE OF SERVICES

The scope of services designed and conducted to meet the objective was as follows:

- Site Reconnaissance, Site Research, Interviews, and User Requirements
- Topography, Geology, Soils, Hydrogeology, and Water Quality Survey
- Site Vicinity Reconnaissance and Off-Site Source Survey
- Historical Site and Site Vicinity Land Use Review
- Identification of Data Gaps
- Data Evaluation, Figure Preparation, and Assessment Report Preparation

### SITE RECONNAISSANCE

On November 17, 2009, SCS personnel conducted a Site reconnaissance to observe and document existing Site conditions.<sup>1</sup> The general Site location is shown in Figure 1 and a Site and Site vicinity plan is shown in Figure 2. Selected color photographs of the Site are presented as Figures 3a through 3s.

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<sup>3</sup> Statements of “likelihood” are made in this Report, based on the professional judgment of SCS. A description of likelihood statements, as made in this Report, is included on page 59.

The Site buildings were observed and the Site grounds and Site perimeters were systematically traversed on foot during the Site reconnaissance. Ms. Mary Lou Houghton, City of San Diego Property Agent, arranged for the Site reconnaissance. Mr. Glen Simpson, City of San Diego Building Maintenance Supervisor; Mr. Erick Erickson, Assistant Facility Service Manager; and Mr. Brad Morrison, Assistant Engineering Manager, accompanied SCS personnel, provided access to the observed features, and answered questions posed by SCS personnel. Access to the Golden Hall stage (Building C) and parking structure (Building B) elevator rooms (Building C) was not available during the Site reconnaissance.

### General Information

The following table summarizes general information in connection with the Site:

<b>Assessor's Parcel Number (APN)</b>	533-433-28
<b>Reported Address</b>	1222 1 <sup>st</sup> Avenue and 202 C Street, San Diego, California
<b>Reported Area</b>	8.18 acres land (less the Civic Theater portion)
<b>Site Land Use</b>	City of San Diego city hall, city operations, fire station, public meeting hall, and parking
<b>Occupant</b>	City of San Diego
<b>Figure Reference</b>	Figures 3a-1, 3a-2, 3b-1, 3b-2, and 3c-1

### Site Buildings

The following table summarizes information in connection with the Site buildings:

<b>Number of Buildings</b>	Four
<b>Interpreted Construction Date</b>	Building A - 1964 Building B - 1964 Building C - 1964 Building D - 1964
<b>Number of Stories</b>	Building A - Five Building B - Twelve parking levels Building C - Two Building D - Thirteen
<b>Construction Type</b>	Building A - Steel-frame, concrete and glass walls, concrete foundation Building B - Steel-frame, concrete walls, concrete foundation Building C - Steel-frame, concrete and glass walls, concrete foundation Building D - Steel-frame, concrete and glass walls, concrete foundation
<b>Figure Reference</b>	Figures 3a-1, 3a-2, 3b-1, 3b-2, 3c-1, and 3h-2

### Block A

Block A was observed to be occupied by five-story office (Operations Building) building (Building A) containing various city departments (e.g., building, planning, and engineering)

(Figures 3c-2) and a fire station (Figure 3d-1). Building A was observed to be served by three cable-operated passenger elevators near the center of the building and a hydraulic freight elevator adjacent to the north perimeter of the building (Please see the Hydraulic Elevator section below for a discussion of this elevator). A diesel fuel dispenser was observed adjacent to the north wall of the fire station (Figure 3d-2). A 6,000-gallon diesel fuel underground storage tank (UST) was reportedly located beneath the sidewalk adjacent (west) to the fire station. A fill port for this UST was observed in the sidewalk (Figure 3e-1). A possible groundwater monitoring well was observed in the sidewalk adjacent (south) of the current UST fill port (Figure 3e-2). This well was reported not to be associated with a reported removal of former USTs (discussed in the Interview sections below).

An emergency response facility (Figure 3f-1) was observed to be located in the basement of Building A. Also located in the basement of Building A was an emergency diesel generator (Figure 3f-2). The source of fuel for the emergency generator was reported to be the fire department UST discussed above. A 373-gallon electrical transformer (Figure 3g-1) was also observed in the basement of Building A. The electrical transformer was reportedly installed when the Site building was constructed (circa 1964). A stormwater lift station was also observed in the Building A basement (Figure 3g-2). A blueprint machine was reportedly located in the building basement. The blueprint machine was reportedly removed approximately 10 years ago and the room is currently used for equipment (cabinets) storage. No obvious indication of releases (staining) was observed on the floor of the former blueprint equipment room.

### **Block B**

Block B was observed to be occupied by a twelve-level parking garage (Building B) (Figure 3h-1). Four cable-operated elevators were observed in the southeast portion of the building. As noted above, access to the elevator room was not available at the time of the Site reconnaissance.

### **Block C**

Block C was observed to be occupied by the two-story Plaza Hall (Golden Hall) building (Building C). This building was observed to include offices, an auditorium/stage (Figure 3i-1), storage rooms, and a printing shop (Figure 3i-2). As noted in the Interview section below, the printing shop reportedly began operation approximately 4 to 5 years ago. Storage rooms and heating/cooling equipment were observed in the basement of Building C (Figure 3j-1). Four hydraulic elevators were observed in Building C (Figure 3i-2) (please see the Hydraulic Elevator section below for a discussion of these elevators). Heating and cooling equipment was observed to be located on the roof and in the basement of Building C (Figures 3i-2 and 3j-1).

### **Block D**

Block D was observed to be occupied by the San Diego City Administration Building (City Hall) (Building D). The building was observed to be occupied by city offices, including the Mayor's office and the City Council Chambers (Figure 3k-1). The building was observed to be served by four cable-operated elevators. A typical view of the elevator room is shown in Figure 3k-1.

### Print Shop

As noted above, a print shop was observed in Building C. Based on the length of time the shop has operated (4 to 5 years), the type of cleaning solvents used (hydrocarbon), and the absence of any obvious releases, there is a low likelihood that a recognized environmental condition exists at the Site.

### Hydraulically Operated Elevators

Hydraulically-operated elevators were observed at various locations (Buildings A and C). The elevators were reportedly serviced by Elevator Service Company under the direction of the Plant Engineer (Mr. Manny Mojica). The elevator rooms were inspected and observed to be occupied only by elevator equipment (Figure 3j-2). No obvious indications of releases of elevator fluid (hydraulic oil containing polychlorinated biphenyls [PCBs]) (e.g. staining or free oil) were observed in the elevator rooms. A record of elevator operations is reportedly kept by Mr. Mojica. Mr. Mojica reported that hydraulic oil had never been added to the elevator systems and no indications of a release had ever been noted. Access to elevator shaft/pit was not available at the time of our Site reconnaissance. Therefore, SCS could not access the potential for releases or the condition of the concrete at the bottom of the shaft/pits. However, as noted above, based on an interview with the Plant Engineer, there is no obvious evidence of releases of hydraulic fluid and the concrete at the base of the shaft/pits are in good condition.

## **Site Grounds**

### **Block A**

The north portion of the Block A was observed to be paved with asphalt and used for parking. The entire block was observed to be surrounded by concrete sidewalks.

### **Block B**

The north perimeter of Block B was observed to contain a landscape area between the sidewalk and A Street. Concrete sidewalks were observed on the west perimeter of Block B and a paved pedestrian walkway was observed on the east perimeter of Block B. Block B was observed to be connected to the third floor of Block A by a pedestrian walkway which spans 1<sup>st</sup> Avenue.

### **Block C**

The east perimeter of Block C was observed to be occupied by a paved pedestrian walkway and plaza. The south and west perimeters of Block C were observed to be occupied by concrete paved sidewalks.

### **Block D**

The north perimeter of Block D was observed to be occupied by a paved pedestrian walkway and plaza. The east perimeter of Block D was observed to be occupied by the Civic Theater. The

south perimeter of Block D was observed to be occupied by concrete paved sidewalk and a San Diego Trolley station. The west perimeter of Block D was observed to be occupied by Block C

### Hazardous Materials/Petroleum Products

Hazardous materials and petroleum products observed to be used or stored at the Site included the following:

Hazardous Materials/Petroleum Products	Storage Location	Approximate Amount
Janitorial supplies (disinfectants and detergent cleaners)	Janitorial closets in Buildings A, C, and D (Figure 3m-1)	Small retail quantities (SRQs) <sup>4</sup>
Gasoline	Building A (fire station) (Figure m-2)	Five 5-gallon containers
Spray paint cans, lubricating oil, cleaners (WD-40)	Building A (fire station)/Building C (printing shop)	SRQs
Acetone	Building C (printing shop)	Four 2-gallon containers
Inks	Building C (printing shop) (Figure 3n-2)	Forty one-pint containers
Naphthalene	Building C (printing shop) (Figure 3n-1)	Five 5-gallon containers
Lubrication oil	Building C basement	Nine 5-gallon containers
Poly(maleicacid) (water treatment)	Building C basement (Figure 3o-1)	50 gallons
Amines (water treatment)	Building C basement (Figure 3o-1)	25 gallons
Polysulfite (water treatment)	Building C basement (Figure 3o-1)	25 gallons
Microbiocide (water treatment)	Building C basement (Figure 3o-1)	Four 5-gallon containers
Hydraulic fluid	Elevator rooms in Building A and C	20 to 30 gallons per elevator
Diesel fuel	UST adjacent to Building A	6,000 gallons

### Hazardous Wastes

Hazardous wastes observed to be generated at the Site included the following:

Hazardous Materials/Petroleum Products	Storage Location	Approximate Amount
Infectious waste	Building A (fire station)	20 pounds

<sup>4</sup> Quantities of hazardous materials in containers of 5 gallons or less, and less than 50 gallons in aggregate.

## Indications of Releases of Hazardous Materials/Wastes or Petroleum Products

The following table summarizes the various indications of releases of hazardous materials/wastes or petroleum products that were noted during the Site reconnaissance and the likelihood<sup>5</sup> that these interpreted releases may have impacted the subsurface at the Site, and whether or not the release is considered to be a recognized environmental condition or a *de minimis*<sup>6</sup> condition as defined by ASTM.

Location	Material/Waste	Indications of Release	Likelihood of Significant Subsurface Impact	REC or <i>de minimis</i>
Building A and B parking	Lubricating oil	Staining to asphalt and concrete pavement	Low	<i>de minimis</i>

The minor surficial staining (interpreted to be from automotive lubricants) to paved surfaces of the Site is considered likely to be *de minimis* as defined by ASTM.

## On-Site Utilities

<b>Gas and Electricity</b>	Reported to be San Diego Gas and Electric
<b>High-Power Transmission Lines</b>	None observed at or adjacent to the Site
<b>Storm Drains</b>	Parking areas (Blocks A, B, and C)
<b>Source of Heating and Cooling</b>	Reported to be San Diego Gas and Electric
<b>Potable Water Source</b>	Reported to be supplied by the City of San Diego Metropolitan Water District
<b>Wastewater Conveyance</b>	Reported to be operated by the City of San Diego Metropolitan Wastewater Department

Two San Diego Gas and Electric (SDG&E) electrical transformers were observed to be located in the basement of Building A. SDG&E has been contacted regarding the possibility of polychlorinated biphenyls (PCBs) being present in transformers purchased by them. SDG&E reported that it has never specified PCBs in its transformers. A copy of a letter from SDG&E explaining this and its PCB testing policy is included in the Appendix. No obvious indications of leaks such as stained concrete were noted near the transformers. As noted in the Interview section below, the Site contact (Mr. Simpson) stated that he has contacted SDG&E in connection with the possible presence of PCBs in the transformer fluid and that SDG&E had never analyzed the transformer fluid for PCBs.

<sup>5</sup> The likelihood that a release of hazardous material/waste or petroleum product has impacted the subsurface is based on one or more of the following: SCS experience with similar releases; the chemical properties of the substance released; the amount and/or frequency of the release; and, the presence or absence of potential pathways to the subsurface.

<sup>6</sup> Releases that have impacted the subsurface may still be considered a *de minimis* condition if they do not present a material risk of harm to the public health or the environment and that generally would not be subject to an enforcement action if brought to the attention of appropriate governmental agencies.

With the possible exception of the inground stormwater lift sump and a diesel UST (discussed above) no obvious indications of wells, cisterns, pits, sumps, dry wells, or bulk storage tanks were observed at the Site.

## SITE RESEARCH

### DEH File Review

The DEH was contacted<sup>ii</sup> and the file for the Site was reviewed. Information from the DEH file and from SCS's in-house HE-17 database is summarized below and copies are included in the Appendix.

### DEH File Reviews for Site Activities

The following tables summarize Compliance Inspection Reports (CIR) dates, hazardous materials and petroleum products reported to be used and stored at the Site, hazardous wastes reported to be generated at the Site, and violations of the hazardous waste control law.

DEH File No. 121379 - San Diego City Fire Station (1222 1<sup>st</sup> Avenue)

### Compliance Inspection Reports (CIRs)

Date	Violations
1989	Recording keeping, waste container not properly labeled
1990	Recording keeping, waste container not properly labeled and not kept closed, <b>diesel fuel in UST fill port, disposal or of hazardous waste to an unauthorized point</b>
1992	UST integrity test results not provided
1993	Record keeping
1994	Record keeping, UST leak detector does not meet performance standards, annual integrity test not conducted, inventory reconciliation not properly implemented
1995	UST pump dispenser meter not calibrated annually as required, inventory reconciliation not performed, UST release detection method does not meet performance standards
1996	UST monitoring/maintenance/calibration records not maintained, UST release detection method does not meet performance standards, inventory reconciliation not properly implemented, facility did not properly report/investigate unauthorized release
1997	Tank owner failed to conduct annual integrity test as required, owner has not tested the pressurized product line leak detection device as required, facility has not complied with proper closure requirements for abandoned UST, hazardous waste containers not properly labeled
2001	Monitoring procedures for UST has not been prepared and implemented, response plan for release into secondary containment not available, facility did not properly report/investigate unauthorized release
2002	UST interstitial space monitoring system not functional, spill container/overfill prevention system not properly installed/maintained, waste disposal receipts file not maintained
2005	Diesel fuel and debris in UST spill container, diesel fuel and water in UST spill sump, diesel dispenser meter leaking into dispenser

Date	Violations
2006	UST operating permit not available, <b>facility did not properly report/investigate unauthorized releases, diesel leaking into under dispenser containment (UDC), UST piping severely corroded</b>
2007	<b>Diesel released into fill sump</b> , fill sump sensor not properly placed
2008	<b>Spill container not in good condition or liquid free</b> , spill containment testing not yet performed

Based on the review of the HE-17 database for the Site, the following hazardous materials or petroleum products were reported to have been historically stored and/or used at the Site.

Hazardous Materials	Maximum Quantity	Annual Quantity
Diesel	6,000	Not listed
Compressed Gas	600 cubic feet	1,200 cubic feet

Based on the review of the HE-17 database for the Site, the following hazardous wastes were reported to have been historically generated at the Site.

Hazardous Waste	Maximum Quantity	Annual Quantity
Infectious waste	20 pounds	240 pounds
Waste oil	55 gallons	220 gallons

Based on readily available information, SCS is unable to assess the significance of the unauthorized disposal violation noted in the 1990 CIR. The 2006 and 2007 CIRs reported diesel fuel leaking into a containment and fill sump.

Based on the reported releases in connection with the UST system at the Site, there is a potential that a recognized environmental condition may exist at the Site. In addition, SCS understands that the Site may be redeveloped. If this UST is removed in connection with redevelopment, there is a potential that diesel-bearing soil may be encountered during redevelopment. Based on the lack of data and SCS's experience, we recommend that the Client determine if the UST has had releases. In addition, based on their interpreted location (adjacent to Building A), SCS recommends the collection of soil samples (to determine if a release has occurred) and soil vapor samples (to determine the potential for health risk due to possible vapor intrusion into Building A).

### **UST Removal Case at Site**

DEH File Number: 121379 San Diego Fire Station Number 1 - 1222 1<sup>st</sup> Street

A 3,000-gallon gasoline and a 3,000-gallon diesel UST were reportedly removed from the Site in April 1999. A third 16,000-gallon diesel UST at the Site was reportedly closed in-place at the same time. Soil samples were reportedly collected from the 3,000-gallon UST excavations and from the north and south ends of the 16,000-gallon UST. Total petroleum hydrocarbon (diesel range) concentrations in all the collected soil samples were reported to be not detected above the

laboratory detection limit (ND). A 6,000-gallon diesel UST was reported to have been subsequently installed at the Site.

Based on the available data, including the absence of constituents of concern (CoCs) in soil samples collected from the UST excavations and SCS's experience, there is a low likelihood that a recognized environment condition exists at the Site as a result of the known and reported removed or closed-in-place USTs at the Site.

### Fire Department Records Review

The City of San Diego Fire Department (SDFD) was contacted regarding hazardous materials/waste or UST records for the current and historical Site addresses.<sup>iii</sup> As of the date of this report, the SDFD reportedly has found files for two addresses (1207 Front Street and 139 A Street). However, copies of the files have not been provided.

### Building Department Records Review

The City of San Diego Building Department (SDBD) records were reviewed for the current and historical Site addresses and are summarized in the table below.<sup>iv</sup> Copies of the SDBD records for the Site are included in the Appendix. Facilities interpreted to use or store hazardous materials or petroleum products and/or generate hazardous wastes are indicated in bold print.

Address	Permit Number	Date Approved	Permit Type	Owner/Applicant
Block A				
120 W B Street	04613	1955	Neon sign	National Auto Glass
140 W B Street	10070	1955	Electrical service	<b>Tufford Motor Co.</b>
140 W B Street	00242	1955	Electrical service	<b>Tufford Motor Co</b>
140 W B Street	00242	1958	Electrical service	<b>Tufford Motor Co</b>
140 W B Street	24411	1958	Electrical service	<b>Tufford Motor Co</b>
140 W B Street	26171	1958	Electrical service	<b>Tufford Motor Co</b>
140 W B Street	45949	1958	Electrical service	<b>Tufford Motor Co</b>
140 W B Street	10667	1959	Electrical service	<b>Midas Muffler</b>
140 W B Street	11291	1959	Electrical service	<b>Midas Muffler</b>
140 W B Street	45949	1961	Electrical service	Wilson Bail Bond
1271 Front Street	0975	1956	Electrical service	<b>Marvin Brown</b>
1207 Front Street	11304	1959	Electrical service	<b>Midas Muffler</b>
1202 1 <sup>st</sup> Avenue	53893	1955	Electrical Service	Ryon
1202 1 <sup>st</sup> Avenue	014087	1959	Electrical Service	Ondo
1242 1 <sup>st</sup> Avenue	6655	1955	Plumbing Service	<b>Zwiener Electrical Co.</b>
1242 1 <sup>st</sup> Avenue	66990	1957	Electrical Service	<b>Zwiener Electrical Co.</b>

Address	Permit Number	Date Approved	Permit Type	Owner/Applicant
1242 1 <sup>st</sup> Avenue	70334	1964	Electrical Service	City of San Diego
1264 1 <sup>st</sup> Avenue	25544	1956	Electrical Service	Log Hut
Block B				
124 B Street	51785	1955	Electrical service	El Serape
132 B Street	91292	1957	Refrigeration service	B&B Café
132 B Street	123592	1958	Refrigeration service	B&B Café
134 B Street	46609	1955	Electrical service	Jack's Place
134 B Street	62041	1955	Electrical service	Jack's Place
134 B Street	97928	1957	Electrical service	Jack's Place
136 B Street	45307	1955	Electrical service	Jack's Place
140 B Street	45307	1955	Electrical service	Jack's Place
140 B Street	11106	1959	Electrical service	Philpot
1244 2 <sup>nd</sup> Avenue	23507	1954	Electrical service	Brown (deli)
1243 1 <sup>st</sup> Avenue	31410	1960	Refrigeration service	Brown's Deli
1245 1 <sup>st</sup> Avenue	49654	1955	Electrical service	Evan Jones
1265 1 <sup>st</sup> Avenue	07764	1965	Electrical service	City Center Parking
Block C				
1125 1 <sup>st</sup> Avenue	26623	None	Electrical service	Nelson-Thomas
1125 1 <sup>st</sup> Avenue	72725	1957	Electrical service	Nelson-Thomas
1165 1 <sup>st</sup> Avenue	13982	1955	Electrical service	<b>Trevelin Oldsmobile</b>
1165 1 <sup>st</sup> Avenue	79030	1957	Electrical service	<b>Sunset Motors Import</b>
1165 1 <sup>st</sup> Avenue	87645	1957	Electrical service	<b>Brown</b>
1165 1 <sup>st</sup> Avenue	97352	1957	Electrical service	<b>Sunset Motors Import</b>
1165 1 <sup>st</sup> Avenue	104186	1957	Electrical service	<b>Sunset Motors Import</b>
1165 1 <sup>st</sup> Avenue	02149	1958	Electrical service	<b>Sunset Motors Import</b>
1165 1 <sup>st</sup> Avenue	08858	1958	Electrical service	<b>Sunset Motors Import</b>
1165 1 <sup>st</sup> Avenue	26415	1960	Electrical service	<b>Sunset Motors Import</b>
208 C Street	21461	1954	Electrical service	Parron Hall Company
208 C Street	22154	1954	Electrical service	Parron Hall Company
208 C Street	99224	1957	Electrical service	Parron Hall Company
150 C Street	78939	1964	Electrical service	SD Community Concourse
202 C Street	24141	1966	Electrical service	SD Community Concourse
202 C Street	47930	1966	Electrical service	SD Community Concourse

Address	Permit Number	Date Approved	Permit Type	Owner/Applicant
202 C Street	44053	1966	Electrical service	SD Community Concourse
202 C Street	44055	1966	Electrical service	SD Community Concourse
202 C Street	44057	1966	Electrical service	SD Community Concourse
202 C Street	18361	1970	Electrical service	SD Community Concourse
202 C Street	97447	1970	Electrical service	SD Community Concourse
202 C Street	74363	1973	Electrical service	SD Community Concourse
202 C Street	85925	1974	Electrical service	SD Community Concourse
202 C Street	85926	1974	Electrical service	SD Community Concourse
202 C Street	97447	1974	Electrical service	SD Community Concourse
202 C Street	51885	1975	Electrical service	SD Community Concourse
202 C Street	57190	1975	Electrical service	SD Community Concourse
202 C Street	57953	1975	Electrical service	SD Community Concourse
202 C Street	70681	1976	Electrical service	SD Community Concourse
202 C Street	81419	1976	Electrical service	SD Community Concourse
202 C Street	11274	1976	Electrical service	SD Community Concourse
202 C Street	15280	1976	Electrical service	SD Community Concourse
202 C Street	15281	1976	Electrical service	SD Community Concourse
202 C Street	47840	1977	Electrical service	SD Community Concourse
202 C Street	61889	1979	Electrical service	SD Community Concourse
202 C Street	32044	1985	Electrical service	SD Community Concourse
Block D				
202 C Street	12394	1974	Plumbing service	City of San Diego
202 C Street	5943-87	1987	Electrical service	City of San Diego
202 C Street	1163-90	1990	Electrical service	City of San Diego
202 C Street	5622-90	1990	Electrical service	City of San Diego

The historical facilities listed above, which were interpreted to use or store hazardous materials or petroleum products and/or generate hazardous wastes, are discussed in the Historical Site Land Use section below.

### Environmental Liens

An Environmental Lien Search was requested from Nationwide Environmental Title Research (NETR).<sup>v</sup> NETR reported that no environmental liens and no other activity and use limitations (AULs) were found for the Site. A copy of the NETR report is included in the Appendix.

## INTERVIEWS

The above-referenced EPA and ASTM standards require that attempts be made to conduct interviews with past and present owners and occupants of the Site to obtain information indicating recognized environmental conditions in connection with the Site. As part of this Assessment, the following contacts were either interviewed or attempts were made to conduct interviews.

Contact	Affiliation to Site	Description
Ms. Mary Lou Houghton	City of San Diego Property Agent	Discussed below
Mr. Glen Simpson	City of San Diego Building Maintenance Supervisor	Discussed below
Mr. Erick Erickson	Assistant Facility Service Manager	Discussed below
Mr. Brad Morrison	Assistant Engineering Manager	Discussed below
Mr. Manny Mojica	Plant Engineer	Discussed below
Mr. Craig Fergusson	City of San Diego	Discussed below

Ms. Houghton stated that the City of San Diego redeveloped the Site in circa 1964 and that its use had not changed since its redevelopment.

Mr. Simpson stated that with the possible exception of diesel fuel for fire engine and emergency generator use (stored in an underground storage tank [UST]), cooling tower treatment chemicals, janitorial cleaners, and printing inks and cleaners, hazardous materials or petroleum products were not used or stored at the Site. Mr. Simpson stated that the printing shop located in Building C was installed approximately 4 to 5 year ago. Mr. Simpson stated that he had contacted SDG&E in connection with the possible presence of PCBs in the transformer fluid and that SDG&E reported that it had never analyzed the transformer fluid for PCBs

Mr. Erickson stated that cooling towers associated with the Site's heating, air conditioning and ventilation (HVAC) system were installed in circa 1964 and, with the exception of a currently unused boiler, have not changed since their installation.

Mr. Morrison stated that all cooling tower treatment chemicals are stored in a secure area of the Building C basement.

Mr. Mojica stated that all hydraulic elevators are service regularly and that addition of hydraulic fluid has not been required since their installation and that he was not aware of any hydraulic fluid release.

Mr. Fergusson was asked about the interpreted monitoring well located south of the current diesel UST. Mr. Fergusson stated that no monitoring wells were installed in connection with the current UST, reported removal of two USTs, and closure in place of another UST at the Site.

Information concerning previous Site owners and tenants was not provided by the Client. However SCS has compiled a list of historical Site tenants which are discussed in the Historical Site Land Use section below. Also, as noted in the Environmental Lien Section above, no environmental liens and no other activity and use limitations (AULs) were found for the Site. Based on the time from the Site redeveloped (early to mid 1960s), interviews with historical owners or tenants was not judged practicable and the information was not reasonably ascertainable.

## USER REQUIREMENTS

In order to qualify for one of the landowner liability protections offered by the Small Business Liability Relief and Brownfields Revitalization Act of 2001 (discussed in the Background section above), 40 CFR Part 312 requires that the user (Client) provide the following information to the environmental professional. The following table summarizes the responses by the Client.

Question	Response
Have environmental cleanup liens been filed or recorded against the Site?	No
Are activity or land use limitations in place at the Site or have they been filed or recorded in the registry?	No
Does the user have specialized knowledge or experience in connection with the Site?	No
Does the purchase price being paid for the Site reasonably reflect the fair market value of the Site? (please note that the Site is reportedly being redeveloped, not sold)	N/A
Is the Client aware of commonly known or reasonably ascertainable information about the Site, which would indicate releases or threatened releases?	No
Are there obvious indications that point to the presence of contamination at the Site?	No

## DATA GAPS IN CONNECTION WITH CURRENT SITE LAND USE

Based on observations and research, and with the possible exceptions below, there are no obvious indications of data gaps in connection with the current Site land use.

- The current Site owner is the City of San Diego. The names and contacts of previous Site owners/tenants were not provided and are not readily available. However SCS has compiled a list of historical Site tenants which are discussed in the Historical Site Land Use section below. Also, as noted in the Environmental Lien Section above, no environmental liens and no other activity and use limitations (AULs) were found for the Site.

Findings and Opinions—Current Site Land Use
Based on observations and research and with the possible exception below, there is a low likelihood that a recognized environmental condition exists at the Site as a result of the current Site land use.

### Findings and Opinions—Current Site Land Use

As noted in the DEH File Review section above, a diesel fuel release (e.g., disposal violation) from the UST adjacent to Building A (1222 1<sup>st</sup> Avenue) was reported. Based on the reported release in connection with the UST system, there is a potential that a recognized environmental condition may exist at the Site. In addition, SCS understands that the Site may be redeveloped. If this UST is removed in connection with redevelopment, there is a potential that diesel-bearing soil may be encountered during redevelopment. SCS recommends the collection of soil samples (to determine if a release has occurred). In addition, based on their interpreted location (adjacent to Building A), SCS recommends the collection of soil samples (to determine if a release has occurred) and soil vapor samples (to determine the potential for health risk due to possible vapor intrusion into Building A).

## TOPOGRAPHY, SOIL, GEOLOGY, HYDROGEOLOGY AND WATER QUALITY SURVEY

### Topography

A topographic map for the Site vicinity was reviewed and is summarized in the following table:

<b>Reported Elevation</b>	50 feet above mean sea level
<b>Reported Slope Direction</b>	Down to the southwest
<b>Source</b>	United States Geological Survey 7.5 Minute Topographic Map, Point Loma Quadrangle, California - San Diego County, 1977, photorevised 1982

### Soil Survey

A soil survey for the Site vicinity was reviewed and is summarized in the following table:

<b>Reported Soil Type</b>	Fill soil
<b>Reported Description</b>	Fill soil reportedly consisted of brown fine sand with trace of fine gravel and silt.
<b>Source</b>	Gradient Engineers, Inc. <i>Phase II Environmental Site Assessment, Department of General Services, Union and A Street Service Station, 1251 Union Street, San Diego, California, dated February 2005</i> (discussed in the Additional SCS Research section below)

### Geology

A geological map for the Site vicinity was reviewed and is summarized in the following table:

<b>Reported Formation</b>	Quaternary Bay Point Formation
<b>Reported Description</b>	The Bay Point Formation is described as being marine and non-marine, poorly consolidated fine- and medium-grained, pale brown, fossiliferous sandstone. The Bay Point Formation overlies the San Diego Formation, which is described as being yellowish-brown, fine- to medium-grained, poorly indurated sandstone. Cobble conglomerate, thin beds of bentonite, marl, and brown mudstone further characterize the formation.

<b>Source</b>	Geologic Map of Landslide Hazards in the Point Loma Quadrangle, San Diego County, California, by Siang S. Tan, 1992, Landslide Hazard Identification Map Number 28, Plate 28B, California Division of Mines and Geology
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## Hydrogeology

Data regarding groundwater depth and flow direction for the Site were not readily available. Based on a review of the DEH files for adjacent facilities (discussed in the Additional SCS Research section below), the reported depth to groundwater in the site vicinity is 47 to 49 feet below grade and the groundwater flow direction is reported to be southwest.

Please note that many variables influence groundwater depth and flow direction, and the actual depth and flow direction at the Site may be different than presented in this section.

## Water Quality Survey

The following table summarizes the reported water quality in the Site vicinity:

<b>Reported Hydrologic Subarea</b>	Lindberg (908.21)
<b>Reported Hydrologic Area</b>	San Diego Mesa (908.20)
<b>Reported Hydrologic Unit</b>	Pueblo San Diego (908.00)
<b>Reported Beneficial Use</b>	No beneficial use and exempt from municipal use
<b>Source</b>	RWQCB's "Comprehensive Water Quality Plan" (Plan) originally adopted in 1974, Amendments to the Plan, adopted in May 1998 by the RWQCB

## SITE VICINITY RECONNAISSANCE AND OFF-SITE SOURCE SURVEY

### Current Site Vicinity Conditions

The following table summarizes land use and observations in the immediate Site vicinity.<sup>vi</sup> For the purpose of this Report, the immediate Site vicinity includes those properties judged to be adjacent<sup>7</sup> to the Site.

<sup>7</sup> Adjacent is defined by ASTM E1527-05 as any real property the border of which is contiguous or partially contiguous with that of the Site or would be contiguous or partially contiguous with that of the Site but for a street, road, or other public thoroughfare separating them.

Direction	Land Use	Comments
North (Blocks A and B)	110 West A Street (Office Building) (commercial and financial offices) (Figure 3-o2) 101 Ash Street (Sempra Energy) (corporate offices) (Figure 3p-1)	No obvious indications of the use, storage, or generation of hazardous materials/wastes or petroleum products were observed; however, hazardous materials/wastes are judged likely to be present as discussed in the permits section below.
East (Blocks B, C, and D)	201 A Street (Cortez Academy) (3q-1) 1200 2 <sup>nd</sup> Avenue (Office Building) (commercial, government, and financial offices) (Figure 3q-2) Civic Theater (Figure 3p-2)	
South (Blocks A, C, and D)	1173 Front Street (San Diego County Sheriff) (Figure 3r-1) 1055 1 <sup>st</sup> Street (Bristol Court Hotel) (Figure 3r-2) 1010 2 <sup>nd</sup> Avenue (Executive Complex) (parking) 1055 2 <sup>nd</sup> Avenue (Westgate Hotel) San Diego Trolley Station (Figure 3s-1)	
Southwest (Block C)	100 West Broadway (Greyhound Bus Station)	
West (Blocks A and C)	Parking lot (County of San Diego Automotive Service Center/Department of General Services) (Figure 3s-2) Parking lot (San Diego County Court House) 110 West C Street (Chamber Building) (Figure 3r-1)	

### Environmental FirstSearch™ Site Assessment Report

A Site Assessment Report<sup>vii</sup> was prepared by the FirstSearch Technologies Corporation (FirstSearch) for the Site. Local, state, and federal regulatory databases were reviewed for the Site and for those facilities within up to 1 mile of the Site perimeter. The FirstSearch report was reported to have been prepared in general accordance with the ASTM standard for the regulatory database review for Phase I Environmental Site Assessments. The locations of the referenced facilities relative to the Site are shown on FirstSearch's "Map of Sites within One Mile," which is included in its report. A description of the various databases, as well as the date each database was most recently updated, is included in the FirstSearch report. The FirstSearch report is attached as an appendix to this Report.

Based on a review of the FirstSearch Report, the following table summarizes the facilities within the selected search circumferences, and whether the Site or a facility that was interpreted to be adjacent to the Site was listed on each database.

Federal or State Government Database	Search Radius	Number of Reported Facilities	On Site	Adjacent to the Site
National Priorities List (NPL)	1.00 mile	0	No	No
Comprehensive Environmental Response Compensation and Liability System (CERCLIS)	0.50 mile	0	No	No
No Further Remedial Action Planned (NFRAP)	0.50 mile	5	No	No
Resource Conservation and Recovery Act-Corrective	1.00 mile	1	No	No

Federal or State Government Database	Search Radius	Number of Reported Facilities	On Site	Adjacent to the Site
Action (RCRA COR ACT)				
RCRA Treatment and Disposal Facilities (RCRA TSD)	0.50 mile	0	No	No
RCRA Generators (RCRA GEN)	0.12 mile	12	No	Yes
RCRA no longer listed facilities (RCRA NLR)	0.12 mile	2	No	No
Federal Engineering and Institutional Controls (IC/EC)	0.25 mile	0	No	No
Emergency Response Notification System (ERNS)	0.12 mile	7	No	No
Tribal Lands	1.00 mile	0	No	No
State/Tribal Sites	1.00 mile	8	No	No
Spills-1990	0.12 mile	3	Yes	No
State/Tribal solid waste list (SWL)	0.50 mile	0	No	No
State/Tribal leaking underground storage tanks (LUST)	0.50 mile	218	Yes	No
State/Tribal underground/aboveground storage tanks (USTs/ASTs)	0.12 mile	34	Yes	No
State/Tribal deed-restriction site listing (EC/IC)	0.25 mile	0	No	No
State/Tribal voluntary cleanup program (VCP)	0.50 mile	0	No	No
State/Tribal Brownfields	0.50 mile	0	No	No
State Permits	0.12 mile	101	Yes	Yes
State Other	0.12 mile	1	No	No

The following sections discuss facilities identified on the regulatory database within the required search radii. The tables in the following sections summarize the following information: facility name, address, distance and direction from the Site, status, the likelihood that the facility has resulted in a recognized environmental condition at the Site, and the rationale for this judgment. The following table provides definitions for abbreviations found in the Rationale column in the tables below:

Abbreviation	Definition
S	Regulatory status (e.g., case closed)
CT	Case type (e.g., soil only)
D	Distance from Site
GWD	Reported depth to groundwater in the Site vicinity
GWF	Reported groundwater flow direction

### Known, Reported, or Suspected Releases within the Site Vicinity

#### No Further Remedial Action Planned (NFRAP)

Four facilities (one listed twice), within 0.5 mile of the Site, were identified on the NFRAP database. These facilities are summarized in the following table:

Facility (Distance and Direction from Site)	Status	REC	Rationale*
Super Plating, Inc. (listed twice) 106 West Market Street (0.34 mile southwest)	Archived site, no further remediation action planned	Low	S, D, GWF
AABCO Incorporated 808 West Cedar Street (0.39 mile northwest)	Archived site, no further remediation action planned	Low	S, D, GWF
Pacific Soap Company 301 West Market Street (0.39 mile southwest)	Archived site, no further remediation action planned	Low	S, D, GWF
Pacific Airmotive 544 7 <sup>th</sup> Avenue (0.44 mile southeast)	Archived site, no further remediation action planned, low priority	Low	S, D, GWF

**Notes:**

REC: Likelihood that the facility has resulted in a recognized environmental condition at the Site.

\*: Rationale abbreviation definitions are provided in the table at the beginning of this section above.

**RCRA COR**

One facility, within one mile of the Site, was identified on the RCRA COR database. This facility is summarized in the following table:

Facility (Distance and Direction from Site)	Status	REC	Rationale*
Solar Turbines Inc. 2200 Pacific Highway (0.71 mile northwest)	Stabilization measures implemented	Low	S, D, GWF

**Notes:**

REC: Likelihood that the facility has resulted in a recognized environmental condition at the Site.

\*: Rationale abbreviation definitions are provided in the table at the beginning of this section above.

**State Site**

Eight facilities, within one mile of the Site, were identified on the State Sites database. These facilities are summarized in the following table:

Facility (Distance and Direction from Site)	Status	REC	Rationale*
Pacific Soap Company 301 West Market Street (0.39 mile southwest)	Referred to another agency (caustic spills)	Low	S, D, GWF
Pacific Airmotive 544 7 <sup>th</sup> Avenue (0.44 mile southeast)	Preliminary assessment complete, no further action for DTSC	Low	S, D, GWF

Facility (Distance and Direction from Site)	Status	REC	Rationale*
San Diego High School 1405 Park Boulevard (0.51 mile northeast)	No action, for Calmortgage only, school site	Low	S, CT, D
San Diego Plating 2060 India Street (0.57 mile northwest)	Preliminary assessment complete, no further action for DTSC	Low	S, D, GWF
Allied Tank Cleaning Corporation 1883 East Harbor Drive (0.60 mile northwest)	Preliminary assessment complete, no further action for DTSC (stained soil)	Low	S, CT, D, GWF
Campbell Industries 501 East Harbor Drive (0.64 mile southwest)	Preliminary assessment complete, no further action for DTSC	Low	S, D, GWF
Solar Turbines Inc. 2200 Pacific Highway (0.71 mile northwest)	Stabilization measures implemented	Low	S, D, GWF
St Paul Villa 2340 Fourth Avenue (0.76 mile northeast)	No action, for Calmortgage only, residential care facility	Low	S, CT, D

**Notes:**

REC: Likelihood that the facility has resulted in a recognized environmental condition at the Site.

\*: Rationale abbreviation definitions are provided in the table at the beginning of this section above.

**ERNS**

Seven facilities, within 0.12 mile of the Site, were identified on the ERNS database. These facilities are summarized in the following table:

Facility (Distance and Direction from Site)	Status	REC	Rationale*
San Diego Trolley 1 <sup>st</sup> and C Street (adjacent [south] to Site)	Case closed, surface spill of 5 gallons of gasoline in auto-trolley accident	Low	S, CT, D, GWF
Greyhound 100 West B Street (adjacent [southwest] to Site)	Case closed, diesel spill on paved surface	Low	S, CT, D, GWF
SDG&E 300 C Street (0.01 mile northeast)	Case closed, solid waste (asbestos) release	Low	S, CT
1350 Front Street (0.03 mile northwest)	Release of raw sewage to storm drain	Low	S, CT, GWF

Facility (Distance and Direction from Site)	Status	REC	Rationale*
Dry Cleaner 910 Broadway Circle (0.08 mile southeast)	Case closed, spill on paved surface	Low	S, CT, GWF
Unknown Broadway Pier (0.08 mile southeast)	Case closed, spill on marine vessel	Low	S, CT, GWF
SDG&E Substation (interpreted not to be located within the search radius)	Not within search radius	NA	NA

**Notes:**

REC: Likelihood that the facility has resulted in a recognized environmental condition at the Site.

\*: Rationale abbreviation definitions are provided in the table at the beginning of this section above.

**Spills**

Three facilities, within 0.12 mile of the Site, were identified on the Spills database. These facilities are summarized in the following table:

Facility (Distance and Direction from Site)	Status	REC	Rationale*
San Diego City Fire Station 1222 1 <sup>st</sup> Street (Site)	Discussed in Additional SCS Research section below.		
Bank of America Plaza 450 B Street (0.09 mile northeast)	Closed in 1987	Low	S, D
County Courthouse 320 West Broadway (0.10 mile southwest)	PCB leak in 1950, remediation under VAP program	Low	S, CT, D, GWF

**Notes:**

REC: Likelihood that the facility has resulted in a recognized environmental condition at the Site.

\*: Rationale abbreviation definitions are provided in the table at the beginning of this section above.

**LUST and HE-17**

These databases list facilities where a release, usually from a UST, is known to have occurred. The following table summarizes the unauthorized release cases listed in the FirstSearch report. Please note that some of the facilities may be listed more than once because different databases sometimes use different names/addresses for the responsible party. Also, please note that a single facility can have multiple releases that result in separate cases. For the purposes of this Assessment, multiple cases for a single property are considered as a single release location with the worst case (e.g., an open groundwater case with free product) taking precedence. In addition, for some facilities information provided by the different databases is contradictory. In such instances, the information reported for the HE-17 LUST database will be used as the current status of the case. This judgment is based on the fact that the DEH is the lead agency for the vast

majority of LUST cases in San Diego County, and the HE-17 database is generally the most current database of the two databases reviewed by FirstSearch.

Based on the project performance by the Lawrence Livermore National Laboratory (LLNL) for distribution and remediation of methyl tertiary butyl ether (MTBE) in groundwater,<sup>viii</sup> on the average, approximately 94 percent of the MTBE plumes studied in detail (as measured by MTBE in a concentration of 20 micrograms per liter [ $\mu\text{g/L}$ ] did not extend more than approximately 400 feet from the source, while approximately 89 percent of the benzene plumes (as measured by benzene in a concentration of 1  $\mu\text{g/L}$ ) extended less than 400 feet.<sup>ix</sup> Therefore, the detailed review radius for open groundwater cases has been conservatively established by SCS at 0.20 mile (approximately 1,000 feet).

The following table summarizes the LUST cases identified on the regulatory database:

Number of Reported Facilities	218
Number of Reported Facilities Within 0.20 Mile	111
Actual Number of Facilities Within 0.20 Mile	37
Number of Facilities Within 0.20 Mile With Open LUST Cases	20
Number of Facilities Within 0.20 Mile With Open Groundwater LUST Cases	1

The DEH files for a facility with a reported LUST case (Greyhound Bus Station), which is located adjacent (southeast) to the Site, and a facility with a reported open groundwater LUST case (County of San Diego Downtown Service Station), which is located west to the Site were reviewed and are discussed in the Additional SCS Research section below. The remaining LUST cases do not meet the criteria for additional analysis (e.g. open groundwater LUST case within 0.20 mile of the Site), and no further evaluation of these LUST cases was conducted.

### **Permitted Hazardous Materials/Waste Facilities within the Site Vicinity**

While there are facilities within the Site vicinity that are reported to be on the UST/AST, RCRA GEN, or the Permits databases (hazardous materials users, hazardous waste generators, and violations), inclusion in these databases is only an indication of the use or storage of hazardous materials, or the generation of hazardous waste and related violations. Only those facilities that were judged to be immediately adjacent to the Site are discussed in this Report. Information regarding the other facilities can be found in the FirstSearch report in the Appendix.

### **USTs and ASTs**

Thirty-four facilities within 0.12 mile of the Site were reported to have or to historically have had USTs or ASTs. One facility (San Diego City Fire Station) (discussed in the Site Reconnaissance section above) was interpreted to be located at the Site and two facilities (County of San Diego General Services and The Executive Complex) (discussed in the Additional SCS Research section below) were interpreted to be located adjacent (south and southwest) to the Site. These facilities are discussed in the Permits section and/or Additional SCS Research section below. The remaining facilities were interpreted not to be located at or adjacent to the Site.

**Waste Generators (RCRA GEN)**

The RCRA GEN database list facilities that generate, store, and/or transport hazardous waste. A RCRA GEN facility is a facility that generates at least 100 kilograms (kg) per month of nonacutely hazardous waste. Fourteen facilities were listed on the RCRA GEN database. One of these facilities (US Navy American Marine Services) was interpreted to be located outside the search radius for this database. Two facilities (located at 110 West A Street and 101 Ash Street) were interpreted to be located adjacent to the Site and are discussed in the Permits section below. The remaining facilities were interpreted not to be located at or adjacent to the Site.

**Permits/Others Database**

One hundred and one facilities within 0.12 mile from the Site were reported by FirstSearch to be included in the Permits/Others database. These databases, maintained by the DEH and DTSC, are a listing of those facilities within the County that use, store, and/or generate hazardous materials/wastes. The database also includes those facilities that have had a known unauthorized release from a UST, which were evaluated in a prior section of this Report.

The regulatory files for facilities located at the Site were reviewed and discussed in the Site Reconnaissance section above. The regulatory files for the facilities located adjacent to the Site were reviewed and are discussed in the following table. The remaining facilities were interpreted not to be located at or adjacent to the Site.

Facility Address and File Number	Direction from Site	Hazardous Materials/Wastes	Violations
File Number: 207021 110 Plaza Street Inc. 110 West A Street	North	Sulfuric acid, Freon II, diesel fuel	Record keeping
File Number: 137804 Prism 110 West A Street	North	Sulfuric acid	Record keeping
File Number: 100568 AGL Investments, LP 110 West A Street	North	Sulfuric acid, Freon II, diesel fuel (AST)	Record keeping
File Number: 114851 Sempra Energy 101 Ash Street	North	Used batteries, acid and caustic waste, hydrocarbon/unspecified solvents, waste oil	Record keeping, incompatible waste not adequately separated
File Number: 110518 Downtown Chiropractic 110 West C Street	West	Photochemical waste	Record keeping
File Number: 150678 Larry Greenberg, DDS 110 West C Street	West	Infectious waste, sharps Infectious waste, general	Record keeping
File Number: 132345 County of San Diego General Services 1174 Front Street	West	Waste oil, inorganic solid waste	Record keeping, containers not kept closed or labels missing

Facility Address and File Number	Direction from Site	Hazardous Materials/Wastes	Violations
File Number: 126436 Westgate Hotel 1055 2 <sup>nd</sup> Avenue (also discussed in Additional SCS Research section below)	South	Compressed gases, caustic, boiler treatment chemicals, diesel fuel (ATS)	Record keeping, containers not properly labeled and not grounded
File Number: 134534 The Executive Complex 1010 2 <sup>nd</sup> Avenue (also discussed in Additional SCS Research section below)	South	Waste oil and mixed oil, asbestos containing waste	Record keeping, containers not properly labeled, facility has an abandoned UST
File Number: 126606 Greyhound Bus Station 120 Broadway (also discussed in Additional SCS Research section below)	Southwest	Waste oil, oil-water sludge, used oil filters, batteries, hydrocarbon solvents	Record keeping

With the possible exception of the Westgate Hotel, Executive Complex, and Greyhound Bus Station (discussed in the Additional SCS Research section below), based on the absence of reported disposal violations and/or releases and SCS's experience, there is a low likelihood that a recognized environmental condition exists at the Site as a result of the listing of these facilities on the Permits database.

### Additional SCS Research

#### Adjacent LUST Case

DEH File Number: 126606      Greyhound Station - 120 West Broadway (located adjacent [southwest] to the Site [Block C])

Two diesel USTs were reported closed in place at the facility in the late 1960s and were removed in 1989 (ERC Environmental and Energy Services Company [ERCE], *Closure Report for the Greyhound Bus Terminal, 120 West Broadway, San Diego, California*, dated December 1991). Soil samples were reportedly collected from the vicinity of the former UST and ERCE estimated that approximately 800 cubic yards of petroleum hydrocarbon-bearing soil was reportedly present at the facility. The facility was subsequently remediated and soil with a concentration of greater than 1,000 mg/kg were reportedly removed and transported off site for disposal. Soil borings to a depth of 20 feet below grade were reportedly drilled and no groundwater was reportedly encountered. A closure letter was issued by the DEH on May 26, 1992.

Based on the reported case status (closed), type of release (soil), material released (diesel), distance of the release from the Site (approximately 300 feet), location with respect to the reported groundwater flow direction (downgradient), and SCS's experience, there is a low likelihood that a recognized environment condition exists at the Site as a result of this known and reported release.

### Adjacent USTs

DEH File Number: 126436 Westgate Hotel - 1055 2<sup>nd</sup> Avenue (located adjacent [south] to the Site [Block C])

A 5,000-gallon gasoline AST within a vault structure was reportedly closed in place and filled with concrete in 1987 (Santa Fe Soils, Inc. [SF Soils], *Westgate Hotel, 1055 2<sup>nd</sup> Street, San Diego California*, dated December 1987). Fuel hydrocarbons were reportedly not detected above the laboratory detection limit (ND) in soil samples collected adjacent to the AST. No groundwater was reportedly encountered at the maximum drilling depth (7 feet below grade). A no further action letter was issued by the DEH in January 1988.

Based on the reported case status (no further action), distance of the AST from the Site (approximately 130 feet), reported absence of constituents of concern (CoCs) in soil samples collected from the UST excavation, the location of the USTs with respect to the reported groundwater flow direction (downgradient), and SCS's experience, there is a low likelihood that a recognized environment condition exists at the Site as a result of the closed-in-place UST at this facility.

DEH File Number: 134534 The Executive Complex - 1010 2<sup>nd</sup> Avenue (located adjacent [south] to the Site [Block C])

A 1,000-gallon gasoline and a 550-gallon diesel UST were reportedly closed in place at the facility in January 1999 (Pacific Southwest Group, *Underground Storage Abandonment Report, 1010 2<sup>nd</sup> Avenue, San Diego, California*, dated January 1999). Prior to abandonment, two soil samples were reportedly collected from the vicinity of the USTs. No CoCs were reportedly present above the laboratory detection limit in the collected samples. A "Tank Closure Complete-No Further Action Required" report was issued by the DEH in February 1999.

Based on the reported case status (no further action), distance of the USTs from the Site (approximately 80 feet), reported absence of CoCs in soil samples collected from the vicinity of the USTs, the location of the USTs with respect to the reported groundwater flow direction (downgradient), and SCS's experience, there is a low likelihood that a recognized environment condition exists at the Site as a result of the known and reported closed-in-place USTs at this facility.

DEH File Number: 132345 County of San Diego/General Services - 1174 Front Street (located adjacent [southwest] to the Site [Block A])

A 10,000-gallon UST was reportedly removed from the facility in 1994. Total petroleum hydrocarbons (TPH) concentrations in soil samples collected from the former UST excavation was reported to be not detected above the laboratory detection limit (ND). A no further action letter was reportedly issued by the DEH in February 1995.

One 100-gallon UST and two buried 55-gallon drums were reportedly removed from the facility in April 1995. Soil samples were reportedly collected from the former UST excavation and analyzed for TPH and total recoverable petroleum hydrocarbons (TRPH). However, the

analytical results were not included in the DEH file for the facility. A no further action letter was reportedly issued by the DEH in May 1995.

Based on the reported case status (no further action), distance of the release from the Site (approximately 130 feet), reported or inferred absence of CoCs in soil samples collected from the UST excavations, and SCS's experience, there is a low likelihood that a recognized environment condition exists at the Site as a result of the known and reported removal of USTs from this facility.

### **Open LUST Case**

DEH File Number: 114741     County of San Diego Downtown Service Station - 1251 Union Street (located 0.05 mile west of the Site)

Two 6,000-gallon gasoline USTs and one 300-gallon waste oil UST were reportedly removed from the Site in 1998 (Burns and McDonnell Waste Consultants, Inc. [BMWCI], *UST Closure Report, San Diego County Downtown Service Station, 1251 Union Street, San Diego, California*, dated November 1998). BMWCI reported that petroleum hydrocarbon-bearing soil was observed to a depth of 19.5 feet below grade in the gasoline UST excavation. Petroleum hydrocarbon-bearing soil was also reportedly observed to a depth of 10 feet below grade in the waste oil UST excavation. A total of 225 cubic yards of soil were reportedly removed from the former UST excavation and disposed off-site. BMWCI recommended a site assessment to determine the lateral and vertical extent of the petroleum hydrocarbon-bearing soil.

A Phase II Environmental Site Assessment was reportedly conducted by Gradient Engineers, Inc. (GEI) (GEI, *Phase II Environmental Site Assessment, Department of General Services, Union and A Street Service Station, 1251 Union Street, San Diego, California*, dated February 2005). Three monitoring wells were reportedly drilled at the facility. GEI reported that CoCs were not present in groundwater samples collected from the monitoring wells. Depth to groundwater was reported to be 47 to 49 feet below grade and the groundwater flow direction was reported to be to the southwest.

At the request of the DEH, a work plan was prepared by Ninyo & Moore (Ninyo & Moore, *Work Plan for County of San Diego Department of General Services, 1251 Union Street, San Diego, California*, dated July 2007) to evaluate groundwater flow direction, gradient, and quality by installing a additional groundwater monitoring well downgradient of the former gasoline UST. An additional objective was to delineate the vertical extent of the CoCs in the soil. No additional information regarding the proposed work plan was included in the file.

Based on the distance of the release from the Site (approximately 200 feet), reported absence of CoCs in groundwater samples collected from the vicinity of the former USTs, the location with respect to the reported groundwater flow direction (downgradient), and SCS's experience, there is a low likelihood that a recognized environment condition exists at the Site as a result of the known and reported release.

### California Division of Oil and Gas

SCS personnel reviewed the California Division of Oil and Gas (DOG) Map regarding oil and gas well locations within 1 mile of the Site.<sup>x</sup> There were no wells interpreted to be located within a 1-mile radius of the Site.

### DATA GAPS IN CONNECTION WITH OFF-SITE SOURCES

Based on the Site vicinity reconnaissance and off-Site source survey, there are no obvious indications of data gaps in connection with off-Site sources.

#### Findings and Opinions—Off-Site Source Survey

Based on the off-Site source survey, several facilities in the Site vicinity were reported to have had releases of hazardous materials/waste or petroleum products. However, there is a low likelihood that a recognized environmental condition exists at the Site as a result of known and reported releases of hazardous materials/wastes or petroleum products from an off-Site source. This judgment is based on one or more of the following: reported regulatory status (e.g., case closed), media affected (e.g., soil contamination only), distance from the Site, reported depth to groundwater, direction from the Site with respect to reported groundwater flow direction, and information obtained through a review of County of San Diego Department of Environmental Health files.

### HISTORICAL LAND USE REVIEW

In accordance with the ASTM Standard and AAI rule, numerous reasonably ascertainable standard historical information sources were reviewed, and an attempt was made to interpret the historical Site and Site vicinity land use back to the obvious first developed use of the Site. The following table summarizes the historical resources reviewed as part of this Assessment:

Resource	Location	Years Available
Aerial Photographs	FirstSearch, San Diego County Department of Cartographic Services, NETR Online <a href="http://www.historicaerials.com">http://www.historicaerials.com</a>	1928, 1948, 1953, 1963, 1966, 1970, 1974, 1978, 1980, 1981, 1983, 1989, 1990-1991, 1995, 1999, 2002, 2003, 2005
City Directories	San Diego Central Library	1926, 1930, 1936, 1940, 1945, 1950, 1955, 1960, 1965, 1970, 1975, 1980, 1985, 1999, 2005, 2009
Sanborn Fire Insurance Maps	In-House Collection	1887, 1888, 1906, 1921, 1950, 1956, 1957, 1959, 1962, 1963, 1965, 1970, 1971
Topographic Maps	In-House Collection	1967 and 1942
Building Department Records	SDBD	1954 to 1990
Interviews	Not applicable	Please see Interview section above

## Historical Site Land Use

The following table provides a chronology of the apparent historical Site land uses as interpreted from a review of information from the sources referenced:

### Historical Aerial Photographs

Historical aerial photographs for the Site were reviewed and are summarized below.

<b>Block A</b>	
1928	The north portion of Block A was observed to be occupied by two buildings. The south portion of the Block was observed to be occupied by three buildings. The center portion of the block was observed to be occupied by three smaller buildings.
1948-1953	The east portion of Block A was observed to be occupied by four buildings. The west portion of Block A was observed to be occupied by two buildings.
1963-1966	The north portion of Block A was observed to be unoccupied and interpreted to be used as a parking lot. The remainder of the block was observed to be occupied by a multi-story building (City Operations Building).
1970-1978	The north portion of Block A was observed to be unoccupied and interpreted to be used as a parking lot. The remainder of the block was observed to be occupied by a multi-story building (City Operations Building).
1980-1983	The north portion of Block A was observed to be unoccupied and interpreted to be used as a parking lot. The remainder of the block was observed to be occupied by a multi-story building (City Operations Building).
1989-1995	The north portion of Block A was observed to be unoccupied and interpreted to be used as a parking lot. The remainder of the block was observed to be occupied by a multi-story building (City Operations Building).
1999-2005	The north portion of Block A was observed to be unoccupied and interpreted to be used as a parking lot. The remainder of the block was observed to be occupied by a multi-story building (City Operations Building).
<b>Block B</b>	
1928	Buildings were observed at the northwest corner, the southern portion of the block and the east central portion of the Block, the northeast corner and the west central portion of the block was observed to be vacant.
1948-1953	The east portion of Block B was observed to be occupied by three buildings. The northeast portion of the Block (1 <sup>st</sup> Avenue and West A Street) was observed to be occupied by two buildings. The southeast portion of the Block was observed to be occupied by a building.
1963-1966	Block B was observed to be occupied by a single structure (Parking Garage).
1970-1978	Block B was observed to be occupied by a single structure (Parking Garage).
1980-1983	Block B was observed to be occupied by a single structure (Parking Garage).
1989-1995	Block B was observed to be occupied by a single structure (Parking Garage).

1999-2005	Block B was observed to be occupied by a single structure (Parking Garage).
<b>Block C</b>	
1928	Block C was observed to be occupied by four of what appeared to be warehouse type buildings extending the width of the Block.
1948-1953	Block C was observed to be occupied by one building (extending the width of the Block) adjacent to B Street and two buildings (extending the width of the block) adjacent to C Street). Three buildings were observed at the center of the Block (adjacent to 1 <sup>st</sup> Avenue).
1963-1966	The west portion of Block C was observed to be occupied by a single structure (Convention Center). The remainder of Block C was observed to an open area (pedestrian walkway and plaza).
1970-1978	The west portion of Block C was observed to be occupied by a single structure (Convention Center). The remainder of Block C was observed to an open area (pedestrian walkway and plaza).
1980-1983	The west portion of Block C was observed to be occupied by a single structure (Convention Center). The remainder of Block C was observed to an open area (pedestrian walkway and plaza).
1989-1995	The west portion of Block C was observed to be occupied by a single structure (Convention Center). The remainder of Block C was observed to an open area (pedestrian walkway and plaza).
1999-2005	The west portion of Block C was observed to be occupied by a single structure (Convention Center). The remainder of Block C was observed to an open area (pedestrian walkway and plaza).
<b>Block D</b>	
1928	Block D was observed to be occupied by a vacant lot (east) and one building (west).
1948-1953	The west portion of Block D was observed to be occupied by one building. The remainder of Block D was observed to be vacant.
1963-1966	Block D was observed to be occupied by a single structure (City Administration Building).
1970-1978	Block D was observed to be occupied by a single structure (City Administration Building).
1980-1983	Block D was observed to be occupied by a single structure (City Administration Building).
1989-1995	Block D was observed to be occupied by a single structure (City Administration Building).
1999-2005	Block D was observed to be occupied by a single structure (City Administration Building).

### Historical Sanborn Fire Insurance Maps

Historical Sanborn fire insurance maps for the Site were reviewed and are summarized below. Site tenants interpreted to use or store hazardous materials or petroleum products and/or generate hazardous wastes are indicated in bold print.

<b>Block A</b>	
1887	The northern portion of Block A was reportedly occupied by a vacant lot (at Front Street and West A Street) and two SFRs and a vacant lot (adjacent to 1 <sup>st</sup> Avenue). The southern portion of Block A was reportedly occupied by SFRs (with the exception of a vacant lot adjacent to West B Street and 1 <sup>st</sup> Avenue). A windmill was reportedly located near the east perimeter of the lot at Front Street and West B Street.
1888	The northern portion of Block A was reportedly occupied by a vacant lot (at Front Street and West A Street) and three SFRs and a boarding house (at 1 <sup>st</sup> Street and West B Street). The southern portion of Block A was reportedly occupied by SFRs and a duplex (with the exception of a vacant lot adjacent to West B Street and 1 <sup>st</sup> Avenue).
1906	Block A was reportedly occupied by SFRs with the exception of vacant lots at Front Street and West A Street and 1 <sup>st</sup> Avenue and West B Street), and a duplex to the east of the SFR at Front Street and West A Street.
1921	Block A was reportedly occupied by a <b>garage (Front &amp; A)</b> at Front Street and West A Street, a <b>garage and automotive sales and service</b> (Powell Motor Car Company) at Front Street and West B Street, store buildings at 1 <sup>st</sup> Avenue and West A Street and a store and restaurant adjacent to the Powell Motor Car Company on West B Street. The remainder of Block A was reportedly occupied by SFRs and MFRs.
1950	The east portion of Block A was reportedly occupied by an <b>auto repair facility, an auto service facility, and an auto parts facility</b> . The northeast portion of Block A (1 <sup>st</sup> Avenue and West A Street) was reportedly occupied by an electrical supply house and a laundry. The southeast portion of Block A was reportedly occupied by stores (vending and glass shops) and a <b>printer</b> .
1956	The east portion of Block A was reportedly occupied by an <b>auto repair facility, an auto service facility, and an auto parts facility</b> . The northeast portion of the Block (1 <sup>st</sup> Avenue and West A Street) was reportedly occupied by six stores and a MFR, the east central portion of Block A was reportedly occupied by an electrical supply house (1242 1 <sup>st</sup> Avenue). The southeast portion of the Block was reportedly occupied by stores (vending and glass shops) and a <b>printer</b> .
1957	The east portion of Block A was reportedly occupied by an <b>auto repair facility, an auto service facility, and an auto parts facility</b> . The northeast portion of the Block (1 <sup>st</sup> Avenue and West A Street) was reportedly occupied by six stores and a MFR, the east central portion of Block A was reportedly occupied by an electrical supply house (1242 1 <sup>st</sup> Avenue). The southeast portion of the Block was reportedly occupied by stores, including a glass shop.
1959	The east portion of Block A was reportedly occupied by <b>two auto repair facilities</b> (adjacent to West A Street and West B Street respectively), <b>an auto service facility, and a private garage</b> . The northeast portion of the Block (1 <sup>st</sup> Avenue and West A Street) was reportedly occupied by six stores and a MFR, the east central portion of Block A was reportedly occupied by an electrical supply house (1242 1 <sup>st</sup> Avenue), and the southeast portion of the Block was reportedly occupied by stores, including a glass shop.
1962- 1963	The east portion of Block A was reportedly occupied by <b>two auto repair facilities</b> (adjacent to West A Street and West B Street respectively), <b>an auto service facility, and a private garage</b> . The northeast portion of the Block (1 <sup>st</sup> Avenue and West A Street) was reportedly occupied by a parking lot, the east central portion of Block A was reportedly occupied by an electrical supply house (1242 1 <sup>st</sup> Avenue) and the southeast portion of the Block was reportedly occupied by stores, including a glass shop.

1965	The east portion of Block A was reportedly occupied by <b>two auto repair facilities</b> (adjacent to West A Street and West B Street respectively), <b>an auto service facility, and a private garage</b> . The northeast portion of the Block (1 <sup>st</sup> Avenue and West A Street) was reportedly occupied by a parking lot, the east central portion of Block A was reportedly occupied by an electrical supply house (1242 1 <sup>st</sup> Avenue) and the southeast portion of the Block was reportedly occupied by stores, including a glass shop.
1970	The north portion of Block A was reportedly used for parking. The central and southern portion of Block A was reportedly occupied by the City Operations Building.
1971	The north portion of Block A was reportedly used for parking. The central and southern portion of Block A was reportedly occupied by the City Operations Building.
<b>Block B</b>	
1887	The western portion of Block B was reportedly occupied (from north to south) by a SFR, a billiard parlor, a lot with a shed, and windmill. The eastern portion of Block B was reported to be vacant with the exception of a windmill adjacent to B Street.
1888	The western portion of Block B was reportedly occupied (from north to south) by a SFR, a billiard parlor, and a lot with a shed. The eastern portion of Block B was reported to be vacant with the exception of a windmill adjacent to B Street.
1906	Block B was reportedly occupied by SFRs/lodging houses with the exception of store at 1 <sup>st</sup> Avenue and B Street.
1921	With the possible exception of a drug store (102 B Street) and stores at 1 <sup>st</sup> Avenue and B Street, a plumbing shop at 1228 2 <sup>nd</sup> Avenue, and a hotel (Harvard) at 2 <sup>nd</sup> Avenue and B street), Block B was reportedly occupied by SFRs, lodging houses, and apartments.
1950	With the possible exception of a drug store (102 B Street) and stores/restaurant at 1 <sup>st</sup> Avenue and B Street, <b>automotive sales</b> at 1235 1 <sup>st</sup> Avenue, a <b>gasoline service station</b> at 2 <sup>nd</sup> Avenue and A Street, a restaurant at 1244 2 <sup>nd</sup> Avenue, a store (1228 2 <sup>nd</sup> Avenue), a <b>tire shop</b> at 1220 2 <sup>nd</sup> Avenue, Block B was reportedly occupied by a lodging house and apartments.
1956	The north portion of Block B was reportedly occupied by parking and a <b>gasoline and oil dispenser</b> (139 A Street). The central portion of the Block was reportedly occupied by parking (adjacent to 1 <sup>st</sup> Street) and a store and restaurant (adjacent to 2 <sup>nd</sup> Street). A lodging house (1219 1 <sup>st</sup> Street), a drug store (102 B Street), and stores/restaurant were reportedly located at 1 <sup>st</sup> Avenue and B Street. A <b>tire store</b> (1220 2 <sup>nd</sup> Avenue) and the Harvard Hotel were reportedly located at 2 <sup>nd</sup> Avenue and B Street).
1957	The north and central portion of Block B was reportedly occupied by parking, a <b>gasoline and oil dispenser</b> (139 A Street), and two stores (1244 and 1228 2 <sup>nd</sup> Avenue). The central portion of the Block was reportedly occupied by parking (adjacent to 1 <sup>st</sup> Street) and a store and restaurant (adjacent to 2 <sup>nd</sup> Street). A lodging house (1219 1 <sup>st</sup> Street), a drug store (102 B Street), and stores/restaurant were reportedly located at 1 <sup>st</sup> Avenue and B Street. An <b>auto parts store</b> (1220 2 <sup>nd</sup> Avenue) and the Harvard Hotel were reportedly located at 2 <sup>nd</sup> Avenue and B Street).
1959, 1962	The north and central portion of Block B was reportedly occupied by parking, a <b>gasoline and oil dispenser</b> (139 A Street), and two stores (1244 and 1228 2 <sup>nd</sup> Avenue). The central portion of the Block was reportedly occupied by parking (adjacent to 1 <sup>st</sup> Street) and a store and restaurant (adjacent to 2 <sup>nd</sup> Street). A lodging house (1219 1 <sup>st</sup> Street), a drug store (102 B Street), and stores/restaurant were reportedly located at 1 <sup>st</sup> Avenue and B Street. An <b>auto parts store</b> (1220 2 <sup>nd</sup> Avenue) and the Harvard Hotel were reportedly located at 2 <sup>nd</sup> Avenue and B Street).
1963	Block B was reportedly vacant.

1965, 1970, and 1971	Block B was reportedly occupied by an open deck garage.
<b>Block C</b>	
1887	SFRs reportedly occupied the north and south portions of Block C with the center portion of Block C reportedly vacant. A windmill was reportedly located at the western perimeter of the northeast lot of Block C, near the southwest corner of the northwest lot of Block C, and in the southwest lot of Block C.
1888	Block C was reported to be unchanged from 1887 except that a duplex and SFR was reportedly located in the west-center lot.
1906	Block C was reported to be occupied by SFRs and MFRs.
1921	Block C was reported to be occupied by three <b>automotive sales stores, a tire store, and a battery store</b> (adjacent to C Street), <b>Smith Savoy Garage</b> (1117 1 <sup>st</sup> Avenue), <b>Sherman's Garage</b> (1125 1 <sup>st</sup> Avenue), <b>auto supply store</b> (1128 2 <sup>nd</sup> Avenue), <b>auto sales and garage</b> (1141 1 <sup>st</sup> Avenue), <b>Motor Inn Garage</b> (1140 2 <sup>nd</sup> Avenue), a duplex (1149 to 1151 1 <sup>st</sup> Avenue), <b>battery charging facility</b> (1156 to 1160 2 <sup>nd</sup> Avenue), <b>auto supply store</b> (1128 2 <sup>nd</sup> Avenue), <b>John Moynihan Garage</b> and <b>Chessman &amp; Company Garage</b> (adjacent to B Street).
1950	Block C was reported to be occupied by a <b>paint store</b> and <b>auto installation store</b> (adjacent to C Street), automotive parking garage, <b>auto rental and service</b> (1140 2 <sup>nd</sup> Avenue), storage (1143 1 <sup>st</sup> Avenue), and a <b>paint store</b> (adjacent to B Street).
1956	Block C was reported to be occupied by a <b>paint store</b> and <b>auto equipment installation store</b> (adjacent to C Street), automotive parking garage (1116 2 <sup>nd</sup> Avenue), <b>a paint store and a garage (with gas and oil)</b> (adjacent to B Street).
1957	The southern portion of Block C was reported to be occupied by a <b>paint store</b> and <b>auto installation store</b> (adjacent to C Street) and an automotive parking garage (1116 2 <sup>nd</sup> Avenue). The central portion of Block C was reportedly to be occupied by two stores (adjacent to 1 <sup>st</sup> Avenue) and a parking lot (adjacent to 2 <sup>nd</sup> Avenue). The northern portion of Block C was reportedly occupied by a <b>garage and automotive sales and repair facility</b> (adjacent to B Street) and two stores.
1959, 1962	The southern portion of Block C was reported to be occupied by a <b>paint store</b> and <b>auto installation store</b> (adjacent to C Street) and an automotive parking garage (1116 2 <sup>nd</sup> Avenue). The central portion of Block C was reportedly to be occupied by two stores (adjacent to 1 <sup>st</sup> Avenue) and a parking lot (adjacent to 2 <sup>nd</sup> Avenue). The northern portion of Block C was reportedly occupied by a <b>garage</b> and <b>automotive sales and repair facility</b> (adjacent to B Street) and two stores.
1963	Block C reported to be vacant.
1965, 1970, and 1971	Block C reported to be occupied by an auditorium, and loading docks.
<b>Block D</b>	
1887	Block D was reportedly occupied by two SFRs.
1888	Block D was reportedly occupied by three SFRs.
1906	Block D was reported to be occupied by three SFRs.

1921	Block D was reportedly occupied by an <b>auto sales room</b> , restaurant, store and <b>hand printing shop</b> (2 <sup>nd</sup> Avenue and C Street), and <b>Earl Anthony Garage</b> (1121 2 <sup>nd</sup> Avenue).
1950	Block D was reported to be occupied by a <b>paint store</b> (adjacent to C Street), <b>auto parts, installation and oil storage</b> (1121 2 <sup>nd</sup> Avenue).
1956	Block D was reported to be occupied by a warehouse.
1957	Block D was reported to be occupied by a warehouse.
1959	Block D was reported to be occupied by a warehouse.
1963	Block D reported to be vacant.
1965, 1970, and 1971	Block D reported to be occupied by the city administration building.

### Historical City Directory Review

Historical city directories were reviewed at the San Diego Central Library for the current and historically adjacent property addresses. Please note that prior to the early 1930s 1<sup>st</sup>, 2<sup>nd</sup>, and 3<sup>rd</sup>, Avenue were described as streets. However for consistency “Avenue” is used in this Report. Also, street addresses west of 1<sup>st</sup> Avenue are designated as “West” and street addressed east of 1<sup>st</sup> Avenue have no prefix (i.e., “east” A Street is designated A Street). Facilities interpreted to have likely store and used hazardous materials or petroleum products, had USTs, and/or generated hazardous waste are noted in bold text.

Year	Description
	<b>Block A</b> 1201, 1207, 1217, 1225, 1229, 1231, 1239, 1243, 1249, 1251, 1253, 1251, 1255, 1271 Front 1218, 1224, 1228, 1230, 1234, 1238, 1240, 1242, 1246, 1254, 1256, 1258, 1260, 1262, 1264 1 <sup>st</sup> Street/Avenue 101, 115, 117, 119, 149 West A Street 100, 102, 106, 108, 110, 112, 118, 120, 124, 130, 140, 148, 918, 934 West B Street
1926	<b>Lacy &amp; Sartori</b> (service station) (1207 Front Street), Dominick (shoe repair) (1239 Front Street), Thomas (grocery) (1271 Front Street)  Taylor (grocery) (1218 1 <sup>st</sup> Avenue), Curtis (paint) (1224 1 <sup>st</sup> Avenue), S&S Furniture (1240 1 <sup>st</sup> Avenue), Havens (locksmith) (1254 1 <sup>st</sup> Avenue), Los Angeles Times, (newspaper distributor) (1256 1 <sup>st</sup> Avenue), Turnell Tile Company (1258 1 <sup>st</sup> Avenue), <b>Model Dye Works</b> (cleaners) (1260 1 <sup>st</sup> Avenue), Hotel Wayne (1262 1 <sup>st</sup> Avenue), Willis (grocery) (1264 1 <sup>st</sup> Avenue)  <b>Robins Rubber Company</b> (102 West B Street), New Western Hotel (106 West B Street), Dickson (restaurant) (110 West B Street), Stevenson (barber) (112 West B Street), <b>Reuther Automotive Equipment Company</b> (120 West B Street), Automotive Glass Company (130 West B Street), <b>Spinning</b> (auto) (140 West B Street)

Year	Description
1930	<p><b>Auburn-San Diego</b> (repair department) (1207 Front Street), Dominick (shoe repair) (1239 Front Street), Thomas (grocery) (1271 Front Street)</p> <p>Taylor (grocery) (1218 1<sup>st</sup> Avenue), Pohl (radios) (1222 1<sup>st</sup> Avenue), Ryan (tailor) (1224 1<sup>st</sup> Avenue), Richards (signs) (1230 1<sup>st</sup> Avenue), Havens (locksmith) (1254 1<sup>st</sup> Avenue), Harris (restaurant) (1256 1<sup>st</sup> Avenue), Turnell Tile Company (1258 1<sup>st</sup> Avenue), <b>Model Dye Works</b> (cleaners) (1260 1<sup>st</sup> Avenue), Hotel Wayne (1262 1<sup>st</sup> Avenue), Willis (grocery) (1264 1<sup>st</sup> Avenue)</p> <p>Davis (auto loans) (102 West B Street), New Western Hotel (106 West B Street), Stevenson (barber) (108 West B Street), Dickson (restaurant) (110 West B Street), Broach (auto glass) (130 West B Street), <b>Auburn-San Diego</b> (autos) (140 West B Street)</p>
1936	<p>Dominick (shoe repair) (1239 Front Street), <b>Darby</b> (auto repair) (1271 Front Street)</p> <p>Skuria (grocery) (1218 1<sup>st</sup> Avenue), Pohl (radios) (1222 1<sup>st</sup> Avenue), House (shoe repair) (1224 1<sup>st</sup> Avenue), Richards (signs) (1230 1<sup>st</sup> Avenue), Harris (restaurant) (1234 1<sup>st</sup> Avenue) Havens (locksmith) (1254 1<sup>st</sup> Avenue), Holt (liquors) (1256 1<sup>st</sup> Avenue), Hubbs (plumber) (1258 1<sup>st</sup> Avenue), <b>McFarquar</b> (cleaners) (1260 1<sup>st</sup> Avenue), Hotel Wayne (1262 1<sup>st</sup> Avenue), Willis (grocery) (1264 1<sup>st</sup> Avenue)</p> <p>Citizen's Thrift (loans) (100 West B Street), New Western Hotel (106 West B Street), <b>Birdsall</b> (printer) (108 West B Street), Beldeman (restaurant) (110 West B Street), Stevenson (barber) (112 West B Street), <b>Auburn-Cord</b> Sales and Service (140 West B Street)</p>
1940	<p><b>Thompson</b> (auto repair) (1271 Front Street)</p> <p>Skuria (grocery) (1218 1<sup>st</sup> Avenue), House (shoe repair) (1224 1<sup>st</sup> Avenue), Johnson (hand laundry) (1230 1<sup>st</sup> Avenue), Chips Place (restaurant) (1234 1<sup>st</sup> Avenue) Havens (locksmith) (1254 1<sup>st</sup> Avenue), Woodward (liquors) (1256 1<sup>st</sup> Avenue), Hubbs (plumber) (1258 1<sup>st</sup> Avenue), <b>McFarquar</b> (cleaners) (1260 1<sup>st</sup> Avenue), Hotel Wayne (1262 1<sup>st</sup> Avenue), Walls (grocery) (1264 1<sup>st</sup> Avenue)</p> <p>Citizen's Thrift (loans) (100 West B Street), New Western Hotel (106 West B Street), Campbell (tourist agent) (108 West B Street), Beldeman (restaurant) (110 West B Street), Stevenson (barber) (112 West B Street), Broach (auto glass) (130 West B Street), <b>Auburn-Cord</b> Sales and Service (140 West B Street)</p>
1945	<p>Kellar (real estate) (1202 1<sup>st</sup> Avenue), Pacific Watchmaker (1222 1<sup>st</sup> Avenue), House (shoe repair) (1224 1<sup>st</sup> Avenue), Lacy (hand laundry) (1230 1<sup>st</sup> Avenue), Hoffman (liquors) (1234 1<sup>st</sup> Avenue), Havens (locksmith) (1254 1<sup>st</sup> Avenue), Seven Eleven Liquor Store (1256 1<sup>st</sup> Avenue), Hubbs (plumber) (1258 1<sup>st</sup> Avenue), <b>McFarquar</b> (cleaners) (1260 1<sup>st</sup> Avenue), Hotel Wayne (1262 1<sup>st</sup> Avenue), Bowen (grocery) (1264 1<sup>st</sup> Avenue)</p> <p>Brownback (antiques) (119 West A Street)</p> <p>New Western Hotel (106 West B Street), Auto Electric Shop (110 West B Street), Sellers (barber) (112 West B Street), California Plate &amp; Glass (120 West B Street)</p>

Year	Description
1950	<p>Ondo (card room) (1202 1<sup>st</sup> Avenue), Pacific Watchmaker (1222 1<sup>st</sup> Avenue), House (shoe repair) (1224 1<sup>st</sup> Avenue), Yip Shee (hand laundry) (1230 1<sup>st</sup> Avenue), Valden (liquors) (1234 1<sup>st</sup> Avenue), Zwinder Electric (1242 1<sup>st</sup> Avenue), Stewart (delicatessen) (1256 1<sup>st</sup> Avenue), Castell (restaurant) (1260 1<sup>st</sup> Avenue), M&amp;M Transfer (1262 1<sup>st</sup> Avenue), Matheson (liquors) (1264 1<sup>st</sup> Avenue)</p> <p><b>Tufford Autos</b> (At Front)</p> <p>Owl Electric (117 West A Street), <b>McFarquar</b> (cleaners) (119 West A Street)</p> <p><b>American Cleaners</b> (100 West B Street), New Western Hotel (106 West B Street), Mc Evoy (general contractor) (108 West B Street)</p>
1955	<p>Club Twelve (card room) (1202 1<sup>st</sup> Avenue), Stanford (manufacture's agent) (1218 1<sup>st</sup> Avenue), Pacific Watchmaker (1222 1<sup>st</sup> Avenue), Auto Code (driving school) (1224 1<sup>st</sup> Avenue), Huey (hand laundry) (1230 1<sup>st</sup> Avenue), Chip Place (tavern) (1234 1<sup>st</sup> Avenue), Zwinder Electric (1242 1<sup>st</sup> Avenue), Cohen (signs) (1252 1<sup>st</sup> Avenue), Twelve Fifty Four Grill (1254 1<sup>st</sup> Avenue), Lucky Liquor &amp; Delicatessen (1256 1<sup>st</sup> Avenue), Log Hut Cafe (restaurant) (1260 1<sup>st</sup> Avenue), Hotel Wayne (1262 1<sup>st</sup> Avenue), The Log Hut (liquors) (1264 1<sup>st</sup> Avenue)</p> <p>Owl Electric (117 West A Street), <b>McFarquar</b> (cleaners) (119 West A Street)</p> <p>New Western Hotel (106 West B Street), Wolf-Brown Inc. (110 West B Street), National Auto Glass Company (120 West B Street), <b>Tufford Autos</b> (140 West B Street)</p>
1960	<p>Quon Mane (wholesaler) (1218 1<sup>st</sup> Avenue), Pacific Watchmaker (1222 1<sup>st</sup> Avenue), Auto Code (driving school) (1224 1<sup>st</sup> Avenue), Zwinder Electric (1242 1<sup>st</sup> Avenue), Cohen (signs), Twelve Fifty Four Grill (1254 1<sup>st</sup> Avenue), Brown's Delicatessen (1256 1<sup>st</sup> Avenue), Huey (hand laundry) (1260 1<sup>st</sup> Avenue), Log Hut Cafe (restaurant) (1264 1<sup>st</sup> Avenue), Hotel Wayne (1262 1<sup>st</sup> Avenue), The Log Hut (liquors) (1264 1<sup>st</sup> Avenue)</p> <p>Owl Electric (117 West A Street)</p> <p>Quon Mane (wholesaler (100 West B Street), New Western Hotel (106 West B Street), Wolf-Brown Inc. (110 West B Street), National Auto Glass Company (120 West B Street), <b>Midas Muffler Shop</b> (140 West B Street)</p>
1965	<p>Vacant (1202 to 1224 1<sup>st</sup> Avenue), Service Auto Park (parking lot) (1292 1<sup>st</sup> Avenue)</p> <p><b>Roy's Garage</b> (1243 Front Street)</p> <p>Vacant (100 and 106 to 108 West B Street)</p>
1970-2009	<p>City Operations Building (1222 1<sup>st</sup> Avenue)</p>

<b>Block B</b>	
1201, 1219, 1221, 1235, 1263, 1 <sup>st</sup> Street/Avenue 1210, 1220, 1228, 1234, 1242, 1248, 1250, 1260, 1272 2 <sup>nd</sup> Street/Avenue 101, 121 123, 139, 143, 1021 A Street 102, 104, 106, 114, 116, 124, 126, 128, 132, 134, 136, 138, 140, 1002, 1008 B Street	
1926	<p>Langham Hotel (1219 1<sup>st</sup> Avenue)</p> <p><b>Harvard Garage</b> (1220 2<sup>nd</sup> Avenue), Julian (plumber) (1228 2<sup>nd</sup> Avenue), Shannon (restaurant) (1244 2<sup>nd</sup> Avenue)</p> <p>Hunter (drugstore) (102 B Street), Langham Hotel (104 B Street), Bradt (tea and coffee) (106 B Street), Nackenhorst (shoe repair) 114 B Street), Frick (barber) (116 B Street), O'Brien (cigars) (118 B Street), Meil (tailor) (124 B Street), Hotel Harvard (128 B Street), Pacific Auto Financing (132 B Street), Hall (typewriters) (134 B Street), Auto Gear &amp; Supply (136 B Street), <b>Associated Tire Dealers</b> (138 B Street), <b>SD Auto Tire Company</b> (140 B Street)</p>
1930	<p>Lagrand Apartments (1219 1<sup>st</sup> Avenue)</p> <p><b>Knight</b> (garage) (1220 2<sup>nd</sup> Avenue), Julian (plumber) (1228 2<sup>nd</sup> Avenue)</p> <p>Marr's Drug Store (102 B Street), Langham Hotel (104 B Street), Bradt (tea and coffee)/San Diego Fire Equipment Company (106 B Street), Blanken &amp; Company (salvage store)/Lippe (grinder)/Radio Salvage Store (114 B Street), Frick (barber) (116 B Street), Hall &amp; Frank (vending machine) (124 B Street), Hotel Harvard (128 B Street), Pacific Auto Financing (132 B Street), Hall's Typewriter Service (134 B Street), <b>Hale</b> (auto parts) (136 B Street)</p>
1936	<p>Lagrand Apartments (1219 1<sup>st</sup> Avenue)</p> <p><b>Cox</b> (tires) (1220 2<sup>nd</sup> Avenue), Car (signs) (1228 2<sup>nd</sup> Avenue), Davis (manufacturer's agent) (1244 2<sup>nd</sup> Avenue)</p> <p><b>Switzer &amp; Robinson Service Station</b> (139 A Street)</p> <p>Cloyd's Pharmacy (102 B Street), Langham Hotel (104 B Street), Bradt/San Diego Fire Equipment Company (106 B Street), Lippe (grinder) (114 B Street), Frick (barber) (116 B Street), Hall (vending machine) (124 B Street), On Lee (laundry) (126 B Street, Hotel Harvard (128 B Street), <b>Davis</b> (cleaners) (136 B Street), Toledo Scales Company (140 B Street)</p>
1940	<p>Lagrand Apartments (1219 1<sup>st</sup> Avenue)</p> <p><b>Cox</b> (tires) (1220 2<sup>nd</sup> Avenue), Davis (wholesale radios) (1228 2<sup>nd</sup> Avenue), Par-Associates (auto loans) (1244 2<sup>nd</sup> Avenue)</p> <p><b>Switzer &amp; Robinson Service Station</b> (139 A Street)</p> <p>Harris (restaurant) (102 B Street), Langham Hotel (104 B Street), Bradt (fire equipment) (106 B Street), Lippe (grinder) (114 B Street), Myers (tailor) (124 B Street), On Lee (laundry) (126 B Street, Hotel Harvard (128 East B Street), <b>Kllean</b> (cleaners) (136 B Street), Toledo Scales Company (140 B Street)</p>

1945	<p>Lagrand Apartments (1219 1<sup>st</sup> Avenue)</p> <p><b>Cox</b> (tires) (1220 2<sup>nd</sup> Avenue), Davis (wholesale radios) (1228 2<sup>nd</sup> Avenue), Johnson (scales) (1244 2<sup>nd</sup> Avenue)</p> <p><b>Miller Service Station</b> (139 A Street)</p> <p>Harris (restaurant) (102 East B Street), Langham Hotel (104 East B Street), San Diego Fire Equipment Company (106 East B Street), Hill (grinder) (114 East B Street), Fedderson (photographer) (116 East B Street), E O'Keefe (restaurant) (124 East B Street), Hotel Harvard (128 East B Street)</p>
1950	<p>Lagrand Apartments (1219 1<sup>st</sup> Avenue), Kash Auto Park (1233 1<sup>st</sup> Avenue)</p> <p>Mc Farland Building Company (1218 2<sup>nd</sup> Avenue), <b>Mc Farland</b> (autos) (1220 2<sup>nd</sup> Avenue), Electronic Equipment Distributors (1228 2<sup>nd</sup> Avenue)</p> <p><b>Robinson Service Station</b> (139 A Street)</p> <p>Harris (restaurant) (102 B Street), Langham Hotel (104 B Street), Monk (barber) (116 B Street), E O'Keefe (restaurant) (124 B Street), Hotel Harvard (128 B Street), Rubin &amp; Son (newspaper distributor) (130 B Street), Turrentine (restaurant) (132 B Street), Harvard Café (134 B Street), Electronic Equipment Distributor (140 B Street)</p>
1955	<p>Downtown Auto Park (1243 to 1263 1<sup>st</sup> Avenue)</p> <p><b>Mc Farland Garage</b> (1218 to 1220 2<sup>nd</sup> Avenue), Electronic Equipment Distributors (1228 2<sup>nd</sup> Avenue, Sunshine Coffee Shop (1244 2<sup>nd</sup> Avenue)</p> <p><b>Larry's Service Station</b> (139 A Street)</p> <p>Tent Café (102 B Street), Langham Hotel (104 B Street), Tailor's Rummage Sale (106 B Street), Fisher (real estate) (116 B Street), Ted's Sandwich Shop (124 B Street), Hotel Harvard (128 B Street), Tex's Barber Shop (130 B Street), Harvard Café (134 B Street), Electronic Equipment Distributor (140 B Street)</p>
1960	<p>Ace Auto Park (1243 1<sup>st</sup> Avenue)</p> <p>Bill's Camera Repair (1244 2<sup>nd</sup> Avenue)</p> <p>Ace Auto Park (135 A Street)</p>
1970-2009	<p>Concourse Parking (1265 1<sup>st</sup> Avenue)</p>

<b>Block C</b>	
1111, 1117, 1125, 1133, 1141, 1145, 1149, 1151, 1155, 1165 1 <sup>st</sup> Street/Avenue 1102, 1116, 1128, 1140, 1152, 1154, 1156, 1158, 1160, 1166 2 <sup>nd</sup> Street/Avenue 101, 115, 129 131, 147, 1031 B Street 102, 110, 120, 122, 124, 130, 132, 136, 140, 144, 146, 148, 150, 1008, 1022, 1042 C Street	
1926	<p><b>Roy Snider Autos</b> (1125 1<sup>st</sup> Avenue), <b>Sunset Electric Company</b> (manufacturing) (1141 1<sup>st</sup> Avenue), <b>United Ignition &amp; Battery</b> (1151 1<sup>st</sup> Avenue), <b>Kavanagh &amp; Johnson</b> (autos) (1165 1<sup>st</sup> Avenue)</p> <p><b>Savoy Garage</b> (1116 2<sup>nd</sup> Avenue), <b>Federal Auto Wrecking &amp; Tire Works</b> (1128 2<sup>nd</sup> Avenue), <b>Motor Inn Garage</b> (1140 2<sup>nd</sup> Avenue)</p> <p><b>Morton Tire Store</b> (102 C Street), <b>International Sales Company</b> (tire sales) (110 East C Street), <b>General Ignition</b> (148 East C Street)</p>
1930	<p><b>Robert's Garage</b> (1125 1<sup>st</sup> Avenue), <b>Dunlop Tire and Rubber Company</b> (1165 1<sup>st</sup> Avenue)</p> <p><b>Savoy Garage</b> (1116 2<sup>nd</sup> Avenue), <b>Federal Auto Wrecking &amp; Tire Works</b> (1128 2<sup>nd</sup> Avenue), <b>Cooper</b> (auto repair) (1140 2<sup>nd</sup> Avenue), <b>Aldrich Used Cars/Short</b> (auto repair) (1160 2<sup>nd</sup> Avenue), <b>Brown</b> (autos) (1166 2<sup>nd</sup> Avenue)</p> <p><b>Porter &amp; Hunter</b> (tire sales) (110 C Street), <b>Robinson Engineering, Inc.</b> (tires) (120 C Street), <b>Bagby Service Station</b> (132 C Street)</p>
1936	<p><b>Paul's Savoy Garage</b> (1117 1<sup>st</sup> Avenue), Bartlett (machine shop)/<b>DeLemos/Hill</b> (auto repair) (1125 1<sup>st</sup> Avenue), Wright Sound Equipment (1141 1<sup>st</sup> Avenue), <b>Lewis</b> (auto repair) (1151 1<sup>st</sup> Avenue), <b>Dunlop Tire and Rubber Company</b> (1165 1<sup>st</sup> Avenue)</p> <p><b>Paul's Savoy Garage</b> (1116 2<sup>nd</sup> Avenue), <b>Hertz Drive-Ur-Self Station, Inc.</b> (auto rental) (1140 2<sup>nd</sup> Avenue), <b>U.S. Grant Radio &amp; Battery Company</b> (1166 2<sup>nd</sup> Avenue)</p> <p><b>Inch Paint Store</b> (102 C Street), <b>Elliott</b> (trucking)/<b>Pickering</b> (tires) (110 C Street), <b>Pep Boys</b> (144 C Street)</p>
1940	<p>Hill (welder) (1125 1<sup>st</sup> Avenue), Wright Sound Equipment (1141 1<sup>st</sup> Avenue), <b>Paul Clark</b> (auto dealer) (1165 1<sup>st</sup> Avenue)</p> <p>Dorman Hotel Supply Company (1116 2<sup>nd</sup> Avenue), Edwards (auto park) (1132 2<sup>nd</sup> Avenue), <b>Hertz Drive-Ur-Self Station, Inc.</b> (auto rental) (1140 2<sup>nd</sup> Avenue), <b>Goldstein</b> (tire repair) (1160 2<sup>nd</sup> Avenue), <b>Ceazen Tire, Inc.</b> (1166 2<sup>nd</sup> Avenue)</p> <p><b>Inch Paint Store</b> (102 C Street), <b>Pickering</b> (tires) (110 C Street), <b>De Lemos</b> (auto repair) (130 C Street), <b>Pep Boys</b> (144 C Street)</p>

1945	<p>Gray Line Taxi (1125 1<sup>st</sup> Avenue), Johnson Advertising (1141 1<sup>st</sup> Avenue), Borland (sign painter)/<b>Gaynon</b> (auto repair) (1151 1<sup>st</sup> Avenue)</p> <p>Dorman Hotel Supply Company (1116 2<sup>nd</sup> Avenue), Edwards (auto park) (1132 2<sup>nd</sup> Avenue) <b>Hertz Drive-ur-Self Station, Inc.</b> (auto rental) (1140 2<sup>nd</sup> Avenue), Rubin (newspaper distributor) (1160 2<sup>nd</sup> Avenue), <b>Caazen Tire, Inc.</b> (1166 2<sup>nd</sup> Avenue)</p> <p><b>Standard Brand Paint</b> (115 B Street)</p> <p><b>Inch Paint Store</b> (102 C Street), Demangos (liquor) (118 C Street), Malo (tailor) (120 C Street), <b>Pep Boys</b> (144 C Street)</p>
1950	<p>Nelson-Thomas Company (office furniture) (1151 1<sup>st</sup> Avenue)</p> <p><b>Pep Boys</b> (auto supplies) (1116 2<sup>nd</sup> Avenue), <b>Dort</b> (auto rental) (1140 2<sup>nd</sup> Avenue), Nelson-Thomas (office furniture) (1160 2<sup>nd</sup> Avenue), Cantor Brothers (electrical appliance) (1166 2<sup>nd</sup> Avenue)</p> <p><b>Standard Brand Paint</b> (115 B Street)</p> <p><b>Mount Palomar Paint</b> (102 C Street), Martin (war surplus store) (120 C Street), <b>Pep Boys</b> (144 C Street)</p>
1955	<p>Nelson-Thomas Company (office furniture) (1125 1<sup>st</sup> Avenue), Colonial Silver Shop (1141 1<sup>st</sup> Avenue), Nelson-Thomas Company (office furniture) (1151 1<sup>st</sup> Avenue)</p> <p>Union Title Insurance and Trust (parking) (1140 2<sup>nd</sup> Avenue), Clinton's Men's Store (1166 2<sup>nd</sup> Avenue)</p> <p><b>Standard Brand Paint</b> (115 B Street)</p> <p><b>Inch Paint Store</b> (102 C Street), <b>Pep Boys</b> (144 C Street)</p>
1960	<p>Nelson-Thomas Company (office furniture) (1125 1<sup>st</sup> Avenue), Colonial Silver Shop (1141 1<sup>st</sup> Avenue), Nelson-Thomas Company (office furniture) (1151 1<sup>st</sup> Avenue), Sunset Merchandise Imports (1165 1<sup>st</sup> Avenue)</p> <p>Service Auto Parks (parking) (1140 2<sup>nd</sup> Avenue), Clinton's Men's Store (1166 2<sup>nd</sup> Avenue)</p> <p><b>Standard Brand Paint</b> (115 B Street)</p> <p><b>Pep Boys</b> (144 C Street)</p>
1965-2009	San Diego City Auditorium/Convention Center ( Golden Hall)
<p><b>Block D</b> 1121 2<sup>nd</sup> Street/Avenue 200, 202 , 208, 212, 214, 216, 218, 222, C Street</p>	
1926	<p><b>Moynihan</b> (autos) (208 C Street), Coleman Mortgage Company (214 C Street), Effie (restaurant) (218 C Street), <b>Hewins Rubber Company</b> (tires) (222 C Street)</p>
1930	<p><b>Hudson-Essex Agency</b> (autos) (208 C Street), Barnet (restaurant) (218 C Street), Kerber (cigars) (222 C Street)</p>

1936	Paul Clark Autos (208 C Street), Hampton (restaurant) (218 C Street), Kerber (cigars) (222 C Street)
1940	Western Auto Supply (208 C Street)
1945	Western Auto Supply (208 C Street)
1950	Western Auto Supply (208 C Street)
1955	Barney & Barney (208 C Street)
1960	Parron-Hall (office supplies) (208 C Street)
1965-2009	San Diego City Hall/Administration Building (202 C Street)

A summary table of historical land uses and features of potential concern is presented below. This summary is based on the historical research described above.

Addresses	Dates	Tenants
<b>Block A</b>		
1207 Front Street	1921-1930, 1950	Service station/automotive repair
1243 Front Street	1950-1965	Automotive repair
1271 Front Street`	1936-1950	Automotive repair/sales and service
1260 1 <sup>st</sup> Avenue	1926-1945	Dry cleaning/dye works
119 West A Street	1950-1955	Dry cleaning
102 West B Street	1926, 1950-1965	Tire repair/automotive body shop
108 West B Street	1936, 1950-1956	Printer
120 West B Street	1926	Automotive services
140 West B Street	1926-1960	Automotive sales and service/muffler shop
<b>Block B</b>		
139 A Street	1936-1962	Gasoline service station
136 B Street	1926-1940	Tire store/Automotive parts/cleaners
138 B Street	1926	Tire store
140 B Street	1926	Tire sales
1220 2 <sup>nd</sup> Avenue	1926-1962	Automotive repair/sales and service/tire sales
1235 1 <sup>st</sup> Avenue	1950	Automotive sales
<b>Block C</b>		
115 B Street	1945-1960	Paint store
102 C Street	1921-1955	Tire sales/paint store
110 C Street	1921-1940	Tire sales
120 C Street	1921-1930	Tire sales
130 C Street	1921, 1940	Automotive repair

Addresses	Dates	Tenants
132 C Street	1921-1930	Gasoline service station
144 C Street	1921-1960	Automotive repair
148 C Street	1921-1926	Automotive repair
1117 1 <sup>st</sup> Avenue	1921-1936	Automotive garage
1125 1 <sup>st</sup> Avenue	1921-1959	Automotive garage
1151 1 <sup>st</sup> Avenue	1921-1945	Automotive garage
1165 1 <sup>st</sup> Avenue	1921-1959	Automotive garage
1116 2 <sup>nd</sup> Avenue	1921-1959	Automotive garage
1128 2 <sup>nd</sup> Avenue	1921-1930	Auto supplies/automotive dismantling
1140 2 <sup>nd</sup> Avenue	1921-1950	Automotive garage/car rental
1160 2 <sup>nd</sup> Avenue	1921-1940	Automotive sales and service/tire repair
1166 2 <sup>nd</sup> Avenue	1921-1959	Automotive sales/repair/tire repair/gas and oil
<b>Block D</b>		
1121 2 <sup>nd</sup> Avenue	1921, 1950	Automotive repair
208 C Street	1926-1955	Automotive repair/Western Auto
222 C Street	1926	Tire sales/repair

Because many of the dates listed above are based on a limited selection of historical resources, they are considered to be approximations only; the actual beginning/ending dates for many of the Site uses listed above may have been earlier or later than indicated.

With the exceptions of the reported presence of gasoline service stations, automotive repair, automotive sales and service facilities, dry cleaning facilities and dye works, automotive dismantling, and burn ash and/or metal-bearing fill (discussed below), no obvious historical facilities, features of concern, or land uses indicative of the use, storage, or generation of hazardous materials/wastes or petroleum products were found in the historical resources reviewed.

Please note that extensive earth work, possibly including soil export, was done as part of the development of the Civic Center Complex. The nature and extent of these development activities and the mitigation or effect o release(s), if any, from historical land use is unknown. In addition, due to the Site's developed status, Phase II investigation to assess possible historical releases may not be practical (e.g., due to building footprint and foundations). In making our Phase II recommendations, we have not attempted to account for these issues, but have instead assumed they could be addressed during a subsequent design phase.

#### Historical Automotive Dismantling Activities

Automotive dismantling activities reportedly occurred at the Site from prior to 1926 to prior to 1936. Based on SCS's experience, auto dismantling and recycling operations, particularly those

on unpaved areas, which we believe may have been the case for the Site, frequently have releases to soil as a result of day-to-day operations. Releases and soil impacts from gasoline, oil, solvents, antifreeze, and metals are not uncommon. And, while heavy end petroleum and metals impacts of the upper several feet of soil are common, it is not unusual to see impacts to deeper soil horizons, and groundwater if it is shallow. These impacts are usually associated with more mobile constituents of concern such as gasoline and solvents. Regulatory records are not available for this time period and SCS is unable to assess the potential that releases have occurred and a recognized environmental condition is present at the Site. SCS recommends a Phase II investigation and/or soil vapor survey to assess the possible presence and extent of releases, if any, as a result of this land use.

#### Historical Gasoline Service Station/Automotive Repair/Automotive Sales and Service Facility

Based on a review of historical resources, several gasoline service stations, automotive repair shops, and automotive sales and service facilities were reported to have been located at or adjacent to the Site. Based on SCS's experience, features of potential concern (FOPCs) (e.g., inground hydraulic lifts, wastewater clarifiers systems, and USTs) were often installed at gasoline service stations and automotive sales and service/repair facilities. Solvents, including chlorinated solvents, were often used to degrease parts and in automotive painting/auto body repair. Based on the number of reported facilities and our experience, there is a moderate likelihood of a recognized environmental condition as a result of these historical land uses

To assess whether any FOPCs are present in association with the former gas station or automotive service activities, SCS's recommends a geophysical survey be conducted in an attempt to determine whether any inground structures, such as USTs or lifts, remain at the Site. A Phase II soil and soil vapor sampling is recommended assess the possible presence and extent of releases, if any, as a result of this land use. Additional detailed research at the San Diego Historical Society may help target these investigations.

#### Dry Cleaning Facilities and Dye Works

Based on a review of historical resources, dry cleaning facilities and dye works were reportedly located at the Site from prior to circa 1926 to circa 1955. No regulatory records were available for the dry cleaning, which would indicate the use of hazardous materials, generation of hazardous waste, or releases to the subsurface. However, based on SCS's experience, dry cleaning facilities use and store tetrachloroethylene (PCE) or other halogenated solvents, generate waste (such as still bottoms and used filters), and often experience releases (to the soil, soil vapor, and/or groundwater). Although, based on available data, there are no known and reported releases in connection with these facilities, in our experience, over time, halogenated solvents have the ability to permeate flooring and building foundations, entering the subsurface soils and potentially impacting groundwater. It is not known what time frame is required for this to occur. Many property owners and lenders have established a Phase II sampling "trigger" of 5 to 7 years (i.e., dry cleaning facilities in operation over 5 years are subject to Phase II subsurface testing for the presence of PCE), and in some cases, as little as two years. Dry cleaners are interpreted to have occupied the Site for approximately 30 years. SCS believes that if these facilities used PCE, there is a moderate to high likelihood of subsurface impacts (and related

recognized environmental conditions), given the tendency for PCE to migrate through flooring and building foundations.

SCS recommends the collection and analysis of soil and soil vapor samples for volatile organic compounds (VOCs). If VOCs are present in the shallow soil vapor, a health risk assessment should be performed to assess possible health risk for future building occupants. If extensive grading, soil excavation, or off-Site soil export will occur during the proposed redevelopment of the Site, we recommend the collection and analysis of soil samples for VOCs (e.g., to protect workers and to know if exported soil will have to be handled and disposed of as a hazardous waste).

#### Burn Ash and/or Metal-bearing Fill

Based on SCS's experience with properties in older neighborhoods of San Diego, there is the potential for burned or incinerated ash from "backyard" incinerators or "burn pits" or metal-bearing fill material to be present or mixed with the soil. Burn ash and/or metal-bearing fill material may contain high concentrations of contaminants of concern, notably certain metals (e.g., copper, lead, zinc, mercury, and cadmium).

The City of San Diego Report on Refuse Dumps (City Planning Commission, City of San Diego, California, Report of Refuse Dumps, Document Number 306491, dated January 1938) was reviewed. The Site was not interpreted to be located in the vicinity of any of the listed facilities. However, this is a limited list of well-documented or major burn dump locations. More typical is the use of vacant and residential properties for this purpose. Based on our historical review, the Site was interpreted to be developed with a vacant land and single-family residences from circa prior to 1887 to 1921.

Please note that based on a review of the DEH files, a voluntary assistance program was reportedly completed for a facility located 0.18 mile northwest of the Site (1550 Front Street) after burn ash and/or metal-bearing fill were encountered during a Phase II Environmental Site Assessment. A total of 99 tons of lead and burn ash were reportedly removed from the facility.

The enforcement actions requiring the remediation of burn ash or metal-bearing fill material at a property are typically driven by redevelopment and/or excavation as well as potential exposure concerns, if present. The Site grounds were observed to be covered with Site buildings and pavement. If the soil beneath the Site did contain burn ash or metal-bearing fill material, SCS judges that under normal circumstances (i.e., no redevelopment or excavation), there are no complete exposure pathways for direct dermal contact with, or ingestion or inhalation of, burn ash or metal-bearing fill material (if present).

Based on SCS's experience, if burn ash is present in soil, the likelihood of an enforced remediation is low as long as the soil remains undisturbed. However, if the burn ash or metal-bearing fill material is present, it would typically be considered a waste management issue if disturbed or particularly if the soil is exported. Any such disturbed materials should be handled, possibly as a hazardous waste, in accordance with appropriate laws and regulations.

If the Site is ever redeveloped (i.e., including the excavation and export of soil), SCS recommends a trenching or drilling program including soil sampling be conducted to assess the presence and volume of impacted soil, if any. Such assessment would ideally involve sampling of the shallow subsurface soil at the Site and subsequent analysis for typical burn ash constituents. Note, to the extent a geotechnical or fault investigational program will be conducted at the Site, SCS has found that a burn ash and/or metal-bearing fill material investigations can be efficiently combined with trenching for geotechnical purposes.

Please note that these qualitative opinions are based on SCS's experience and professional judgment. Our confidence in these opinions is limited and actual Site conditions may be different. Should you desire a confirmation of, or a higher degree of confidence in, these opinions, subsurface assessment activities would be required (e.g., soil sampling and analysis).

For convenience, these features of potential concern have been summarized in an attachment included in the Appendix.

### Historical Site Vicinity Land Use

The following table provides a chronology of the apparent historical Site vicinity land uses as interpreted from a review of information from the sources referenced. Facilities interpreted to have likely stored and used hazardous materials or petroleum products, and/or generated hazardous waste are noted in bold text.

Year	Description
<b>Block A</b>	
<b>1305 Front Street, 1302 1<sup>st</sup> Street, and 102 to 148 West A Street (North)</b>	
1926	<b>Model Machine Shop</b> (130 West A Street)
1930	<b>Lee Autos</b> (1302 1 <sup>st</sup> Avenue)
1936	<b>Lee Autos</b> (1302 1 <sup>st</sup> Avenue), California Plate & Window Glass Company (120 West A Street)
1940	<b>Lee Autos</b> (1302 1 <sup>st</sup> Avenue), California Plate & Window Glass Company (120 West A Street)
1945	University of California (1302 1 <sup>st</sup> Avenue)
1950	<b>Lee Autos</b> (1302 1 <sup>st</sup> Avenue)
1955	<b>Lee Autos</b> (1302 1 <sup>st</sup> Avenue)
1960	<b>Lee Autos</b> (1302 1 <sup>st</sup> Avenue)
1965	<b>Lee Autos</b> (1302 1 <sup>st</sup> Avenue)
1950	None
1975-2009	Bank of California Plaza/110 Building (110 West A Street)
<b>Block B (see Historical Site Land Use section above) (East)</b>	
<b>1165 Front Street, 1172 1<sup>st</sup> Avenue, and 101 to 147 West B Street (South)</b>	
1926	Gavin & Company (1165 Front Street)

Year	Description
1930	Gavin & Company (1165 Front Street)
1936	Residential
1940	California Department of Employment/San Diego County Probation Department (1165 Front Street), <b>Gray</b> (gasoline service station) (101 West B Street)
1945	California Department of Employment/San Diego County Probation Department (1165 Front Street), <b>Gray</b> (gasoline service station) (101 West B Street)
1950	California Department of Employment/San Diego County Probation Department (1165 Front Street), <b>Barrett</b> (gasoline service station) (101 West B Street)
1955	California Department of Employment/San Diego County Probation Department (1165 Front Street), <b>Cox</b> (gasoline service station) (101 West B Street)
1960	California Department of Employment/San Diego County Probation Department (1165 Front Street), <b>Gray</b> (gasoline service station) (101 West B Street)
1965	Ace Auto Park) (101 West B Street), California Department of Employment/San Diego County Probation Department (145 West B Street)
1970 to 2009	Charter Oil Building (110 West C Street)/San Diego County Sheriff (1173 Front Street)
<b>1200 to 1268 Front Street, 201 West A Street, and 200, 212 West B Street (West)</b>	
1926	Gavin & Company (1165 Front Street)
1930	Sterne Auto Accessories (1220 Front Street), <b>Crowder Carburetors/Electrical</b> (1224 Front Street), <b>Keeler Auto Specialties</b> (1228 Front Street), <b>May</b> (gasoline service station) (212 West B Street)
1936	<b>Crowder</b> (auto electrical) (1224 Front Street), <b>Ceazan Tires, Ltd.</b> (200 West B Street)
1940	Robinson (kennels) (1260 Front Street), <b>Hawkinson Nu-Tread Tires.</b> (200 West B Street)
1945	<b>Skinner</b> (tires) (1224 Front Street), Stutz Enterprises (office) (200 West B Street)
1950	<b>Motorville Used Cars</b> (1250 to 1268 Front Street)
1955	Hyster & Company (1220 Front Street), <b>Gray</b> (used cars) (1250 Front Street), <b>San Diego Hydromantic Transmission</b> (200 West B Street)
1960	<b>San Diego Hydromantic Transmission</b> (200 West B Street)
1965- 2009	Parking
<b>Block B</b>	
<b>1303 1<sup>st</sup> Street, 1304 2<sup>nd</sup> Street, and 100 to 148 A Street (North)</b>	
1926	Residential
1930	Residential
1936	Residential
1940	Residential
1945	<b>Lenox &amp; Kettenhofer</b> (used cars) (122 A Street)

Year	Description
1945	<b>Marassi Motors</b> (used cars) (122 A Street)
1950	<b>Marassi Motors</b> (used cars) (122 A Street)
1955	<b>Marassi Motors</b> (used cars) (122 A Street)
1960	None listed
1965-2009	SDG&E/Sempra (offices) (101 Ash Street)
<b>201, 205, 223 A Street, 202 B Street, and 1201 to 1251 2<sup>nd</sup> Avenue (East)</b>	
1926	<b>Pickford Tires</b> (202 B Street)
1930	<b>Scratchfield</b> (gasoline service station) (1251 2 <sup>nd</sup> Avenue), Calencia Wrought Iron (201 A Street), <b>Pickford-Baker Auto Supply</b> (202 B Street)
1936	Calencia Wrought Iron (201 A Street), Scherer (auto financing) (200 B Street)
1940	Calencia Wrought Iron (201 A Street), <b>General Paint Company</b> (202 B Street)
1945	Rowland (auto park) (205 A Street), <b>General Paint Company</b> (202 B Street)
1950	Kelly (auto park) (205 A Street), <b>King Paint Company</b> (202 B Street)
1955	Medico-Dental Auto Park (205 A Street), <b>King Paint Company</b> (202 B Street)
1960	Security First National Bank (offices) (201 A Street), Service Auto Park (205 A Street)
1965	Security First National Bank (offices) (201 A Street), Service Auto Park (205 A Street), Security Pacific Bank Building (offices) (233 A Street)
1970	Security First National Bank (offices) (201 A Street), Service Auto Park (205 A Street), Center City Building (offices) (233 A Street)
1975	Bank of Tokyo (offices) (201 A Street), Center City Building (233 A Street)
1980	Bank of Tokyo (offices) (201 A Street), Center City Building (233 A Street)
1985	Lloyds Bank (offices) (201 A Street), Center City Building (233 A Street)
1990	Kelsey-Jenney College (education) (201 A Street), Center City Building (offices) (233 A Street)
1994	Kelsey-Jenney College (education) (201 A Street), Center City Building (offices) (233 A Street)
1999	Kelsey-Jenney College (education) (201 A Street), Center City Building (offices) (233 A Street)
2005-2009	Kelsey-Jenney College (education) (201 A Street), Center City Building (offices) (233 A Street)
<b>Block C (see Historical Site Land Use section above) (South)</b>	
<b>Block A (see Historical Site Land Use section above) (West)</b>	

<b>Block C</b>	
<b>Block B (see Historical Site Land Use section above) (North)</b>	
<b>201, 215 B Street and 1101 to 1171 2<sup>nd</sup> Avenue (East)</b>	
1926	<b>Persian Drive Garage</b> (1127 2 <sup>nd</sup> Avenue), <b>Conniry Autos</b> (1145 2 <sup>nd</sup> Avenue), <b>Dick</b> (auto radiators) (1159 2 <sup>nd</sup> Avenue), <b>Standard Motor Parts Company</b> (1171 2 <sup>nd</sup> Avenue) <b>Lee Tire Company</b> (215 B Street), <b>Moynihan Autos</b> (208 C street)
1930	<b>Trousdale Auto Repair</b> (1127 2 <sup>nd</sup> Avenue), Dorney (cigars) (1131 2 <sup>nd</sup> Avenue), Phillips Public Address Systems (1143 2 <sup>nd</sup> Avenue), United Parcel Service (1145 2 <sup>nd</sup> Avenue), Hanks Investment Company (1159 2 <sup>nd</sup> Avenue), Motor Vehicle Publishing House (1163 2 <sup>nd</sup> Avenue) Christian Science Meeting Room (1169 2 <sup>nd</sup> Avenue), Naylor Financial (1171 2 <sup>nd</sup> Avenue) <b>Valley Rubber Supply Company</b> (215 B Street), <b>Hudson-Essex Agency</b> (208 C street)
1936	San Diego Shopping News Company (1127 to 1129 2 <sup>nd</sup> Avenue), <b>Dunlop Tire &amp; Rubber Company</b> (1143 2 <sup>nd</sup> Avenue), United Parcel Service (1145 2 <sup>nd</sup> Avenue), Hanks Investment Company (1159 2 <sup>nd</sup> Avenue), Jasper (publisher) (1163 2 <sup>nd</sup> Avenue) Farmers Automobile Insurance Company (1167 2 <sup>nd</sup> Avenue) Pacific Auto Finance Company (201 B Street), <b>Paul Clark Autos</b> (208 C street)
1940	San Diego Progress Journal (1127 to 1129 2 <sup>nd</sup> Avenue), <b>Mercalf</b> (garage) (1145 2 <sup>nd</sup> Avenue), <b>Simplex Motor Auto Parts</b> (1159 2 <sup>nd</sup> Avenue), Jasper (mailing service) (1163 2 <sup>nd</sup> Avenue) Mitos (restaurant) (1167 2 <sup>nd</sup> Avenue) Vacant (201 B Street), <b>Western Auto Supply</b> (208 C street)
1945	San Diego Daily Journal (1127 2 <sup>nd</sup> Avenue), Smith (restaurant supply) (1145 2 <sup>nd</sup> Avenue), Jasper Mailing Service (1163 2 <sup>nd</sup> Avenue), Virginia Cafe (restaurant) (1167 2 <sup>nd</sup> Avenue) Pohl (radio representative) (211 B Street), <b>Western Auto Supply</b> (208 C street)
1950	Waiters and Bartenders Union (1127 2 <sup>nd</sup> Avenue), <b>Economy Auto Parts</b> (1129 2 <sup>nd</sup> Avenue), Smith & Company (restaurant supply) (1145 2 <sup>nd</sup> Avenue), Jasper Mailing Service (1163 2 <sup>nd</sup> Street) Home Financing (201 B Street), <b>Western Auto Supply</b> (208 C street)
1955	Waiters and Bartenders Union (1127 2 <sup>nd</sup> Avenue), Smith & Company (shipping department) (1129 2 <sup>nd</sup> Avenue), Smith & Company (restaurant supply) (1145 2 <sup>nd</sup> Avenue), Jasper Mailing Service (1163 2 <sup>nd</sup> Avenue) Cogswell Insurance Agency (201 B Street), Barney & Barney Insurance (208 C street)
1960	Bud's Gym (1127 2 <sup>nd</sup> Avenue), Harbor Reproduction, Inc. (lithography) (1129 2 <sup>nd</sup> Avenue), Hall Lock & Safe Company (1145 2 <sup>nd</sup> Avenue), Jasper Mailing Service (1163 2 <sup>nd</sup> Avenue) Cogswell Insurance Agency (201 B Street), Parron-Hall Corporation (office supplies) (208 C street)
1965-2009	San Diego Civic Theater
<b>1069 1<sup>st</sup> Avenue, 1060/1090 2<sup>nd</sup> Avenue, 101 to 147 C Street (South)</b>	
1926	<b>Border Rubber Company</b> (tires) (115 C Street), Menke (cigars) (123 C Street)
1930	<b>Van Fleet &amp; Durkee</b> (gasoline service station) (1057 1 <sup>st</sup> Avenue), <b>Dort</b> (auto rentals) (1060 2 <sup>nd</sup> Avenue) Coppens (cigars) (123 C Street)

1936	<b>Richfield Service Station</b> (1057 1 <sup>st</sup> Avenue), <b>Dort</b> (auto rentals) (1060 2 <sup>nd</sup> Avenue) Palmer (beer) (109 C Street), Sullivan (liquors) (115 C Street), Scofield (civil engineer) (119 C Street)
1940	<b>Dort</b> (used cars) (1060 2 <sup>nd</sup> Avenue) Dyer (restaurant) (101 to 109 C Street), McMahon (cigars) (115 C Street)
1945	Associated Photographic Studios (1060 2 <sup>nd</sup> Avenue) Dyer (restaurant) (101 C Street), Morrison (liquors) (123 C Street)
1950	Eddie, Inc. (liquor) (101 C Street)
1955	Hutsel (real estate) (1070 2 <sup>nd</sup> Avenue), Eddie's Club (liquor) (101 C Street)
1960	Service Auto Parks (101 C Street)
1965	Service Auto Parks (101 C Street)
1970	Westgate Life Insurance Company (1057 1 <sup>st</sup> Avenue)
1975	Vacant (1057 1 <sup>st</sup> Avenue)
1980	National University (1057 1 <sup>st</sup> Avenue), Executive Travel (127 C Street)
1985	Executive Hotel (1055 1 <sup>st</sup> Avenue), Executive Travel (127 C Street)
1990	Executive Hotel (1055 1 <sup>st</sup> Avenue), C Street Salon (127 C Street)
1995	Executive Hotel (1055 1 <sup>st</sup> Avenue), C Street Salon I (127 C Street)
1999-2009	Bristol Court Hotel (1055 1 <sup>st</sup> Avenue), C Street Salon (127 C Street)
<b>102 to 110 West C Street, 1147 Front Street, 1104 to 1160 1<sup>st</sup> Avenue (West)</b>	
1926	<b>Forrest Auto Repair</b> (1132 1 <sup>st</sup> Avenue), <b>De Lannoy Auto Repair</b> (1134 1 <sup>st</sup> Avenue), <b>Brown Auto Repair</b> (1144 1 <sup>st</sup> Avenue), <b>Dupree Garage</b> (1152 1 <sup>st</sup> Avenue)
1930	<b>Paul and Riley</b> (auto repair) (1134 1 <sup>st</sup> Avenue), <b>Dupree Garage</b> (1152 1 <sup>st</sup> Avenue) <b>Western Auto Supply Company</b> (102 West C Street)
1936	<b>Carnes</b> (auto repair) (1144 1 <sup>st</sup> Avenue), <b>Payne</b> (auto repair) (1152 1 <sup>st</sup> Avenue) <b>Western Auto Supply Company</b> (102 West C Street)
1940	<b>Anderson</b> (auto repair) (1134 1 <sup>st</sup> Avenue), Smith Hotel and Bar Supplies (1144 1 <sup>st</sup> Avenue), <b>Black</b> (auto repair) (1152 1 <sup>st</sup> Avenue) Paris Inn Cafe (102 West C Street)
1945	Paris Inn Cafe (102 West C Street)
1950	California Department of Employment (1144 1 <sup>st</sup> Avenue), Abbott Supply Company (street poster service) (1148 1 <sup>st</sup> Avenue), Brown Baker (1147 Front Street) Kennedy (restaurant) (102 West C Street)
1955	California Department of Employment (1144 1 <sup>st</sup> Avenue), Johnson (outdoor advertising) (1148 1 <sup>st</sup> Avenue) Paris Inn (restaurant) (102 West C Street)
1960	<b>Standard Brands Paint Company</b> 1150 1 <sup>st</sup> Avenue)
1965	Service Auto Park (1120 1 <sup>st</sup> Avenue), Ace Parking) (101 West B Street)
1970	Service Auto Park (1120 1 <sup>st</sup> Avenue)
1975	Service Auto Park (1120 1 <sup>st</sup> Avenue)

1980	Electronics Capital Building (110 West C Street)
1985	Electronics Capital Building (110 West C Street)
1990	Electronics Capital Building (110 West C Street)
1995	Electronics Capital Building (110 West C Street)
1999-2009	Charter Oil Building (110 West C Street)/San Diego County Sheriff (1173 Front Street)
<b>Block D</b>	
<b>1121 to 1127 2<sup>nd</sup> Avenue (North) (See Block C East)</b>	
<b>1100 to 1012 3<sup>rd</sup> Avenue and 226 to 250 C Street (East)</b>	
1926	Lewis (restaurant) (1100 3 <sup>rd</sup> Avenue), Freeberg (cigars) (1112 3 <sup>rd</sup> Avenue) Savoy Theater (226 C Street)
1930	Lewis (restaurant) (1100 3 <sup>rd</sup> Avenue), Freeberg (cigars) (1112 3 <sup>rd</sup> Avenue) Savoy Theater (226 C Street)
1936	Freeberg (cigars) (1112 3 <sup>rd</sup> Avenue) Savoy Theater (226 C Street)
1940	Rumble (cigars) (1112 3 <sup>rd</sup> Avenue) Savoy Theater (230 C Street)
1945	<b>Standard Service Station</b> (250 C Street)
1950	<b>Standard Service Station</b> (250 C Street)
1955	<b>Standard Service Station</b> (248 C Street)
1960	<b>Standard Service Station</b> (248 C Street)
1965-2009	San Diego Civic Theater
<b>201 to 223 C Street, 1055 2<sup>nd</sup> Avenue (South)</b>	
1926	<b>Perfect Tire Shop</b> (211 C Street), Hammon (restaurant) (211 C Street)
1930	San Diego Leather & Finding Company (211 C Street), Thatcher (cigars) (219 C Street, Vidal (tailor) (223 C Street)
1936	Jorgenson (fruit) (211 C Street), Sellers (restaurant) (215 C Street), Meers (cigars) (219 C Street, Vidal (tailor) (223 C Street)
1940	Balason (grocer) (211 C Street), Michelfilder (restaurant) (215 C Street), Lambru (cigars) (219 C Street, Vidal (tailor) (223 C Street)
1945	Balason (grocer) (211 C Street), George's Cafe (restaurant) (215 C Street), Lambru (cigars) (219 C Street, Vidal (tailor) (223 C Street)
1950	George's Cafe (restaurant) (215 C Street), Balason (grocer) (219 C Street, Glavas) (card room) (223 C Street)
1955	G S Cafe (restaurant) (215 C Street), Uneda (market) (219 C Street), Tex's Card Room (223 C Street)
1960	U.S. Marine Corps Recruiting Station (211 C Street), Tasty Food Cafe (restaurant) (215 C Street), Uneda (market) (219 C Street), LaMarr Card Room (223 C Street)

1965	Tasty Food Café (restaurant) (215 C Street), Uneda (market) (219 C Street)
1970-2009	Little America Westgate Hotel (1055 2 <sup>nd</sup> Avenue)
<b>Block C (see Historical Site Land Use section above) (West)</b>	

A summary table of historical Site vicinity land uses and features of potential concern is presented below. This summary is based on the historical research described above.

Addresses	Dates	Tenants
122 A Street	1945-1965	Used car sales
101 West B Street	1940-1960	Gasoline service station
202 B Street	1926-1955	Tire repair/automotive supply/paint store
102 West C Street	1926-1950	Automotive sales and service/Auto supply
250 C Street	1945-1960	Gasoline service station
1220-1224 Front Street	1930-1955	Automotive repair/sales and service Tire repair
1057 1 <sup>st</sup> Avenue	1930-1936	Gasoline service station
1132-1134 1 <sup>st</sup> Avenue	1926-1940	Automotive repair
1152 1 <sup>st</sup> Avenue	1926-1940	Automotive repair
1302 1 <sup>st</sup> Avenue	1930-1965	Automobile sales and service
1127 2 <sup>nd</sup> Avenue	1926-1930	Automotive repair
1143-1145 2 <sup>nd</sup> Avenue	1926-1940	Automotive sales and repair/tire repair

With the exceptions of the possible presence of burn ash and/or metal-bearing fill (discussed above), and historical facilities known to store/use hazardous materials or petroleum products and generate hazardous waste (e.g., gasoline service stations, automotive repair shops, automotive sales and service facilities, and paint stores), no obvious historical facilities, features of concern, or land uses indicative of the use, storage, or generation of hazardous materials/wastes or petroleum products were found in the historical resources reviewed.

#### Historical Gasoline Service Station/Automotive Repair/Automotive Sales and Service Facility/Paint Store

Based on a review of historical resources, a gasoline service station, automotive repair shops, automotive sales and service facilities, and paint stores were reported to have been located adjacent to the Site. Based on a review of other available sources (e.g., regulatory databases), no information was available in connection with the operations of these facilities. Based on the limited information available regarding possible releases from these facilities, SCS is unable to assess the likelihood that a recognized environmental condition exists at the Site as a result of the reported presence of historical gasoline service station, automotive repair shops, and automotive sales and service facilities adjacent to the Site. However, there is a potential that releases from

this facilities may have migrated onto the Site resulting in a potential historical recognized environmental condition at the Site.

For convenience, these features of potential concern have been summarized in an attachment (Summary Table of Known and Suspected Recognized Environmental Conditions) included in the Appendix.

As noted in the Historical Land Use section above, these qualitative opinions are based on SCS's experience and professional judgment. Our confidence in these opinions is limited and actual Site conditions may be different. Should you desire a confirmation of, or a higher degree of confidence in, these opinions, subsurface assessment activities would be required.

## DATA GAPS IN CONNECTION WITH THE HISTORICAL SITE LAND USE

Readily available historical information was not available prior to 1921. Thus, we are unable to determine the Site usage from the date of first development as recommended by ASTM. Based on our experience and available historical information, the Site was interpreted to have possibly first been developed for residential use.

### Findings and Opinions—Historical Site and Site Vicinity Land Use

Based on a review of historical resources and with the possible exceptions below, there is a low likelihood that a recognized environmental condition exists at the Site as a result of a release of hazardous materials/wastes or petroleum products from a known or interpreted historical Site or Site vicinity land use.

Please note that extensive earth work, possibly including soil export, was done as part of the development of the Civic Center Complex. The nature and extent of these development activities and the mitigation or effect o release(s), if any, from historical land use is unknown. In addition, due to the Site's developed status, Phase II investigation to assess possible historical releases may not be practical (e.g., due to building footprint and foundations). In making our Phase II recommendations, we have not attempted to account for these issues, but have instead assumed they could be addressed during a subsequent design phase.

#### Historical Automotive Dismantling Activities

Automotive dismantling activities reportedly occurred at the Site from prior to 1926 to prior to 1936. Based on SCS's experience, auto dismantling and recycling operations, particularly those on unpaved areas, which we believe may have been the case for the Site, frequently, have releases to soil as a result of day-to-day operations. Releases and soil impacts from gasoline, oil, solvents, antifreeze, and metals are not uncommon. And, while heavy end petroleum and metals impacts of the upper several feet of soil are common, it is not unusual to see impacts to deeper soil horizons, and groundwater if it is shallow. These impacts are usually associated with more mobile constituents of concern such as gasoline and solvents. Regulatory records are not available for this time period and SCS is unable to assess the potential that releases have

**Findings and Opinions—Historical Site and Site Vicinity Land Use**

occurred and a recognized environmental condition is present at the Site. SCS recommends a Phase II investigation and/or soil vapor survey to assess the possible presence and extent of releases, if any, as a result of this land use.

Historical Gasoline Service Station/Automotive Repair/Automotive Sales and Service Facility

Based on a review of historical resources, several gasoline service stations, automotive repair shops, and automotive sales and service facilities were reported to have been located at or adjacent to the Site. Based on SCS's experience, features of potential concern (FOPCs) (e.g., inground hydraulic lifts, wastewater clarifiers systems, and USTs) were often installed at gasoline service stations and automotive sales and service/repair facilities. Solvents, including chlorinated solvents, were often used to degrease parts and in automotive painting/auto body repair. Based on the number of reported facilities and our experience, there is a moderate likelihood of a recognized environmental condition as a result of these historical land uses

To assess whether any FOPCs are present in association with the former gas station or automotive service activities, SCS's recommends a geophysical survey be conducted in an attempt to determine whether any inground structures, such as USTs or lifts, remain at the Site. A Phase II soil and soil vapor sampling is recommended assess the possible presence and extent of releases, if any, as a result of this land use. Additional detailed research at the San Diego Historical Society may help target these investigations.

Dry Cleaning Facilities and Dye Works

Based on a review of historical resources, dry cleaning facilities and dye works were reportedly located at the Site from prior to circa 1926 to circa 1955. No regulatory records were available for the dry cleaning, which would indicate the use of hazardous materials, generation of hazardous waste, or releases to the subsurface. However, based on SCS's experience, dry cleaning facilities use and store tetrachloroethylene (PCE) or other halogenated solvents, generate waste (such as still bottoms and used filters), and often experience releases (to the soil, soil vapor, and/or groundwater). Although, based on available data, there are no known and reported releases in connection with these facilities, in our experience, over time, halogenated solvents have the ability to permeate flooring and building foundations, entering the subsurface soils and potentially impacting groundwater. It is not known what time frame is required for this to occur. Many property owners and lenders have established a Phase II sampling "trigger" of 5 to 7 years (i.e., dry cleaning facilities in operation over 5 years are subject to Phase II subsurface testing for the presence of PCE), and in some cases, as little as two years. Dry cleaners are interpreted to have occupied the Site for approximately 30 years. SCS believes that if these facilities used PCE, there is a moderate to high likelihood subsurface impacts and related recognized environmental conditions, given the tendency for PCE to migrate through flooring and building foundations.

SCS recommends the collection and analysis of soil vapor samples for volatile organic compounds (VOCs). If VOCs are present in the shallow soil vapor, a health risk assessment should be performed to assess possible health risk for future building occupants. If extensive

**Findings and Opinions—Historical Site and Site Vicinity Land Use**

grading, soil excavation, or off-Site soil export will occur during the proposed redevelopment of the Site, we recommend the collection and analysis of soil samples for VOCs (e.g., to protect workers and to know if exported soil will have to be handled and disposed of as a hazardous waste).

**Burn Ash and/or Metal-bearing Fill**

Based on SCS's experience with properties in older neighborhoods of San Diego, there is the potential for burned or incinerated ash from "backyard" incinerators or "burn pits" or metal-bearing fill material to be present or mixed with the soil. Burn ash and/or metal-bearing fill material may contain high concentrations of contaminants of concern, notably certain metals (e.g., copper, lead, zinc, mercury, and cadmium).

The Site was not interpreted to be located in the vicinity of any of the listed refuse dump. More typical is the use of vacant and residential properties for this purpose. Based on SCS's historical review, the Site was interpreted to be developed with a vacant land and single-family residences from circa prior to 1887 to 1921.

Please note that based on a review of the DEH files, a voluntary assistance program was reportedly completed for a facility located 0.18 mile northwest of the Site (1550 Front Street) after burn ash and/or metal-bearing fill were encountered during a Phase II Environmental Site Assessment. A total of 99 tons of lead and burn ash were reportedly removed from the facility.

The enforcement actions requiring the remediation of burn ash or metal-bearing fill material at a property are typically driven by redevelopment and/or excavation as well as potential exposure concerns, if present. The Site grounds were observed to be covered with Site buildings and pavement. If the soil beneath the Site did contain burn ash or metal-bearing fill material, SCS judges that under normal circumstances (i.e., no redevelopment or excavation), there are no complete exposure pathways for direct dermal contact with, or ingestion or inhalation of, burn ash or metal-bearing fill material (if present).

Based on SCS's experience, if burn ash is present in soil, the likelihood of an enforced remediation is low as long as the soil remains undisturbed. However, if the burn ash or metal-bearing fill material is present, it would typically be considered a waste management issue if disturbed or particularly if the soil is exported. Any such disturbed materials should be handled, possibly as a hazardous waste, in accordance with appropriate laws and regulations.

Based on the available data, SCS is unable to assess with confidence the likelihood that burn ash or metal-bearing fill material is present at the Site. However, we understand that the Site might be redeveloped including soil possible soil excavation.

If the Site is ever redeveloped (i.e., including the excavation and export of soil), SCS recommends a trenching or drilling program including soil sampling be conducted to assess the presence and volume of impacted soil, if any. Such assessment would ideally involve sampling

**Findings and Opinions—Historical Site and Site Vicinity Land Use**

of the shallow subsurface soil at the Site and subsequent analysis for typical burn ash constituents. Note, to the extent a geotechnical or fault investigational program will be conducted at the Site, SCS has found that a burn ash and/or metal-bearing fill material investigations can be efficiently combined with trenching for geotechnical purposes.

## 5 CONCLUSIONS AND RECOMMENDATIONS

This Assessment has been conducted by an environmental professional whose qualifications<sup>8</sup> were made known to the Client. The conclusions and recommendations presented below are based on the review of readily available data obtained as part of this Assessment, current regulatory guidelines, the Site and Site vicinity reconnaissance, and SCS's experience.

SCS has performed a Phase I Environmental Site Assessment of Assessor Parcel Number (APN) 533-433-28 (with the Exception of the Civic Theater Portion of the Parcel Adjacent to Third Avenue,) 1222 1st Avenue and 202 C Street, San Diego, California in general conformance with the American Society for Testing and Materials (ASTM) Standard Practice for Phase I Environmental Site Assessment Process E 1527-05 and the Environmental Protection Agency (EPA), 40 CFR Part 312, Standards and Practices for All Appropriate Inquiries; Final Rule (AAI). Any exceptions to, or deletions from, the ASTM and AAI Scope of Work were previously described in this Report where applicable.

A summary of the conclusions and recommendations are also included in the Summary Table of Known and Suspected Recognized Environmental Conditions included in the report after the figures. Please note that extensive earth work, possibly including soil export, was done as part of the development of the Civic Center Complex. The nature and extent of these development activities and the mitigation or effect o release(s), if any, from historical land use is unknown. In addition, due to the Site's developed status, Phase II investigation to assess possible historical releases may not be practical (e.g., due to building footprint and foundations). In making our Phase II recommendations, we have not attempted to account for these issues, but have instead assumed they could be addressed during a subsequent design phase.

With the possible exceptions below, there is a low likelihood that recognized environmental conditions are present at the Site as a result of the current or historical Site land use or from a known and reported off-Site source.

- Diesel Fuel Release

As noted in the DEH File Review section above, diesel fuel releases from the UST adjacent to Building A (1222 1<sup>st</sup> Avenue) was reported. Based on the reported releases in

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<sup>8</sup> SCS declares that, to the best of our professional knowledge and belief, the reviewer meets the definition of Environmental Professional as defined in §312.10 of 40 CFR 312 and we have the specific qualifications based on education, training, and experience to assess a property of the nature, history, and setting of the subject property. We have developed and performed the All Appropriate Inquiries in conformance with the standards and practices set forth in 40 CFR Part 312. The qualifications of the report preparers are included in the Appendix.

connection with the UST system, there is a potential that a current recognized environmental condition may exist at the Site. In addition, SCS understands that the Site may be redeveloped. If this UST is removed in connection with redevelopment, there is a potential that diesel-bearing soil may be encountered during redevelopment. SCS recommends the collection of soil samples (to determine if a release has occurred). In addition, based on their interpreted location (adjacent to Building A), SCS recommends the collection of soil samples (to determine if a release has occurred) and soil vapor samples (to determine the potential for health risk due to possible vapor intrusion into Building A).

- Historical Automotive Dismantling Activities

Automotive dismantling activities reportedly occurred at the Site from prior to 1926 to prior to 1936. Based on SCS's experience, auto dismantling and recycling operations, particularly those on unpaved areas, which we believe may have been the case for the Site, frequently, have releases to soil as a result of day-to-day operations. Releases and soil impacts from gasoline, oil, solvents, antifreeze, and metals are not uncommon. And, while heavy end petroleum and metals impacts of the upper several feet of soil are common, it is not unusual to see impacts to deeper soil horizons, and groundwater if it is shallow. These impacts are usually associated with more mobile constituents of concern such as gasoline and solvents. Regulatory records are not available for this time period and SCS is unable to assess the potential that releases have occurred and a recognized environmental condition is present at the Site. SCS recommends a Phase II investigation and/or soil vapor survey to assess the possible presence and extent of releases, if any, as a result of this land use.

- Historical Gasoline Service Station/Automotive Repair/Automotive Sales and Service Facility

Based on a review of historical resources, several gasoline service stations, automotive repair shops, and automotive sales and service facilities were reported to have been located at or adjacent to the Site. Based on SCS's experience, features of potential concern (FOPCs) (e.g., inground hydraulic lifts, wastewater clarifiers systems, and USTs) were often installed at gasoline service stations and automotive sales and service/repair facilities. Solvents, including chlorinated solvents, were often used to degrease parts and in automotive painting/auto body repair. Based on the number of reported facilities and our experience, there is a moderate likelihood of a recognized environmental condition as a result of these historical land uses.

To assess whether any FOPCs are present in association with the former gas station or automotive service activities, SCS recommends a geophysical survey be conducted in an attempt to determine whether any inground structures, such as USTs or lifts, remain at the Site. A Phase II soil and soil vapor sampling is recommended to assess the possible presence and extent of releases, if any, as a result of this land use. Additional detailed research at the San Diego Historical Society may help target these investigations.

- Dry Cleaning Facilities and Dye Works

Based on a review of historical resources, dry cleaning facilities and dye works were reportedly located at the Site from prior to circa 1926 to circa 1955. No regulatory records were available for the dry cleaning, which would indicate the use of hazardous materials, generation of hazardous waste, or releases to the subsurface. However, based on SCS's experience, dry cleaning facilities use and store tetrachloroethylene (PCE) or other halogenated solvents, generate waste (such as still bottoms and used filters), and often experience releases (to the soil, soil vapor, and/or groundwater). Although, based on available data, there are no known and reported releases in connection with these facilities, in our experience, over time, halogenated solvents have the ability to permeate flooring and building foundations, entering the subsurface soils and potentially impacting groundwater. It is not known what time frame is required for this to occur. Many property owners and lenders have established a Phase II sampling "trigger" of 5 to 7 years (i.e., dry cleaning facilities in operation over 5 years are subject to Phase II subsurface testing for the presence of PCE), and in some cases, as little as two years. Dry cleaners are interpreted to have occupied the Site for approximately 30 years. SCS believes that if these facilities used PCE, there is a moderate to high likelihood subsurface impacts and related recognized environmental conditions, given the tendency for PCE to migrate through flooring and building foundations.

SCS recommends the collection and analysis of soil vapor samples for volatile organic compounds (VOCs). If VOCs are present in the shallow soil vapor, a health risk assessment should be performed to assess possible health risk for future building occupants. If extensive grading, soil excavation, or off-Site soil export will occur during the proposed redevelopment of the Site, we recommend the collection and analysis of soil samples for VOCs (e.g., to protect workers and to know if exported soil will have to be handled and disposed of as a hazardous waste).

- Burn Ash and/or Metal-bearing Fill

Based on SCS's experience with properties in older neighborhoods of San Diego, there is the potential for burned or incinerated ash from "backyard" incinerators or "burn pits" or metal-bearing fill material to be present or mixed with the soil. Burn ash and/or metal-bearing fill material may contain high concentrations of contaminants of concern, notably certain metals (e.g., copper, lead, zinc, mercury, and cadmium).

The Site was not interpreted to be located in the vicinity of any of the listed refuse dump. More typical is the use of vacant and residential properties for this purpose. Based on SCS's historical review, the Site was interpreted to be developed with a vacant land and single-family residences from circa prior to 1887 to 1921.

Please note that based on a review of the DEH files, a voluntary assistance program was reportedly completed for a facility located 0.18 mile northwest of the Site (1550 Front Street) after burn ash and/or metal-bearing fill were encountered during a Phase II

Environmental Site Assessment. A total of 99 tons of lead and burn ash were reportedly removed from the facility.

The enforcement actions requiring the remediation of burn ash or metal-bearing fill material at a property are typically driven by redevelopment and/or excavation as well as potential exposure concerns, if present. The Site grounds were observed to be covered with Site buildings and pavement. If the soil beneath the Site did contain burn ash or metal-bearing fill material, SCS judges that under normal circumstances (i.e., no redevelopment or excavation), there are no complete exposure pathways for direct dermal contact with, or ingestion or inhalation of, burn ash or metal-bearing fill material (if present).

Based on SCS's experience, if burn ash is present in soil, the likelihood of an enforced remediation is low as long as the soil remains undisturbed. However, if the burn ash or metal-bearing fill material is present, it would typically be considered a waste management issue if disturbed or particularly if the soil is exported. Any such disturbed materials should be handled, possibly as a hazardous waste, in accordance with appropriate laws and regulations.

Based on the available data, SCS is unable to assess with confidence the likelihood that burn ash or metal-bearing fill material is present at the Site. However, we understand that the Site might be redeveloped including soil possible soil excavation.

If the Site is ever redeveloped (i.e., including the excavation and export of soil), SCS recommends a trenching or drilling program including soil sampling be conducted to assess the presence and volume of impacted soil, if any. Such assessment would ideally involve sampling of the shallow subsurface soil at the Site and subsequent analysis for typical burn ash constituents. Note, to the extent a geotechnical or fault investigational program will be conducted at the Site, SCS has found that a burn ash and/or metal-bearing fill material investigations can be efficiently combined with trenching or drilling for geotechnical purposes.

## 6 REPORT USAGE AND FUTURE SITE CONDITIONS

This Report is intended for the sole usage of the Client and other parties designated by SCS. The methodology used during this Assessment was in general conformance with the requirements of the Client and the specifications and limitations presented in the Agreement between the Client and SCS. This Report contains information from a variety of public and other sources, and SCS makes no representation or warranty about the accuracy, reliability, suitability, or completeness of the information. Any use of this Report, whether by the Client or by a third party, shall be subject to the provisions of the Agreement between the Client and SCS. Any misuse of or reliance upon the Report shall be without risk or liability to SCS.

Phase I Environmental Site Assessments are qualitative, not comprehensive, in nature and may not identify all environmental problems or eliminate all risk. For every property, but especially for properties in older downtown or urban areas, it is possible for there to be unknown,

unreported recognized environmental conditions, underground storage tanks, or other features of concern that might become apparent through demolition, construction, or excavation activities, etc. In addition, the scope of services for this project was limited to those items specifically named in the scope of services for this Report. Environmental issues not specifically addressed in the scope of services for this project are not included in this Report.

Land use, condition of the properties within the Site, and other factors may change over time. The information and conclusions of this Report are judged to be relevant at the time the work described in this Report was conducted. This Report should not be relied upon to represent future Site conditions unless a qualified consultant familiar with the practice of Phase I Environmental Site Assessments in San Diego County is consulted to assess the necessity of updating this Report.

The property owners at the Site are solely responsible for notifying all governmental agencies and the public of the existence, release, or disposal of any hazardous materials/wastes or petroleum products at the Site, whether before, during, or after the performance of SCS services. SCS assumes no responsibility or liability for any claim, loss of property value, damage, or injury that results from hazardous materials/wastes or petroleum products being present or encountered within the Site.

Although this Assessment has attempted to assess the likelihood that the Site has been impacted by a hazardous material/waste release, potential sources of impact may have escaped detection for reasons that include, but are not limited to: 1) inadequate or inaccurate information rightfully provided to SCS by third parties, such as public agencies and other outside sources; 2) the limited scope of this Assessment; and 3) the presence of undetected, unknown, or unreported environmental releases.

## 7 LIKELIHOOD STATEMENTS

Statements of “likelihood” have been made in this report. Likelihood statements are based on professional judgments of SCS. The term “likelihood,” as used herein, pertains to the probability of a match between the prediction for an event and its actual occurrence. The likelihood statement assigns a measure for a “degree of belief” for the match between the prediction for the event and the actual occurrence of the event.

The likelihood statements in this Report are made qualitatively (expressed in words). The qualitative terms can be approximately related to quantitative percentages. The term “low likelihood” is used by SCS to approximate a percentage range of 10 to 20 percent; the term “moderate likelihood” refers to an approximate percentage range of 40 to 60 percent; and the term “high likelihood” refers to an approximate percentage range of 80 to 90 percent.

## 8 SPECIAL CONTRACTUAL CONDITIONS BETWEEN USER AND ENVIRONMENTAL PROFESSIONAL

There were no special contractual conditions between the user of this Assessment and the environmental professional, SCS.

## 9 ENDNOTES

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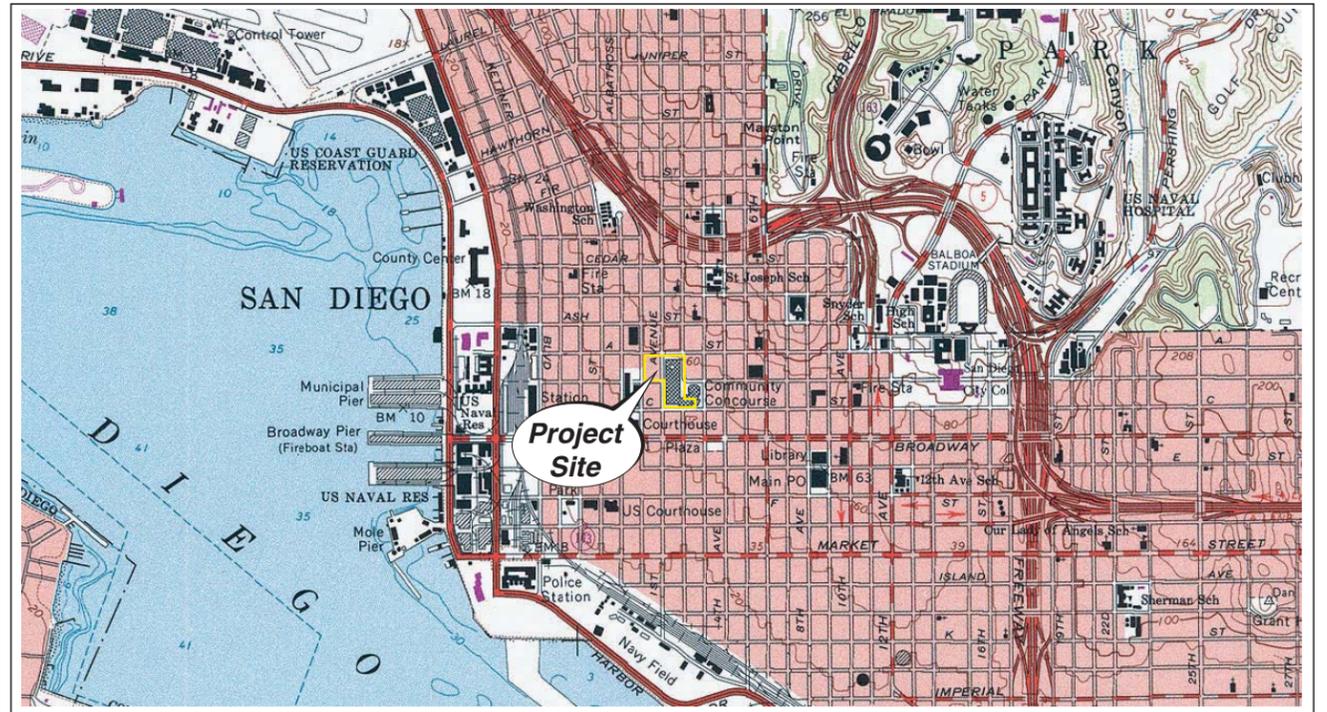
- <sup>i</sup> Site reconnaissance conducted by Harry Bishop and Cristobal Ramirez (SCS) on November 17, 2009.
- <sup>ii</sup> Records request - County of San Diego Department of Environmental Health by Harry Bishop (SCS) on November 6, 2009.
- <sup>iii</sup> Information provided by Client.
- <sup>iv</sup> Information provided by Client.
- <sup>v</sup> Nationwide Environmental Title Research (NETR) report dated December 4, 2009.
- <sup>vi</sup> Site vicinity reconnaissance conducted by Harry Bishop and Cristobal Ramirez (SCS) on November 17, 2009.
- <sup>vii</sup> FirstSearch Technologies Corporation, 2009, Site Assessment Report: Unpublished report prepared for address 202 C Street, dated November 13, 2009.
- <sup>viii</sup> Lawrence Livermore National Laboratory Report, Methyl Tertiary Butyl Ether (MTBE) Impacts to California Groundwater, dated March 25, 1999.
- <sup>ix</sup> Lawrence Livermore National Laboratory Report, Recommendations to Improve the Cleanup Process for California's Leaking Underground Fuel Tanks (LUFTs), dated October 16, 1995.
- <sup>x</sup> California Division of Oil and Gas (DOG) Regional Wildcat Map W1-7, September 19, 1998.

# FIGURES



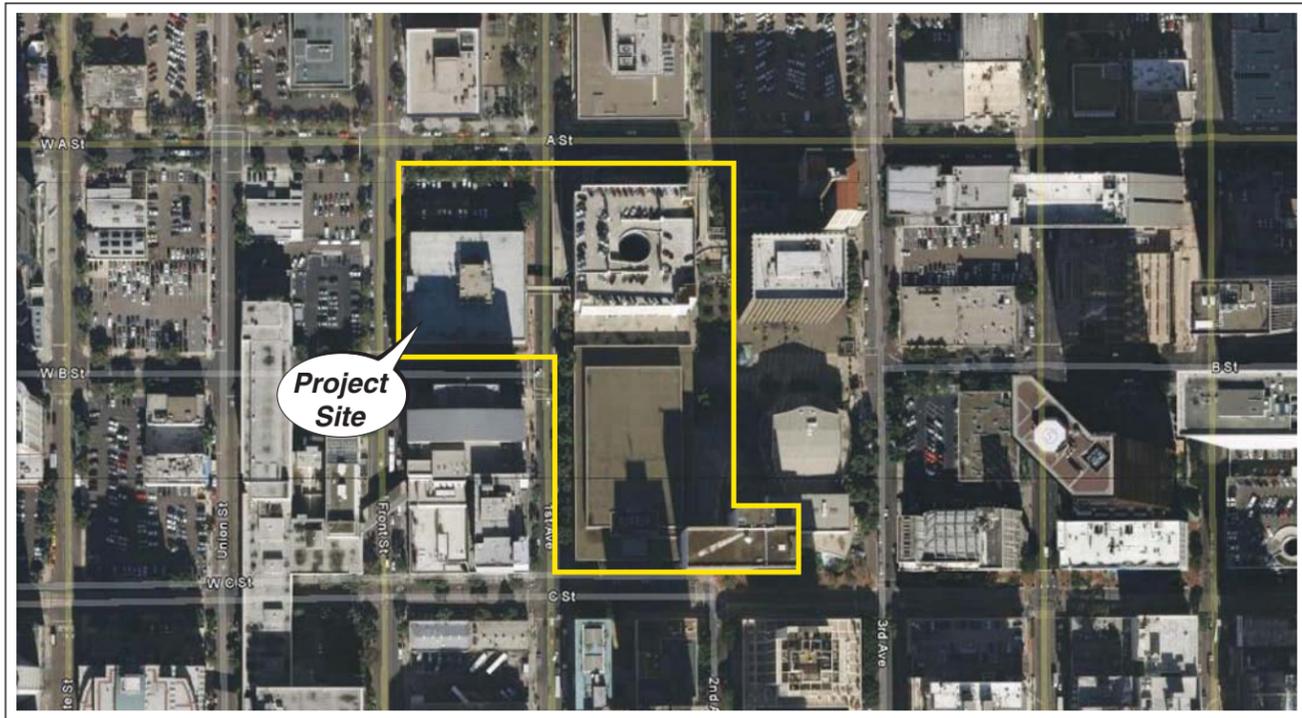


**REGIONAL SITE LOCATION**



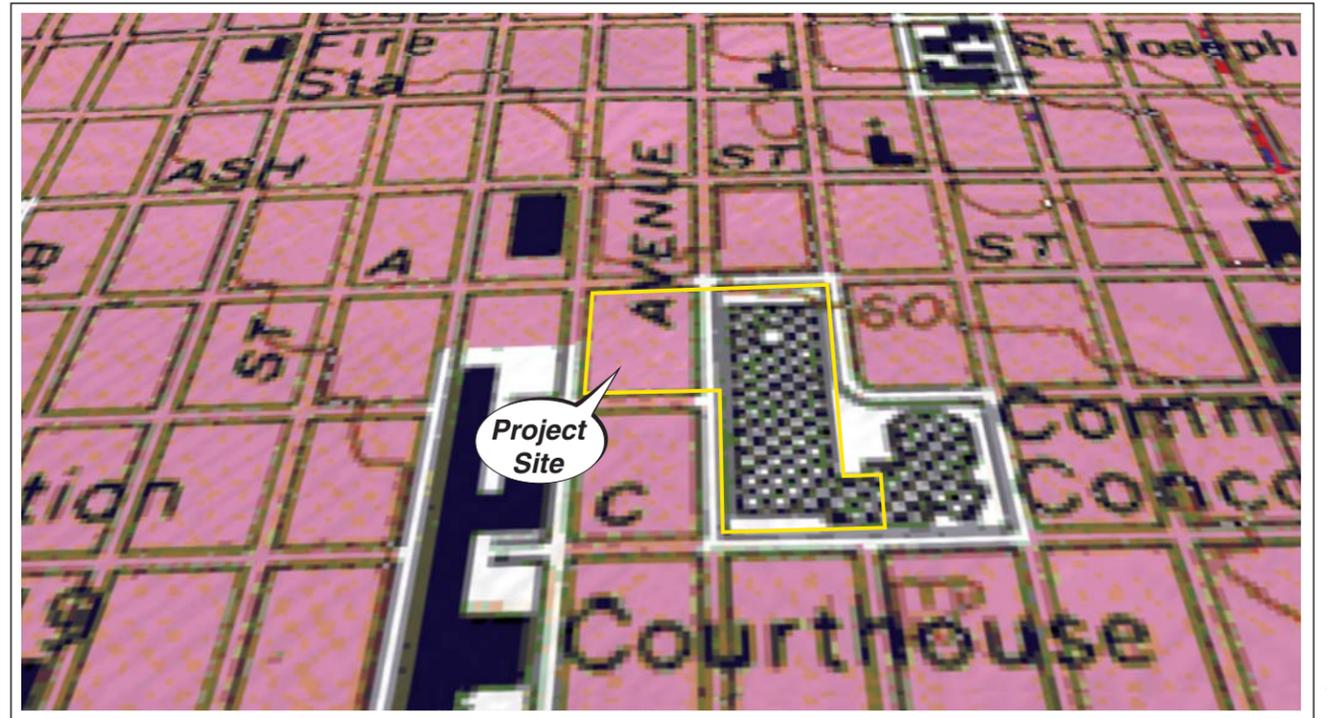
**2-DIMENSIONAL SITE LOCATION**

Reference:  
U.S.G.S. 7.5 Minute Quadrangle map  
Point Loma, California - 1977. Photo revised 1982.



**SITE AERIAL PHOTOGRAPH**

Reference:  
Google Earth Aerial Photograph  
San Diego, California - January 2008



**3-DIMENSIONAL SITE LOCATION**

Reference:  
U.S.G.S. 7.5 Minute Quadrangle map  
Point Loma, California - 1977. Photo revised 1982.

Disclaimer: This figure is based on available data. Actual conditions may differ. All locations and dimensions are approximate.

**SCS ENGINEERS**

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San Diego, California 92123

**4-WAY SITE LOCATION MAP**

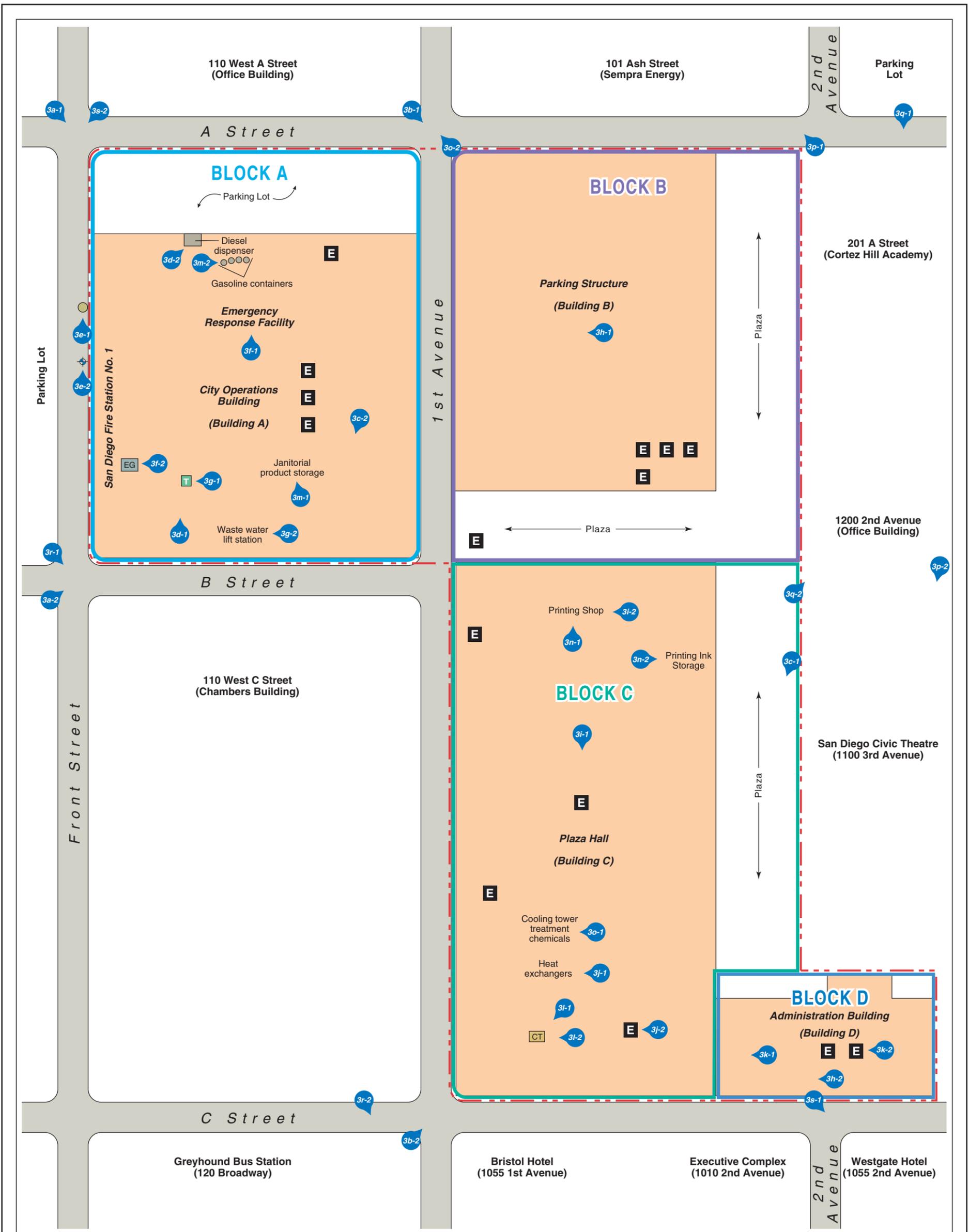
Center City Development Corporation  
Assessor's Parcel Number (APN) 533-433-28, with the  
Exception of the Civic Center Portion of the Parcel  
Adjacent to Third Avenue, A Street, San Diego, California

Project No.:  
01206548.00

**Figure 1**

Date Drafted:  
2/5/10





**EXPLANATION**

- Approximate Site boundary
- T Electrical transformer
- EG Emergency generator
- E Elevators
- CT Cooling tower
- 3b-2 Location and direction of Site photograph
- Underground storage tank fill port
- + Monitoring well



Disclaimer: This figure is based on available data. Actual conditions may differ. All locations and dimensions are approximate.

**SCS ENGINEERS**  
 Environmental Consultants  
 8799 Balboa Avenue, Suite 290  
 San Diego, California 92123

**SITE AND SITE VICINITY PLAN**  
**Center City Development Corporation**  
 Assessor's Parcel Number (APN) 533-433-28, with the Exception of the Civic Center  
 Portion of the Parcel Adjacent to Third Avenue, A Street, San Diego, California

Project No.:  
01206548.00

**Figure 2**

Date Drafted:  
2/5/10





1) View of Block A looking southeast.



2) View of Block A looking northeast.

**SCS ENGINEERS**

Environmental Consultants  
8799 Balboa Avenue, Suite 290  
San Diego, California 92123

**PHOTOGRAPHIC PLATE**  
**Center City Development Corporation**

Assessor's Parcel Number (APN) 533-433-28, with the  
Exception of the Civic Center Portion of the Parcel  
Adjacent to Third Avenue, A Street, San Diego, California

Project No.:  
01206548.00

**Figure 3a**

Date Drafted:  
2/5/10



1) View of Block B looking southeast.



2) View of Block C looking northeast.

**SCS ENGINEERS**

Environmental Consultants  
8799 Balboa Avenue, Suite 290  
San Diego, California 92123

**PHOTOGRAPHIC PLATE**  
**Center City Development Corporation**

Assessor's Parcel Number (APN) 533-433-28, with the  
Exception of the Civic Center Portion of the Parcel  
Adjacent to Third Avenue, A Street, San Diego, California

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01206548.00

**Figure 3b**

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1) View of Block C/D looking southwest.



2) View of typical office area (Building A).

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Exception of the Civic Center Portion of the Parcel  
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Project No.:  
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**Figure 3c**

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1) Typical view of fire station (Building A).



2) View of diesel dispenser (Building A).

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**Figure 3d**

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1) View of diesel underground storage tank fill port in sidewalk (Building A).



2) View of monitoring well in sidewalk (Building A).

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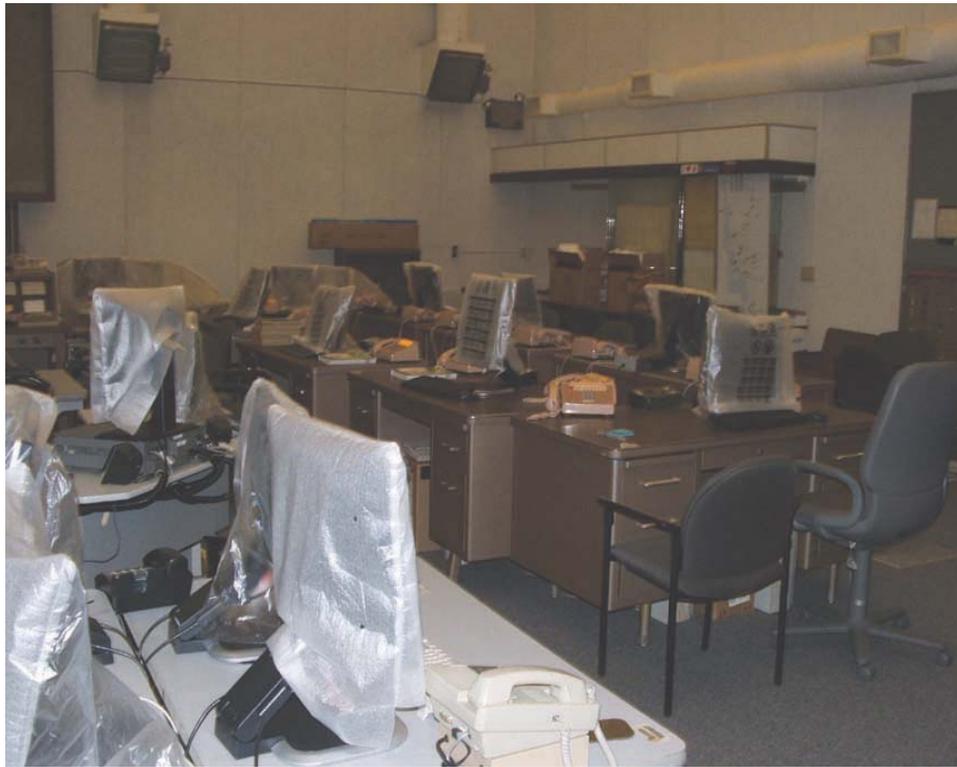
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**Figure 3e**

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1) Typical view of emergency response facility (Building A, basement).



2) View of emergency diesel generator (Building A, basement).

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**Figure 3f**

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1) Typical view of electrical transformer (Building A, basement).



2) View of waste water lift station (Building A, basement).

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**Figure 3g**

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1) Typical view of parking area (Building B).



2) Typical view of lobby (Building D).

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**Figure 3h**

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1) Typical view of convention hall auditorium (Building C).



2) Typical view of printing facility (Building C).

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**Figure 3i**

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2/5/10



1) Typical view of heating/cooling system (Building C, basement).



2) Typical view of elevator room (Building C, basement).

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Exception of the Civic Center Portion of the Parcel  
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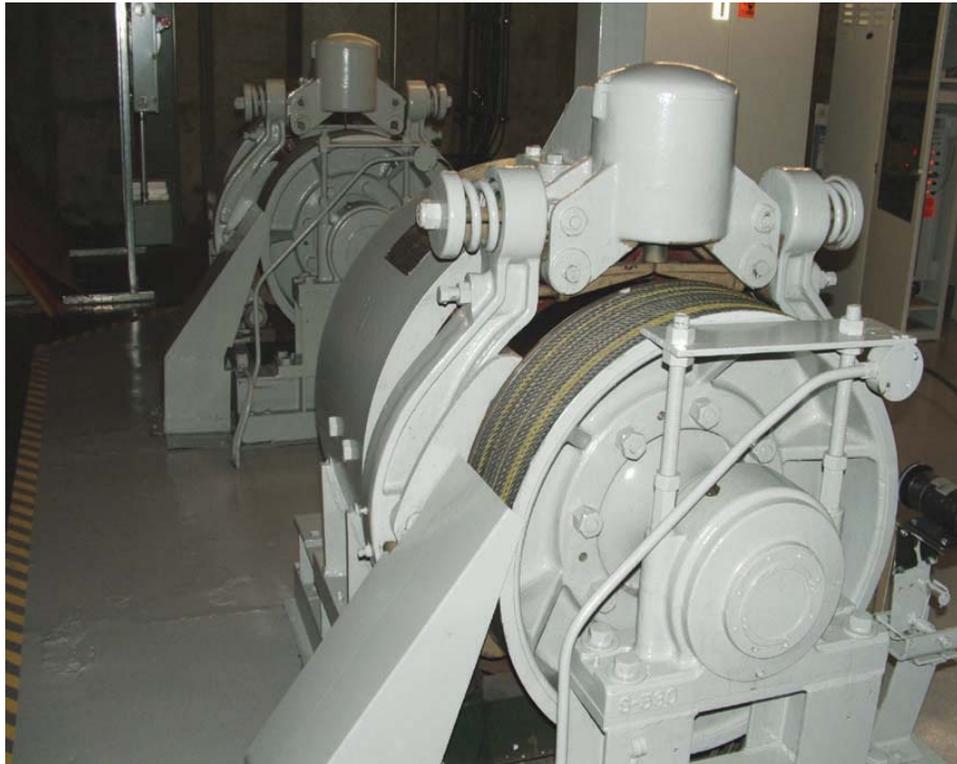
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**Figure 3j**

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2/5/10



1) Typical view of council room (Building D).



2) Typical view of elevator room (Building D, roof).

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**Figure 3k**

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1) Typical view of cooling tower (Building C, roof).



2) View of typical service elevator (Building C, basement).

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**Figure 3I**

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1) Typical view of janitorial product storage (Building A, basement).



2) Typical view of gasoline containers (Building A, fire department).

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**Figure 3m**

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1) Typical view of printing cleaner storage (Building C).



2) Typical view of printing ink storage (Building C).

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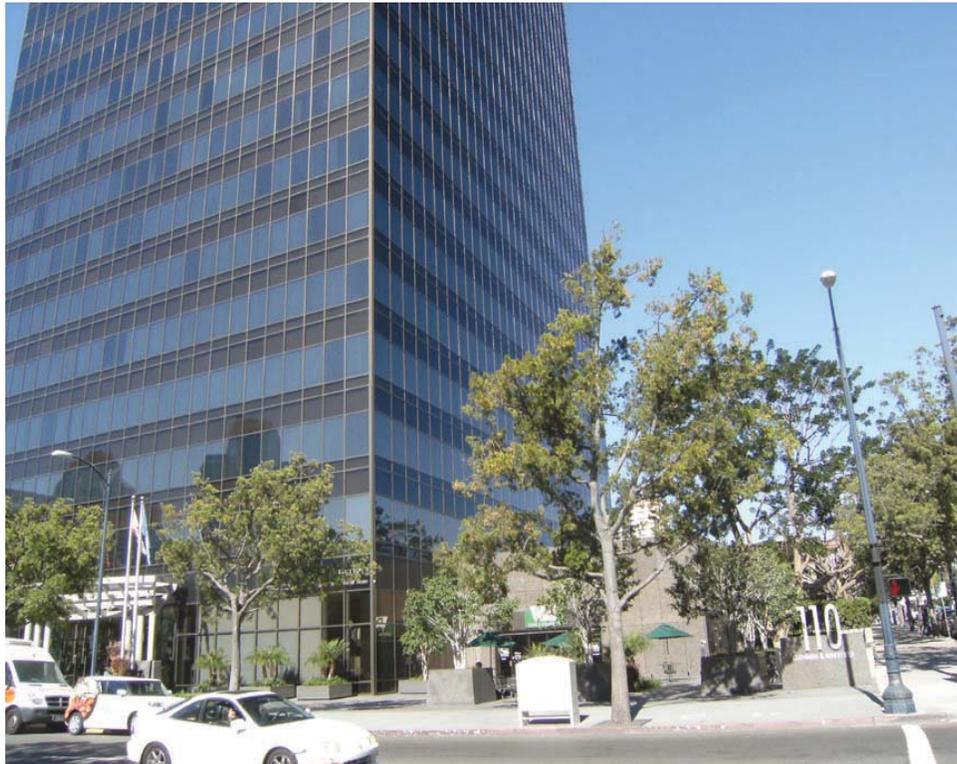
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**Figure 3n**

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1) Typical view of cooling tower treatment chemicals (Building C, basement).



2) View of the adjacent property to the north of the Site (Block A).

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**Figure 3o**

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1) View of the adjacent property to the north of the Site (Block B).



2) View of the adjacent property to the north of the Site (Block D) and east of the Site (Block C).

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**Figure 3p**

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1) View of the adjacent property to the east of the Site (Block B).



2) View of the adjacent property to the east of the Site (Block B) and northeast of the Site (Block C).

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**Figure 3q**

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1) View of the adjacent property to the south of the Site (Block A) and west of the Site (Block C).



2) View of the adjacent property to the south of the Site (Block C).

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Adjacent to Third Avenue, A Street, San Diego, California

Project No.:  
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**Figure 3r**

Date Drafted:  
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1) View of the adjacent property to the south of the Site (Block D).



2) View of the adjacent property to the west of the Site (Block A).

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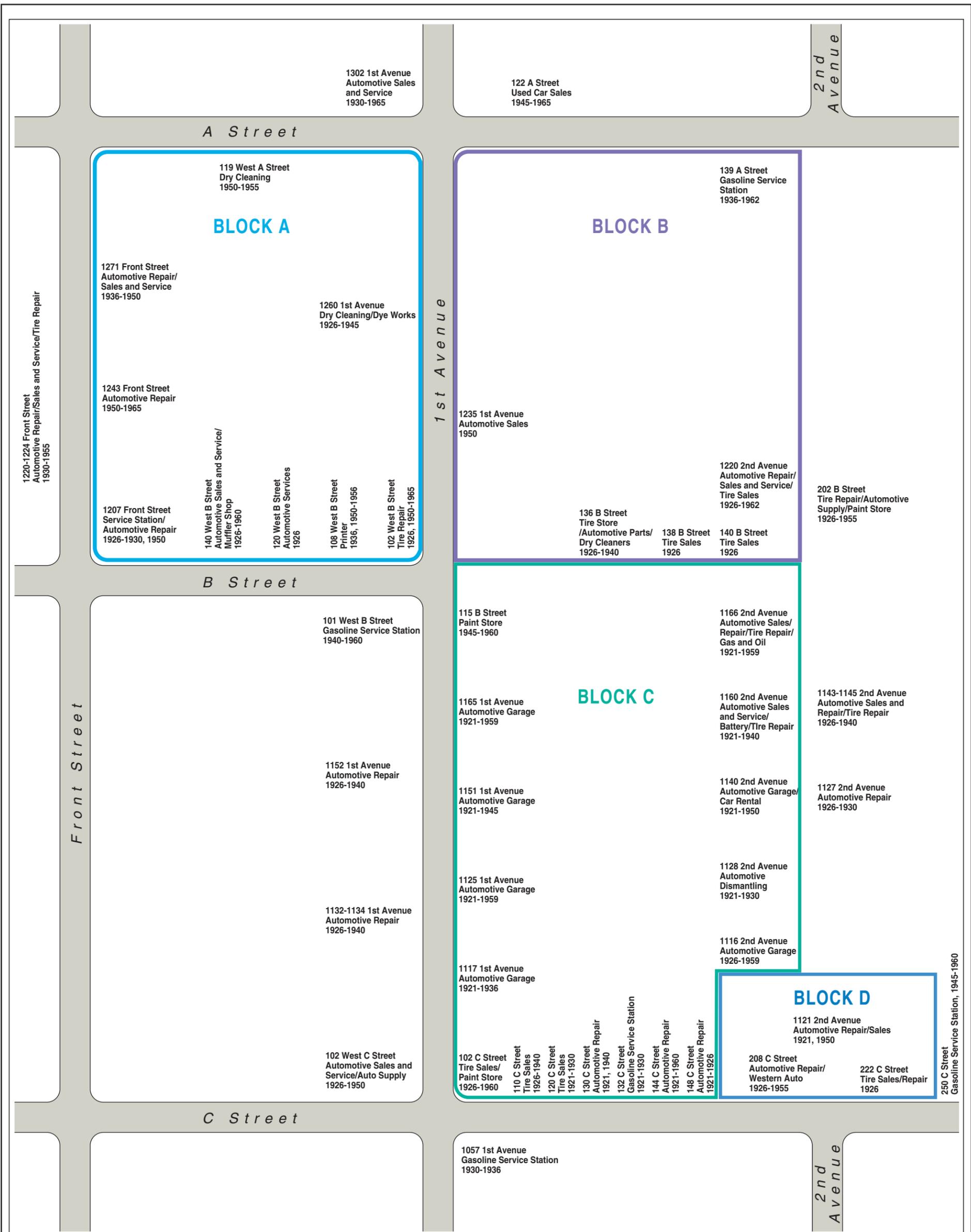
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Exception of the Civic Center Portion of the Parcel  
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Project No.:  
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**Figure 3s**

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**EXPLANATION**

Please note that the historical features of potential environmental concern were located on this figure based on a review of Sanborn Fire Insurance maps and interpreted locations based on facility and address listings in historical city directories. Since locations were provided by historical maps or interpreted by historical address listings, the facility locations depicted above may not be accurate and should be considered approximate.



Disclaimer: This figure is based on available data. Actual conditions may differ. All locations and dimensions are approximate.

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**HISTORICAL FEATURES OF POTENTIAL CONCERN**

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Assessor's Parcel Number (APN) 533-433-28, with the Exception of the Civic Center  
Portion of the Parcel Adjacent to Third Avenue, A Street, San Diego, California

Project No.:  
01206548.00

**Figure 4**

Date Drafted:  
2/5/10

# **TABLES**



**TABLE 1  
SUMMARY TABLE OF KNOWN OR SUSPECTED RECOGNIZED ENVIRONMENTAL CONDITIONS**

The following table presents a summary of the recognized environmental conditions identified and discussed in this Report.

Issue	Potential Contaminant(s)/ Inground Structures	Likelihood of Recognized Environmental Condition (REC) or Historical REC (HREC)				Comment	Recommendation
		Potential	Low	Moderate	High		
Possible release from a current underground storage tank (UST) system adjacent to Building A (1222 1 <sup>st</sup> Street)	Contaminants: diesel fuel	✓				Based on the reported releases (e.g., disposal violation) in connection with the UST system, there is a potential that a current recognized environmental condition may exist at the Site. In addition, SCS understands that the Site may be redeveloped. If this UST is removed in connection with redevelopment, there is a potential that diesel-bearing soil may be encountered during redevelopment.	SCS recommends the collection of soil samples (to determine if a release has occurred) and soil vapor samples (to determine the potential for health risk due to possible vapor intrusion into Building A).
<p>Automotive repair shops, painting and body shops, automotive sales and service, and tire repair shops were reportedly historically located at the Site as follows:</p> <p><u>Block A</u></p> <p>1243 Front Street      1950-1965  1271 Front Street      1936-1950  102 West B Street      1926, 1950-1965  120 West B Street      1926  140 West B Street      1926-1960</p> <p><u>Block B</u></p> <p>136 B Street            1926-1940  138 B Street            1926  140 B Street            1926  1220 2<sup>nd</sup> Avenue        1926-1962  1235 1<sup>st</sup> Avenue        1950</p> <p><u>Block C</u></p> <p>102 C Street            1926-1960  110 C Street            1926-1940  120 C Street            1921-1930  130 C Street            1921, 1940  144 C Street            1921-1960  148 C Street            1921-1926  1117 1st Avenue        1921-1936  1125 1st Avenue        1921-1959  1151 1st Avenue        1921-1945  1165 1st Avenue        1921-1959  1116 2<sup>nd</sup> Avenue        1921-1959  1140 2<sup>nd</sup> Avenue        1921-1950  1160 2<sup>nd</sup> Avenue        1921-1940  1166 2<sup>nd</sup> Avenue        1921-1959</p>	<p>Contaminants: gasoline, waste oil, solvents (petroleum and halogenated)</p> <p>Structures: USTs, inground lifts, inground clarifiers, parts cleaning units</p>			✓	✓	Based on SCS's experience, features of potential concern (FOPCs) (e.g., inground hydraulic lifts, wastewater clarifiers systems, USTs, and parts cleaning units) were often installed at automotive sales and service/repair facilities/tire shops and solvents, including chlorinated solvents were often used to degrease parts and in automotive painting/auto body repair facilities	To assess whether any FOPCs are present in association with the former automotive service activities, SCS's recommends a geophysical survey be conducted in an attempt to determine whether any inground structures, such as USTs or lifts, remain at the Site. A Phase II soil and soil vapor sampling is recommended assess the possible presence and extent of releases, if any, as a result of this land use.

Issue	Potential Contaminant(s)/ Inground Structures	Likelihood of Recognized Environmental Condition (REC) or Historical REC (HREC)				Comment	Recommendation
		Potential	Low	Moderate	High		
<p><u>Block D</u>  1121 2<sup>nd</sup> Avenue 1921, 1950  208 C Street 1926-1955  222 C Street 1926</p>							
<p>Gasoline service stations were reportedly historically located at the Site as follows:  1207 Front Street 1926-1930, 1950 (Block A)  139 A Street 1936 -1962 (Block B)  132 C Street 1921-1930 (Block C)  1166 2<sup>nd</sup> Avenue 1921-1959 (Block C)  (gas and oil dispenser)</p>	<p>Contaminants:  gasoline, waste oil, solvents (petroleum and halogenated)  Structures:  Underground storage tanks (USTs), inground lifts, inground clarifiers, parts cleaning units</p>			✓	✓	<p>Based on SCS's experience, features of potential concern (FOPCs) (e.g., inground hydraulic lifts, wastewater clarifiers systems, USTs, and parts cleaning units) were often installed at gasoline service stations.</p>	<p>To assess whether any FOPCs are present in association with the former gas station, SCS's recommends a geophysical survey be conducted in an attempt to determine whether any inground structures, such as USTs or lifts, remain at the Site. A Phase II soil and soil vapor sampling is recommended assess the possible presence and extent of releases, if any, as a result of this land use.</p>
<p>Facilities known to likely store and use solvents and generate solvent wastes (e.g., paint stores, printing shops, and dry cleaners) were reportedly historically located at the Site as follows:  <u>Block A</u>  1260 1st Avenue 1926-1945  119 West A Street 1950-1955  108 West B Street 1936, 1950-1956  <u>Block B</u>  136 B Street 1926-1940  <u>Block C</u>  115 B Street 1945-1960  102 C Street 1921-1955</p>	<p>Contaminants:  petroleum and halogenated solvents  Structures:  USTs</p>			✓	✓	<p>Based on SCS's experience, these facilities typical stored and used halogenated solvents (including tetrachloroethylene [PCE] at dry cleaning facilities), generate halogenated waste (such as still bottoms and used filters), and often experience releases (to the soil, soil vapor, and/or groundwater). Although, based on available data, there are no known and reported releases in connection with these facilities, in our experience, over time, halogenated solvents have the ability to permeate flooring and building foundations, entering the subsurface soils and potentially impacting groundwater.</p>	<p>SCS recommends the collection and analysis of soil vapor samples for volatile organic compounds (VOCs). If VOCs are present in the shallow soil vapor, a health risk assessment should be performed to assess possible health risk for future building occupants. If extensive grading, soil excavation, or off-Site soil export will occur during the proposed redevelopment of the Site, we recommend the collection and analysis of soil samples for VOCs (e.g., to protect workers and to know if exported soil will have to be handled and disposed of as a hazardous waste).</p>
<p>Automotive dismantling activities reportedly occurred at 1128 2nd Avenue (Block C) from 1921-1930</p>	<p>Contaminants:  gasoline, oil, solvents, antifreeze, and metals  Structures: USTs</p>	✓				<p>Releases and soil impacts from gasoline, oil, solvents, antifreeze, and metals are common. And, while heavy end petroleum and metals impacts of the upper several feet of soil are common, it is not unusual to see impacts to deeper soil horizons. These impacts are usually associated with more mobile constituents of concern such as gasoline and solvents</p>	<p>SCS recommends a Phase II investigation and/or soil vapor survey to assess the possible presence and extent of releases, if any, as a result of this land use.</p>

Issue	Potential Contaminant(s)/ Inground Structures	Likelihood of Recognized Environmental Condition (REC) or Historical REC (HREC)				Comment	Recommendation
		Potential	Low	Moderate	High		
Based on SCS's experience with properties in older neighborhoods of San Diego, there is the potential for burned or incinerated ash from "backyard" incinerators or "burn pits" or metal-bearing fill material to be present or mixed with the soil. Burn ash and/or metal-bearing fill material may contain high concentrations of contaminants of concern, notably certain metals (e.g., copper, lead, zinc, mercury, and cadmium).	Contaminants: Heavy metals (copper, lead, zinc, mercury, and cadmium)	✓				It our understanding that the Site is to be redeveloped and as such, the burn ash or lead-bearing soil, if present, is a potential recognized environment condition and would typically be considered a waste management issue if disturbed or particularly if the soil is exported. Any such disturbed materials should be handled, possibly as a hazardous waste, in accordance with appropriate laws and regulations.	SCS recommends sampling for burn ash and/or metal-bearing fill CoCs if indications of burn ash and/or metal-bearing fill are encountered during redevelopment.
Gasoline service stations, automotive repair shops, automotive sales and service facilities, and paint stores were reported to have been located adjacent to the Site as follows:  122 A Street                    1945-1965 101 West B Street            1940-1960 202 B Street                    1926-1955 102 West C Street            1926-1950 250 C Street                    1945-1960 1220-1224 Front Street      1930-1955 1057 1st Avenue              1930-1936 1132-1134 1st Avenue        1926-1940 1152 1st Avenue              1926-1940 1302 1st Avenue              1930-1965 1127 2nd Avenue              1926-1930 1143-1145 2nd Avenue        1926-1940	Contaminants: gasoline, waste oil, solvents (petroleum and halogenated)	✓				Constituents of concern (CoCs) interpreted to have been used/ generated at these facilities may have been historically released and migrated beneath the Site	SCS recommends the collection and analysis of soil vapor samples for volatile organic compounds (VOCs). If VOCs are present in the shallow soil vapor, a health risk assessment should be performed to assess possible health risk for future building occupants.

Likelihood Statements

Statements of "likelihood" have been made in this report. Likelihood statements are based on professional judgments of SCS. The term "likelihood," as used herein, pertains to the probability of a match between the prediction for an event and its actual occurrence. The likelihood statement assigns a measure for a "degree of belief" for the match between the prediction for the event and the actual occurrence of the event.

The likelihood statements in this Report are made qualitatively (expressed in words). The qualitative terms can be approximately related to quantitative percentages. The term "low likelihood" is used by SCS to approximate a percentage range of 10 to 20 percent; the term "moderate likelihood" refers to an approximate percentage range of 40 to 60 percent; and the term "high likelihood" refers to an approximate percentage range of 80 to 90 percent.

# **Historical Resources Technical Report**

## **San Diego Civic Center Project Historical Assessment**

**1222 1<sup>st</sup> Avenue, 1100–1200 3<sup>rd</sup> Avenue, and  
150–202 C Street  
San Diego, California 92101**

**Prepared for:**

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Centre City Development Corporation  
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**Prepared by:**



**Daniel Paul and David Greenwood, Architectural Historians  
811 West Seventh Street, Suite 800  
Los Angeles, California 90017**

**April 2010**



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## **Acronyms**

- CEQA California Environmental Quality Act
- CFR Code of Federal Regulations
- CRHR California Register of Historical Resources
- DPR California Department of Parks and Recreation
- NRHP National Register of Historic Places
- SDDA San Diego Downtown Association

## Executive Summary

The proposed project calls for the construction of a new city hall, a paseo, and adjacent commercial spaces. The site of the proposed project is currently occupied by the San Diego Convention and Performing Arts Center, a two-level 158,000-square-foot meeting and conference venue; the Community Concourse plaza; and the City of San Diego Administration Building, a 189,000-square-foot, 13-level office building that houses city administrative and legislative functions. Hazardous materials remediation work will occur at both buildings prior to demolition. The site, including the plaza, will be excavated, and a new city hall with below-grade parking, an office tower, and council chambers will be constructed. The Malcolm Leland-designed concrete friezes along the façade of the San Diego Convention and Performing Arts Center will be removed and preserved prior to building demolition. A new use and location (on site or off site) for the friezes will be determined at a future date.

This technical report is a historical resources evaluation of a grouping of resources in and around the San Diego Civic Center. This includes a program of buildings completed by 1965 known as the San Diego Community Concourse (Community Concourse). This grouping of resources is also known as the Charles C. Dail Concourse, in honor of former Mayor Charles C. Dail, who served from 1955 to 1963. The Community Concourse and its associated resources are being evaluated for National Register of Historic Places (NRHP), California Register of Historical Resources (CRHR), and San Diego Historical Landmark listing. The four buildings being evaluated as part of the Community Concourse include the City of San Diego Administration Building (City Hall), San Diego Convention and Performing Arts Center (itself commonly called the San Diego Concourse), the San Diego Civic Theatre, and the Community Concourse Parking Garage (commonly known as the Evan V. Jones Parkade). In addition, two nearby resources that were completed after the Community Concourse are being evaluated. These include the City Operations Building, and the Bow Wave Fountain, located just above the original Community Concourse plaza.<sup>1</sup>

Within the survey area, the resources listed below appear to possess historic significance and for CEQA purposes are considered historical resources:

- For their collective contribution to spearheading a revitalization of downtown San Diego and their association with the group San Diegans, Inc., the buildings and features that make up the Community Concourse appear to be individually eligible under San Diego Historical Landmark Criterion A (“Exemplifies or reflects special elements of the city’s, community’s, or neighborhood’s historical, archeological, cultural, social, economic, political, aesthetic, engineering, landscaping, or architectural development”) and

---

<sup>1</sup> Although the Bow Wave Fountain is commonly perceived to be part of the original Community Concourse, the fountain was completed in 1972 in conjunction with the Security Pacific Bank tower directly north of it.

Criterion B (“Is identified with persons or events significant in local, state, or national history”).

Additionally, for its distinctive form and for possessing expressionist design qualities that were commonly seen on 1960s-era theaters, the San Diego Civic Theatre is also eligible as a San Diego Historical Landmark under Criterion C (“Embodies distinctive characteristics of a style, type, period, or method of construction or is a valuable example of the use of indigenous materials or craftsmanship”)

- For possessing distinctive design qualities, as applied to what is typically a perfunctory building type, the Community Concourse Parking Garage appears to be individually eligible under San Diego Historical Landmark Criterion C (“Embodies distinctive characteristics of a style, type, period, or method of construction or is a valuable example of the use of indigenous materials or craftsmanship”). For being a notable, representative work of the firm of Tucker, Sadler and Bennett, the garage is also eligible as a San Diego Historical Landmark under Criterion D (“Is representative of the notable work of a master builder, designer, architect, engineer, landscape architect, interior designer, artist, or craftsman”). Because of its level of design significance and the rarity of its design features, the garage is also eligible for the CRHR at the local level under Criterion 3 (“Embodies the distinctive characteristics of a type” and “possesses high artistic values”).
- For its association with Malcolm Leland, a prolific and recognized Southern California artist and sculptor of the post–World War II era, the Bow Wave Fountain appears to be eligible under San Diego Historical Landmark Criterion D (“Is representative of the notable work of a master builder, designer, architect, engineer, landscape architect, interior designer, artist, or craftsman”). In addition, the Bow Wave Fountain appears to be CRHR eligible under Criterion 3 because it “represents the work of a master,” the artist Malcolm Leland.

## Introduction

This historical resources technical report evaluates the aforementioned properties according to NRHP criteria, subject to Section 106 of the National Historic Preservation Act (NHPA); CRHR criteria, subject to the California Environmental Quality Act (CEQA); and San Diego Historical Landmark criteria.

### **Significance Criteria—National Register of Historic Places**

Cultural resource significance is evaluated in terms of eligibility for listing in the NRHP. The NRHP significance criteria applied to the cultural resources in this study are defined in 36 Code of Federal Regulations (CFR) 60.4 as follows:

The quality of significance in American history, architecture, archaeology, engineering, and culture is present in districts, sites, buildings, structures, and objects that possess integrity of location, design, setting, materials, workmanship, feeling, and association and (a) are associated with events that have made a significant

contribution to the broad patterns of our history; (b) are associated with the lives of persons significant in our past; (c) embody the distinctive characteristics of a type, period, or method of construction or represent the work of a master or possess high artistic values or represent a significant and distinguishable entity whose components may lack individual distinction; or (d) have yielded, or may be likely to yield, information important in prehistory or history.

For a property to convey its historical significance, it must retain integrity (i.e., the physical qualities or character-defining features that illustrate its significance under NRHP criteria must be intact). Integrity is judged on seven aspects: location, design, setting, workmanship, materials, feeling, and association. These seven factors can be grouped into three types of integrity considerations. Location and setting relate to the relationship between the property and its environment. Design, materials, and workmanship most often apply to historic buildings and relate to construction methods and architectural details. Feeling and association are the least objective criteria, pertaining to the overall ability of the property to convey a sense of the time and place in which it was constructed (U.S. Department of the Interior 1991).

### **Significance Criteria—California Register of Historical Resources**

The State CEQA Guidelines describe three ways in which a property can qualify as a significant historical resource for the purposes of CEQA review: 1) The resource is listed in or determined eligible for listing in the CRHR; 2) the resource is included in a local register of historical resources, as defined in Section 5020.1(k) of the Public Resources Code or identified as significant in a historical resource survey meeting the requirements of Section 5024.1(g) of the Public Resources Code, unless the preponderance of evidence demonstrates that it is not historically or culturally significant; or 3) the lead agency determines the resource to be significant as supported by substantial evidence in light of the whole record (California Code of Regulations, Title 14, Division 6, Chapter 3, Section 15064.5).

The CRHR was created by the state legislature in 1992. It is intended to serve as an authoritative listing of historical and archaeological resources in California. Additionally, the eligibility criteria for the CRHR are intended to serve as the definitive criteria for assessing the significance of historical resources under CEQA, thereby establishing a consistent set of criteria for the evaluation process for all public agencies statewide. For a historical resource to be eligible for listing in CRHR, it must be significant at the local, state, or national level under one or more of the following four criteria:

1. The resource is associated with events that have made a significant contribution to the broad patterns of California's history and cultural heritage;
2. The resource is associated with the lives of persons who were important in our past;
3. The resource embodies the distinctive characteristics of a type, period, region, or method of construction or represents the work of an important creative individual or possesses high artistic values; or

4. The resource yields, has yielded, or may be likely to yield information important in prehistory or history.

### **Significance Criteria—San Diego Historical Landmark**

To be designated as historically significant, the site in question must meet at least one of the following City of San Diego criteria:

- A. Exemplifies or reflects special elements of the city's, a community's, or a neighborhood's historical, archaeological, cultural, social, economic, political, aesthetic, engineering, landscaping, or architectural development;
- B. Is identified with persons or events significant in local, state, or national history;
- C. Embodies distinctive characteristics of a style, type, period, or method of construction or is a valuable example of the use of indigenous materials or craftsmanship;
- D. Is representative of the notable work of a master builder, designer, architect, engineer, landscape architect, interior designer, artist, or craftsman;
- E. Is listed or has been determined eligible by the National Park Service for listing on the National Register of Historic Places or is listed or has been determined eligible by the California State Office of Historic Preservation for listing on the California Register of Historical Resources; and
- F. Is a finite group of resources, related to one another in a clearly distinguishable way, or is a geographically definable area or neighborhood, containing improvements that have a special character, historical interest, or aesthetic value or represent one or more architectural periods or styles in the history and development of the city.

### **Report Organization**

The following is a historical resources technical report for the San Diego Community Concourse and associated resources in the project area and the vicinity of the Community Concourse, which includes the City Operations Building and the Bow Wave Fountain. It begins by briefly describing the physical context of the Community Concourse and the other surveyed resources within the project area.

A large portion of this report is devoted to historic context. This context is divided into three separate theme categories that, together, cover three of the four NRHP and CRHR criteria; archeological resources (Criterion D/4) are not evaluated as part of this report. The first theme focuses on the Community Concourse in relation to design trends of the early 1960s. The next theme concerns downtown San Diego and the role of San Diegans, Inc. The third theme addresses the work of Malcolm Leland, the sculptor who designed the Bow Wave Fountain and contributed artistically to many of the Community Concourse buildings.

Next, the methodology for research and the field survey is explained, followed by a description of all surveyed resources, including the Community Concourse. Following

that are discussions of significance, or lack thereof, for various buildings and associated resources, including the Community Concourse. Finally, the Findings and Conclusions section includes a chart that lists the status code for each resource along with the potential impacts and their mitigation measures. Within the various appendices of the report are building permits, ownership and occupant information, maps, completed Department of Parks and Recreation (DPR) forms, and preparers' qualifications (see Appendices A through F).

### **Personnel**

Daniel Paul, ICF architectural historian, conducted the field visit and research and prepared the context and DPR 523a forms for this project. David Greenwood, ICF architectural historian, was also part of the field visit and research and helped prepare the report. Elizabeth Hilton, ICF architectural historian, assisted in preparing the report.

## **Project Setting**

### **Project Area**

The project area is the San Diego Community Concourse, which is located in the center of downtown San Diego. There are five buildings and a fountain on the subject parcel (533-433-28-00), which has a lot area of 356,320.8 square feet and width of 1,000 feet. The legal description of the parcel is "ST CLDS & BLKS 13 THRU 15 & BLK 1."

The project area is bounded to the north by A Street, to the east by the Community Concourse Parking Garage (Evan V. Jones Parkade) between A Street and B Street, to the north by B Street from the garage to 3<sup>rd</sup> Avenue, to the east by 3<sup>rd</sup> Avenue between B Street and C Street, to the south by C Street between 1<sup>st</sup> Avenue and 3<sup>rd</sup> Avenue, and to the west by 1<sup>st</sup> Avenue. The project area also includes the City Operations Building, which is bounded by A Street, 1<sup>st</sup> Avenue, B Street, and Front Street. The downtown area is located within a grid plan. The square blocks of the downtown area are developed with commercial buildings.

### **Physical Project Setting**

The setting for the Community Concourse is the high-density urban landscape of downtown San Diego, which is commonly known as "Centre City." The Community Concourse is southwest of Balboa Park and Interstate 5, southeast of Lindbergh Field, and east/northeast of San Diego Bay. Within the immediate vicinity is a high concentration of multi-story office buildings. There are also older single-story commercial buildings along C Street, west of 1<sup>st</sup> Avenue. The San Diego Metropolitan Transit System (MTS) Orange Line trolley runs along C Street from America Plaza, through the civic center, to San Diego City College. One- to three-story office buildings are present along A Street, and open public parking lots are located north of A Street, west of Front Street, and east of 3<sup>rd</sup> Avenue. Two older, multi-story commercial buildings are located near the project site, and at the southwest corner of A Street and 3<sup>rd</sup> Avenue is

a 14-story commercial office building that was constructed in 1929. A theater, constructed in 1926, is located at the northeast corner of C Street and 3<sup>rd</sup> Avenue.

### **Project Vicinity**

The Community Concourse is bounded to the north by A Street, to the east by 3<sup>rd</sup> Avenue, to the south by C Street, and to the west by 1<sup>st</sup> Avenue. The Security Pacific Bank tower property, just north of the San Diego Civic Theatre, was not originally part of the Community Concourse and is not within the present survey area. In addition, the Bow Wave Fountain, within the project boundary, was not constructed in conjunction with the Community Concourse but was completed in 1972 with the Security Pacific Bank tower property. Resources within the survey area include the City of San Diego Administration Building (City Hall); San Diego Convention and Performing Arts Center; San Diego Civic Theatre; Community Concourse Parking Garage, commonly known as the Evan V. Jones Parkade; the Bow Wave Fountain; and all open-space plaza and landscape features that were originally associated with the Community Concourse. The City Operations Building, located at the northwest corner of B Street and 1<sup>st</sup> Avenue, is west of the Evan V. Jones Parkade and connected by a concrete skybridge. Historic photographs can be found in Appendix C.

### **Historical Overview**

#### ***The Community Concourse in Context to 1960s Design Trends***

The Community Concourse buildings, including the City Operations Building, which is part of this evaluation, appear to reflect the various design systems that were prominent in early 1960s. These trends include post–World War II modernism, as practiced by Mies van der Rohe; Brutalism; Expressionism; New Formalism; and what architect and writer Peter Blake called “Literal Functionalism.”<sup>2</sup> These various trends, and how the Community Concourse buildings correspond with them, are discussed below.

Architectural modernism in the 1960s, particularly the first half of the decade, was modernism in transition. As modernist principals and ideas became increasingly misappropriated over time, many architects began to question them. The International style originally called for an emphasis of volume over mass and asymmetrical compositions and moved away from the use of traditional ornament.<sup>3</sup> By the post–World War II era, and especially through the early 1960s, modernism moved toward Literal Functionalism, buildings that superficially followed modernist principals, primarily through a desire to be cost efficient or to conform to “tasteful” orthodoxies.<sup>4</sup> In cities, most often this involved the blind replication of flat-roofed, box-shaped towers with standard-issue curtain walls and open ground floors akin to the work completed in the United States during the postwar era by the German modernist and former Bauhaus instructor Mies van der Rohe. However, beginning in the 1950s, other modernists who

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<sup>2</sup> Blake, Peter. 1964. Functionalism. In Gerd Hatje (ed.), *Encyclopedia of Modern Architecture*. New York: Harry N. Abrams, p. 113.

<sup>3</sup> Hitchcock, Henry Russell, and Philip Johnson. 1995. *The International Style*. New York: W. W. Norton & Company, p. 29.

<sup>4</sup> Blake, Functionalism, p. 113.

had been stalwart proponents of the International style, most notably the Swiss architect Le Corbusier, began to change course and design buildings with sculptural, shapely forms that often had symbolic connotations. This trend in architecture is referred to as Expressionism. Le Corbusier's best-known examples of such work include Ronchamp Cathedral in France, constructed in 1954, and a series of buildings in the City of Chandigarh, India, constructed between 1952 and 1959. Le Corbusier constructed the majority of these buildings using poured-in-place concrete. Often, this concrete was left unfinished; the imprints of wood planks used in the construction process of various building components remained. This style of building design was initially called "béton brut," a French term for "raw concrete." This combination of a new shapeliness of form and exposed concrete surfaces with a béton brut surface became the primary characteristics of Brutalism, a highly popular 1960s-era design motif.

Le Corbusier's Expressionism, in particular, influenced a group of Scandinavian modernists who were known for their openness to unorthodox designs, regardless of prevailing orthodoxy. Among these architects were Jorn Utzon, Alvar Aalto, and, the most prolific of the three, Eero Saarinen. As an example, Saarinen's 1962 TWA terminal at New York's JFK Airport is constructed of poured concrete with dramatic, sculptural curves. Its overall form represents a bird in flight, with outstretched wings and a beak that doubles as a porte-cochere. Saarinen's 1955 MIT chapel is a small, circular brick-clad building that is surrounded by reflecting pools. It opens to the sky through a large oculus, with a hanging blade sculpture by Henry Bertoina. Such buildings were far from the Literal Functionalism that was often seen in postwar American architecture.

Another significant design trend of the late 1950s and early 1960s was New Formalism, which attempted to reconcile modernism with Greek and Roman Classical architecture. The Classical language, as applied in New Formalism, did not replicate traditional decorations but presented familiar motifs such as stylized columns, elevated podiums, marble cladding, and a flat-roofed rectangular massing that was akin to ancient temples.<sup>5</sup> At that time, the primary practitioners of New Formalism included Philip Johnson, Minoru Yamasaki, and Edward Durrell Stone. New Formalism was frequently used on civic and civic-cultural buildings of the 1960s. Its best-known example is the Lincoln Center for the Performing Arts (1962–1966), located in Manhattan. The Dorothy Chandler Pavilion in Los Angeles, completed in 1964 by architect Welton Becket, is probably the design system's most prominent Southern California example.<sup>6</sup>

The various buildings of San Diego Civic Center exhibit a variety of features of the aforementioned design systems. However, for the majority of the buildings within the civic center, the distinctiveness with which these elements are conveyed is less than exceptional. The City of San Diego Administration Building is an intact but common example of an early 1960s-era tower that follows the Miesian model. Like similar towers of the early

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<sup>5</sup> Rifkind, Carole. 2005. *A Field Guide to Contemporary American Architecture*. New York: Plume, pp. 114–115. Rifkind used the term "Modernist Classic" to describe what is more commonly referred to as New Formalism. Its features, as cited from Rifkin, are listed in the text.

<sup>6</sup> Nichols Chris (ed.). 2003. *Built by Becket: A Centennial Celebration*. Los Angeles: The Modern Committee of the Los Angeles Conservancy.

1960s, the building, with its decorative concrete screen at its façade, mosaic detailing, and Malcolm Leland panel work at the rear elevation, possesses some expressionist qualities but is unwilling to break away from the ubiquitous flat-roofed box shape altogether. Ultimately, the tower reads as yet another example of Literal Functionalism.

Frank Hope's design for the San Diego Convention and Performing Arts Center presents the exposed concrete associated with Brutalism coupled with long, expansive colonnades that present a New Formalist quality to the building. The use of exposed concrete on the building appears to be a cost-saving measure rather than a significant work celebrating Brutalism. Many important examples of the Brutalist style feature specially treated concrete surfaces, either through raking, preserving a given wood imprint, or sometimes applying a hyper-refined smoothness to the surface. Examples of Brutalism deemed significant often include a dynamic play of solids and voids, as seen in Kahn's 1965 Salk Institute in La Jolla. Other notable examples, such as Hope's Jack Murphy Stadium (Qualcomm Stadium) in Mission Valley, incorporate a dynamic play of sculptural shapes and forms.<sup>7</sup> The San Diego Convention and Performing Arts Center presents none of these qualities and is instead a long, continuous box with uniformly sandblasted concrete coupled with infilled bays of exposed concrete block. The New Formalist qualities of the San Diego Convention and Performing Arts Center include a long colonnade that covers a double-height loggia, which itself looks out onto a plaza. The arrangement is akin to the ancient Greek stoa, a long, colonnaded public building that faced an open plaza or marketplace within the agora—the public space of a given Greek city-state (*polis*).<sup>8</sup> Yet, where better known examples of New Formalism incorporate elegant materials, smooth wall surfaces, and delicate details, the sandblasted, exposed concrete Brutalism of the San Diego Convention and Performing Arts Center instead detracts from its New Formalist qualities and, again, conveys the building as a budget project.

The San Diego Civic Theatre, by Ruocco, Kennedy & Rosser, features expressionist qualities, a significant design trend of 1960s-era civic theaters. The best-known examples of such theaters include the Sydney Opera House, by Jorn Utzon and others, 1957–1973, and the Berlin Philharmonic by Han Scahroun, 1963. Designed to symbolically reference billowing sails, the Sydney Opera House thoroughly annihilates the box shape, which, by the early 1960s, was seen as highly over used. The Berlin Philharmonic, with its roof of sweeping arcs and points, was designed to be as functional as possible for the acoustical experience.

Lloyd Ruocco, a significant master architect for San Diego, was proud of the fact that the San Diego Civic Theatre broke apart the box at a time when many new public buildings in San Diego were conforming to it.<sup>9</sup> Its oval façade is distinctive. The full-

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<sup>7</sup> Serriano, Pierluigi, and Julius Schulman. 2000. *Modernism Rediscovered*. Köln, Germany: Taschen, pp. 468–471.

<sup>8</sup> Roth Leland. 2007. *Understanding Architecture: Its Elements, Meaning, and History*. Boulder, CO: Westview Press, pp. 221–222, 227.

<sup>9</sup> Britton, James III. 1981. Ruocco Remembered: Designs that Compliment Nature's Eye. *San Diego Union Tribune*, June 21, p. F3.

height concrete fins with intriguing window slits of bronze glass, all with a variegated placement across the front of the building, add to the expressionist qualities of the building. Its concrete bricks are similar in texture to the rough brick material of Alvar Aalto's curving Baker Dormitories at MIT (1948), with a physical tactility that rebelled against prevailing Miesian trends of smooth steel and glass. However, similar to the San Diego Convention and Performing Arts Center, the San Diego Civic Theatre appears to key into certain Brutalist tendencies as a means to be cost efficient. The theater exudes this cost efficiency through a combination of exposed, albeit textured, concrete block; exposed concrete fins; sandblasted aggregate posts at the entry; and plain stucco volumes atop the building, at its rear, and on its dressing room component, all of which are highly visible from 3<sup>rd</sup> Avenue, C Street, or the plaza itself. The theater design was not Ruocco's alone but, instead, was done in concert with two other architects, with input from Community Concourse supervising architect Samuel Hamill.<sup>10</sup> James Britton II, a local architectural critic for the *San Diego Union Tribune*, referred to the building as "an average house, not the masterpiece that Ruocco dreamed of for San Diego."<sup>11</sup>

The one other building designed as part of the original Community Concourse complex is the one building with exceptional design significance. Completed by Tucker, Sadler & Bennett in 1964, the Community Concourse Parking Garage, commonly known as the Evan V. Jones Parkade, exhibits expressionist tendencies in ways that are relatively thoughtful and unique, particularly as they are applied to what is typically a perfunctory and vernacular building type (i.e., the parking garage). Although the exterior of the structure is relatively plain (a decorative screen intended for the building was never completed because of budget issues), ground-level and interior features appear to reference various works by Eero Saarinen. The auto ramps have a smooth, rounded, sculptural quality that is highly akin to the lines of Saarinen's previously mentioned TWA terminal. In the center of the building are four arches that encompass a circular core space that features a planter at the ground level. Eleven stories above the planter, the circular core opens to the sky with a large oculus. The scheme is highly similar to the interior space of Frank Lloyd Wright's Guggenheim Museum of 1959 or Saarinen's MIT chapel. In 1972, Tucker, Sadler & Bennett also completed the Security Pacific Bank tower near the northeast corner of the concourse property. In its dominant pillars and angled profiles, the building is highly similar to yet another expressionist design by Saarinen, his 1956 CBS Building in New York City.

Within the survey area of the proposed project, across from the parkade but connected by a skybridge, is the City Operations Building, completed by the firm of Hatch and Heimerdinger in 1971. Brutalist in style, the building possesses a tapered base with an abstract, textured design pattern within it. Vertical lines run up the building between hooded window bays and raked concrete spandrels. Yet, despite these features, the City

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<sup>10</sup> Ibid.; Price, Margaret. 1983. *Transcript of an Interview with Samuel W. Hamill (1903–1989)*. San Diego, CA: San Diego Historical Society Oral History Program, March 4.

<sup>11</sup> Britton III, Ruocco Remembered, p. F3.

Operations Building does not appear to be a significant example of Brutalist architecture and is fundamentally a box with applied vertical lines that were, by then, associated with Literal Functionalism.

As was commonly practiced in other major cities during the early 1960s, the majority of the buildings within the Community Concourse appear to be using various design trends of the period as an excuse for cost efficiency rather than a reason to celebrate the unique philosophical ideas of a given design system. Overall, the design scheme is a safe, no-chances design—perhaps necessarily so, not just for its small budget, but for its serious mission: to spearhead new development and therefore save Centre City at a time when it desperately needed such intervention. In this regard, the Community Concourse was successful and is therefore significant to San Diego history.

### ***Downtown San Diego and San Diegans, Inc.***

From the turn of the century through the 1920s, the 38-block area of downtown San Diego underwent a vigorous expansion, highlighted by the completion of the John D. Spreckels Building in 1927. The growth of downtown came to an abrupt halt in the early 1930s because of economic setbacks resulting from the Great Depression. While the San Diego region gained back much of its economic vitality by the 1950s, downtown continued to face economic neglect as developers and the business community turned their attention toward rapid suburbanization along the outskirts of the city. In the period between 1950 and 1957, the overall population of San Diego grew by 47.8%, while the downtown population declined by 8.6%. The growing fear of urban sprawl was coupled with a need to inject life into downtown.<sup>12</sup> As property values dropped in the area, city government and the San Diego Downtown Association (SDDA) were slow to respond to the needs of the decaying area and exhibited limited interest in urban renewal.<sup>13</sup>

In 1959, without a concerted effort to revitalize downtown on the horizon, a group of San Diego businessmen and community leaders came together to form San Diegans, Inc., and aggressively address the area's economic issues. Prior to the efforts of San Diegans, Inc., a number of roadblocks hindered efforts to solve the problems that were facing downtown. The SDDA often turned its attention to improving downtown retail sales rather than defining a long-term vision for urban renewal and new development. The SDDA was also unable to craft a clear plan for revitalization because of competing interests of landowners and tenants within its membership.<sup>14</sup>

In 1956, the SDDA established an ad hoc committee, chaired by a prominent downtown jeweler named Joseph E. Jessop Sr., to provide recommendations on how to infuse energy and growth into the area. Over the next 2 years, Jessop brought together a

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<sup>12</sup> Pourade, Richard F. 1977. *City of the Dream*. La Jolla, CA: Copley, p. 188.

<sup>13</sup> Hof, Reiner M. 1990. San Diegans, Inc.: The Formative Years, 1958–1963: The Redevelopment of Downtown San Diego. In *The Journal of San Diego History*, vol. 36, no.1, winter. Available: <<https://www.sandiegohistory.org/journal/90winter/sdinc.htm>>.

<sup>14</sup> Ibid.

collection of downtown landowners and top business executives for six meetings that focused on a “plan of action” for downtown revitalization. The committee determined that it would need to form a new group with restrictive membership and a more focused approach to downtown renewal than the SDDA. In August of 1958, this offshoot of the SDDA took the name San Diegans, Inc., and formally incorporated as a nonprofit organization the following year. The two groups were able to coexist, with the SDDA interested in promoting retail sales for downtown businesses and San Diegans, Inc., interested in the construction of new buildings.<sup>15</sup>

After studying urban renewal efforts of other cities, including Los Angeles, Baltimore, and Vancouver, San Diegans, Inc., introduced a three-stage program that would tackle economic deterioration in downtown through 1) the preparation of an economic study to help the city council create a general plan, 2) the development of a master plan for downtown, and 3) implementation of the general plan. San Diegans, Inc., then commissioned Western Real Estate Research Corporation to conduct an economic study of the downtown area that would provide clear direction for planning and redevelopment. The study recommended the development of apartment units, hotels, office space, and a convention center.<sup>16</sup> San Diegans, Inc., presented the study to the city, which, by 1960, persuaded government leaders to develop a master plan for central San Diego. Shortly thereafter, the city planning department unveiled plans for a Community Concourse, composed of a convention hall, city hall, and performing arts center within a four-block area of downtown.<sup>17</sup>

The final Community Concourse project was the work of a consortium of San Diego-based architects, supervised by the architect Samuel Hamill. A significant local architect, Hamill had redesigned many of the Balboa Park Exposition buildings for cultural purposes and was the chief designer of the 1938 San Diego Civic Center. For the new Community Concourse, Hamill was not listed as architect on any building permits, though he did mention in an interview that he tried to unify the various architects and follow modernist ideas, which were popular in Europe.<sup>18</sup> Hamill appears to have had more of a ministerial role, working as a coordinator between the various architects, San Diegans, Inc., and the other stakeholders.

Although the construction of the Community Concourse had the support of city government, funding for the project nearly fell through after voters rejected a bond measure that would have covered the cost of construction. The city was reluctant to seek federal assistance for the project because of red tape and imposed restrictions. As a result, San Diegans, Inc., began a fundraising campaign and successfully raised the \$1.6 million

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<sup>15</sup> Ibid.

<sup>16</sup> Ibid.

<sup>17</sup> Ruocco, Lloyd. 1962. Centre City...Heart or Only Center? In *Omniart Monthly*, January.

<sup>18</sup> Price, *Transcript of an Interview with Samuel W. Hamill*. Hamill's exact quote is “I tried to unify [the architects] in pursuing a philosophy as followed by some of these foreigners who were then very popular. There was one man who invented this craze about form and function, form follows function, and the other one is less is more—all those silly things.” Presumably, Hamill is referring to Mies van der Rohe.

needed for construction. The organization provided the money to the city as an outright gift on a 5-year pledge basis. The donations allowed the city council to move forward with construction of the Community Concourse.<sup>19</sup>

Even before construction was completed in 1965, the Community Concourse served as a catalyst for other downtown construction projects. In anticipation of new development, private investment totaling \$38 million paid for the 20-story Home Federal building and two other skyscrapers. Although significant residential construction in downtown would not become a reality until the 1980s, San Diegans, Inc., continued to pursue development that would benefit not only downtown San Diego but also the region as a whole. In 1963, the National Municipal League awarded San Diego with the title of “All American City” for the quality and scope of its urban renewal program.<sup>20</sup> Although the Community Concourse was called “the most conveniently located on the Pacific Coast and one of the most beautiful” by Guilford Whitney of the Community Concourse Advisory Board, there was some initial criticism of the project for its limited plaza and open space as well as its failure to stand out as an architecturally striking civic monument. Despite some of these perceived shortcomings, the new configuration of public buildings completed a 40-year struggle to bring a convention hall to the area and, more importantly, played a vital role in the economic revival of downtown San Diego.<sup>21</sup>

### ***The Work of Malcolm Leland***

A unifying feature of the Community Concourse is the continuous bands and friezes of the abstract concrete-panel design motif by artist and sculptor Malcolm Leland. In 1972, Leland also completed the Bow Wave Fountain, located in the upper portion of the plaza, and part of the Security Pacific Bank tower courtyard.

For his work, primarily in Southern California, Malcolm Leland has become a recognized name in the field of postwar-era architectural sculpture and sculpture in general. Leland studied art at the Yale School of Fine Arts, the University of Southern California, and the Jepson Art Institute. By the late 1950s, Leland was an art instructor at Chouinard Institute, which was a significant Los Angeles art school, known today as the Art Center College of Design in Pasadena, California. Early in his career, Leland was a ceramicist, creating vessels such as planters, garden sculptures, and bird shelters. This early 1950s work, based out of his El Segundo design studio, won Leland a Good Design Award from the New York Museum of Modern Art and garnered numerous articles in the press. Now, as then, these vessels, which have become recognized as collectable pieces of mid-century modern design, are still produced through the Los Angeles company Architectural Pottery. During the next phase of his career, Leland focused on the opportunities of architectural sculpture, an often affixed, decorative component of a given building that in its abstraction was ornamental but not traditional ornament. This work was developed at a time when strict modernist canons regarding ornament were beginning to loosen, and notably, it was developed on the West Coast,

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<sup>19</sup> Hof, San Diegans, Inc.

<sup>20</sup> Ibid., pp. 7–9.

<sup>21</sup> Pourade, *City of the Dream*, pp. 214–215.

which did not possess the strict adherence to modernist dogmas of other parts of the country. Leland's first significant commission for his architectural sculpture designs came in 1957 when Richard Neutra hired him for his Los Angeles Hall of Records building. In consultation with Gladding McBean, a ceramics company, Leland developed an extruded, angled terracotta piece that could be repeated over the building's eight stories and anchored by metal hooks on steel rods. The affect was a modern-style screen, one that concealed the building's ventilation ducts yet, through its openings, allowed air to flow freely.<sup>22</sup>

Perhaps Leland's best-known applied sculpture design is the abstract pattern he created for the American Cement Company building on Wilshire Boulevard in Los Angeles, working with the architectural firm of DMJM from 1960 to 1962. In keeping with the theme of the business that was housed in the building, Leland developed a 10-foot-tall concrete X shape with a slightly biomorphic, bone-like character. Each of these X shapes was readily assembled into adjacent units to form a continuous all-over screen across the entirety of the 10-story building, minus the base. The nearby parking garage for the building features its own concrete screen with a protruding, narrow crest-like shape.

Leland's concrete work at the San Diego Civic Center is the primary and most significant unifying element of the various buildings, each of which was completed by a different architectural firm. Leland developed two separate but related versions of panels for the civic center complex. Inside the Evan V. Jones Parkade, Leland devised a shape that is essentially a slightly organic, imaginative form, a rounded square that is bisected by a vertical line that loosely conveys the effect of an elongated, four-point diamond, albeit with flat rather than pointed ends. The other panels that Leland designed across the complex are slightly larger in scale and symmetrical, with six vertical legs and an indented center, loosely echoing the curved square motif seen on the previously mentioned parking garage tiles. The crème-colored concrete tiles convey an abstract but safe design pattern, very much in keeping with 1960s design trends.

By the late 1960s and through the 1970s, Malcolm Leland contributed to or completed various standalone objects, such as the clock tower at Pomona College; the door and grillwork of the Millikan Memorial Library at the California Institute of Technology; a water sculpture at the University of Nevada, Reno; and a footbridge at the University of California, Los Angeles sculpture court, among other creations. In San Diego, Leland completed various exterior elements for the San Diego Museum of Art's west wing, the column appliqué and spandrel design on the California First Bank in downtown San Diego, and the copper façade of the J.C. Penney store in Mission Valley. In 1972, within the courtyard of the Community Concourse, Leland completed *Bow Wave*, a tall bronze and steel sculpture set within a circular pool. For this semi-abstract piece, Leland designed a nozzle that would provide fans of arcing water on either side of the sculpture

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<sup>22</sup> Los Angeles County Arts Commission. n.d. *County Collection: Los Angeles County Hall of Records*. Available: <<http://www.lacountyarts.org/civicart/projectdetails/id/107>>.

to simulate the prow of a ship moving through water.<sup>23</sup> Originally, the jets shot water at a rate of 4,000 gallons per minute, with the “waves” projecting much higher and wider than they presently do.<sup>24</sup> The effect, the inspiration for which Leland received while watching a ship steam into San Diego Bay, was one of movement when viewed head on by westbound B Street traffic and pedestrians in the plaza. The water at the rear portion of the pool rippled in a wake effect. Although *Bow Wave* was completed with another project that post-dates the completion of the Community Concourse, the work is intimately associated and identified with the plaza, and its low-rise base is frequently used as a gathering place for people who visit the Community Concourse. Leland completed at least one other fountain in San Diego at Mesa Vista Hospital. Determining whether this fountain still exists was not part of the research for this study.

As Southern California mid-century modernism has grown in popularity, so has the work of Malcolm Leland. Leland’s work has been exhibited at the Los Angeles County Museum of Art, the Los Angeles Museum of Contemporary Art, the San Francisco Museum of Art, and other galleries and venues in Southern California.<sup>25</sup>

## Methods and Results

### Archival Research

To complete the historic evaluation of the Community Concourse and its associated buildings, a variety of archival sources were reviewed. Preliminary research was conducted through the ProQuest historic *Los Angeles Times* database, which yielded general information about the project, including project architects. As part of the preliminary review process, architectural historians reviewed books on the subject of national civic architecture as well as architectural guidebooks pertaining to the subject resources in San Diego. Within the City of San Diego, the collection at the San Diego Historical Society was reviewed, which yielded numerous newspaper articles, promotional and planning materials about the concourse, historic photographs, plus one pertinent oral history. The California Room of the San Diego Public Library yielded numerous historic articles, primarily from the *San Diego Union Tribune* collection. Original building permits and assessor’s records were reviewed for all of the subject resources. Telephone interviews were conducted with architect Hal Sadler, who worked on the Community Concourse Parking Garage, and with designer Malcolm Leland.

### Field Survey

On Thursday, February 11, 2010, architectural historians David Greenwood and Daniel Paul conducted an intensive-level field survey of all subject resources. The architectural historians toured and photo-documented the Community Concourse, other buildings, and the fountain in the project area. Using information gained from the field survey and archival research, the architectural historians were able to complete DPR forms for the

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<sup>23</sup> Malcolm Leland. n.d. *Malcolm Leland*. Information sheet.

<sup>24</sup> Ibid.

<sup>25</sup> Leland, Malcolm. 1992. *Resume*, p.1.

Community Concourse, its individual buildings, and the Bow Wave Fountain. To complete the DPR forms, the historians followed the instructions provided by the California Office of Historic Preservation in its Instructions for Recording Historical Resources dated March 1995.

### **Description of Surveyed Resources**

Contemporary and historic photographs of the subject properties can be found in Appendix E.

#### ***Community Concourse: Overview***

The Community Concourse is a 4-acre complex that serves as the civic center for the city of San Diego. The concourse consists of four buildings: the multi-story City of San Diego Administration Building (City Hall), the San Diego Convention and Performing Arts Center, the San Diego Civic Theatre, and the Community Concourse Parking Garage (Evan V. Jones Parkade). The buildings are arranged on a large parcel with a north-south orientation. The southernmost edge of this property contains the administration building. To the east is the theater, and behind the administration building is the long, low-rise San Diego Convention and Performing Arts Center. Affixed at north end is the multi-story Community Concourse Parking Garage. A portion of the Community Concourse is devoted to an open plaza with various landscape and artwork elements that are character-defining features of the concourse. Terrazzo design work is present in the southern (original) portion of the plaza; centered within the terrazzo is a map of San Diego in 1543. Planters with irregular shapes that bordered what was originally the upper end of the courtyard, just below what was then B Street, are still present and feature mature tree and bush specimens. The original light fixtures, either paired on metal poles or affixed to various buildings, feature drop-down white globes that are suspended beneath circular metal-strip casings. Facing the courtyard, as well as other public elevations, is a continuous band of repeating decorative concrete panels by Malcolm Leland that runs across all of the concourse buildings. The Leland panel work, featuring an abstract, slightly biomorphic vertical design, unifies the buildings, which were created by different architects.

#### ***City of San Diego Administration Building***

The City of San Diego Administration Building is a rectangular 15-story office tower. The building features a scored concrete base at either end of its south-facing front elevation. Between the base elements are numerous glass-enclosed bays at the ground level. Applied vertical aluminum strips break up the bays. Double-height square columns of scored concrete, similar to the base, are present at the lower levels of the front elevation. Above the ground level is a slightly recessed balcony with varying shades of pink mosaic tile. Above the two lower levels, the upper levels display large, full-height vertical concrete fins that protrude out from the south elevation between each window bay. Affixed to the fins at the spandrel level of each floor are pairs of concave sunshades. The ribbon windows with aluminum frames behind the shades are not readily visible. The upper 13 floors of the south elevation are framed in a continuous, box-shaped concrete surround. The concrete fins touch the upper edge of the surround. Behind them is a blank concrete background with a light pink color. The two lower levels of the tower are slightly recessed from the 13 upper

levels. The ground floor is largely open. In the middle is a breezeway that leads to the courtyard behind the building. The narrow side elevations of the building are concrete and largely blank, except for the four scored lines that run from just above the recessed lower levels to the top of the tower.

The rear elevation of the City of San Diego Administration Building faces north and looks onto the concourse plaza. The rear elevation is treated differently from the front, with six window bays across each floor, each with a grouping of four windows. Each bay is recessed slightly behind a large concrete framing system that runs across the entirety of the elevation. Taupe-colored metal spandrels are also present on the rear elevation. Affixed to the rear elevation is a large, rectangular component, presumably an elevator shaft, that is clad entirely in concrete. It features the same full-height scoring seen on the side elevations. The elevator shaft does not touch the ground but, instead, ends at the roof of a low-rise component of the theater. Likewise, the southern end of the San Diego Convention and Performing Arts Center is also connected to the administration building.

### ***San Diego Civic Theatre***

The San Diego Civic Theatre is a four-story performance venue with an irregular plan, located in the southeast portion of the Community Concourse. The exterior of the building has variegated massing and materials. The front elevation is predominately semi-circular and made of textured concrete block with irregularly placed full-height vertical concrete fins. Between many of the fins are narrow, full-height window bays that peak outward. The windows are set in bronze mullions. The recessed ground level features multiple sets of bronze-framed single-leaf glass doors set behind exposed, sandblasted aggregate concrete posts. The fin and window treatment seen at the front elevation continues across the side elevations, albeit with fewer intervals. The side elevations curve inward and toward the five-story, flat-roofed fly loft, a tall rectangular component that is clad in smooth, painted concrete. The fly loft is affixed to the administration building. Much of the fly loft's exterior does not touch the ground but, instead, joins a two-story, flat-roofed covered loading area. The loading area features an irregular plan, with massing that curves inward or bulges out at various elevations. The exterior of the loading area features textured concrete with irregular scoring that echoes the irregular pattern of the fins on the theater's front elevation. Affixed to the south elevation of the loading component is an elliptical decorative fountain and pool with brick trim. This water feature is divided into three separate but asymmetrical parts; at the present time, it is empty. The fountain is trimmed with smaller scale concrete panels by Malcolm Leland, which are seen on other parts of the concourse. However, research indicates that this particular fountain is not by Leland. Surrounding the fountain is a small landscaped area with palm trees, birds of paradise, and flower specimens. This landscaping appears to be a later addition. At the rear of the building is a single-story, flat-roofed dressing room with a slightly irregular square plan. The dressing room is stucco clad, and the decorative concrete panels seen across other parts of the civic center run the length of the exposed elevations directly beneath the roofline. Affixed to these panels are globe-style light fixtures set within an open canopy framework. A proclamation with metal lettering dating from the 1965 dedication ceremony for the

concourse is present on the north elevation of the dressing room component. Just northeast of the theater is a standalone metal sign and marquee. It consists of three concave, backlit faces that are supported by three exposed metal beams.

### ***San Diego Convention and Performing Arts Center***

The San Diego Convention and Performing Arts Center is a large two-story, flat-roofed building with a rectangular plan and a north-south orientation. Its southern portion is connected to the administration building. The predominant exterior feature is a continuous, repeating set of full-height exposed concrete columns. The columns meet a continuous lintel and, together, define a series of repeating bays. Each bay features a round-cornered header. Between the first and second levels, traversing the entirety of the building, are vertically oriented concrete decorative panels with an abstract design by Malcolm Leland. On the east-facing elevation, which faces the courtyard, is a long, wide, double loggia that is recessed within these bays at each level. The upper-level loggia has a metal railing at its outer edge; this railing has been altered. Inset within the loggia and the colonnade, the structural elevation itself has bays of exposed concrete block. The main entries to the building are also on this elevation and feature paired aluminum-framed glass doors with a large transom. The east-facing elevation features four ticket windows at the lower level and two additional ticket windows at the upper level. The upper-level bays in the southwest portion of the building feature fixed glazing set between thick aluminum mullions. The San Diego Convention and Performing Arts Center features original backlit signage with a 1960s-era logo; marquees are affixed at various places. At the south end of the long, covered concourse, glass-door entries to the administration building are present. The rear portion of the building is connected to the Community Concourse Parking Garage by a full-width terrace.

### ***Community Concourse Parking Garage***

The Community Concourse Parking Garage, known as the Evan V. Jones Parkade, is 11 stories tall and built from pre-stressed concrete. Each elevation of the square plan is 200 feet long and eight bays wide. The bays are delineated by full-height vertical concrete pilasters with full-height scoring that is akin to the scoring on the side elevations of the administration building. At the top of the structure, the pilasters join a seamless lintel and form rounded, open bays, similar to those on the San Diego Convention and Performing Arts Center. The spandrels, which support various ramps within the structure, are slightly pitched and angled. At the ground level of the structure are numerous pedestrian ramps as well as small planters with various bush and tree specimens. Many of these ramps are clad in the previously mentioned decorative concrete paneling that is seen throughout the concourse. The pedestrian ramps are made of concrete and often modeled with soft, rounded edges. A portion of the ramp from the east elevation was removed when B Street was closed because of the construction of the Security Pacific Bank tower, just east of the structure. Floating concrete stairways are also present and located adjacent to the ramps. In the middle of the Community Concourse Parking Garage is a composition with four arches that correspond to the four sides of the structure. Each of these arches, at the voussoirs, is clad in the previously mentioned decorative concrete paneling. In the middle is a circular planter with various tree and

bush specimens. Large circular stepping stones lead to the planter from various openings. A water feature that was once adjacent to the planter has been removed. Above the planter is a circular, dramatically scaled, open-core atrium that rises 11 stories. The cylindrical quality of the open space is defined by the spiral of the ramps. In terms of engineering, the structure is essentially two separate garages that are intertwined, with cars entering from 1<sup>st</sup> Avenue parking on odd-numbered floors and cars entering from 2<sup>nd</sup> Avenue parking on even-numbered floors. A portion of the 2<sup>nd</sup> Avenue exit ramp was removed with the construction of the nearby Security Pacific Bank tower in 1972.

### ***Bow Wave Fountain***

Directly above the plaza and congruent with B Street is the Bow Wave Fountain by Malcolm Leland, the artist who designed the unifying decorative panel that is found on the various buildings. The fountain itself consists of five large, mottled bronze panels with horizontal scoring to simulate shiplap cladding. The panels meet at a prow that simulates that of a ship or a boat. The outer panels flare outward at their upper edges. A fan of water shoots off either side at the front of the piece to simulate a ship cutting through water. Originally, the fan of water was much higher than it is now and echoed the curved side panels of the sculpture. The pool in which the sculpture resides is circular but has a square-shaped base, just slightly above ground level. The Bow Wave Fountain and all portions of the courtyard north of it were completed in conjunction with the nearby Security Pacific Bank tower in 1972.

### ***City Operations Building***

The City Operations Building is a rectangular, five-story, flat-roofed institutional building. Brutalist in style, the building displays exposed concrete elevations with a base that is highly textured with an irregular jagged-edge design. The ground level features wide arched openings that seem to permit vehicle access. A row of arched clerestory window openings, which are deeply recessed, is present directly above this base. A wraparound concrete eave separates the ground floor from the upper stories. The building's upper three levels feature thin bays that are separated by full-height vertical pilasters. Across the middle floors, these windows are topped by small, segmented arched hoods of concrete. At the upper level, the windows are topped by hoods with a more pronounced arch shape. The spandrel panels above and below the windows have raked concrete detailing. The ground floor of the building's south elevation serves as a fire station. Three garage bays are present, and each has a multi-light roll-up door. Atop the building is a square component that appears to house mechanical systems. It features a pattern of thin, vertical vents with arched headers. A concrete skybridge connects the City Operations Building to the Community Concourse Parking Garage to the east. The bridge is constructed of exposed concrete and has a solid concrete railing. The eave directly above the building's ground floor wraps upward, forming a hood where the bridge enters the building. Original metal lettering that spells out "Operations Building" is visible above the bridge as one enters the building.

# Significance Evaluations

## California Historical Resource Status Codes

Community Concourse	
City of San Diego Administration Building	5S3
San Diego Convention and Performing Arts Center	5S3
San Diego Civic Theatre	5S3
Community Concourse Parking Garage	3CS
Bow Wave Fountain	3CS
City Operations Building	6Z

### The Community Concourse

The Community Concourse appears to be eligible as a San Diego Historical Landmark under Criterion A (“Exemplifies or reflects special elements of the city’s, community’s, or neighborhood’s historical, archeological, cultural, social, economic, political, aesthetic, engineering, landscaping, or architectural development”). Additionally, the Community Concourse appears to be eligible as a San Diego Historical Landmark under Criterion B (“Is identified with persons or events significant in local, state, or national history”). The period of significance for the Community Concourse is 1964–1965, which encompasses the completion dates for the various buildings within the complex.

With respect to city Criterion A, the Community Concourse does reflect “special elements” of the community’s economic development. It is largely credited with spearheading downtown (City Centre) revitalization at a time when it was desperately needed. Its completion in 1965 served as a catalyst that drew millions of dollars to downtown for major construction projects.<sup>26</sup> In anticipation of new construction, private investment totaling \$38 million financed the 20-story Home Federal building and two other skyscrapers.<sup>27</sup> With respect to city Criterion B, the concourse played an important role in Centre City revitalization, an “event” that was significant in local history. It is also associated with “persons” who were significant in local history, namely, San Diegans, Inc. As for city Criterion C, the Community Concourse as a whole does not embody distinctive characteristics of a style, type, period, or method of construction and is not a valuable example of the use of indigenous materials or craftsmanship. Except for the Community Concourse Parking Garage and the San Diego Civic Theatre, other buildings associated with the Community Concourse do not appear to be historically significant examples of architectural design.

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<sup>26</sup> City of San Diego. 2007. *San Diego Modernism: Historic Context Statement*. San Diego, CA: City of San Diego, October, p. 49.

<sup>27</sup> Hof, San Diegans, Inc.

San Diego Historical Landmark Criterion D states that a resource can be a city landmark if it “is representative of the notable work of a master builder, designer, architect, engineer, landscape architect, interior designer, artist, or craftsman.” The discussion of specific buildings, below, will address the master architect issue as it pertains to each resource.

Overseeing the completion of the Community Concourse was Samuel W. Hamill, FAIA. A significant local architect, Hamill had redesigned many of the Balboa Park Exposition buildings for cultural purposes and was the chief designer of the 1938 San Diego Civic Center. For the new Community Concourse, Hamill was not listed as architect on any building permits, though he did mention in an interview that he tried to unify the various architects and follow modernist ideas, which were popular in Europe.<sup>28</sup> Hamill appears to have had more of a ministerial role, working as a coordinator between the various architects; San Diegans, Inc.; and the other stakeholders. Though Hamill is associated with the Community Concourse, he was not as actively involved as he was during his earlier Civic Center and Balboa Park projects, both of which allowed him a much more hands-on role as an architect.

The social significance of the Community Concourse as a whole does not appear to warrant CRHR listing under Criterion 1 or NRHP Criterion A. The historic importance of the Community Concourse is highly specific to San Diego, if not Centre City itself. The same holds true for San Diegans, Inc. as an entity of persons. The significance of San Diegans, Inc. to the civic center is strictly at the municipal level, and that association does not appear to warrant listing the Community Concourse as a historic resource under CRHR Criterion 2 or NRHP Criterion B. With respect to the design, the Community Concourse as a complex does not appear to have design significance under CRHR Criterion 3 or NRHP Criterion C. Furthermore, the concourse does not appear to meet NRHP Criterion Consideration G as an exceptional example of a resource under 50 years old.

#### **City of San Diego Administration Building (City Hall)**

Similar to the other resources that make up the Community Concourse, the City of San Diego Administration Building appears eligible as a San Diego Historical Landmark under city Criteria A and B. This is due to the building being an element of the Community Concourse project, which played a significant role in spearheading the revitalization of downtown San Diego (City Centre) in the mid-1960s. Like the other buildings of the Community Concourse, the City Administration Building is considered a significant achievement of San Diegans, Inc., a locally important group of business people who advocated the regeneration of downtown through the use of city planning and various downtown business and development measures. The City Administration Building does not embody distinctive examples of a design that would warrant historic landmark listing under city Criterion C. The City of San Diego Administration Building is a relatively intact but common example of a 1960s-era tower, with its rectangular box

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<sup>28</sup> Price, *Transcript of an Interview with Samuel W. Hamill*. Hamill’s exact quote is “I tried to unify [the architects] in pursuing a philosophy as followed by some of these foreigners who were then very popular. There was one man who invented this craze about form and function, form follows function, and the other one is less is more—all those silly things.” Presumably, Hamill is referring to Mies van der Rohe.

elevation, decorative concrete screen with vertical concrete fins across the front elevation, mosaic accents, articulated base with mosaic-covered balconies, and open ground floor that includes a breezeway to the concourse itself. Though the mosaic city seal on the path of the breezeway is one of the more distinguished design elements of the building, this alone does not elevate the design of the building to a level of historic significance. Overall, the design reads as a common example of its type, a Miesian-inspired postwar tower with some added decorative elements.

The City Administration Building does not appear eligible as a San Diego Historical Landmark under city Criterion D. Although George Hatch completed other municipal buildings in the city, including the San Diego Central Library and the City Operations Building (Hatch and Heimerdinger), Hatch, Wulf & Fifield Associates, the architectural firm of record for the building, is not considered a master architectural firm, according to the San Diego Modernism Historic Context Statement.

The level of significance of the resource as an element that spearheaded downtown revitalization and was associated with San Diegans, Inc. does not appear to warrant listing on either the CRHR or the NRHP under Criterion 1 or 2 or Criterion A or B, respectively. The building has served as the center of San Diego government since 1964, but this alone does not render the building historically significant under CRHR Criterion 1 or NRHP Criterion A.

Just as its design and architectural significance is insufficient for city listing under local Criterion C or D, the building does not appear to have the quality of significance necessary for listing as a significant example of 1960s mid-century modern architectural design under CRHR Criterion 3 or NRHP Criterion C. The City Administration Building is not NRHP eligible under Criterion Consideration G as an exceptionally significant resource that is less than 50 years old.

### **San Diego Convention and Performing Arts Center**

Completed in 1964, and similar to the other resources that make up the Community Concourse, the San Diego Convention and Performing Arts Center appears eligible as a San Diego Historical Landmark under city Criteria A and B. This is due to the building being an element of the Community Concourse project, which played a significant role in spearheading the revitalization of downtown San Diego (City Centre) in the mid-1960s. The San Diego Convention and Performing Arts Center, as a component of the Community Concourse, is associated with persons who were significant to local history, namely, San Diegans, Inc., a locally important group of business people who advocated the regeneration of downtown through the use of city planning and various downtown business and development measures. By the mid-1970s, the San Diego Convention and Performing Arts Center was a locally notable venue for rock acts such as Pink Floyd, the Rolling Stones, and Bob Dylan, among others.<sup>29</sup> However, the fact that such significant rock bands performed within the venue does not contribute to its significance under city Criterion A or B.

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<sup>29</sup> City of San Diego. 1998. *San Diego Concourse and Civic Theatre: Concerts, 1965–1997*.

The San Diego Convention and Performing Arts Center does not embody distinctive examples of a design that would warrant historic landmark listing under city Criterion C. As a mix of Brutalist and New Formalism design systems, the building is not a successful example of either. Similar to other institutional buildings of the 1960s, the building appears to use Brutalist design traits as an excuse for cost efficiency. The materials, which include exposed concrete block and sandblasted aggregate, are displayed on a large, rectangular box-shape building that fails to convey the dynamism of more sculptural examples of Brutalism. Its extended colonnade is a character-defining feature of the building and akin to a loggia or an ancient Greek stoa, a long, low public building with an extended portico that provided shelter for the public and often faced a marketplace. However, this alone does not appear to make the building a distinctive design.

The San Diego Convention and Performing Arts Center does not appear to be eligible as a San Diego Historical Landmark under city Criterion D. Although Frank L. Hope & Associates is a significant postwar San Diego architectural firm, the San Diego Convention and Performing Arts Center does not appear to be “representative of the notable work of a master builder, designer, architect, engineer, landscape architect, interior designer, artist, or craftsman” in a manner that would warrant its listing as a city landmark under city Criterion D. San Diego’s Jack Murphy Stadium (Qualcomm Stadium), another Brutalist work by the same firm, possesses shapeliness and sculptural dynamics that render it a better example of the Brutalist design system and a “representative” work by the firm. Furthermore, the decorative concrete panels across the building’s spandrel designed by Malcolm Leland, a notable artist, do not lend the building design significance that would make it eligible under city Criterion D.

The level of significance of the San Diego Convention and Performing Arts Center as a constituent element of the concourse, which spearheaded downtown revitalization and was associated with San Diegans, Inc., does not appear to warrant listing of the building on either the CRHR or the NRHP under Criterion 1 or 2 or Criterion A or B, respectively. Just as the resource’s design significance does not meet city Criterion C as an unremarkable example of Brutalist and New Formist design, the resource is also ineligible for CRHR Criterion 3 or NRHP Criterion C. The San Diego Convention and Performing Arts Center is not NRHP eligible under Criterion Consideration G as an exceptionally significant resource that is less than 50 years old.

### **San Diego Civic Theatre**

The San Diego Civic Theatre was opened in January of 1965 as the last component of the Community Concourse. Similar to the other resources that make up the Community Concourse, the San Diego Civic Theatre appears eligible as a San Diego Historical Landmark under city Criteria A and B. This is due to the building being a component of the Community Concourse project, which played a significant role in spearheading the revitalization of downtown San Diego (City Centre) in the mid-1960s. The Community Concourse is the crowning achievement of San Diegans, Inc., a locally important group of business people who advocated the regeneration of downtown through the use of city planning and various downtown business and development measures.

Its curvilinear form is relatively unique and hearkens back to trends in European expressionist modernism between world wars and again during the 1960s when the Miesian model of the supremacy of the box form was beginning to be scrutinized. The 1950s and 1960s saw a trend in concert hall design that featured expressionist forms. The better known of these include the Berlin Philharmonic (1956–1963), by Hans Scharoun, and the Sydney Opera House (1957–1973), by Jorn Utzon. Regionally, the Dorothy Chandler Pavilion (1964) in Los Angeles, by Welton Becket and Associates, is not a typical rectangle but instead features convex elevations. The form of the San Diego Civic Theatre is arguably more expressionist than the Dorothy Chandler Pavilion.

Because the theater, for San Diego, embodies the distinctive characteristics of the 1960s-era theater building type, the resource appears eligible as a local-level resource under city Criterion C. This is due largely to its expressive form, which was highly distinct among other 1960s-era rectangular public buildings in San Diego.

The San Diego Civic Theatre does not appear eligible under city Criterion D. Although the theater is associated with San Diego master architect Lloyd Ruocco, who was one of the city's first architects to embrace modernism, this specific design was not Ruocco's alone but was done in concert with two other architects (Selden Kennedy and William Rosser) with whom Ruocco did not have an architectural practice. Ruocco worked with the two others through an arrangement made by stakeholders at that time, and all design decisions were made between the three architects, with additional input from outside committees and supervising architect Samuel Hamill. James Britton II, a local architectural critic for the *San Diego Union Tribune*, referred to the building as “an average house, not the masterpiece that Ruocco dreamed of for San Diego.”

The level of significance of the theater as a constituent element of the concourse, which spearheaded downtown revitalization and was associated with San Diegans, Inc., does not appear to warrant listing on either the CRHR or the NRHP under Criterion 1 or 2 or Criterion A or B, respectively. The San Diego Civic Theatre presents no known associations to any other significant individuals or events that would warrant listing on either the CRHR under Criterion 1 or 2 or the NRHP under Criterion A or B.

Even though the theater appears eligible as a San Diego Historical Landmark under Criterion C, the quality of its design appears inadequate to warrant listing on either the CRHR under Criterion 3 or the NRHP under Criterion C. Despite its novel shape, which is in keeping with other 1960s-era expressionist modern theaters, the San Diego Civic Theatre exudes cost efficiency through its use of exposed, albeit textured, concrete block; exposed concrete fins; sandblasted aggregate posts at the entry; and plain stucco volumes both atop the building at its rear and on its dressing room component—all highly visible from either 3<sup>rd</sup> Avenue, C, Street, or the concourse itself. Because the theater is not a representative work of Ruocco as a master architect, the resource does not meet CRHR or NRHP Criterion 3 or C, respectively. The San Diego Civic Theatre is not NRHP eligible under Criterion Consideration G as an exceptionally significant resource that is less than 50 years old.

### **Community Concourse Parking Garage (Evan V. Jones Parkade)**

Similar to the other resources that make up the Community Concourse, the Community Concourse Parking Garage appears eligible as a San Diego Historical Landmark under city Criteria A and B. This is due to the building being a component of the Community Concourse, which played a significant role in spearheading the revitalization of downtown San Diego (City Centre) in the mid-1960s, and the crowning achievement of San Diegans, Inc., a locally important group of business people who advocated the regeneration of downtown through the use of city planning and various downtown business and development measures.

The Community Concourse Parking Garage appears to be eligible as a San Diego Historic Landmark under Criterion C (“Embodies distinctive characteristics of a style, type, period, or method of construction or is a valuable example of the use of indigenous materials or craftsmanship”). The Community Concourse Parking Garage, today known as the Evan V. Jones Parkade, exhibits a quality of design and features that are extremely uncommon for a post–World War II parking garage. In early press, observers described the structure as “one of the most beautifully designed buildings in the [Concourse] complex.” In July of 1965, the parkade was photographed by the noted architectural photographer Julius Schulman and featured on the cover of *Arts and Architecture* magazine, the preeminent California architectural journal of the post–World War II era.<sup>30</sup>

A parking garage as the cover image of an important architectural journal is highly unusual.

As one nears the exterior of the structure, the pedestrian experience at the ground level is highly considered, with floating, extended beam stairways; criss-crossing pedestrian walkways adorned in decorative concrete paneling; and sculptural vehicle entry and exit ramps with soft, rounded corners. The center space of the parkade is highly unusual and unexpected. Surrounding it at the ground level are four open arches with voussoirs that are clad by artist Malcolm Leland’s decorative concrete panels, which can be seen across the rest of the Community Concourse. Centered within these arches, and within the structure itself, is a circular planter at ground level. This area originally included a water feature, but it has been removed. Directly above the planter, extending the entire 11-story elevation of the structure, is an open-core atrium. This full-height space is perfectly circular and opens to a large oculus at the center of the building. The circular shape of the space is defined by a spiraling vehicle circulation element. As described in the earlier essay about the Community Concourse in contrast to 1960s design trends, the garage appears to take various cues from the work of mid-twentieth century master architect Eero Saarinen. The sculptural quality of the ramps, with smooth lines rendered in concrete, is akin to Saarinen’s 1962 TWA terminal building at New York’s JFK airport. The interior oculus of the garage, enclosed by arches, is akin to Saarinen’s 1955 MIT chapel. Additionally, the spiraling ramps within the building’s center, if the oculus is to be viewed as a skylight, appears to reference Frank Lloyd Wright’s 1959 New York Guggenheim Museum, which features the spiraling ramp motif as the prominent interior feature of the building.

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<sup>30</sup> *Arts and Architecture*. 1965. Tucker, Sadler & Bennett, Architects. July, cover, pp. 32–33.

Engineered by George Devlin of Detroit, the garage was designed to hold 1,000 cars, all of which could exit the structure within 20 minutes.<sup>31</sup> As originally designed, the structure was essentially two garages in one. Cars entering from 1<sup>st</sup> Avenue followed a spiraling ramp to the first, third, fifth, seventh, and ninth floors.<sup>32</sup> Vehicles entering from 2<sup>nd</sup> Avenue parked on the second, fourth, sixth, eighth, and tenth floors. The southern portion of the 2<sup>nd</sup> Avenue ramp was removed in 1972 during construction of the adjacent Security Pacific Bank tower, which was by Tucker, Sadler & Bennett. Two other ramps remain in full. A water feature that was originally located within the center of the building is now gone. However, the garage retains the rest of its original fabric, including a variety of period features with a considered relationship to the ground level pedestrian. These alterations do not compromise the integrity of the resource in such a manner that its significant character-defining features are no longer conveyed.

The Community Concourse Parking Garage appears to be eligible under city Criterion D (“Is representative of the notable work of a master builder, designer, architect, engineer, landscape architect, interior designer, artist, or craftsman”). The firm of Tucker, Sadler & Bennett operated in San Diego for more than 40 years, producing hundreds of commercial and residential projects. These include various downtown towers; multiple buildings at the University of California, San Diego and San Diego State University; and custom residences, among many others. In 2007, Hal Sadler received the Lifetime Achievement Award from the California Council of the American Institute of Architects.<sup>33</sup>

The level of significance of the Community Concourse Parking Garage as a constituent element of the concourse, which spearheaded downtown revitalization and was associated with San Diegan, Inc., does not appear to warrant listing of the building on either the CRHR or the NRHP under Criterion 1 or 2 or Criterion A or B, respectively. At some point after its period of significance, 1964, the structure was named for Evan V. Jones, a former parking superintendent for the City of San Diego. However, this association alone does not appear to render the resource eligible for CRHR listing under Criterion 2 or NRHP listing under Criterion B.

The Community Concourse Parking Garage appears to be eligible for the CRHR at the local level under Criterion 3 for embodying the distinctive characteristics of a type, for representing the work of a master architectural firm, and for possessing high artistic values. The structure is a considered and distinctive example of what is normally a perfunctory building type: the parking garage. When completed, the structure was recognized as a special example of its type. As previously mentioned, the structure features a variety of intact post-World War II design elements that cater to the pedestrian. The sculptural arrangement of spiraling auto ramps that define a monumental, multi-story interior core, along with a landscaped planter topped by a skylight, is unusually well considered for a parking garage.

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<sup>31</sup> Sadler, Hal. 2009. Telephone interview with author Daniel Paul. Los Angeles, CA. February 19.

<sup>32</sup> *San Diego Union Tribune*. 1964. Eleven-story 1,000-Car Garage Has Built-in 2-in-1 Feature. September 14, p. C9.

<sup>33</sup> City of San Diego, *San Diego Modernism*, p. 107.

Even though the Community Concourse Parking Garage appears to be CRHR eligible, the structure does not appear NRHP eligible under Criterion C. A decorative screen intended for the exterior of the building was never completed, and the previously mentioned losses of the ramp and the water feature compromise the structure's integrity. With respect to its design, the structure, in general, does not possess the quality of significance that would warrant listing on the NRHP under Criterion C. Additionally, the garage is not an exceptionally significant resource that would warrant NRHP listing under Criterion Consideration G for structures younger than 50 years old.

### **The Bow Wave Fountain**

The Bow Wave Fountain, completed by Malcolm Leland in 1972, is located near the Community Concourse but was not designed in conjunction with it. Instead, the Bow Wave Fountain was designed in conjunction with the Security Pacific Bank tower, located just north of the San Diego Civic Theatre.

Although the fountain was created by a master artist, it has lost integrity with respect to special elements that exemplify the city's aesthetic development and therefore is not eligible for city Criterion A. According to the artist himself, the water pump appears to be failing. Symmetrical fans of water that originally projected much higher and, therefore, were in proper composition with the upper edges of the piece itself, at present project about one-third of their original intended height.

The Bow Wave Fountain has no known associations with persons or events that were significant in local, state, or national history and is therefore ineligible for San Diego Historical Landmark listing under city Criterion B. Furthermore, the Bow Wave Fountain does not appear eligible as a San Diego Historical Landmark under city Criterion C, again due to the above-mentioned alteration. However, the Bow Wave Fountain is still a rare example of a fountain that was designed by prolific mid-century designer and artist, Malcolm Leland, and the object does appear eligible under city Criterion D ("Is representative of the notable work of a master builder, designer, architect, engineer, landscape architect, interior designer, artist, or craftsman"). It should be noted that Malcolm Leland's work has been exhibited at the Los Angeles County Museum of Art, the Los Angeles Museum of Contemporary Art, the San Francisco Museum of Art, and other galleries and venues in Southern California.<sup>34</sup>

The Bow Wave Fountain is not associated with significant events or persons that would warrant its listing on either the CRHR under Criterion 1 or 2 or the NRHP under Criterion A or B. However, the Bow Wave Fountain does appear to be eligible for the CRHR under Criterion 3 as a significant example at the local level of the work of a master artist (i.e., Malcolm Leland). The object's quality of significance, combined with the diminished effect from its failing water pump, does not render it eligible for NRHP listing under Criterion C. Finally, the resource is not considered to possess exceptional significance under NRHP Criterion Consideration G that would warrant listing despite its being less than 50 years old.

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<sup>34</sup> Leland, Malcolm. 1992. *Resume*, p.1.

### **City Operations Building**

Completed in 1971, the City Operations Building does not have any historic association to the Community Concourse or to San Diegans, Inc. as do the other buildings that are being evaluated. There are no known associations to historic events or persons that would warrant listing of the City Operations Building as a San Diego Historical Landmark under city Criterion A or B. The City Operations Building is of the Brutalist style, featuring exposed concrete, a tapered base with a textured design pattern, and exposed concrete window hoods and headers. An exposed concrete skybridge appears to have been completed in conjunction with the building. Despite these features, the building is essentially a rectangular box and does not possess either the sculptural massing or the dynamic use of solids and voids seen in more significant examples of Brutalism. As an intact but unexceptional example of Brutalism, the City Operations Building does not appear eligible under city Criterion C as a distinctive example of Brutalism. The building was designed by the firm of Hatch and Heimerdinger. George Hatch, architect of the San Diego Central Library and the City of San Diego Administration Building, appears to have been the lead architect. According to the City of San Diego Modernism Historic Context Statement, neither George Hatch nor the firm of Hatch and Heimerdinger appears to be a “master architect” in manner that would warrant listing of the building as a San Diego Historical Landmark under city Criterion D.

The same lack of significant associations to events or persons that renders the building ineligible as a local landmark also applies to its ineligibility for CRHR or NRHP listing under Criterion 1 or 2 or Criterion A or B, respectively. The resource does not possess adequate design or architect significance for landmark listing under CRHR Criterion 3 or NRHP Criterion C. In addition, the City Operations Building does not possess exceptional significance that would warrant NRHP listing under Criteria Consideration G for properties that are less than 50 years old.

## **Findings and Conclusions**

As supported by a memo from the City of San Diego dated April 13, 2010, and the above analysis, the San Diego Community Concourse, which includes the City Administration Building, the San Diego Convention and Performing Arts Center, the San Diego Civic Theatre, and the Community Concourse Parking Garage, are all historical resources for CEQA purposes. Additionally, the Bow Wave Fountain by the artist Malcolm Leland is a historical resource for CEQA purposes.

For their collective contribution in spearheading a revitalization of downtown San Diego and for their association with the group San Diegans, Inc., the buildings and features constructed as part of the Community Concourse appear to be eligible under San Diego Historical Landmark Criterion A (“Exemplifies or reflects special elements of the city’s, community’s, or neighborhood’s historical, archeological, cultural, social, economic, political, aesthetic, engineering, landscaping, or architectural development”) and city Criterion B (“Is identified with persons or events significant in local, state, or national history”). The Community Concourse as a complex possesses a period of significance of

1964 to 1965. Individually, the buildings within the Community Concourse include the City Administration Building, period of significance 1964; the San Diego Convention and Performing Arts Center, period of significance 1964; the San Diego Civic Theatre, period of significance 1965; and the Community Concourse Parking Garage, period of significance 1964.

Within the Community Concourse, the San Diego Civic Theatre appears to be eligible under city Criterion C (“Embodies distinctive characteristics of a style, type, period, or method of construction or is a valuable example of the use of indigenous materials or craftsmanship”).

Within the concourse, the Community Concourse Parking Garage appears to be eligible under city Criterion C. Additionally, the garage is eligible under city Criterion D (“Is representative of the notable work of a master builder, designer, architect, engineer, landscape architect, interior designer, artist, or craftsman”). For possessing a variety of design features and elements that are sensitively handled and uncommon on the vernacular parking structure building type, the resource appears to be CRHR eligible under Criterion 3 (“The resource embodies the distinctive characteristics of a type, period, region, or method of construction or represents the work of an important creative individual or possesses high artistic values”).

Located just outside the boundary of the Community Concourse and possessing a period of significance of 1972, the Bow Wave Fountain is a historical resource for CEQA purposes. For its association with Malcolm Leland, a prolific and recognized Southern California artist and sculptor of the post–World War II era, the Bow Wave Fountain appears to be eligible as a San Diego Historical Landmark under Criterion D (“Is representative of the notable work of a master builder, designer, architect, engineer, landscape architect, interior designer, artist, or craftsman”). For representing the work of an important creative individual, the Bow Wave Fountain also appears to be eligible for listing on the CRHR under Criterion 3.

Constructed in 1971, the City Operations Building was not part of the Community Concourse and does not meet any city, CRHR, or NRHP criteria for landmark listing. The building is not a historical resource for CEQA purposes.

### **Impacts Discussion**

Impacts on cultural resources from the proposed project were evaluated by determining whether demolition activities would affect areas that contain historical sites listed on or eligible for listing on the NRHP or the CRHR or are designated as San Diego Historical Landmarks.

**The proposed project would demolish two buildings that are contributing elements of the Community Concourse complex, resulting in a significant impact on a historical resource.**

- **Community Concourse Complex.** The proposed project would entail demolishing two buildings within the Community Concourse boundary, the City Administration Building and the San Diego Convention and Performing Arts Center. Demolition of contributing elements of the historical resource are a significant effect under Section 15064.5 (b) (2)(C) of the State CEQA Guidelines.
- **Community Parking Garage (Evan V. Jones Parkade), 150 C Street, is individually eligible for the CRHR under Criterion 3.** The Community Concourse Parking Garage would not be demolished; however, the garage would be altered on the south elevation's second level terrace walkway. The garage was determined to be eligible individually as a San Diego Historical Landmark and CRHR eligible and therefore is considered a historical resource for the purposes of CEQA.

The proposed project would remove the convention center's second-floor terrace walkway, which connects to the garage's south elevation. The second-floor terrace walkway was not part of the original design of the garage. However, because the terrace walkway is attached, its removal would alter the present appearance of the resource, and repairs would be required. Alteration of a historical resource in a way that materially affects the physical characteristics that convey its significance in an adverse manner could be a significant effect under Section 15064.5 (b) (2)(A) of the State CEQA Guidelines.

- **Bow Wave Fountain, located in the center of the four square blocks bounded by A Street, 3<sup>rd</sup> Avenue, C Street, and 1<sup>st</sup> Avenue, is individually eligible for the CRHR under Criterion 3.** The Bow Wave Fountain would not be demolished and would be preserved in place. The fountain was determined to be eligible individually as a San Diego Historical Landmark and CRHR eligible and therefore is considered a historical resource for the purposes of CEQA. The proposed project would not cause a significant effect to the fountain because it would not be demolished or materially altered in an adverse manner.

### **Mitigation Measures**

**Community Concourse Complex.** The proposed project would entail demolishing two buildings within the Community Concourse boundary, the City Administration Building, and the San Diego Convention and Performing Arts Center. Prior to demolition of these elements of the Community Concourse, the buildings should be documented by narrative and photographs according to the standards of the Historic American Buildings Survey, and that documentation deposited in local archives such as the San Diego Public Library. In accordance with Section 15126.4(b)(2) of the CEQA Guidelines, the documentation would not mitigate the effects to a point where clearly no significant effect on the environment would occur.

**Community Concourse Parking Garage (Evan V. Jones Parkade), 150 C Street, individually eligible for the CRHR under Criterion 3.** The following mitigation measure may reduce the effect of the proposed project to less than significant: 1) remove the terrace walkway in a manner that preserves the important character-defining features of the garage and follow the Secretary of the Interior's Standards for the Treatment of Historic Properties (Secretary's Standards) when making repairs, in accordance with Section 15064.5 (b)(3) of the State CEQA Guidelines.

If the removal of the terrace walkway and redesign of the south elevation's second floor area is approved by the San Diego Historical Resources Board, that would reduce the effect to less than significant because the original portion of the historical resource that is still extant character-defining features would remain.

**Bow Wave Fountain, located in the center of the four square blocks bounded by A Street, 3<sup>rd</sup> Avenue, C Street, and 1<sup>st</sup> Avenue, is individually eligible for the CRHR under Criterion 3.** No mitigation is required because no significant effect was identified.

### **Conclusion**

For their collective contribution in spearheading a revitalization of downtown San Diego and for their association with the group San Diegans, Inc., the buildings and features constructed as part of the Community Concourse appear to be eligible under San Diego Historical Landmark Criteria A and B, period of significance 1964–1965. Additionally, within the Community Concourse, the San Diego Civic Theatre, period of significance 1965, is also eligible for landmark listing under city Criterion C. The proposed project would entail demolishing two buildings within the Community Concourse boundary, the City Administration Building and the San Diego Convention and Performing Arts Center. Demolition of contributing elements of a historic district are a significant effect under Section 15064.5 (b) (2)(C) of the State CEQA Guidelines.

Also within the Community Concourse, the Community Concourse Parking Garage is eligible under city Criteria C and D, in addition to CRHR Criterion 3. For the purposes of CEQA, all of the above are historical resources. Removal of the terrace walkway from the Community Concourse Parking Garage may be a significant effect, but the effect would be mitigated to a less-than-significant level by 1) carrying out repair and the design review in accordance with the Secretary Standards, 2) leaving extant the original portion of the historical resource.

For its association with Malcolm Leland, a prolific and recognized Southern California artist and sculptor of the post–World War II era, the Bow Wave Fountain, period of significance 1972, is eligible for San Diego Historical Landmark listing under city Criterion D and CRHR listing under Criterion 3. For CEQA purposes, the Bow Wave Fountain is a historical resource. It would be preserved in place, and no impacts or mitigation were identified.

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*San Diego Union Tribune*. 1964. Eleven-story 1,000-Car Garage Has Built-in 2-in-1 Feature. September 14, p. C9.

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# **APPENDIX A**

## **Building Development Information**

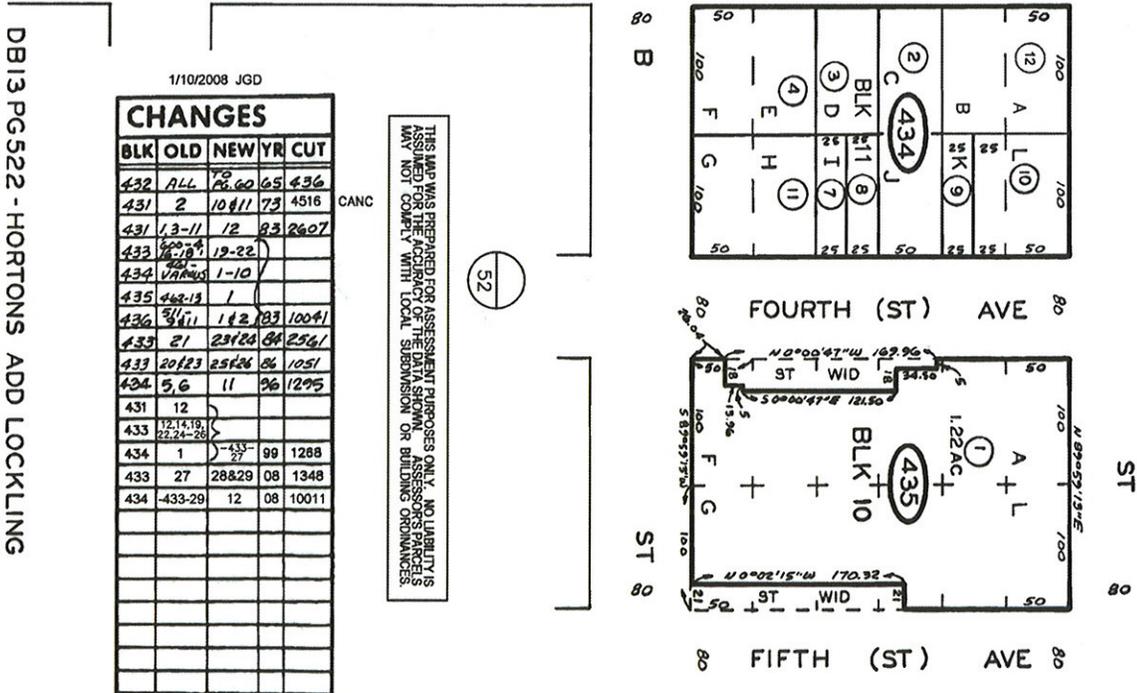
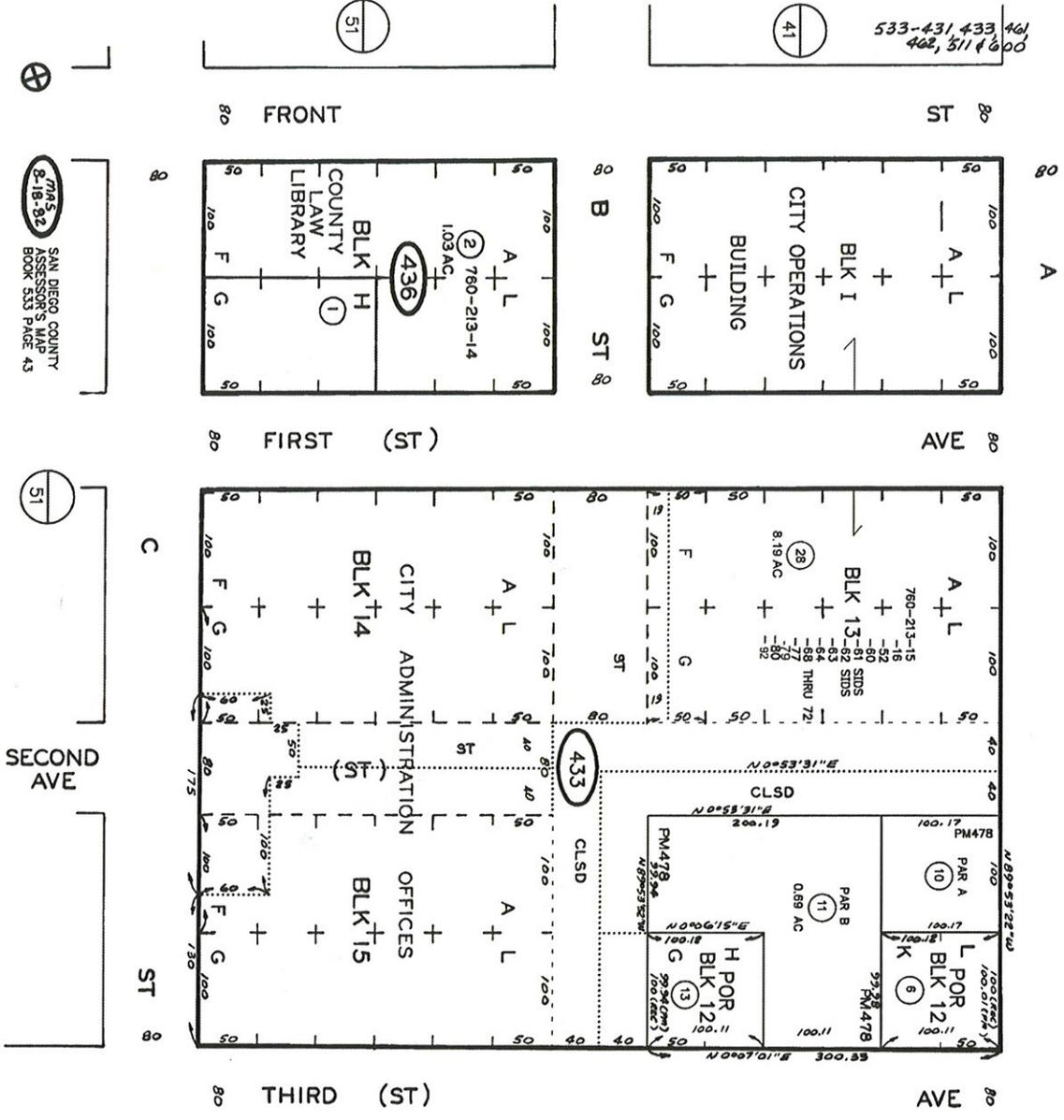
County Assessor's Building Record

Notice of Completion

Construction Permits

Lot and Block Book Page





51

8-18-82 SAN DIEGO COUNTY ASSESSOR'S MAP BOOK 533 PAGE 43

51

51

DB13 PG522 - HORTONS ADD LOCKLING

1/10/2008 JGD

BLK	OLD	NEW	YR	CUT
432	ALL	70	00	436
431	2	10	11	4516
431	1,3-11	12	03	2607
433	19-22	19-22		
434	1-10			
435	1			
436	142	03	100	41
433	21	23	08	2561
433	20	23	06	1051
434	5,6	11	06	1295
431	12			
433	12,14,19,27-28			
434	1	99	12	68
433	27	28	08	1348
434	433-29	12	08	10011

CANC

THIS MAP WAS PREPARED FOR ASSESSMENT PURPOSES ONLY. NO LIABILITY IS ASSUMED FOR THE ACCURACY OF THE DATA SHOWN. ASSESSOR'S PARCELS MAY NOT COMPLY WITH LOCAL SUBDIVISION OR BUILDING ORDINANCES.

52

42

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42

44

45

534

533-43



# Building Permit Application

APPLICANT FILL INSIDE HEAVY LINES

PLAN FILE NUMBER  
**11858A**

BUILDING PERMIT NUMBER

**A65278**

NAME (OR NAME OF BUSINESS)

**City of San Diego**

MAILING ADDRESS

**1600 - Pacific Highway**

CITY

**San Diego**

TELEPHONE NUMBER

**239-7511**

NAME

**Hatch, Wulff, Fifield & Associates**

ADDRESS

**1405 - 5th Avenue**

CITY

**San Diego**

TELEPHONE NUMBER

**239-2397**

NAME

**(A joint venture)**

**Nielsen, Stoll, Lindgren & Swinerton**

ADDRESS

**P. O. Box 10787**

CITY

**San Diego**

TELEPHONE NUMBER

**295-2165**

STATE LICENSE NUMBER

**211791**

CITY LICENSE NUMBER

**19813**

LOT

BLOCK #

**14 & 15**

TRACT

**Horton's Addition**

WORK TO BE DONE

**Construct new office building**

NEW

MOVE

NUMBER OF STORIES

**15**

ADD

DEMOLISH

ALTER

RESIDENTIAL

NUMBER OF DWELLING UNITS

REPAIR

NON-RESIDENTIAL

PROPOSED USE

**Administration Building**

I hereby acknowledge that I have read this application; that the information given is correct, and that I am the owner, or the duly authorized agent of the owner. I agree to comply with city and state laws regulating construction; and in doing the work authorized thereby, no person will be employed in violation of the Labor Code of the State of California relating to Workmen's Compensation Insurance.

SIGNATURE (OWNER OR AGENT)

DATE SIGNED

**NIELSEN, STOLL, LINDGREN & SWINERTON**

**6-12-63**

ADDRESS  
**P.O. Box 10787**

COUNTY SANITATION DISTRICT RECEIPT NO.

PRIVATE DISPOSAL APPROVAL

PLOT PLAN CHECK & APPROVED:

**Correct**

HEALTH DEPT. APPROVAL

JOB ADDRESS

**202 "C" Street**

SIDE YARD

**0**

SET BACK

**0**

REAR YARD

**0**

USE ZONE

**C**

MAP NUMBER

**22**

VACANT SITE

YES  NO

B.L.S. CODE

**016**

ECONOMIC LOCATION

CENSUS TRACT

**L-58**

BUILDING AREA

**140,000**

LOT AREA

**100**

VARIANCE NUMBER

ENCROACHMENT PERMIT REQUIRED

YES  NO

PERMIT NUMBER

STREET GRADE CHECK

METER SIZE

**Cleared per John Bell**

SERVICE SIZE

**WC**

CHECKED BY

**WC**

REMARKS

TYPE CONNECTION

BOOK

PAGE

VERIFIED BY

**Cleared per John Bell**

**WC**

FIRE ZONE

**I**

TYPE OF CONSTRUCTION

**I**

STREET IMPROVED

YES  NO

SPECIAL INSPECTOR REQUIRED

YES  NO

OCCUPANCY GROUP

**F-2/B-3**

PLAN CHECKED BY

**E. Vossekemper**

PLAN CHECK RECEIPT NUMBER

	NO. OF BLDGS.	PER/BLDG.	TOTAL FEE
1 BUILDING VALUATION	<b>1</b>	<b>\$3,000,000.00</b>	
2 BUILDING PERMIT FEE	<b>NO FEE</b>		
3 LESS PLAN CHECK FEE			
4 SUB-TOTAL OF 2-3			
5 PLUS SEWER FEE			
6 PLUS WATER FEE			
AMOUNT DUE			<b>NO FEE</b>

ATTENTION

THIS PERMIT AUTHORIZES ONLY THE WORK NOTED

INSPECTION DEPARTMENT



CITY OF SAN DIEGO

## APPLICATION APPROVAL

THIS APPLICATION IS NOT A PERMIT UNTIL SIGNED BY THE DIRECTOR OF BUILDING INSPECTION, OR HIS DEPUTY, AND FEES ARE PAID, AND RECEIPT IS ACKNOWLEDGED IN SPACE PROVIDED.

DIRECTOR'S SIGNATURE

DATE

**6/13/63**

and portion vacated 2nd Ave. between "B" & "C" Streets

PLANNING  
ENG.  
WATER  
SEWER  
INSPECTION



Building Permit Application			APPLICANT FILL INSIDE HEAVY LINES		PLAN FILE NUMBER	BUILDING PERMIT NUMBER																																
NAME (OR NAME OF BUSINESS)			CITY OF SAN DIEGO		11560-7	103452																																
MAILING ADDRESS			1600 Pacific Hwy		JOB ADDRESS																																	
CITY			San Diego		11560-7 Street																																	
TELEPHONE NUMBER			239-7511		SIDE YARD	SET BACK																																
NAME			F. Hope & Assoc.		USE ZONE	REAR YARD																																
ADDRESS			1475 6th		1																																	
CITY			San Diego		MAP NUMBER	VACANT SIDE																																
TELEPHONE NUMBER			232-7426		22	<input checked="" type="checkbox"/> YES <input type="checkbox"/> NO																																
NAME			F. B. Young Const. Co.		B.L.S. CODE	ECONOMIC LOCATION																																
ADDRESS			2141 Main St			CENSUS TRACT																																
CITY			San Diego		BUILDING AREA	6-53																																
TELEPHONE NUMBER					LOT AREA	VARIANCE NUMBER																																
STATE LICENSE NUMBER					ENCROACHMENT PERMIT REQUIRED																																	
CITY LICENSE NUMBER					<input type="checkbox"/> YES <input checked="" type="checkbox"/> NO																																	
LOT			BLOCK		METER SIZE	SERVICE SIZE																																
A 2141			14			CLEARANCE																																
WORK TO BE DONE			Construct Convention Hall		CHECKED BY																																	
PROPOSED USE			Convention Hall		REMARKS																																	
<input checked="" type="checkbox"/> NEW <input type="checkbox"/> ADD <input type="checkbox"/> ALTER <input type="checkbox"/> REPAIR			<input type="checkbox"/> MOVE <input type="checkbox"/> DEMOLISH <input type="checkbox"/> RESIDENTIAL <input checked="" type="checkbox"/> NON-RESIDENTIAL		TYPE CONNECTION																																	
NUMBER OF STORIES			7		BOOK	PAGE																																
NUMBER OF DWELLING UNITS						VERIFIED BY																																
SPECIAL INSPECTOR REQUIRED			<input type="checkbox"/> YES <input checked="" type="checkbox"/> NO		STREET IMPROVED																																	
OCCUPANCY GROUP			1		<input checked="" type="checkbox"/> YES <input type="checkbox"/> NO																																	
PLAN CHECKED BY					SPECIAL INSPECTOR REQUIRED																																	
PLAN CHECK RECEIPT NUMBER					OCCUPANCY GROUP																																	
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	NO. OF BLDG.	FEE/BLDG.	TOTAL FEE																																			
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7 AMOUNT DUE 4 & 5 & 6			NO FEE																																			
SIGNATURE (OWNER OR AGENT)			DATE SIGNED		<b>ATTENTION</b> THIS PERMIT AUTHORIZES ONLY THE WORK NOTED  INSPECTION DEPARTMENT  CITY OF SAN DIEGO																																	
ADDRESS			Assistant City Manager																																			
COUNTY SANITATION DISTRICT RECEIPT NO.			PRIVATE DISPOSAL APPROVAL		<b>APPLICATION APPROVAL</b> THIS APPLICATION IS NOT A PERMIT UNTIL SIGNED BY THE DIRECTOR OF BUILDING INSPECTION, OR HIS DEPUTY, AND FEES ARE PAID, AND RECEIPT IS ACKNOWLEDGED IN SPACE PROVIDED.  DIRECTOR'S SIGNATURE DATE																																	
PLOT PLAN CHECK & APPROVED:			HEALTH DEPT. APPROVAL:																																			

# Building Permit Application

APPLICANT FILL  
INSIDE HEAVY LINES

PLAN FILE NUMBER: 100-100-100-100  
BUILDING PERMIT NUMBER: 100-100-100-100

NAME (OR NAME OF BUSINESS): City of San Diego  
MAILING ADDRESS: Civic Center  
CITY: San Diego

JOB ADDRESS: 100-100-100-100  
SIDE YARD: [ ] SET BACK: [ ] REAR YARD: [ ]  
USE ZONE: [ ] MAP NUMBER: [ ] VACANT SITE:  YES  NO

NAME: Tucker Sadler Design  
ADDRESS: 2718 Fifth  
CITY: San Diego

B.L.S. CODE: [ ] ECONOMIC LOCATION: [ ] CENSUS TRACT: 1-53  
BUILDING AREA: [ ] LOT AREA: [ ] VARIANTS NUMBER: [ ]  
ENCROACHMENT PERMIT REQUIRED:  YES  NO PERMIT NUMBER: [ ] STREET GRADE CHECK: [ ]

NAME: F. H. Young Design, Inc.  
ADDRESS: 2718 Main Street  
CITY: San Diego

METER SIZE: [ ] SERVICE SIZE: [ ] CLEARANCE: [ ] CHECKED BY: [ ]  
REMARKS: [ ]  
TYPE CONSTRUCTION: [ ] ROOM: [ ] DATE: [ ] VERIFIED BY: [ ]

STATE LICENSE NUMBER: [ ] CONTRACTOR LICENSE NUMBER: [ ]  
LOT: [ ] BLOCK: [ ] TRACT: [ ]  
WORK TO BE DONE: Construct Exhibition & Parking Facility

SPECIAL INSPECTOR REQUIRED:  YES  NO OCCUPANCY GROUP: B2FB  
PLAN CHECKED BY: [ ] PLAN CHECK RECEIPT NUMBER: [ ]

PROPOSED USE: Parking and Exhibition  
NUMBER OF STORIES: 13  
NUMBER OF DWELLING UNITS: 0  
RESIDENTIAL:  NON-RESIDENTIAL:

	NO. OF UNITS	PER BLDG.	TOTAL FEE
1 BUILDING VALUATION			
2 BUILDING PERMIT FEE			
3 LESS PLAN CHECK FEE			
4 SUB-TOTAL OF 2-3			
5 PLUS SEWER FEE			
6 PLUS WATER FEE			
7 AMOUNT DUE 4 & 5 & 6			NO FEE

I hereby acknowledge that I have read this application; that the information given is correct and that I am the owner, or the duly authorized agent of the owner, and I agree to comply with city and state laws regulating construction, and in doing the work authorized thereby, no person will be employed in violation of the Labor Code of the State of California relating to Workmen's Compensation Insurance.

SIGNATURE (OWNER OR AGENT): [Signature] DATE SIGNED: [ ]  
ADDRESS: [ ]

## ATTENTION

THIS PERMIT AUTHORIZES ONLY THE WORK NOTED  
INSPECTION DEPARTMENT



CITY OF SAN DIEGO

## APPLICATION APPROVAL

THIS APPLICATION IS NOT A PERMIT UNTIL GRANTED BY THE DIRECTOR OF BUILDING INSPECTION, OR HIS DEPUTY, AND FEES ARE PAID, AND RECEIPT IS ACKNOWLEDGED IN SPACE PROVIDED.

DIRECTOR'S SIGNATURE: [Signature]  
DATE: [ ]

COUNTY SANITATION DISTRICT RECEIPT NO. [ ] PRIVATE DISPOSAL APPROVAL [ ]

PLOT PLAN CHECK & APPROVAL: [ ]

HEALTH DEPT. APPROVAL: [ ]



# Building Permit Application

APPLICANT FILL  
INSIDE HEAVY LINES

PLAN FILE NUMBER  
4460-B 11660-A

BUILDING PERMIT NUMBER  
AD3462

NAME (OR NAME OF BUSINESS)  
City of San Diego

JOB ADDRESS  
150 C Street

MAILING ADDRESS  
1500 Pacific Hwy

SIDE YARD  
SRT BACK  
RIAR YARD

CITY  
San Diego

TELEPHONE NUMBER  
239-7511

USE ZONE  
C

MAP NUMBER  
22

VACANT SITE  
 YES  NO

NAME  
F. Hope & Assoc.

B.L.S. CODE  
ECONOMIC LOCATION  
CENSUS TRACT  
L-53

ADDRESS  
1475 5th

BUILDING AREA  
LOT AREA  
100

VARIANCE NUMBER

CITY  
San Diego

TELEPHONE NUMBER  
232-7626

ENCROACHMENT PERMIT REQUIRED  
 YES  NO

PERMIT NUMBER

STREET GRADE CHECK

NAME  
F. E. Young Constr. Co.

METER SIZE  
SERVICE SIZE  
CLEARANCE  
CHECKED BY

ADDRESS  
2141 Main St

REMARKS

CITY  
San Diego

TELEPHONE NUMBER  
234-1841

TYPE CONNECTION  
BOOK  
PAGE  
VERIFIED BY

STATE LICENSE NUMBER  
CITY LICENSE NUMBER

FIRE ZONE  
TYPE OF CONSTRUCTION  
STREET IMPROVED  
 YES  NO

LOT  
BLOCK  
TRACT  
A thru L 14 Hortons Addition

SPECIAL INSPECTOR REQUIRED  
 YES  NO

OCCUPANCY GROUP  
B-2 & F-2

WORK TO BE DONE  
Construct Convention Hall

PLAN CHECKED BY  
PLAN CHECK RECEIPT NUMBER

PROPOSED USE  
Convention Hall

	NO. OF BLDGS.	PER/BLDG.	TOTAL FEE
1 BUILDING VALUATION		4,131.00	
2 BUILDING PERMIT FEE			
3 LESS PLAN CHECK FEE			
4 SUB-TOTAL OF 2-3			
5 PLUS SEWER FEE			
6 PLUS WATER FEE			
7 AMOUNT DUE 4 & 5 & 6			00 FEE

NEW  MOVE  
 ADD  DEMOLISH

NUMBER OF STORIES  
2

ALTER  RESIDENTIAL  
 REPAIR  NON-RESIDENTIAL

NUMBER OF DWELLING UNITS

PROPOSED USE  
Convention Hall

I hereby acknowledge that I have read this application; that the information is true and correct; and that I am the owner, or the duly authorized agent of the owner, I agree to comply with city and state laws regarding construction and in doing the work authorized thereby, no person will be employed in violation of the Labor Code of the State of California relating to Workers' Compensation Insurance.

SIGNATURE (OWNER OR AGENT)  
DATE SIGNED

ADDRESS  
Assistant City Manager

## ATTENTION

THIS PERMIT AUTHORIZES ONLY THE WORK NOTED

INSPECTION DEPARTMENT



## APPLICATION APPROVAL

THIS APPLICATION IS NOT A PERMIT UNLESS SIGNED BY THE DIRECTOR OF BUILDING INSPECTION, OR HIS DEPUTY, AND FEES ARE PAID, AND RECEIPT IS ACKNOWLEDGED IN SPACE PROVIDED.

DIRECTOR'S SIGNATURE  
DATE

COUNTY EXAMINATION DISTRICT  
PRIVATE DISPOSAL APPROVAL

PLAT PLAN CHECK & APPROVED

HEALTH DEPT. APPROVAL

INSPECTOR

**Building Permit Application** APPLICANT FILL INSIDE HEAVY LINES

**OWNER**  
 NAME (OR NAME OF BUSINESS) *Sectras Corp*  
 MAILING ADDRESS (NUMBER) (STREET) *411 So. Main St*  
 CITY *Los Angeles* TELEPHONE NUMBER *620/6155*

**DESIGNER**  
 NAME *Malcom Leland*  
 ADDRESS (NUMBER) (STREET) *P.O. Box 54 Potrero Ca.*  
 CITY *Potrero Ca.* TELEPHONE NUMBER *478-5806*

**BUILDER**  
 NAME *M.A. Golden Co.*  
 ADDRESS (NUMBER) (STREET) *12 Camino De La Reina*  
 CITY *San Diego* TELEPHONE NUMBER *232-6538*  
 STATE LICENSE NUMBER *174* CLASS NO. *A* CITY LICENSE NUMBER

**JOB LOCATION**  
 LOT *12* BLOCK *Horton's Addition* SUBDIVISION *UNIT*  
 JOB ADDRESS *1200 Third Ave*  
 CONDITION OF SOIL AT JOB SITE  
 ORIGINAL  COMPACTED FILL  LOOSE FILL  
 NO. OF EXISTING BUILDINGS ON LOT AND USE *one office bldg*

**PROPOSED WORK**  
 DESCRIBE WORK TO BE DONE *erect fountain sculpture*  
 EXISTING USE OF BUILDING OR PROPERTY  
 PROPOSED USE OF BUILDING OR PROPERTY *Fountain*

I hereby acknowledge that I have read this application; that the information given is correct; and that I am the owner, or the duly authorized agent of the owner. I agree to comply with city and state laws regulating construction; and in doing the work authorized thereby, no person will be employed in violation of the Labor Code of the State of California, relating to Workmen's Compensation Insurance.

SIGNATURE (OWNER OR AGENT) *[Signature]* DATE SIGNED *9-28-71*  
 AGENT FOR *M.A. Golden Co.*  
 ADDRESS *123 Camino De La Reina.*

METER SIZE SERVICE SIZE CREDIT CHECKED BY  
 REMARKS  
 NO. ADDITIONAL CONNECTIONS REQ'D. TYPE CONN. CHECKED BY  
 REMARKS

JOB ADDRESS *1200 Third Ave*

CENBUS TRACT NUMBER *53.00* PERMIT NUMBER *117398*

USE ZONE *C* COORD. INDEX *200-1719* PLAN FILE NO. *41675-D*

LOT AREA ALLOW COVERAGE % TOTAL AREA COVERED SQ. FT.  
 SETBACK FRONT YARD REAR YD. SIDE YD. (INT.) (STR.) NAME OF STREET  
 VARIANCE NO. LOT SPLIT DATE HEALTH DEPT. APPROVAL

L.D. PRMT REQ'D  YES  NO ST. IMP. PRMT. REQ'D  YES  NO ENCR PRMT REQ'D  YES  NO CHECKED BY  
 CURB TO P.L. F. S. WORK TO BE DONE SIGN MOVE ALTER DEMOLISH REPAIR NON-RESID RESIDENTIAL  
 PLAN CHK RECPT. AMT. \$ *3209660*

VALUATION OF WORK NO. UNITS PER UNIT TOTAL *5007*

FUND & ACC'T. PLAN CHECK FEE SUPPLEMENTAL PLAN CHK. FEE BUILDING PERMIT FEE  
 100 7342 SUB-TOTAL *5007*  
 506 79750 SEWER FEE  
 500 79080 WATER FEE

SPECIAL INSPECTION REQUIRED FOR  
 CONCRETE  
 MASONRY  
 WELDING, H.S. BOLTS  
 PILE DRIVING  
 OTHER (IDENTIFY)

TOTAL FEES DUE  
 FIRE ZONE *1* TYPE OF CONST. OCCUP. ORP.  
 BLDG. AREA NO. STORIES TOT. FLR AREA  
 SPRINKLERS REQ'D FOR

ATENTION THIS PERMIT AUTHORIZES ONLY THE WORK NOTED  
 INSPECTION DEPARTMENT  
  
 CITY OF SAN DIEGO

PLANS CHECKED DATE  
 PLANS APPROVED DATE  
 PLOT PLAN CHR'D & APPR'D DATE  
 APPLICATION APPROVAL  
 THIS PERMIT DOES NOT BECOME VALID UNTIL SIGNED BY THE DIRECTOR OF BUILDING INSPECTION, OR HIS DEPUTY, AND FEES ARE PAID, AND RECEIPT IS ACKNOWLEDGED IN SPACE PROVIDED.  
 SIGNATURE OF DIR. OF INSP. DEPUTY  
 DATE *9/28/71* INSPECTOR

**APPENDIX B**  
**Ownership and Occupant Information**



### **Ownership and Occupant Information**

Except for the Bow Wave fountain, all surveyed buildings and associated resources have been owned and occupied by the City of San Diego since their completion.

The property owners of the Bow Wave fountain parcel are Lowe SD CA Prop LLC.  
1200 3<sup>rd</sup> Avenue, San Diego, CA, 92101-4110



## **APPENDIX C**

### **Maps**

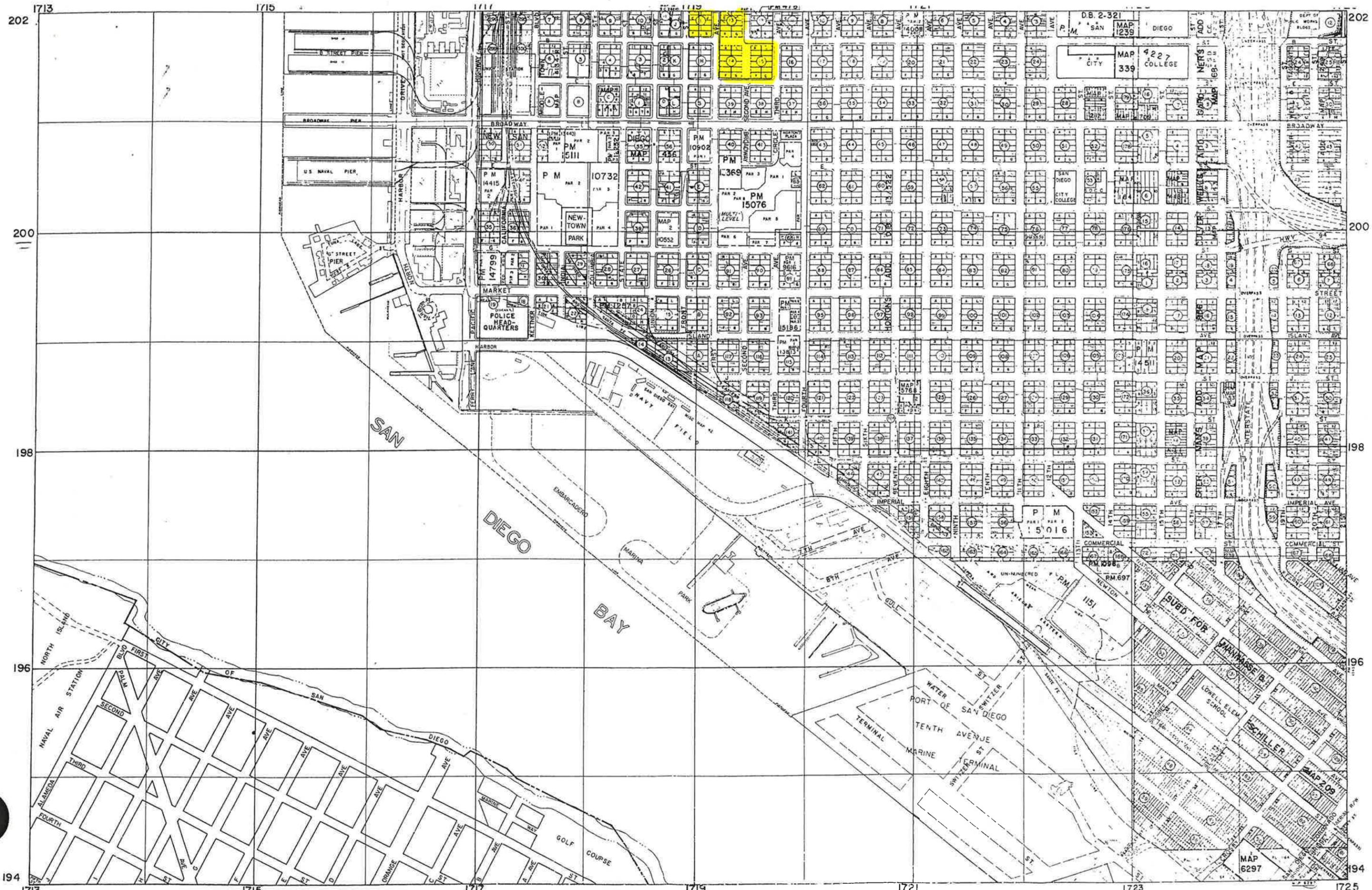
City of San Diego 800 Scale Engineering Map (with north arrow and location)

Regional Vicinity Map

Project Location Map

Area of Potential Effects (APE) Map





CITY OF SAN DIEGO, CALIFORNIA

BASE MAP

202-17.3 194-1713







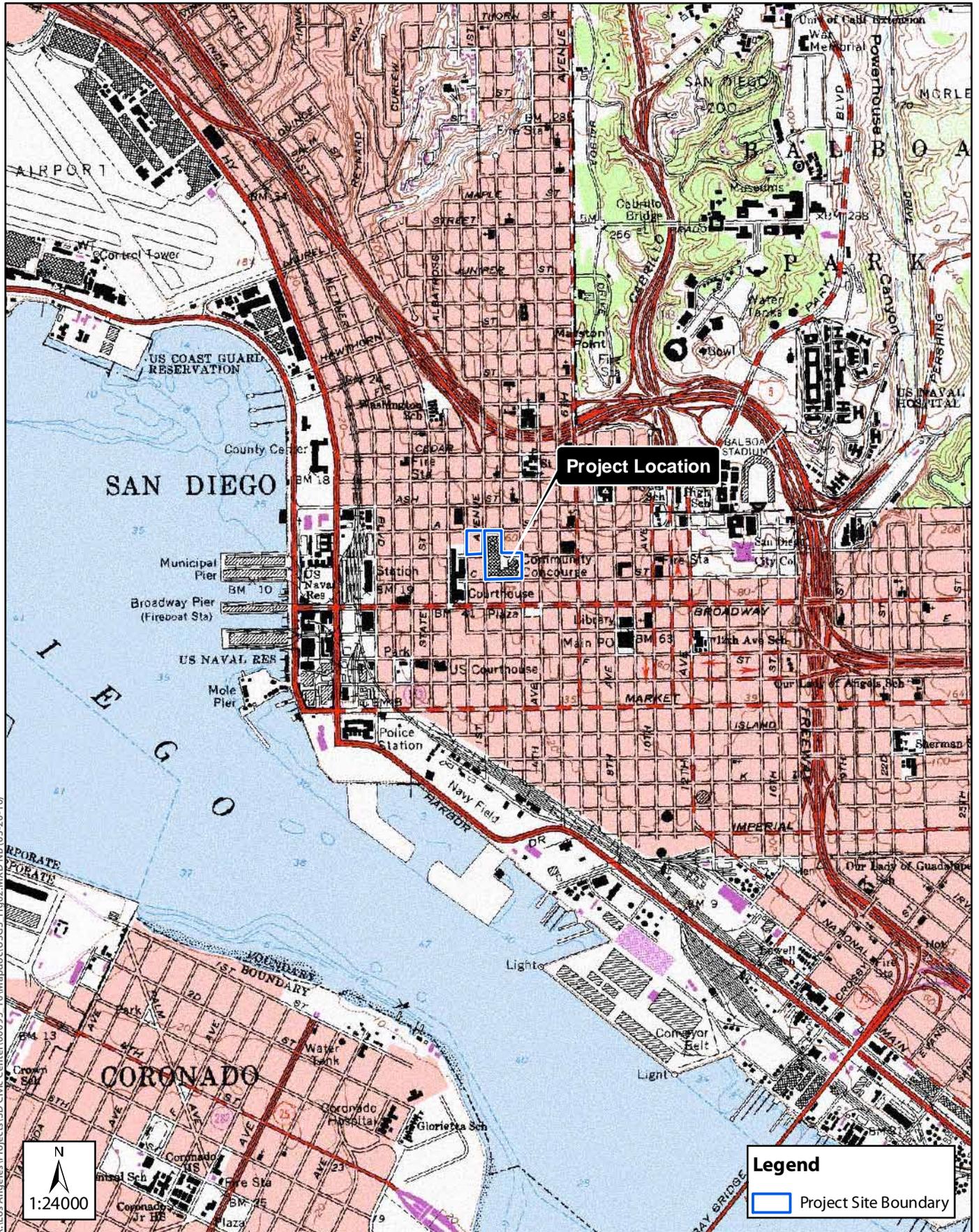
K:\Los Angeles\Projects\SD Civic Center\00095\_10\Mapdoc\Reg\_Loc\_FIG01.MXD NB (03-20-10)

SOURCE: ESRI Streetmap USA (2008)

**Figure 1**  
**Regional Location**



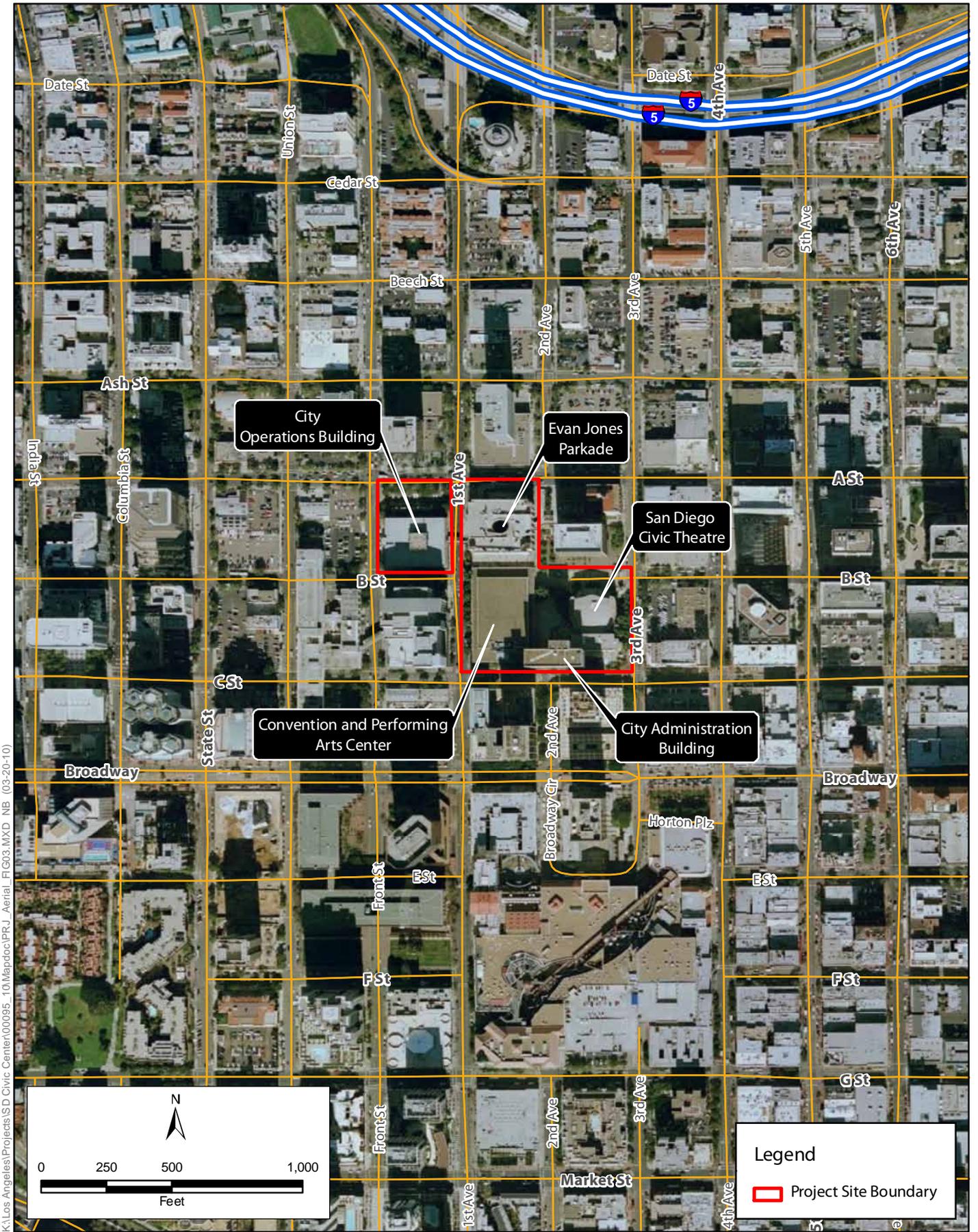




Source: USGS 7.5' Quad., California: Point Loma (1978)

Figure 2  
USGS Map





K:\Los Angeles\Projects\SD Civic Center\00095\_10\Mapdoc\PRJ\_Aerial\_FIG03.MXD NB (03-20-10)

SOURCE: ESRI Streetmap USA (2008)



**Figure 3**  
**Project Vicinity**



**APPENDIX D**  
**DPR Forms**

DPR 523 Inventory Forms: Primary, BSO, and Continuation Sheets



# PRIMARY RECORD

Other Listings \_\_\_\_\_  
Review Code \_\_\_\_\_ Reviewer \_\_\_\_\_ Date \_\_\_\_\_

Page 1 of 5

\* Resource Name or #: San Diego Community Concourse

P1. Other Identifier: San Diego Civic Center/ Charles C. Dial Community Concourse

\* P2. Location:  Not for Publication  Unrestricted a. County San Diego

b. USGS 7.5' Quad Point Loma, CA Date 1996 T \_\_\_\_\_; R \_\_\_\_\_; \_\_\_\_\_ 1/4 of \_\_\_\_\_ 1/4 of Sec \_\_\_\_\_; \_\_\_\_\_ B.M.

c. Address 202 C St. City San Diego Zip 92101

d. UTM: (Give more than one for large and/or linear feature) Zone 11, 484744 mE/ 369980 mN

e. Other Locational Data: (e.g. parcel #, legal description, directions to resource, elevation, additional UTMs, etc. as app

Assessor's Parcel Number: 533-433-28-00. ST CLSD&BLKS 13 THRU 15.

\* P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.)  
The San Diego Community Concourse is a four acre complex that serves as the civic center for the city of San Diego, CA. The Concourse consists of four buildings: the Community Concourse Parking Garage (Evan V. Jones Parkade), the Convention and Performing Arts Center, the multi-story City Administration building which houses City Hall, and the San Diego Civic Theatre. The buildings are arranged on a large property that traverses north-south. The southernmost edge of this property contains the City Administration Building. Affixed to its right (east) is the Civic Theatre, and attached due west of and behind the Administration Building is the long, low-rise Convention and Performing Arts Center. Affixed at north end of it is the multi-story Evan Jones Parkade. A primary part of the Civic Center property is devoted to an open plaza with various landscape features and other artworks that are character defining features of the District. Terrazzo design work is present upon the southern (original) portion of the plaza, and centered within it is a terrazzo map of San Diego in the year 1542. Irregular shaped planters that border what was originally the upper end of the court just below what was then B Street are still present and feature mature tree and bush specimens. Original light fixtures have drop-down white globes suspended beneath circular metal strip casings and are either paired off of metal poles or are affixed to various buildings. Facing the Courtyard and other public elevations, running across all of the Concourse buildings are a continuous band of repeating concrete panels by the artist Malcolm Leland. The panels have an abstract, slightly bimorphic vertical design, and unify the buildings that were completed by various architecture firms.

\* P3b. Resource Attributes: (List attributes and codes) HP14. Government Building

\* P4. Resources Present:  Building  Structure  Object  Site  District  Element of District  Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects) P5b. Description of Photo: (View, date, etc.)



Looking south, 3/19/2010,

Photo#P1030103.jpg

\* P6. Date Constructed/Age and Sources:

Prehistoric  Historic  Both

1964-1965

\* P7. Owner and Address:

City of San Diego

202 C Street

San Diego, CA 92101

M--Municipal

\* P8. Recorded by: (Name, affiliation, address)

Daniel Paul / David Greenwood

ICF International

811 W 7th ST, Suite 800

Los Angeles, CA 90017

\* P9. Date Recorded: 3/26/2010

\* P10. Survey Type: (Describe)

Intensive Level Survey

\* P11. Report Citation: (Cite survey report/other sources or "none") Downtown San Diego Community Plan Environmental Imp

\* Attachments:  NONE  Location Map  Sketch Map  Continuation Sheet  Building, Structure, and Object Record  
 Archaeological Record  District Record  Linear Feature Record  Milling Station Record  Rock Art Record  Artifact Record  
 Photograph Record  Other: (List) \_\_\_\_\_

# BUILDING, STRUCTURE, AND OBJECT RECORD

Page 0 of 5

\* NRHP Status Code 5S3

\* **Resource Name or #:** San Diego Community Concourse

B1. Historic Name: San Diego Community Concourse

B2. Common Name: San Diego Civic Center; Charles C. Dial Community Concourse

B3. Original Use: Institutional

B4. Present Use: Institutional

\* **B5. Architectural Style:** various (Modern)

\* **B6. Construction History:** (Construction date, alterations, and date of alterations.)

First models constructed in 1962, project underwent numerous changes before being constructed with various buildings completed between 1964 and 1965.

\* **B7. Moved?**  No  Yes  Unknown Date \_\_\_\_\_ Original Location: \_\_\_\_\_

\* **B8. Related Features:**

B9a. Architect: various

b. Builder: \_\_\_\_\_

\* **B10. Significance:** Theme Community Planning and Development Area Downtown San Diego (Centre City)

Period of Significance 1964-1965 Property Type Institutional Applicable Criteria City: A,B

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The Community Concourse appears to be eligible as a City of San Diego Historical Landmark under Criterion A: "Exemplifies or reflects special elements of the city's, a community's, or a neighborhood's historical, archeological, cultural, social, economic, political, aesthetic, engineering, landscaping, or architectural development." Additionally, the Community Concourse appears to be eligible as a City of San Diego Historical Landmark under Criterion B: "Is identified with persons or events significant in local, state, or national history." The period of significance for the Community Concourse is 1964-1965, which encompasses the completion dates for the various buildings within the complex. Regarding City Criterion A, the Community Concourse "reflects special elements of a community's economic development." Largely credited with spearheading downtown (City Centre) revitalization at a time when it was desperately needed, its completion in 1965 served as a catalyst that drew millions of dollars to downtown for major construction projects. In anticipation of new construction, private investment totaling \$38 million financed the 20-story Home Federal building and two other skyscrapers. For city Criterion B, the Concourse's important place in Centre City revitalization is also an "event" significant in local history, one that is associated with the "persons" of San Diegans, Inc. Regarding City Criterion C, the Community Concourse as a whole does not embody distinctive characteristics of a style, type, period, or method of construction and is not a valuable example of the use of indigenous materials or craftsmanship. Except for the Community Concourse Parking Garage and the San Diego Civic Theatre, other buildings associated with the Community Concourse do not appear to be historically significant examples of architectural design. City of San Diego Historical Landmark Criterion D states that a resource can be a city landmark if (Please see continuation sheet)

B11. Additional Resource Attributes: (List attributes and codes): \_\_\_\_\_

\* **B12. References:**

Brooks, Joe. "Gala Ceremonies Open Concourse." The San Diego Union 16 Sept. 1964: a1. Print.

Hof, Reiner M. "San Diegans, Inc. [...]" The Journal of San Diego History V.36, No.1, Winter 1990.

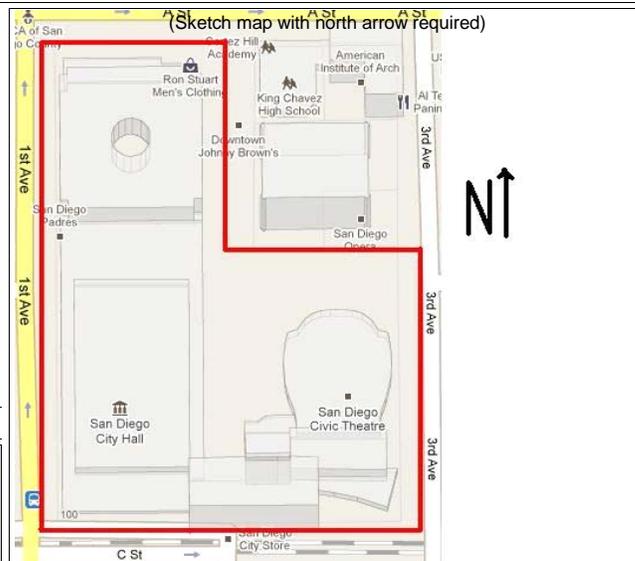
City of San Diego. "San Diego Modernism: Historic Context Statement" 2007, 91-92.

B13. Remarks:

\* **B14. Evaluator:** Daniel Paul

Date of Evaluation: 3/26/2010

(This space reserved for official comments.)



## CONTINUATION SHEET

Page 1 of 5 \* Resource Name or #: (Assigned by recorder San Diego Community Concourse  
\* Recorded by: Daniel Paul / David Greenwood \* Date: 3/26/2010  
 Continuation  Update

Significance, ctd:

it "is representative of the notable work of a master builder, designer, architect, engineer, landscape architect, interior designer, artist, or craftsman." Overseeing the completion of the Community Concourse was Samuel W. Hamill, FAIA. A significant local architect, Hamill had redesigned many of the Balboa Park Exposition buildings for cultural purposes and was the chief designer of the 1938 San Diego Civic Center. For the new Community Concourse, Hamill was not listed as architect on any building permits, though he did mention in an interview that he tried to unify the various architects and follow Modernist ideas, which were popular in Europe. Hamill appears to have had more of a ministerial role, working as a coordinator between the various architects, San Diegans, Inc., and the other stakeholders. Though Hamill is associated with the Community Concourse, the work is not representative of Hamill the way that the pre-existing Civic Center, and his Balboa Park work is; both of which he had a much more hands-on involvement as an architect. The Community Concourse does not appear eligible as a landmark under City Criterion D.

The social significance of the Community Concourse as a whole does not appear to warrant California Register listing under Criterion 1, or National Register Criterion A. The historic importance of the Community Concourse is highly specific to San Diego, if not Centre City itself. The same holds true for San Diegans, Inc. as an entity of persons. The significance of San Diegans, Inc. to the civic center is strictly at the municipal level, and that association does not appear to warrant the listing of the Community Concourse as a historic resource under California Register Criterion 2 or National Register Criterion B. With respect to the design, the Community Concourse as a complex does not appear to have design significance under CRHR Criterion 3, or NRHP Criterion C. Furthermore, the Concourse does not appear to meet NRHP Criterion Consideration G as an exceptional example of a resource under 50 years old.

Page 5\_ of \_5 Resource Name or #: San Diego Community Concourse

APN: 533-433-28-00

\* Recorded by: Daniel Paul / David T. Greenwood

Continuation     Update

**P3b.** Resource Attributes: HP14. Government building[s]; HP12. Civic Auditorium; HP 10. Theater; HP39. Other.

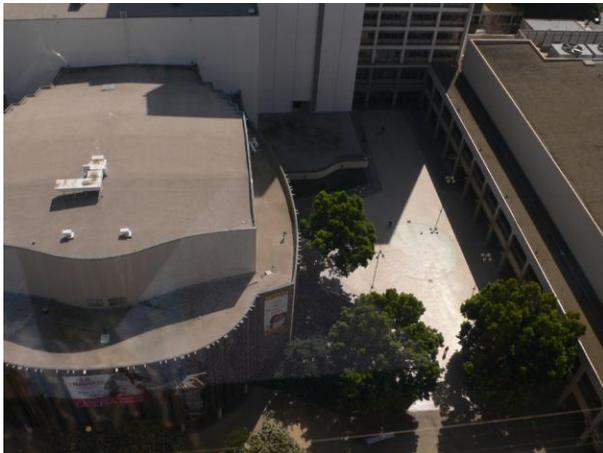
**Additional Photographs:**



Plaza, Community Concourse. View: North.



Plaza, Community Concourse. View: South.



Overview, Community Concourse.  
View: South, downward.



Terrazzo detail, Community Concourse Plaza.

# PRIMARY RECORD

Other Listings \_\_\_\_\_  
Review Code \_\_\_\_\_ Reviewer \_\_\_\_\_ Date \_\_\_\_\_

Page 1 of 3

\* Resource Name or #: City Administration Building

P1. Other Identifier: \_\_\_\_\_

\* P2. Location:  Not for Publication  Unrestricted a. County San Diego  
b. USGS 7.5' Quad Point Loma, CA Date 1996 T \_\_\_\_\_; R \_\_\_\_\_; \_\_\_\_\_ 1/4 of \_\_\_\_\_ 1/4 of Sec \_\_\_\_\_; \_\_\_\_\_ B.M.  
c. Address 202 C Street City San Diego Zip 92101  
d. UTM: (Give more than one for large and/or linear feature) Zone 11, 484745 mE/ 3619921 mN  
e. Other Locational Data: (e.g. parcel #, legal description, directions to resource, elevation, additional UTMs, etc. as app  
Assessor's Parcel Number: 533-433-28-00. ST CLSD&BLKS 14 AND 15.

\* P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.)  
The City of San Diego City Administration Building is a rectangular plan, 15 story office tower. The building features a scored concrete base at either end of its south-facing front elevation. Between the base elements are numerous large, glass-enclosed open bays at the ground level. Double height square columns of scored concrete, similar to the base, are present at the lower levels of the front elevation. Above the ground level is a slightly recessed balcony of pink mosaic tile of varying shades. Applied vertical aluminum strips further break up these bays. Above these lower two levels, the upper 13 levels contain large, full-height vertical concrete fins that protrude out from the south elevation between each window bay. Affixed to these fins at the spandrel level of each floor are a pairs of concave shaped sunshades. The windows behind them are not readily visible and are ribbon windows with aluminum frames. The entirety of the upper 13 floors' south elevation is enframed in a continuous, box-shaped, concrete surround. At the upper portion of the south elevation, alternating metal fins are full-height and touch the upper edge of the surround, and behind them at the uppermost level is a blank concrete background of a light pink color. The two lower levels of the tower are slightly recessed from the 13 upper levels. The ground floor is largely open. In middle of it is a breezeway that connects over to the courtyard behind the tower. The narrow, side elevations of the tower are concrete and are largely blank except for four scored lines that run the entirety of their height, from just above the recessed lower two levels to the top of the tower. (See continuation sheet).

\* P3b. Resource Attributes: (List attributes and codes) HP14. Government Building

\* P4. Resources Present:  Building  Structure  Object  Site  District  Element of District  Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects)



P5b. Description of Photo: (View, date, etc.)

Looking north, 2/11/2010,  
Photo#P1030069.jpg

\* P6. Date Constructed/Age and Sources:  
 Prehistoric  Historic  Both

1964

\* P7. Owner and Address:  
City of San Diego

202 C Street  
San Diego, CA 92101  
M--Municipal

\* P8. Recorded by: (Name, affiliation, address)  
Daniel Paul / David Greenwood

ICF International  
811 W 7th ST, Suite 800  
Los Angeles, CA 90017

\* P9. Date Recorded: 3/26/2010

\* P10. Survey Type: (Describe)  
Intensive Level Survey

\* P11. Report Citation: (Cite survey report/other sources or "none") Downtown San Diego Community Plan Environmental Imp

\* Attachments:  NONE  Location Map  Sketch Map  Continuation Sheet  Building, Structure, and Object Record  
 Archaeological Record  District Record  Linear Feature Record  Milling Station Record  Rock Art Record  Artifact Record  
 Photograph Record  Other: (List) \_\_\_\_\_

# BUILDING, STRUCTURE, AND OBJECT RECORD

Page 2 of 3

\* NRHP Status Code 5S3

\* Resource Name or #: City Administration Building

B1. Historic Name: San Diego City Administration Building

B2. Common Name: San Diego City Hall

B3. Original Use: City Administration Building

B4. Present Use: City Administration Building

\* B5. Architectural Style: Modern

\* B6. Construction History: (Construction date, alterations, and date of alterations.)

On June 13, 1963, San Diego Building permit No. H17398 was issued to City of San Diego to construct a new office building for the estimated cost of \$3,000,000.

\* B7. Moved?  No  Yes  Unknown Date \_\_\_\_\_ Original Location: \_\_\_\_\_

\* B8. Related Features:

B9a. Architect: George Hatch

b. Builder: \_\_\_\_\_

\* B10. Significance: Theme Community Planning and Development

Area San Diego (City Centre)

Period of Significance 1965

Property Type Building

Applicable Criteria City: A, B

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

Similar to the other resources that comprise the Community Concourse, the City of San Diego Administration Building appears eligible as a San Diego Historical Landmark under City Criterion A and B. This is due to the building being an element of the Community Concourse project, which played a significant role in spearheading the revitalization of downtown San Diego (City Centre) in the mid-1960s. Like the other buildings of the Community Concourse, the City Administration Building is considered a significant achievement of San Diegans, Inc., a locally important group of business people who advocated the regeneration of downtown through the use of city planning and various downtown business and development measures. The City Administration Building does not embody distinctive examples of a design that would warrant historic landmark listing under City Criterion C. The City of San Diego Administration Building is a relatively intact but common example of a 1960s-era tower, with its rectangular box elevation, decorative concrete screen with vertical concrete fins across the front elevation, mosaic accents, articulated base with mosaic-covered balconies, and open ground floor that includes a breezeway to the concourse itself. Though the mosaic city seal on the path of the breezeway is one of the more distinguished design elements of the building, this alone does not elevate the design of the building to a level of historic significance. Overall, the design reads as a common example of its type, a Miesian-inspired postwar tower with some added decorative elements. (See continuation sheet).

B11. Additional Resource Attributes: (List attributes and codes): \_\_\_\_\_

\* B12. References:

B13. Remarks:

\* B14. Evaluator: Daniel Paul

Date of Evaluation: 3/26/2010

(This space reserved for official comments.)



## CONTINUATION SHEET

Page 3 of 3 \* Resource Name or #: (Assigned by recorder City Administration Building)  
\* Recorded by: Daniel Paul / David Greenwood \* Date: 3/26/2010  
 Continuation  Update

### P3a. Description:

The rear elevation of the City Administration Building faces north onto the Concourse plaza. The rear elevation is treated differently than the front, with 6 window bays across each floor, each with groupings of four windows. Each bay is slightly recessed behind a large concrete framing system running across the entirety of the elevation. Tope-colored metal spandrel panels are also present at the rear elevation. Affixed at the rear elevation is a large, rectangular plan component, presumably elevator shafts, that is clad entirely in concrete and features the same full-height scoring seen at the side elevations. This component does not touch the ground, but instead ends at the roof of a low-rise component building of the Civic Theatre. Likewise, the southern end of the Convention and Performing Arts Center is also connected to the City Administration Building.

### B10. Significance:

The City Administration Building does not appear eligible as a City of San Diego Historical landmark under City Criterion D. Although George Hatch completed other municipal buildings in the city, including the San Diego Central Library and the City Operations Building (Hatch and Heimerdinger), Hatch, Wulf & Fifield Associates, the architectural firm of record for the building, is not considered a master architectural firm, according to the San Diego Modernism Historic Context Statement.

The level of significance of the resource as an element that spearheaded downtown revitalization, one associated with San Diegans Inc, does not appear to warrant listing of the resource on either the California Register or the National Register under Criteria 1 or 2, or A or B, respectively. The building has served as the center of San Diego Government since 1964 but this alone does not render the building historically significant under California Register Criterion 1 or National Register Criterion A.

Just as its design and architectural significance is insufficient for City listing under local Criteria C or D, the building does not appear to have the quality of significance necessary for listing as a significant example of 1960s mid-century modern architectural design under California Register Criterion 3, or National Register Criterion C. The City Administration Building is not National Register eligible under Criterion Consideration G as an exceptionally significant resource that is less than 50 years old.

# PRIMARY RECORD

Other Listings \_\_\_\_\_  
Review Code \_\_\_\_\_ Reviewer \_\_\_\_\_ Date \_\_\_\_\_

Page 1 of 3

\* Resource Name or #: San Diego Convention and Performing Arts Center

P1. Other Identifier: \_\_\_\_\_

\* P2. Location:  Not for Publication  Unrestricted a. County San Diego

b. USGS 7.5' Quad Point Loma, CA Date 1996 T \_\_\_\_\_; R \_\_\_\_\_; \_\_\_\_\_ 1/4 of \_\_\_\_\_ 1/4 of Sec \_\_\_\_\_; \_\_\_\_\_ B.M.

c. Address 202 C St. City San Diego Zip 92101

d. UTM: (Give more than one for large and/or linear feature) Zone 11, 484686 mE/ 3169979 mN

e. Other Locational Data: (e.g. parcel #, legal description, directions to resource, elevation, additional UTMs, etc. as app  
Assessor's Parcel Number: 533-433-28-00. ST CLSD&BLK 14.

\* P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.)  
The San Diego Convention and Performing Arts Center, is a large, two-story, flat-roofed, rectangular plan building acclimated north-south, and its southern portion is connected to the City Administration Building. Its predominant exterior feature is a continuous, repeating set of full-height exposed concrete columns seamlessly meeting a continuous lintel, that together define a series of repeating bays. Each of these bays features round- cornered headers. Between the first and second levels, traversing the entirety of the building and running continuously between these are repeating concrete decorative panels of a vertically oriented abstract design completed by the artist Malcolm Leland. At the east facing elevation, which faces onto the court, recessed within these bays at each level is a long, wide, double loggia. The upper level loggia has metal railing at its outer edge, and this railing has been altered. Inset within the loggia and the colonnade, the structural elevation itself has bays of exposed concrete block. The main entries into the building are also at the east elevation, and feature paired aluminum framed glass doors with a large transom window. The east-facing elevation features four ticket windows at the lower level, and two additional ticket windows at the upper level. The upper level bays of the building's southwest portion feature fixed glazing set between thick aluminum mullions. The Convention and Performing Arts Center Building features original backlit signage with a 1960s era logo, and marquees are also affixed at various places. At the south end of the long covered concourse glass door entries into the City Administration Building are present. (See continuation sheet).

\* P3b. Resource Attributes: (List attributes and codes) HP12 Civic auditorium

\* P4. Resources Present:  Building  Structure  Object  Site  District  Element of District  Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects)

P5b. Description of Photo: (View, date, etc.)

Looking west, 2/11/2010,  
Photo#P1030064.jpg

\* P6. Date Constructed/Age and Sources:  
 Prehistoric  Historic  Both

1964

\* P7. Owner and Address:

City of San Diego

202 C Street

San Diego, CA 92101

\* P8. Recorded by: (Name, affiliation, address)

Daniel Paul / David Greenwood

ICF International

811 W 7th ST, Suite 800

Los Angeles, CA 90017

\* P9. Date Recorded: 3/26/2010

\* P10. Survey Type: (Describe)

Intensive Level Survey



\* P11. Report Citation: (Cite survey report/other sources or "none") Downtown San Diego Community Plan Environmental Imp

\* Attachments:  NONE  Location Map  Sketch Map  Continuation Sheet  Building, Structure, and Object Record  
 Archaeological Record  District Record  Linear Feature Record  Milling Station Record  Rock Art Record  Artifact Record  
 Photograph Record  Other: (List) \_\_\_\_\_

# BUILDING, STRUCTURE, AND OBJECT RECORD

Page 2 of 3

\* NRHP Status Code 5S3

\* Resource Name or #: San Diego Convention and Performing Arts Center

B1. Historic Name: \_\_\_\_\_

B2. Common Name San Diego Convention and Performing Arts Center

B3. Original Use: Civic Theater

B4. Present Use: Convention and Performing Arts

\* B5. Architectural Style: Brutalism and New Formalism

\* B6. Construction History: (Construction date, alterations, and date of alterations.)

On April 12, 1965, San Diego Building permit No. A63462 was issued to City of San Diego to erect a convention hall for the estimated cost of \$4,131,990.

\* B7. Moved?  No  Yes  Unknown Date \_\_\_\_\_ Original Location: \_\_\_\_\_

\* B8. Related Features:

B9a. Architect: \_\_\_\_\_ b. Builder: \_\_\_\_\_

\* B10. Significance: Them \_\_\_\_\_ Area San Diego

Period of Significance 1965 Property Type \_\_\_\_\_ Applicable Criteria City: A,B

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

Completed in 1964 and similar to the other resources that comprise the Community Concourse, the Convention and Performing Arts Center appears eligible as a San Diego Historical Landmark under City Criterion A and B. This is due to the building being an element of the Community Concourse project, which played a significant role in spearheading the revitalization of downtown San Diego (City Centre) in the mid-1960s., the Convention and Performing Arts Center, as a component of the Community Concourse is associated to the significant "persons" of San Diegans, Inc., a locally important group of business people who advocated the regeneration of downtown through the use of city planning and various downtown business and development measures. By the mid-1970s, the San Diego Convention and Performing Arts Center was a locally notable venue for rock acts such as Pink Floyd, the Rolling Stones, and Bob Dylan, among others. However, the fact that such significant rock bands performed within the venue does not contribute any more to its significance under City Criteria A or B. The Convention and Performing Arts Center does not embody distinctive examples of a design that would warrant historic landmark listing under City Criterion C. As a mix of Brutalist and New Formalism design systems, the building is not a successful example of either. Similar to other institutional buildings of the 1960s, the building appears to use Brutalist design traits as an excuse for cost efficiency. The materials, which include exposed concrete block and sandblasted aggregate, are displayed on a large, rectangular box-shape building that fails to convey the dynamism of more sculptural examples of Brutalism. (please see continuation sheet)

B11. Additional Resource Attributes: (List attributes and codes): \_\_\_\_\_

\* B12. References:

B13. Remarks:

\* B14. Evaluator: \_\_\_\_\_

Date of Evaluation: \_\_\_\_\_

(This space reserved for official comments.)



## CONTINUATION SHEET

Page 3 of 3 \* Resource Name or #: (Assigned by recorder San Diego Convention and Performing Arts Center  
\* Recorded by: Daniel Paul / David Greenwood \* Date: 3/26/2010  
 Continuation  Update

### P3a. Description:

The northern, rear portion of the building is connected to the Community Concourse Parking Garage by a full-width terrace.

### B10. Significance:

Its extended colonnade is a character-defining feature of the building and akin to a loggia or an ancient Greek Stoa: a long, low public building with an extended portico that provided shelter for the public and often faced a marketplace. However, this alone does not appear to make the building a distinctive design.

The Convention and Performing Arts Center does not appear to be eligible as a City Landmark under city Criterion D. Although Frank L. Hope & Associates is a significant postwar San Diego architectural firm, the San Diego Convention and Performing Arts Center does not appear to be “representative of the notable work of a master builder, designer, architect, engineer, landscape architect, interior designer, artist, or craftsman” in a manner that would warrant its listing as a city landmark under city Criterion D. San Diego’s Jack Murphy Stadium (Qualcomm Stadium), another Brutalist work by the same firm, possesses shapeliness and sculptural dynamics that render it a better example of the Brutalist design system and a “representative” work by the firm. Furthermore, the decorative concrete panels across the building’s spandrel designed by Malcolm Leland, a notable artist, do not lend the building design significance to make it eligible under City Criterion D.

The level of significance of the Convention and Performing Arts Center as a constituent element of the Concourse that spearheaded downtown revitalization, a resource associated with San Diegans Inc, does not appear to warrant listing of the building on either the California Register or the National Register under Criteria 1 or 2, or A or B, respectively. Just as the resource’s design significance does not meet City Criteria C, as an unremarkable example of Brutalist and New Formalist design, the resource is ineligible for California Register Criterion 3, or National Register Criterion C. The San Diego Convention and Performing Arts Center is not NRHP eligible under Criterion Consideration G as an exceptionally significant resource that is less than 50 years old.

# PRIMARY RECORD

Other Listings \_\_\_\_\_  
Review Code \_\_\_\_\_ Reviewer \_\_\_\_\_ Date \_\_\_\_\_

Page 1 of 2

\* Resource Name or #: San Diego Civic Theatre

P1. Other Identifier: \_\_\_\_\_

\* P2. Location:  Not for Publication  Unrestricted a. County San Diego

b. USGS 7.5' Quad Point Loma, CA Date 1996 T \_\_\_\_\_; R \_\_\_\_\_; \_\_\_\_\_ 1/4 of \_\_\_\_\_ 1/4 of Sec \_\_\_\_\_; \_\_\_\_\_ B.M.

c. Address 1100 3rd Avenue City San Diego Zip 92101

d. UTM: (Give more than one for large and/or linear feature) Zone 11, 484781 mE/ 361969 mN

e. Other Locational Data: (e.g. parcel #, legal description, directions to resource, elevation, additional UTMs, etc. as app

Assessor's Parcel Number: 533-433-28-00. ST CLSD&BLK 15.

\* P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.)  
The San Diego Civic Theatre is an irregular plan, four story performance venue located at the southeast portion of the Community Concourse. The exterior of the building has variegated massing and materials. The front elevation is predominately semi-circular and is made of textured concrete block with irregularly placed full-height vertical concrete fins along the front and side elevations. Between many of these fins, at the semi-circular front elevation, are narrow, full-height window bays that peak outward. These windows are set in bronze mullions. The recessed ground level at the front elevation features multiple, continuous sets of bronze- framed single leaf glass doors set behind exposed, sandblasted aggregate concrete posts. The fin and window treatment seen at the front elevation continues across the side elevations, albeit with fewer intervals. The side elevations curve inward and toward the five-story flat roofed flyloft, which is a tall rectangular component clad in smooth, painted concrete. The theatre's flyloft is affixed to the City Administration Building. Much of the flyloft's exterior does not touch the ground, but instead meets beneath it a two story flat roofed, covered loading area. The loading area is of an irregular plan, with massing that curves inward or bulges out at various elevations. This loading area exterior is of textured concrete with irregular scoring that echoes the irregular patterning of the fins upon the theater's frontal elevations. Affixed to the south elevation of the loading component is an elliptical shaped decorative fountain and pool with brick trim. This water feature is divided into three separate composed but asymmetrical parts, and at the present time it is empty. (please see continuation sheet)

\* P3b. Resource Attributes: (List attributes and codes) HP10 Theater

\* P4. Resources Present:  Building  Structure  Object  Site  District  Element of District  Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects)



P5b. Description of Photo: (View, date, etc.)

Looking southeast, 2/11/2010,

Photo#P1030037.jpg

\* P6. Date Constructed/Age and Sources:

Prehistoric  Historic  Both

1965

\* P7. Owner and Address:

City of San Diego

202 C Street

San Diego, CA 92101

M--Municipal

\* P8. Recorded by: (Name, affiliation, address)

Daniel Paul / David Greenwood

ICF International

811 W 7th ST, Suite 800

Los Angeles, CA 90017

\* P9. Date Recorded: \_\_\_\_\_

\* P10. Survey Type: (Describe)

Intensive Level Survey

\* P11. Report Citation: (Cite survey report/other sources or "none") Downtown San Diego Community Plan Environmental Imp

\* Attachments:  NONE  Location Map  Sketch Map  Continuation Sheet  Building, Structure, and Object Record  
 Archaeological Record  District Record  Linear Feature Record  Milling Station Record  Rock Art Record  Artifact Record  
 Photograph Record  Other: (List) \_\_\_\_\_

# BUILDING, STRUCTURE, AND OBJECT RECORD

Page 2 of 2

\* NRHP Status Code 5S3

\* Resource Name or #: San Diego Civic Theatre

B1. Historic Name: San Diego Civic Theatre

B2. Common Name: San Diego Civic Theatre

B3. Original Use: Civic Theater

B4. Present Use: Civic Theater

\* B5. Architectural Style: Expressionist Modern

\* B6. Construction History: (Construction date, alterations, and date of alterations.)

\* B7. Moved?  No  Yes  Unknown Date \_\_\_\_\_ Original Location: \_\_\_\_\_

\* B8. Related Features:

B9a. Architect: Ruocco, Kennedy & Rosser

b. Builder: \_\_\_\_\_

\* B10. Significance: Them Community Planning and Development Area San Diego

Period of Significance 1965

Property Type Building

Applicable Criteria City: A,B,C

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The San Diego Civic Theatre was opened in January of 1965 as the last component of the Community Concourse. Similar to the other resources that comprise the Community Concourse, the San Diego Civic Theatre appears eligible as a San Diego Historical Landmark under City Criterion A and B. This is due to the building being a component of the Community Concourse project, which played a significant role in spearheading the revitalization of downtown San Diego (City Centre) in the mid-1960s. The Community Concourse is the crowning achievement of San Diegans, Inc., a locally important group of business people who advocated the regeneration of downtown through the use of city planning and various downtown business and development measures. Its curvilinear form is relatively unique and harkens back to trends in European expressionist Modernism between world wars and again during the 1960s when the Miesian model of the supremacy of the box form was beginning to be scrutinized. The 1950s and 1960s saw a trend in concert hall design that featured expressionist forms. The better known of these include the Berlin Philharmonic (1956–1963), by Hans Scharoun, and the Sydney Opera House (1957–1973), by Jorn Utzon. Regionally, the Dorothy Chandler Pavilion (1964) in Los Angeles, by Welton Becket and Associates, is not a typical rectangle but instead features convex elevations. The form of the San Diego Civic Theatre is arguably more expressionist than the Dorothy Chandler Pavilion. Because the Theatre, for San Diego, embodies the distinctive characteristics of the 1960s era theatre building type, the resource appears eligible as a local level resource under City Criterion C. This is due largely to its expressive form, which was also highly distinct among other 1960s era public buildings in San Diego, which by and large were of rectangular construction. (See Continuation Sheet)

B11. Additional Resource Attributes: (List attributes and codes): \_\_\_\_\_

\* B12. References:

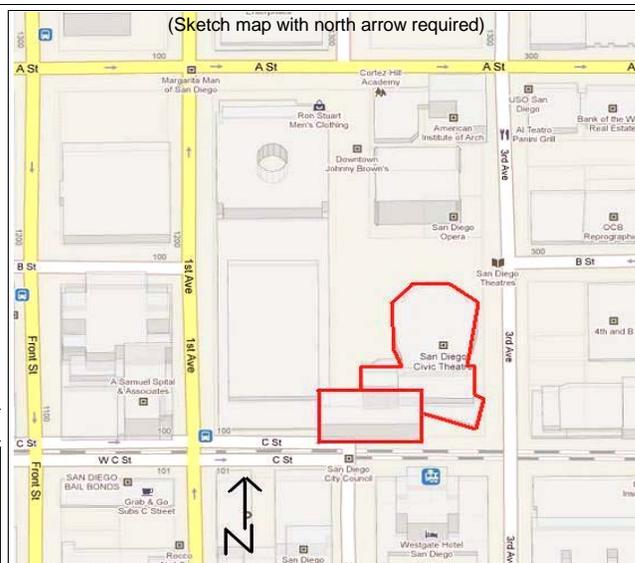
Britton III, James. Ruocco Remembered [...] San Diego Union Tribune, Jun 21, 1981: F3.

B13. Remarks:

\* B14. Evaluator: Daniel Paul

Date of Evaluation: 3/26/2010

(This space reserved for official comments.)



## CONTINUATION SHEET

Page 3 of 2 \* Resource Name or #: (Assigned by recorder San Diego Civic Theatre  
\* Recorded by: Daniel Paul / David Greenwood \* Date: \_\_\_\_\_  
 Continuation  Update

### P3a. Description:

The fountain is trimmed with smaller scale concrete panels by Malcolm Leland, which are seen on other parts of the concourse. However, research indicates that this particular fountain is not by Leland. Surrounding the fountain is a small landscaped area with palm trees, birds of paradise, and flower specimens. This landscaping appears to be a later addition. At the rear of the building is a single-story, flat-roofed dressing room with a slightly irregular square plan. The dressing room is stucco clad, and the decorative concrete panels seen across other parts of the civic center run the length of the exposed elevations directly beneath the roofline. Affixed to these panels are globe-style light fixtures set within an open canopy framework. A proclamation with metal lettering dating from the 1965 dedication ceremony for the concourse is present on the north elevation of the dressing room component. Just northeast of the theater is a standalone metal sign and marquee. It consists of three concave, backlit faces that are supported by three exposed metal beams.

### B10. Significance:

The San Diego Civic Theatre does not appear eligible under City Criterion D. Although the theater is associated with San Diego master architect Lloyd Ruocco, who was one of the city's first architects to embrace Modernism, this specific design was not Ruocco's alone but was done in concert with two other architects (Selden Kennedy and William Rosser) with which Ruocco did not have an architectural practice. Ruocco worked with the two others through an arrangement made by stakeholders at that time, and all design decisions were made between the three architects, with additional input from outside committees and supervising Samuel Hamill. James Britton II, a local architectural critic for the San Diego Union Tribune, referred to the building as "an average house, not the masterpiece that Ruocco dreamed of for San Diego."

The level of significance of the Civic Theatre as a constituent element of the Concourse that spearheaded downtown revitalization, a resource associated with San Diegans Inc, does not appear to warrant listing of the building on either the California Register or the National Register under Criteria 1 or 2, or A or B, respectively. The San Diego Civic Theatre presents no known associations to any other significant individuals or events that would warrant listing of the resource on either the California Register under Criteria 1 or 2, or the National Register under Criteria A or B.

Even though the theatre appears eligible as a City Historical Landmark under Criterion C, the quality of its design appears inadequate to warrant listing on either the California Register under Criterion 3, or the National Register under Criterion C. Despite its novel shape, which is in keeping with other 60s-era expressionist modern theatres, the Civic Theatre theater exudes cost efficiency through its use of exposed, albeit textured, concrete block; exposed concrete fins; sandblasted aggregate posts at the entry; and plain stucco volumes both atop the building at its rear and on its dressing room component—all highly visible from either 3rd Avenue, C, Street, or the Concourse itself. Because the Theatre is not a representative work of Ruocco as a master architect, the resource does not meet California Register or National Register Criteria 3 or C. The San Diego Civic Theatre is not National Register eligible under Criterion Consideration G as an exceptionally significant resource less than 50 years old.

# PRIMARY RECORD

Other Listings \_\_\_\_\_  
Review Code \_\_\_\_\_ Reviewer \_\_\_\_\_ Date \_\_\_\_\_

Page 1 of 5

\* Resource Name or #: Community Concourse Parking Garage

P1. Other Identifier: Evan V. Jones Parkade

\* P2. Location:  Not for Publication  Unrestricted a. County San Diego

b. USGS 7.5' Quad Point Loma, CA Date 1996 T \_\_\_\_\_; R \_\_\_\_\_; \_\_\_\_\_ 1/4 of \_\_\_\_\_ 1/4 of Sec \_\_\_\_\_; \_\_\_\_\_ B.M.

c. Address 150 C Street City San Diego Zip 92101

d. UTM: (Give more than one for large and/or linear feature) Zone 11, 484690 mE/ 3620077 mN

e. Other Locational Data: (e.g. parcel #, legal description, directions to resource, elevation, additional UTM's, etc. as app  
Assessor's Parcel Number: 533-433-28-00. ST CLSD&BLK 13.

\* P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.)  
The Community Concourse Parking Garage, today called the Evan V. Jones Parkade, is an eleven story, square plan, prestressed concrete parking garage. Each elevation of the square is 200 feet and eight bays wide. Each bay is delineated by full-height vertical concrete pilasters that have full-height scoring akin to the side elevations of the City Administration building. At the top of the structure, the pilasters join a seamless lintel and form rounded, open bays similar to those upon the Convention and Performing Arts Center. The spandrels, which support various ramps within the structure, are slightly pitched, and their stack appears angled. At the ground level of the structure are numerous pedestrian ramps and small planters with various bush and tree specimens. Many of these ramps are clad in the previously mentioned decorative concrete paneling seen throughout the Concourse. These various pedestrian ramps are of concrete and are often modeled, with soft, rounded edges. A portion of the ramp off the east elevation was removed when B Street was closed off due to the construction of the Security Pacific bank Tower due east of the structure. Floating concrete stairways are also present adjacent these ramps. In the middle of the Community Concourse Parking Garage is a composition of four arches corresponding to the four sides of the structure. Each of these arches, at their voussoirs, is clad in the previously mentioned decorative concrete paneling. Centered within its middle is a circular planter with various tree and bush specimens. Large circular stepping stones lead to the planter from various openings. A former water feature adjacent the planter has been removed. (See continuation sheet).

\* P3b. Resource Attributes: (List attributes and codes) HP39. Other

\* P4. Resources Present:  Building  Structure  Object  Site  District  Element of District  Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects)



P5b. Description of Photo: (View, date, etc.)

Looking southwest, 2/11/2010,

Photo#P1030022.jpg

\* P6. Date Constructed/Age and Sources:

Prehistoric  Historic  Both

1964

\* P7. Owner and Address:

City of San Diego

202 C Street

San Diego, CA 92101

M--Municipal

\* P8. Recorded by: (Name, affiliation, address)

Daniel Paul / David Greenwood

ICF International

811 W 7th ST, Suite 800

Los Angeles, CA 90017

\* P9. Date Recorded: 3/26/2010

\* P10. Survey Type: (Describe)

Intensive Level Survey

\* P11. Report Citation: (Cite survey report/other sources or "none") Downtown San Diego Community Plan Environmental Imp

\* Attachments:  NONE  Location Map  Sketch Map  Continuation Sheet  Building, Structure, and Object Record  
 Archaeological Record  District Record  Linear Feature Record  Milling Station Record  Rock Art Record  Artifact Record  
 Photograph Record  Other: (List) \_\_\_\_\_

# BUILDING, STRUCTURE, AND OBJECT RECORD

Page 2 of 5

\* NRHP Status Code 3CS

\* **Resource Name or #:** Community Concourse Parking Garage

B1. Historic Name: Community Concourse Parking Garage

B2. Common Name: Evan V. Jones Parkade

B3. Original Use: Parking Garage

B4. Present Use: Parking Garage

\* **B5. Architectural Style:** Modern

\* **B6. Construction History:** (Construction date, alterations, and date of alterations.)

\* **B7. Moved?**  No  Yes  Unknown Date \_\_\_\_\_ Original Location: \_\_\_\_\_

\* **B8. Related Features:**

B9a. Architect: Tucker, Sadler and Bennett b. Builder: \_\_\_\_\_

\* **B10. Significance:** Them Architecture Area San Diego (City Centre)

Period of Significance 1964 Property Type Structure Applicable Criteria City: A-D CR: 3

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

Similar to the other resources that comprise the Community Concourse, the Community Concourse Parking Garage appears eligible as a San Diego Historical Landmark under City Criterion A and B. This is due to the building being a component of the Community Concourse project, which played a significant role in spearheading the revitalization of downtown San Diego (City Centre) in the mid-1960s, and because the Concourse appears to be the crowning achievement of San Diegans, Inc., a locally important group of business people who advocated the regeneration of downtown through the use of city planning and various downtown business and development measures. The Community Concourse Parking Garage appears to be eligible as a San Diego Historic Landmark under City Criterion C. The Garage exhibits a quality of design and features that are extremely uncommon for a post-World War II parking garage. In early press, observers described the structure as “one of the most beautifully designed buildings in the [Concourse] complex.” In July of 1965, a picture of it by noted architectural photographer Julius Schulman was featured on the cover of Arts and Architecture magazine, the preeminent California architectural journal of the post-World War II era. As one nears the exterior of the structure, the pedestrian experience at the ground level is highly considered, with floating, extended beam stairways; criss-crossing pedestrian walkways adorned in decorative concrete paneling; and sculptural entry and exit ramps. The center space of the Garage is highly unusual and unexpected. Surrounding it at the ground level are four open arches with voussoirs that are clad by artist Malcolm Leland’s decorative concrete panels, seen on other buildings of the Community Concourse. Centered within these arches, and within the structure itself, is a circular planter at ground level. This area originally included a now-removed water feature. Directly above the planter, extending the entire (See continuation sheet).

B11. Additional Resource Attributes: (List attributes and codes): \_\_\_\_\_

\* **B12. References:**

“Tucker, Sadler and Bennett, Architects.” Arts and Architecture. Jul., 1965: cover, 32-33.

“11-story, 1,000-Car Garage [...]” San Diego Union Tribune. Sep. 14, 1964: C9.

Hal Sadler. Telephone interview with author Daniel Paul. Los Angeles, CA., Feb. 19, 2009.

B13. Remarks:

\* **B14. Evaluator:** Daniel Paul

Date of Evaluation: 3/26/2010

(This space reserved for official comments.)



## CONTINUATION SHEET

Page 3 of 5 \* Resource Name or #: (Assigned by recorder Community Concourse Parking Garage  
\* Recorded by: Daniel Paul / David Greenwood \* Date: 3/26/2010  
 Continuation  Update

### P3a. Description:

Above the planter is a circular shaped, dramatically scaled open core atrium that rises eleven stories and opens to the sky. The cylindrical quality of the open space is defined by the spiral of the ramps. Engineering-wise, the structure is essentially two separate garages intertwined, with cars entering from 1st Avenue parking on odd numbered floors, and cars entering from Second Avenue parking on even numbered floors. A portion of the second avenue exit ramp was removed with the construction of the nearby Security Pacific Bank Tower in 1972.

### B10. Significance:

11-story elevation of the structure, is an open-core atrium. This full-height space is perfectly circular and opens to a large oculus at the center of the building. The circular shape of the space is defined by a spiraling vehicle circulation element. The Garage appears to take various cues from the work of mid-twentieth century Master architect Eero Saarinen. The sculptural quality of the ramps, of smooth lines rendered in concrete, is akin to Saarinen's TWA Terminal building at New York's JFK Airport of 1962. The interior oculus of the garage, enclosed by arches, is akin to Saarinen's MIT Chapel of 1955. Additionally, the spiraling ramps within the buildings center, if the oculus is to be viewed as a skylight, appears to reference Frank Lloyd Wright's New York Guggenheim Art Museum of 1959, which features the spiraling ramp motif as the prominent interior feature of the building. Engineered by George Devlin of Detroit, the garage was designed to hold 1,000 cars, all of which could exit the structure within 20 minutes. As originally designed, the structure was essentially two garages in one. Cars entering from 1st Avenue followed a spiraling ramp to the first, third, fifth, seventh, and ninth floors. Vehicles entering from 2nd Avenue parked on the second, fourth, sixth, eighth, and tenth floors. The southern portion of the 2nd Avenue ramp was removed in 1972 during construction of the adjacent Security Pacific Bank tower, which was by Tucker, Sadler & Bennett. Two other ramps remain in full. Additionally, a water feature originally located within the center of the building is now gone. However, the garage retains the rest of its original fabric, including a variety of period features with a considered relationship to the ground level pedestrian. These alterations do not compromise the integrity of the resource in such a manner that its significant character defining are no longer conveyed. The Community Concourse Parking Garage appears to be eligible under City Criterion D. The firm of Tucker, Sadler & Bennett operated in San Diego for more than 40 years, producing hundreds of commercial and residential projects. These include various downtown towers; multiple buildings at the University of California, San Diego and San Diego State University; and custom residences, among many others. In 2007, Hal Sadler received the Lifetime Achievement Award from the California Council of the American Institute of Architects. The level of significance of the Community Concourse Parking Garage as a constituent element of the Concourse that spearheaded downtown revitalization, a resource associated with San Diegans Inc, does not appear to warrant listing of the building on either the California Register or the National Register under Criteria 1 or 2, or A or B, respectively. At some point after its period of significance, 1964, the structure was named for Evan V. Jones, the former parking superintendent for the City of San Diego. However, this association alone does not appear to render the resource eligible for California Register listing under Criterion 2 or National Register listing under Criterion B. The Community Concourse Parking Garage appears to be eligible for the California Register of Historical Resources at the local level under Criterion 3 for embodying the distinctive characteristics of a type, for representing the work of a master architectural firm, and for possessing high artistic values. The structure is a considered and distinctive example of what is normally a perfunctory building type: the parking garage. When completed, the structure was recognized as a special example of its type, and was probably one of very few parking garages to be the cover shot of a prominent architectural and design journal. As previously mentioned, the structure features a variety of intact, Post World War II design elements that cater to the pedestrian. The sculptural arrangement of spiraling auto ramps defining a monumental, multi-story interior core with a landscaped planter topped by a skylight is unusually well considered for a parking garage. Even though the Community Concourse Parking Garage appears to be California Register eligible, the structure does not appear National Register eligible under Criterion C. A decorative screen intended for the exterior of the building was never completed, and the previously mentioned losses to the ramp and the water feature comprise the structure's integrity. The structure in general does not possess the quality of significance regarding its design that would warrant listing on the National Register of Historic Places under Criterion C. Additionally, the Garage is not an exceptionally significant resource that would warrant National Register listing under Criterion Consideration G for structures younger than 50 years old.

# CONTINUATION SHEET

Page 3 of 4 \* Resource Name or #: (Assigned by recorder Evan V. Jones Parkade)

\* Recorded by: Daniel Paul / David Greenwood

\* Date: 3/26/2010

Continuation

Update



Ramp Detail, Community Concourse Parking Garage, view east.



Interior spiral and landscape, view south.



Interior spiral and oculus, view south.



Planter detail., view south.



1st Avenue Ramp detail, view southeast.



Roof detail, view west.

State of California-The Resources Agency

State of California-The Resources Agency  
DEPARTMENT OF PARKS AND RECREATION  
**CONTINUATION SHEET**

Primary # \_\_\_\_\_  
HR # \_\_\_\_\_  
Trinomial \_\_\_\_\_

Page 5\_ of \_5 Resource Name or #: **San Diego Community Concourse Parking Garage**

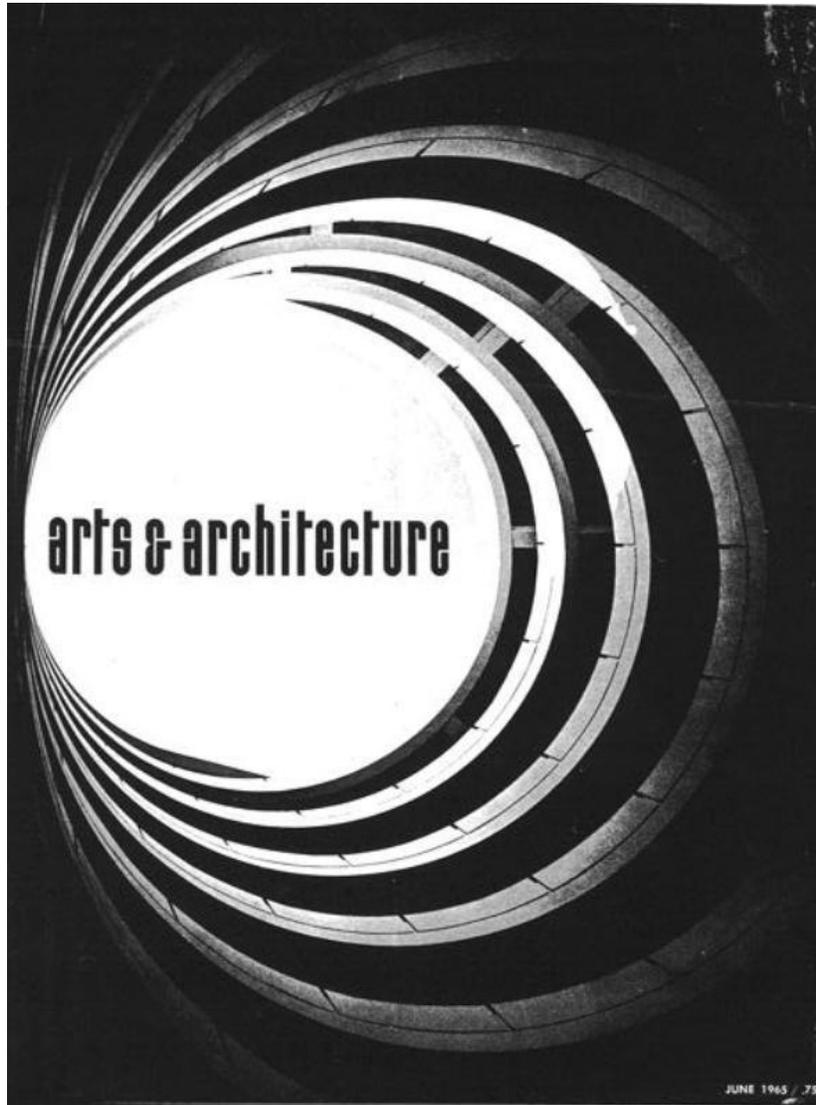
**APN: 533-433-28-00**

\* Recorded by: **Daniel Paul / David T. Greenwood**

Continuation     Update

**P3b.** Resource Attributes: HP39. Other.

**Additional Photographs:**



*Arts and Architecture* magazine Cover. Photo: Julius Schulman. June, 1965.

Primary # \_\_\_\_\_  
HR # \_\_\_\_\_  
Trinomial \_\_\_\_\_  
NRHP Status Code 3CS

# PRIMARY RECORD

Other Listings \_\_\_\_\_  
Review Code \_\_\_\_\_ Reviewer \_\_\_\_\_ Date \_\_\_\_\_

Page 1 of 2

\* Resource Name or #: Bow Wave

P1. Other Identifier: \_\_\_\_\_

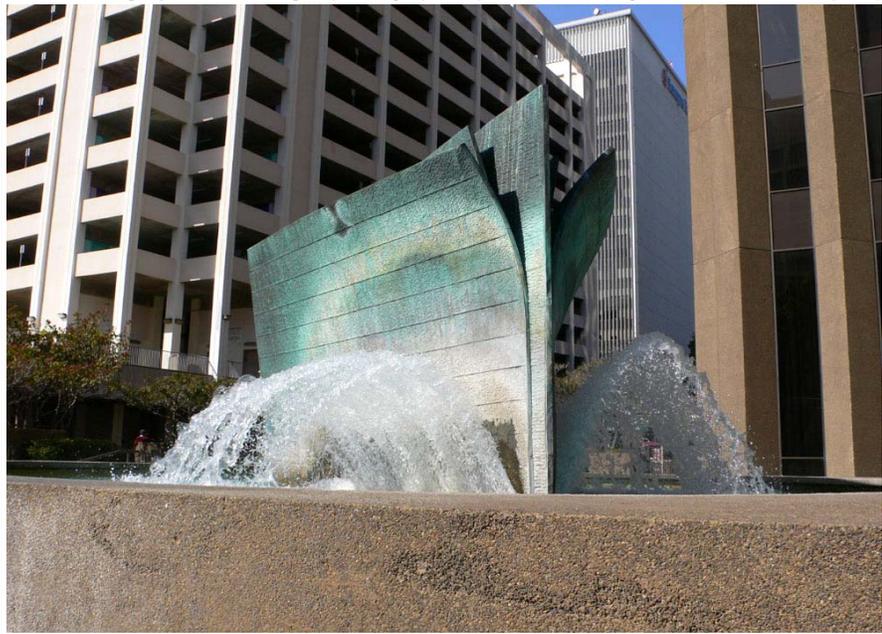
\* P2. Location:  Not for Publication  Unrestricted a. County San Diego  
b. USGS 7.5' Quad Point Loma, CA Date 1996 T \_\_\_\_\_; R \_\_\_\_\_; \_\_\_\_\_ 1/4 of \_\_\_\_\_ 1/4 of Sec \_\_\_\_\_; \_\_\_\_\_ B.M.  
c. Address 1200 3rd Avenue City San Diego Zip 92101  
d. UTM: (Give more than one for large and/or linear feature) Zone \_\_\_\_\_, \_\_\_\_\_ mE/ \_\_\_\_\_ mN  
e. Other Locational Data: (e.g. parcel #, legal description, directions to resource, elevation, additional UTM's, etc. as app  
Assessor's Parcel Number: 533-433-11-00. PAR B, located directly above the Civic Center plaza at a point congruent with B Street.

\* P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.)  
Bow Wave is a public sculpture and fountain consisting of five large, mottled bronze panels with horizontal scoring to simulate shiplap cladding. The five panels converge in an irregular manner at a point, facing southeast, that simulates a ship's prow. The outer panels flare outward at their upper edges, and a fan of water shoots off either side of the outer panels to simulate a ship cutting through water. The simulated prow of the the sculpture is acclimated southeast. Originally, the fan of water protruded much higher that it presently does now, and its fanning form echoed the curved side panels of the sculpture. The pool in which the sculpture resides is circular, and it is above ground in a square shaped base made of stained aggregate. The Bow Wave fountain and all portions of the courtyard north of it were completed in conjunction with the nearby Security Pacific bank tower in 1972. The stained aggregate of its base matches the cladding upon the tower's exterior.

\* P3b. Resource Attributes: (List attributes and codes) HP39. Other

\* P4. Resources Present:  Building  Structure  Object  Site  District  Element of District  Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects)



P5b. Description of Photo: (View, date, etc.)

Looking northwest, 2/11/2010,  
Photo#P1030044.jpg

\* P6. Date Constructed/Age and Sources:  
 Prehistoric  Historic  Both

1972, SD Building Permit #H17398

\* P7. Owner and Address:  
Lowe SD California Prop LLC  
1200 3rd Avenue  
San Diego, CA 92101-4110  
P--Private

\* P8. Recorded by: (Name, affiliation, address)  
Daniel Paul / David Greenwood

ICF International  
811 W 7th ST, Suite 800  
Los Angeles, CA 90017

\* P9. Date Recorded: \_\_\_\_\_

\* P10. Survey Type: (Describe)  
Intensive Level Survey

\* P11. Report Citation: (Cite survey report/other sources or "none") Downtown San Diego Community Plan Environmental Imp

\* Attachments:  NONE  Location Map  Sketch Map  Continuation Sheet  Building, Structure, and Object Record  
 Archaeological Record  District Record  Linear Feature Record  Milling Station Record  Rock Art Record  Artifact Record  
 Photograph Record  Other: (List) \_\_\_\_\_

# BUILDING, STRUCTURE, AND OBJECT RECORD

Page 2 of 2

\* NRHP Status Code 3CS

\* Resource Name or #: Bow Wave

B1. Historic Name: Bow Wave

B2. Common Name: Bow Wave

B3. Original Use: other

B4. Present Use: other

\* B5. Architectural Style: Modern

\* B6. Construction History: (Construction date, alterations, and date of alterations.)

On September 28, 1971, San Diego Building permit No. H17398 was issued to erect a fountain sculpture for the estimated cost of \$500.

\* B7. Moved?  No  Yes  Unknown Date \_\_\_\_\_ Original Location: \_\_\_\_\_

\* B8. Related Features:

B9a. Architect: \_\_\_\_\_ b. Builder: Malcolm Leland (artist)

\* B10. Significance: Them Design Area San Diego (Centre City)

Period of Significance 1972 Property Type Object Applicable Criteria City: D CR:3

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The Bow Wave Fountain, completed by Malcolm Leland in 1972, is located near the Community Concourse but was not designed in conjunction with it. Bow Wave was designed with the Security Pacific bank tower located just north of the Civic Theatre. Although the fountain was created by a master artist, it has lost proper integrity to exemplify special elements of the City's aesthetic development, and is therefore not eligible for City Criterion A. According to the artist himself, the water pump appears to be failing. Symmetrical fans of water that originally projected much higher, and therefore were in proper composition with the upper edges of the piece itself, at present project about one-third of their original, intended height.

Bow Wave has no known associations to persons or events significant in local, state or national history, and is therefore ineligible for City landmark listing under Criterion B. Bow Wave does not appear eligible as a City Landmark under City Criterion C, again due to the above-mentioned alteration. However, the Bow Wave Fountain is still a rare example of a fountain designed by prolific mid-century designer and artist Malcolm Leland, and the object appears eligible under City Criterion D: "Is representative of the notable work of a master builder, designer, architect, engineer, landscape architect, interior designer, artist, or craftsman." Malcolm Leland's work has been exhibited at the Los Angeles County Museum of Art, the Los Angeles Museum of Contemporary Art, the San Francisco Museum of Art, and other galleries and venues in Southern California. Leland is also responsible for the decorative concrete panels present across various Community Concourse buildings. (Please see continuation sheet)

B11. Additional Resource Attributes: (List attributes and codes): \_\_\_\_\_

\* B12. References:

Leland, Malcolm. Telephone Interview with author Daniel Paul. Los Angeles, CA. Feb, 2010.

Leland, Malcolm "Malcolm Leland," information sheet, n.d. courtesy of Malcolm Leland.

Leland, Malcolm. Resume, c.1992: 1. courtesy of Malcolm Leland.

B13. Remarks:

\* B14. Evaluator: Daniel Paul

Date of Evaluation: 3/26/2010

(This space reserved for official comments.)



## CONTINUATION SHEET

Page 1 of 2 \* Resource Name or #: (Assigned by recorder Bow Wave  
\* Recorded by: Daniel Paul / David Greenwood \* Date: \_\_\_\_\_  
 Continuation  Update

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Significance, Ctd.

Bow Wave is not associated with significant events or persons that would warrant listing on either the California Register under criteria 1 or 2, or the National Register under Criteria A and B. The Bow Wave Fountain does appear to be eligible for the California Register of Historical Resources, significant at the local level under Criterion 3 as the work of a master, the artist Malcolm Leland. However, the level of the object's quality of significance, combined with the diminished affect of its failing water pump, does not render it eligible for National Register listing under Criterion C. Additionally, the resource is not of exceptional significance under National Register Criterion Consideration G that would warrant NRHP listing despite being less than 50 years old.

# PRIMARY RECORD

Other Listings \_\_\_\_\_  
Review Code \_\_\_\_\_ Reviewer \_\_\_\_\_ Date \_\_\_\_\_

Page 1 of 2

\* Resource Name or #: City Operations Building

P1. Other Identifier: \_\_\_\_\_

\* P2. Location:  Not for Publication  Unrestricted a. County San Diego  
b. USGS 7.5' Quad Point Loma, CA Date 1996 T \_\_\_\_\_; R \_\_\_\_\_; \_\_\_\_\_ 1/4 of \_\_\_\_\_ 1/4 of Sec \_\_\_\_\_; \_\_\_\_\_ B.M.  
c. Address 1222 1st Avenue City San Diego Zip 92101  
d. UTM: (Give more than one for large and/or linear feature) Zone 11, 484609 mE/ 3620057 mN  
e. Other Locational Data: (e.g. parcel #, legal description, directions to resource, elevation, additional UTM, etc. as app  
Assessor's Parcel Number: 533-433-28-00. ST CLSD&BLK 1.

\* P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.)  
The San Diego City Operations Building is a rectangular plan, five-story, flat roofed institutional building. Brutalist in style, the building possesses exposed concrete elevations with a base that is highly textured in an irregular jagged edged design. The ground level features wide, arched openings that seem to permit vehicle access. A row of arched, clerestory window openings, deeply recessed, is present directly above this base. At the building's exterior, an extended, wraparound concrete eave separates the ground floor from the upper stories. The building's upper three levels feature thin bays separated by full-height vertical pilasters. Across the middle floors, these windows are topped by small segmental arched hoods of concrete. At the upper level, the windows are topped by hoods of a more pronounced arch shape. The spandrel panels above and below windows have raked concrete detailing. The ground floor of the building's south elevation, west side, serves as a fire station. A fire station is built into the southern elevation. As part of it, three garage bays are present and each has roll-up metal doors, with multi-light glass windows above in the transom. Atop the building is a square plan component that appears to house machinery and it features a pattern of thin, vertical vents with arched headers. A concrete skybridge connects the City Operations Building to the Community Concourse Parking Garage due east of it. This bridge is of exposed concrete with solid concrete railing. The eave directly above the building's ground floor wraps up above the bridge, forming a hood over it as the bridge enters the building. Original metal lettered signage indicating "Operations Building" is present above the bridge as one enters into the building.

\* P3b. Resource Attributes: (List attributes and codes) HP14. Government Building

\* P4. Resources Present:  Building  Structure  Object  Site  District  Element of District  Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects)

P5b. Description of Photo: (View, date, etc.)



Looking northwest, 2/11/2010,

Photo#P1030081.jpg

\* P6. Date Constructed/Age and Sources:  
 Prehistoric  Historic  Both

1971

\* P7. Owner and Address:

City of San Diego

202 C Street

San Diego, CA 92101

M--Municipal

\* P8. Recorded by: (Name, affiliation, address)

Daniel Paul / David Greenwood

ICF International

811 W 7th ST, Suite 800

Los Angeles, CA 90017

\* P9. Date Recorded: 3/26/2010

\* P10. Survey Type: (Describe)

Intensive Level Survey

\* P11. Report Citation: (Cite survey report/other sources or "none") Downtown San Diego Community Plan Environmental Imp

\* Attachments:  NONE  Location Map  Sketch Map  Continuation Sheet  Building, Structure, and Object Record  
 Archaeological Record  District Record  Linear Feature Record  Milling Station Record  Rock Art Record  Artifact Record  
 Photograph Record  Other: (List) \_\_\_\_\_

# BUILDING, STRUCTURE, AND OBJECT RECORD

Page 2 of 2

\* NRHP Status Code 6Z

\* Resource Name or #: City Operations Building

B1. Historic Name: City Operations Building

B2. Common Name: City Operations Building (COB)

B3. Original Use: Municipal Operations Building

B4. Present Use: Municipal Office Building

\* B5. Architectural Style: Brutalism

\* B6. Construction History: (Construction date, alterations, and date of alterations.)

\* B7. Moved?  No  Yes  Unknown Date \_\_\_\_\_ Original Location: \_\_\_\_\_

\* B8. Related Features:

B9a. Architect: George Hatch

b. Builder: \_\_\_\_\_

\* B10. Significance: Them Architecture Area San Diego

Period of Significance 1971

Property Type \_\_\_\_\_

Applicable Criteria N/A

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

Completed by the firm of Hatch and Heimerdinger in 1971, the City Operations Building (COB) is of the Brutalist style, featuring exposed concrete, a tapered base with a textured design pattern, and exposed concrete window hoods and headers. An exposed concrete skybridge appears to have been completed in conjunction with the building. Despite these features, the building is essentially a rectangular box, and does not possess either the sculptural massing or the dynamic use of solids and voids seen in more significant examples of Brutalism. An intact but unexceptional example of Brutalism, the City Operations Building does not appear eligible under City and National Register Criterion C, or California Register Criterion 3 for design significance. George Hatch, architect of the San Diego Main Branch library and the City Administration Building, appears to have been the lead architect. Neither George Hatch nor the firm of Hatch and Heimerdinger appear to be "Master Architects," in manner that would warrant listing of the building under City Criterion D. Unlike the other nearby buildings completed six years earlier as part of the Community Concourse, this resource does not possess municipal level significance for its association to significant persons or events that would warrant listing under City Criterion B. Additionally, research yielded no known events or persons associated to the COB that would warrant listing of the building under Criterion A or B of the National Register, or Criterion 1 or 2 of the California Register of Historical Resources. Based on the above, the resource does not appear eligible for the National Register of Historic Places under Special Criterion Consideration G for resources of exceptional importance that are less than 50 years old.

B11. Additional Resource Attributes: (List attributes and codes): \_\_\_\_\_

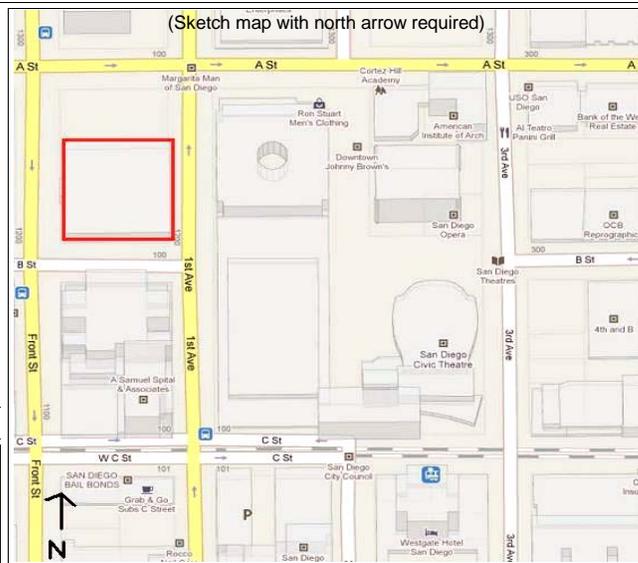
\* B12. References:

B13. Remarks:

\* B14. Evaluator: Daniel Paul

Date of Evaluation: 3/26/2010

(This space reserved for official comments.)





## **APPENDIX E**

### **Contemporary and Historic Photographs**

Contemporary Photographs

Historic Photographs





**Figure 1. City Administration Building, looking northeast, south and west elevation.**



**Figure 2. City Administration Building, looking north, lower south elevation.**



**Figure 3. City Administration Building, looking northwest, south street elevation.**



Figure 4. City Administration Building, looking east, lobby west elevation.



Figure 5. City Administration Building, looking west, lobby east elevation.



Figure 6. City Administration Building, looking east at the terrazzo San Diego City seal.



Figure 7. City Administration Building, looking northwest, east and south elevations.

Appendix E Contemporary Photographs



**Figure 8. City Administration Building at upper left, looking west, east elevation.**



**Figure 9. City Administration Building, looking south, north elevation.**



Figure 10. Convention/Performing Arts Building, looking northwest, east elevation.



Figure 11. Convention/Performing Arts Building, looking west, east elevation, main entry.



**Figure 12. Convention/Performing Arts Building, looking southwest, east elevation.**



**Figure 13. Convention/Performing Arts Building, looking north, ground floor walkway.**

Appendix E Contemporary Photographs



Figure 14. Convention/Performing Arts Building, looking south, 2<sup>nd</sup> floor walkway.



Figure 15. Civic Theater Building, looking southwest, north elevation, 2<sup>nd</sup> floor.

Appendix E Contemporary Photographs



Figure 16. Convention/Performing Arts Building, looking northwest, south elevation.

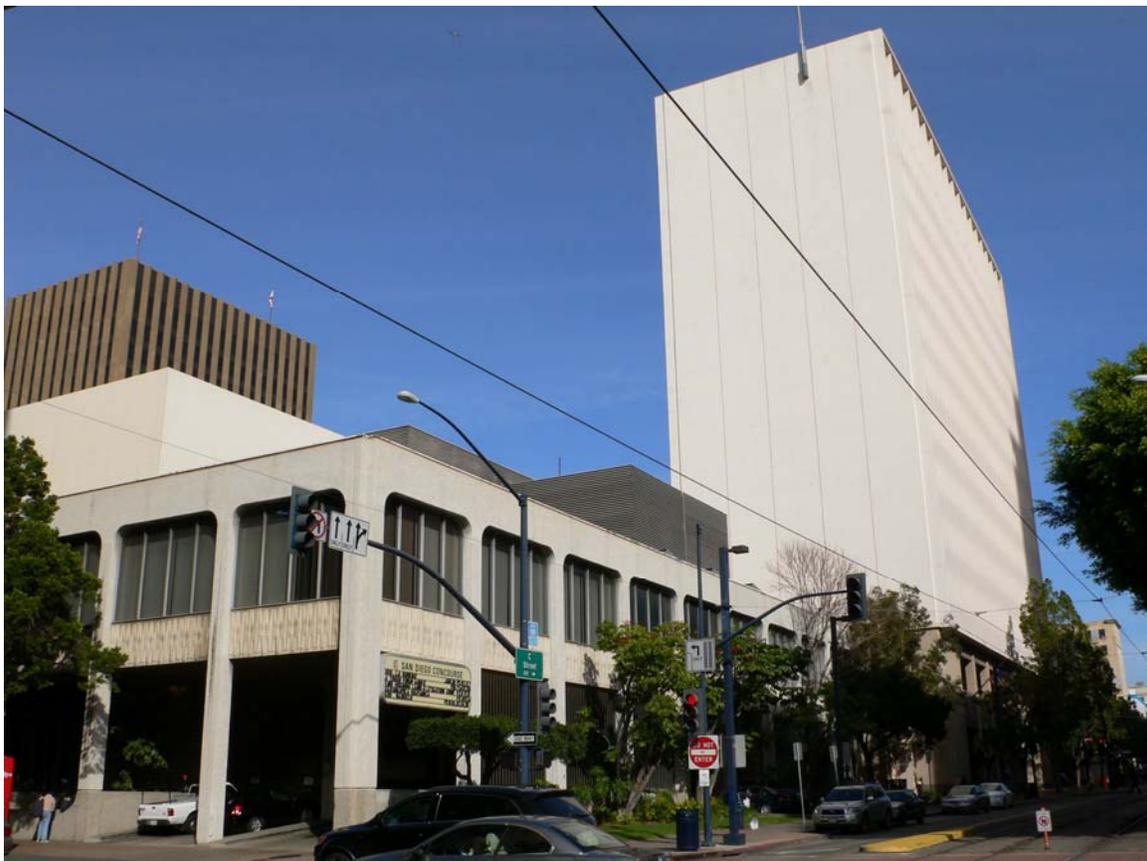


Figure 17. Convention/Performing Arts Building, looking northeast, south elevation.

Appendix E Contemporary Photographs



**Figure 18. Convention/Performing Arts Building, looking northeast, west elevation.**

Appendix E Contemporary Photographs



Figure 19. Civic Theater Building, looking southeast, north and west elevations.



Figure 20. Civic Theater Building, looking south, north elevation.



**Figure 21. Civic Theater Building, looking east, west elevation.**



**Figure 22. Civic Theater Building, looking southeast, southwest end of lower story.**

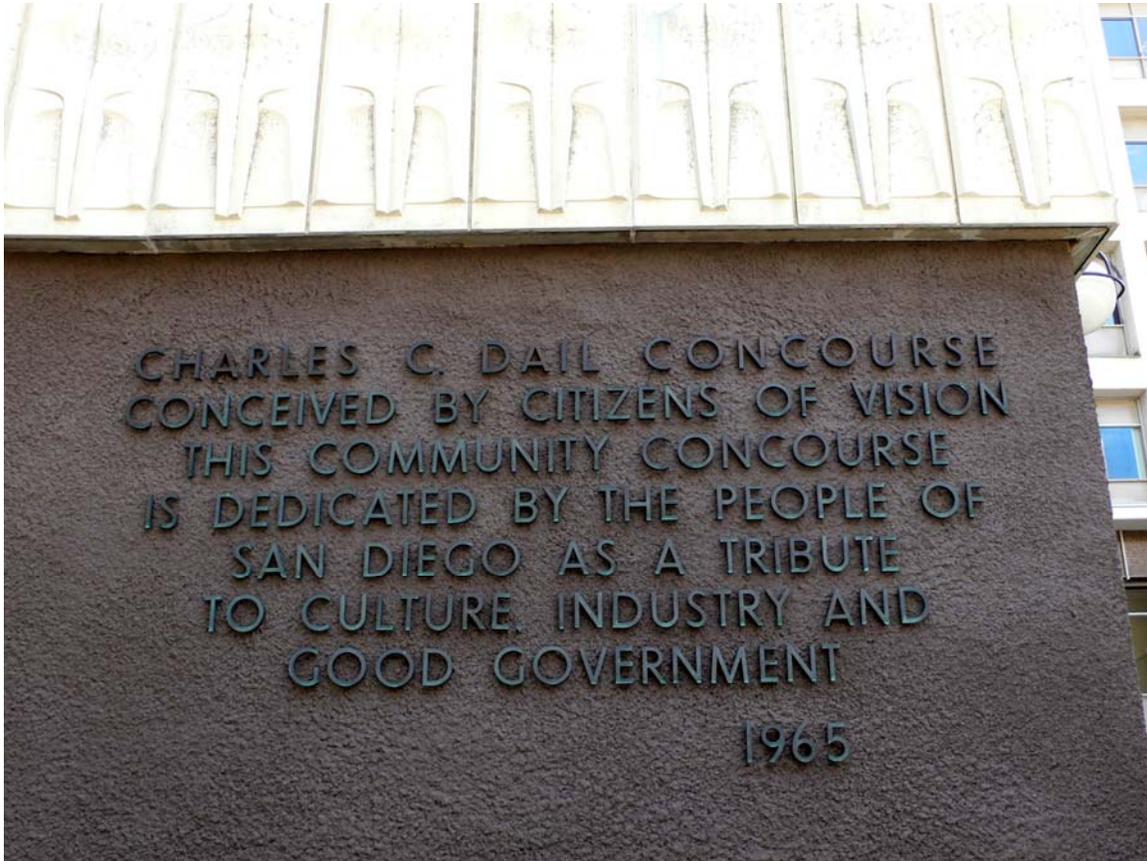


Figure 23. Civic Theater Building, looking south, south elevation of lower story.



Figure 24. Civic Theater Building, looking southwest, north and south elevations.

Appendix E Contemporary Photographs



Figure 25. Civic Theater Building, looking west, east elevation, north end.



Figure 26. Civic Theater Building, looking west, east elevation, south end.



**Figure 27. Civic Theater Building at right, looking northwest, south elevation.**



**Figure 28. Evan Jones Parkade, looking southeast, west and part of north elevation.**



**Figure 29. Evan Jones Parkade, looking south, north and part of west elevation.**



Figure 30. Evan Jones Parkade, looking northwest, south elevation.

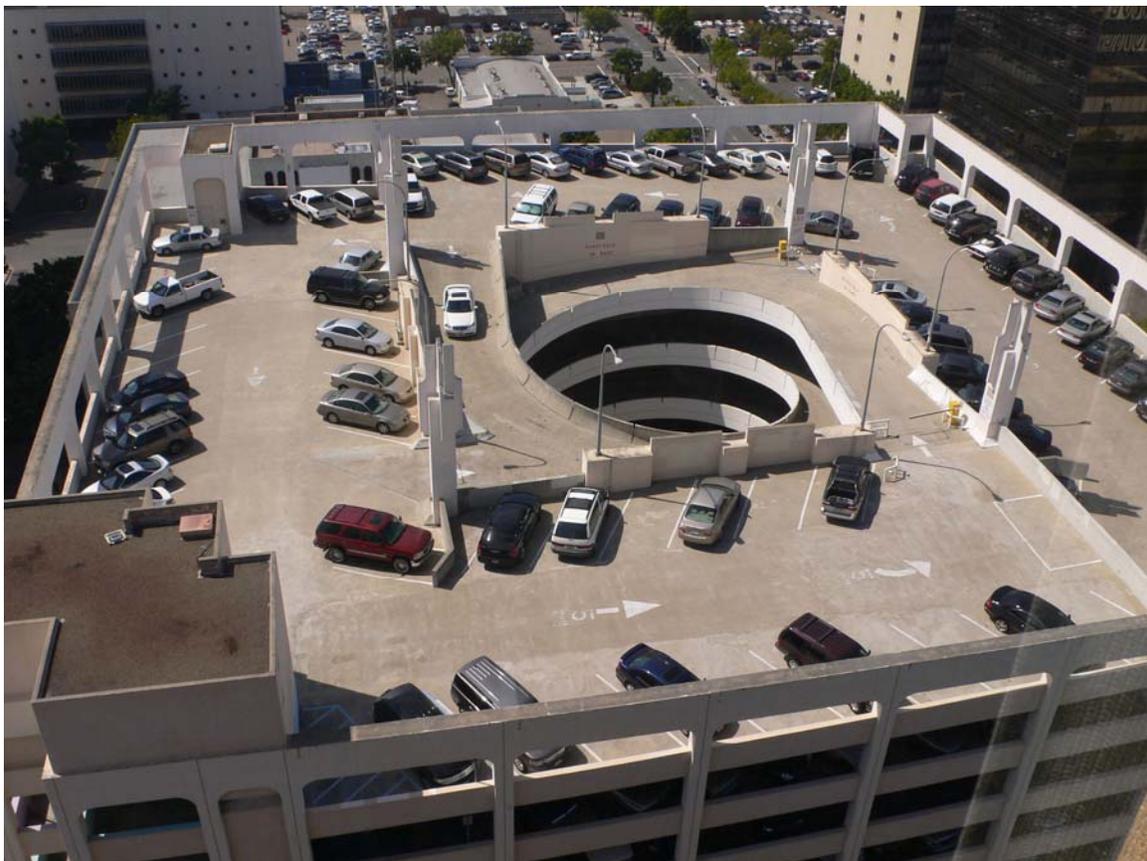


Figure 31. Evan Jones Parkade, looking west, rooftop elevation.



Figure 32. Evan Jones Parkade, looking west, east elevation.



Figure 33. Evan Jones Parkade, looking southwest, north and east elevation.



**Figure 34. Evan Jones Parkade, looking southeast, lower east elevation.**



**Figure 35. Evan Jones Parkade, looking southwest, north and east elevation.**

Appendix E Contemporary Photographs

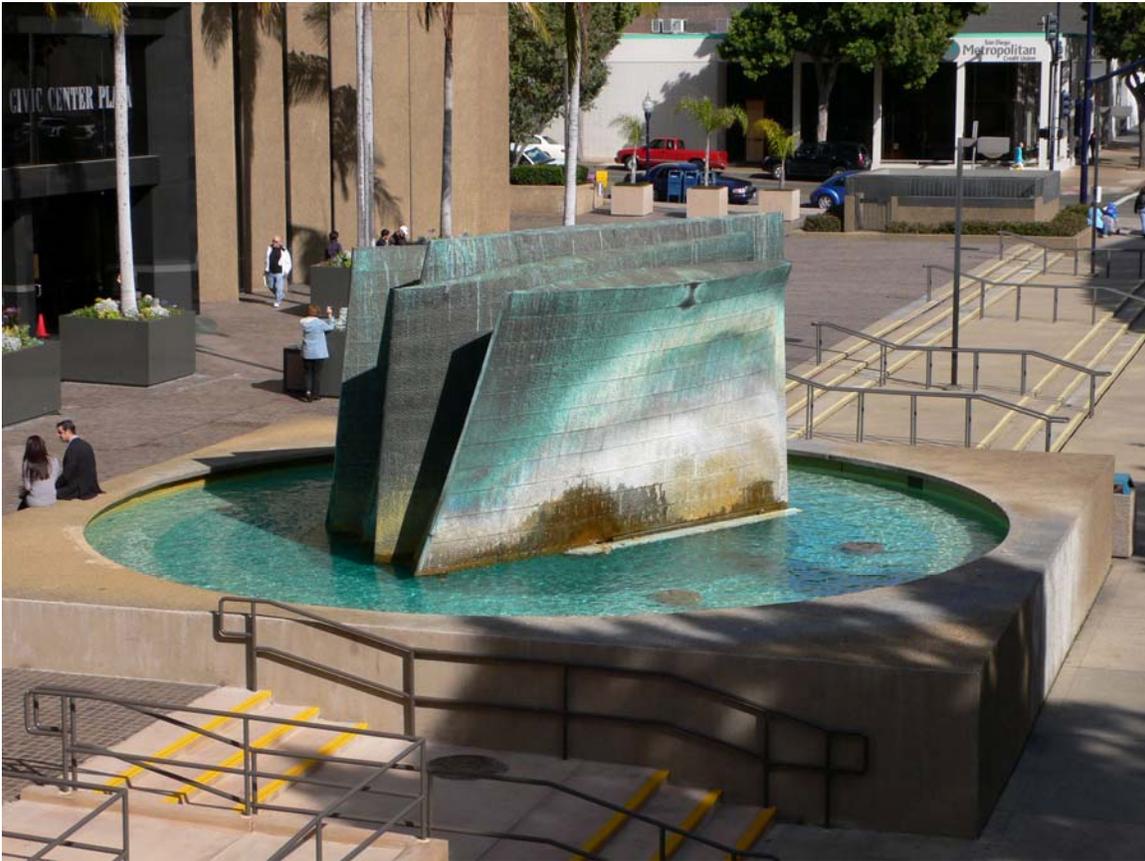


Figure 36. Bow Wave sculpture, looking east.

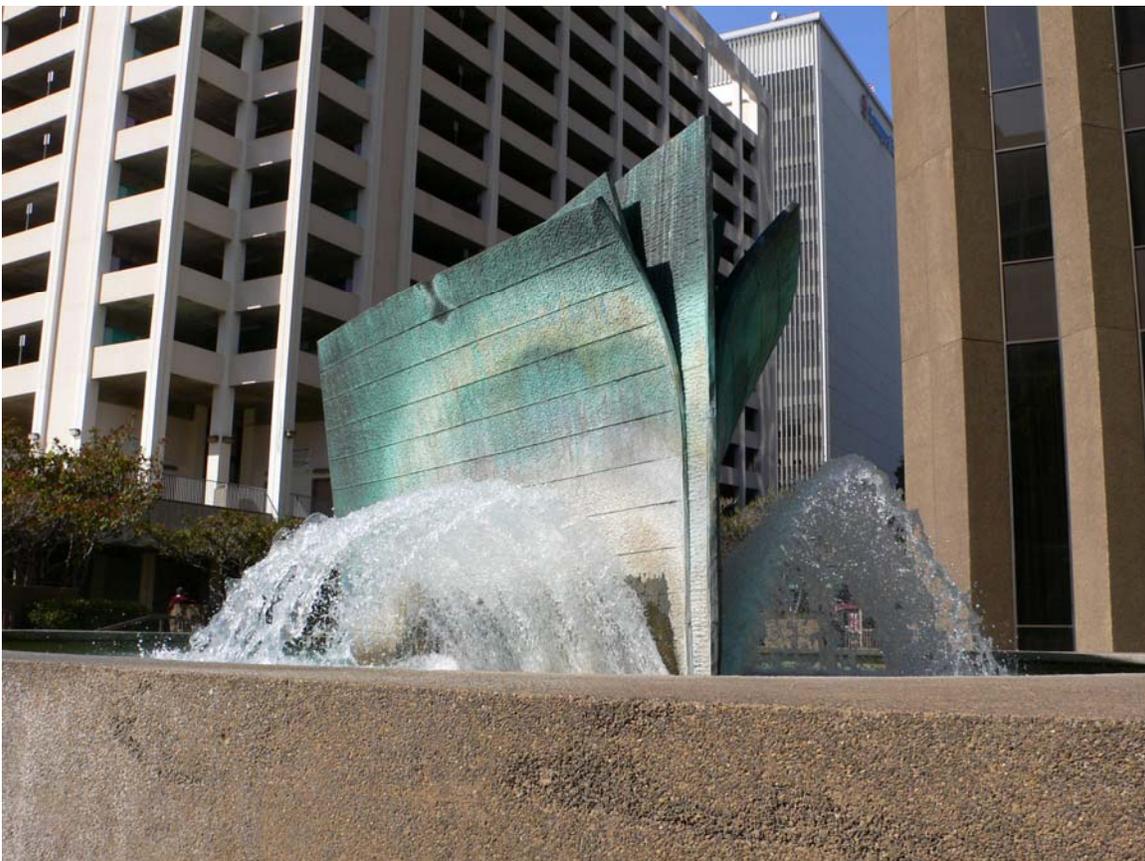


Figure 37. Civic Theater Building, looking northwest.



**Figure 38. City Operations Building, looking northeast, west and part of south elevations.**



**Figure 39. City Operations Building, looking east, west elevation.**



Figure 40. City Operations Building at right, looking east, north elevation.



Figure 41. City Operations Building, looking west along walkway, north elevation.



Figure 42. City Operations Building, looking southwest along 1<sup>st</sup> Avenue, east elevation.



Figure 43. City Operations Building, looking west, upper east elevation.



Figure 44. City Operations Building, looking west along bridge, upper east elevation.

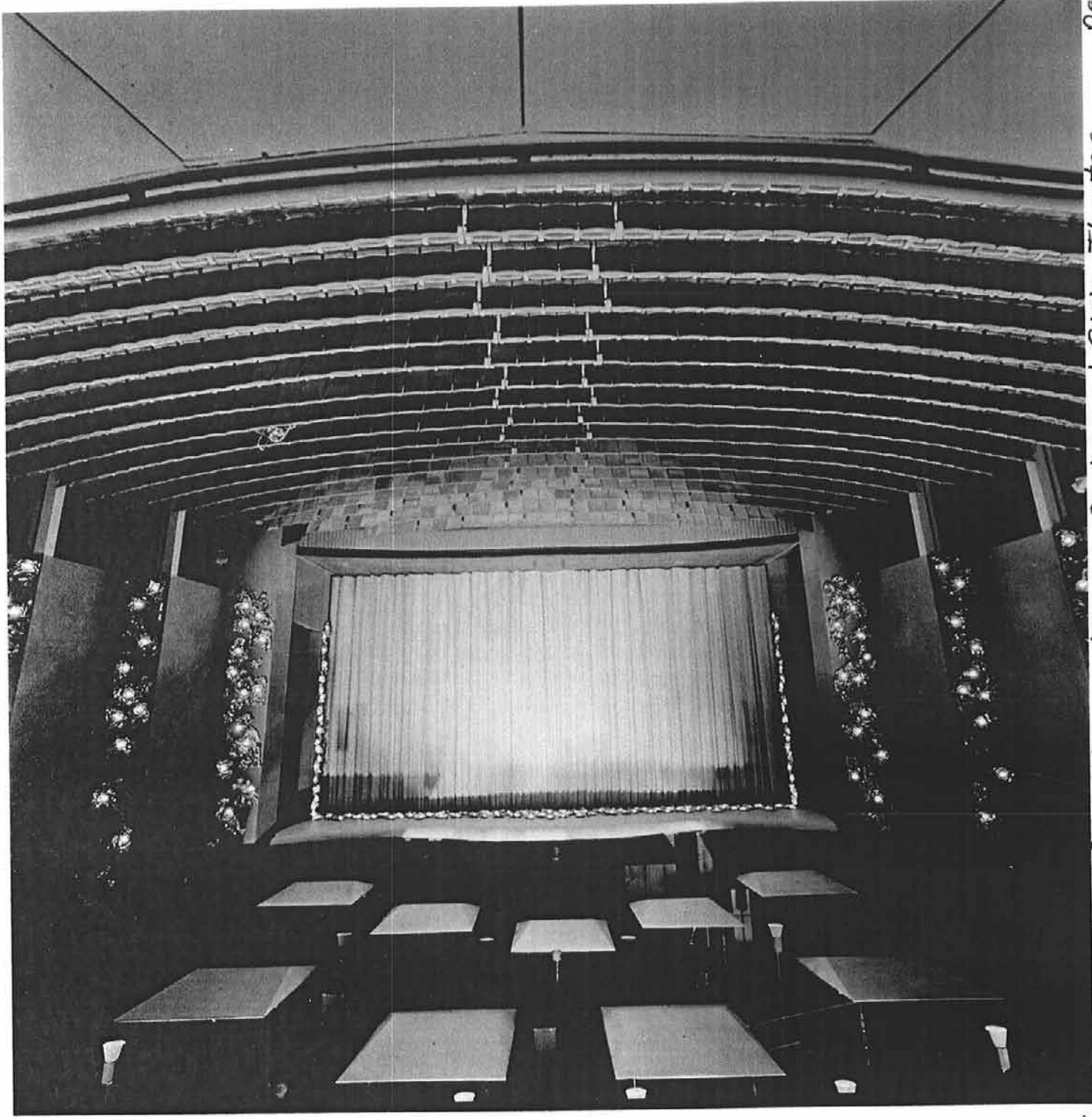


Figure 45. City Operations Building, looking northwest, east and part of south elevation.

UT 85: E3345 #2 San Diego Skyline View - Aerial / Over Civic Theatre Looking South



Dec. 31, 1964

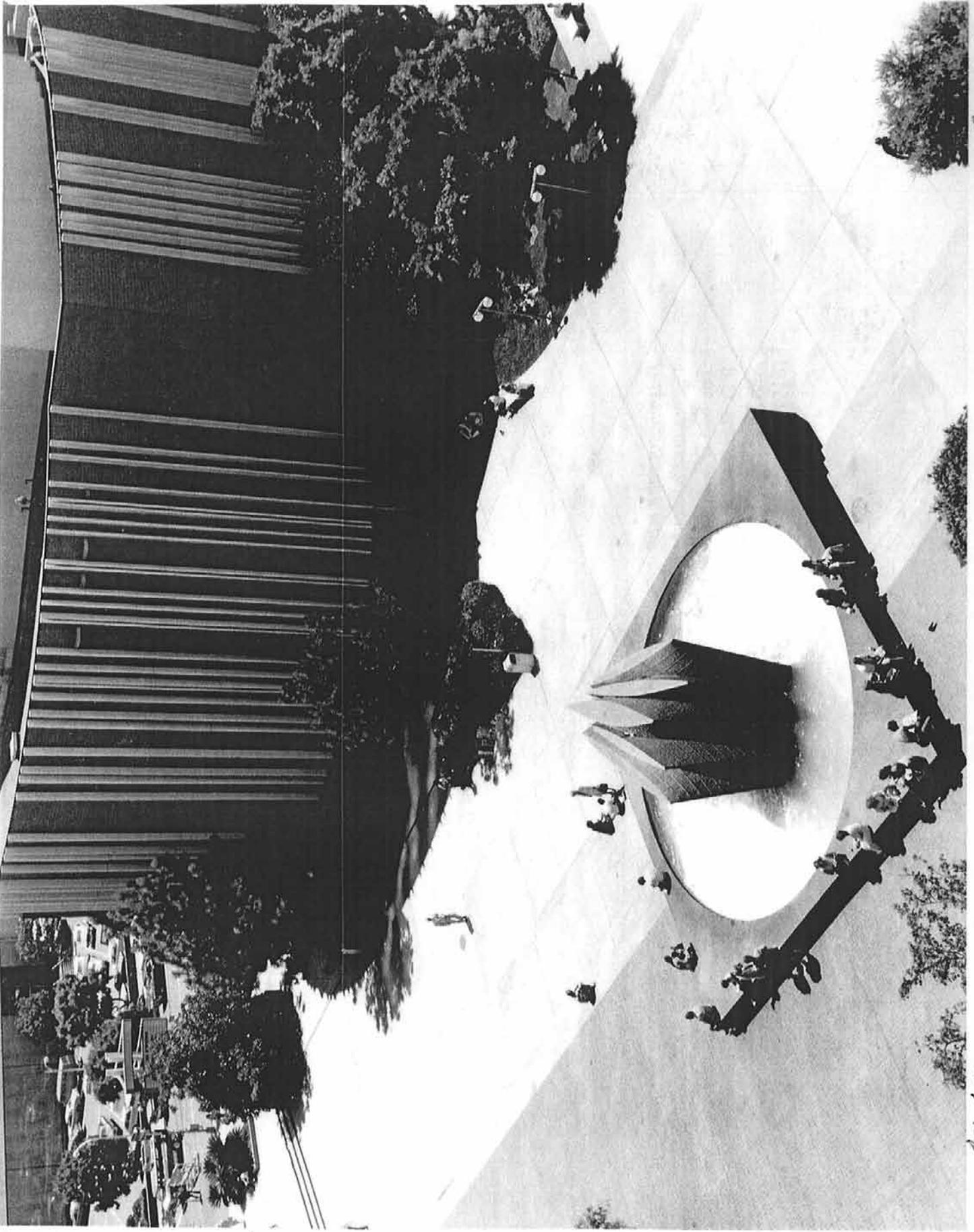


Dec. 30, 1964

UT 85: E 2592 #7E Theatres-C, Stage and Lower Seating at Civic Theatre

UT 85: E 2932

00-1-100  
08-62-8



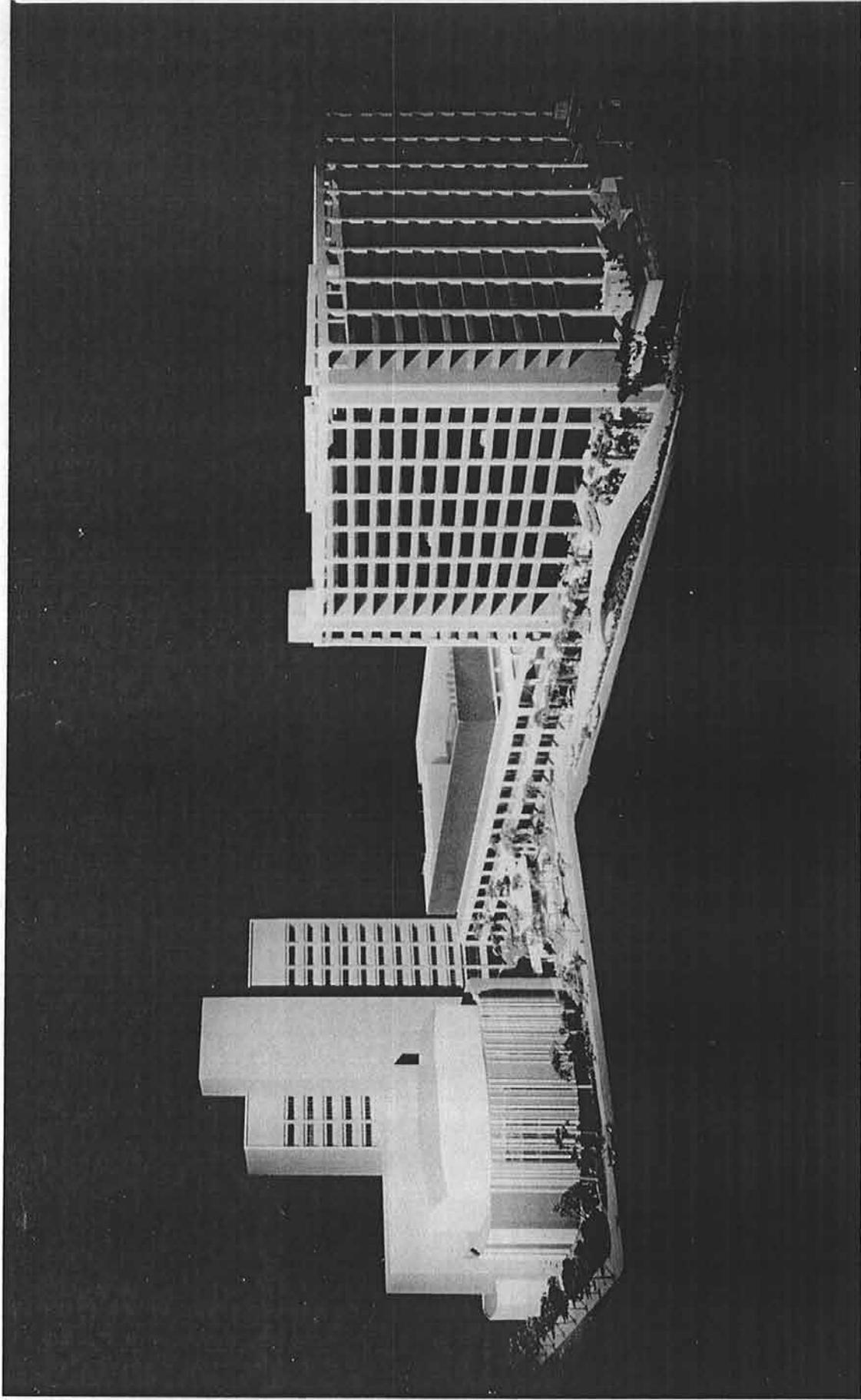
0059 108

UT 85: E3345#2

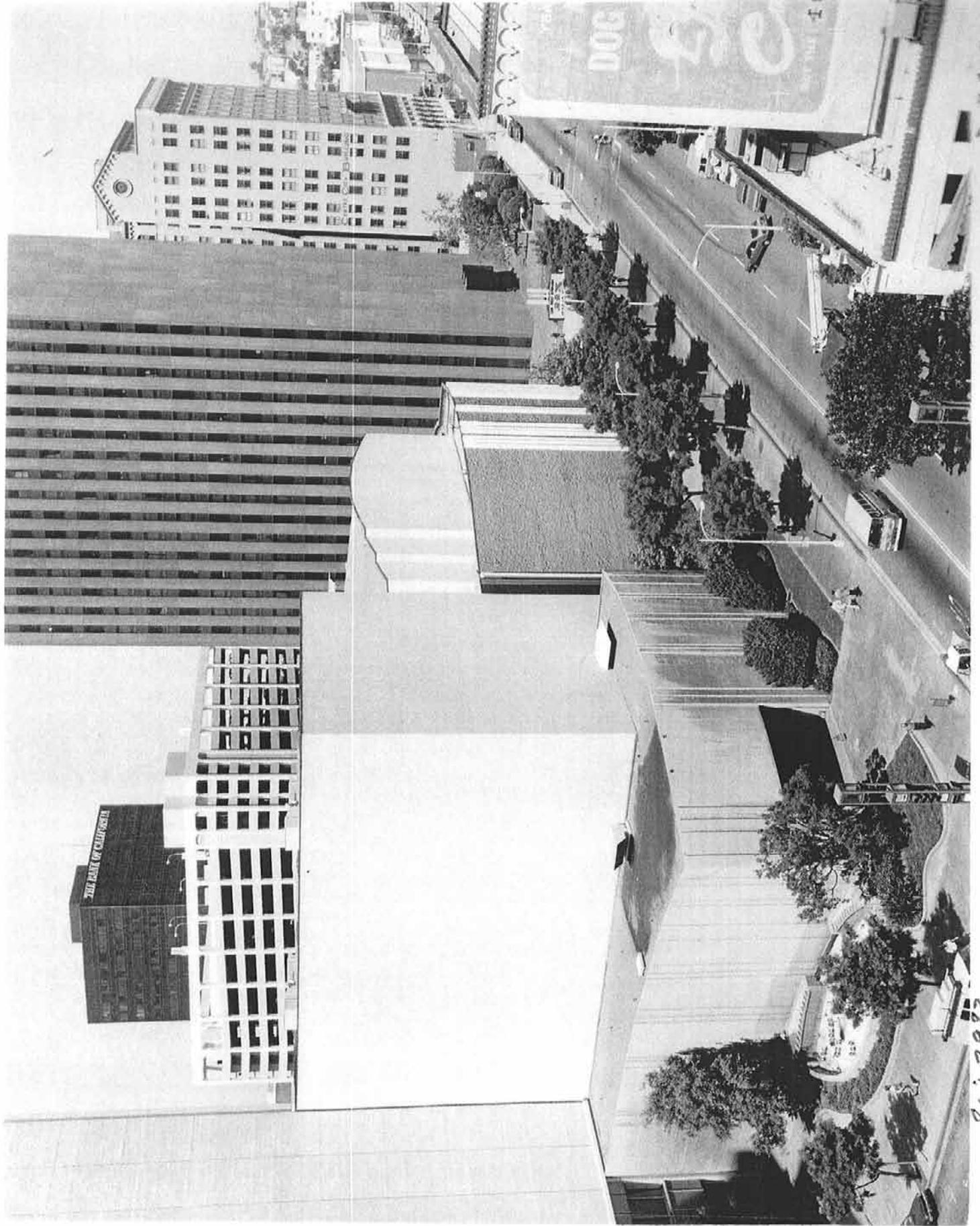
San Diego, Skyline View - Looking Over Civic Theatre to San Diego Harbor

Dec. 31, 1964



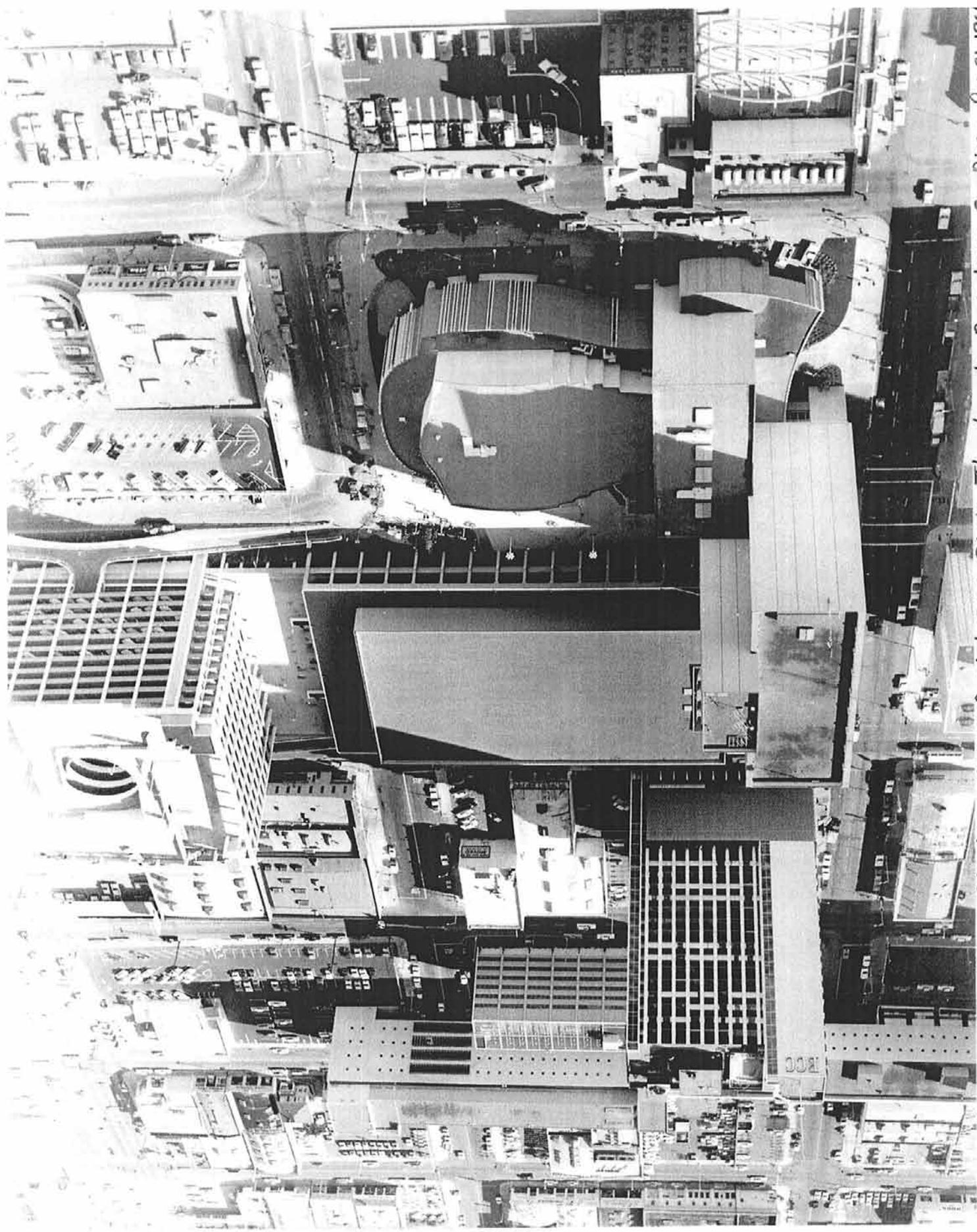


Model A Photo  
concept

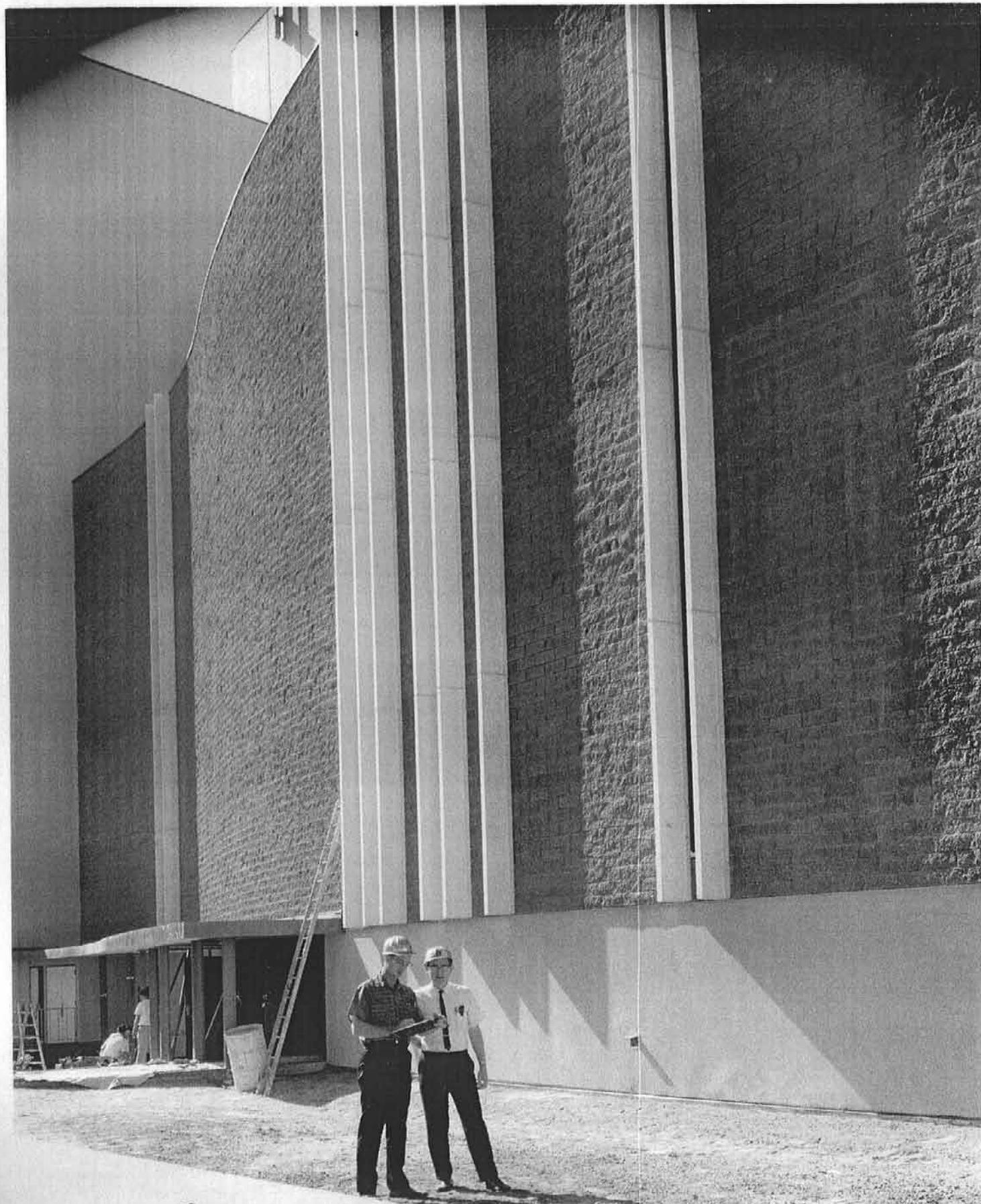


80:2983

3-21-10



UT 85:E3345#2 San Diego Skyline View - Aerial Over Civic Theatre to Downtown San Diego Dec. 31, 196



BUSINESS - EXTERIOR

15-0000 DAVID THEATER

1964

1964



May 2, 1964

UT85: E33945 #2 San Diego Skyline View - Aerial Over Civic Theatre to San Diego Bay Dec. 31, 1964



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The San Diego Historical Society

P.O. Box 81825

San Diego, CA 92138

(619) 232-6203 x 127



**APPENDIX F**  
**Research Articles**

Arts and Architecture

San Diego Union Tribune

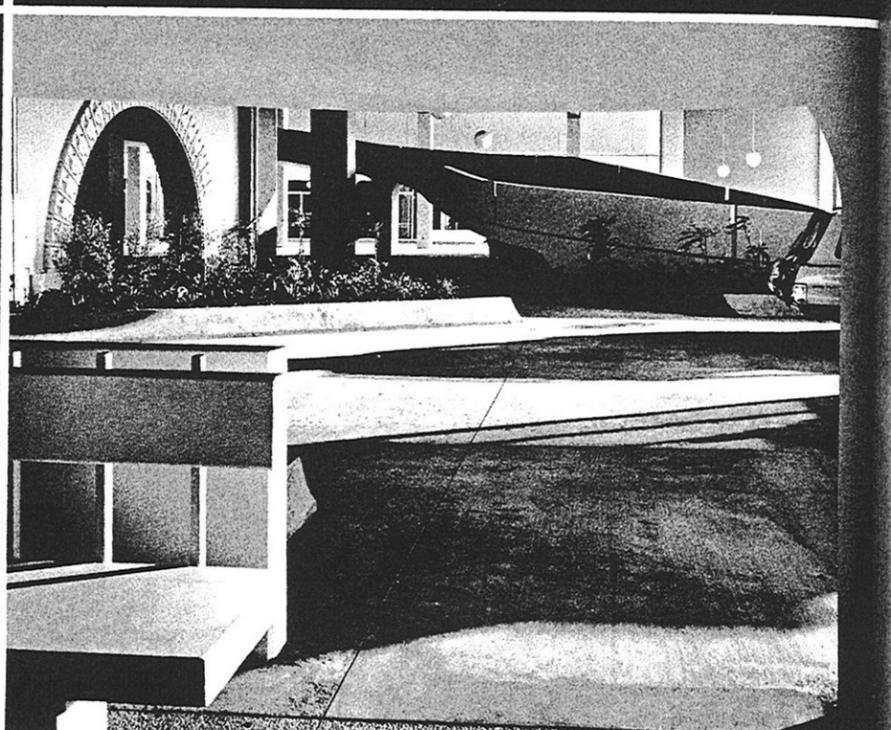
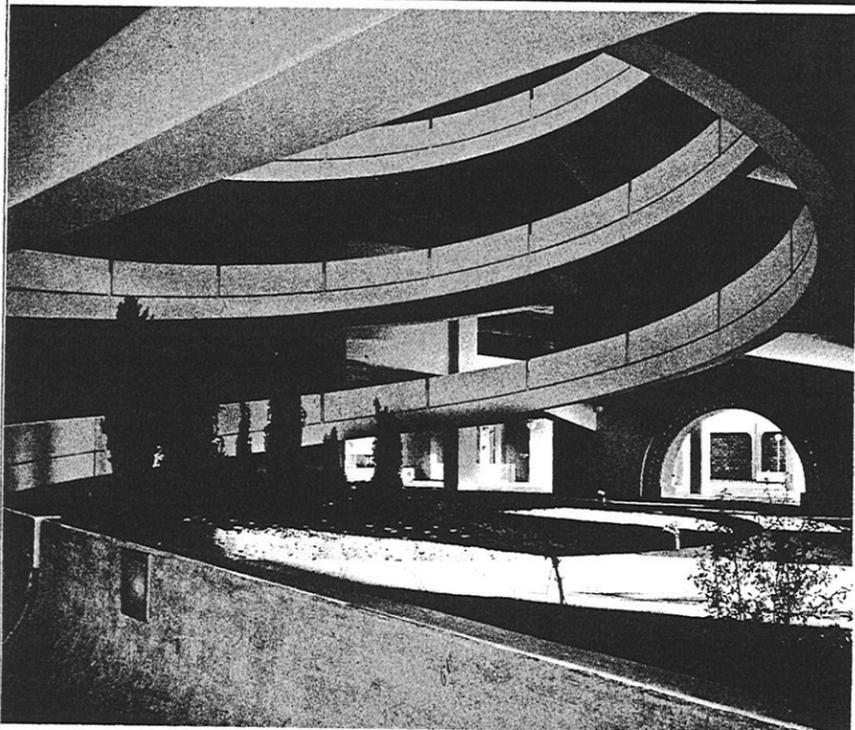
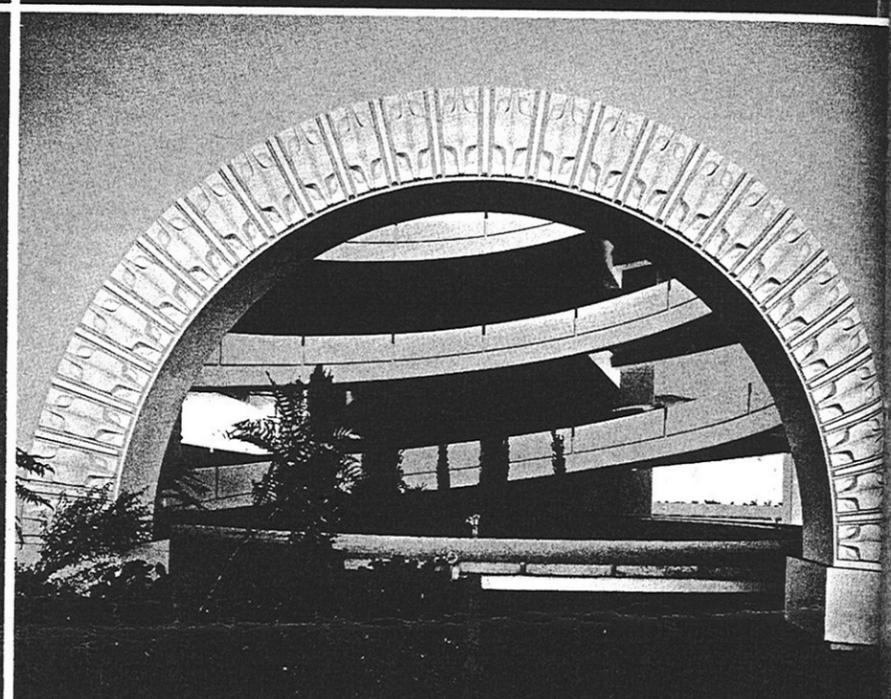
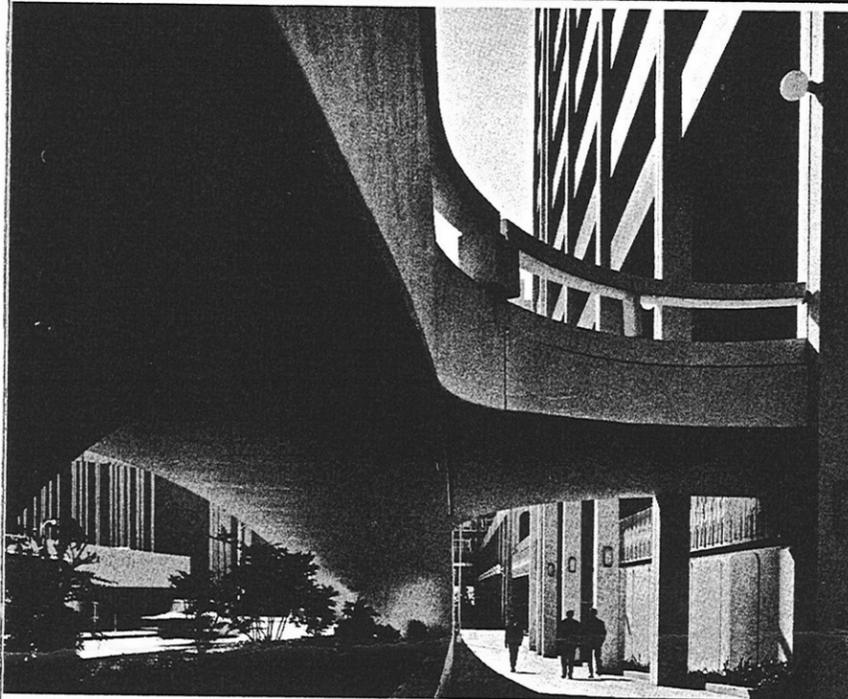
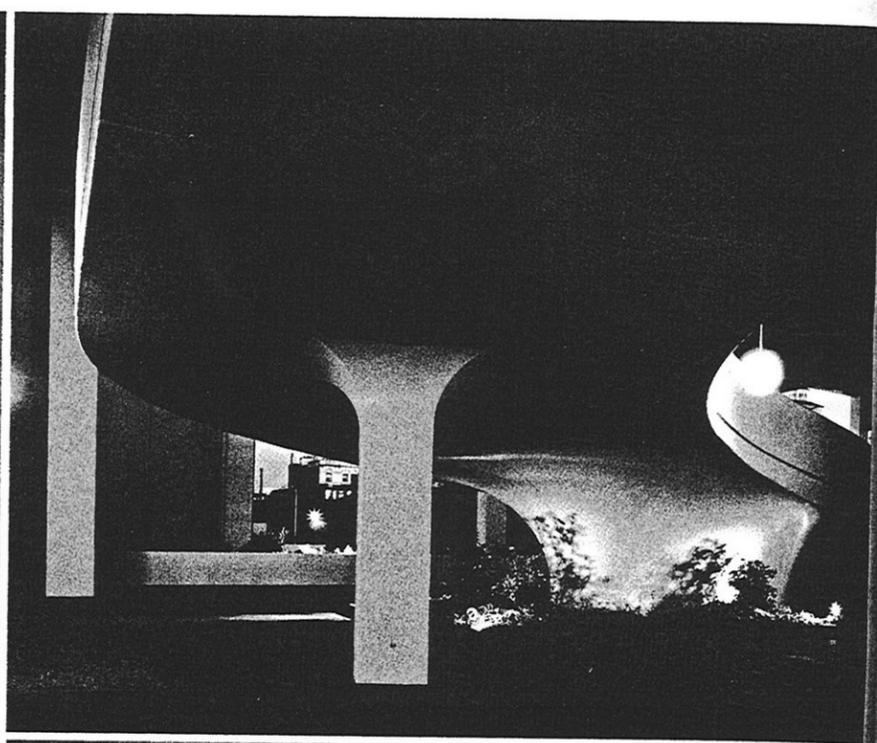
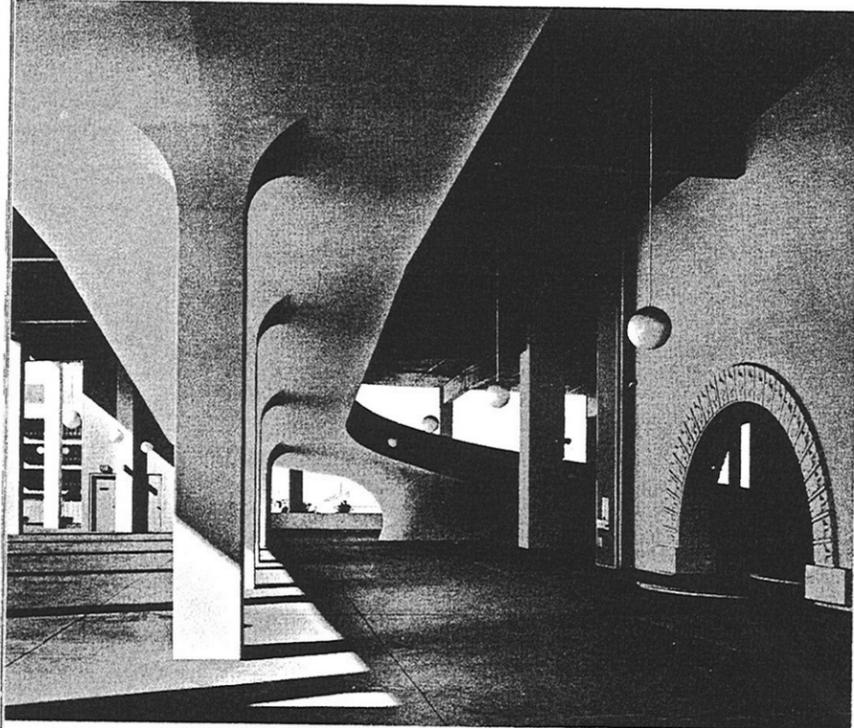


JIM TRASK  
22591 - 182ND AVE.  
STRATHMORE, CALIFORNIA

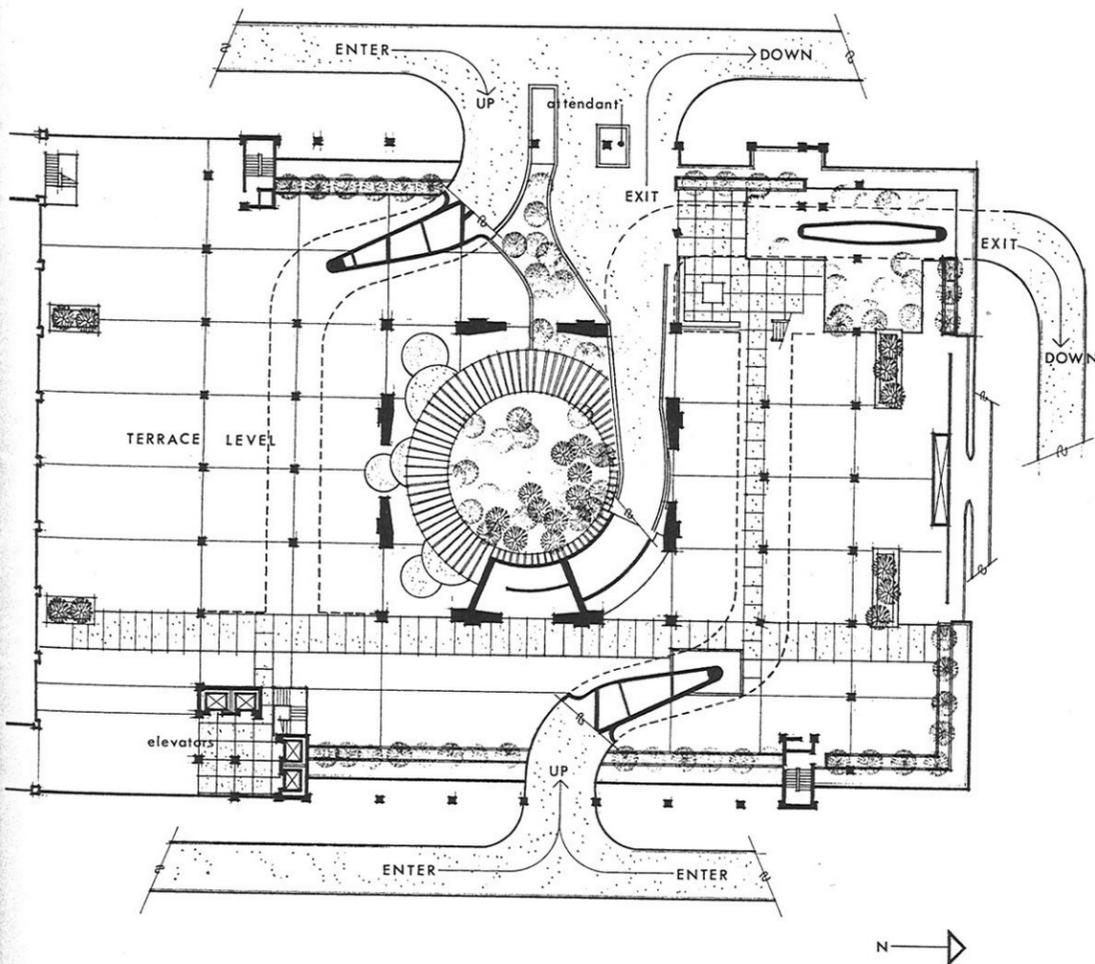


**arts & architecture**

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nut  
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## TUCKER, SADLER AND BENNETT, ARCHITECTS

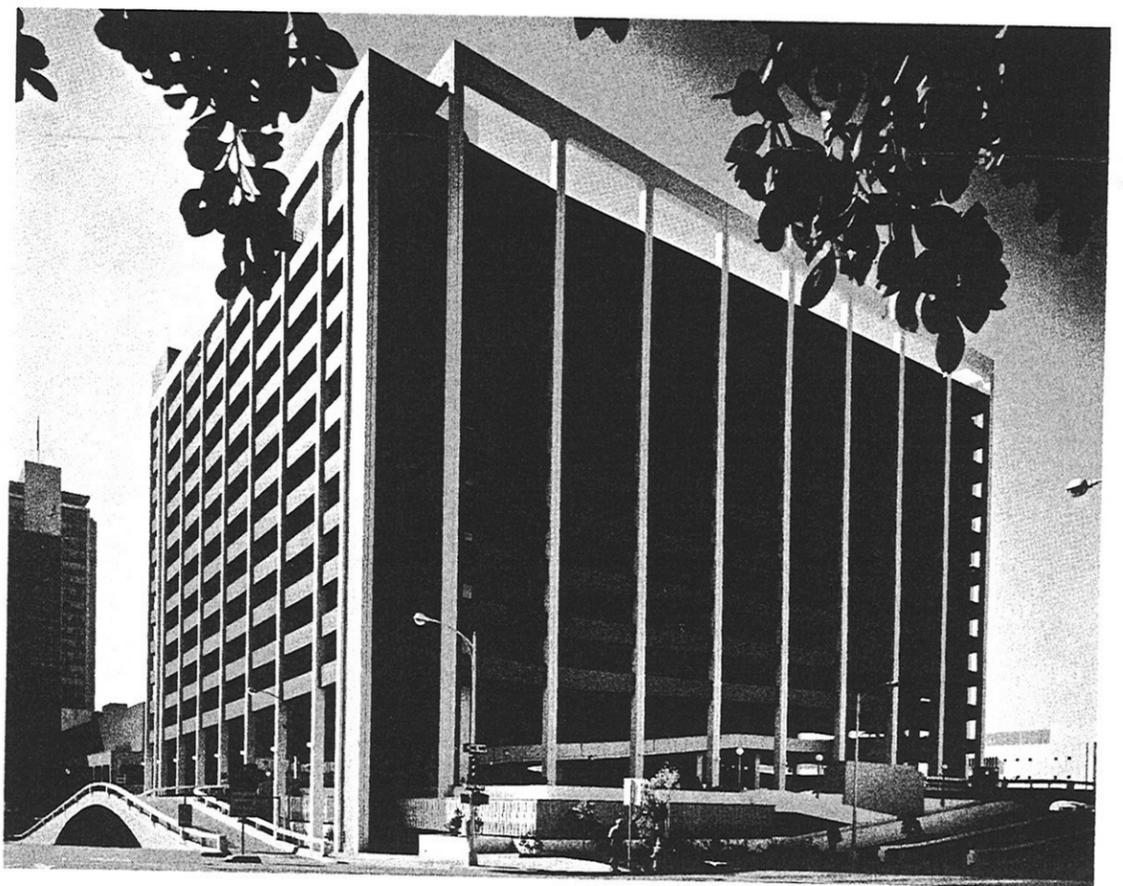
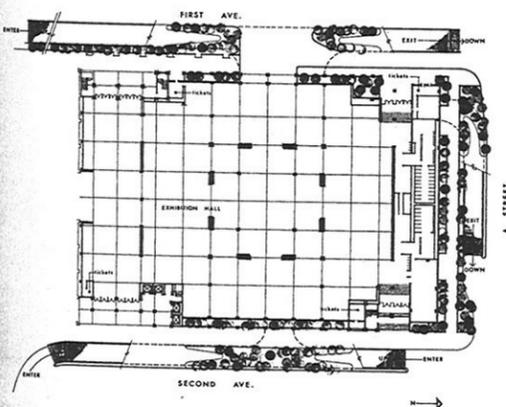
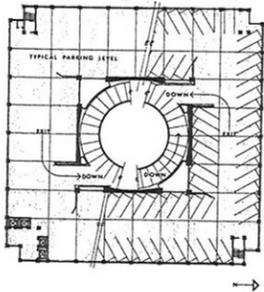


This prestressed concrete building contains a 56,000-square-foot exhibition hall surmounted by a 10-story parking tower and is part of the new San Diego, Calif., Center City Concourse. The one-story exhibition hall, partially below grade, is separated from the tower by a terrace level which is a city block in size.

The parking facility and terrace will serve as the main circulation elements, serving and connecting all buildings in the concourse—convention hall, administration offices and theater. The 200'x 200' tower has a circular core of two separate but intermeshed express exit ramps serving alternate floors, creating two garages with a capacity of 500 cars each. Access from the street to the continuous, sloping parking levels is by sculptured ramps.

The terrace acts as a transition area for cars and for visitors alighting from or waiting for their autos. It is intended as a place where people will gather and is to be used for art exhibitions, garden shows and the like.

Cost of the structure was approximately \$3.5 million. Prestress consultant was James Libby; contractor was F. E. Young Construction Company.



# \$37.8 Million In Building

## \$600,000 Tax Gain Forecast

By EDWIN G. MARTIN

Private developers propose \$37.88 million of new buildings in San Diego's central area as a result of the Center City project, it was reported yesterday.

The new buildings will be built to replace structures now valued at \$2.58 million for a net gain of \$35.29 million, according to Edward Goodwin, president of San Diegans, Inc.

Goodwin gave the figures at the annual meeting of the organization, composed of a group of San Diego businessmen interested in strengthening the economy of the central area.

Guests at the meeting, held at the 10th Avenue Marine Terminal, were Mayor Dail, members of the City Council, harbor commissioners and city planning commissioners.

### \$720,913 IN TAXES

Goodwin, who was re-elected president of San Diegans, Inc., said the new buildings will bring the city, the county and other taxing agencies a total of \$720,913 a year in taxes, compared with \$49,270 at present.

The tax estimates were based on the buildings being assessed at 25 per cent of real value on a combined tax rate of \$7.61 for each \$100 of assessed valuation, the current rate for a San Diego resident.

On the other hand, the seven city projects in the Center City plan will result in the loss of \$69,816 a year in taxes, Goodwin said. When this amount of taxes is deducted from the estimated gain, the net over-all tax increase will be \$69,827 a year.

### ADDITIONAL PROJECTS

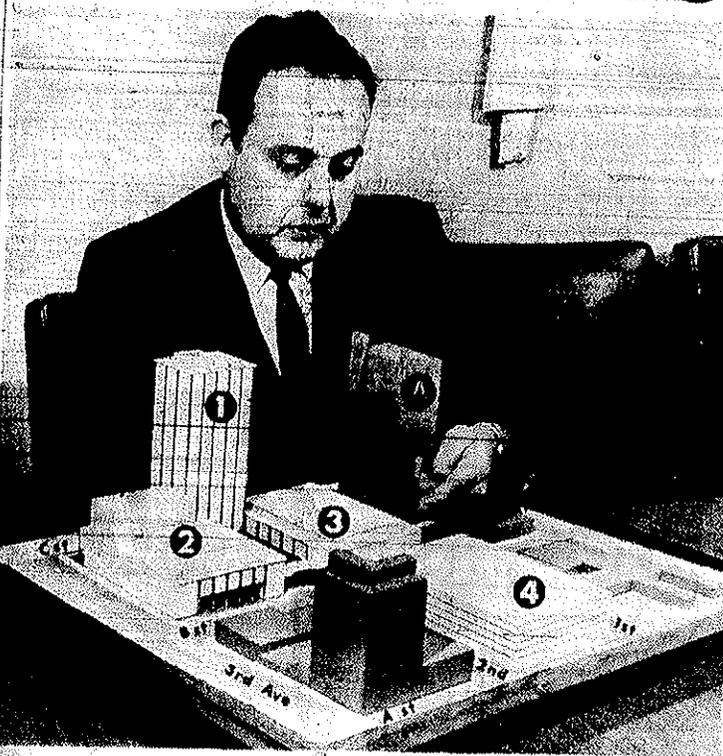
"There is no question in my mind but that additional projects now on the drawing boards will come into being by 1965 and that the basic net increase in overall tax revenue from the central area will exceed \$1 million a year," Goodwin declared.

Included in the \$37.88 million of new buildings listed by Goodwin were Eward Kahn's First Avenue and C Street skyscraper, C Arnold Smith's Second Avenue and Broadway tower, the new Home Federal Savings & Loan Building, the El Cortez Convention Center and a proposed U.S. Coast Guard addition.

Goodwin said the Center City project, which will involve the spending of \$46 million by the city over a 46-year period, will result in the most efficient Center City area of any city in the United States, containing a population of 28,000 in 100 acres.

### FIVE-MINUTE WALK

I know of no city with a population of more than 20,000 that will have a central retail area, the central transit district and the county, state and federal buildings within five minutes' walk.



Model of Community Concourse and City Hall is displayed by Henry E. Brink, an architect on project. New Kahn building (A) and Security First National's existing building (B) are shown with proposed City Hall (1), Civic Theater (2), Convention Hall (3) and 1,000-car parking garage (4). City Hall, theater and convention hall will face on C Street.

### ARCHITECTS' AID

## Council Sees Model Of Center City

A model of San Diego's proposed new \$7.5 million Community Concourse and \$14-million City Hall was unveiled yesterday.

The City Council was shown the model in the board room of the Bank of America.

It was prepared by Sam W. Hamill, supervising architect of San Diego's Center City project as a guide to architects, who will be hired this month to design the buildings.

The Community Chest Concourse is the name given to a Convention Hall and Civic Theater. The two facilities will be separate buildings but they will be linked to the 17-story City Hall and a 1,000-car parking garage.

### COMMON UTILITIES

Hamill said the four structures will have common utilities and basement and the Concourse and City Hall a common lobby.

City Mgr. Tom Fletcher said the council probably will receive recommendations on hiring architects for the four structures Jan. 30.

The Center City Advisory Board, headed by Guilford Whitney, expects to have its recommendations on architecture next week. The board has been interviewing architects who applied to design the structures.

## Navy Pilot Tells Of Fatal Collision

By DICK BOWMAN

A Navy pilot who narrowly escaped death Sunday when his single-engine plane sheared off the tail of another plane killing its pilot yesterday described the tragic mishap.

### FATHER OF 2

The interview took place on the flight deck of the attack carrier Ticonderoga, which had just returned to North Island after an eight-month cruise in the Western Pacific. The crash, which occurred 1,500 and 2,000 feet when I first saw Apee's plane about a mile and a half behind me. I never saw him again until his plane slid under my wings. My prop hit his aircraft between the tail and the cockpit. I didn't see him hit the water."

## Boost Sought For Building Permit Fees

By CHARLES ROSS

A 15 to 25 per cent increase in fees for county building permits was proposed to the Board of Supervisors yesterday by J. S. Waldrep, chief of the county Building Inspection Department.

Waldrep said the increase is necessary to offset a drop in building and to update fees, which have not been increased for several years.

Fees for homes in the more expensive category should be increased considerably more if he fell or was thrown from a car, since they require more hours of inspection than tract homes. Price range in this category is from \$2,500 to \$50,000.

## In 14% Rise Over 1960

### Boney Hails Total Of \$3,265,000 As 70% Of Goal

By NATALIE BEST

The 1961 United Team campaign raised \$3,265,000, which represented 70.5 per cent of the goal and 14.2 per cent increase over the amount raised the previous year, Henry A. Boney, campaign chairman, told the United Fund of San Diego executive committee yesterday.

Boney described San Diego as second to the top in percentage increase across the country.

"The national average increase across the country was about 3 per cent, with Providence, R.I., having the largest percentage increase in a major city with 16 per cent," Boney said.

Boney lauded the military as a major reason for the success of the campaign.

"The performance by the military was truly outstanding," Boney said. "They raised \$640,000 as compared to \$471,000 last year. The rest of us had better catch up."

### MERGER PLANNED

In addition to the campaign report, the merger of the San Diego Community Chest with the United Fund, on or about Feb. 1, was announced by John McQuilken, president of the chest.

Joint committees will meet in the next three weeks to set up the new organization, which probably will be called United Community Services," McQuilken said.

The Community Service, organized here 41 years ago, has been a beneficiary agency of the United Fund since the latter's organization in 1953. This year United Fund will allocate \$1,808,244 to the chest, the largest single allotment.

The Community Chest, in the U.S. Grant Hotel, will (Continued on A-16, Col. 4)

# Kell Jury On

## REQUEST FOR 'FIR

Requests began copies of "The Fi articles on reading Sunday in The Sa

The series, written by Diego Union's editor in booklets for booklets will be

In addition to Union has received from the city sch

Davis' series; ing, along with so all levels of educ County.

Persons desiring requests to "The I Union, 940 Third

# Ryan P 300 En

### Release Of L Will Spur Bu

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San Diegans bring then overcoats hook heater thermostats a

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At Lindbergh Field

## Miramar CPO Hunted In Theft

The manager of the chief petty officer's mess at Miramar Naval Air Station yesterday was named in a federal warrant charging him with embezzling \$12,129.

Named in the warrant was Charles Watson Moody, 35, a chief aviation electronics technician at 874 Hudson Dr., whom the Navy said was last seen by his wife a week ago yesterday.

Thomas E. Bishop, special agent in charge of the Federal Bureau of Investigation, said his office is conducting a search for Moody and declined further comment.

The Navy said the shortage was discovered Friday during an audit following Moody's failure to return to duty.

The shortage amounted to a three-day liberty.

Moody was last seen at 7 a.m. Jan. 8 by his wife, who called him the Navy said.

The warrant for Moody's arrest was issued by U.S. at

San Diego.

at Lindbergh Field

## Weath It Real

San Diegans bring then overcoats hook heater thermostats a

maintained that the to tains were only slight

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# Teaching Machine Limitations Told

(Continued from p-18)

to do better and as a result they did nothing: It lacked inspiration." That is the problem of the mechanical age, he told the teachers. "It's up to you to help solve it right now," he added. "Inspiration will always remain the quality of good teachers. Without it they become mere machines." Smith was one of three speakers addressing teachers' audiences at the convention, where 1,200 teachers are registered. It opened Friday and will continue through today.



HALLETT SMITH calls for inspiration

Walter Loban, a University of California professor at Berkeley, urged the group to re-emphasize oral language as a way to teaching better composition. TEACHERS CAUTIONED From research with 300 youngsters in the Berkeley-Oakland area, Loban said, he has found that those who speak well also write well. Loban cautioned teachers against downgrading children who do not speak standard English, saying, "It doesn't necessarily indicate a low mentality." Children should be taught standard English, however, he added, "because of its place in business and educational worlds."

In a closing business session, delegates elected Miss Viola Granstaff, supervisor of high school English for the San Diego City schools, as secretary for 1962. Other officers include Edward Farrell of the University of California, Berkeley, president; Fred Marcus, Los Angeles State College, vice-president; William G. Strayer, University of Redlands, program chairman.

"SIRIOUS MATTER" Maude Edmondson, Merced City School District membership chairman, and George Niemeiz, Elk Grove High School, member at large.

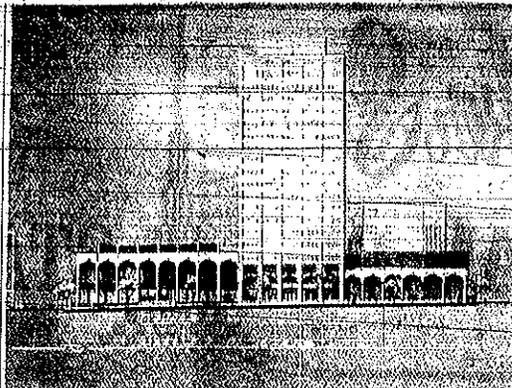
Living Howe, a professor of English at Stanford University, another speaker, traced the changing image of heroes in modern literature.

"SPEECH DISCUSSED" "Today's hero is more the man or woman carrying heavy mental or moral burdens. There is no longer the image of a man with excessive physical ability — instead it appears that heroes are the people restrained from performance by restraints in the mind."

Helen Mowers, president of the Greater San Diego Council of English Teachers, presided at the opening session Friday. Discussion sessions were conducted yesterday on a set of persons nominated for this honorary position.

Delegates, who passed the resolution unanimously, asked that copies be sent to the journal, chairman of the rules committee and speaker of the literature and psychology assembly.

The 1963 convention was set for March 21-24 at the Statler Hilton Hotel in Los Angeles. Delegates called upon state officials in a resolution, to meet the selection of a state poet laureate as a "serious" matter. "The resolution read: 'Resolved that the California Association of Teachers of English recommend to the California Legislature that it give serious consideration to seeking the advice of professional organizations qualified to assess the literary ability of persons nominated for this honorary position.'"



Drawing shows how Center City complex might look. City Hall is in the center flanked by theater, right, and convention hall, left.

## CONCOURSE FINANCING Center City Drive Plan Broadened

(Continued from p-13) basic construction of the facilities is assured. However, the more money donated the better they will be built.

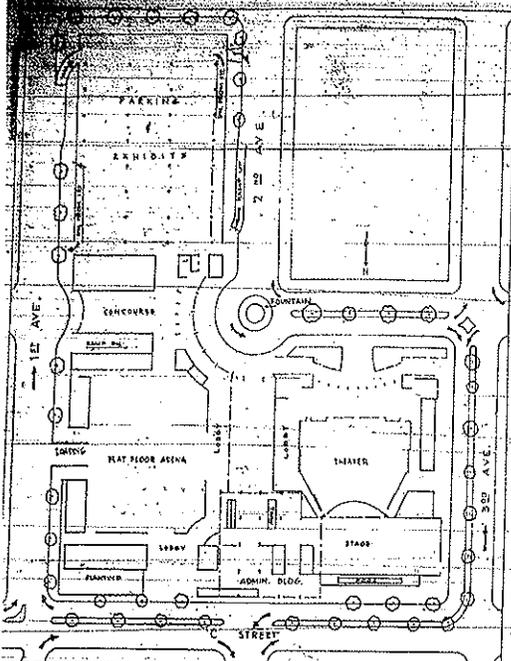
As an example, Whitney said it may be possible to install larger or more comfortable seats in the theater. Or more landscaping may be placed. "The point is, we here in San Diego should want these facilities to be the finest possible," Whitney declared. "I believe everyone agrees that we've neglected them for years. Now that we're building them, we should try to make them outstanding in every way."

## BLUEJACKET REVIEW TO SALUTE RED CROSS

A bluejacket brigade will salute the American Red Cross for its services to the armed forces in a parade Friday at the Naval Training Center. O. W. Todd Jr., San Diego County Chapter chairman, announced yesterday. Todd said the review of 3,000 recruits will open the observance of Red Cross Month in San Diego County. Capt. Donald J. Thomas, center commander, has invited Red Cross volunteers from throughout the county to attend the 3:30 p.m. review where Donald W. Stout, Red Cross Pacific Area manager, will award the American Spirit Honor Medal to the outstanding recruit. Todd characterized the brigade review as a signal honor for Red Cross workers and "an important occasion for all of us."

## Flying Metal Kills Inspector At Test Plant

A mechanical inspector was killed by a flying bar yesterday following a malfunctioning of a new high-energy machine in a test at the Advanced Products Division of General Dynamics Corp. at 2920 Harbor Dr. He was identified by police as Dennis O. Willis, 46, a quality control inspector who was on temporary duty here from his home in Riverside. Officers said Willis was working with George C. Berg, test engineer of 800 Lata Lane, El Cajon, on a test of a nitrogen gas powered metal forming machine. The inspector apparently moved from behind a protective shield and was struck in the face by a metal bar which broke from the machine, the inspectors said. He was pronounced dead at the scene. Deputy Coroner Max Hurler said Willis is survived by his wife, Melba, who is staying at 2333 1/2 Monroe Ave., a son, Craig, 10, and a step-daughter, John, 10, an Airman attache in a session at 8 a.m. today on committee and speaker of the literature and psychology assembly.



General plan of the Center City complex is shown in this drawing. Parking-exhibit building is in upper left between A and B streets. Convention Hall is lower left and Civic Theater is lower right. B Street and Second Avenue would be closed to provide for open concourses. City administration building would be built on stills to allow passage between theater and auditorium fronting on C Street. Existing Security First National Bank Building is in block shown at upper right.

## Three Assembly Buildings To Supplement City Hall

(Continued from p-13) in with the concourse-walkway concourse between the building will be created onways, escalators, elevators, arena and parking building. get and concrete stills beyond such. The plan envisions a fountain in lower levels can contribute. A huge basement will be built in the circle at Second to the open effect and the built under the theater and Avenue and B Street to accessibility of the rest of the convention hall for utilities, hence the appearance, storage and work areas. Refreshing pools are also planned. The city hall will rise 16. Hamill's plan calls for Sec. marked for the main corridors above the street with Second Avenue to be closed to concourse and outside the rear office buildings starting between B and C Streets. It will wall of the theater. Most of about the fourth level. The be closed between First and the buildings probably will see under this will be the second, where it will become be of concrete.

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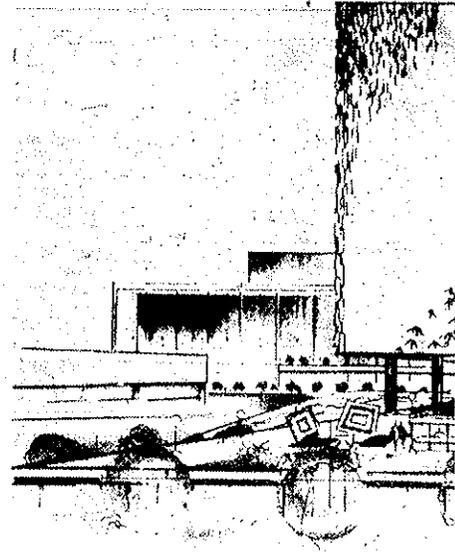
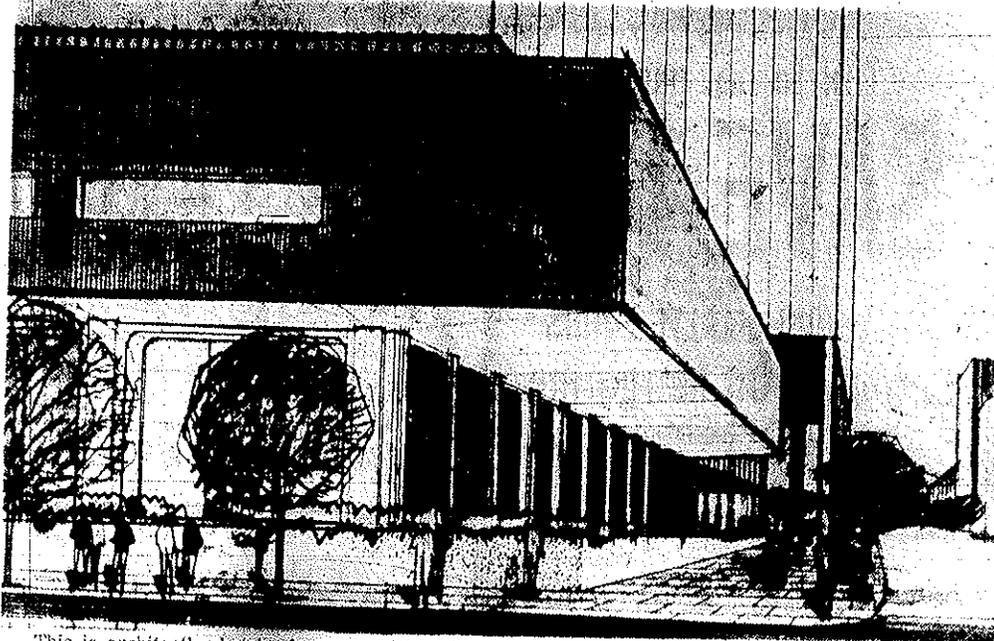
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## First Concourse Designs



This is architect's sketch of proposed \$4 million dollar convention hall in Center City Concourse.

looking northeast from First Avenue and C. Street. Hall would have arena, grand lobby, assembly hall.

One of main features of garage-exhibit hall shown in this architectural drawing would be terraced.

### Council OKs Mission Bay Park Names

The City Council yesterday ordered a resolution drawn adopting a list of new names for various places in Mission Bay Park.

The new names were suggested by the Mission Bay Commission after two years of study. The city Planning Commission split 3-3, on approving the names with objections that a few names were too commercial.

"I don't like some of these names, but we've wasted enough time," said Councilwoman Helen Cobb in making the motion to have the resolution drawn.

Forty-six names are involved. Of these, 13 are the same as existing names, eight are changes, and 25 are new names for places now unnamed.

The changes are "Vacation Isle" for Tierra del Fuego; "Bahia Point" for Gleason Point; "Santa Barbara Cove" for Asher Cove; "Piesta Island" for Cabrillo Island; "Mariners Basin" for Glenn Rick Bay; Pacific Cove" for Perez Cove; "Dana Landing" for Dana Basin, and "Sail Bay" for Crescent Bay.

Mayor Dail said he thought a proposal that a plaque or other suitable object be placed in Mission Bay in memory of the late Matthew Gleason was "a good idea."

### Marxist Banned At SDS Talks At Off-Campus Event

See 'Letters to the Editor'—Page 6-2.

By NICK WILLIAMS  
A controversy over what constitutes an "open" meeting continued yesterday when a Marxist editor, originally scheduled to speak on the San-

Diego State campus, was asked to address a student group in an El Cajon Boulevard restaurant.

The talk by Joseph Hansen, a former secretary of the late Marxist Leon Trotsky, first was planned as an on-campus, open meeting

under the sponsorship of the Committee for Student Action, a student group.

A student-faculty committee Monday imposed a moratorium on open meetings. The CSA officers reset Hansen's meeting, which was to be presented in the college's Humanities-Social Sciences Building at 11 a.m. yesterday.

A meeting between a CSA officer and college authorities yesterday morning, however, produced an agreement that the Monday decision of the college Lectures and Concerts Board ruled the meeting "open" because of advance publicity given it by Allan Lachman, CSA vice-president, said he therefore asked Hansen to speak at a luncheon in the restaurant.

#### MEETING BANNED

Herbert C. Peiffer, dean of students, said he and Dr. Stanley Pincus, a history professor and the CSA faculty adviser, concurred with Lachman that the LAC ruling banned the meeting on the campus until the moratorium is lifted.

Lachman said he and other CSA officers agreed to hold the meeting off-campus, at the Campus Chuck Wagon, 6265 El Cajon Blvd., until the definition of an open or closed meeting is set by a school body.

The wait lasted only a few hours. The Campus Activities Board, a joint student-faculty group, adopted the following definition of a closed meeting shortly after Hansen's talk

### Youth Admits Stab Killing Of Hillcrest Man

By LEONARD ASH

An 18-year-old sailor was arrested yesterday on a warrant charging him with stabbing to death a floral designer whose body was found in a ransacked apartment Monday morning.

The sailor, Francisco Suloan, was arrested about 5:30 p.m. on his ship, the landing ship deck Oak Hill, soon after it docked at a buoy off Shelter Island. The ship had been at sea since Monday.

Police said the sailor admitted stabbing Neil D. Steeves, 39, when Steeves made indecent advances.

#### STABBED

Steeves was stabbed nine times in the chest, upper arm and back, apparently with one of four kitchen knives found in his five-room apartment at 3574 Fourth Ave.

Detective Lt. William J. Schenck, head of the police homicide detail, said a cigarette lighter with "USS Oak Hill" engraved on it led to the sailor's arrest.

Schenck said evidence showed there had been a drinking party in the victim's apartment early Sunday and his detectives theorized that a sailor leaving the apartment in the early morning after drinking might have been stopped for questioning by police.

#### BLOOD FOUND

He said detectives found

## Col Cen

Architectural drawing of the first two buildings built in San Diego's City Concourse complex unveiled yesterday for City Council.

### GIFTS FOR SEEN TO

Private gifts to help Col-course will exceed of the Center City Adv Philip L. Gildred, via the prediction in annou now have reached \$1.3 million.

"There are several prival of directors of s "These should be read day and, together with the \$1.5 million."

The City Council will private gifts, and giving method, at its meeting a The council last week the \$1.5 million from Ma Gildred said the late are:

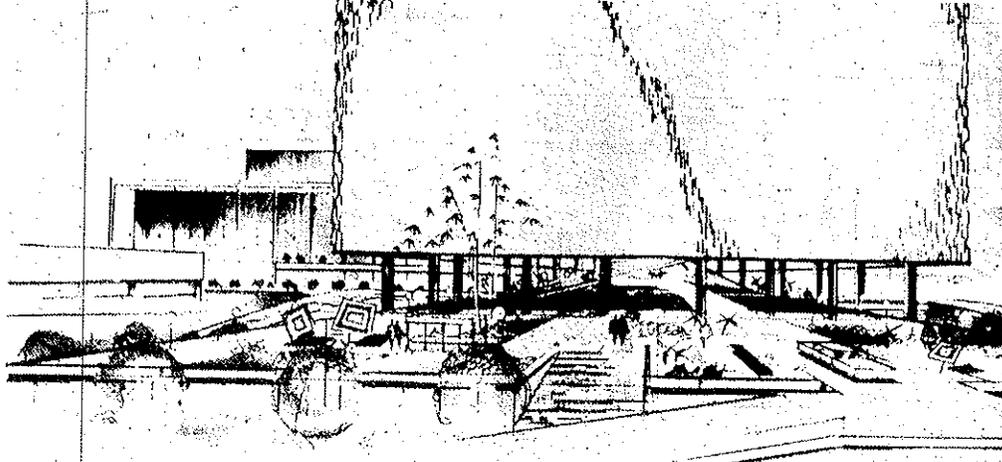
San Diego Trust & Hellbron, \$8,000, Ed St Mrs. Allan Klamb, \$1,000, Ernest estate, \$10,000, Gen. Cola Baiting, \$5,000, Service Arms \$5,000 and Philip L. Gild

### Congress Urged To Correct School Impact Aid Short

School Impact Aid Short

### Youths L

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One of main features of garage-exhibit hall shown in this architectural drawing would be terrace running under building, which would be on stilts. Cars will enter by ramp in foreground.

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### BLOOD FOUND

He said detectives found traces of blood on a banister on the stairway leading to the victim's second story apartment, indicating the slayer might have been injured in a struggle with the victim.

The detective said patrol car logs were checked and it was learned that Sinohui had been arrested about 5:30 a.m. Sunday in the 2200 block of Fourth Avenue for drunk-driving, and turned over to the Shore Patrol.

He said Shore Patrol headquarters was checked and it was learned Sinohui had been sent to Naval Hospital for treatment of a cut on the little finger of his right hand, and released to his ship.

# Council Gets Center Plans

Architectural drawings of the first two buildings to be built in San Diego's Community Concourse complex were unveiled yesterday for the city council. Shown were sketches of the \$4-million convention hall, the \$15-million complex are the \$3.1-million parking garage, the \$4-million city hall and a \$3.5-million civic theater. Construction of these is planned later.

## GIFTS FOR CONCOURSE SEEN TOPPING GOAL

Private gifts to help finance San Diego's Community Concourse will exceed the \$1.5 million goal, an official of the Center City Advisory Board predicted yesterday. Philip L. Gildred, vice chairman of the board, made the prediction in announcing that actual gifts or pledges now have reached \$1,400,000.

"There are several substantial pledges pending approval of directors of several companies," Gildred said. "These should be ready to announce possibly by Monday and, together with others, should bring us well over the \$1.5 million."

The City Council will act on a resolution accepting the private gifts, and giving formal approval to the financing method, at its meeting at 10 a.m. tomorrow.

The council last week extended the deadline for raising the \$1.5 million from March 15 to March 31.

Gildred said the latest donors to the concourse fund are: San Diego Trust & Savings Bank, \$15,000; Fred H. Heilbron, \$8,000; Ed Stricher & Sons, \$3,000; Mr. and Mrs. Allan Klauber, \$1,500; Ryan Foundation, \$1,300; Al Wuest estate, \$10,000; Seven-Up Bottling Co., \$10,000; Coca Cola Bottling Co., \$10,000; Pepsi Cola Bottling Co., \$10,000; Service Auto Parks, \$5,000; Evan V. Jones, \$5,000; and Philip L. Gildred, \$5,000.

## Youths Deny Guilt In Riot; Trial Set

Three youths yesterday pleaded innocent to charges they were connected with the Saracens, a club, which was set for April 4 in a drive-in restaurant riot in which a police sergeant was killed in the face.

Trial was set for April 4 for the three youths at their arraignment before Municipal Court Judge Roy Fitzgerald on misdemeanor charges.

They are Ernest R. Barrios, laborer of 6922 Glidden St.; Mario M. Gomez, a stock clerk of 3400 Ashford St.; and Terry W. Maszewicz, of 3111 Alayle St. All are 18.

### FREE ON BAIL

Maszewicz is charged with three counts of battery. Bar-

## Cadets Home; Heroic

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## Post-Sur Cancer I Called E

...with a new drug excellent benefit cancer patients for months, the conference at Compa. Med. a told last night.

Dr. Byron E. ... of the Medical Center, also describing drug "M" in a seminar on cancer and treatment in N. ...

### TONIC EFFECT

Hall, who talks therapy, said it is to watch in the the toxic effect. The system. He can be used for ... steps its ...

He said the di used promptly a preferably start later.

The use of the tum a patient 4 for four to 17 me ing to recent st said.

### REPRESSION N

He said exper shown remarkab in patients. Hall will respond th most within six are going to resp

He was one of ers at the annual ference sponsore stiv in cooperat California Meda tion's Commissio and the American city's California

Dr. Russell R. zoon at the Presy cal Center in San said about half of with cancer of the colon should be tought this rate only a moved.

## CALIF AUSTR

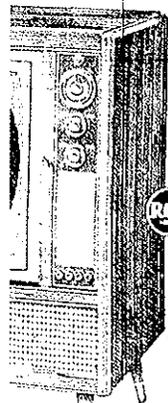
The mysterio ... (Continued on a-20, Col. 1)

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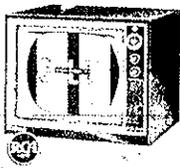
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and 29 members with 25 years or more membership yesterday at a luncheon at El Cortez Hotel.

W. H. Harry Fraser, charter president when the club was founded in 1920 and 42-year-member, was special guest of honor.

Herbert C. Peiffer, past president of 1954 was chairman of the day and gave news highlights of past years as he introduced the presidents since 1920 who were present.

Those receiving past presidents' certificates were: John J. Frazer, 1924; Philip Davenport, 1932; Emery D. Sherwin, 1937; Thomas F. Mohar, 1938; Dr. Frank M. Lowe, 1939; Elwood G. Mer-

## Council Gets Center Plans

(Continued from a-17)

of three major areas in the convention hall. The other two are a grand lobby and an assembly hall with adjacent meeting rooms.

The assembly hall will seat 504. Hope said. Seventeen meeting rooms capable of accommodating 50 to 416 persons will be next to it.

Main entrances to the convention hall will be from First Avenue and B Street, Hope said.

Sadler said studies showed that the best garage for the complex would be a self-park, continuous-ramp type similar to the parkade at Seventh Avenue and C Street but bigger.

He said his design proposes a large, open terrace of a full 200-by-300-foot block beneath the multistory parking tower. The open terrace will have reflection pools, flower shops and an outdoor eating area.

The garage will be designed so that 1,000 cars can leave in 20 minutes. He said an underground garage was contemplated, but was ruled out as too expensive because the underground water problem, insufficient ventilation and interference with the B Street water flume.

finance covered the entire first and last year.

He quoted one board member, whom he did not identify, as saying President Kennedy's late signature on the impact aid bill "would save us some money."

In addition to Fraser, those honored for being members more than 25 years were: George V. Hill, 41 years; Harvey Atherton, 37; Clay Lewis, 37; Lowe, 37; Courtland Temple, 36; Ralph Cook, 35; Howard F. Moore, 35; Aseline, 34; Bonham, 34; Frank Bueck, 34; Thomas F. Bomar, 33; Philip Davenport, 32; Willard Newman, 32; T. Don Perkins, 32; Sherwin, 32, and Louis Bodmer, 29.

Legler Bonbough, 27; Kidd, 26; Joseph Maddocks, 26; Ewart Goodwin, 26; Glenn Miner, 26; Russell S. Stowell, 26; Borthwick, 25; Keider, 25, and Edward P. Scott, 25.

President Lawrence Carr Jr. presided.

"The philosophy and strength of Kiwanis Club has helped San Diego grow through the years," Fraser, 85, a retired mortgage and loan business executive, said. A native of San Francisco, he has lived here since 1900.

Eleven new members were inducted preceding the "Legion of Honor" ceremonies.

## Board Rejects Aged Care Plan

The Board of Supervisors yesterday rejected a proposal they establish a separate pay schedule and increase rates for the care of the aged in convalescent homes in the county.

The proposal had been made by officers of the San Diego Convalescent Hospital, a private corporation. T. M. Heggland, county administrator, said the average monthly rate of \$225 is sufficient to meet the county's needs.

Funeral Friday  
For F. J. Doerr

(Continued from a-17)

ated from Grossmont High School, where he completed in track.

After the war he studied law at the University of San Francisco. He was graduated in 1949 and was admitted to practice in 1950.

He was a member of the law firm of Linley and Doerr, now Linley, Duffy and Smith, until his appointment to the bench. He was city attorney of El Cajon from December, 1955, until the judicial appointment.

In 1958 he was named to the Planning and Zoning Committee of the American Bar Association for his part in the development of planning and zoning ordinances in El Cajon.

Gov. Brown appointed Doerr to the Municipal Court bench in 1959 and he was re-elected the following year.

Doerr succeeded R. Fenton Garfield this year as presiding judge of the El Cajon court. His illness, however, had restricted his time on the court bench since last year.

Survivors include his widow, Ethel, a daughter, Lisa, 6; a son, Donovan, 4; his parents, Mr. and Mrs. Frank J. Doerr of Alpine; a brother, William of El Cajon, and a sister Mrs. Patricia Blankenship of Flinn Springs.

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# The San Diego Union

LOCAL NEWS  
D PAGE a-15

SAN DIEGO, CALIFORNIA, TUESDAY MORNING, SEPTEMBER 10, 1963



Identical triplets have arrived at Naval Training Center for boot training. From left are Larry, Gary and Harry Winter, 13, of Peabody, Kan.

## KANSANS JOIN NTC Doing Double Take —It's Triplets

A company commander at the Naval Training Center is about to inherit three problems that are as late as yesterday: the powers above him had not told him about. Included in his command will be identical triplets. Technically they are identical, but one weighs 20 pounds more than his 18-pound brothers. A Navy chief petty officer said, "By the time we're through with them here they'll all weigh the same."

The triplets are Larry, Gary and Harry Winter, 13, of Peabody, Kan. Larry made a comment to a reporter yesterday that may give a hint of things to come. "Our commander looked at one of us yesterday, then he saw another one of us, and said: 'That man sure gets around.'"

The Navy, however, may be getting a better bargain in the three than it yet realizes. "Not only are the three youngsters in a career but they are championship baseball players."

## Accident Bill Hits \$22 Million

It cost San Diegoans \$22 million to pay for their traffic accidents in 1962. This was the loss in wages, medical bills, insurance and property damage, according to a National Traffic Safety Council estimate. Benjamin A. Geerts, executive director of the San Diego County Traffic Safety Council, presented the figures at a luncheon meeting of the San Diego Highway Development Association.

## Kickoff Is Today In UCS Fund Drive

Today is kickoff day in the McCone campaign chairman United Community Services. "It's a giant effort, but the San Fund \$1 million 'Help Your San Diego' has reached the stage. Whether it can take such an effort in stride, we have of November more than 17,000 grown up as a community volunteer workers will canvass San Diego County from house to house soliciting funds to finance 1964 operations of 78 UCS member agencies. Red Cross, YMCA, Boy Scouts, Girl Scouts and Veterans, and Rear Adm. A. E. O'Connell, commander, 11th Naval District, military division. The fund drive will be held in the afternoon at the year ahead is related directly to the amount of money raised in this campaign. "The \$1 million goal these agencies need to meet their 1964 obligations calls for a higher level of giving from every citizen in San Diego County," said Richard S. Broadway.

## Memorial To Phil Swing Planned At Centre City

Plans are under way to construct a fountain as a memorial to the late Phil Swing near the C Street entrance to the Community Concourse. The advisory board will not choose a design for the memorial but did agree on its central location east of the C Street administration building entrance near the Civic Center. The design and construction of the fountain will be reviewed by Fletcher before a recommendation is made. The advisory board will not choose a design for the memorial but did agree on its central location east of the C Street administration building entrance near the Civic Center. The design and construction of the fountain will be reviewed by Fletcher before a recommendation is made.

## MELTING POT 6 Battle For 5th District Council Seat

This is the third in a series of articles analyzing the City Council and Board of Education campaigns in next Tuesday's municipal primary election. Related stories on 0-14, 0-16 and on editorial on 0-2.

## Concourse Plaza Plans Are Outlined

Old Map, Image Of Cabrillo Ship Feature Design  
A 16th century map of San Diego and a reproduction of the ship which brought Cabrillo's explorers here in 1542 will be the focal point of the Community Concourse interior plaza. The Centre City Advisory Board yesterday gave tentative approval to the \$150,000 plaza which is the last major segment of the \$16 million Concourse to be designed. When the Concourse is completed early in 1964, the plaza will serve as an interior court for the four major buildings.

Architect Selden Kennedy said the map and image of Cabrillo's galleon would be placed near the center of the plaza and set in the Venetian terrazzo stones which will cover the open area. The four points of the compass will be marked and the area set off by trees in movable tubs, he said. The tree would complement ground landscaping on the interior sides of all buildings.

Plans call for the plaza to open into the C Street section of the Concourse near the intersection of the building entrance. It would extend north, flanked on the west by the convention auditorium, the east by the civic theater and on the north by the exhibit hall-parade garage building. Kennedy said the design concept had been reviewed by city planners who had expressed some concern about blocking off some plaza entrances when the Concourse was not in use.

## NEW IS A BUSY TIME New Teacher Realizes Dream

Lunchon isolates teachers, page a-20, editorial, page b-2.  
Mrs. Jacky Lynn Gauthier is a happy, busy woman with red hair and freckles who always wanted to be a teacher and starting next Monday she will be just that. But right now she and 4,200 other men and women hired to staff the city's public schools probably are not in the next 10 months just getting ready for the youngsters. What do teachers do between the time they report for duty and the start of classes? Plenty, most of them will reply, and Mrs. Gauthier is no exception. Last Thursday she and 505 new teachers showed up to begin assignments in the city's schools. Mrs. Gauthier is one of six assigned to Perry Elementary School, 6195 Ranger Road. In two fast days Thursday and Friday Mrs. Gauthier met everyone at Perry from custodians and secretaries to Irvin L. McClure, starting his second year as Perry's principal. She toured the school and began what will be a long acquaintance with the 700 classroom. She was intro-

## Army Engineers Map Cliff Erosion Curb



A stack of textbooks and guides got a going over by Mrs. Jacky Lynn Gauthier and Irvin L. McClure, Perry Elementary School principal. Mrs. Gauthier will teach a combined fifth-sixth grade.

## City To Consider \$1.8 Million Plan

By MICHAEL O'CONNOR  
The Army Corps of Engineers has made a tentative recommendation for a \$1.8 million federal state and city project to control beach erosion at Sunset Cliffs and Bird Rock. William Herron, chief of the Rivers and Harbors Planning Section, said the recommendation has been forwarded to district officers in San Francisco for review. In a telephone interview from his Los Angeles office, Herron said the corps recommended three basic ways to control the erosion. One phase calls for shoring up eroded Bird Rock slopes with rip rap. INSTALL ROCK RUBBLE In the second phase, rock rubble would be installed at scarlines of Sunset Cliffs with dikes constructed in the ocean near some severely eroded cays. The third phase calls for dredging of material from the mouth of the San Diego River Flood Control Channel for filling a beach area south of Santa Cruz Avenue. Herron said the federal government would pay 50 percent of the project cost, with the city and state sharing the difference. The complete recommendation will be submitted to the city this week, he said. PAR IN FUTURE He said the recommendation would require approval of the San Francisco and Washington, D.C., levels of the corps, hearings by congressional public works committees and authorization of funds by Congress. Under normal circumstances this would take from one to four years, he said. Herron emphasized that the corps' recommendation is not intended to be an ultimate answer to Sunset Cliffs erosion problems. "The area has been plagued for a century by the eroding cliff line with some street and homes jeopardized. The area also is parkmarked with dangerous rocks."

## Another Hot, Clear Day Due, Fire Peril Rises

A hot sun blazed for a clear sky for the second straight day yesterday, sending inland temperatures into the 100s and pushing the hazard to the highest point this year. A hot sun blazed for a clear sky for the second straight day yesterday, sending inland temperatures into the 100s and pushing the hazard to the highest point this year. A hot sun blazed for a clear sky for the second straight day yesterday, sending inland temperatures into the 100s and pushing the hazard to the highest point this year.

## NOW IS A BUSY TIME New Teacher Realizes Dream

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## Holdup? No. Confusion: Ends In Birth

The birth of little Angelo Roberts yesterday afternoon was a signal event. Trouble is, the signal was misinterpreted by a score of persons telling about in Horton Plaza. They thought a holdup was in progress. It all started about 11:30 p.m. when an auto on the Plaza backfired. A fireman, police ran into the Plaza Hotel, 315 Plaza St. Their only aim was to help Mrs. Peggy Roberts, 32, deliver the little lad. But Angelo already had arrived. So police took mother and

## Thief Hooked Fish Bait Really For The Birds

A high-flying thief fell for an anchovy trap yesterday and was captured near the Coronado Islands. The thief is a bird—or fish—warrior—a bird that does its fishing in the air, according to Ernie Waggoner, head keeper at birds at the San Diego Zoo. Pelicans dive for fish, but must throw their catch into the air before gulping it down, Waggoner said. Frigate fish, pelicans and swallows the fish in mid-air, he added. Picture, page a-3. This bird's career of piracy and crime ended yesterday when it swooped down on some anchovy bait wrapped around a fish hook near the sports fishing boat New Fleeterette. A woman at the other end of the line thought she was fishing. Waggoner said the bird caught the bait. "It was like pulling a kite in a big wind," Waggoner said. The bird fought for

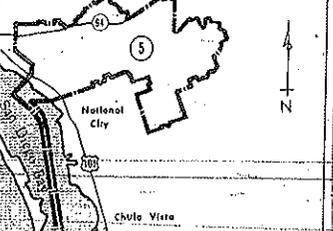
## SMOKING DANGER Baldison urged all person using the national forest to be extremely careful with fire, matches and cigarettes.

Baldison said the rain of week ago had not lessened fire danger. "Smoking is prohibited in the national forest of San Diego. Skies will be clear except for variable high clouds. Some frontal fog patches are expected."

## REPORT AWAYED Boulevard said much of the problem on Sunset Cliffs Boulevard had been caused by drainage from access streets. The boulevard skirts the cliff edge and has been undercut in some portions.

## JOB ISSUE BIGGEST Holdup? No. Confusion: Ends In Birth

The birth of little Angelo Roberts yesterday afternoon was a signal event. Trouble is, the signal was misinterpreted by a score of persons telling about in Horton Plaza. They thought a holdup was in progress. It all started about 11:30 p.m. when an auto on the Plaza backfired. A fireman, police ran into the Plaza Hotel, 315 Plaza St. Their only aim was to help Mrs. Peggy Roberts, 32, deliver the little lad. But Angelo already had arrived. So police took mother and



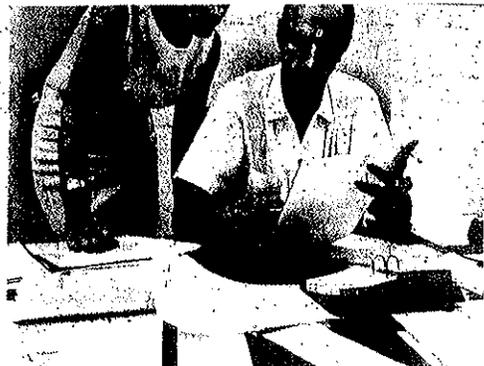
The San Diego Union

MONDAY, SEPT. 14, 1964 SECTION C



Grand Opening  
Festival Starts  
Tomorrow





John Webb, experienced operator of convention halls, is manager of the \$21.5 million Community Concourse. His secretary is Marjory Foster.

signed by Robert Taylor of The San Diego Union's art staff. Photos on the cover as well as the pictures inside of the Concourse buildings were taken by Ted Winfield of The Union's photo department.

on the Convention 260 Parking Buildings...

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**EXPERIENCED HAND**

**John Webb Takes Concourse Reins**

San Diego's big show for 1964—the \$21.5 million Community Concourse—is ready to roll and the city has turned over its reins to an experienced hand in the center ring.

Running the concourse and its multiplicity of activities requires the touch of both a showman and a discerning business executive.

These qualities are part of the mold of John Webb, the concourse manager.

Webb, an experienced operator of convention halls, has been in San Diego two years laying the groundwork for shows, exhibits and concourse activities which will spring to reality in the next few weeks.

All bookings and arrangements for activities will be made through Webb's office in Convention Hall. From the initial booking down to the

location of seats and the purchase of refreshments, Webb is busy.

Policies for the concourse will be set and the facility administered by a seven-member board of directors in the non-profit Community Facilities Corp. Guilford Whitney, a San Diego merchant, is chairman of the board.

Other members are Walter DeBrunner, Harry Callaway, Fred Stalder, Morley Golden, John McCloskey and Philip Gifford.

The chain of command from the board leads to Webb, who is chief of the operating staff. His contract will be under control of the operating board.

His secretary, Marjory Foster, joined Webb soon after his arrival here when he worked in conjunction with the Convention and Tourist Bureau. She assists in booking

(Continued on p. 5, Col. 2)

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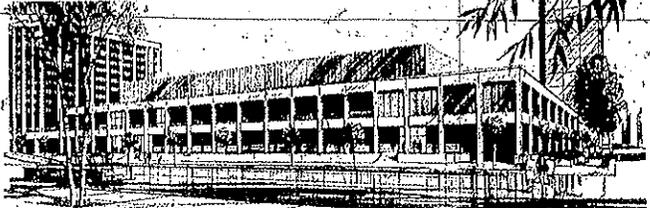
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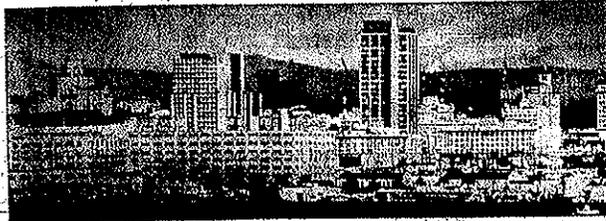
**SAN DIEGO'S PROUDEST MOMENT...**

**"Things Are Looking Up Downtown"**

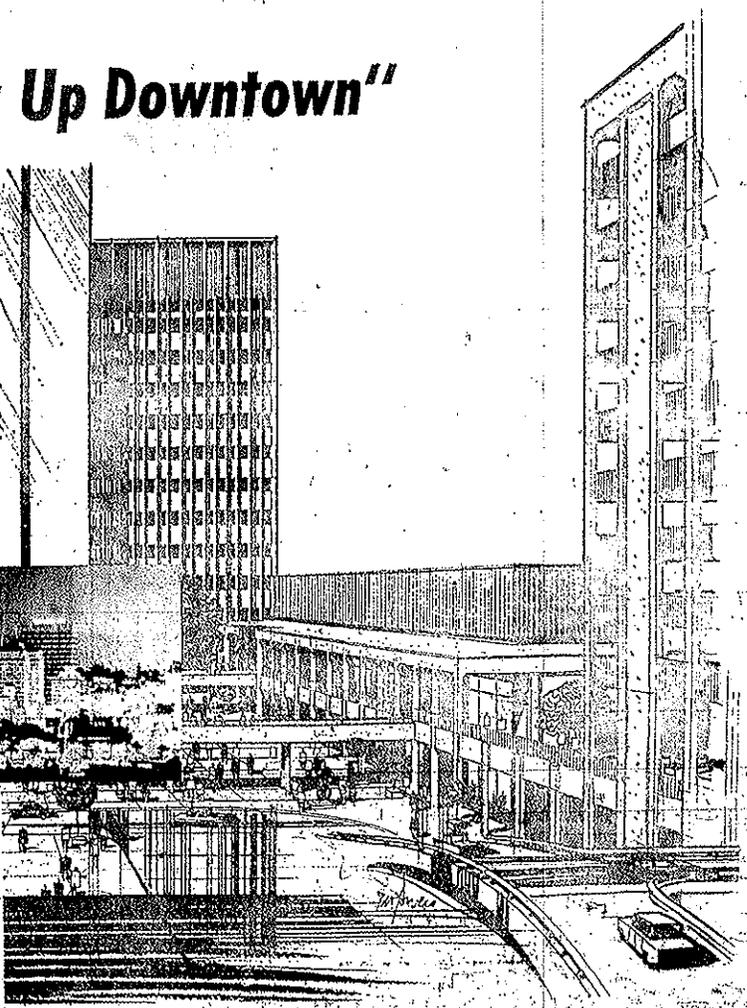
**DOWNTOWN SAN DIEGO'S HORIZONS ARE CHANGING...  
 DRAMATICALLY CHANGING...**

In our new Community Concourse we see, in graphic and moving reality, a convincing and compelling picture of present-day San Diego in terms of new-built greatness.

The 392 members of this Association here pay tribute to those individuals and institutions which have helped build these stalwart and beautiful buildings in the heart of Downtown. Their example of faith is indeed inspiring and presages new horizons which will continue to widen — with quickened tempo.



**SAN DIEGO DOWNTOWN ASSOCIATION**  
 631 HOME TOWER 234-0331

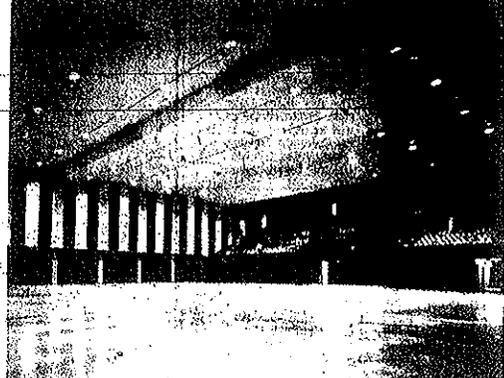


14 S P 14



The Exhibit Hall shown here, has 30,000 square feet of interior space plus 25 square feet on a terrace. It is the first story of an 11-level structure in the Community Concourse.

The other 10 levels constitute the parking garage which will accommodate 1,000 autos. The Exhibit Hall is a San Diego place in the magnitude of display centers.



Convention Center is one of four major buildings in the Community Concourse. The stage at left can be raised 1 foot above floor level or lowered to the basement as an elevation.

In carry exhibit material to the main floor. At right is the balcony seating 1,200, with the press box above. Portable seats will increase seating capacity to 5,000.

**PARKING FOR 1,000 CARS**

**Exhibit Hall Gives City Status As Display Center**

By GEORGE SHROY  
The Exhibit Hall in the Community Concourse gives San Diego in the major league of display centers. John Webb, Concourse manager, said.

The huge contains about 30,000 square feet of interior space and another 25,000 square feet on the terrace over outside.

This is nearly three times as much space as has been used here previously for product exhibits and services—several. Harold De's building had to be added to avoid even that much usable area.

The Exhibit Hall is the first story of an 11-level structure built at a cost of \$15 million. The other 10 levels constitute the Concourse parking garage and will accommodate 1,000 autos.

**DOUBLE GARAGE**

The garage actually is two interlocking 500-car garages, Webb said.

Cars entering on First Avenue will follow a practically spiraling ramp leading to the first, third, fifth, seventh and ninth parking levels.

Autos using the Second Avenue entrance will have a choice of spaces on the second, fourth, sixth, eighth and tenth levels, Webb said.

This double spiral arrangement permits a smoother traffic flow. The entire garage could be completed in 20 months under optimum conditions, engineers have said.

**LANDSCAPING IS MOBILE**

The new style of landscaping called for the Community Concourse complex will be in the interior court and lobby.

Landscaping plans are set for the interior sides of the four major buildings with types in movable tubs to support floor designs of the blocks.

**CONVENTION CENTER**

The 100,000 square foot convention center is centrally located.

The Long Beach Sports Arena has 71,000 square feet. The most offered in Denver is 25,000 in Dallas 48,000 and in Houston 50,000.

**COMMUNITY CONCOURSE GOES "BALDWIN FROM BORGENS"**

The restaurant caterer camp and opened from a circular main area of the terrace area. Attached will be used along with the 30,000 square feet indoor hall for exhibits.

The view skyward from the planting area gives a funnel effect to the circular garage ramps.

**FAVORABLE COMPARISON**

Webb said the amount of space available for displays is favorable in comparison with square footage in Los Angeles, San Francisco, Long Beach, Denver, Dallas and Houston considered competitors with San Diego for national exhibitors' shows.

Los Angeles has 50,000 square feet available in its new Sports Arena. There are 25,000 square feet of floor space in the San Francisco Civic Palace and 40,000 in that city's Civic Auditorium with



Our beautiful new Community Concourse has chosen the excellent Baldwin as the first piano of this great new convention and social center.

Baldwin is First Choice overwhelmingly throughout the nation for Symphonies, University Auditoriums, School Boards, and Artistic First-class of Baldwin's excellence of workmanship, cabinets, touch, brilliant tone and over 100 years of experience in producing the finest instruments.

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DOWNTOWN... SEVENTH & BROADWAY

**Air Conditioning Is Top Attraction**

The new 5,000-seat convention facility in the Community Concourse will enable the Convention and Tourist Bureau to bring "the big ones" to San Diego, the bureau said.

Bureau officials said San Diego will be equipped to accommodate 51.3 per cent of the 29,000 conventions held each year across the nation with the new hall.

The hall is one of four major buildings in the \$23.5 million Concourse. Cost of construction of the hall is \$15 million.

The convention facility with 22,500 square feet of floor space has several special features.

Among them is a combination stage-elevator. The stage rises from the floor about four feet for conventions and shows.

Underneath is an elevator with a capacity of 25 tons which will lift exhibits and other objects from the basement to the level of the floor in the hall. Automobile dealers plan to use this feature to display new models.

The 5,000 seats include 1,200 in the balcony, where there will be about 35 of the 50

The other seats will be portable with about 35 of the 38 rows to be on tiers in an aisle, eight lines.

In addition to the main floor the hall has 22 meeting rooms. The entire hall is air-conditioned including the smaller rooms.

Banquet tables will be available in the hall and inclusion of the tables will leave a seating capacity of 2,800 to 3,000.

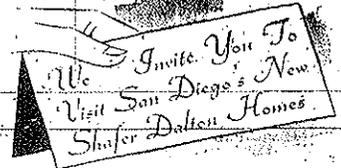
The stage does not have through side-stage space to accommodate shows which the companion theater will feature, but it can handle some concerts in addition to conventions.

Capacity of the rooms which can be grouped for larger meetings, varies from 25 to 600.

Food for banquets will be catered and held in building kitchens in the building.

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**\$77,000 FOUNTAIN**

**Swing Memorial Planned**

A proposed \$77,000 memorial fountain is but a drop in the ocean to what one man accomplished in a lifetime. A committee of 11 men are attempting to get the one million-plus residents of San Diego County to help build that memorial. Even that many people will not match the work of that one man—late Congressman Phil Swing.

In Southern California, Swing and water are synonymous. It was Swing whom many Southern Californians credit with the development of the area.

It was through his efforts in Congress that a Colorado River water was channeled to the Southwest sector of the state. This was accomplished during his tenure in Congress, 1898-99.

What better memorial to regional San Diego's of the man than a Phil Swing Memorial Fountain—a beautiful dancing, sparkling tribute? The committee is working to raise \$77,000 to construct the fountain to be located in the proposed Community Concourse Plaza.

The fountain will be situated on 37th St. (Continued on p. 37, col. 4)

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Morley Golden, chairman of the Community Concourse opening events committee, and William Blackstone, right, discuss construction progress on \$4 million theater shown in background. Blackstone is M. H. Golden Construction Company's field office manager for the Civic Theater.

**3,000-Seat Civic Theater Will Be Area Showpiece**

The city is pointing with pride to the \$21.5 million Community Concourse and holding a reserve of extra praise for the 3,000-seat Civic Theater. Located at the corner of C Street and Third Avenue, the theater is looked upon as the new cultural heart of San Diego. It will begin full-time operation early next year.

Designed to meet a long standing need in San Diego, the theater will have six seating levels and a stage area capable of holding productions of any magnitude.

**EXPERIENCED HAND**

**John Webb Takes Concourse Reins**

(Continued from Page 1-3) arrangements and handles other office duties. Building superintendent for all structures is Charles S. Morrison. A staff of seven maintenance men will be under his command.

Robert F. Kerrigan, well known to local sports fans as a former San Diego Padres baseball pitcher, will be concessions manager. His responsibility covers permanent concession areas and arrangements for special catering.

**JOBS OPEN**  
Interviews are being conducted by Webb to select an office manager and a ticket manager, positions which will be filled out of the concourse staff.

A native of Indiana and graduate of Southern Methodist University in Dallas, Webb has been active in convention hall management since 1951. He was business manager of the Long Beach auditorium and stadium from 1951 to 1957 and manager of the Santa Monica Civic Auditorium from 1957 to 1962.

Webb has earned a reputation for seeking out prospective clients of convention facilities. One of his major business coups was being the Academy Awards presentation ceremonies out of Hollywood to Santa Monica Civic Auditorium when he directed that operation.

However, bookings alone do not complete the job. Webb must insure that the 12-month calendar of dates for the hall is full wherever possible. He must be able to guarantee a room or hall assignment and, then set in motion a series of services required to make the event a success.

**DOUBLE PURPOSE**  
It was designed not only to provide a theater for opera, concerts—and drama—but to supply an auxiliary assembly area for the 5,000-seat convention hall.

Ruocco, Kennedy and Rossier, the architects, and the M. H. Golden Construction Co. builders have checked all details to insure the city obtains a first-rate theater.

It is a structure designed both for the audience and the performer and is one of the most functional buildings in the concourse, even though it occupies only a 150 by 250 foot site.

**CLOUDS FOR SOUND**  
Acoustical features easily noticeable are steel "ribs" which hang by cables from the ceiling to bounce sound from the stage to all areas of the theater.

To preserve the theater's intimacy for productions requiring fewer seats, a screen of visually opaque but acoustically transparent drapery will be lowered to separate the audience from areas not in use.

An addition on the terrace level is the grand salon, a splendidly decorated room which will serve as a gathering place for patrons before or between acts of production.

Performers will find a 39 by 39 foot orchestra pit with

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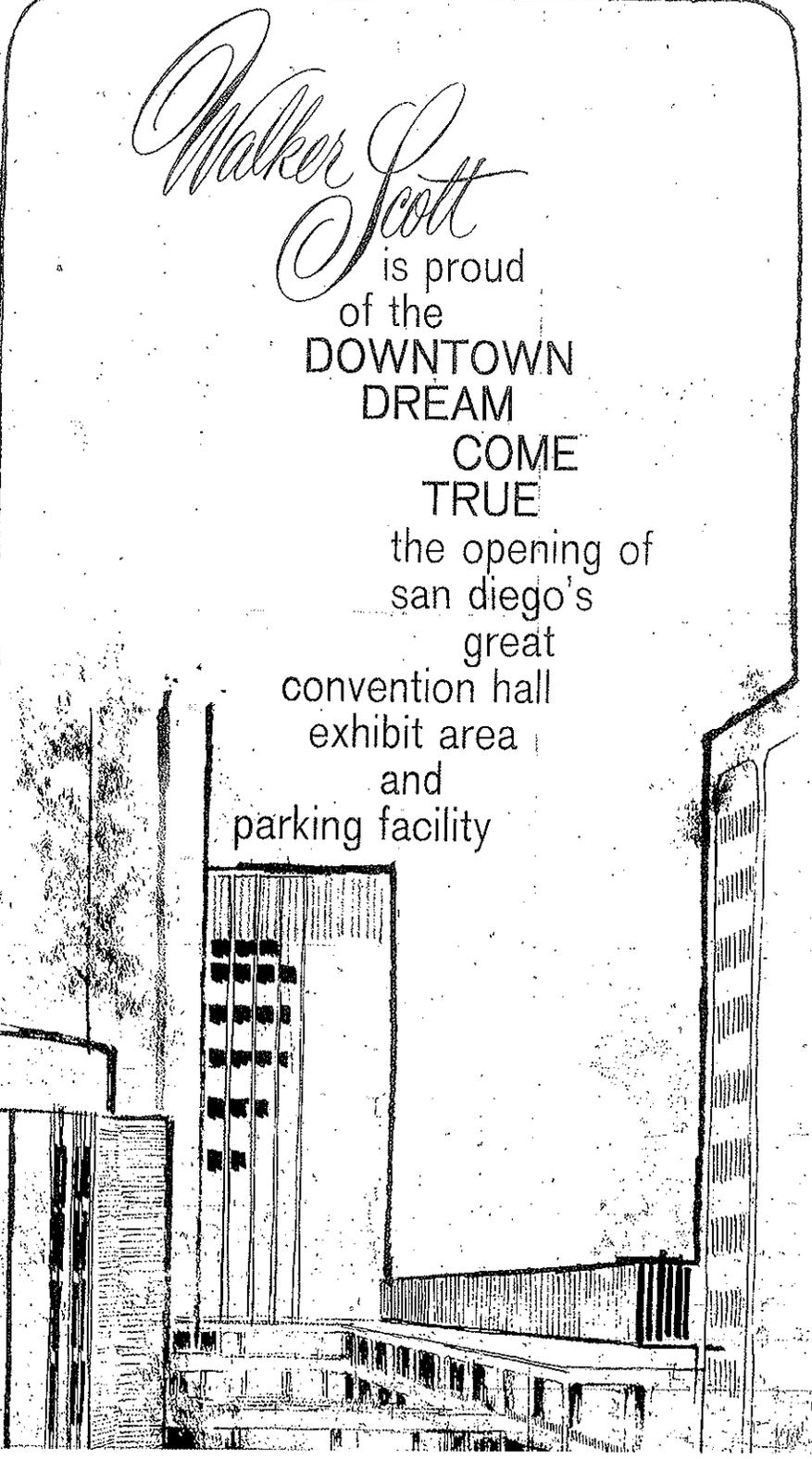
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Walker Scott is proud of the DOWNTOWN DREAM COME TRUE the opening of san diego's great convention hall exhibit area and parking facility

# Community Concourse To Be Hub Of San Diego Activity

(Continued from Page C-2)  
 ests, particularly in new office buildings.  
 Frequently, the city developed the \$21.5 million Community Concourse plan. The starting point for a vast spectrum of improvements, large and small. It also was to fill the need for public facilities when the city was extremely in need.

**PARKING GARAGE**  
 The parking problem was solved by a \$4.1 million, 11-level garage for 1,000 cars, which would be linked with an open exhibit area with 65,000 square feet of usable space.

Then came the convention hall, a two-level, 5,000-seat facility which San Diego's solid tourist and convention business had needed for years. It cost \$3.6 million.  
 Visitors to the Concourse during the opening week will see these units, equipped with a glass facade, which indicate what kind of an investment has been made in recamping the core area.  
 Still following the broad development plan is the 11-level administration building which will be the center of city government activities when opened later this year. The move downtown from Civic Center is the first step.

Since it will be a natural adjunct to the county courthouse and jail, a proposed federal building, the state office building, an ever-increasing number of financial and business offices fanning out in all directions from the 1,000-SEAT THEATER.  
 The last unit, also considered a long-time San Diego need, is the \$4 million civic theater due for completion soon after the administration building is opened. It will have 2,000 seats on six levels.  
 Overall, the city has invested \$4.1 million in land and

\$14 million in engineering and architectural fees to make the complete Concourse a reality.  
 Not all of the buildings could have been finished if had not been for the type of citizen interest and cooperation that has been part of the project since its inception.  
 When it was learned in design stages that the theater and other facilities would be inadequate under early cost estimates, a massive private fundraising campaign was undertaken.  
 Within four months, the city obtained \$1.5 million in necessary additional funds from

private contributors. The 150 donors contributed in amounts ranging from \$3 to \$200,000.  
 San Diego's Concourse is unique in the respect that it resembles many large-scale urban renewal programs of other cities but did not require a penny of federal assistance to be implemented.  
 City capital improvement funds, the loan arrangement with retirement monies, and the private subscriptions made the project possible.  
 And this is only the start.  
 The city has a long range program of capital improvements which will be added in the perimeter around the concourse. Other office buildings are being considered. Interest has been expressed in a new downtown hotel.

## 200 Firms, Groups To Show Products

Participating in the community concourse exhibit are 200 firms and groups. The exhibit, which will be open from Sept. 20 to Oct. 2, is the largest ever held in San Diego. The exhibit hall can accommodate many more displays than former exhibit areas, most of which are in Balboa Park.  
 The exhibit hall can accommodate many more displays than former exhibit areas, most of which are in Balboa Park.

## Products Go On Display

(Continued from Page C-2)  
 from Feb. 25 through March 1. A teen fair will be held March 5 through 8, and the U.S. Chamber of Commerce will use the hall March 11-12. The American Legion has booked June 23 through 27.  
 Patrick J. O'Toole, president of National Shows, Inc., said the first shows will be the first major ones in San Diego of their kind. The exhibit hall can accommodate many more displays than former exhibit areas, most of which are in Balboa Park.

## Special Landscaping Planned For Garage

The central garage area is to be landscaped with potted plants which can be replaced in season by blooms in full color.

## Boldrick's fine shoes

Look... for the lustre of Vitality

**PATENTS**

You'll see it come into Fall dressed in a downpour of stable colors. And Vitality lustre patents come in clicking on high, mid or lowered away heels. Silhouettes are simple or snatched with detail. Here, two patents ready to glow through Fall.



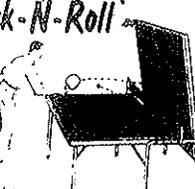
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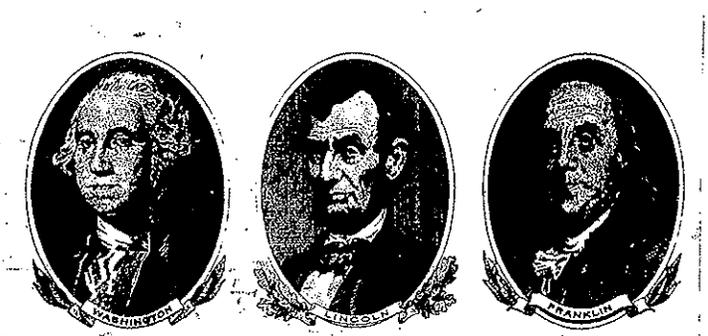
All San Diego Can Be Justly Proud of the Magnificent Centre City Community Concourse

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These are some of your friends and neighbors

# Concourse Shows Best Talents Of Many Designers

No architect in San Diego has had more first-hand experience in civic building construction than Sam W. Hamill, supervising architect for the Community Concourse.

Hamill worked on plans for the present Civic Center which was completed in 1937. He also took a nationwide tour to gather ideas for the downtown Concourse and the downtown building which will be open in December.

Hamill's direct ties with the Concourse date back to early Centre City discussions in San Diego. In 1924, a San Diegoans, Inc., financed study which broke the ice on a new interest in downtown and eventually the Concourse concept.

**COOPERATIVE WORK**

Hamill was employed by the city to make recommendations on administration building needs and visited projects in Mexico City, Miami, Detroit, Toronto, Vancouver, Seattle and New York before attempting the first schematic drawings.

Although Hamill has been instrumental in tying the architectural strings for the mammoth project, the overall work has been truly a cooperative venture of several building designers.

The City Council agreed early in the discussion phase that the principal architect would be limited to those in San Diego. In addition to the principal designers dozens of others have been called in for specialized advice.

These are the consultants who did the job on each structure:

**Barank & Newman** of Los Angeles and New York, acoustical consultant; **Hilburg & Turpin**, illumination consultant; and **Vallin**, cost estimator.

**Administration Building**—Hatch, Wolf and Fifield, architects; **John Ruskin** of San Diego, structural engineer; **C. D. Wahl & Associates** of San Diego and Los Angeles, mechanical and electrical engineer; **Dr. R. W. Young** of San Diego, acoustical consultant; **Jack H. Lewis** of San Diego, specifications consultant; and **Vallin**, cost estimator.

It was Hamill's responsibility to coordinate the work of all these technical consultants and to insure that their efforts fitted the entire Concourse development plan.



SAM W. HAMILL

## Young Builds 2 Structures

Two highly utilitarian parts of the new Community Concourse have been built by the F. E. Young Construction Co., a firm that has been growing with San Diego since 1925.

The Convention Hall and Parking Garage take their place in the San Diego scheme with a variety of structures erected by this same firm, which range from the County Courthouse and the Public Library to the Atlas mobile test stand in Sycamore Canyon; from the new Mercy Hospital under construction and the Madison and Morse high schools to the Miramar Filtration Plant and the Navy Electronics Laboratory.

The firm, located at 2141 Main St., was founded 40 years ago by the late F. E. Young Sr.

## Yost Facilities Ready

The three-level Convention Hall in the Community Concourse covers more than a full city block, and contains enough floor space to attract the largest of conventions.

# Public Will Help Build Fountain

(Continued from Page C-5)

located in the northwest corner of Third Avenue and Street.

George W. Sears, a committee member is accepting monetary gifts of any amount which may be mailed to Post Office Box 1311, San Diego 92112.

The committee is working to complete the memorial in January, when the nearby Civic Theater is scheduled to open.

The fountain will not be the first memorial by which Young will be remembered. There is a concrete structure of the intersection of California, Arizona and Nevada states for which he is responsible—(House) Dam.

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Major Curran has predicted a significant increase in downtown business investment following completion of the \$21.5 million Community Concourse.

Curran said new spending on restaurants and other commercial facilities is expected to accommodate visitors using the Convention Hall and other concourse attractions.

Spending Mark Set—An all-time record of \$238.5 million was spent here in 1953 by out-of-county visitors.

**SAN DIEGO'S WORLD CHAMPION CHARGERS**

**SALUTE THE NATION'S ALL AMERICAN CITY**

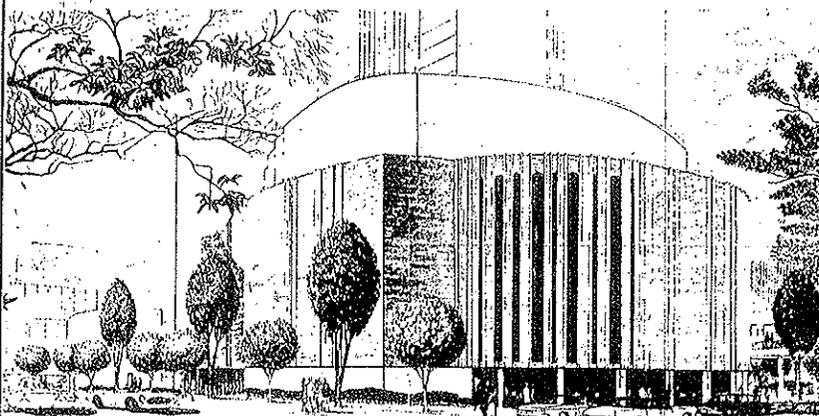
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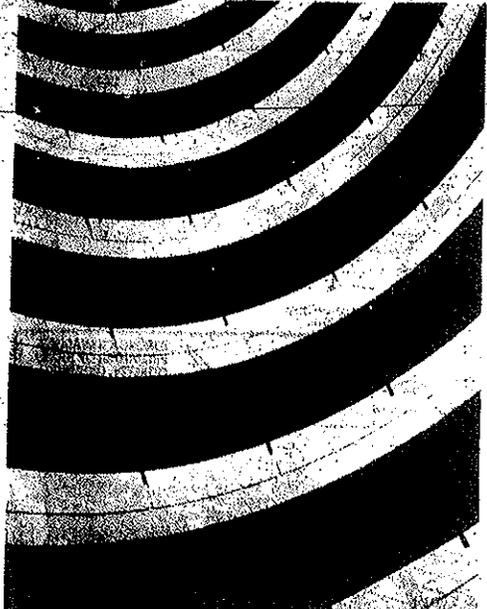
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 For Its Presentation To The People Of  
 San Diego On January 12, 1965'



M. H. GOLDEN CONSTRUCTION COMPANY



Inside ramps of Community Concourse parking garage form a spiral of beauty and convenience. The 10 levels will accommodate 1,000 cars. **20C AN HOUR, \$1 A DAY**

### 11-Story, 1,000-Car Garage Has Built-In, 2 In 1 Feature

The multitude of attractions and services in the \$21.5 million Community Concourse will make an easy stroll away for San Diegans thanks to the 11-level parking garage.

Located between First and Second avenues off A Street the 1,000-car garage was built by the F. E. Young Construction Co. under a \$4.1 million contract which included the adjacent 40,000-square-foot exhibit hall.

Pre-opening visitors to the Concourse frequently have observed that the garage is one of the most beautifully designed buildings in the complex. It has been engineered primarily to insure a smooth flow of traffic in and out of the Concourse area.

James Casey, concourse project engineer, describes the facility as "two parking garages in one."

Cars entering on First Avenue will follow a spiraling ramp leading to the first, third, fifth, seventh and ninth levels. Cars using the Second

avenue side will be parked on the second, fourth, sixth, eighth and tenth levels.

The exhibit hall is the first story of the structure and is served by four passenger elevators from all parking levels. Visitors using the garage also will be able to be within easy walking distance of the outdoor terrace and plaza, an area which gives the Concourse a 20,000-square-foot bonus in display space.

San Diego Center City Parking Corp., headed by Evan Jones, will operate the garage. There is a seal of fees,

approved by the City Council, designed to make the structure a self-supporting operation. There will be no free parking.

Under terms of a city contract, the minimum hours of operation will be 7 a.m. to 6 p.m. with parking available any time exhibit, convention or theater facilities are in operation.

Rates are 20 cents hour or fractions of an hour with a \$1 daily maximum. The \$1 pays for parking from 7 a.m. to 6 p.m. when an automobile is left in the garage.

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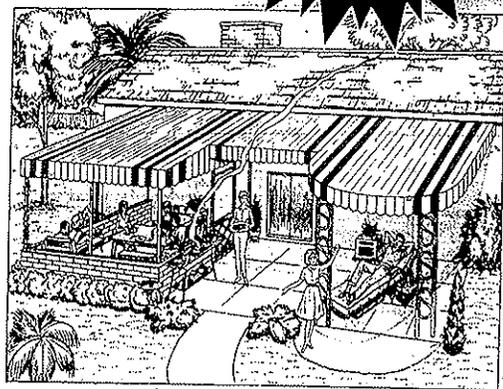
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We are happy to be one of the made-in-San Diego-elan. Visit our exhibit and pick up a brochure - The Alan John Story. Our best wishes to the civic leaders who made the Concourse a reality.

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There is nothing like ALAN JOHN on the entire West Coast. Here you can choose from 4,000 suits, 2,500 sport coats and 1,500 pairs of slacks... and you skip the middlemen.

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14 SEP 14



## Five Organizations

### Join 'Pop Pourri'

(Continued from Page 1-B)  
... at 3:30 p.m. Sunday. The band will perform under the direction of Chief Warrant Officer A. M. Glesak.

A three-hour folk dance program with performers in the costumes of 22 different nations will be presented at 1 p.m. Sunday. Hosts for the program will be the Cabrillo Folk Dancers, Chula Vista Folk Dancers, the San Diego Folk Dancers and La Jolla University Dancers. German, Swiss, Ukrainian and Swedish folk dances will be featured. Concluding the musical events of the week's opening will be a one-hour concert by the Marine Corps Recruit De-

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We are proud to have played a major role in the construction of the San Diego Centre City Community Concourse.

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**CONGRATULATIONS**  
To The Centre City COMMUNITY CONCOURSE  
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**SAN DIEGO CHAMBER OF COMMERCE**  
SAN DIEGO, CALIFORNIA 92101  
TELEPHONE 232-0174

September 14, 1964

To All Residents of San Diego County:

**B.I.G. (Build Industrial Growth)** is proud of San Diego County's burgeoning economy, its products and services. We intend to continue progression of business and industry in both national and area wide programs. Included in our local efforts during 1964-65 will be (1) assistance to all existing industries concerning expansion programs, and (2) personal contacts by representatives of the B.I.G. Committee with each of approximately 800 industrial firms of San Diego County.

Our national advertising program will be stepped up during the coming year. We will continue our close contacts with the management of numerous small and large plants throughout the nation.

If we urge you to visit the **Try San Diego First** booth at the San Diego Products and Economy Exhibit, September 15-20. Learn what has been done through this program in bolster San Diego County's economy.

Try San Diego first develops business and new industry. The Try San Diego First Committee conservatively estimates that in the past 12 months the program has resulted in 165 more retail establishments, 3,075 more workers employed, 5,725 more passenger cars registered, \$12,595,000 more bank deposits, \$3,891,000 additional households and 19,740 more residents in the County.

These programs are working for you. Continue with your support!

**William R. Quirk**  
William R. Quirk, President

# Nielsen

**SAN DIEGO'S CENTRE CITY ADMINISTRATION BUILDING**  
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## FIRST FEDERAL SALUTES THE NEW CENTRE CITY CONCOURSE

**PIONEER FINANCIAL INSTITUTION IN SAN DIEGO'S CENTRE CITY AREA**

First Federal Savings and Loan Association of San Diego has occupied the same Northwest corner of Fourth Avenue and "B" Streets since organization in 1934. First Federal just a job and a skip from the Concourse, can certainly claim to be the pioneer financial institution, and a cornerstone of the new Financial Centre adjacent to the Centre City area. First Federal, long an important factor in San Diego's growth and prosperity program, considers the Centre City Development the most outstanding "go ahead" achievement in recent years.

A Quality Financial Institution with Assets over 100 Million Dollars

Harold E. STARKER, Chairman of the Board, J. BLANK A. MORTON, President

**FIRST FEDERAL SAVINGS AND LOAN ASSOCIATION OF SAN DIEGO**  
DOWNTOWN, 3127 JEFFERSON STREET



An example of magnificent architectural designs represented in the Community Concourse is San Diego's new City Hall. This picture of light and shadow is the skin or tunic on the south side of the structure to be completed later this year.

## 14-STORY CITY HALL City Government Gets More Space

City councilmen and hundreds of municipal employees who carry out their duties will be housed in the new home of the City of San Diego.

The move of city offices from Civic Center to the 14-story administration building scheduled to begin in mid-November and be completed in early December.

The new building has 156,153 square feet of office space compared to 99,738 in Civic Center, the home of municipal government since 1939.

After the move, the Pacific Highway Civic Center will be turned over to the county, now a joint tenant.

In addition to 445 employees from Civic Center, another 175 will work in the concourse structure after moves from three branch building inspection offices which are to be moved from administrative buildings designed by architect Paul F. Williams with movable panels installed to increase space.

Visitors to city hall may be able to do most of their business on the first floor where such frequently used offices as building inspection, civil service information, business services and parking meter cashier are to be located.

The mayor and the eight council offices will take over the 11th floor. The council is to meet here by two members next fall.

Another feature will be individual telephone numbers for each office instead of the standard city number. This eliminates the necessity for a switchboard.

## Dr. Urey To Discuss Ranger 7 Sunday

Nuclear chemist Dr. Harold C. Urey, a University of California, San Diego professor, will speak on space exploration at an 8 p.m. lecture Sunday, Sept. 20 in the new Convention Hall.

Urey, who was awarded the Nobel Prize in 1934 for his discovery of heavy hydrogen, will speak on "Some interesting Features of the Space Program."

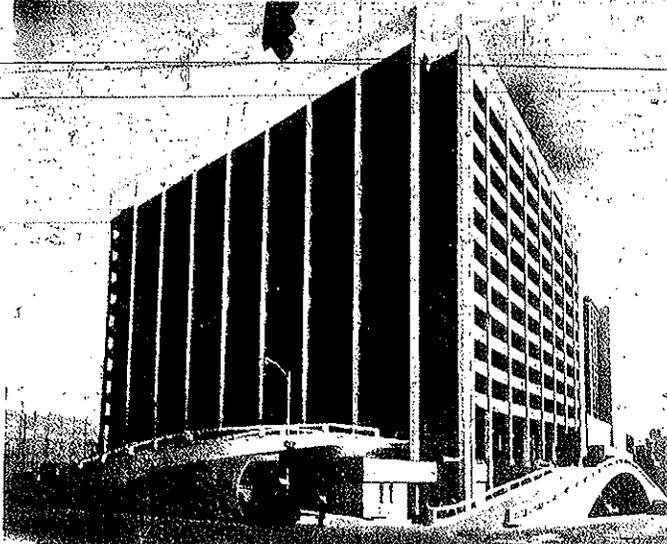
A recognized authority on the moon, Urey will lecture on the Ranger 7 moonshot which reached the moon July 31. He will also show

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One of the key buildings in the \$21.5 million Community Concourse is the parking garage, which occupies 10 levels above the ground floor Exhibit Hall. This view of the northwest corner, at A Street-First Avenue side, shows graceful ramps leading to and from the 1,000 parking spaces.

# Community Concourse To Be Hub Of San Diego

By MICHAEL O'CONNOR

The \$21.5 million Community Concourse could be described as a giant dose of adrenaline for downtown San Diego.

When fully opened early next year it will be the center of government, trade and entertainment activities for the greater San Diego area.

It also will provide the spark for an expensive, long range program for full development of the 24-block Centre City area extending in a broad fan downtown from the Interstate 5 crosswalk

Early ideas on the Concourse plans for its construction, and preparation for the future projects it will stimulate have all come about through a partnership of the city and citizens.

Large cities throughout the United States have at some time or another since World War II discovered that the exodus to the suburbs creates a series of serious problems in the important downtown business areas. Predominant among the problems are traffic congestion, depreciation of property values, and a hazardous pattern of land development.

San Diego citizens developed the entire Centre City concept out of concern for the future of the downtown area. The concern was not unique to San Diego.

The citizens' group retained Western Real Estate Research Corp. to produce a detailed and comprehensive study to determine the logical function of the central area in a modern, and growing city.

A total of \$35,000 was subscribed by San Diegans, Inc., to pay for the study.

In the spring of 1960, the report was presented with the conclusion that downtown was the best location for administrative, financial, cultural, governmental and general and specialized retailing activities.

The study was presented to the City Council and preliminary work began on the city's role in the receding plan. A program of land acquisition was started for major municipal buildings using money borrowed from the city employees retirement fund. The city's interest plus a general surge of new feeling for the downtown area sparked investment by private investors.

San Diegans, Inc., a group of 50 business leaders, sensed this problem when it was organized in 1959. At that time, downtown problems were becoming apparent but there was no catalyst to correct them.

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# County's Products Go On Parade

From candy and flowers to missiles and airplanes, San Diego products will be displayed during dedication week in the Community Concourse exhibit hall.

John Webb, concourse manager said the San Diego County products exhibit scheduled tomorrow through Sept. 20 was oversubscribed several weeks ahead of time.

More than 225 exhibitors using nearly 350 booths in doors and on the terrace will display a myriad of products grown, manufactured, harvested or assembled in the county.

Webb said he expects 150,000 to 250,000 visitors for the show, the first of many booked in the \$3.5 million exhibit hall, one of four major structures in the Community Concourse between First and Third avenues and A and C streets.

Among the products they will see will be surfboards, dairy and poultry, newspapers, wood novelties, art, farm equipment, electric motors, sausage, jewelry and honey.

After the San Diego products exhibit, manufacturers from across the nation will have an opportunity to show their wares in in the new building.

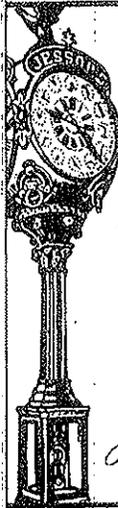
From Oct. 5 to 13 the Home Furnishings Institute will conduct an exhibit including interior decorations in the hall.

Next in line is an outdoor sports show, including hunting, fishing and camping equipment Oct. 27 through Nov. 9. This also is being prepared by the Home Furnishings Institute.

National Shows, Inc., which is producing the shows, has received more than 450 inquiries about the exhibits.

Local automobile dealers have booked the exhibit hall Nov. 13 through 27 for showing new model year cars.

In addition, a dog show is slated for next Feb. 20 and 21, and a custom car display showing new model year cars. (Continued on C-6, Col. 2)



Jessop's, a family of fine jewelers for over 100 years... and part of San Diego's progress for 82, takes pride in saluting Centre City on the occasion of its opening.

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## FESTIVAL HIGHLIGHTS

Here is a time table of events for the grand opening festival.

### TOMORROW

5 p.m.—Grand opening ceremony at Convention Hall's Second Avenue and B Street entrance.

6 p.m.—Free guided tours of Community Concourse begin.

6:30 p.m.—San Diego Convention and Tourist Bureau dinner, Convention Hall. Admission by ticket.

### WEDNESDAY

2 to 10 p.m.—San Diego Products and Economy Exhibit, at Exhibit Hall. Admission free.

6:30 p.m.—"Pop Pour!" of entertainment Convention Hall. Admission by ticket.

### THURSDAY

10 to 2 p.m.—San Diego Products and Economy Exhibit.

6:30 p.m.—San Diego Civic Arts for Youth program, Convention Hall. Admission free.

### FRIDAY

2 to 10 p.m.—San Diego Products and Economy Exhibit.

8 p.m.—Armed Forces Dance for service personnel in uniform, Convention Hall. Admission free.

### SATURDAY

2 to 10 p.m.—San Diego Products and Economy Exhibit.

6 p.m.—Square dance roundup by San Diego County Square Dancers, Convention Hall. Free tickets available at door.

### SUNDAY

1 to 4 p.m.—Folk dances of 27 nations, Convention Hall. Admission Free.

2 to 10 p.m.—San Diego Products and Economy Exhibit.

6:30 p.m.—Concert by Marine Corps Recruit Depot Band, Convention Hall. Admission free.

8 p.m.—Nobel Prize winner, Dr. Harold C. Urey, Professor of chemistry, University of California at San Diego, speaks on space research, Convention Hall. Admission free.

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Available at Your Favorite Store

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Gas Turbine Energy System supplies heat and electricity  
Gas turbines provide total energy at high school  
Solar's standard 1100 hp gas turbine compressor  
Liquid petroleum pump driven by a Solar turbine

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SKMR-1 Hydrokimmer—Four Saturn marine gas turbines power the SKMR-1 Hydrokimmer, which can attain 70 knots over land or water. Built by Textron's Bell Aircraft Systems Co., the 56 ft, aluminum-hulled Hydrokimmer is the largest air cushion vehicle ever built in the U.S.

Gas Turbine Energy Systems produce electricity, heat—Two Solar 300 hp T-350 Gas Turbine Energy System packages are used to supply electricity and heat at Standard Pipe Protection's Houston pipe coal-

Solar 1100 hp gas turbine compressor set—Solar gas turbine compressor sets are serving gas gathering, transmission and distribution applications throughout the U.S. The compact, heavy duty unit includes a Saturn gas turbine and matching centrifugal gas compressor. A typical gas compressor installation in this set used by California Pipeline Co. at Bethune, S. C.

Turbine-powered liquid petroleum pump—Mid-America Pipeline Co. uses 2½ Solar Saturn gas turbine-



Watching balloons rise at dedication of Community Concourse are, from left, former Mayor and Mrs. Charles C. Dail and Mayor Frank Curran. At right is Morley H. Golden, master of ceremonies.

# Russians ( Monster V

## Reapportioning Snags Aid Bill, Adjournment

### Senators Reject Tough, Moderate Stands On Issue

WASHINGTON -- (AP) -- The Senate headed into a deepening stalemate over reapportionment of state legislatures yesterday, rejecting both a tough and a moderate stand on the issue and leaving the fate of the \$3.3-billion foreign aid bill in doubt.

## CALIFORNIAN, PEDAL TO N.Y.

NEW YORK (UPI)—A 45-year-old man and his three young sons arrived in New York yesterday ending a 57-day cycle trip.

Adrian W. Cannon, a bookstore owner, said he and his sons arrived on their own bikes, had left California July 20.

Cannon said he and his sons arrived in New York after a 57-day trip which took them to 11 states. He said they had 21 flat tires. "I lost 11 pounds, all of the fat and we found that this country has a hospitable heart from coast to coast. We wanted to see the fair, but to show the world in some small way that Americans aren't so soft," he said. They carried only a changeable mattress and blankets.

Even more obscure were the prospects for final adjournment of Congress, which hinges on clearing the massive aid program.

### COMPROMISE REJECTED

In two quick votes the senators turned down: A compromise "sense of Congress" substitute for a rider on the foreign aid bill proposed chiefly by Sen. Everett M. Dirksen of Illinois, the Republican leader. The vote was 42 to 40.

A much tougher substitute by Sen. Strom Thurmond, D-S.C., that is identical with a separate House-passed bill that would strip federal courts of jurisdiction over reapportioning state legislatures. It was defeated 56 to 21.

The twin defeats cleared the way for Senate liberals to pursue a filibuster against Dirksen's rider, which is designed to delay reapportionment, ordered by the Supreme Court.

Only one test vote has been held so far on Dirksen's rider—announced he was removing a 49 to 38 decision against five prominent commanders.

### 5 RELIEVED

Air Commodore Nguyen Cao Ky, 34, the officer who almost singlehandedly saved Khanh's government, told reporters the five principal officers of the revolt have been relieved of their commands and faced punitive action. They are:

1. Maj. Gen. Duong Duc, former commander of the nation's largest army corps and nominal leader of the revolt.

2. Brig. Gen. Lam Van Phat, Duc's partner in the revolt and apparently the chief architect. He has been hiding since Sunday.

3. Brig. Gen. Duong Ngoc Lam, who previously held the three positions of Saigon Mayor, capital military district commander and commander of the government militia.

4. Col. Lum Quang Tho, the armor commander whose tanks and armored cars threatened the premier's office and headquarters of the joint general staff.

5. Col. Juynh Van Ton, the highly ambitious commander of the 7th Division, whose troops occupied the capital Sunday.

## Thurmond S To Goldwater

### Former States Rights Car Schedules Talk To State

WASHINGTON (UPI)—Sen. Strom Thurmond of South Carolina has decided to bolt the Democratic party and become a Goldwater Republican, it was disclosed last night.

Thurmond's office said he would go on statewide television in South Carolina tonight to make a statement on the presidential race. Aides said he refused to reveal details until he could relay his feelings to the people of the state.

Other sources said the senator was switching party allegiance. This was revealed after the Columbia Broadcasting System said the senator, 61, was bolting the Democrats.

### COMPARED VIEWS

Since rising to national prominence as governor, senator and 1948 States Rights presidential nominee, Thurmond never has supported the Democratic national ticket.

Thurmond said in 1961 that the southern people "believe in what Sen. Goldwater has advocated and is advocating." He also said his own views were

## Goldwater R High Court A

By LYN NOFZIGER

The San Diego Union's National Political Editor, ST. PETERSBURG, Fla., to constitute Sen. Barry Goldwater last night renewed his attack on the new rule of the United States Supreme Court, saying that "something must be done, and done immediately, to swing away from Goldwater

## Health Plan Suffers Setback In House Move

The San Diego Union's Washington Bureau Costley News Service

WASHINGTON—The administration's program of health care for the aged suffered a severe setback yesterday when House backers abandoned plans for a test vote on the controversial issue.

Without a fight, supporters of the Medicare plan let the Senate-approved legislation go to a Senate-House conference.

Rep. Cecil King, D-Calif., key Medicare backer, announced he was dropping his plan to submit to a House vote a motion to instruct House conference members to back health care for the aged under Social Security.

### RETURNED TO HOUSE

The Medicare provision was attached by the Senate to a House bill increasing Social Security taxes and benefits. The bill had been returned to the House to settle differences.

Of the 12 men who will decide the fate of the measure in the joint conference, eight are known opponents of Medicare and only four are avowed supporters. Their final recommendations must be acted on by both the Senate and the House.

King said in an interview that he shelved his motion because many House members favoring Medicare were reluctant to vote for a motion that would tie the hands of their conferees.

### TELLS HOPES

"Many of the members were

## Gala Ceremonies Open Concourse

### Dedication Rites, Dinner Inaugurate \$21.5 Million Municipal Complex

By JOE BROOKS

San Diego yesterday unwrapped a sparkling new municipal gem which is expected to prove one of its brightest civic assets.

With pomp and pageantry and speeches and songs, the city dedicated its Community Concourse—a \$21.5 million complex in the center of the city which will serve as a center for conventions, cultural events and for the city government.

An estimated 3,000 San Diegans gathered outside the massive complex in what soon will become the Concourse plaza to hear Mayor Frank Curran say the event "culminates a lot of hard work, determination, concentration and even consternation."

They also heard Guilford H. Whitney, chairman of the Community Concourse advisory board, describe the public facility as "the most conveniently located on the Pacific Coast and one of the most beautiful."

At the conclusion of the hour-long opening ceremony, Curran and former Mayor Charles C.

San Diego Union Staff Photo

Mrs. Charles C. Dail and Mayor Frank Curran. At right is Morley H. Golden, master of ceremonies.

Dail pulled two long white cords to release a mass of balloons which soared into the blue sky, signalling the opening of the Convention Hall, Exhibit Hall and Parking Garage.

Then the doors were thrown open and curious and proud San Diegans surged into the gleaming new buildings.

They browsed through the San Diego Products and Economy Exhibit, the first show to be staged in the Exhibit Hall, which occupies the main floor and the terrace level of the parking garage.

Shortly afterward, 1,000 members and guests of the San Diego Convention and Tourist Bureau inaugurated the Convention Hall by attending the bureau's annual dinner.

Morley H. Golden, chairman of the mayor's committee for

(Continued on a-2, Col. 2)

## S. Viet Nam's Chief Starts To Purge Plotters

SAIGON -- (UPI) -- Premier Nguyen Khanh began purging political plotters from his military high command yesterday. The Communist Viet Cong moved to take advantage of the turbulent political situation and called for a nationwide military offensive, anti-government demonstrations, and strikes to ensure a final Red victory.

Khanh, apparently acting at the prompting of eight young officers who helped save him from Sunday's abortive coup, announced he was removing five prominent commanders.

### 5 RELIEVED

Air Commodore Nguyen Cao Ky, 34, the officer who almost singlehandedly saved Khanh's government, told reporters the five principal officers of the revolt have been relieved of their commands and faced punitive action. They are:

1. Maj. Gen. Duong Duc, former commander of the nation's largest army corps and nominal leader of the revolt.

2. Brig. Gen. Lam Van Phat, Duc's partner in the revolt and apparently the chief architect. He has been hiding since Sunday.

3. Brig. Gen. Duong Ngoc Lam, who previously held the three positions of Saigon Mayor, capital military district commander and commander of the government militia.

4. Col. Lum Quang Tho, the armor commander whose tanks and armored cars threatened the premier's office and headquarters of the joint general staff.

5. Col. Juynh Van Ton, the highly ambitious commander of the 7th Division, whose troops occupied the capital Sunday.

## Johnson Stresses Medicare Support

By EDWIN MARTIN

Costley News Service

MIAMI BEACH, Fla.—President Johnson departed from his prepared speech to a labor union yesterday to seek support for a health care plan financed

...ry of supplies from Turkey "through normal channels."

2 Removing all Greek Cypriot armed posts, if the Turkish Cypriots do the same.

3 Providing financial aid for resettlement and protection of Turkish Cypriots who have been forced to abandon their homes.

4 Granting a general amnesty "so that any Turkish rebel who may be under criminal charges for offenses committed during the rebellion may be relieved of any fear of arrest and punishment."

5. Accepting U.N. suggestions for peace" provided such measures do not affect a political solution of the problem."

A Turkish Cypriot spokesman said there would be no comment on the proposals, but earlier he reacted with skepticism to reports that the blockade would be lifted.

"Time alone will tell whether the announcement is a mere maneuver on the eve of the Security Council meeting or whether it will be observed and carried out in practice," he said.

**SUPPLIES LEFT**

Finnish U.N. troops conveyed food donated by Makarios to the surrounded Turkish Cypriot village of Kokkita, but villagers shouted, "Take it away. We don't want it."

Although the villagers tried to stop the convoy from unloading, the troops left the supplies at the village and returned to Nicosia.

Turkey, meanwhile, announced that emergency rations for Kokkita will arrive at the eastern Cyprus port of Famagusta tomorrow. Deputy Premier Kemal Satir said the supplies will be taken by truck from Famagusta to Kokkita in north-west Cyprus under U.N. protection.

**Russia Claims Moscow Aim**

(Continued from Page a-1)

...ed, we will crush the enemy.

In a direct reference to Mao Tse-tung, he said, "Some countries are fanning hostility against other nations, and the man who is fanning such hostility is the one who met with the Japanese Socialists."

"What he said about territorial revision is contrary to Communist discipline and is inimical not only to the Soviet Union but is against Communist principles. That man did not speak like a Communist."

Khrushchev was quoted as saying that Russia would return the islands of Habomai and Shikotan captured from Japan in World War II if the United States gave up control of the island of Okinawa.

checking accounts

ONE OF 50 SERVICES



**WOULD-BE COED**

Sandra Louise Sanborn, 12, of Boulder, passed entrance examinations at University of Colorado with strong grades. Her father, an electronics engineer, and mother, who holds a master's degree in education, instead have enrolled her at Mt. St. Gertrude's Academy to be nearer girls her age.

**Gala Ceremonies Open Concourse**

(Continued from Page a-1)

the opening, set the keynote for the occasion when he told the audience "today the sky is the limit." This legend was repeated in a large sign hanging from the terrace level of the Convention Hall.

Golden said the occasion opened a new era for San Diego, adding stature to its metropolitan center and serving its residents for meetings, shows, displays and cultural events.

**PRIVATE FUNDS**

Both Golden and Whitney noted that San Diegans can be particularly proud of the Community Concourse because they contributed \$1.6 million of the cost by private subscription.

Whitney said the money was raised "in six weeks by citizens who did what seemed impossible."

The speakers addressed the audience from a bunting-draped stand which faced the now-abuilding 14-story city administration building. They were flanked by the Naval Training Center Band and the Recruit Training Command Choir from the center.

Part of the crowd looked down on the festivities from the terrace level of the Convention Hall and Parking Garage. There also was a cluster of watchers across the street at the Security National Bank. The bank bore a large sign which said, "Congratulations City of San Diego from Security National Bank."

**SPURT FORESEEN**

Milton R. Cheverton, president of the Convention and Tourist Bureau, explained what the convention buildings will mean to San Diego. He said within three years convention spending here will almost double, from \$22 million in 1963 to \$50 million by 1966.

Before the speeches, the flags of all 50 states were marched onto the terrace level of the Convention Hall by Navy blue-jackets.

On the reviewing stand were city officials, county officials, representatives from Baja California, state officials and various persons who have been associated with the Concourse project.

**PRaise FOR DAIL**

In his talk, Curran said particular praise should go to former mayor Dail, "the individual who gave us the direction and the leadership in working out the program." Dail was given a standing ovation.

The administration building will be open in December and the 3,000-seat Civic Theater in January.

The San Diego Products and Economy Exhibit will remain open from 2 to 10 p.m. all week. More than 300 San Diego products are on view from macadamia nuts to missiles.

A half-hour concert by the band and choir preceded the dedication.

**Term-Suspended For Arvey's Son**

ST. LOUIS (UPI)—Eugene R. Arvey has been given a suspended, five-year jail term for fraud on condition that he continue intensive psychiatric care.

Arvey's father, Jacob, former Democratic political leader in Illinois, was in court but had no comment.

Arvey pleaded guilty to interstate transportation of forged General Motors Acceptance Corporation securities and to interstate transportation of part of the funds received from a St. Louis bank for the forgeries.

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**NELSON PHOTO SUPPLIES**

House of Commons and will face an electorate of approximately 36 million people. Besides Conservative and Labor party candidates, Liberals, Communists, Independents and Scottish Nationalists will be seeking office.

If the Conservatives win, it will be their fourth consecutive victory and Douglas-Home will carry on as prime minister. A Labor win would carry Wilson to the premiership.

British parliaments are elected for five-year terms but a government can be brought down anytime if it is defeated on an important policy issue in a Commons vote.

**PROSPECTS DISCUSSED**

Asked about his party's prospects when he returned to London from Balmoral Douglas-Home replied with a grin: "They have never been better. I am confident we will be successful."

Wilson, who had been consulted in advance about the election arrangements, predicted victory in a television interview.

Liberal party leader Jo Grimond expressed delight that the election date now had been finally fixed.

"The great majority of sensible people want liberalism," he added. "This is shown by our increase in the opinion polls."

negotiations, the Vatican communique said the two parties declared themselves willing to have further exchanges of views.

The communique gave no details of the accord, but Vatican sources said it:

1. Guaranteed further freedoms for Catholics in Hungary.
2. Eased restrictions on priests. At present they need Hungarian government permission before they can preach in church.

The Vatican sources said they understood the church in Hungary was granted liberty to communicate with the Vatican, to train priests and religious order members and to provide religious instruction.

Vatican sources said it was the first direct accord signed between the Holy See and a Communist government. It represented a major development in relations between the Catholic Church and the Communist world.

**Reds Invite Pakistani**

KARACHI, Pakistan (Reuters)—President Mohammed Ayub Khan has accepted an invitation to visit Russia from the Soviet government, it was announced here yesterday. The announcement said the date of the visit would be given later.

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James S. Copley, left, greets former President Dwight D. Eisenhower, Dr. Kenneth D. Wells and Col. John H. Glenn Jr. to board of directors meeting of Freedoms Foundation at Borrego Springs.

# Concourse Dedication Rite Today

### \$21.5 Million Edifice Complete; Public To View Theater 1st Time

Theater story told—Page 6-1

The city's completed \$21.5 million Community Concourse will be dedicated at 2 p.m. today. Following the ceremony, visitors will get their first public preview of the Civic Theater, last of the five major components to be completed.

Mayor Curran will preside at the dedication. The festivities will be in the central plaza area, facing B Street at Second Avenue. Veils will be removed from plaques and inscriptions, and ribbons will be cut, opening the way for public tours.

The first and twelfth floors of the 14-level city administration building, the exhibit hall, the 5,000-seat convention hall, and the new theater will be open.

#### CONCERT TUESDAY

The first performance in the 3,000-seat theater, built at a cost of \$4 million, will be Tuesday. It will be a concert by the San Diego Symphony, with two opera singers; Dorothy Kirsten and Brian Sullivan. Other events will follow through the week.

Among participants at the dedication will be George Bean, former city manager who was involved in much of the planning for the concourse. Bean, who left San Diego in September, 1961, now is city manager of Grand Rapids, Mich. He was invited here by San Diegans; Inc.

Today's event will be a culmination of initial uses of the concourse which began Sept. 15 with a grand opening. Formal ceremonies on that date marked the opening of the first three structures in the concourse—the 11-level parking garage, the exhibit hall, and the convention hall.

#### GOVERNMENT MOVES

Subsequently the City Administration Building, fronting on C Street, was completed and the city government moved to its new home from the waterfront Civic Center early in November.

This week's events, the first chapter in a new era in culture and entertainment in the Civic Theater, also is the final chapter in a two-year building program.

## Sniper Slaying Still A Mystery After A Year

By JERRY MARCUS

The week before Valentine's Day last year a 20-year-old sailor bought a \$1.75 box of candy and walked two blocks with his bride to admire the ocean.

They walked to the end of a patio at 5116 Narragansett Ave., Ocean Beach, where the swirling surf laps at the base of the cement wall.

One of the last things Johnny Ray Swindle, 20, ever did was to place the box of candy on the retaining wall behind which the couple was standing. It was a moonless night as they looked upon the ocean.

#### SHOT FIVE TIMES

Atop an ice-plant-covered hill about 50 feet away from Johnny and his 19-year-old wife of two weeks, Joyce Ann, someone aimed a rifle at their backs and began pulling the trigger.

The killer shot them five times from his vantage point on the hill. He then moved down to the patio to shoot them twice more at close range.

Swindle was shot four times and his wife was shot in the left arm, the back and the head. The couple was found at 8:35 p.m., 40 minutes after Swindle.

(Continued on a-32, Col. 1)

#### STATE OF CITY

### Curran Due To Report On Progress

Mayor Curran will report to citizens of San Diego Tuesday on affairs of the city during his first year as chief executive.

His second State of the City Message also will outline goals for 1965.

The message will be delivered at 10:30 a.m. during a regular City Council meeting on the 12th floor of the Administration Building.

It will be carried live on television and radio by stations KFMB and KOGO.

Curran is expected to spend 40 minutes in analyzing 1964 and presenting his views for 1965.

## Eisenhower Joins Borrego Meeting

### Presides At Freedoms Foundation Board Session, Praises Group's Aims

By KEN ZUMWALT

San Diego Union Staff Writer

BORREGO SPRINGS—Gen. Dwight D. Eisenhower yesterday flew here from his winter vacation home at Palm Desert to preside at the semi-annual meeting of the board of trustees of Freedoms Foundation at Valley Forge.

The officers and trustees were guests of James S. Copley, chairman of the corporation publishing the Copley Newspapers. He is also a trustee of Freedoms Foundation.

Making the flight with the former president was Herbert Hoover Jr., another trustee. The

all-day meeting of the national patriotic organization was held in La Casa del Zorro.

Although it was a closed business session, Gen. Eisenhower spoke to newspapermen prior to his return flight.

"Freedoms Foundation at Valley Forge is a great non-profit citizens organization alerting the people of the United States to the value of freedom and what it means to them.

"In my opinion," he said, "anyone supporting Freedoms Foundation just makes it more certain that the beliefs handed down unchanged from our founding fathers will be perpetuated."

Col. John H. Glenn Jr., the astronaut, and Col. John S. D. Eisenhower, the former president's son who is also executive vice president of Freedoms Foundation, were at the meeting. Glenn took time out before the session for a two-mile jog up and down Yaqui Pass Road which runs past La Casa del Zorro.

Others at the meeting were Dr. Kenneth D. Wells, president; Den Belding, chairman of the executive committee; Gen. Bruce C. Clarke, Frank Gard Jameson, Dr. Laurence C. Jones, A. C. Rubel, George D. Bailey, William Herbert Carr, Walker L. Cisler, Patrick J. Frawley Jr. and Wallace D. Hall, all trustees.

Also Richard H. Foltz, senior vice president, Lt. Cmdr. W. Jerome Hagerty, regional vice

## Navy Marks 10 Years Of 'Atomic Age'

By ROBERT ZIMMERMAN  
The San Diego Union's Military Writer

The message was directed from a submarine commander to the president of the United States, and it was terse and to the point: "Under way on nuclear power."

The message came from the submarine Nautilus Jan. 17, 1955—ten years ago next Sunday. With these four words, the U.S. Navy entered the era of nuclear power.

Ten years later the Navy has nearly 50 nuclear-powered submarines in the fleet, and a task force of three surface vessels also propelled by the energy of atomic fission.

The decade also has brought atomic seapower to San Diego's doorstep—with four nuclear submarines making this their home port.

The four — Scamp (SSN588), Sculpin (SSN590), Snook

#### AEROSPACE WORKER'S PROBLEM

## Need For Job In New Skill Filled By Employment Unit

By JOE BROOKS

John Doe was a technical writer at a big aerospace plant here. He made \$12,000 a year.

Things were rosy for John until the government con-

have turned to private employment agencies.

This is the biggest challenge presently facing the 25 to 30 employment agencies in the San Diego area, according to Ken Barnes, legislative chair-

like him, wants to stay in San Diego, he must reconcile himself to starting his career all over again. In a new field. At the bottom.

Barnes said the employment agency will take on

## Navy Official Ends Service Friday

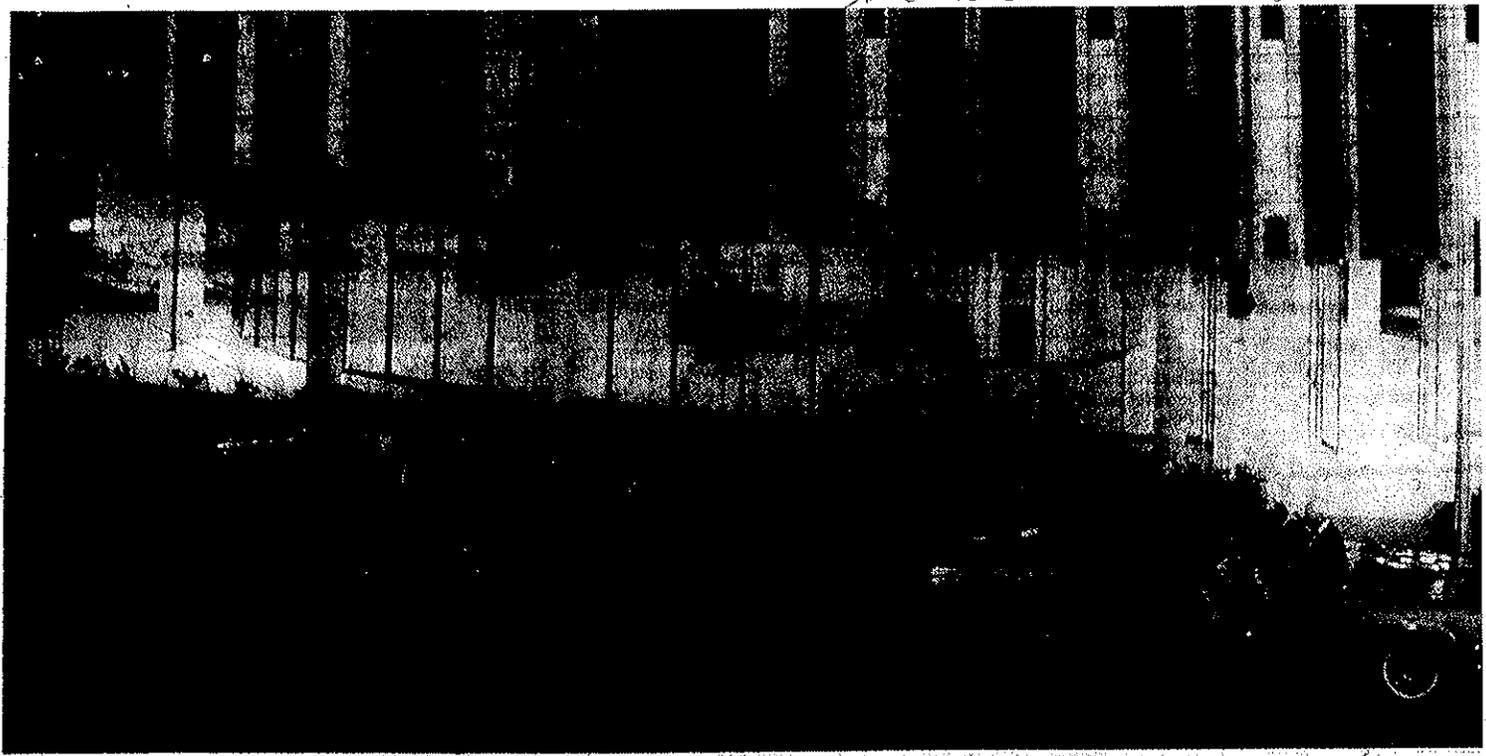
Paul B. Fay Jr., will make his last appearance as under-secretary of the Navy in San Diego Friday.

The 46-year-old Californian, whose resignation is effective Friday, will speak at a luncheon meeting of the San Diego Council of the Navy League. He will be honored at a parade and review of Navy and Marine Corps recruits at the Naval Training Center.

The review will bring together 1,300 recruits from the Marine Corps Recruit Depot and 1,800 Navy recruits from NTC, making it one of the largest

He has indicated that his talk to the Navy League here will deal with serious personnel problems facing the Navy and Marine Corps. The luncheon will be at noon in the Palm Room of the U. S. Grant Hotel.

The parade at NTC, scheduled to begin at 2:45 p.m. will be open to the public and will be the first review in several years bringing together Marines and sailors from San Diego's two recruit training installations.



**CULTURE'S NEW HOME:** Bright lights and formally dressed theatergoers are symbolic of the air of anticipation surrounding San Diego's Civic Theater opening Tuesday. At the theater's entrance are representatives of the many local cultural organiza-

tions expected to utilize the theater. Included are, from left, Lowell Davies, Mrs. Thomas Hamilton, Philip Klauber, Mrs. Jack Galen Whitney, Mrs. Michael Ibs Gonzales, Morley Golden, Mrs. John Chambers and David Thompson. At the door are Mrs.

Norman Lawson new theater is chuk, assisted by photographers.

# The Civic Theater: On A Note O

By ALAN M. KRIEGSMAN

Architecture, the poet tells us, is frozen music. And in the spirit of this resonant metaphor the citizens of San Diego may well look upon their new Civic Theater, on the threshold of its opening Tuesday evening, as a grand, graceful symphony of fulfilled aspirations.

Or they may view it rather as a noble cantata, proclaiming once and for all time to the world at large our city's determination to seek a high place in the cultural firmament.

To the many who dreamt, fought and worked toward this moment over the years, the reality of the theater will be the final, triumphant note in a sweeping crescendo of hope, vision, planning and effort. To the Community Concourse, which the theater itself now makes a completed entity, it will become a living symbol and instrument of the downtown renaissance. And to all of the city and its inhabitants, present and future, it will stand henceforth as an artistic capital, the seat and dwelling place of the very finest San Diego has to offer in the realm of performance.

No one knows now, of course, what performances will sound or look like in the hall, and no one will know until Tuesday's inaugural concert by the San Diego Sym-

phony. Experts have been consulted, and every effort has been made to insure optimum visual and acoustical effect, and to avoid mistakes encountered elsewhere. Experience, however, has taught us not to look

for certainties in these matters. Part of the excitement of the opening, in fact, is the anticipation of discovering for the first time the theater's natural sound, sight and "atmosphere."

The tremendous benefits of the particularly in relation to what begins have been previously accustomed will be apparent from the moment the open. The orchestra, for example, v

## It's Everyone's Business

By MORLEY GOLDEN

There is a body of thought in our country, made popular in many novels, which holds that the businessman is the enemy of culture.

The major argument for such thought is that the two are naturally inimical—that business deals in tangible returns and that the returns of art must almost always be intangible by their nature.

Like many generalities, this one is absurd. Within our own

city, we have hundreds of examples of businessmen who have been among the most active in the preservation and promotion of our cultural life.

Among them I count my associates in San Diegans, Inc., who have made the major contributions in money and time to the completion of our new theater, a project that represents a shot of adrenalin in our increasingly

healthy cultural veins.

At the moment our attention is focused on our Centre City. But we need not look very far into the neighborhoods and communities of our area to find additional business support for theaters, music, art, literature, etc.

I have often wondered why. (Continued on E-7, Col. 5)

Morley Golden



# Dedicated At Brief Rite

## Theater Opening Is Next

Picture—Page a-3  
By JIM McVICAR

The \$21.5 million Community Concourse was dedicated as a tribute to culture, industry and good government yesterday in ceremonies attended by city officials and hundreds of citizens.

People crowded the central plaza area south of B Street and many lined the second level balcony of Convention Hall. Others, who sought a better view, watched the ceremony from the upper levels of the parking garage.

"Conceived by citizens of vision, this Community Concourse is dedicated to the people of San Diego as a tribute to culture, industry and good government," reads the inscription in raised gold letters on the south side of the plaza.

### MAYOR OFFICIATES

The inscription was concealed by curtains which were officially opened at a signal from Mayor Curran.

The Concourse consists of a convention hall, exhibit hall, 1,000 car parking garage, city administration building and civic theater. The bulk of the financing was provided by an investment loan from the City Employees Retirement System.

The weather was delightful for a dedication, and the mayor promised he would please the crowd even more by keeping the ceremony short. He kept his word. The introductions and remarks, followed by the traditional ribbon cutting, took slightly over 20 minutes.

### 'RED TAPE' CUT

The mammoth satin ribbon which stretched across the plaza, was bright red. Someone made a comment about cutting red tape as the mayor, councilmen and others, symbolically and officially opened the completed complex of community buildings.

Most of the city council, City Mgr. Tom Fletcher and former City Mgr. George Bean, Guilford Whitney and Morley Gold-joined the mayor at the rostrum for the dedication.

Mayor Curran said San Diego has been waiting a long time for completion of all the concourse facilities.

"We were almost finished about a month ago when we moved into our new city administration building," he said. "Now we have completed the Civic Theater."

### THEATER 'BEAUTIFUL'

The mayor described the theater, which will have its opening tomorrow, as "not only beautiful, but functional." He stressed that San Diego can now accommodate any theatrical event produced.

The stage is the second (Continued on a-18, Col. 1)



Guilford Whitney, holding giant scissors, helps mayor and city councilmen cut ribbon dedicating

Community Concourse. Smaller shears, held by mayor, actually did the job.

## Crash na-Bound Overturns Escondido

ma man was killed her persons hurt yes- in their small foreign turned on U.S. 395 sccondido and left the the California High- reported.

Coroner W. T. Souza s G. Neal, 23, a cafe m Pomona, was dead at Palomar Memori- in Escondido at 5:18 offered internal inju- said.

said the driver. Dap- psey, 24, of Pomona, r passenger, Robert of Upland, escaped injuries.

### OUT

n R. B. Sipes said old him he fell asleep ng south on the high- four miles north of The car hit the he west side of the overturned at least came to rest in an Sipes said.

id both Neal and were thrown out of as the car skidded ng the berm, went t broadside skid and

men were en route at the time of the uza said.

### D FRIENDS

urvived by his par- nd Mrs. Harold Neal phia, Ark. He and were childhood l came to California adelphia last June,

h increased the 1965 fic fatality toll to 15, even in rural areas. e last year the toll cluding one in rural

### Plans Talks

### fic Safety

ibson, chairman of of Supervisors, has he will call a meet- munity leaders to remedy for the highway death toll.

officials of the mil- ce agencies, the l other governmental ill be invited. Gibson ecided to hold the fter discussing the th officials of the ty Council.

## Sun Today

weather prevailed Diego County again and more of the pected today. en said a light Santa low is bringing dry e area, keeping day- satures comfortable. s high was 65, and hest reading should hts are expected to The mercury dipped yesterday.

## Orange Pickers

## Scion Killed In Motorcycle Racing Crash

A member of a prominent Escondido family was fatally injured yesterday in a motorcycle accident on a dirt race track near Valley Center.

Dead was Frederick Allin Einer V, 21, of 725 Beacon Place, Escondido, son of Frederick Allin IV, president of Einer Brothers, Inc., general contractors.

The victim died of head injuries in Palomar Memorial Hospital, Escondido, at 2:07 p.m., about an hour after he was admitted. He was a truck driver for his father's firm.

The California Highway Patrol said the accident occurred at 12:10 p.m. while Einer was racing on the privately owned track about two miles east of Valley Center Road.

Patrolman A. T. Scott said Einer was racing with Darell R. Ames, 29, of 903 Goldenrod St., Escondido, when they came out of a curve. He said Einer went into a slide and the motorcycles collided.

Ames was treated at the hospital for cuts.

The CHP said the death would not be listed as a traffic fatality because the crash occurred on private property.

## Symphony Event To Fill New Hall

Sellout Crowd Due For Program In Civic Theater Tomorrow Night

By BILL PARRY

Tickets were sold out two weeks ago for tomorrow night's performance by the San Diego Symphony Orchestra. And reservations for the dinner preceding the colorful musical production were sold out more than a month ago.

This is the kind of enthusiasm that will mark the grand opening of San Diego's new Civic Theater. The events will be sponsored by the San Diego Symphony Association and the San Diego Opera Guild.

J. Dallas Long, chairman for the evening, will welcome the first nighters after they walk from the 7 p.m. dinner in the Convention Hall beneath a 150-foot long canopy to the steel and concrete theater.

### 1,025 FOR DINNER

Long said 1,025 persons have reservations for the dinner and 3,006 will be on hand when the curtain rises at 8:45 p.m. on the first music ever to be heard in the theater.

Mrs. Jack Oatman is directing arrangements for the dinner and Mrs. Fred Boss is chairman of decorations for the week, according to William L. Denton, symphony manager.

Strolling violinists will provide background music at the symphony dinner.

Philip M. Klauber is symphony association president and Mrs. Lionel U. Ridout is president of the opera guild.

### 4 SHOWS BOOKED

The symphony is the first of

### E CLAMPUS VITUS

## Historical Unit Honors S.D. Editor

A historical group with a singular name and a noble purpose honored San Diego news media in general and one editor in particular at its annual installation banquet Saturday night.

The Squibob chapter of E Clampus Vitus paid tribute to the press and initiated Richard F. Pourade, editor emeritus of The San Diego Union, to honorary membership during ceremonies in the Bronze Room, La Mesa.

The organization, which dates back to '49er days in California, researches and documents historical incidents and sponsors installation of markers all over the state.

Its new president or "noble grand humbug" is Ben F. Dixon. He replaces J. Francis Mergen, who becomes an ex-noble grand humbug.

Joe E. Mensor, another ex-noble grand humbug, read the list of new officers, explaining that they were "undemocratically selected" by the board of Clampelders.

As he called off the list of officers "in order of least importance," he asked them questions such as "Will you feed the hungry?" "Will you protect the widows and orphans, especially the widows?" and "Will you clothe the naked?"

It was explained that when

(Continued on a-21, Col. 2)

Wolf?" The one night production will open under auspices of the Broadway Theater League and the Old Globe Theater and star Kendall Clark and Vicki Cummings. Bryarly Lee and Donald Briscoe will be featured in the hit, which played to standing crowds when it opened a two-year run in October, 1962 on Broadway.

### 'PINAFORE' DUE

The third event will be the D'Oyly Carte Opera Company's presentation of "HMS Pinafore," the first international triumph written by Gilbert and Sullivan. The curtain goes up at 8:30 p.m. on the sea story. set (Continued on a-18, Col. 3)

## DRAMA SET FOR THEATER

Theatergoers who attend Wednesday's performance of "Who's Afraid of Virginia Woolf" will be among the first to see San Diego's new Civic Theater.

The high-powered drama by Edward Albee begins at 8 p.m.

An announcement in the "Leisure and Arts" section of The San Diego Union yesterday listed curtain time as 8:30 p.m. A spokesman for the theater said the earlier time is correct.

## Early Leader In Concourse

This photo and the one at right show the two faces of winter, New York City's and San Diego's. Snowy view is of Times Square, with pedestrians

spending their way through snow and sleet. Traffic was thinned by the all-day snow. Statue is of George M. Cohan, composer. (Story on Page a-1)

# San Diego Moves Ahead



Crowds gathered in plaza, on Convention Hall balcony and in parking garage to watch ceremonies

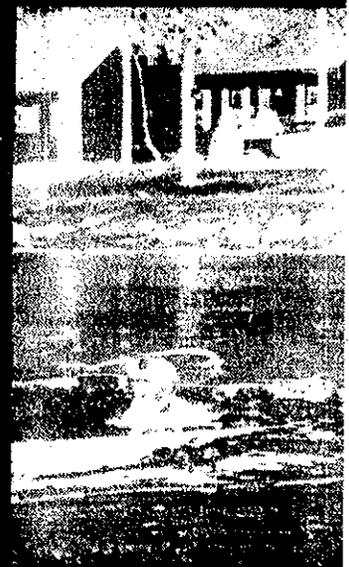
—San Diego Union Staff Photo by Phil MacMahon  
Dedicating Community Concourse. Ribbon was cut prior to public tour. (Story on Page a-15)

# An Icy Skid, Then A Crash



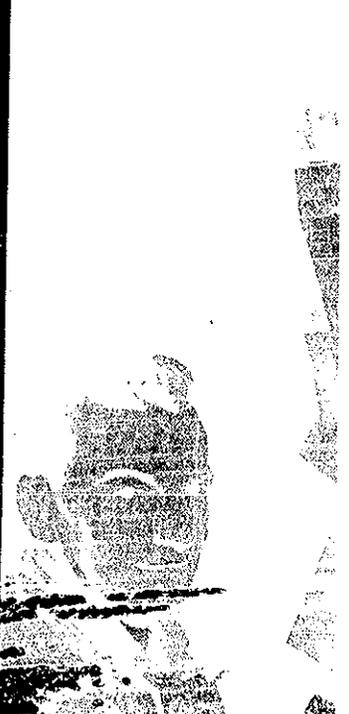
Balmy winter weather of San Diego two young anglers to try their luck at

# Rescue



Policeman John Mulligan, left, is rescued after he plunged through ice pond after he rescued James Devareness, 12, right.

# Sculpt



# City Buildings Take Shape

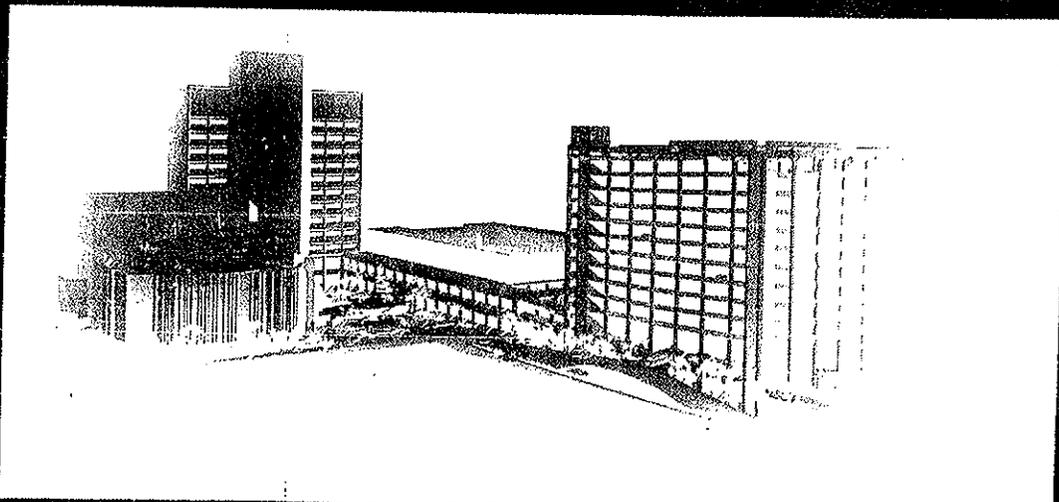


View northwest from corner of C Street and Third Avenue shows Community Concourse beginning to take shape on three-block site. Convention Hall is rising at left

across First Avenue from Electronics Capital Building. Girders next to it facing C Street, are lower framework of City Hall building. At right, excavation has started

for 3,000-seat theater. Rear, 11-level parking garage is beginning to rise. Community Concourse project will cost \$16 million, with completion due by December, 1964.

San Diego Union Staff Photo



Here is how Community Concourse complex will look when completed. At left, theater faces B Street at Third Avenue.

with new city hall behind it. Low structure at center is convention hall, with parking garage extending to A Street at right.

Parking garage will hold 1,000 cars, and two-level convention hall will have seating capacity of 5,000 when needed.



## Beauties

Miss America of 1962, Jacquelin Mayer, right, is applauded by contestants at registration for Atlantic City pageant. Hats were prominent among



## Sweets For

Julieanne Essen, 3, shares her candy with Hereford yearling from her father's ranch at Charter Oaks as the



# New Idea Will Help City Hall Shed Old Look

PAI 10-5-89

Almost anyone who has tried to find San Diego's City Hall for the first time can tell you a story about searching for the anonymous, faceless 13-story building on C Street.

In 1964, the city moved to this underaker's paradise of poorly lit spaces from the stately quarters it shared with the county in what is now the County Administration Center on Pacific Highway.

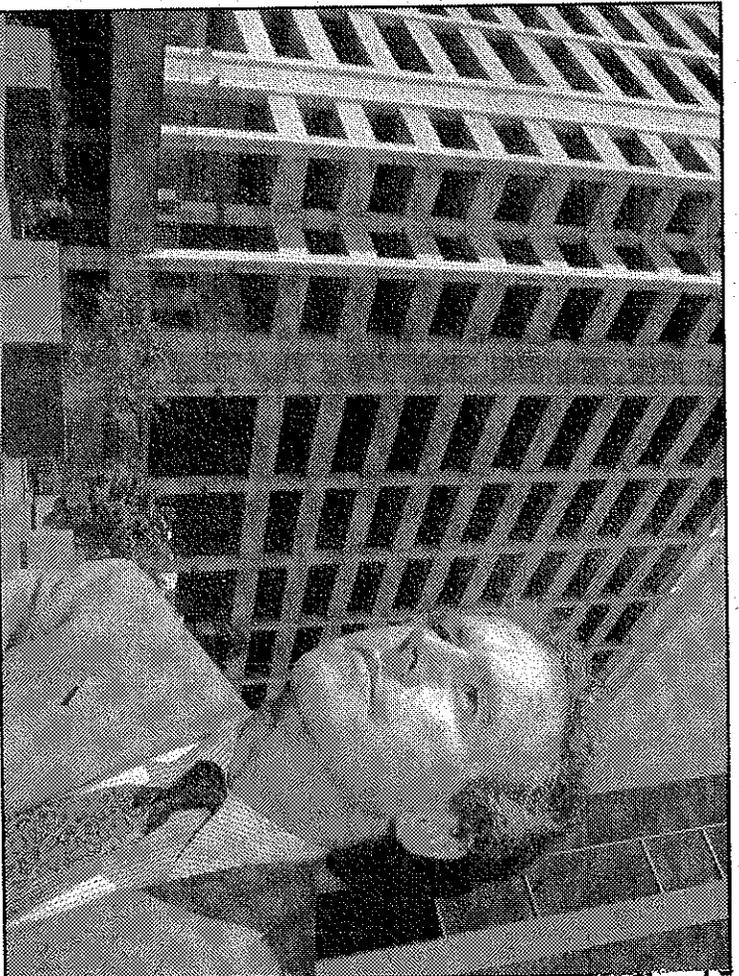
There is no cause for civic pride in the present city headquarters. Re-  
**SAN DIEGO COUNTY**  
presenting this, the city has ambitious plans to open a signature civic center by 1995 on four city blocks east of 12th Avenue and Broadway downtown.

The man charged with delivering a first-class, world-class project is Portland, Ore., architect Donald Stastny.

Stastny was chosen by city officials because of his experience with design competitions and because he wants to use a competition process called "design/develop," thought by many to be the ultimate marriage of good design, feasible construction, and quality and cost controls.

By selecting a group of potential architects first, then asking developers to team up with them, design is given the priority it deserves.

Although various incarnations of the design/develop approach have been used in the past, the San Diego civic center will be an extremely refined version. This



DAVID McNEW

Architect Donald Stastny's unusual "design/develop" concept will be used to build a signature civic center on four city blocks in downtown San Diego.

competition could well become a model for major public building competitions for years to come, according to Stastny.

Among the building designs resulting from competitions run by Stastny are the Beverly Hills Civic Center, designed by Urban Innovations Group with Charles Moore; the Domane Clos Pegase winery in Northern California designed by Michael Graves; and a planned Walt Disney Concert Hall in Los Angeles, designed by architect Frank Gehry. (Stastny has also designed and planned a number of large projects.)

Los Angeles Times

## Sign

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be valuable tools.  
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as before final se-

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ed to do the job.  
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Clearly, Stastny isn't known for producing middle-of-the-road architecture. He likes innovation, and hires world-class firms to sort through proposals from internationally known architects such as Michael Graves, the leading postmodernist, and Frank Gehry, the architect and artist whose bizarre combinations of forms and materials have inspired a whole generation of cutting-edge Southern California architecture.

Unlike other consultants who place de-  
**Please see CITY HALL, Page 8**

so needs to be  
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2th floor of the  
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ment."

# CITY HALL: Architects, Builders to Team on Design

Continued from Page 1

sign decisions in the hands of juries composed of city officials and assorted civic leaders, Stasny believes that only those trained in design should judge the merits of architecture.

A jury including a prominent architect, landscape architect, urban planner, developer and outside city official who has been through a competition will ensure that good architecture doesn't take a back seat to economics, as it can in projects of this magnitude.

Instead of merely selecting a design based on a ballpark construction cost, then hiring a contractor to build it, Stasny's design/develop approach combines top architects with developers who will commit, in writing, to bringing the 1-million-square-foot project in on budget, in this case \$242.3 million.

Materials and construction guidelines will be prepared so that finalists meet quality standards. That, it is hoped, will minimize developer bidding wars and encourage design competition.

Because of high construction costs and tight budgets, many cities are trying to "privatize" the development of public buildings, according to Stasny, and the design/develop method is one way to do this. Teams including experts in design, budgeting and project management should minimize city staff costs.

Stasny outlined the process. In a month or two, the city will issue a "request for qualifications"

10-5-89 part V p 8

**"There's a feeling among citizens of every city that city officials and employees should not work in luxurious surroundings. The reality is that what we need in city government is efficiency. A workplace that is equal in quality to the private sector will give us better service."**

**Donald Stasny**  
Architect

through word of mouth, direct mailings and architectural publications, inviting architects to apply for the job.

Applicants will have to supply not just resumes, but one art board (30-by-40 inches), giving some idea of their intended designs. Younger, less-known firms may want to catch the jury's attention with actual preliminary design proposals, Stasny said, whereas older firms might submit only a site plan or art work from earlier projects.

The jury will narrow the field to 20 or 30 architects.

Next, a second request for qualifications will go out to developers, asking them to approach the architects and form "design/develop" teams including contractors and other professional consultants.

Several teams will be interviewed by the jury, and three to five candidates will be chosen to compete for the job.

Finalists will each receive \$100,000 to develop their proposals.

The city should choose a winning proposal by next fall.

The design/develop process has drawbacks. Developers may be reluctant to team up with less-known architects. The odds against a small firm winning are probably substantial.

So there's a chance San Diego will miss out on some great designs. For example, the excellent new Escondido City Hall was designed by Pacific Associates Planners Architects from San Diego; in 1985, when they were selected from an international field of more than 100, they had no experience doing large public buildings. Yet, probably because their thinking was undated by years of experience, they produced the freshest design.

According to Stasny, young architects won't necessarily be out of the picture. They can team up with architects who specialize in project management and the production of construction drawings.

Public opinion will be important, Stasny said. Public meetings,

mailed questionnaires and phone polls can all be valuable tools. There will be at least one public display of designs before final selection.

Stasny realizes that the civic center could be the key to a successful urban center in San Diego.

With the civic center at the east edge of downtown, and the Navy's planned complex on the waterfront, Broadway will assume its natural role as the primary downtown street. This will be a shift, since most key streets—5th Avenue, 1st Avenue and others—run north and south, not east and west.

A well-designed civic center will help people adjust to the change. Skeptics have grumbled that the significant project cost is unjustified, that existing city buildings on C Street and nearby could be altered or expanded to do the job. Stasny doesn't agree, for several reasons.

"There's a feeling among citizens of every city that city officials and employees should not work in luxurious surroundings," he said. "The reality is that what we need in city government is efficiency. A workplace that is equal in quality to the private sector will give us better service."

"A city hall also needs to be accessible. Anyone held up on the elevator trying to get to the council chamber on the 12th floor of the existing City Hall knows that the current configuration doesn't lead to accessible government."

**APPENDIX G**  
**Preparers' Qualifications**



## **Preparers' Qualifications**

**David Greenwood** is a U.S. Secretary of the Interior Qualified Architectural Historian with 11 years of professional experience in conducting architectural/historic surveys, including Section 106 and CEQA compliance surveys. Mr. Greenwood holds a bachelor of arts degree in architecture from the University of Southern California.

**Elizabeth Hilton** is a U.S. Secretary of the Interior Qualified Architectural Historian with more than 8 years of experience in conducting architectural/historic surveys, including Section 106 and CEQA compliance surveys. Ms. Hilton holds a bachelor's degree in historic preservation and community planning from the College of Charleston in South Carolina and a master's degree in historic preservation from the Art Institute of Chicago.

**Daniel Paul** is a U.S. Secretary of the Interior Qualified Architectural Historian with more than 6 years of professional experience in the field of architectural history and 15 years of experience in the field of historic preservation. Mr. Paul holds a bachelor of arts degree, with honors, in art history from California State University, Fullerton and a master of arts degree, with high honors, in art history from California State University, Northridge.



**WATER DEMAND STUDY**  
**FOR THE PROPOSED**  
**CIVIC CENTER COMPLEX PROJECT**

**April 2010**

Prepared for: Centre City Development Corporation  
401 B Street, Suite 400  
San Diego, CA 92101

Prepared by: ICF International  
9775 Businesspark Avenue, Suite 200  
San Diego, CA 92131



# Introduction

The following Water Demand Study provides an evaluation of water supply available to the proposed Civic Center Complex Project (proposed project) in support of California Environmental Quality Act (CEQA) compliance and the project's Addendum to the Final Environmental Impact Report (FEIR) for the San Diego Downtown Community Plan, Centre City Planned District Ordinance (PDO), and Redevelopment Plan for the Centre City Project (SCH 2003041001, revised March 2006). Water use for the proposed project has been calculated on a per employee basis for existing demand, proposed demand, and proposed demand less the existing demand with Leadership in Energy and Environmental Design (LEED) demand reductions included. The intent of this analysis is to describe existing water demand and compare the proposed project water demand to existing demand and then determine potential reduction benefit from implantation of LEED certifiable strategies.

## Project Description

### Project Location

The proposed Civic Centre Complex Project is a redevelopment project located on a block west of the San Diego Civic Theatre, bounded by First and Third avenues, and A and C streets in the Civic/Core neighborhood of the Centre City Redevelopment Project, downtown San Diego. Centre City includes approximately 1,500 acres of the metropolitan core of San Diego, bounded by Interstate 5 on the north and east and San Diego Bay on the south and southwest. The Civic/Core neighborhood is located in the north-central portion of the downtown planning area. Figure 1 provides a map of the project vicinity.

### Existing Conditions

The project site is currently classified a Central Business District per the San Diego Community Plan and several structures occupy the area. The northwest portion of the block includes an above-grade parking structure known as the Evans Jones Parkade. This structure is planned to remain in place to provide parking for the proposed project. The Civic Center Plaza and a restaurant are located on the northeast portion of the block, and the southeast portion of the block includes the Civic Theatre. These buildings would also remain in place after development of the proposed project. The southwest portion of the block is currently occupied by a two-level 158,000 square-foot meeting and conference venue, known as the San Diego Concourse, and a 189,000 square-foot, 13-level office building, known as the City Administrative Building. The San Diego Concourse and City Administrative Building are both proposed by the project for demolition.

### *Sources of Water Supply*

The City of San Diego Water Department provides water service to downtown and delivers more than 200,000 million acre-feet of water annually. The water department delivers potable water throughout an area of approximately 330 square miles. During an average year the City's water supply is made up of 10 to 20 percent of local rainfall, with the remaining amount imported from regional water suppliers including the Metropolitan Water District (MWD) and the San Diego County Water Authority (SDCWA). Potable water pipelines are located underneath the majority of downtown's streets mimicking the above-ground street grid pattern. The existing facilities that service the existing site would continue to serve the project. Table 1 below describes the comparison of current water use to proposed water use.

**Table 1: Current and Project Water Usage**

	Existing Daily Use Million Gallons per Day	Project Daily Use Million Gallons per Day	Net Increase in Water Demand Gallons per Day
Project Site	0.015	0.019	0.004

**Water Demand per Employee**

The water demand per employee is calculated in Table 2. This calculation assumes each employee would utilize restroom facilities 3 times per day, operating both toilets and sinks. The calculation also includes an approximate use of drinking water. The total water demand is determined to be 6.9 gallons per employee per day.

**Table 2: Water Demand per Employee**

Land Use	Restroom Use (# trips/day)	Toilet flush (gal)	Lavatory (gal)	Drinking (gal)	Water Demand (gal/employee)
Commercial/Retail	3	1.5	0.6	0.2	6.9

**Existing Water Use**

The existing water use for the San Diego Concourse and City Administrative Building is included in Table 3 below. The total demand in gallons per day (gpd) is calculated to be 15,456 or 0.015 million gallons per day (mgd). This total equates to the annual average demand calculated on a daily basis. In order to predict greater potential demands that occur during periods of increased system use, the maximum day and peak hour rates are calculated. Maximum day and peak hour factors were determined by applying the total average annual demand to the City's demand curves for the Coastal/Downtown region (CSDWD, 1999). The existing facilities water demand is 0.039 mgd for a maximum day rate and 0.093 mgd for a peak hour rate.

**Table 3: Existing Land Use, Employees, and Water Demand**

Land Use	Total No. Employees	Water Demand (gal/ employee)	Water Demand (gal/day)	Water Demand (MGD)	Max Day Factor (coastal/ downtown)	Peak Hour Factor (coastal/ downtown)	Max Day Demand (MGD)	Peak Hour Demand (MGD)
Centre City Building (N of Civic Center plaza)	339	6.9	2,339.1	0.0023	2.5	6	0.006	0.014
Civic Center Associates Building (N of Civic Center plaza)	118	6.9	814.2	0.0008	2.5	6	0.002	0.005
Civic Center Plaza (NE Block)	885	6.9	6,106.5	0.0061	2.5	6	0.015	0.037
Restaurant (NE Block)	28	6.9	193.2	0.0002	2.5	6	0.000	0.001
Parkade (NW Block)	10	6.9	69	0.0001	2.5	6	0.000	0.000
Civic Theatre (SE Block)	200	6.9	1,380	0.0014	2.5	6	0.003	0.008

Community Concourse (SW Block)	60	6.9	414	0.0004	2.5	6	0.001	0.002
City Administrative Building (SW and SE Block)	600	6.9	4,140	0.0041	2.5	6	0.010	0.025
<b>Totals</b>	<b>2,240</b>		<b>15,456</b>	<b>0.0155</b>			<b>0.039</b>	<b>0.093</b>

## Proposed Project

The Civic Center Complex Project proposes to provide a new City Hall facility for the City of San Diego. Figure 2 illustrates the proposed project's site plan. The new City Hall building is proposed to consolidate nearly all current downtown City employees into one new facility. The 19-story (approximately 300 feet tall) building would consist of an office tower and ground floor retail space. Adjacent to the tower is an approximate one-acre public plaza comprised of hardscape and landscape elements.

A maximum capacity of 2,420 City employees and elected officials and 400 members of the public are estimated to occupy the space. The project is consistent with the San Diego Community Plan and a discretionary approval is required for a Centre City Planned Development Permit. Figure 3 shows the SANDAG 2030 forecast for the project area. Based upon SANDAG's most recent population estimates, 4,147 city employees are forecasted for the project area.

For the purposes of employee estimation for all facilities existing or proposed, the number of employees assigned to each building is based on actual reported employee numbers or the total square footage of the building divided by 300 square feet per employee. This accounts for all of the employees within the project area. All non-employees that may be present intermittently in the project area are not included, but expected to fall within the maximum daily and peak hour estimates based on the City's peaking factors (CSDWD, 1999).

The proposed land use, employees and water use is included in Table 4 and calculates the average annual demand of the proposed project. It is important to note that these totals include reductions for the demolition of the existing facilities as the new facilities come on line. Water demand less existing demand for 4,050 employees will be 0.0279 mgd. Table 5 calculates the water demand for the proposed landscaping. This calculation relies on land use type and square footage of area to be landscaped. Table 4 and 5 are combined to determine total water demand for the 4,050 employees of the proposed project. This total is greater than the water demand of the existing facilities by approximately 12,500 gallons per day. This increase is expected given the overall increase of employees for the proposed project. Peaking factors are also calculated by the same methods used in Table 3.

**Table 4: Proposed Land Use, Employees, and Water Demand**

Land Use	Total No. Employees	Water Demand (gal/employee)	Water Demand (gal/day)	Water Demand (MGD)	Max Day Factor (coastal/downtown)	Peak Hour Factor (coastal/downtown)	Max Day Demand (MGD)	Peak Hour Demand (MGD)
Centre City Building (N of Civic Center plaza)	339	6.9	2,339.1	0.0023	2.5	6	0.006	0.014
Civic Center Associates Building (N of Civic Center plaza)	118	6.9	814.2	0.0008	2.5	6	0.002	0.005
Civic Center Plaza (NE Block)	885	6.9	6,106.5	0.0061	2.5	6	0.015	0.037
Restaurant (NE Block)	28	6.9	193.2	0.0002	2.5	6	0.000	0.001
Parkade (NW Block)	10	6.9	69	0.0001	2.5	6	0.000	0.000
Civic Theatre (SE Block)	200	6.9	1,380	0.0014	2.5	6	0.003	0.008
New City Hall Office Tower	2,420	6.9	16,698	0.0167	2.5	6	0.042	0.100
New City Hall Office Tower - RETAIL SPACE	50	6.9	345	0.0003	2.5	6	0.001	0.002
<b>Totals</b>	<b>4,050</b>		<b>27,945.0</b>	<b>0.0279</b>			<b>0.070</b>	<b>0.168</b>

**Table 5: Proposed Landscaping**

Land Use	Total No. Employees	Water Demand (gal/employee)	Water Demand (gal/day)	Water Demand (MGD)	Max Day Factor (coastal/downtown)	Peak Hour Factor (coastal/downtown)	Max Day Demand (MGD)	Peak Hour Demand (MGD)
Landscaped Park	4,000	0.09	4,000	0.004	2.5	6	0.01	0.024
<b>Totals</b>			<b>4,000</b>	<b>0.004</b>			<b>0.01</b>	<b>0.024</b>

To understand the additional savings in water demand, Table 6 calculates water demand using LEED reductions. LEED for New Construction addresses new construction and major renovation of commercial and institutional projects. Performance is evaluated in five environmental categories, one of which is Water Efficiency. LEED is a point-based system administered by the U.S. Green Building Council, with points awarded for meeting the specific requirements of credits in each of the categories.

Of the 69 possible points in LEED, only five are directly associated with water efficiency. These five points are apportioned among three LEED Water Efficiency credits:

- Credit 1 – Water-efficient Landscaping, two points
- Credit 2 – Innovative Wastewater Technologies, one point
- Credit 3 – Water Use Reduction, two points

Some water efficiency technologies and strategies can easily be incorporated at any point in the design process, or even late in the construction process, while others require early planning and integration of multiple disciplines. Understanding the requirements for each Water Efficiency Credit, as well as the design strategies for meeting those requirements and the planning process necessary to successfully develop and incorporate those strategies, is critical to optimizing water efficiency on LEED projects.

The LEED Reduction Factors applied to Table 6 included 50% for landscaping such as drip irrigation, 40% for waterless urinals, dual flush toilets and low flow facets, and 25% for gray water use on landscaping (Starr, 2007). However, feasibility of the project implementing the use of gray water is unknown at this time.

Table 6 illustrates that the proposed project water demand would be reduced after implementation of LEED conservation measures. The proposed project including LEED water saving measures would have a net water demand of 0.019 mgd, a reduction of 0.013 mgd from the proposed project without LEED considerations. There may also be opportunity for additional savings through the use of cooling tower water and agreements with NRG who provide cooling tower water services. If cooling water can be provided in a more efficient manner than is currently in use at the existing facilities, greater water demand savings can occur with the proposed project. There may also be opportunity to find efficiencies in the treatment and discharge of wastewater effluent from the project.

**Table 6: Proposed Facilities w/LEED Reductions Applied**

Land Use	LEED Reduction	Water Demand (MGD)	Max Day Demand (MGD)	Peak Hour Demand (MGD)
Proposed Office/Retail Space	0.600	0.017	0.042	0.101
Proposed Landscaping	0.500	0.002	0.005	0.012
<b>Totals</b>		<b>0.019</b>	<b>0.047</b>	<b>0.113</b>

Table 7 makes a direct comparison to the existing facility water demand to the proposed project water demand. Table 8 includes this comparison with LEED reductions applied to both the structures and landscaping.

**Table 7: Comparison of Existing and Proposed Project Features**

Land Use	Water Demand (MGD)	Max Day Demand (MGD)	Peak Hour Demand (MGD)
Existing Facilities - Totals	0.015	0.039	0.093
Proposed Facilities - Totals	0.032	0.080	0.192
<b>Increase in Proposed Water Demand</b>	<b>0.016</b>	<b>0.041</b>	<b>0.099</b>

**Table 8: Comparison of Existing and Proposed Project Features w/LEED reductions**

Land Use	Water Demand (MGD)	Max Day Demand (MGD)	Peak Hour Demand (MGD)
Existing Facilities - Totals	0.015	0.039	0.093
Proposed Facilities - Totals	0.019	0.047	0.113
Increase in Proposed Water Demand	0.004	0.008	0.020

The proposed project will increase the water demand at the site over the existing demand. This is due to the fact that the proposed project is expected to increase the total number of employees by approximately 1,921. This would bring the total to 4,050 employees, which is accommodated by the SANDAG's Regional Growth Forecast of 4,147 (see Figure 3).

## References

(CSDWD, 1999). City of San Diego Water Department, Capital Improvements Program Guidelines and Standards, Book 2, Figure 2.1 – Peak Hour Demand Factor, Figure 2.2 – Max Day Demand Factor. July 1999.

(Starr, 2007). J. Starr and J. Nicolow. How Water Works for LEED.  
<http://www.facilitiesnet.com/green/article/How-Water-Works-for-LEED--7545>, Accessed: April 3, 2010





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Source: ZGF



Figure 1  
Location Map

April 2010



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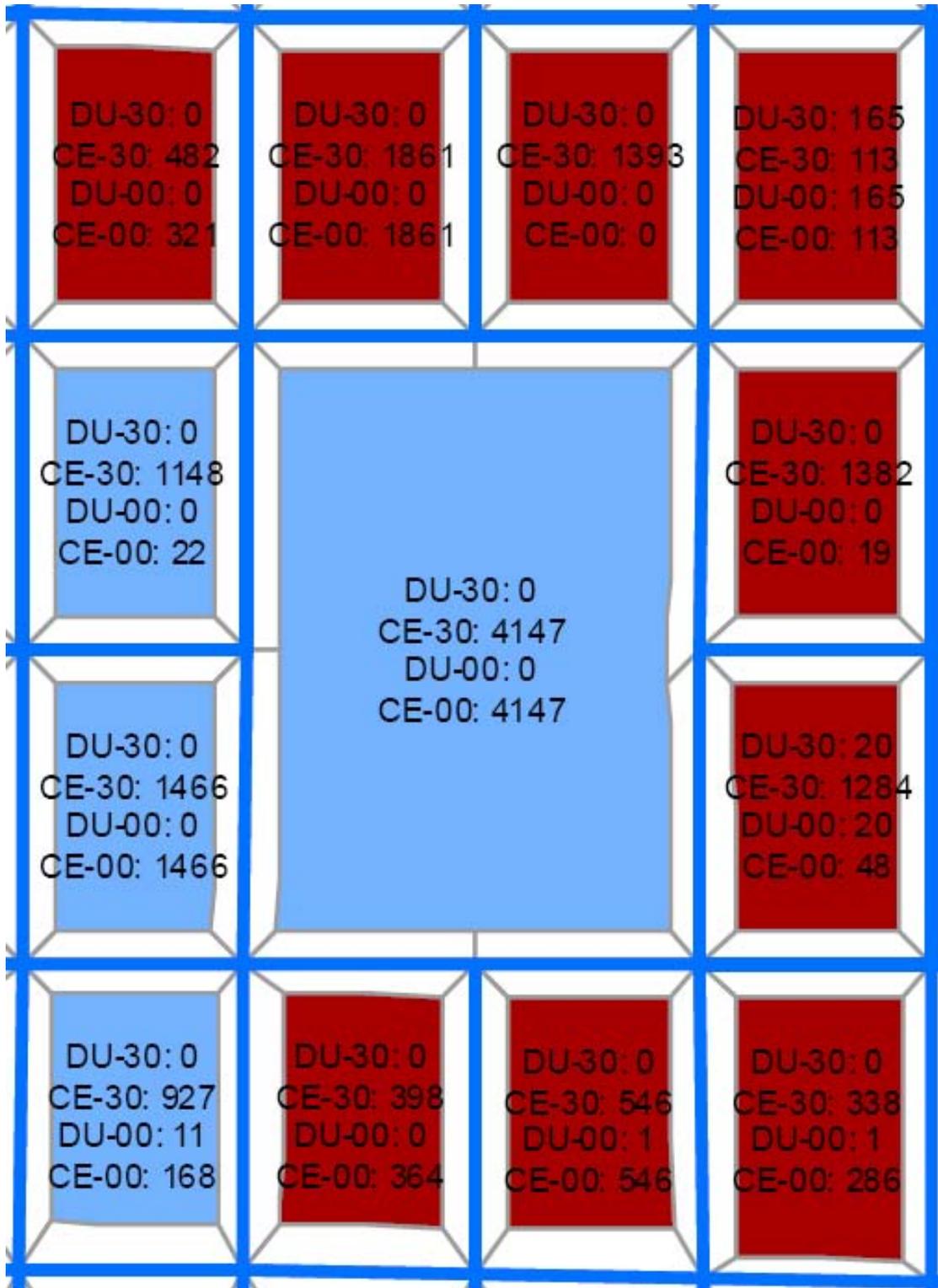
Source: ZGF

 NO SCALE

Civic Center Complex Project  
Water Demand Study

Figure 2  
Site Plan  
April 2010





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Source: ZGF



Figure 3  
SANDAG 2030 Forecast



## MEMORANDUM

Date: April 23, 2010

To: Brad Richter, CCDC  
Bob Starks, ICF

From: Mark Peterson, Fehr & Peers

**Subject: Civic Center Traffic Assessment**

SD10-0007

The purpose of this memo is twofold:

1. Document the net increase in daily traffic (ADT) associated with the proposed Civic Center project; and
2. Review traffic operations at the proposed project driveway to be located on First Avenue, just north of C Street.

### Net Project Trip Generation

The table below provides a summary comparison of land use types and the resulting daily trip generation (ADT) for both the existing and proposed civic center complexes.

#### Existing and Proposed Civic Center Trip Generation

Land Use	Quantity	Trip Generation Rate	Daily Trips
<i>Existing</i>			
Governmental Office	347,000 SF	10 / 1,000 SF	3,470
Subtotal			3,470
<i>Proposed</i>			
Governmental Office	551,794 SF	10 / 1,000 SF	5,518
Retail	16,087 SF	18 / 1,000 SF	290
Subtotal			5,808
Net Increase			2,338

Source: Centre City Cumulative Trip Generation Rates, May 2003

As shown in the table, the existing Civic Center complex generates a total of 3,470 daily trips (ADT). The proposed project, including additional governmental office and retail square footage, will generate a total of 5,808 daily trips (ADT), representing a net increase of 2,338 daily trips (ADT) over existing conditions.

The net increase in trips associated with the proposed project is less than the CCDDC trip generation threshold of 2,400 daily trips, and therefore the proposed Civic Center project does not trigger the need for conduct of a more detailed traffic impact study.

### Project Driveway Operations

The proposed Civic Center project includes a new driveway along First Avenue, just north of C Street, providing access to a two-level 461 space subterranean parking facility. Separate loading dock access is also provided along First Avenue approximately mid-block between C Street and B Street.

Our review of the proposed site plan and the driveway/loading dock configurations noted the following:

1. Effectively, access to the proposed driveway will be right-turn in and right-turn out due to First Avenue being a one-way northbound roadway. As such this will simplify the movements to and from the driveway and minimize conflicts.
2. The signalized intersection of First Avenue/C Street located just south of the driveway location should provide adequate gap opportunities for the exiting volumes, minimizing the on-site queuing of vehicles. While the same signal will also meter the flow of inbound traffic from the south, maintaining adequate driveway throat distance prior to any ticketing dispensing machine or gate will be important to minimize the potential for queuing and spillback onto First Avenue.
3. The provision of separate access points eliminates the potential for conflicts between truck movements accessing the loading docks and traffic movements entering and exiting the parking structure.
4. Access to the loading dock will require back-in truck maneuvers, creating the potential for blockage of traffic on First Avenue. Use of the loading docks should be minimized or restricted during peak traffic periods.

In summary, we do not see any significant issues associated with the parking driveway and loading dock operations. Please contact us with any questions or need for clarification on the above.