

NOTES:

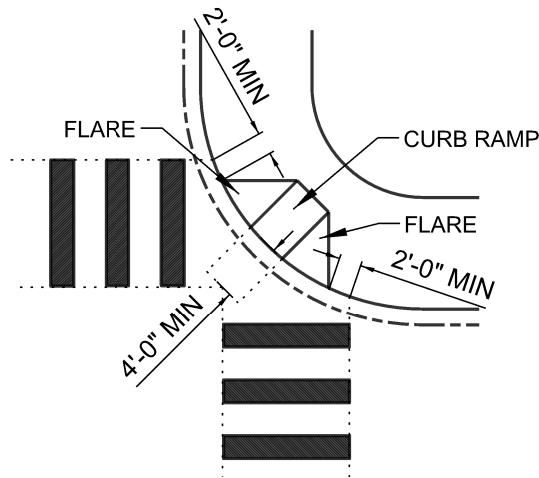
1. A CURB RAMP IS REQUIRED FOR EACH SIDEWALK CROSSING. IF A PEDESTRIAN CAN CROSS IN TWO DIRECTIONS FROM AN INTERSECTION CORNER, TWO CURB RAMPS SHALL BE PROVIDED. IF A PEDESTRIAN CAN ONLY CROSS IN ONE DIRECTION FROM AN INTERSECTION CORNER, ONLY A SINGLE CURB RAMP SHALL BE PROVIDED.
2. THE CURB RAMP TYPES SHALL MEET THE FOLLOWING REQUIREMENTS:
 - A. TYPE A AND A1 CURB RAMPS ARE LOCATED WITHIN THE CURB RETURN WITH A RAMP THAT HAS A RUNNING SLOPE OF 5% OR GREATER, AND SIDE APPROACH RUNNING SLOPES BELOW 5%.
 - B. TYPE B CURB RAMPS ARE LOCATED OUTSIDE THE CURB RETURN WITH A RAMP THAT HAS A RUNNING SLOPE OF 5% OR GREATER, AND SIDE APPROACH RUNNING SLOPES BELOW 5%.
 - C. TYPE C1, C2, AND C3 CURB RAMPS HAVE SIDE APPROACH RUNNING SLOPES OF 5% OR GREATER.
 - D. TYPE D CURB RAMPS ARE LOCATED AT ALLEY CORNERS.
 - E. TYPE E ARE BLENDED TRANSITIONS WITH A RAMP RUN SLOPE BELOW 5%.
3. OPPOSING CURB RAMPS AT A SINGLE CROSSING SHALL LINE UP. ALIGN THE CURB RAMP WITH THE CROSSWALK SO THERE IS A STRAIGHT PATH OF TRAVEL FROM THE TOP OF THE RAMP TO THE CURB RAMP ON THE OTHER SIDE, TO THE MAXIMUM EXTENT FEASIBLE.
4. PULL BOXES, MANHOLES, VAULTS, AND OTHER UTILITIES SHALL BE RELOCATED OR INCORPORATED ONTO THE CURB RAMP AREA PROVIDED THAT THE ACCESS COVER IS STABLE, FIRM, SLIP RESISTANT, AND FLUSH OR ADJUSTED TO GRADE. COORDINATE THE WORK WITH THE ENGINEER
5. UTILITY POLES MAY BE INCORPORATED INTO THE FLARES OF THE CURB RAMP PROVIDED THAT THE REQUIRED ACCESSIBLE ROUTE WIDTH IS COMPLIANT.
6. RETAINING CURBS OR WALLS:
 - A. INSTALL A RETAINING CURB/WALL AT THE BACK OF THE CURB RAMP IF NOT ABLE TO GRADE EXISTING SLOPE AT A 3:1 RATIO OR FLATTER WITHIN THE ROW.
 - B. THE FRONT OF THE RETAINING CURB/WALL SHALL BE IN LINE WITH THE BACK OF THE SIDEWALK. TAPER THE BACK OF THE SIDEWALK WITHIN THE 4'-0" TRANSITION AREA OR TO THE NEXT JOINT, WHICHEVER IS GREATER.
 - C. IN LIEU OF A FLARE, A RETAINING CURB WILL BE ALLOWED IF IT IS IMPRACTICAL TO REMOVE THE EXISTING OBSTRUCTION, SUCH AS A CURB INLET, UTILITY INFRASTRUCTURE, A SIGNIFICANT OR REGISTERED TREE, OR THE TREE CANNOT BE TRANSPLANTED.
 - D. IF PEDESTRIANS ARE ABLE TO ACCESS THE PAVED AREA DIRECTLY BEHIND THE RETAINING CURB, INSTALL A PROTECTIVE RAILING PER **SDG-140**.
7. GRADE BREAKS AT THE TOP AND BOTTOM OF THE RAMPS AND CURB RAMPS SHALL BE PERPENDICULAR TO THE DIRECTION OF THE RAMP RUN. GRADE BREAKS SHALL NOT BE PERMITTED ON THE SURFACE OF RAMP RUNS AND TURNING SPACES. SURFACE SLOPES THAT MEET AT GRADE BREAKS SHALL BE FLUSH TO DELINEATE THE CURB RAMP.
8. PROVIDE A 1/2" DEEP TOOLED JOINT WITH 1/8" RADIUS EDGES AS SHOWN ON DRAWINGS.
9. INSTALL A 1/4" EXPANSION JOINT FILLER BETWEEN THE NEW CURB RAMP GUTTER AND THE EXISTING SIDEWALK.
10. PONDING IS NOT ALLOWED WITHIN THE CURB RAMP LIMITS, AND THE DRAINAGE PATTERN SHALL NOT BE ALTERED.
11. THE ADJUSTMENT OF THE CROSS SLOPE AT THE RAMP OPENING SHALL NOT CAUSE GUTTER TRICKLE FLOW TO SPILL ONTO TRAVELLED LANES OR PONDING ANYWHERE.
12. TRANSITIONS FROM RAMPS TO WALKS AND SIDEWALK GUTTER OR STREET SURFACE SHALL BE FLUSH AND AT THE SAME LEVEL. PAVEMENT AT THE STREET SURFACE SHALL BE MILLED TO ACHIEVE FLUSH CONDITION.

SHEET 1 OF 3

REVISION	BY	APPROVED	DATE	CITY OF SAN DIEGO - STANDARD DRAWING	RECOMMENDED BY THE CITY OF SAN DIEGO STANDARDS COMMITTEE
ORIGINAL	SS	A. OSKOUJ	12/03		
UPDATED	FC	J.NAGELVOORT	06/18	GENERAL CURB RAMP NOTES	<i>Alaine James</i> 3/28/2024 COORDINATOR R.C.E. 81047 DATE
UPDATED	FC	J.NAGELVOORT	09/18		
UPDATED	HN	J.NAGELVOORT	04/19		
UPDATED	FC	J.NAGELVOORT	09/20		
UPDATED	FM	R. AMEN	03/24		
UPDATED	FM	R. AMEN	03/24		
				DRAWING NUMBER	SDG-131

NOTES:

13. THE REMOVAL OF EXISTING PAVEMENT, CONCRETE CURB, GUTTER, SIDEWALK, AND EXISTING CURB RAMP FOR THE INSTALLATION OF A NEW CURB RAMP SHALL COMPLY WITH **SDG-156**.
14. DIAGONAL CURB RAMPS WITH FLARED SIDES SHALL HAVE A SEGMENT OF CURB 2'-0" LONG MINIMUM LOCATED ON EACH SIDE OF THE CURB RAMP AND WITHIN THE MARKED CROSSING. SEE SINGLE CURB RAMP CORNER DETAIL BELOW.



SINGLE CURB RAMP CORNER

15. PERPENDICULAR CURB RAMPS (TYPE A, TYPE A1, TYPE B, TYPE C2 AND TYPE C3) SHALL HAVE A 4'-0" MINIMUM CLEAR SPACE BEYOND THE BOTTOM OF THE GRADE BREAK WITHIN THE PEDESTRIAN STREET CROSSING AND WHOLLY OUTSIDE THE TRAFFIC LANES OF THE ROADWAY (VEHICULAR AND BIKE LANES). SEE SINGLE CURB RAMP CORNER DETAIL IN NOTE 14.
16. CURB RAMP AND FORM WORK SLOPES SHALL BE CHECKED WITH A 2-FOOT CALIBRATED DIGITAL LEVEL. NO PORTION OF A RAMP RUN SHALL EXCEED THE MAXIMUM SLOPE REQUIREMENT.
17. CONTRACTOR SHALL PROVIDE AN EMBEDDED STAMP ON TOP OF THE CURB AT THE OPENING OF THE CURB RAMP. THE STAMP SHALL BE 1/4" DEEP AND 3/4" TO 1" HIGH, AND SHALL ONLY INCLUDE THE NAME OF THE CONTRACTOR AND THE YEAR OF INSTALLATION. THE STAMP SHALL BE COMPLETELY FLUSH WITH THE CURB SURFACE.
18. THE SLOPE OF THE RAMP SHALL BE UNIFORM ALONG EACH RAMP RUN.
19. THE CROSS SLOPE OF THE RAMP SHALL BE MEASURED PERPENDICULARLY TO THE PATH OR DIRECTION OF TRAVEL.
20. ANY DEVIATION FROM THESE PROVISIONS REQUIRES PRIOR APPROVAL FROM THE ENGINEER.
21. CONCRETE SHALL BE 520-C-2500 (MIN).
22. THE EXISTING CONCRETE SPANDREL OF A CROSS GUTTER SHALL BE REMOVED AND REPLACED IN ITS ENTIRETY AS PART OF THE CURB RAMP INSTALLATION.
23. THE EXISTING CONCRETE ALLEY APRON SHALL BE REMOVED AND REPLACED IN ITS ENTIRETY AS PART OF THE CURB RAMP INSTALLATION.
24. TRANSITION AREA(S) ARE REQUIRED IF THE CURB RAMP IMPROVEMENTS TIE INTO AN EXISTING SIDEWALK CROSS SLOPE EXCEEDING 2%. FOR CURB RAMP TYPES C1, C2, C3, AND D, IF THE SIDE APPROACH RAMP SLOPE EXCEEDS 5%, THEN EXTEND THE TRANSITION AREA AN ADDITIONAL 4'-0" LONG MINIMUM WITH A 1.5% CROSS SLOPE (2% MAXIMUM). THEREFORE, (2) 4'-0" LONG MINIMUM SIDEWALK PANELS SHALL BE INSTALLED.

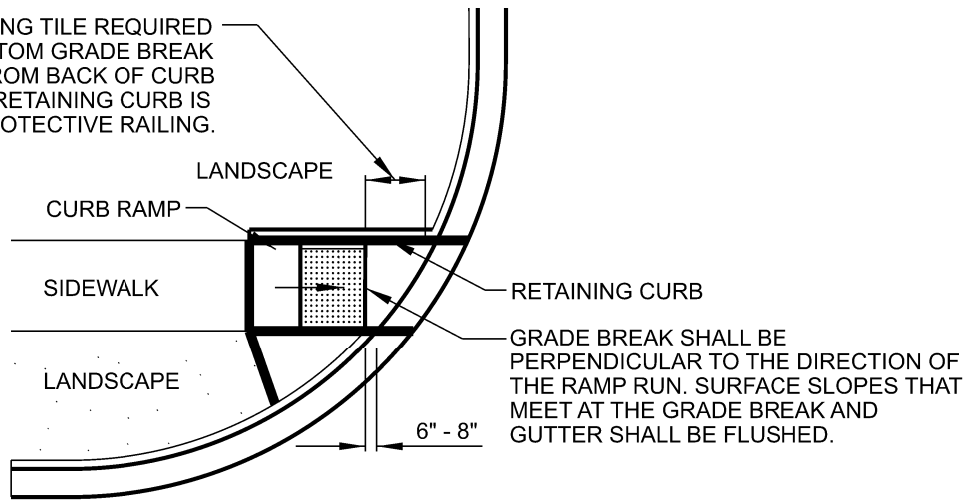
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NOTES:

25. DETECTABLE WARNING TILE SHALL BE LOCATED SO THE EDGE NEAREST THE CURB IS 6" MINIMUM AND 8" MAXIMUM FROM THE LINE AT THE FACE OF THE CURB.

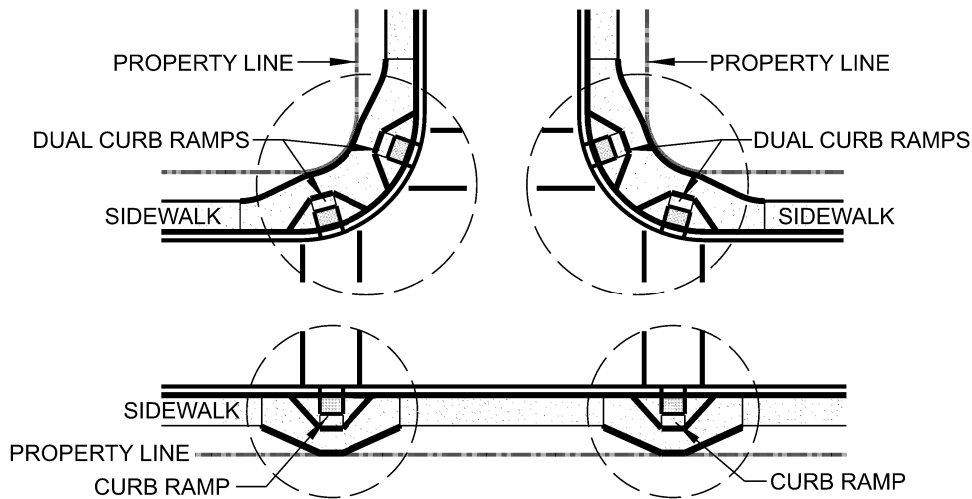
SEE DETAIL BELOW FOR EXCEPTION.

NO DETECTABLE WARNING TILE REQUIRED WHEN BOTH ENDS OF THE BOTTOM GRADE BREAK ARE LESS THAN 5'-0" FROM BACK OF CURB AND ACCESS ACROSS THE RETAINING CURB IS PREVENTED BY LANDSCAPE OR PROTECTIVE RAILING.



DETAIL - NOTE 25
NOT TO SCALE

26. CURB RAMPS SHALL BE INSTALLED AT ALL PEDESTRIAN CROSSINGS AT INTERSECTIONS, INCLUDING AT T-INTERSECTIONS. SEE DETAIL BELOW.



DETAIL - NOTE 26
NOT TO SCALE

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CITY OF SAN DIEGO – STANDARD DRAWING

GENERAL CURB RAMP NOTES

RECOMMENDED BY THE CITY OF SAN DIEGO STANDARDS COMMITTEE

Alaine James 3/28/2024
COORDINATOR R.C.E. 81047 DATE

DRAWING NUMBER **SDG-131**