



SAN YSIDRO PARKING STUDY

APRIL 2026

Prepared by:



TRANSPORTATION

**CITY OF SAN DIEGO
DEPARTMENT OF TRANSPORTATION**

San Ysidro Parking Study

West San Ysidro Blvd and Border Village District

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Study Date:

February 26, 2026 & February 28, 2026

Report Issued:

April 2026

Study Area:

West San Ysidro Boulevard corridor between Alverson Road and Cypress Drive, Border Village District which encompasses East San Ysidro Boulevard, Border Village Road, Bolton Hall Road, Virginia Avenue, Louisiana Avenue, and Front Street located east of Interstate 5. All study areas are located within the San Ysidro Community Parking District, San Ysidro Parking Meter Zone, and San Ysidro Business Improvement District encompassing a variety of commercial uses and multimodal transit access.

Purpose:

To assess current parking demand, evaluate parking occupancy and duration, and identify opportunities for improved parking management strategies—including metered and time-limited parking—to support business access and transportation planning.

Key Recommendations:

- Implement 2-hour metered parking in Border Village District of San Ysidro per the study at hand and the previous recommendations of the 2023 Parking Study.
- Improve Existing Parking Designations
- Provide Clarity in Parking Signs and Markings
- Continued Enforcement & Community Collaboration

Table of Contents

Chapter 1.....	4
Study Area.....	4
Chapter 2.....	5
Data Collection.....	5
Findings.....	15
Chapter 3.....	17
Recommendations & Next Steps.....	17
Appendix A – San Ysidro Parking Study September 2023 (Prepared by CR Associates)	20

Chapter 1

Study Area

The City of San Diego prepared a parking study along commercial corridors in San Ysidro to understand existing parking demand and to consider effective management of on-street parking within the community. The segments studied fall within the San Ysidro Community Parking District and Parking Meter zone as well as the San Ysidro Business Improvement District. Figure 1 depicts the study segments and the boundary of the community parking district (Parking Meter Zone). Properties along the studied areas are zoned community commercial and have the following businesses along the corridor:

- Shoe and clothing retail
- Fast-food retail
- Grocery stores
- Banks
- Nutrition stores
- Auto repair shops
- Tax preparation services.
- Barbershops
- Hair salons
- Car insurance businesses
- Several money exchange businesses

The majority of the studied segments have 2-hour time-limited parking restriction on-street from 8am-6pm, where Sundays and certain holidays are excluded. Portions of West San Ysidro Boulevard and East San Ysidro Boulevard are designated as a class II bicycle facility in the San Ysidro Community Plan; however, currently, no bike lane markings exist. The Metropolitan Transit System (MTS) operates bus routes 906 & 907 along West San Ysidro Boulevard and East San Ysidro Boulevard connecting residents, employees and visitors from the port of entry to the Iris Transit Center.

It is important to note that several of the studied roadways were previously studied by CR Associates in September 2023, based on which the San Ysidro Community Parking District and Parking Meter Zone were established. Results and recommendations from the previous study can be found in Appendix A.

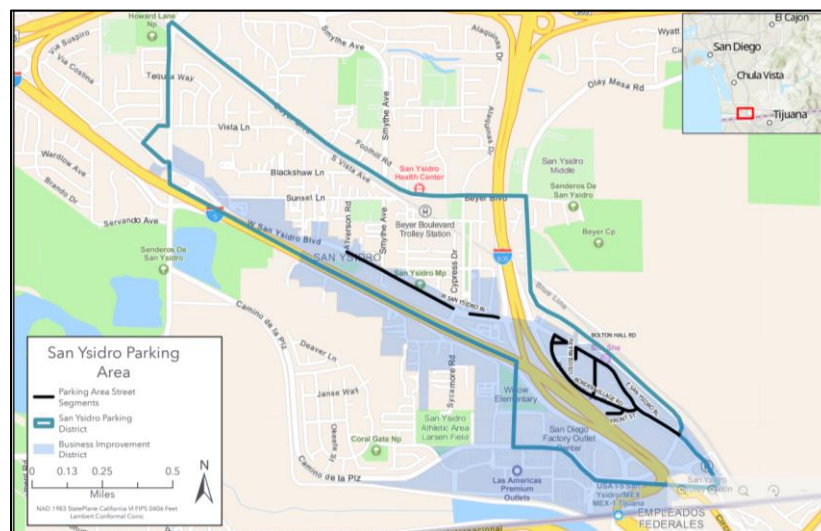


Figure 1 – San Ysidro Parking Study Street Segments

Chapter 2

Data Collection

The study included parking occupancy and duration data for over 370 on-street parking spaces along West San Ysidro Boulevard, and several streets in the Border Village District of the community. Parking data was collected between the hours of 8am-6pm on Thursday, February 26, 2026, and Saturday, February 28, 2026.

Parking occupancy refers to the percentage of available parking spaces that are occupied. It is a key metric in parking management that is used to understand parking demand and is helpful in optimizing space utilization. Average occupancy for each studied segment is shown below. Unregulated street segments such as: Front Street, Louisiana Street, and Virginia Avenue, had the highest parking occupancy.

Table 2-1a & Table 2-1b indicate the average parking occupancy for unregulated and regulated studied segments. Regulated segments are segments with 2-hour time limit parking signs. Unregulated segments have no parking restriction currently in place.

Average parking occupancy for unregulated street segments collectively is nearly double the average parking occupancy for regulated segments. Figure 2 depicts average occupancy for each segment studied.

Street Name	Segment	Side	Parking Spaces*	Avg. Occupancy
West San Ysidro Blvd	Smythe Ave - Cottonwood Rd	S	14	91%
East San Ysidro Blvd	Border Village Rd - Bolton Hall Rd	N	1	10%
Front St	Border Village Rd - Cul De Sac	N	2	95%
		S	7	79%
Louisiana Avenue	E San Ysidro Blvd - Cul De Sac	E	1	0%
		W	3	90%
Virginia Avenue	E San Ysidro Blvd - Border Village Rd	E	17	93%
		W	23	87%
East San Ysidro Blvd	Willow Rd - E Park Ave	S	26	77%

Table 2-1a: Average Parking Occupancy for Unregulated Segments – Thursday, February 26, 2026

Street Name	Segment	Side	Parking Spaces*	Avg. Occupancy
West San Ysidro Blvd	Alverson Rd - Smythe Ave	N	23	10%
		S	11	20%
West San Ysidro Blvd	Smythe Ave - Cottonwood Rd	N	12	46%
West San Ysidro Blvd	Cottonwood Rd - Cypress Dr	N	18	83%
		S	7	91%
East San Ysidro Blvd	Louisiana Ave - Virginia Ave	N	-	-
		S	3	3%
East San Ysidro Blvd	Virginia Ave - Border Village Dr (S)	N	25	44%
		S	28	46%
East San Ysidro Blvd	Border Village Rd (S) - Camino De La Plaza	N	-	-
		S	10	62%
Border Village Rd	E San Ysidro Blvd (S) - Virginia Ave	N	24	49%
		S	14	21%
Border Village Rd	E San Ysidro Blvd (N) - Virginia Ave	N	44	24%
		S	56	16%
Bolton Hall Rd	E San Ysidro Blvd - Border Village Rd	N	-	-
		S	4	43%

Table 2-1b: Average Parking Occupancy for Regulated Segments – Thursday, February 26, 2026

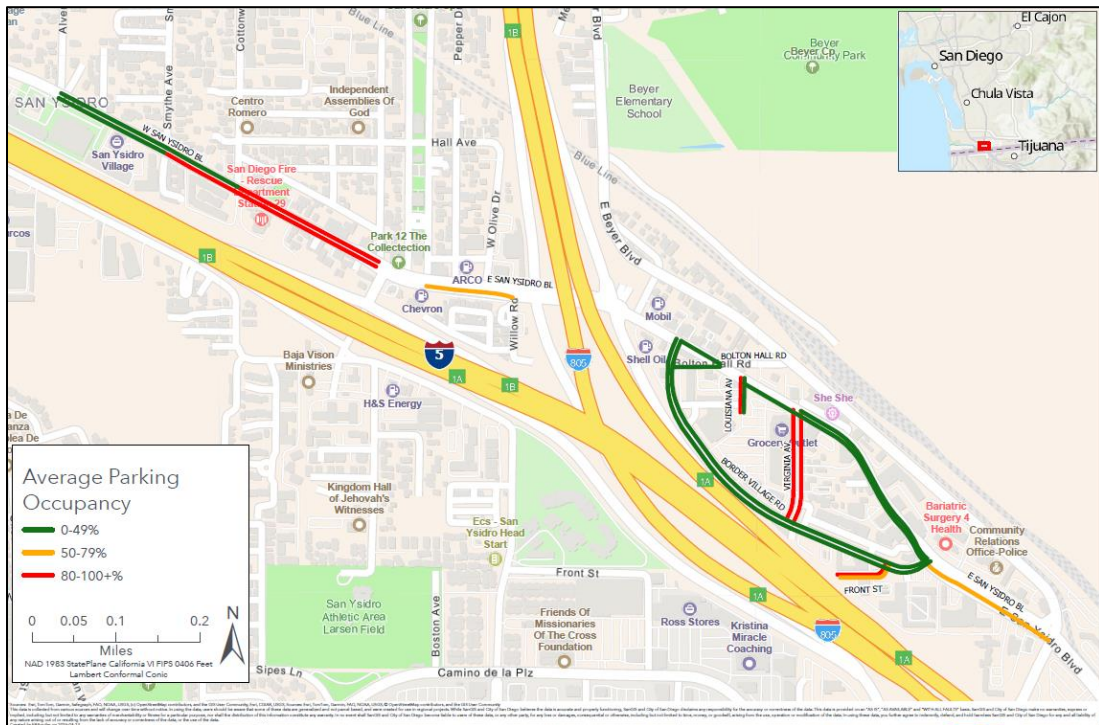


Figure 2: Average Parking Occupancy, Thursday, February 26, 2026

It is helpful to understand parking demand along each study segment by hour as parking demand often fluctuates due to fronting businesses, off street parking availability, and other characteristics.

The peak hour is the highest hour of parking occupancy across the segment. Table 2-2a & Table 2-2b show the highest parking occupancy and the peak hour in which it was first reached for unregulated and regulated segments.

Overall, more than half of the studied segments showed a peak occupancy exceeding 80%. In unregulated segments, the first hour in which peak occupancy was achieved was in the first study hour (8-9AM) due to many parked vehicles not moving throughout the day.

Street Name	Segment	Side	Peak Hour	Peak Occupancy
West San Ysidro Blvd	Smythe Ave - Cottonwood Rd	S	9-10AM	93%
East San Ysidro Blvd	Border Village Rd - Bolton Hall Rd	N	3-4PM	100%
Front St	Border Village Rd - Cul De Sac	N	8-9AM	100%
		S	8-9AM	86%
Louisiana Avenue	E San Ysidro Blvd - Cul De Sac	E	N/A	0%
		W	8-9AM	100%
Virginia Avenue	E San Ysidro Blvd - Border Village Rd	E	2-3PM	106%
		W	8-9AM	91%
East San Ysidro Blvd	Willow Rd - E Park Ave	S	3-4PM	81%

Table 2-2a: Peak Parking Occupancy for Unregulated Segments – Thursday, February 26, 2026

Street Name	Segment	Side	Peak Hour	Peak Occupancy
West San Ysidro Blvd	Alverson Rd - Smythe Ave	N	5-6PM	35%
		S	8-9AM	27%
West San Ysidro Blvd	Smythe Ave - Cottonwood Rd	N	4-5PM	83%
West San Ysidro Blvd	Cottonwood Rd - Cypress Dr	N	8-9AM	94%
West San Ysidro Blvd	Cottonwood Rd - Cypress Dr	S	8-9AM	100%
East San Ysidro Blvd	Louisiana Ave - Virginia Ave	N	-	-
		S	8-9AM	33%
East San Ysidro Blvd	Virginia Ave - Border Village Dr (S)	N	1-2PM	60%
		S	3-4PM	68%
East San Ysidro Blvd	Border Village Rd (S) - Camino De La Plaza	N	-	-
		S	3-4PM	90%
Border Village Rd	E San Ysidro Blvd (S) - Virginia Ave	N	10-11AM	58%
		S	5-6PM	57%
Border Village Rd	E San Ysidro Blvd (N) - Virginia Ave	N	11AM-12PM	30%
		S	8-9AM	21%
Bolton Hall Rd	E San Ysidro Blvd - Border Village Rd	N	-	-
		S	3-4PM	100%

Table 2-2b: Peak Parking Occupancy for Regulated Segments – Thursday, February 26, 2026

Parking duration is a widely used metric for understanding parking demand. It is important to understand how many of the available parking spaces are occupied just as important is understanding how long the parked vehicle occupies the parking space. Lower duration per parker leads to more turnover on the existing parking spaces, which means spaces are being used effectively as more vehicles can park in the same spaces. Along commercial corridors, it is important to have lower parking duration so that multiple patrons can visit fronting businesses.

Table 2-3a & Table 2-3b show the average parking duration for unregulated and regulated study segments. The average parking duration for regulated study segments was 2.6 hours, which is slightly higher than its current 2-hour parking time limit.

Conversely, the average parking duration for unregulated segments was nearly double at 5.0 hours, which is considered low parking turnover for businesses. The highest parking duration was observed along the west side of Louisiana Avenue at 9 hours; however, this side is currently unregulated, which may be why two of the three parked vehicles did not move for the entire 10-hour study period.

Average duration greater than 2 hour indicates a need for better enforcement in regulated segments. Figure 3 depicts the average duration along each segment.

Street Name	Segment	Side	Parking Spaces*	Avg. Duration (hours)
West San Ysidro Blvd	Smythe Ave - Cottonwood Rd	S	14	5.6
East San Ysidro Blvd	Border Village Rd - Bolton Hall Rd	N	1	1.0
Front St	Border Village Rd - Cul De Sac	N	2	6.3
		S	7	5.5
Louisiana Avenue	E San Ysidro Blvd - Cul De Sac	E	1	0.0
		W	3	9.0
Virginia Avenue	E San Ysidro Blvd - Border Village Rd	E	17	5.9
		W	23	8.7
East San Ysidro Blvd	Willow Rd - E Park Ave	S	26	5.0

Table 2-3a: Average Parking Duration for Unregulated Segments – Thursday, February 26, 2026

Street Name	Segment	Side	Parking Spaces*	Avg. Duration (hours)
West San Ysidro Blvd	Alverson Rd - Smythe Ave	N	23	1.7
		S	10	2.4
West San Ysidro Blvd	Smythe Ave - Cottonwood Rd	N	12	2.6
West San Ysidro Blvd	Cottonwood Rd - Cypress Dr	N	18	3.9
		S	7	4.9
East San Ysidro Blvd	Louisiana Ave - Virginia Ave	N	-	-
		S	3	1.0
East San Ysidro Blvd	Virginia Ave - Border Village Dr (S)	N	25	3.0
		S	28	1.9
East San Ysidro Blvd	Border Village Rd (S) - Camino De La Plaza	N	-	-
		S	10	2.5
Border Village Rd	E San Ysidro Blvd (S) - Virginia Ave	N	24	4.2
		S	14	2.5
Border Village Rd	E San Ysidro Blvd (N) - Virginia Ave	N	44	4.6
		S	56	2.5
Bolton Hall Rd	E San Ysidro Blvd - Border Village Rd	N	-	-
		S	4	1.0

Table 2-3b: Average Parking Duration for Regulated Segments – Thursday, February 26, 2026

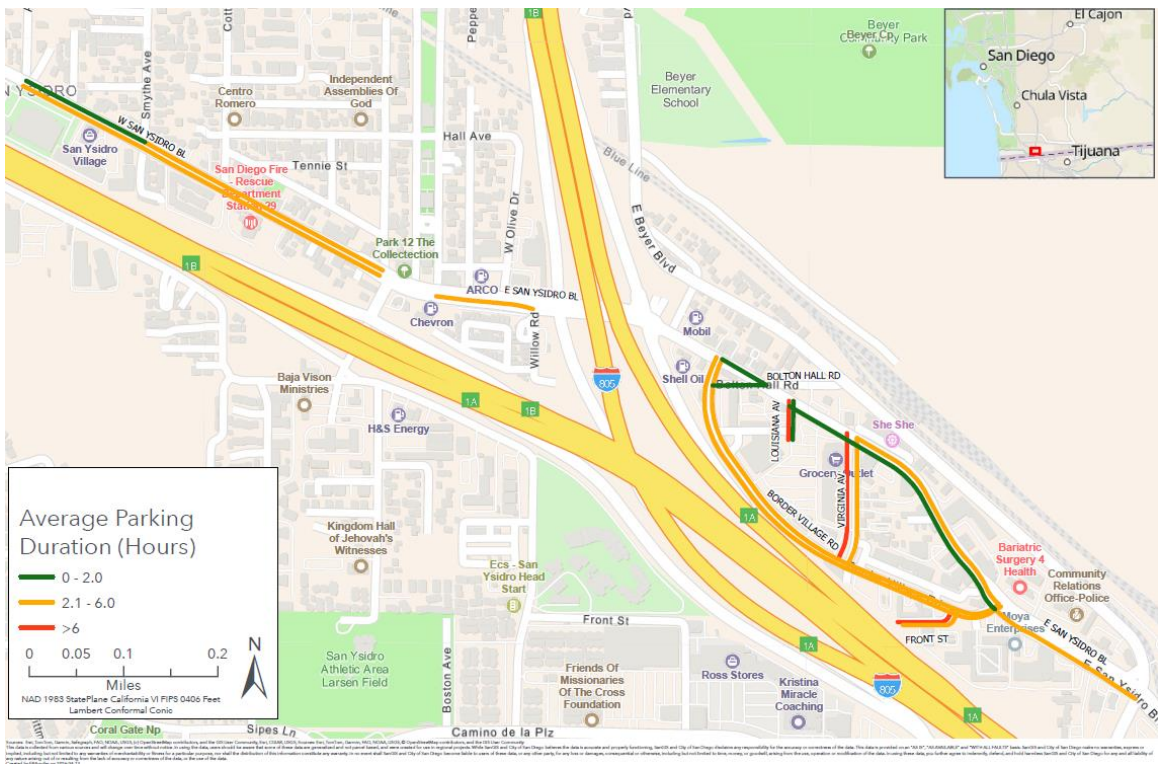


Figure 3: Average Parking Duration – Thursday, February 26, 2026

Average parking occupancy observed on Saturday’s data collection resulted in 49.0% average occupancy. As with Thursday’s data sample, unregulated street segments such as: Front Street and Virginia Avenue had the highest parking occupancy.

Table 2-4a and Table 2-4b indicate the average parking occupancy for unregulated and regulated studied segments. Figure 4 depicts average parking occupancy for each segment studied.

Street Name	Segment	Side	Parking Spaces*	Avg. Occupancy
West San Ysidro Blvd	Alverson Rd - Smythe Ave	N	23	12%
		S	11	17%
West San Ysidro Blvd	Smythe Ave - Cottonwood Rd	S	14	87%
West San Ysidro Blvd	Cottonwood Rd - Cypress Dr	S	7	100%
Front St	Border Village Rd - Cul De Sac	N	2	100%
		S	7	104%
Louisiana Avenue	E San Ysidro Blvd - Cul De Sac	E	1	0%
		W	3	67%
Virginia Avenue	E San Ysidro Blvd - Border Village Rd	E	17	105%
		W	23	93%
East San Ysidro Blvd	Willow Rd - E Park Ave	S	26	59%

Table 2-4a: Average Parking Occupancy for Unregulated Segments – Saturday, February 28, 2026

Street Name	Segment	Side	Parking Spaces*	Avg. Occupancy
West San Ysidro Blvd	Smythe Ave - Cottonwood Rd	N	12	78%
West San Ysidro Blvd	Cottonwood Rd - Cypress Dr	N	18	83%
East San Ysidro Blvd	Border Village Rd - Bolton Hall Rd	N	1	10%
		S	-	-
East San Ysidro Blvd	Louisiana Ave - Virginia Ave	N	-	-
		S	3	0%
East San Ysidro Blvd	Virginia Ave - Border Village Dr (S)	N	25	32%
		S	28	43%
East San Ysidro Blvd	Border Village Rd (S) - Camino De La Plaza	N	-	-
		S	10	66%
Border Village Rd	E San Ysidro Blvd (S) - Virginia Ave	N	24	45%
		S	14	32%
Border Village Rd	E San Ysidro Blvd (N) - Virginia Ave	N	44	30%
		S	56	21%
Bolton Hall Rd	E San Ysidro Blvd - Border Village Rd	N	-	-
		S	4	35%

Table 2-4b: Average Parking Occupancy for Regulated Segments – Saturday, February 28, 2026

Table 2-5a and Table 2-5b show the highest parking occupancy and the peak hour in which it was first reached for unregulated and regulated study segments.

Street Name	Segment	Side	Peak Hour	Peak Occupancy
West San Ysidro Blvd	Alverson Rd - Smythe Ave	N	8-9AM	22%
		S	9-10AM	27%
West San Ysidro Blvd	Smythe Ave - Cottonwood Rd	S	8-9AM	93%
West San Ysidro Blvd	Cottonwood Rd - Cypress Dr	S	10-11AM	114%
Front St	Border Village Rd - Cul De Sac	N	8-9AM	100%
		S	9-10AM	100%
Louisiana Avenue	E San Ysidro Blvd - Cul De Sac	E	N/A	0%
		W	8-9AM	67%
Virginia Avenue	E San Ysidro Blvd - Border Village Rd	E	8-9AM	106%
		W	8-9AM	96%
East San Ysidro Blvd	Willow Rd - E Park Ave	S	8-9AM	67%

Table 2-5a: Peak Parking Occupancy for Unregulated Segments - Saturday, February 28, 2026

Street Name	Segment	Side	Peak Hour	Peak Occupancy
West San Ysidro Blvd	Smythe Ave - Cottonwood Rd	N	8-9AM	83%
West San Ysidro Blvd	Cottonwood Rd - Cypress Dr	N	8-9AM	100%
East San Ysidro Blvd	Border Village Rd - Bolton Hall Rd	N	5-6PM	100%
		S		-
East San Ysidro Blvd	Louisiana Ave - Virginia Ave	N		-
		S	N/A	0%
East San Ysidro Blvd	Virginia Ave - Border Village Dr (S)	N	9-10AM	44%
		S	5-6PM	75%
East San Ysidro Blvd	Border Village Rd (S) - Camino De La Plaza	N	9-10AM	-
		S	3-4PM	90%
Border Village Rd	E San Ysidro Blvd (S) - Virginia Ave	N	12-1PM	58%
		S	5-6PM	57%
Border Village Rd	E San Ysidro Blvd (N) - Virginia Ave	N	12-1PM	41%
		S	8-9AM	25%
Bolton Hall Rd	E San Ysidro Blvd - Border Village Rd	N		-
		S	12-1PM	75%

Table 2-5b: Peak Parking Occupancy for Unregulated Segments - Saturday, February 28, 2026

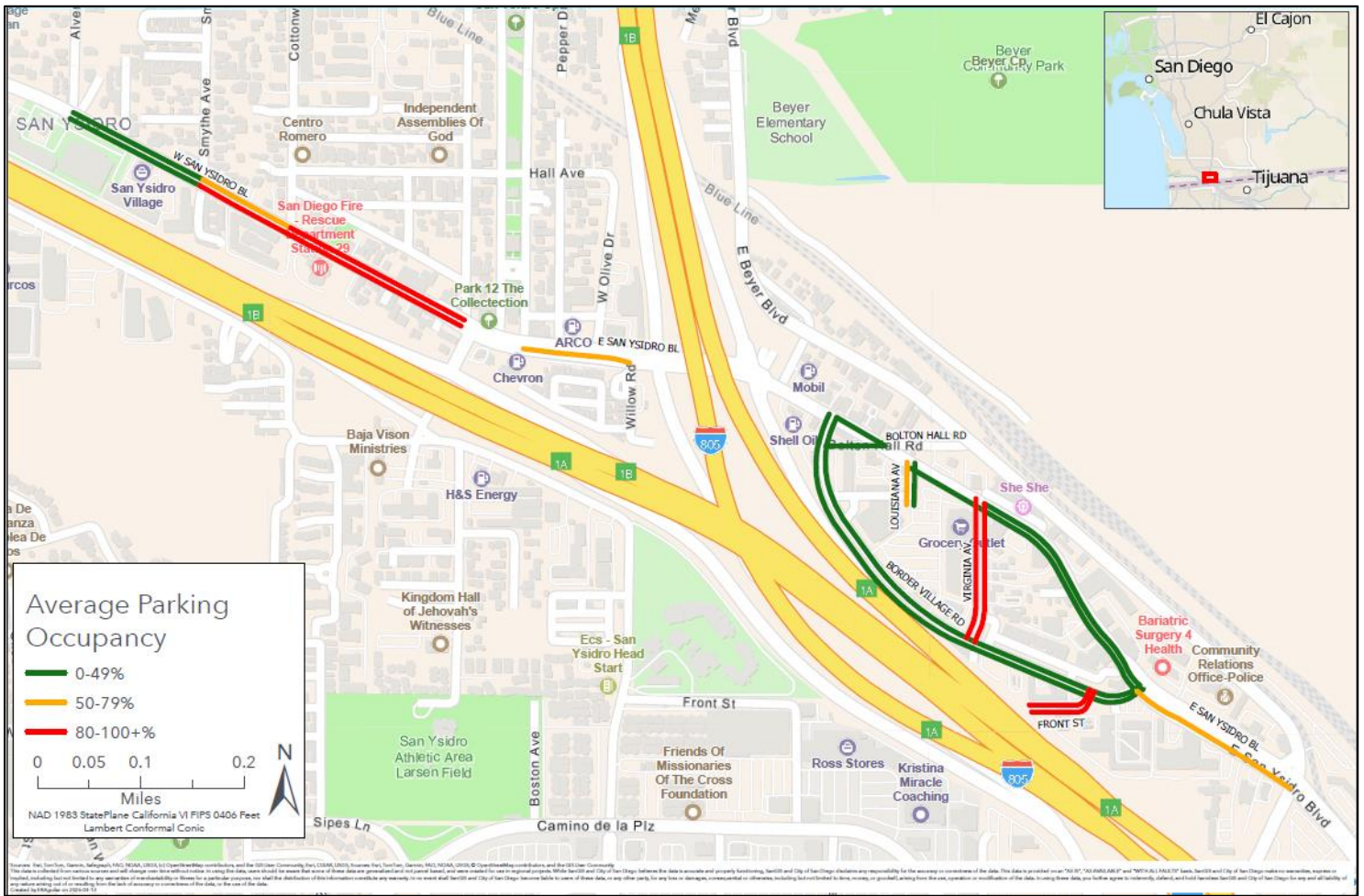


Figure 4: Average Parking Occupancy – Saturday, February 28, 2026

Table 2-6 shows the average parking duration per study segment. The average parking duration for all study segments on Saturday was 3.8 hours, which is similar to Thursday’s observed average duration. The highest parking duration was observed along the east side of Virginia Avenue at 9.4 hours; however, this side is currently unregulated, which may be why nearly all parked vehicles did not move for the entire 10-hour study period.

Conversely, the segment with the lowest average parking duration observed were in regulated street segments of East San Ysidro Boulevard and Bolton Hall Road. As anticipated, the average parking duration nearly doubled for unregulated street segments when compared to regulated street segments. Figure 5 depicts the average parking duration along each segment.

Street Name	Segment	Side	Parking Spaces*	Avg. Duration (hours)
West San Ysidro Blvd	Alverson Rd - Smythe Ave	N	23	2.1
		S	11	1.9
West San Ysidro Blvd	Smythe Ave - Cottonwood Rd	S	14	4.1
West San Ysidro Blvd	Cottonwood Rd - Cypress Dr	S	7	4.7
Front St	Border Village Rd - Cul De Sac	N	2	6.7
		S	7	8.1
Louisiana Avenue	E San Ysidro Blvd - Cul De Sac	E	1	0.0
		W	3	6.7
Virginia Avenue	E San Ysidro Blvd - Border Village Rd	E	17	9.4
		W	23	7.6
East San Ysidro Blvd	Willow Rd - E Park Ave	S	26	4.9

Table 2-6a: Average Parking Duration for Unregulated Segments - Saturday, February 28, 2026

Street Name	Segment	Side	Parking Spaces*	Avg Duration (hours)
West San Ysidro Blvd	Smythe Ave - Cottonwood Rd	N	12	5.5
West San Ysidro Blvd	Cottonwood Rd - Cypress Dr	N	18	3.9
East San Ysidro Blvd	Border Village Rd - Bolton Hall Rd	N	1	1.0
		S	-	-
East San Ysidro Blvd	Louisiana Ave - Virginia Ave	N	-	-
		S	3	0.0
East San Ysidro Blvd	Virginia Ave - Border Village Dr (S)	N	25	2.3
		S	28	1.8
East San Ysidro Blvd	Border Village Rd (S) - Camino De La Plaza	N	-	-
		S	10	2.9
Border Village Rd	E San Ysidro Blvd (S) - Virginia Ave	N	24	2.8
		S	14	2.5
Border Village Rd	E San Ysidro Blvd (N) - Virginia Ave	N	44	4.1
		S	56	3.1
Bolton Hall Rd	E San Ysidro Blvd - Border Village Rd	N	-	-
		S	4	1.0

Table 2-6b: Average Parking Duration for Regulated Segments - Saturday, February 28, 2026

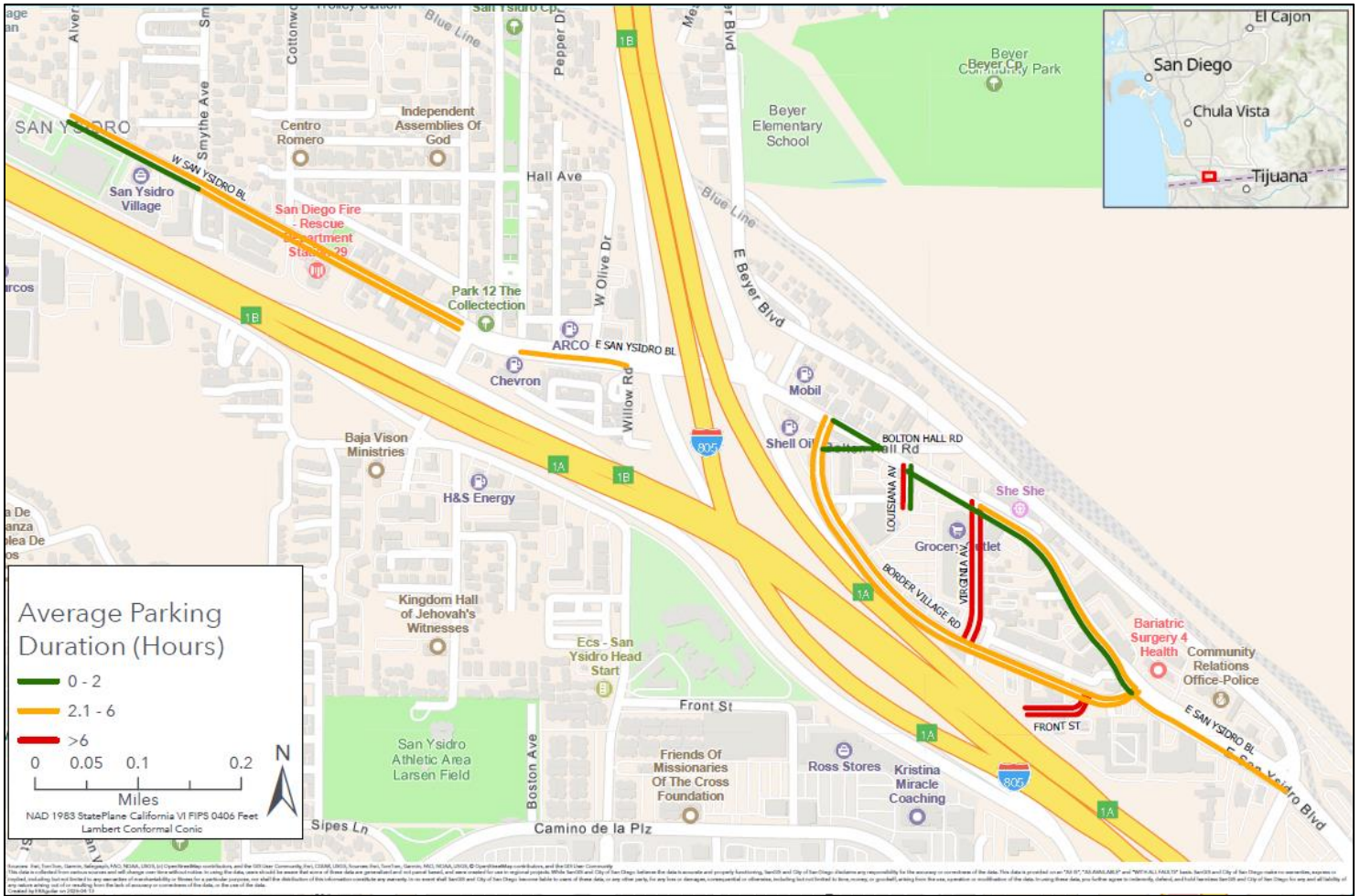


Figure 5: Average Parking Duration - Saturday, February 28, 2026

Findings

Parking utilization appeared to be relatively similar between the weekday (Thursday) and weekend (Saturday). Average occupancy increased slightly on Saturday from 47% to 49% and average duration also slightly increased from 3.7 hours on Thursday to 3.8 hours on Saturday.

Business corridors in the City of San Diego typically have time limited parking to increase turnover and allow for more people to visit nearby businesses. Several vehicles were found to have been parked during the entire 10 hours that data was collected, showing a need for time-limited parking.

It is important to note that several off-street lots near the studied street segments offer long-term parking. Utilization of these lots were studied in the previous parking study and can be found in Appendix A. These parking spaces are available for visitors that are looking to park their vehicles for long periods of time, which is common near the San Ysidro border crossing.

The highest average parking occupancy and duration were experienced along Virginia Avenue, Louisiana Avenue, Front Street and the south side of West San Ysidro Boulevard, between Cypress Drive and Cottonwood Road, which are unregulated.

East San Ysidro Boulevard between Virginia Avenue and Border Village Drive (S) had the most vehicles violating the existing 2-hour parking time limit followed by Border Village Road. The table below shows the number of vehicles violating the existing 2-hour time limit restriction for each study date. Over one-third of vehicles violated the 2-hour time limit restriction on each study date.

Study Day	# of Vehicles Violating Time Limit Restriction	% of Vehicles Violating Time Limit Restriction
Thursday, February 26, 2026	96	35%
Saturday, February 28, 2026	107	38%

Table of Vehicles Violating Existing 2 Hour Parking Regulation by Study Date

The subsequent table on the next page provides the number of vehicles violating the existing 2-hour parking restriction per studied street segment. It is also important to note that data showed several vehicles were observed to have remained parked in the same exact on-street parking space on Thursday and Saturday. It is uncertain if these vehicles ever moved from Thursday to Saturday.

Street Name	Segment	Side	Parking Spaces*	# of Vehicles Violating 2 Hr Time Limit Restriction	
				Thursday	Saturday
West San Ysidro Blvd	Alverson Rd - Smythe Ave	N	N	2	5
		S	S	2	5
West San Ysidro Blvd	Smythe Ave - Cottonwood Rd	N	N	6	6
West San Ysidro Blvd	Cottonwood Rd - Cypress Dr	N	N	6	6
		S	S	7	6
East San Ysidro Blvd	Louisiana Ave - Virginia Ave	N	N	-	-
		S	S	0	0
East San Ysidro Blvd	Virginia Ave - Border Village Dr (S)	N	N	19	13
		S	S	11	12
East San Ysidro Blvd	Border Village Rd (S) - Camino De La Plaza	N	N	-	-
		S	S	13	12
Border Village Rd	E San Ysidro Blvd (S) - Virginia Ave	N	N	15	12
		S	S	4	7
Border Village Rd	E San Ysidro Blvd (N) - Virginia Ave	N	N	13	15
		S	S	8	12
Bolton Hall Rd	E San Ysidro Blvd - Border Village Rd	N	N	-	-
		S	S	0	0

Table of Number of Vehicles Violating Existing 2 Hour Parking Regulation by Study Segment

The segments with the highest parking duration were Louisiana Avenue, Virginia Avenue and Front Street, all of which currently do not have any time limit parking restrictions. For studied segments with existing 2-hour time limit regulations, all but one segment along Bolton Hall Road and two segments along East San Ysidro Boulevard had an average parking duration exceeding 2 hours.

On Thursday, 10% of the vehicles observed had license plates issued from Mexico and 3% of parked vehicles observed displayed an ADA placard. Similarly, on Saturday, 9% of the vehicles observed had license plates issued from Mexico and 2% of parked vehicles observed displayed an ADA placard.

Data also showed vehicles parked in red curbed areas as well as blocking driveways during both study days. 19 vehicles (or 4% of all parked vehicles) were observed parked in red curb and driveways on Thursday; 16 vehicles (or 3% of parked vehicles) were observed on Saturday. Data also showed that several vehicles violated the curb drive lane on the southside of West San Ysidro Boulevard between Cottonwood Road and Cypress Drive from 4pm-7pm. This corridor has restricted parking Monday through Friday, between 4pm and 7pm, in order to provide an additional eastbound drive lane onto nearby freeway entrances.

Study Day	# of Vehicles Violating Red Curbs & Driveways	% of Vehicles Violating Red Curbs & Driveways
Thursday, February 26, 2026	19	4%
Saturday, February 28, 2026	16	3%

Table of Vehicles Violating Red Curbs & Driveways by Study Date

Chapter 3

Recommendations & Next Steps

Based on the results of the study, it is recommended to implement time limited and metered parking along the following street segments:

- East San Ysidro Boulevard between Border Village Road to Camino De La Plaza – Both sides
- Border Village Road between East San Ysidro Boulevard (N) to East San Ysidro Boulevard (S) - Both sides
- Front Street between Border Village Road to end of cul de sac – South side
- Bolton Hall Road between East San Ysidro Boulevard to Border Village Road - South side
- Louisiana Avenue between East San Ysidro Boulevard to end of cul de sac - Both sides
- Virginia Avenue between East San Ysidro Boulevard to Border Village Road - Both sides
- East San Ysidro Boulevard between Willow Road and E Park Avenue – South side

Figure 6 depicts the studied street segments where metered parking is recommended. Data shows that businesses along these corridors and segments can benefit from higher turnover, which metered parking has been effective in achieving.

Some street segments have parking occupancy that justifies implementation of paid parking to reach parking occupancy numbers consistent with industry standards. In addition, some street segments have existing 2-hour time limit restrictions, which the data shows were not being met.

Metered parking is typically the next step in parking management when time limit restrictions do not provide the desired results, but enhanced enforcement is key to help regulate the metered parking.

It is important to note that although several street segments along West San Ysidro Boulevard were studied indicated higher parking occupancy and/or parking duration, it is not recommended to implement metered parking at these segments due to the heavy mix of residential and commercial uses. Segments identified for time limited parking and metered parking above are primarily commercial.

Recommendations for metered parking are based on the following:

Short Term Access for businesses

Data shows that high parking occupancy along some unregulated street segments. A combination of regulated and unregulated parking leads to extremely high parking usage on unregulated streets as these tend to be utilized more heavily and for longer durations due to the lack of parking restrictions.

In addition, there is very low turnover along the street segments studied. Turnover is needed to support economic activity in the area that is specific for regular customers and frequent customers rather than long term parkers. This also includes employee parking spillover into public spaces.

Data shows that although there is a time limit of 2 hours within study segments, there are continuous violations in which some vehicles were observed to be parked for long periods of time throughout the study period. Parking meters would ensure parking access to businesses such as retail, banks, auto parts stores, tax preparation services, markets, pawn shops, car insurance businesses, money exchange businesses and would also prevent vehicular storage on public streets.

Enforcement Efficiency and Clarity

The implementation of paid parking within the studied areas with continuous violations to the time limit regulations would provide clear and objective time limits that would reduce ambiguity compared to sign only restrictions. It is anticipated that metered parking will significantly improve enforceability and compliance.

In addition, parking meters will help with the management of spillover of vehicles that are parking all day due to the close proximity to the San Ysidro Border Crossing. This close proximity is causing an intrusion in areas intended for short term parking for community serving uses.



Figure 6 – Recommended Metered Parking Segments

Additional Parking Management strategies recommended:

In addition to the proposed metered parking at the segments identified above, it is recommended to implement the following changes to improve parking management in the studied area:

Repurpose Curb Designations

Data shows that some curb designations are not being utilized as intended. For example, there is more than 360 feet of white curb (passenger loading zone) along East San Ysidro Boulevard, is significantly underutilized. There are also several short-term parking spaces designated as 15 Minute or 30 Minute Parking within the study area, in which several vehicles were observed to violate the 15-minute or 30-minute restriction. An existing taxi zone along the south side of Louisiana Avenue was not observed to be in use during any of the study periods. It is unclear what use the taxi zone is serving since this area is fronting an off-street parking lot. It is recommended to repurpose some of these curb spaces and request additional enforcement in these areas.

Clarity in Parking and No Parking Areas

Parking data showed some vehicles parked in areas where parking should be prohibited. For example, data showed vehicles parked along the north side of Front Street, west of Border Village Road where no curb or sidewalk exist. Further down the street there is a “No Parking Any Time” sign. It is recommended to install additional “No Parking Any Time” signs along the north side to clearly identify the areas in which parking is restricted.

Data also showed several vehicles parked along curb returns which is prohibited. It is recommended to install red curb or sign these areas for “No Parking” to clarify where parking is prohibited.

In some instances, parking restriction signs, such as “Begin” signs, were observed but “End” signs appeared to be missing and vice versa. This confuses motorists on whether a certain restriction applies where they are parking. The north sides of West San Ysidro Boulevard between Smythe Avenue and Cypress Drive are missing “Begin” signs to clearly notify motorists that these segments are 2-hour parking between 8am-6pm.

Lastly, field observations determined a lack of consistent signage along certain street segments. It is recommended to install time limit and paid parking signage at consistent intervals to alleviate any confusion on time limit restrictions.

Appendix A – San Ysidro Parking Study September 2023 (Prepared by CR Associates)



SAN YSIDRO PARKING STUDY

SEPTEMBER 2023

Prepared For



City of San Diego
1 Civic Center Way
San Diego, CA 92101

Prepared By



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Table of Contents

1.0	Introduction.....	1
2.0	San Ysidro Community Plan.....	2
3.0	Project Description.....	3
4.0	Existing Parking Inventory.....	5
5.0	Existing Parking Demand	9
5.1	On-Street Parking Occupancy	9
5.2	Off-Street Parking Occupancy (Public Lots).....	19
5.3	Off-Street Parking Occupancy (Private Lots)	19
5.4	Parking Duration	21
5.5	Weekend Parking Duration Observation	23
6.0	Curb Management Recommendations.....	24

Appendices

Appendix A - Data Collection and Calculations

Appendix B - Occupancy Results – All Periods

Appendix C - High Parking Duration

List of Figures

Figure 3.1 - Project Regional Location.....	4
Figure 4.1 - Existing Parking Inventory Classification Map	6
Figure 5.1 - Weekday Occupancy – Peak Period by Time of Day.....	11
Figure 5.2 - Weekday Occupancy – Peak Percentages	12
Figure 5.3 - Saturday Occupancy – Peak Period by Time of Day.....	14
Figure 5.4 - Saturday Occupancy – Peak Percentages	15
Figure 5.5 - Sunday Occupancy – Peak Period by Time of Day	17
Figure 5.6 - Sunday Occupancy – Peak Percentages.....	18

List of Tables

Table 4.1 - Public Parking Supply within Study Area	8
Table 5.1 - Weekday Parking Occupancy	10
Table 5.2 - Saturday Parking Occupancy.....	13
Table 5.3 - Sunday Parking Occupancy	16
Table 5.4 - Total Occupancy – Off-Street Parking.....	19
Table 5.5 - Lot Occupancy Observations – Off Street Parking.....	20
Table 5.6 - Daily Parking Duration	22
Table 5.7 - Vehicles parked over 9 hours.....	23

1.0 Introduction

The purpose of this parking study (Study) is to evaluate existing parking conditions in the eastern part of the San Ysidro Community Planning Area and Business Improvement District (BID) in relation to the [San Ysidro Community Plan](#) (adopted November 2016). Parking and management of the curb space is a key element for creating a cohesive transportation system. Use of the curb does not only serve vehicle parking but supports micro-mobility, transit, passenger loading, delivery, and outdoor cafes. Comprehensive management of the curb is necessary to create a thriving and multi-modal environment that supports public health, economic vitality, and sustainability goals.

The San Ysidro planning area, located in the southernmost part of the City of San Diego, contains one of the primary border crossing points between the United States and Mexico, with more than 52 million people crossing northbound in 2021¹. Covering an area of approximately 1,800 acres, San Ysidro is surrounded by the State Route 905 to the north, Tijuana River to the west, Otay Mesa community to the east, and the international border with Mexico to the south. San Ysidro's location, adjacent to Mexico, provides abundant opportunities for cultural exchange and commerce, serving both the tourist and the resident population. The San Ysidro Land Port of Entry is one of the world's busiest land ports and directly impacts the community of San Ysidro. Because of its location as the gateway for binational trade and mobility, San Ysidro experiences significant border traffic congestion, air quality impacts, and barriers to connectivity. Understanding existing conditions for parking can help unlock the potential to better optimize use of the right-of-way to support alternative and sustainable ways to move around in support of the vision outlined in the San Ysidro Community Plan.

¹ Includes pedestrians and vehicle occupants. US Department of Transportation, Bureau of Transportation Statistics.

<https://data.bts.gov/Research-and-Statistics/Border-Crossing-Entry-Data/keg4-3bc2/data>

2.0 San Ysidro Community Plan

The San Ysidro Community Plan involved extensive community engagement and collaboration between local residents, businesses, and community stakeholders over several years, prior to its adoption in 2016. The Community Plan aims to transform the area, historically focused on retail, into a vibrant and connected community with multimodal facilities and public spaces, new housing, transit, parks and other amenities. The eastern part of the community which is the focus of the Parking Study is referenced in the Community Plan as the “Border Village District.” The Plan’s vision for the Border Village District is to “reestablish the area as a tourist and visitor destination.” Additional housing in the Village District will be mixed-use, offering an opportunity for people to live near their workplace while promoting public gathering spaces for the enjoyment of visitors and residents.

New housing and development will require comprehensive mobility solutions, which will need to evolve to meet changing transportation demands. In support of this vision, the Community Plan outlined several mobility strategies to support walkability and alternative modes of transportation such as pedestrian plazas, wayfinding, improved lighting, on-demand shuttles, and traffic calming measures. The Community Plan also identified several mobility policies that focus on curb and parking:

- 3.10.4 Implement on-street parking management strategies in the Village and commercial areas to more effectively use street parking space and increase turnover and parking availability
- 3.10.5 Consider the use of metered parking in commercial areas to provide short-term parking for retail customers and visitors while discouraging long-term residential, employee and border-user parking.
- 5.6.3 Pursue shared parking and parking district strategies that facilitate “park once” practices and encourages shoppers to navigate the area on foot.

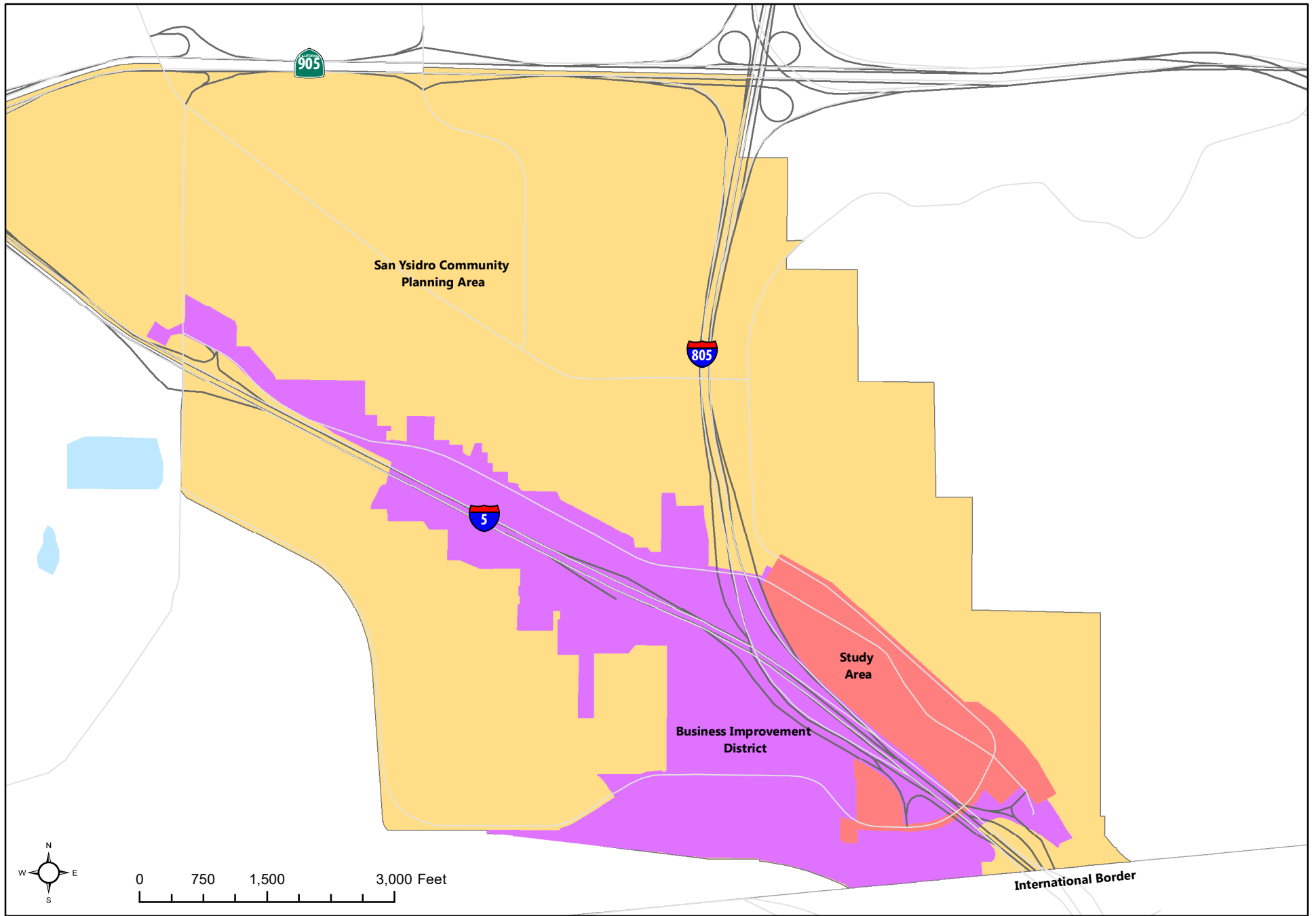
In addition to the San Ysidro Community Plan, there are several major regional transportation projects led by the San Diego Association of Governments (SANDAG), which will further transform transportation in San Ysidro. SANDAG’s Border to Bayshore Bikeway is an approximately 6.7-mile route that will provide safe biking connections within and between Imperial Beach, San Ysidro, and the San Ysidro Port of Entry. This project will make it safer and easier for people of all ages and comfort levels to bike to local shops, parks, schools, transit stations, and other community destinations within the area. The construction of this major bike project will impact available parking along Beyer Boulevard, amplifying the need for additional strategies to optimize available parking supply and transportation demand management. SANDAG is also partnering with the San Diego Metropolitan Transit System (MTS), the City of San Diego, and Caltrans to develop the planning documents for Phase 1 of the San Ysidro Mobility Hub, which will outline near-term improvements to pedestrian safety and connectivity, operational capacity, and space optimization at the existing San Ysidro Transit Center. The potential reconfiguration of the transit station will require community-focused solutions to address pedestrian, cyclist, and vehicle traffic while providing convenient pick up of passengers, supporting transit operations, and parking near the transit station. Lastly, SANDAG’s Advancing Border Connectivity project is exploring the implementation of a suite of technology solutions to improve the flow of people and goods along the US-Mexico Border. The project scope focuses on signal improvements and a regional border management system to improve multimodal options in the area.

3.0 Project Description

From incoming bike facilities to a fresh design of the San Ysidro transit center, these regional projects highlight the importance of evaluating existing curb conditions and developing complementary strategies to support mobility in San Ysidro. The City of San Diego is conducting an inventory of available parking and utilization within San Ysidro's Border Village, bounded by the I-805 and I-5 freeways to the west and the MTS Blue Line Trolley to the east. The land use in the prospective district area ranges from residential, industrial, office, commercial retail, and restaurants. **Figure 3.1** displays the location of the San Ysidro Community, Business Improvement District (BID), and parking study area. The intent of the Study is to develop a suite of comprehensive strategies to support the future of San Ysidro and the bustling activity within the Border Village.

This study was conducted to evaluate existing parking inventory and parking demand for both on-street and off-street parking spaces within the study area. This Study is organized into the following sections:

- Existing Parking Inventory – This section provides a detailed quantification of the existing parking supply within the study area, including documentation of any unique attributes of the supply such as cost, time, and purpose restrictions.
- Existing Parking Demand – This section presents and analyzes parking occupancy data collection within the study area. This section also examines parking turnover along public on-street parking.
- Curb Management Recommendations – This section outlines future considerations and projects that may help address findings of the Study.



**San Ysidro Parking District Project
Parking Study**



*Figure 3.1
Project Regional Location*

4.0 Existing Parking Inventory

The existing parking inventory within the study area consists of both on-street and off-street parking facilities. On-street parking is available along the following roadway segments:

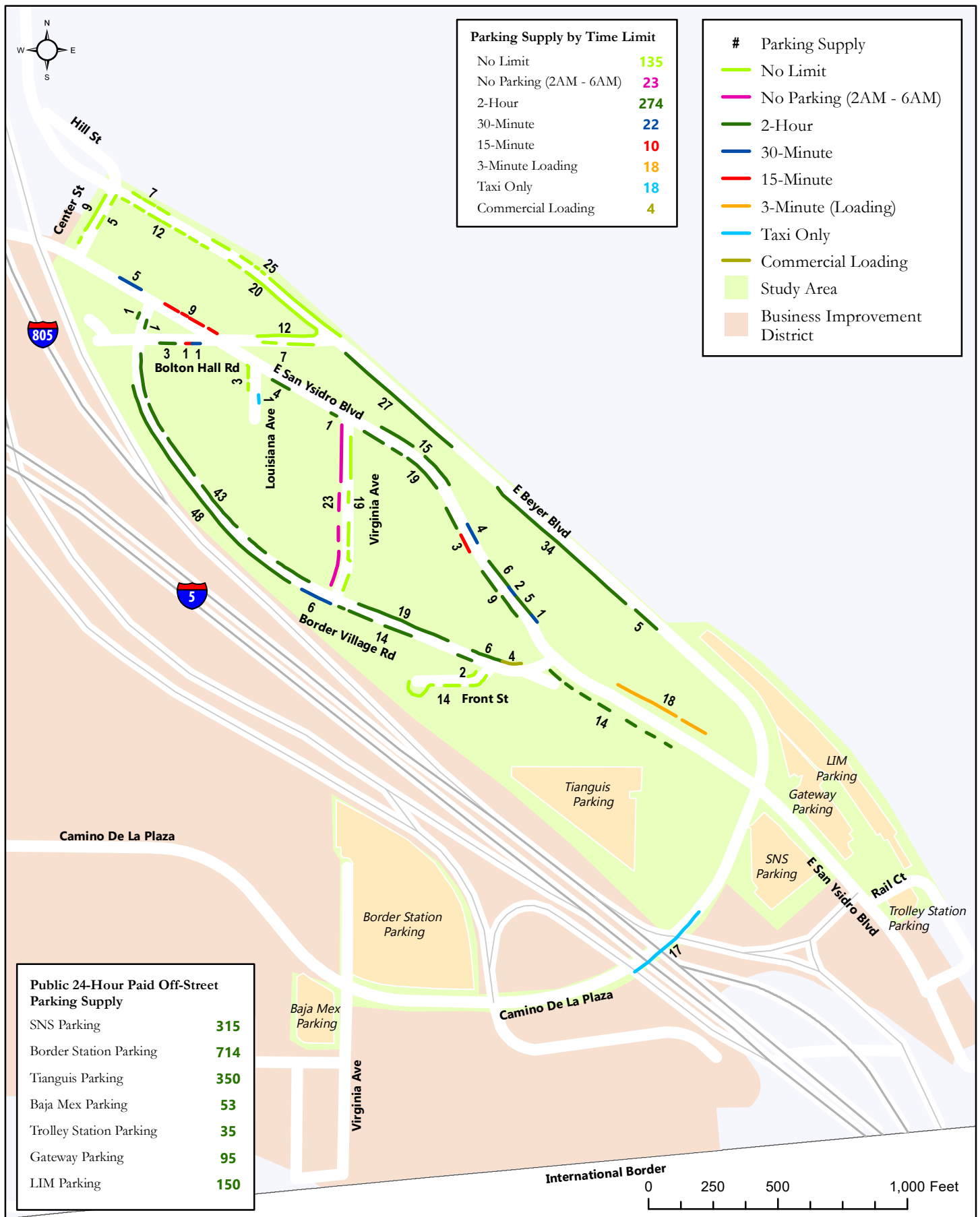
- Border Village Road, between E. San Ysidro Boulevard (north) and E. San Ysidro Boulevard (south)
- E. San Ysidro Boulevard, between Center Street and E. Beyer Boulevard
- E. Beyer Boulevard, between Center Street and E. San Ysidro Boulevard
- Center Street, between E. San Ysidro Boulevard and E. Beyer Boulevard
- Bolton Hall Road, between Border Village Road and E. Beyer Boulevard
- Virginia Avenue, between Border Village Road and E. San Ysidro Boulevard
- Camino De La Plaza, between Interstate 5 southbound ramps and E. San Ysidro Boulevard
- Louisiana Avenue, between E. San Ysidro Boulevard and end of cul-de-sac
- Front Street, between Border Village Road and end of cul-de-sac

Public off-street facilities are available at the following locations:

- SNS Parking – Located on the southeast corner of the E. Beyer Boulevard and E. San Ysidro Boulevard intersection. Single access point located along East San Ysidro Boulevard.
- Border Station Parking – Located on the northeast corner of the Virginia Avenue and Camino De La Plaza intersection. Single access point located along Camino De La Plaza.
- Tianguis Parking – Located on the south side of E. San Ysidro Boulevard between Border Village Road and Camino De La Plaza. This location also serves as the Tianguis Swap Meet between Friday and Sunday. Two access points located along East San Ysidro Boulevard
- Baja Mex Parking – Located on the southwest corner of the Virginia Avenue and Camino De La Plaza intersection. Single access point located along Camino De La Plaza. It should be noted that this lot is currently demolished and will have a parking structure constructed complete with 350 parking spaces.
- Trolley Station Parking – Located on the south side of Rail Court across from Jack in the Box fast food restaurant. Single access point located at Rail Court.
- Gateway Parking – Located on the east side of East San Ysidro Boulevard between Camino De La Plaza and Jack in the Box fast food restaurant. Single access point located along East San Ysidro Boulevard.
- LIM Parking – Located between the Gateway Parking lot and the freight railroad tracks to the east. Single access point located at Rail Court.

Apart from the seven public parking lots, the majority of businesses in the study area have their own off-street parking lots, with at least 15 larger-sized private parking lots discussed in detail later in this study.

Figure 4.1 shows the total parking supply along each block segment within the study area, parking supply time restrictions, and the total parking supply provided within the seven off-street public parking facilities. The quantity of parking supply within the study area was determined through a combination of aerial imagery interpretation with field verification.



**San Ysidro Parking District Project
Parking Study**



*Figure 4.1
Existing Parking Inventory
Classification Map*

Unmarked on-street parking was estimated using a method which interprets aerial imagery from Google Earth and Google Street View in order to determine the extents and length of segments where on-street parking is allowed. Spatially referenced lines along curbside locations where on-street parking is allowed were subsequently drawn in Google Earth.

The parking supply was then estimated by dividing the extents of each line segment by 20 feet, rounding to the nearest whole number. Typically, marked parking stalls are about 22 feet in length. The 20-foot interval was chosen to reflect actual field conditions where vehicles are parked in a configuration in which the maximum storage of vehicles is possible when street parking is unmarked. Any usage restrictions which apply to specific parking locations, such as cost, time limits or use purpose were gathered in the field.

Table 4.1 presents the total supply of public parking by each of the unique time restrictions represented in the study area.

As indicated, the study area has several on-street parking restrictions, which account for approximately 16.7% of the parking supply in the area. Only 6.1% of the parking supply in the study area is on-street and without any time limit. All time restrictions for on-street parking spaces are enforced daily between 8:00 AM and 6:00 PM, except for the following cases:

- Parking is restricted between 2:00 AM and 6:00 AM on the west side of Virginia Avenue, from East San Ysidro Boulevard to Border Village Road.
- There is a 2-hour parking limit enforced 24-hours a day along East Beyer Boulevard, between Bolton Hall Road and East San Ysidro Boulevard.
- Street sweeping is conducted every Wednesday along all segments within the study area.
- Only East San Ysidro Boulevard has signs indicating street sweeping and prohibiting parking between 12:00 AM and 3:00 AM on Wednesdays.

Within the study area, there are a total of seven off-street public parking facilities providing an estimated 1,712 parking spaces, representing 77.2% of the total parking supply. The Border Station Parking lot is the largest, offering 714 spaces, or almost one-third of the total parking supply in the study area. All off-street parking facilities operate 24 hours a day and require payment at varying hourly and daily rates, which depend on location and day of the week. The observed hourly rates range from \$6 per hour to the highest daily rate of \$75 per day. Only the Tianguis Parking location offers monthly parking at \$199 per month. These off-street parking facilities are designed to accommodate lower turnover trip activities within the study area.

Table 4.1 - Public Parking Supply within Study Area

Time Restriction	Location	Cost	# of Spaces	% of Grand Total
Long Term Parking (on-street)				
No Time Limit	On-Street	Free	135	6.1%
No Parking (2AM-6AM)	On-Street	Free	23	1.0%
2-Hour Limit	On-Street	Free	274	12.4%
Subtotal			432	19.5%
Short Term Parking (on-street)				
30-Minute Limit	On-Street	Free	22	1.0%
15-Minute Limit	On-Street	Free	10	0.5%
3-Minute Limit (Loading)	On-Street	Free	18	0.8%
Taxi Only	On-Street	Free	18	0.8%
Commercial Loading	On-Street	Free	4	0.2%
Subtotal			72	3.3%
Off Street Parking				
	SNS Parking	\$6 / hour \$25 / 8 hours	315	14.2%
	Border Station Parking	\$9 - \$14 / hour	714	32.2%
	Tianguis Parking	\$7 / day \$199 / month	350	15.8%
≥2-Hour Limit ¹	Baja Mex Parking ²	\$12 - \$20 / day	53	2.4%
	Trolley Station Parking	\$6 / hour \$75 / day	35	1.6%
	Gateway Parking	\$8 / 8 hours	95	4.3%
	LIM Parking	\$8 - \$12 / 8 hours	150	6.7%
Subtotal			1,712²	77.2%
Subtotal				
Grand Total			2,216	

Source: CR Associates (2022)

Notes:

¹ All off-street parking open 24-hours

² Supply to increase by approximately 300 spaces with completion of off-street parking structure at the Baja Mex parking lot.

5.0 Existing Parking Demand

To obtain a comprehensive overview of parking demand in the study area, the project team used three methods to collect parking information. Firstly, automatic license plate recognition (ALPR) technology was utilized on May 26, June 4, and June 5, 2022, to gather data on existing on-street parking demand. Findings were evaluated and aggregated for this Study in the sections below. Public off-street parking usage was provided by the City of San Diego for the same study period, although this only represented the total number of tickets purchased and not average parking demand per lot. Finally, private off-street parking was collected separately on April 6, 2023, using the drive-by method to estimate parking lot occupancy based on the number of occupied parking spaces during lunchtime.

Parking occupancy by off-street and on-street parking locations are summarized for the study weekday, Saturday, and Sunday in the following sections. Data collection and calculations are provided in **Appendix A**.

5.1 On-Street Parking Occupancy

Parking occupancy was collected during the aforementioned dates for one-hour intervals from 8 AM to 6 PM. The peak parking occupancy within the study area during Thursday, Saturday, and Sunday time periods are displayed in **Table 5.1** through **Table 5.3**, respectively. The parking occupancy information is also presented graphically in the following figures

Figure 5.1 displays a weekday map of segments, and their associated peak periods.

Figure 5.2 displays a weekday map of segments associated with their peak percentages.

Figure 5.3 displays a map of segments, and their associated peak periods.

Figure 5.4 displays a map of segments associated with their peak percentages on Saturday.

Figure 5.5 displays a map of segments, and their associated peak periods.

Figure 5.6 displays a map of segments associated with their peak percentages.

The tables and graphics reveal that while several roadway segments had parking demand exceeding 85%, with only Center Street reaching 100% parking occupancy for multiple periods throughout the day. The observed parking demand could be attributed to the limited number of parking spaces on Center Street. On other roadway segments, where parking demand exceeded 85%, the high parking demand was only on one side of the street, where on-street parking is limited and along cul-de-sac streets that also have limited on-street parking supply.

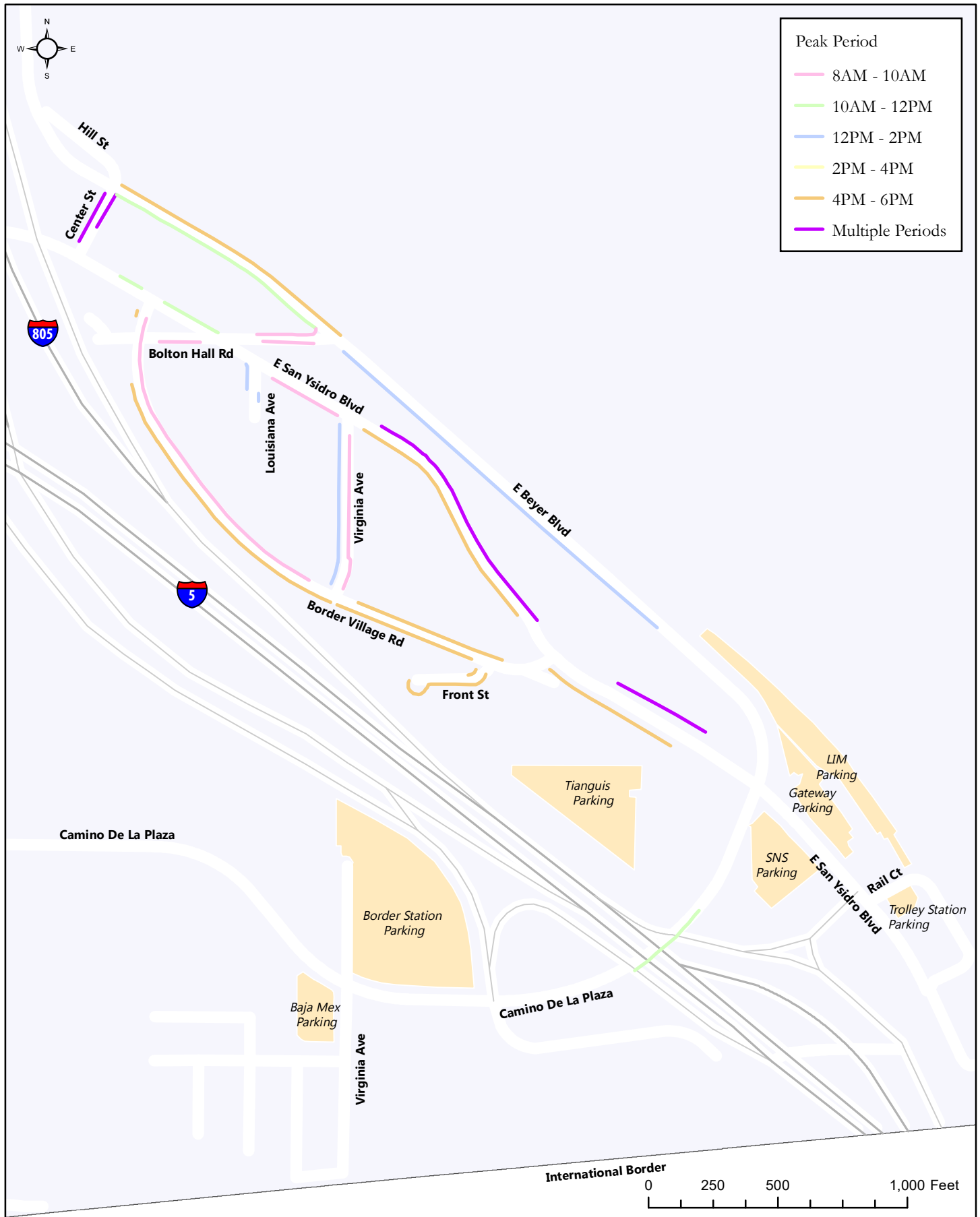
Similar to the weekday, the parking data from Saturday and Sunday reveal that multiple roadway segments have parking demand of 85% or higher. Center Street still experiences 100% parking demand for several periods. Streets that face industrial development, such as Louisiana Avenue, Virginia Avenue, and East Beyer Boulevard between Center Street and Bolton Hall Road, have parking demand of 85% or higher for most of the study period. However, it is important to note that while some of these streets have high parking demand, adjacent street segments do not, suggesting that they may be used by long-term overnight parkers who leave their vehicles on the US side of the border before returning to Mexico. Several businesses and residents in the area recounted similar anecdotal observations though this cannot be confirmed in the Study. Additional figures displaying occupancy information for all data collection periods on a typical weekday are provided in **Appendix B**.

Table 5.1 - Weekday Parking Occupancy

Roadway	Segment	Side	Occupancy										Peak Period
			8AM-9AM	9AM-10AM	10AM-11AM	11AM-12PM	12PM-1PM	1PM-2PM	2PM-3PM	3PM-4PM	4PM-5PM	5PM-6PM	
Center Street	E. Beyer Boulevard and E. San Ysidro Boulevard	East	80%	60%	80%	80%	60%	100%	100%	100%	100%	100%	Multiple
		West	78%	100%	78%	89%	100%	78%	67%	100%	89%	100%	Multiple
Louisiana Avenue	Cul-de-Sac	Both	50%	50%	75%	75%	100%	50%	50%	50%	50%	12PM - 2PM	
Virginia Avenue	E. San Ysidro Boulevard and Border Village Road	East	84%	74%	53%	68%	68%	63%	58%	79%	63%	58%	8AM - 10AM
		West	39%	43%	39%	35%	39%	65%	57%	48%	48%	43%	12PM - 2PM
Camino De La Plaza	E. San Ysidro Boulevard and Interstate 5 southbound ramps	East	18%	29%	35%	24%	18%	12%	12%	18%	6%	18%	10AM - 12PM
E. Beyer Boulevard	Center Street and Bolton Hall Road	North	69%	81%	75%	75%	63%	69%	69%	69%	69%	97%	4PM - 6PM
		South	50%	56%	63%	53%	50%	50%	53%	44%	56%	56%	10AM - 12PM
E. Beyer Boulevard	Bolton Hall Road and E. San Ysidro Boulevard	South	6%	6%	9%	9%	20%	15%	11%	11%	8%	18%	12PM - 2PM
E. San Ysidro Boulevard	Center Street and Border Village Road (north)	North	0%	20%	40%	60%	40%	20%	40%	40%	40%	40%	10AM - 12PM
E. San Ysidro Boulevard	Border Village Road (north) and Virginia Avenue	North	0%	22%	22%	67%	44%	33%	33%	11%	33%	33%	10AM - 12PM
E. San Ysidro Boulevard	Louisiana Avenue and Virginia Avenue	South	40%	40%	0%	0%	0%	0%	0%	0%	0%	20%	8AM - 10AM
E. San Ysidro Boulevard	Virginia Avenue and Border Village Road (south)	North	30%	39%	36%	64%	64%	52%	55%	55%	45%	52%	Multiple
		South	14%	21%	39%	57%	86%	82%	68%	82%	57%	93%	4PM - 6PM
E. San Ysidro Boulevard	Border Village Road (south) and E. Beyer Boulevard	North	33%	39%	39%	39%	39%	28%	17%	50%	50%	39%	Multiple
		South	43%	29%	57%	36%	29%	43%	43%	71%	100%	86%	4PM - 6PM
Bolton Hall Road	Border Village Road and E. Beyer Boulevard	North	75%	67%	67%	67%	58%	42%	33%	33%	42%	42%	8AM - 10AM
		South	67%	100%	67%	75%	83%	83%	67%	92%	83%	92%	8AM - 10AM
Border Village Road	E. San Ysidro Boulevard (north) and Virginia Avenue	North	27%	25%	16%	23%	11%	11%	11%	14%	16%	23%	8AM - 10AM
		South	18%	11%	20%	11%	16%	15%	18%	7%	25%	24%	4PM - 6PM
Border Village Road	Virginia Avenue and E. San Ysidro Boulevard (south)	North	36%	44%	36%	40%	44%	36%	44%	60%	68%	88%	4PM - 6PM
		South	21%	14%	7%	21%	0%	7%	0%	21%	21%	50%	4PM - 6PM
Front Street	Cul-de-Sac	Both	56%	13%	75%	56%	50%	56%	75%	63%	69%	100%	4PM - 6PM

Source: CR Associates (2023)

 Note:
Bold > 85% occupancy



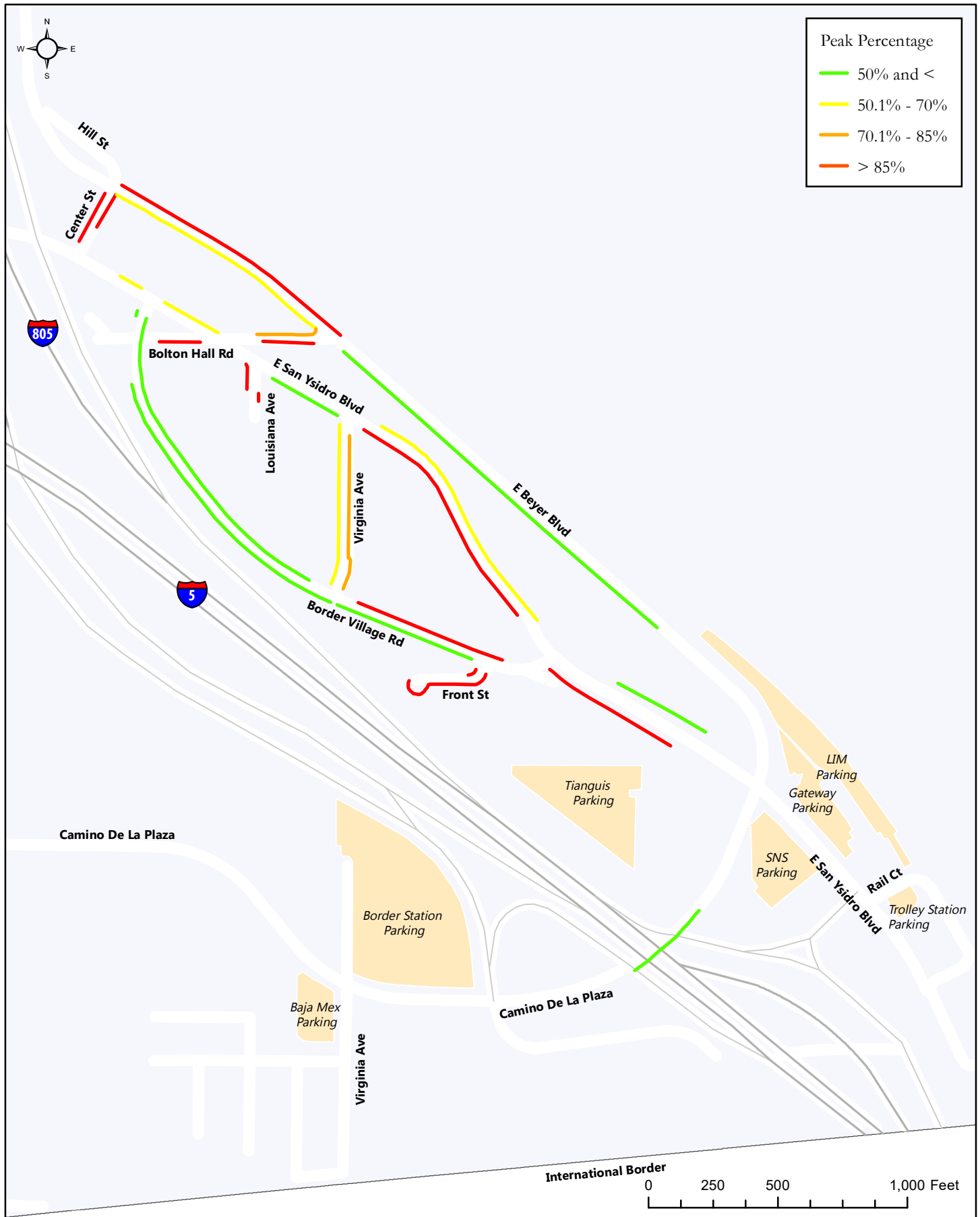
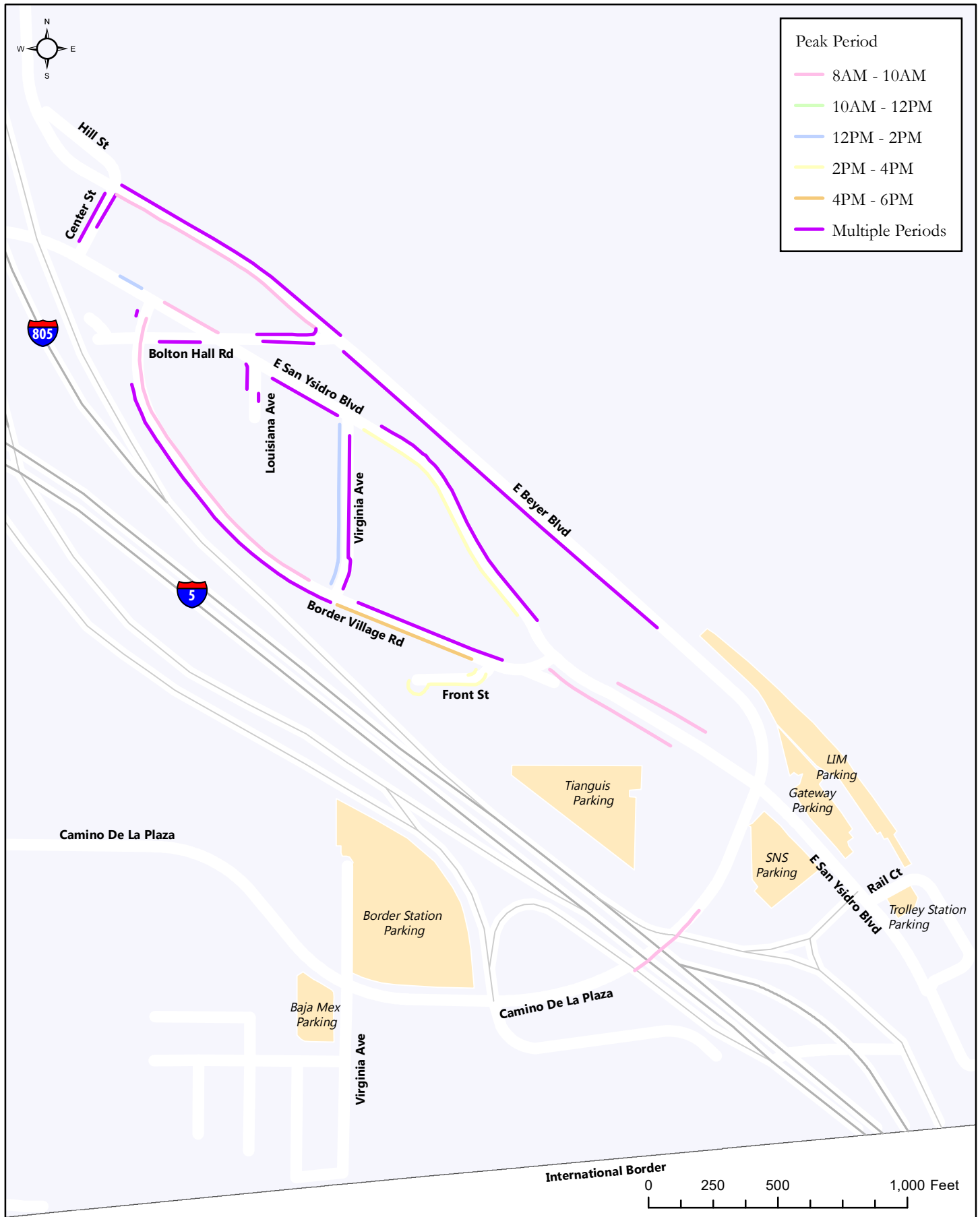


Table 5.2 - Saturday Parking Occupancy

Roadway	Segment	Side	Occupancy										Peak Period
			8AM-9AM	9AM-10AM	10AM-11AM	11AM-12PM	12PM-1PM	1PM-2PM	2PM-3PM	3PM-4PM	4PM-5PM	5PM-6PM	
Center Street	E. Beyer Boulevard and E. San Ysidro Boulevard	East	100%	60%	60%	100%	40%	60%	80%	60%	60%	100%	Multiple
		West	89%	100%	100%	100%	100%	67%	100%	100%	100%	100%	Multiple
Louisiana Avenue	Cul-de-Sac	Both	0%	50%	50%	75%	75%	100%	75%	100%	75%	75%	Multiple
Virginia Avenue	E. San Ysidro Boulevard and Border Village Road	East	100%	100%	100%	100%	100%	95%	89%	89%	95%	95%	Multiple
		West	91%	96%	96%	96%	91%	100%	87%	87%	91%	83%	12PM-2PM
Camino De La Plaza	E. San Ysidro Boulevard and Interstate 5 southbound ramps	East	59%	47%	12%	24%	29%	18%	18%	18%	18%	0%	8AM-10AM
E. Beyer Boulevard	Center Street and Bolton Hall Road	North	97%	91%	94%	100%	88%	81%	100%	91%	88%	81%	Multiple
		South	94%	75%	78%	72%	81%	72%	72%	66%	66%	84%	8AM-10AM
E. Beyer Boulevard	Bolton Hall Road and E. San Ysidro Boulevard	South	24%	24%	23%	24%	21%	21%	23%	21%	18%	24%	Multiple
E. San Ysidro Boulevard	Center Street and Border Village Road (north)	North	0%	0%	40%	20%	60%	20%	0%	20%	0%	0%	12PM-2PM
E. San Ysidro Boulevard	Border Village Road (north) and Virginia Avenue	North	11%	56%	44%	44%	33%	0%	33%	0%	11%	0%	8AM-10AM
E. San Ysidro Boulevard	Louisiana Avenue and Virginia Avenue	South	20%	0%	20%	0%	0%	0%	0%	0%	0%	20%	Multiple
E. San Ysidro Boulevard	Virginia Avenue and Border Village Road (south)	North	42%	39%	39%	42%	48%	58%	70%	73%	73%	73%	Multiple
		South	46%	54%	86%	75%	68%	71%	82%	89%	75%	68%	2PM-4PM
E. San Ysidro Boulevard	Border Village Road (south) and E. Beyer Boulevard	North	33%	50%	28%	22%	28%	17%	17%	22%	17%	17%	8AM-10AM
		South	100%	79%	64%	79%	71%	71%	64%	71%	79%	71%	8AM-10AM
Bolton Hall Road	Border Village Road and E. Beyer Boulevard	North	58%	58%	58%	42%	42%	42%	58%	33%	42%	42%	Multiple
		South	92%	100%	83%	100%	83%	83%	92%	92%	92%	100%	Multiple
Border Village Road	E. San Ysidro Boulevard (north) and Virginia Avenue	North	34%	36%	25%	25%	27%	30%	25%	14%	23%	30%	8AM-10AM
		South	18%	18%	20%	15%	15%	20%	13%	11%	9%	18%	Multiple
Border Village Road	Virginia Avenue and E. San Ysidro Boulevard (south)	North	60%	72%	64%	84%	92%	100%	92%	100%	92%	100%	Multiple
		South	14%	43%	7%	7%	0%	0%	36%	36%	36%	64%	4PM-6PM
Front Street	Cul-de-Sac	Both	63%	63%	63%	63%	56%	63%	75%	63%	63%	69%	2PM-4PM

Source: CR Associates (2023)

 Note:
Bold > 85% occupancy



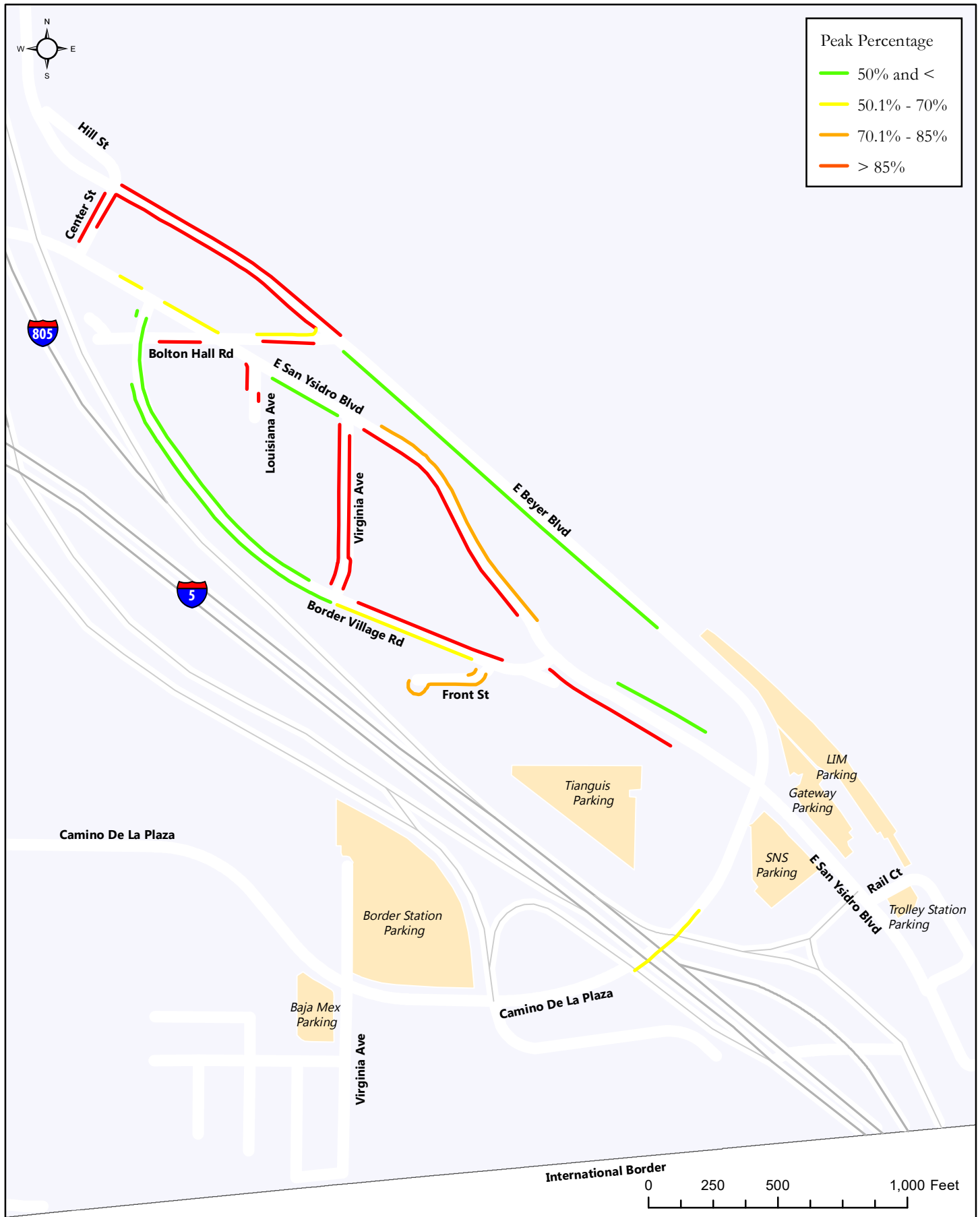
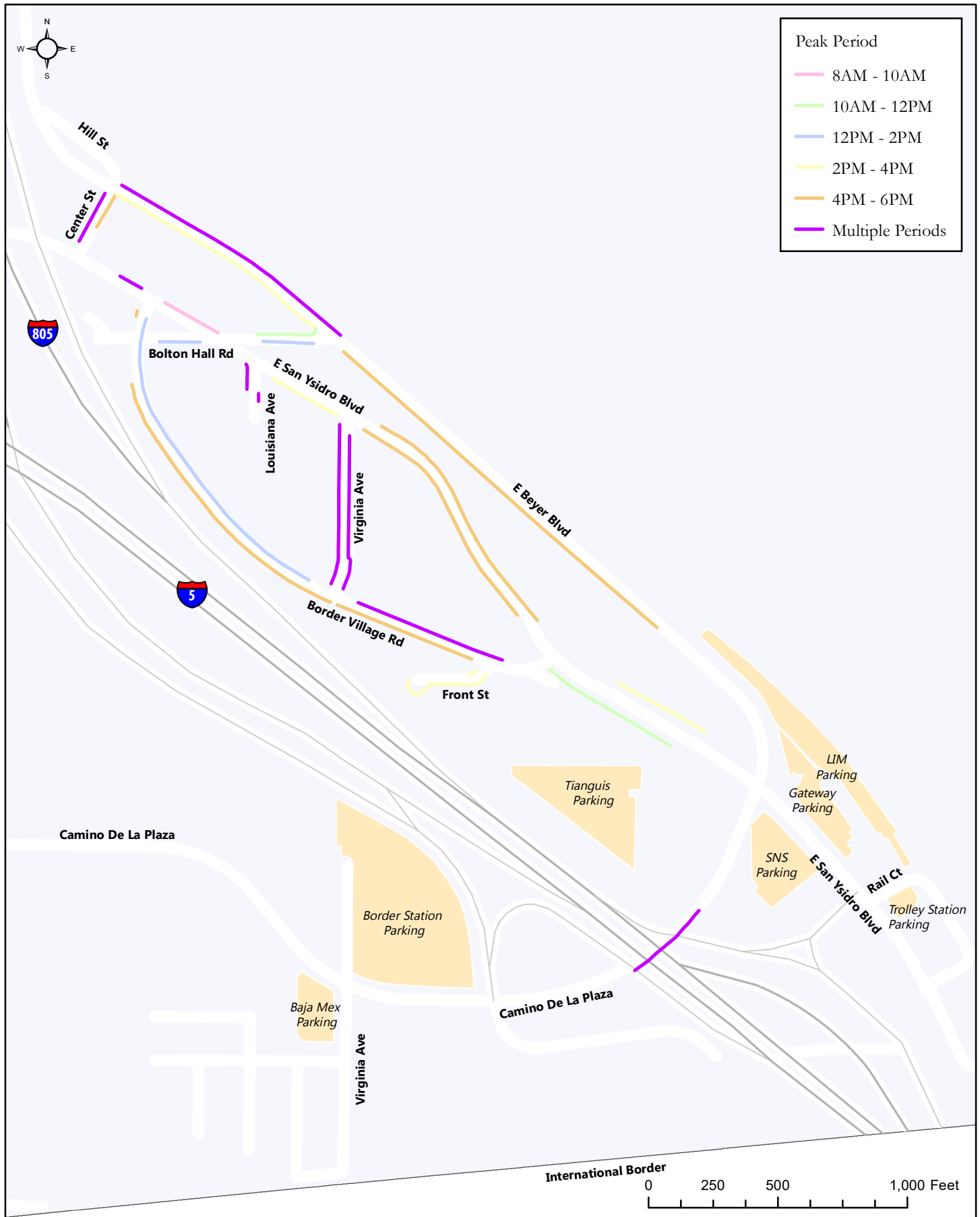


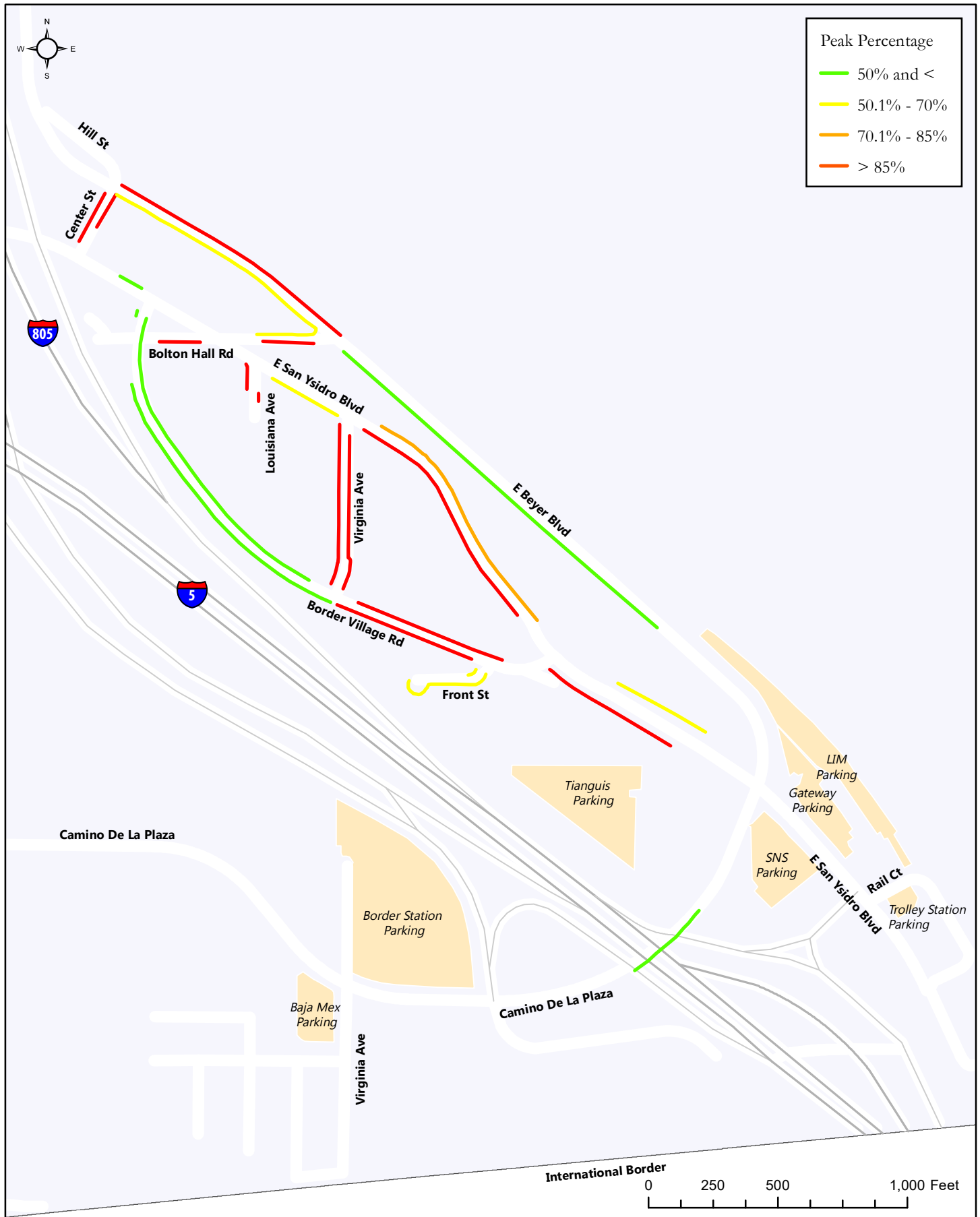
Table 5.3 - Sunday Parking Occupancy

Roadway	Segment	Side	Occupancy										Peak Period	
			8AM-9AM	9AM-10AM	10AM-11AM	11AM-12PM	12PM-1PM	1PM-2PM	2PM-3PM	3PM-4PM	4PM-5PM	5PM-6PM		
Center Street	E. Beyer Boulevard and E. San Ysidro Boulevard	East	40%	40%	60%	60%	40%	60%	60%	60%	60%	60%	100%	4PM-6PM
		West	100%	89%	67%	78%	67%	78%	100%	100%	100%	100%	100%	Multiple
Louisiana Avenue	Cul-de-Sac	Both	75%	75%	100%	100%	75%	75%	75%	100%	100%	75%	75%	Multiple
Virginia Avenue	E. San Ysidro Boulevard and Border Village Road	East	89%	84%	89%	89%	95%	100%	100%	100%	84%	84%	89%	Multiple
		West	87%	87%	87%	87%	83%	83%	83%	70%	74%	74%	74%	Multiple
Camino De La Plaza	E. San Ysidro Boulevard and Interstate 5 southbound ramps	East	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	Multiple
E. Beyer Boulevard	Center Street and Bolton Hall Road	North	88%	94%	88%	100%	97%	100%	97%	88%	94%	100%	100%	Multiple
		South	66%	59%	56%	63%	59%	63%	66%	69%	66%	53%	53%	2PM-4PM
E. Beyer Boulevard	Bolton Hall Road and E. San Ysidro Boulevard	South	23%	30%	26%	23%	24%	20%	23%	26%	30%	32%	32%	4PM-6PM
E. San Ysidro Boulevard	Center Street and Border Village Road (north)	North	20%	20%	20%	20%	20%	20%	20%	20%	20%	20%	20%	Multiple
E. San Ysidro Boulevard	Border Village Road (north) and Virginia Avenue	North	0%	11%	0%	0%	0%	0%	0%	0%	0%	0%	0%	8AM-10AM
E. San Ysidro Boulevard	Louisiana Avenue and Virginia Avenue	South	0%	0%	0%	0%	0%	0%	0%	0%	60%	0%	20%	2PM-4PM
E. San Ysidro Boulevard	Virginia Avenue and Border Village Road (south)	North	48%	45%	52%	67%	67%	64%	73%	73%	82%	76%	76%	4PM-6PM
		South	79%	79%	71%	64%	71%	71%	82%	89%	96%	96%	96%	4PM-6PM
E. San Ysidro Boulevard	Border Village Road (south) and E. Beyer Boulevard	North	28%	22%	39%	28%	44%	39%	39%	56%	39%	39%	39%	2PM-4PM
		South	64%	79%	93%	71%	71%	71%	79%	79%	71%	57%	57%	10AM-12PM
Bolton Hall Road	Border Village Road and E. Beyer Boulevard	North	50%	50%	50%	58%	50%	50%	42%	50%	50%	50%	50%	10AM-12PM
		South	67%	75%	83%	83%	83%	100%	75%	83%	92%	83%	83%	12PM-2PM
Border Village Road	E. San Ysidro Boulevard (north) and Virginia Avenue	North	23%	23%	18%	30%	27%	32%	16%	14%	25%	20%	20%	12PM-2PM
		South	22%	18%	16%	18%	13%	16%	11%	22%	31%	18%	18%	4PM-6PM
Border Village Road	Virginia Avenue and E. San Ysidro Boulevard (south)	North	100%	80%	100%	100%	100%	100%	92%	100%	100%	96%	100%	Multiple
		South	64%	43%	50%	64%	64%	50%	57%	93%	93%	100%	100%	4PM-6PM
Front Street	Cul-de-Sac	Both	50%	50%	50%	50%	50%	50%	56%	50%	50%	50%	50%	2PM-4PM

Source: CR Associates (2023)

 Note:
Bold > 85% occupancy





5.2 Off-Street Parking Occupancy (Public Lots)

Off-street parking supply was also evaluated to determine general utilization trends in comparison to on-street observations. Data on the off-street parking lots was provided by the San Ysidro Improvement Corporation and utilized to determine the utilization of off-street parking at the seven public lots, which primarily serve customers who require long-term parking while traveling to and from Mexico. **Table 5.4** presents the total number of tickets sold at each off-street parking facility for a 24-hour period on a typical weekday and both weekend days.

Table 5.4 - Total Occupancy – Off-Street Parking

Off-Street Parking Location	# Spaces	Total # of Parking Tickets Purchased		
		Thursday (05/26/2022)	Saturday (06/04/2022)	Sunday (06/05/2022)
SNS Parking	315	385	187	200
Border Station Parking	714	262	427	297
Tianguis Parking	350	138	346	110
Baja Mex Parking	53	56	89	71
Trolley Station Parking	35	120	154	117
Gateway Parking	95	155	380	146
LIM Parking	150	352	401	265
Total		1,468	1,984	1,206

Source: San Ysidro Improvement Corporation (2022)

According to the data, Saturday had the highest overall off-street parking occupancy for six of the seven parking lots in the study area. The SNS parking lot was the only exception, as it experienced higher demand on Thursday than on Saturday or Sunday. The Gateway, Lim, and Trolley Station parking lots, which have lower costs compared to nearby lots, were the most frequently used.

5.3 Off-Street Parking Occupancy (Private Lots)

Data regarding off-street parking occupancy was also gathered for private parking lots. These are lots linked to commercial sites intended to provide parking for both employees and customers. The collection of parking occupancy data employed a drive-by method during a weekday afternoon when commercial parking demand is projected to be at its peak. **Table 5.5** shows the occupancy of these private parking lots.

Table 5.5 reveals that 10 out of the 15 parking lots have a parking demand of 40% or less, while 5 lots have a parking demand of 80% or higher. The Border Village Shopping Center parking lot stands out with a parking demand of 95% during noon hours. The following observations were also made for certain parking lots experiencing high parking demand:

- The Postal Annex Lot frequently experiences a high demand for parking in its front area, while the back lot generally sees less usage.

- The Express Pawn Shop, previously situated at 561 E San Ysidro Blvd, has relocated to 390 E San Ysidro Blvd. Consequently, the lot at the old address is now being utilized by the public, likely due to a lack of parking enforcement.
- The Border Village Shopping Center's parking lot typically faces high parking demand. However, it's noteworthy that many vehicles aren't parked near the entrance to the stores but closer to E San Ysidro Blvd. This pattern suggests these vehicles may not belong to the shopping center's employees or customers, but rather to the general public due to lack of parking enforcement.
- There were numerous observations of vehicles waiting to collect passengers from the border crossing. These vehicles were typically either running or occupied, with drivers remaining inside.

Table 5.5 - Lot Occupancy Observations – Off Street Parking

Lot	Address	% Occupied Thursday (05/26/2022)
1. Lolita Tax Services Lot	318 E San Ysidro Blvd	90%
2. 99 Cents Only Lot	121 Louisiana Ave	40%
3. Church's Texas Chicken Lot	423 E San Ysidro Blvd	30%
4. ABC Money Exchange Lot	502 E San Ysidro Blvd	30%
5. Adriana's Insurance Services Lot	505 E San Ysidro Blvd	30%
6. Express Pawn Shop & Money Exchange Lot	561 E San Ysidro Blvd	80%
7. Little Caesars Pizza Lot	601 E San Ysidro Blvd	40%
8. Wells Fargo Bank Lot	637 E San Ysidro Blvd	40%
9. Postal Annex Lot (front)	641 E San Ysidro Blvd	80%
10. San Ysidro Improvement Corporation Lot	663 E San Ysidro Blvd	40%
11. Gran Vinta Restaurant Lot	3020 E Beyer Blvd	80%
12. AutoZone Auto Parts Lot	636 E San Ysidro Blvd	10%
13. Border Village Shopping Center Lot	4650 Border Village Rd	95%
14. Goodwill Retail Store Lot	630 Front St	10%
15. Metro by T-Mobile Lot	644 E San Ysidro Blvd	20%

Source: CR Associates (2022)

5.4 Parking Duration

Generally, areas with high demand for off-street parking and high on-street parking turnover tend to yield greater returns on investment compared to areas with low parking demand. In this study, parking duration was determined by utilizing ALPR technology to collect license plate information and determine the length of time a vehicle stayed parked in a specific spot. The on-street parking duration on Thursdays and Saturdays in the study area is summarized in **Table 5.6**, while Sundays were not considered since parking regulations are only enforced on weekdays and Saturdays from 8:00 AM to 6:00 PM. Additional discussion regarding weekend observation is provided in the next section.

The results presented in Table 5.6 show that the longest average length of stay during weekdays (more than 3 hours) occurred along roadway segments without parking restrictions, particularly those adjacent to residential or industrial land uses. On the other hand, East San Ysidro Boulevard had an average length of stay of less than two hours, except for the segment between Center Street and Bolton Hall Road. Although a few vehicles parked for more than two hours along East San Ysidro Boulevard, they represented only a small percentage of the overall vehicles parked along the street. The same trend was observed on Saturdays, with an increase in the number of vehicles parking for more than two hours. Additional observation details are provided in **Appendix C**.

Table 5.6 - Daily Parking Duration

Roadway	Segment	Side	Thursday (05/26/2022)		Saturday 06/04/2022)	
			Mean Length of Stay (Hours) ¹	Vehicles Exceeding 2-Hours	Mean Length of Stay (Hours) ¹	Vehicles Exceeding 2-Hours
Center Street	E. Beyer Boulevard and E. San Ysidro Boulevard	East	3:35	5	2:26	4
		West	3:53	10	4:30	12
Louisiana Avenue	Cul-de-Sac	Both	6:00	3	3:37	5
Virginia Avenue	E. San Ysidro Boulevard and Border Village Road	East	3:47	14	4:17	23
		West	2:41	13	5:08	21
Camino De La Plaza	E. San Ysidro Boulevard and Interstate 5 southbound ramps	East	1:36	5	2:33	8
E. Beyer Boulevard	Center Street and Bolton Hall Road	North	4:17	28	5:11	31
		South	2:54	18	4:15	24
E. Beyer Boulevard	Bolton Hall Road and E. San Ysidro Boulevard	South	1:56	6	2:33	18
E. San Ysidro Boulevard	Center Street and Border Village Road (north)	North	1:18	1	1:36	1
E. San Ysidro Boulevard	Border Village Road (north) and Virginia Avenue	North	1:04	0	1:00	0
E. San Ysidro Boulevard	Louisiana Avenue and Virginia Avenue	South	1:00	0	1:20	0
E. San Ysidro Boulevard	Virginia Avenue and Border Village Road (south)	North	1:47	13	4:09	26
		South	1:33	12	2:06	26
E. San Ysidro Boulevard	Border Village Road (south) and E. Beyer Boulevard	North	2:01	4	2:00	5
		South	1:47	10	3:06	12
Bolton Hall Road	Border Village Road and E. Beyer Boulevard	North	4:30	8	5:42	7
		South	2:12	7	2:45	9
Border Village Road	E. San Ysidro Boulevard (north) and Virginia Avenue	North	1:33	6	2:00	12
		South	1:41	5	2:30	11
Border Village Road	Virginia Avenue and E. San Ysidro Boulevard (south)	North	2:17	13	3:06	28
		South	1:21	2	2:44	10
Front Street	Cul-de-Sac	Both	4:12	15	6:00	12

Source: CR Associates (2022)

Note:

¹This is calculated based on data collected on one-hour increments and does not consider vehicles spaces parked in 3-minute, 15-minute, and 30-minute spaces that may arrive and depart within the same hour of vehicle observed. Correcting for this would lower mean length of stay only for parking segments that have less than one-hour parking time restriction.

5.5 Weekend Parking Duration Observation

Drawing from the observations documented in the previous section and feedback from stakeholders, a supplementary observation was conducted to ascertain the number of vehicles parked for prolonged periods during the weekend. The emphasis was on vehicles parked for more than 9 hours or throughout the entire weekend. **Table 5.7** displays the quantity of vehicles observed to have parked for over 9 hours on both Saturday and Sunday, including those that remained parked throughout the entire weekend. During the observed period, 108 vehicles were noted to have parked for over 9 hours on Saturday, with this number rising to 138 on Sunday. Furthermore, 76 vehicles were observed to remain parked throughout the whole weekend.

Table 5.7 - Vehicles parked over 9 hours

Roadway	Segment	Side	SAT	SUN	Parked All Weekend
			9+ HRS	9+ HRS	
Camino De La Plaza	Interstate 5 southbound ramps and E. San Ysidro Boulevard	East	0	0	-
E. Beyer Boulevard	E. San Ysidro Boulevard and Center Street	North	14	16	11
Center Street	E. Beyer Boulevard and E. San Ysidro Boulevard	West	5	5	5
Center Street	E. Beyer Boulevard and E. San Ysidro Boulevard	East	2	1	1
E. Beyer Boulevard	Center Street and Bolton Hall Road	South	11	13	9
E. Beyer Boulevard	Bolton Hall Road and E. San Ysidro Boulevard	South	2	8	2
E. San Ysidro Boulevard	E. Beyer Boulevard and Border Village Road (south)	North	1	2	-
E. San Ysidro Boulevard	Border Village Road (south) and Virginia Avenue	North	11	11	7
E. San Ysidro Boulevard	Virginia Avenue and Border Village Road (north)	North	0	0	-
E. San Ysidro Boulevard	Border Village Road (north) and Center Street	North	0	1	-
Louisiana Avenue	Cul-de-Sac	Both	0	3	-
E. San Ysidro Boulevard	Louisiana Avenue and Virginia Avenue	South	0	0	-
E. San Ysidro Boulevard	Virginia Avenue and Border Village Road (south)	South	3	7	2
E. San Ysidro Boulevard	Border Village Road (south) and E. Beyer Boulevard	South	5	5	3
Border Village Road	E. San Ysidro Boulevard (south) and Virginia Avenue	North	6	12	2
Border Village Road	Virginia Avenue and E. San Ysidro Boulevard (north)	North	2	1	1
Bolton Hall Road	Border Village Road and E. Beyer Boulevard	South	7	6	6
Bolton Hall Road	E. Beyer Boulevard and Border Village Road	North	4	5	3
Border Village Road	E. San Ysidro Boulevard (north) and Virginia Avenue	South	2	5	2
Border Village Road	Virginia Avenue and E. San Ysidro Boulevard (south)	South	0	4	-
Front Street	Cul-de-Sac	Both	7	8	6
Virginia Avenue	E. San Ysidro Boulevard and Border Village Road	West	15	14	9
Virginia Avenue	Border Village Road and E. San Ysidro Boulevard	East	11	11	7
Total			108	138	76

Source: CR Associates (2022)

6.0 Curb Management Recommendations

Based on the parking utilization observed and occupancy analysis, comprehensive parking management is recommended to manage available parking supply and enhance turnover among on-street spaces. The Study determined that parking demand is generally consistent with regular daytime business hours (8 AM – 6 PM). Observations show that local residents frequently use the on-street parking spaces at the northern end of the study area, with these spots exhibiting 50% to 100% occupancy throughout the day, be it a typical weekday, Saturday, or Sunday. In the heart of the study area, where commercial and retail spaces line the streets, there's also heavy dependence on on-street parking, with peak use typically between 2 PM and 6 PM. However, field observations suggest that this demand for parking - in both public spaces and some private lots - is tied to border crossing activities rather than commercial activities along the corridor since off-street commercial lots remain relatively underutilized throughout the day.

On-street and off-street is generally available within the area. Despite this, the following street segments experienced the highest utilization, regardless of a weekday, Saturday, or Sunday:

- East Beyer Boulevard (both sides) between Center Street and Bolton Hall Road
- Center Street (both sides) between East Beyer Boulevard and East San Ysidro Boulevard
- Virginia Avenue (both sides) between East San Ysidro Boulevard and Border Village Road
- East San Ysidro Boulevard (both sides) between Virginia Avenue and Border Village Road (south)
- East San Ysidro Boulevard (south side) between Border Village Road (south) and East Beyer Boulevard
- Border Village Road (north side) between Virginia Avenue and East San Ysidro Boulevard (south)

Additionally, the Study found that certain on-street parking spaces in the study area are underutilized, particularly those farthest from the border crossing. Meanwhile, higher utilization of off-street public paid parking lots appear to be used by the public. Although there are still available spaces in these paid parking lots, the cost associated with parking in these lots may dissuade some users resulting in higher utilization of on-street parking where there is no cost to park on street. The Study also illustrates the need for additional education and enforcement to support existing time-limited parking in the area. Several parked vehicles exceeded time limits, which was much more prevalent on the weekends. It's also important to highlight the distinction amongst the parking impacts observed in the commercial areas and those within the Study's residential areas. Parking management strategies should consider impacts to the residential portion of the study area as needs for businesses and residents vary during different times of day and on weekends. In light of upcoming projects in this area such as the Border to Bayshore Bikeway, which will impact parking on East Beyer Blvd, one of the main residential streets in this study area, strategies should be explored to continue to expand mobility choices for residents.

To address many of these findings, the City should consider the development of a suite of comprehensive parking strategies. This includes an evaluation of long-term and short-term parking spaces (15-minute, passenger loading, commercial loading, taxi zones) to ensure there is adequate space to support many of the curb uses within the area. In areas where time-limited has not proven

to be effective, additional education and enforcement paired with parking meters may help improve parking turnover.

In addition to evaluating available curb space and designations, other mobility improvements can help maximize available supply and reduce demand for parking. Potential solutions to consider include:

- Streetscape and pedestrian improvements, including pedestrian-scaled lighting, enhanced crossings, and pedestrian plazas can support alternative modes of transportation.
- Mobility services such as car share, secure bike parking, on-demand shuttles, and charging infrastructure expand mobility options for those traveling within the area while reducing the reliance on a personal vehicle. Expanding mobility options has the added benefit of supporting sustainable residential developments.
- Gateway features and a wayfinding network can provide transportation information and real-time availability of parking. Information should include options for people of all modes and be available in multiple languages to ensure residents and visitors alike are aware of available options.
- Shared parking agreements and valet services can utilize available off-street parking for the public and/or residents to use, especially during seasonal increases in visitors and parking demand.

Although this parking study was primarily focused on the Border Village District, it is important to consider the importance of developing community-wide parking and mobility solutions. The development of a holistic community-wide approach will be important to ensure the influence of parking and mobility beyond the specified study area are considered.

Establishment of a community parking district (CPD) can help implement many of the strategies outlined above including management of on-street parking to support turnover in the area. This recommendation is consistent with mobility policies outlined in the adopted San Ysidro Community Plan. Per [Council Policy 100-18](#), a CPD develops and implements community-focused parking management strategies in areas that are adversely impacted by parking. A CPD can help to manage available parking supply, evaluate curb designations, and works with the City to develop a work plan and budget on an annual basis. Per Council Policy 100-18, parking meter revenue collected in the defined CPD area is re-invested back to the community and provides a sustainable funding source for a CPD to implement mobility improvements that address parking demand and supply. Establishment of a CPD also creates a public forum for the community to discuss and advise the City and other stakeholders on curb and mobility management. The CPD is required to establish a Parking Advisory Board, which may provide input on regional transportation projects underway and develop proactive initiatives to mitigate impacts on curb space and parking. CPD initiatives should be coordinated with regional efforts like the Border to Bayshore Bikeway and the San Ysidro Mobility Hub, which will impact parking in the area. Additionally, the San Ysidro Community Plan envisions an expansion of community-serving uses as well as new residential development and enhanced public spaces. As the community evolves, a CPD can ensure mobility is front and center to support changing community needs.

Appendix A - Data Collection and Calculations

THU	Roadway	Segment	Side	Supply	Occupancy										Occupancy									
					8AM-9AM	9AM-10AM	10AM-11AM	11AM-12PM	12PM-1PM	1PM-2PM	2PM-3PM	3PM-4PM	4PM-5PM	5PM-6PM	8AM-9AM	9AM-10AM	10AM-11AM	11AM-12PM	12PM-1PM	1PM-2PM	2PM-3PM	3PM-4PM	4PM-5PM	5PM-6PM
S1	Camino De La Plaza	Interstate 5 southbound ramps and E. San Ysidro Boulevard	East Side	17	3	5	6	4	3	2	2	3	1	3	18%	29%	35%	24%	18%	12%	18%	6%	18%	
S3	E. Beyer Boulevard	E. San Ysidro Boulevard and Center Street	North Side	32	22	26	24	24	20	22	22	22	31	69%	81%	75%	75%	63%	69%	62%	69%	69%	97%	
S4	Center Street	E. Beyer Boulevard and E. San Ysidro Boulevard	West Side	9	7	10	7	8	9	7	6	9	8	9	78%	111%	78%	89%	100%	78%	67%	100%	89%	100%
S5	Center Street	E. Beyer Boulevard and E. San Ysidro Boulevard	East Side	5	4	3	4	4	3	5	5	5	5	5	80%	60%	80%	80%	60%	100%	100%	100%	100%	100%
S6	E. Beyer Boulevard	Center Street and Bolton Hall Road	South Side	32	16	18	20	17	16	16	17	14	18	18	50%	56%	63%	53%	50%	50%	53%	44%	56%	56%
S7	E. Beyer Boulevard	Bolton Hall Road and E. San Ysidro Boulevard	South Side	66	4	4	6	6	13	10	7	7	5	12	6%	6%	9%	20%	15%	11%	11%	8%	18%	
S8	E. San Ysidro Boulevard	E. Beyer Boulevard and Border Village Road (south)	North Side	18	6	7	7	7	7	5	3	9	9	7	33%	39%	39%	39%	39%	28%	17%	50%	50%	39%
S9	E. San Ysidro Boulevard	Border Village Road (south) and Virginia Avenue	North Side	33	10	13	12	21	1	17	18	18	15	17	30%	39%	36%	64%	64%	52%	55%	55%	45%	52%
S10	E. San Ysidro Boulevard	Virginia Avenue and Border Village Road (north)	North Side	9	0	2	2	6	4	3	3	1	3	3	0%	22%	22%	67%	44%	33%	33%	11%	33%	33%
S11	E. San Ysidro Boulevard	Border Village Road (north) and Center Street	North Side	5	0	1	2	3	2	1	2	2	2	2	0%	20%	40%	60%	40%	20%	40%	40%	40%	40%
S14	Louisiana Avenue	Cul-de-Sac	Both Sides	4	2	2	3	3	4	2	2	2	2	2	50%	50%	75%	75%	100%	50%	50%	50%	50%	50%
S15	E. San Ysidro Boulevard	Louisiana Avenue and Virginia Avenue	South Side	5	2	2	0	0	0	0	0	0	0	1	40%	40%	0%	0%	0%	0%	0%	0%	0%	20%
S16	E. San Ysidro Boulevard	Virginia Avenue and Border Village Road (south)	South Side	28	4	6	11	16	24	23	19	23	16	26	14%	21%	39%	57%	86%	82%	68%	82%	57%	93%
S17	E. San Ysidro Boulevard	Border Village Road (south) and E. Beyer Boulevard	South Side	14	6	4	8	5	4	6	6	10	14	12	43%	29%	57%	36%	29%	43%	43%	71%	100%	86%
S18	Border Village Road	E. San Ysidro Boulevard (south) and Virginia Avenue	North Side	25	9	11	9	10	11	9	11	15	17	22	36%	44%	36%	40%	44%	36%	44%	60%	68%	88%
S19	Border Village Road	Virginia Avenue and E. San Ysidro Boulevard (north)	North Side	44	12	11	7	10	5	5	6	7	10	27%	25%	16%	23%	11%	11%	11%	14%	16%	23%	25%
S20	Bolton Hall Road	Border Village Road and E. Beyer Boulevard	South Side	12	8	12	8	9	10	10	8	11	10	11	67%	100%	67%	75%	83%	83%	67%	92%	83%	92%
S21	Bolton Hall Road	E. Beyer Boulevard and Border Village Road	North Side	12	9	8	8	8	7	5	4	4	5	5	75%	67%	67%	67%	58%	42%	33%	33%	42%	42%
S22	Border Village Road	E. San Ysidro Boulevard (north) and Virginia Avenue	South Side	55	10	6	11	6	9	8	10	4	14	13	18%	11%	20%	11%	16%	15%	18%	7%	25%	24%
S23	Border Village Road	Virginia Avenue and E. San Ysidro Boulevard (south)	South Side	14	3	2	1	3	0	1	0	3	3	7	21%	14%	7%	21%	0%	7%	0%	21%	21%	50%
S24	Front Street	Cul-de-Sac	Both Sides	16	9	2	12	9	8	9	12	10	11	16	56%	13%	75%	56%	50%	56%	75%	63%	69%	100%
S26	Virginia Avenue	E. San Ysidro Boulevard and Border Village Road	West Side	23	9	10	9	8	9	15	13	11	11	10	39%	43%	39%	35%	39%	65%	57%	48%	48%	43%
S27	Virginia Avenue	Border Village Road and E. San Ysidro Boulevard	East Side	19	16	14	10	13	12	11	15	12	11	11	84%	74%	53%	68%	63%	58%	79%	63%	58%	58%
SAT	Roadway	Segment	Side	Supply	Occupancy										Occupancy									
					8AM-9AM	9AM-10AM	10AM-11AM	11AM-12PM	12PM-1PM	1PM-2PM	2PM-3PM	3PM-4PM	4PM-5PM	5PM-6PM	8AM-9AM	9AM-10AM	10AM-11AM	11AM-12PM	12PM-1PM	1PM-2PM	2PM-3PM	3PM-4PM	4PM-5PM	5PM-6PM
S1	Camino De La Plaza	Interstate 5 southbound ramps and E. San Ysidro Boulevard	East Side	17	10	8	2	4	5	3	3	3	0	59%	47%	12%	24%	29%	18%	18%	18%	18%	0%	
S3	E. Beyer Boulevard	E. San Ysidro Boulevard and Center Street	North Side	32	21	29	30	32	28	26	32	29	28	26	97%	91%	94%	100%	88%	81%	100%	91%	88%	81%
S4	Center Street	E. Beyer Boulevard and E. San Ysidro Boulevard	West Side	9	8	9	9	9	9	6	9	9	10	12	89%	100%	100%	100%	100%	67%	100%	100%	111%	133%
S5	Center Street	E. Beyer Boulevard and E. San Ysidro Boulevard	East Side	5	7	3	3	6	2	3	4	3	3	5	140%	60%	60%	120%	40%	60%	60%	60%	60%	100%
S6	E. Beyer Boulevard	Center Street and Bolton Hall Road	South Side	32	30	24	25	23	26	23	21	21	27	94%	75%	78%	72%	81%	72%	66%	66%	66%	84%	
S7	E. Beyer Boulevard	Bolton Hall Road and E. San Ysidro Boulevard	South Side	66	16	16	15	16	14	14	15	14	12	16	24%	24%	23%	24%	21%	21%	23%	21%	18%	24%
S8	E. San Ysidro Boulevard	E. Beyer Boulevard and Border Village Road (south)	North Side	18	6	9	5	4	5	3	3	4	3	3	33%	50%	28%	22%	28%	17%	17%	22%	17%	17%
S9	E. San Ysidro Boulevard	Border Village Road (south) and Virginia Avenue	North Side	33	14	13	13	14	16	19	23	24	24	42%	39%	39%	42%	48%	58%	70%	73%	73%	73%	
S10	E. San Ysidro Boulevard	Virginia Avenue and Border Village Road (north)	North Side	9	1	5	4	4	3	0	3	0	1	0	11%	56%	44%	44%	33%	0%	33%	0%	11%	0%
S11	E. San Ysidro Boulevard	Border Village Road (north) and Center Street	North Side	5	0	0	2	1	3	1	0	1	0	0	0%	0%	40%	20%	60%	20%	0%	20%	0%	0%
S14	Louisiana Avenue	Cul-de-Sac	Both Sides	4	0	2	2	3	3	6	3	4	3	3	0%	50%	50%	75%	75%	150%	75%	100%	75%	75%
S15	E. San Ysidro Boulevard	Louisiana Avenue and Virginia Avenue	South Side	5	1	0	1	0	0	0	0	0	0	1	20%	0%	20%	0%	0%	0%	0%	0%	0%	20%
S16	E. San Ysidro Boulevard	Virginia Avenue and Border Village Road (south)	South Side	28	13	15	24	21	19	20	23	25	21	19	46%	54%	86%	75%	68%	71%	82%	89%	75%	68%
S17	E. San Ysidro Boulevard	Border Village Road (south) and E. Beyer Boulevard	South Side	14	14	11	9	11	10	10	9	10	11	10	100%	79%	64%	79%	71%	71%	64%	71%	79%	71%
S18	Border Village Road	E. San Ysidro Boulevard (south) and Virginia Avenue	North Side	25	15	18	16	21	23	29	23	25	23	25	60%	72%	64%	84%	92%	116%	92%	100%	92%	100%
S19	Border Village Road	Virginia Avenue and E. San Ysidro Boulevard (north)	North Side	44	15	16	11	11	12	13	11	6	10	13	34%	36%	25%	25%	27%	30%	25%	14%	23%	30%
S20	Bolton Hall Road	Border Village Road and E. Beyer Boulevard	South Side	12	11	12	10	13	10	11	11	11	12	12	92%	100%	83%	108%	83%	83%	92%	92%	92%	100%
S21	Bolton Hall Road	E. Beyer Boulevard and Border Village Road	North Side	12	7	7	7	5	5	7	4	5	5	5	58%	58%	58%	42%	42%	42%	58%	33%	42%	42%
S22	Border Village Road	E. San Ysidro Boulevard (north) and Virginia Avenue	South Side	55	10	10	11	8	8	11	7	6	5	10	18%	18%	20%	15%	15%	20%	13%	11%	9%	18%
S23	Border Village Road	Virginia Avenue and E. San Ysidro Boulevard (south)	South Side	14	2	6	1	1	0	0	5	5	5	9	14%	43%	7%	7%	0%	0%	36%	36%	36%	64%
S24	Front Street	Cul-de-Sac	Both Sides	16	10	10	10	10	9	10	12	10	10	11	63%	63%	63%	63%	56%	63%	75%	63%	63%	69%
S26	Virginia Avenue	E. San Ysidro Boulevard and Border Village Road	West Side	23	21	22	22	22	21	23	20	20	21	19	91%	96%	96%	96%	91%	100%	87%	87%	91%	83%
S27	Virginia Avenue	Border Village Road and E. San Ysidro Boulevard	East Side	19	19	20	21	20	19	18	17	17	18	18	100%	105%	111%	105%	100%	95%	89%	89%	95%	95%
SUN	Roadway	Segment	Side	Supply	Occupancy										Occupancy									
					8AM-9AM	9AM-10AM	10AM-11AM	11AM-12PM	12PM-1PM	1PM-2PM	2PM-3PM	3PM-4PM	4PM-5PM	5PM-6PM	8AM-9AM	9AM-10AM	10AM-11AM	11AM-12PM	12PM-1PM	1PM-2PM	2PM-3PM	3PM-4PM	4PM-5PM	5PM-6PM
S1	Camino De La Plaza	Interstate 5 southbound ramps and E. San Ysidro Boulevard	East Side	17	0	0	0	0	0	0	0	0	0	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
S3	E. Beyer Boulevard	E. San Ysidro Boulevard and Center Street	North Side	32	28	30	28	33	31	34	31	28	30	35	88%	94%	88%	103%	97%	106%	97%	88%	94%	109%
S4	Center Street	E. Beyer Boulevard and E. San Ysidro Boulevard	West Side	9	10	8	6	7	6	7	10	10	9	10	111%	89%	67%	78%	67%	78%	111%	111%	100%	111%
S5	Center Street	E. Beyer Boulevard and E. San Ysidro Boulevard	East Side	5	2	1	3	3	2	3	3	3	3	5	40%	40%	60%	60%	40%	60%	60%	60%	60%	100%
S6	E. Beyer Boulevard	Center Street and Bolton Hall Road	South Side	32	21	29	30	20	19	20	21	22	21	17	66%	59%	56%	63%	59%	63%	66%	69%	66%	53%
S7	E. Beyer Boulevard	Bolton Hall Road and E. San Ysidro Boulevard	South Side	66	15	20	17	15	16	13	15	17	20	21	23%	30%	26%	23%	24%	20%	23%	26%	30%	32%
S8	E. San Ysidro Boulevard	E. Beyer Boulevard and Border Village Road (south)	North Side	18	5	4	7	5	8	7	7	10	7	7	28%	22%	39%	28%	44%	39%	39%	56%	39%	39%
S9	E. San Ysidro Boulevard	Border Village Road (south) and Virginia Avenue	North Side	33	16	15	17	22	22	21	24	24	27	25	48%	45%	52%	67%	67%	64%	73%	73%	82%	76%
S10	E. San Ysidro Boulevard	Virginia Avenue and Border Village Road (north)	North Side	9	0	1	0	0	0	0	0	0	0	0	0%	11%	0%	0%	0%	0%	0%	0%	0%	0%
S11	E. San Ysidro Boulevard	Border Village Road (north) and Center Street	North Side	5	1	1	1	1	1	1	1	1	1	1	20%	20%	20%	20%	20%	20%	20%	20%	20%	20%
S14	Louisiana Avenue	Cul-de-Sac	Both Sides	4	3	3	4	4	3	3	3	4	3	3	75%	75%	100%	100%	75%	75%	100%	75%	100%	75%
S15	E. San Ysidro Boulevard	Louisiana Avenue and Virginia Avenue	South Side	5	0	0	0	0	0	0	0	3	0	1	0%	0%	0%	0%	0%	0%	0%	60%	0%	20%
S16	E. San Ysidro Boulevard	Virginia Avenue and Border Village Road (south)	South Side	28	22	22	20	18	20	20	23	25	27	27	79%	79%	71%	64%	71%	71%	82%	89%	96%	96%
S17	E. San Ysidro Boulevard	Border Village Road (south) and E. Beyer Boulevard	South Side	14	9	11	13	10	10	10	11	11	10	8	64%	79%	93%	71%	71%	71%	79%	79%	71%	57%

Appendix B - Occupancy Results – All Periods

Weekday Occupancy Results

Weekday Parking Occupancy – Morning (8:00 AM to 10:00 AM)

During this period, most of the on-street parking without a time limit were observed with occupancy between 50% and 84%. The majority of these segments are surrounded by residential land uses. Only two of these segments are located outside of the residential area and serve commercial and retail land uses, which is the eastern side of Virginia Avenue and the Front Street cul-de-sac. Almost all other on-street parking in the study area were observed with less than 50% occupancy. It is probable that since the collection took place during the morning commute peak period, the parked vehicles of many residents within the study area who work regular business hours and had not yet left for work were captured during the data collection.

Weekday Parking Occupancy – Late Morning (10:00 AM to 12:00 PM)

During this period, on-street parking with a 2-hour parking limit fronting commercial and retail land uses were observed with occupancy between 57% and 69%. This pattern of usage coincides with typical business hours associated with commercial and retail land uses that these on-street parking spaces serve.

Additionally, on-street parking without a time limit were observed with occupancy between 63% and 75% when compared to the 8AM to 10AM period previously discussed.

Weekday Parking Occupancy – Midday (12:00 PM to 2:00 PM)

During this period, on-street parking with a 2-hour parking limit fronting commercial and retail land uses, particularly along East San Ysidro Boulevard between Virginia Avenue and Border Village Road (south), were observed with increased occupancy with one segment greater than 85% occupancy.

Additionally, on-street parking without a time limit had observed occupancy between 50% and 69%, the lowest percentage for this classification of parking across all observed periods on a typical weekday.

Weekday Parking Occupancy – Afternoon (2:00 PM to 4:00 PM)

During this period, on-street parking with and without time limit parking fronting commercial and retail land uses along East San Ysidro Boulevard and Virginia Avenue were observed to increase in occupancy to ranges between 55% and 82% when compared to the previous period (midday).

On-street parking without a time limit fronting residential land uses were also observed to increase in occupancy when compared to the previous period (midday) with occupancies between 53% and 69%.

Weekday Parking Occupancy – Late Afternoon (4:00 PM to 6:00 PM)

During this period, the on-street parking, regardless of time limit, experienced the highest percentage of occupancy with nearly a third of the segments with greater than 85% occupancy. This pattern may be due to patrons making trips to commercial and retail land uses in the evening. For on-street parking adjacent to residential land uses, residents may also be commuting home from work where they park in on-street parking locations and where they will remain for the rest of the night.

Saturday Occupancy Results

Saturday Parking Occupancy – Morning (8:00 AM to 10:00 AM)

During this period, most of the on-street parking without a time limit was observed with occupancy greater than 85%. The majority of these segments are surrounded by residential land uses. Only three of these segments are located outside of the residential area and serve commercial and retail land uses, which are along Virginia Avenue and Bolton Hall Road.

On-street parking with 2-hour limit along East San Ysidro Boulevard between Border Village Road (south) and East Beyer Boulevard was observed with occupancy greater than 85%. This may be due to the Tianguis Swap Meet that takes place Friday through Sunday. The same patrons that visit the Tianguis Swap Meet may also park along Border Village Road and East San Ysidro Boulevard to the north just before Virginia Avenue as these segments were observed with 54% to 72% occupancy.

Lastly, it is during this period that taxis along Camino De La Plaza were observed with 59% occupancy, the highest observed among all five observed periods.

Saturday Parking Occupancy – Late Morning (10:00 AM to 12:00 PM)

During this period, fronting residential land use were observed with slightly less occupancy when compared to the previous period (morning). One segment drops from greater than 85% occupancy to 78% occupancy.

Additionally, the on-street parking segment with 2-hour limit with greater than 85% occupancy fronting the Tianguis Swap Meet appear to shift north along East San Ysidro Boulevard near commercial and retail land uses when compared to the previous period (morning). The segment fronting the swap meet was observed with 79% occupancy.

Saturday Parking Occupancy – Midday (12:00 PM to 2:00 PM)

During this period, on-street parking occupancy were observed to be very similar within the residential areas when compared to the previous period (late morning). Study segments fronting commercial and retail land uses along East San Ysidro Boulevard, Virginia Avenue, and Border Village Road (south of Virginia Avenue) were observed with occupancy between 58% and 71% with three segments greater than 85%.

Saturday Parking Occupancy – Afternoon (2:00 PM to 4:00 PM)

During this period, on-street parking occupancy were observed to be similar within residential areas along East Beyer Boulevard with a slight increase of occupancy at adjacent roadway segments of Center Street and Bolton Hall Road when compared to the previous period (midday).

Additionally, the three on-street parking with 2-hour limit fronting commercial and retail land uses along East San Ysidro Boulevard, Virginia Avenue, and Border Village Road (south of Virginia Avenue) were observed with continued pattern of greater than 85% occupancy. Two other adjacent segments were observed with increased occupancy between 73% and 75% (north side of East San Ysidro Boulevard and the Front Street cul-de-sac).

Saturday Parking Occupancy – Late Afternoon (4:00 PM to 6:00 PM)

During this period, on-street parking occupancy were observed to be similar within residential areas when compared to the previous period (afternoon).

Additionally, the three on-street parking with 2-hour limit fronting commercial and retail land uses along East San Ysidro Boulevard, Virginia Avenue, and Border Village Road (south of Virginia Avenue) were observed with continued pattern of greater than 85% occupancy. The south side of Border Village Road south of Virginia Avenue increased in occupancy to 64% while two other adjacent segments were observed with decreased occupancy (south side of East San Ysidro Boulevard and the Front Street cul-de-sac) when compared to previous period (afternoon). This pattern may be directly related to the closing hours of the Tianguis Swap Meet which ends at 4:00 PM Friday through Sunday.

Sunday Occupancy Results

Sunday Parking Occupancy – Morning (8:00 AM to 10:00 AM)

During this period, most of the on-street parking without a time limit were observed with occupancy greater than 85%. The majority of these segments are surrounded by residential land uses. Only two of these segments are located outside of the residential area and serve commercial and retail land uses, which are along Virginia Avenue.

On-street parking with 2-hour limit along the south side of East San Ysidro Boulevard between Virginia Avenue and Border Village Road (south) as well as East San Ysidro Boulevard, south of Border Village Road, was observed with 79% occupancy. Border Village Road, south of Virginia Avenue, was also observed with high occupancy with the south side at 64% occupancy and the north side greater than 85%. This may be due to the Tianguis Swap Meet that takes place Friday through Sunday as these patrons may park along these segments.

Sunday Parking Occupancy – Late Morning (10:00 AM to 12:00 PM)

During this period, fronting residential land use were observed with slightly increased occupancy when compared to the previous period (morning). Two segments increase from less than 50% occupancy to 60% occupancy. On-street parking without time restrictions along Virginia Avenue were observed with continued greater than 85% occupancy.

Additionally, the on-street parking segment with 2-hour limit along the north side of East San Ysidro Boulevard between Virginia Avenue and Border Village Road (south) as well as south side of East San Ysidro Boulevard fronting the Tianguis Swap Meet were observed with increased occupancy when compared to the previous period (morning). This may be related to increased number of visitors to the Tianguis Swap Meet or patrons making trips to adjacent commercial and retail land uses.

Sunday Parking Occupancy – Midday (12:00 PM to 2:00 PM)

During this period, on-street parking occupancy were observed to be very similar within the residential areas when compared to the previous period (late morning). Study segments fronting commercial and retail land uses along East San Ysidro Boulevard, Virginia Avenue, and Border Village Road (south of Virginia Avenue) were observed with similar occupancy percentages when compared to previous period (late morning). Only the west side of Virginia Avenue and the south side East San Ysidro Boulevard that fronts the Tianguis Swap Meet were observed with slight decreases in occupancy percentages from greater than 85% to 83% and 71%, respectively.

Sunday Parking Occupancy – Afternoon (2:00 PM to 4:00 PM)

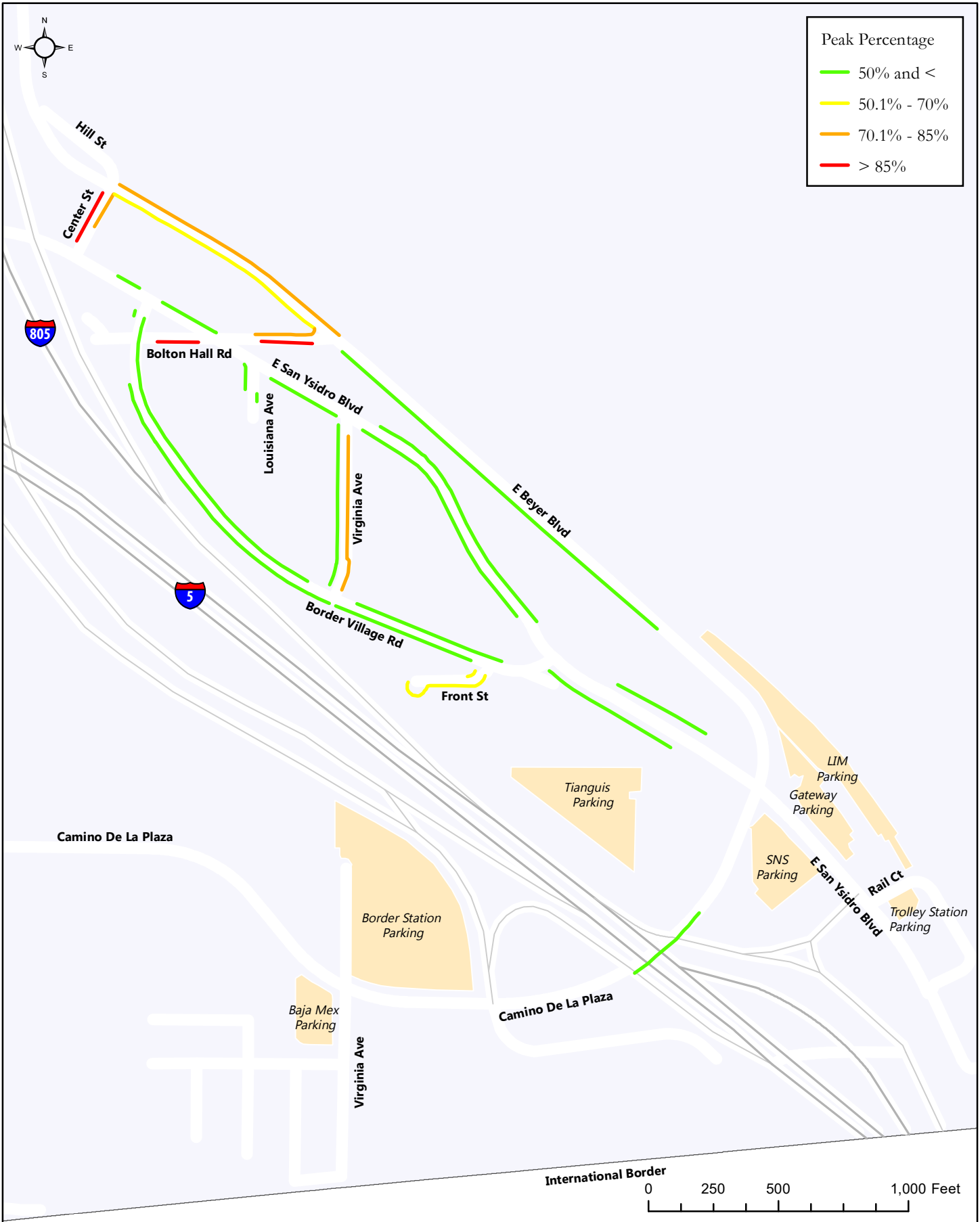
During this period, on-street parking occupancy were observed to be similar within residential areas along East Beyer Boulevard with a slight increase of occupancy at the adjacent roadway segment of Center Street when compared to the previous period (midday).

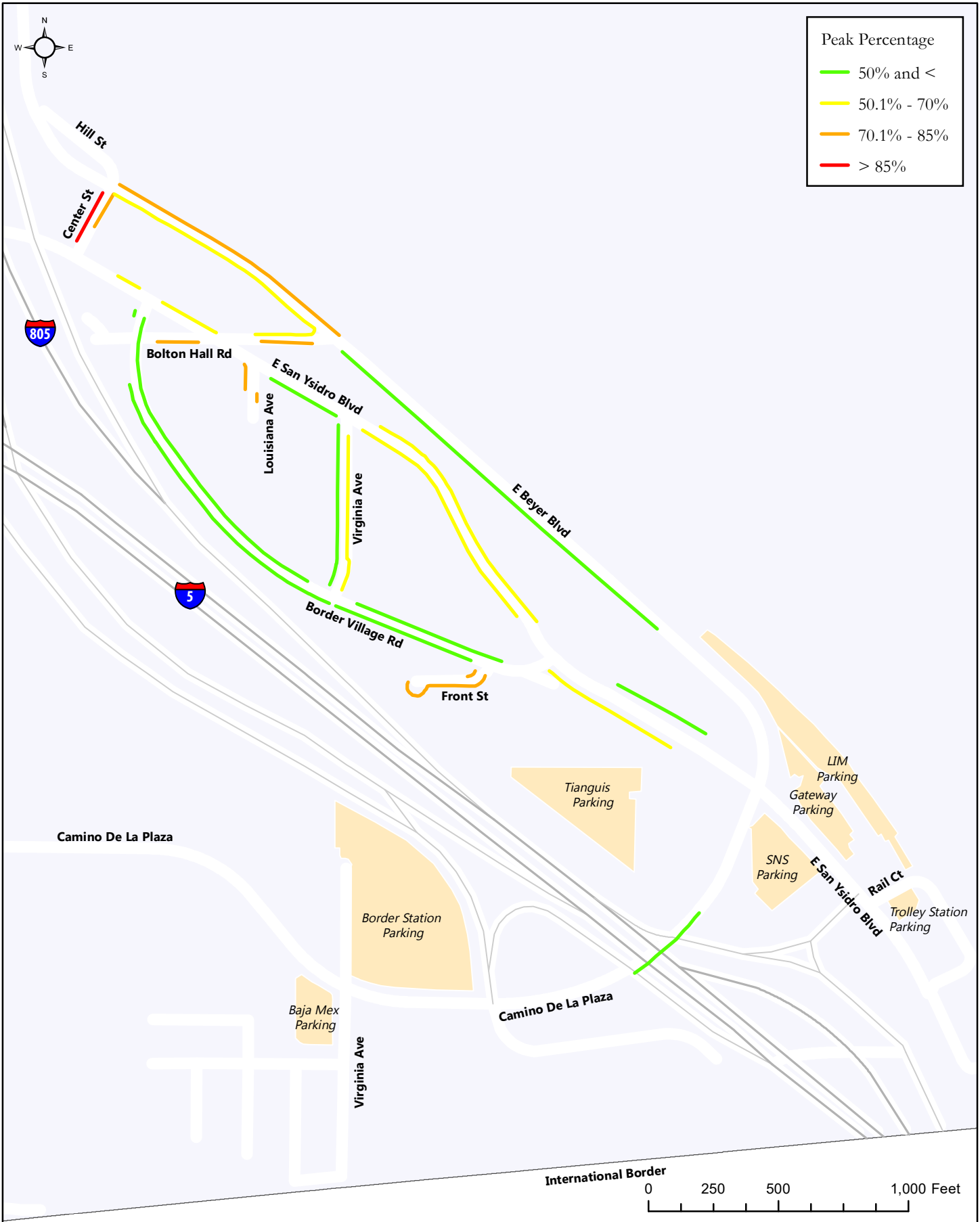
Additionally, seven street segments with on-street parking fronting commercial and retail land uses along East San Ysidro Boulevard, Virginia Avenue, and Border Village Road (south of Virginia Avenue) were observed with 73% or greater occupancy making this the busiest period of the day. Front Street was also observed with 56% occupancy, the highest percentage among all observed periods.

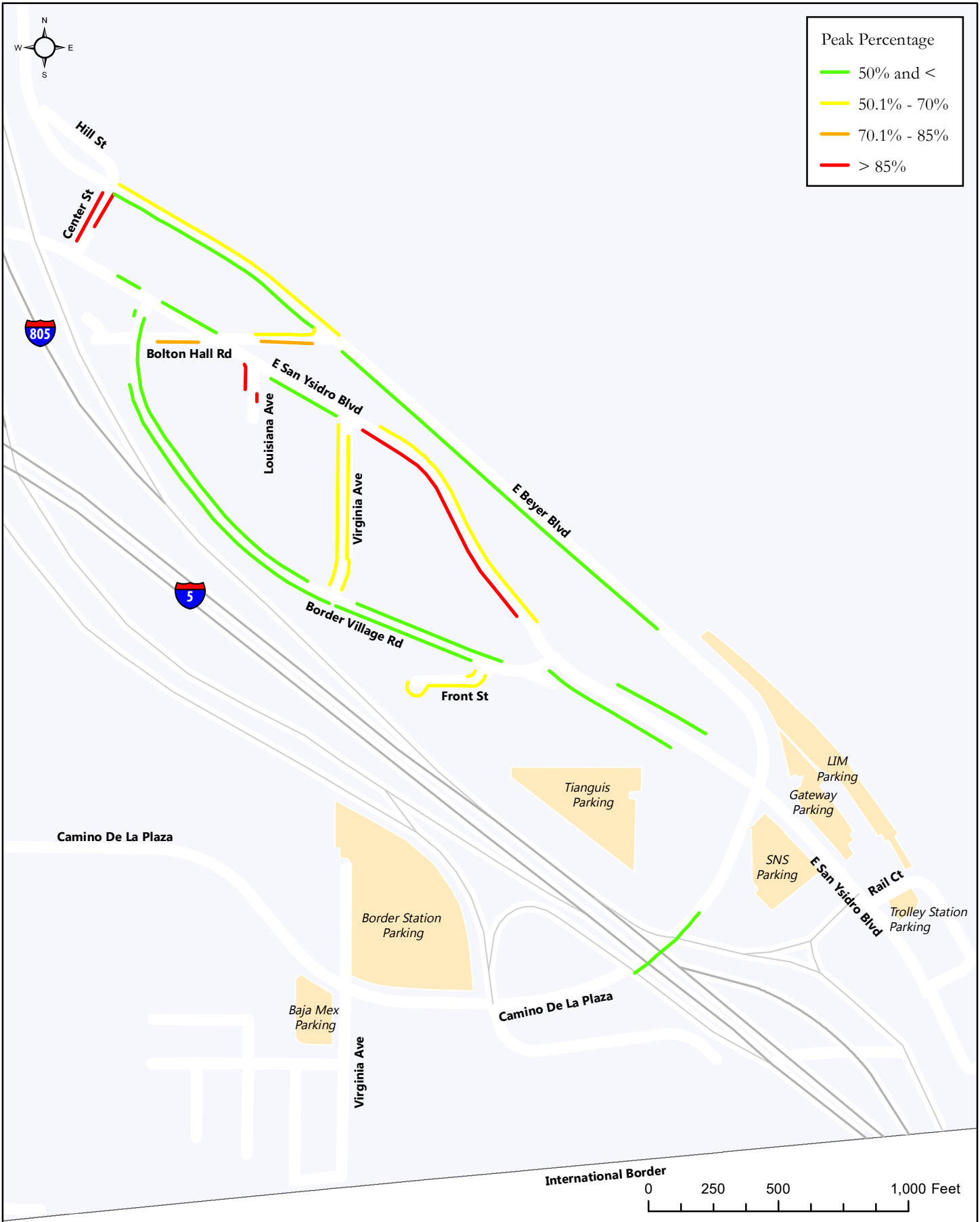
Sunday Parking Occupancy – Late Afternoon (4:00 PM to 6:00 PM)

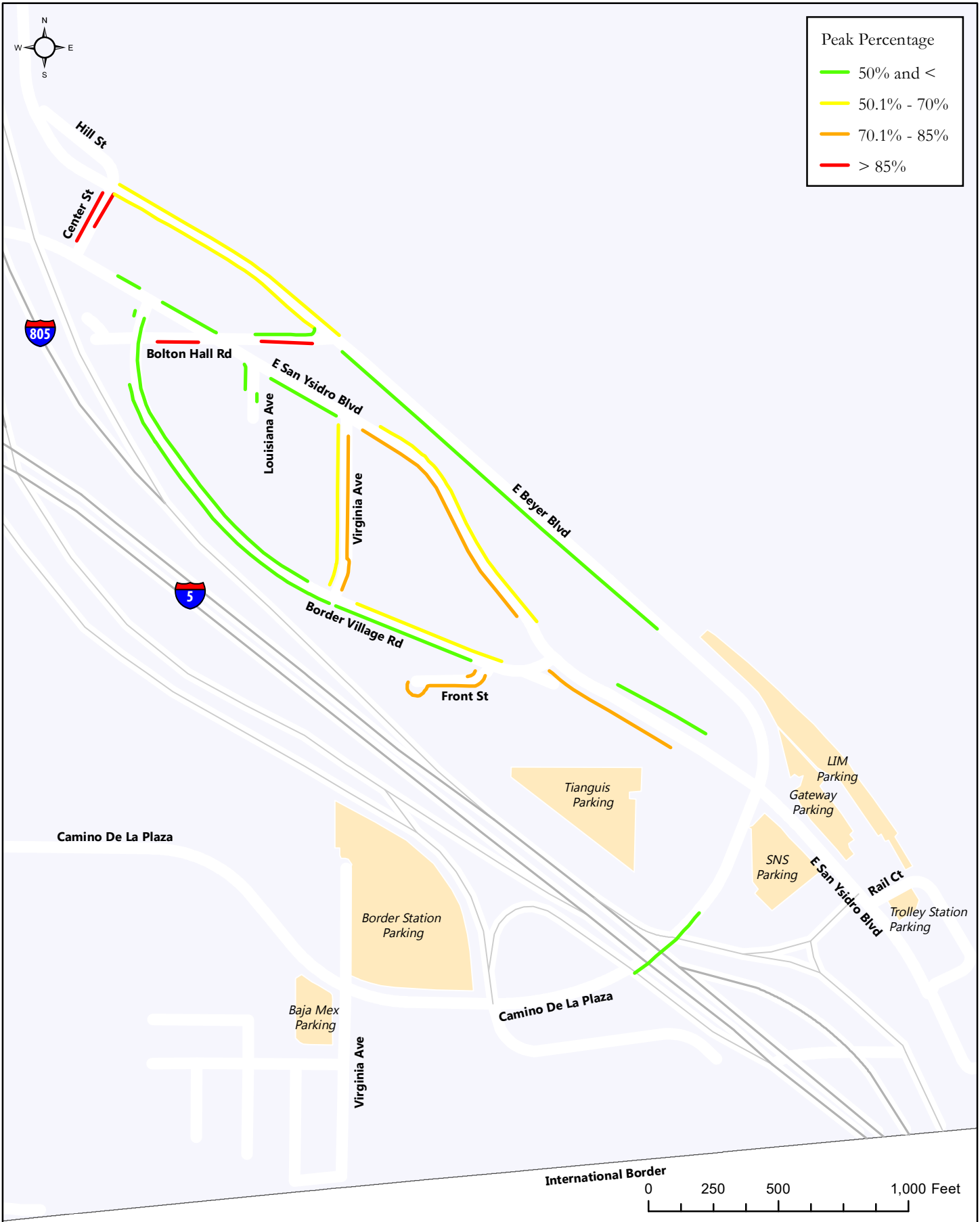
During this period, on-street parking occupancy were observed to be similar within residential areas when compared to the previous period (afternoon) with slight increases along adjacent street segments of Center Street and the south side of Bolton Hall Road. This may be due to residents coming home for the day and parking their vehicles where on-street parking without time limits exist and where they will remain for the rest of the night.

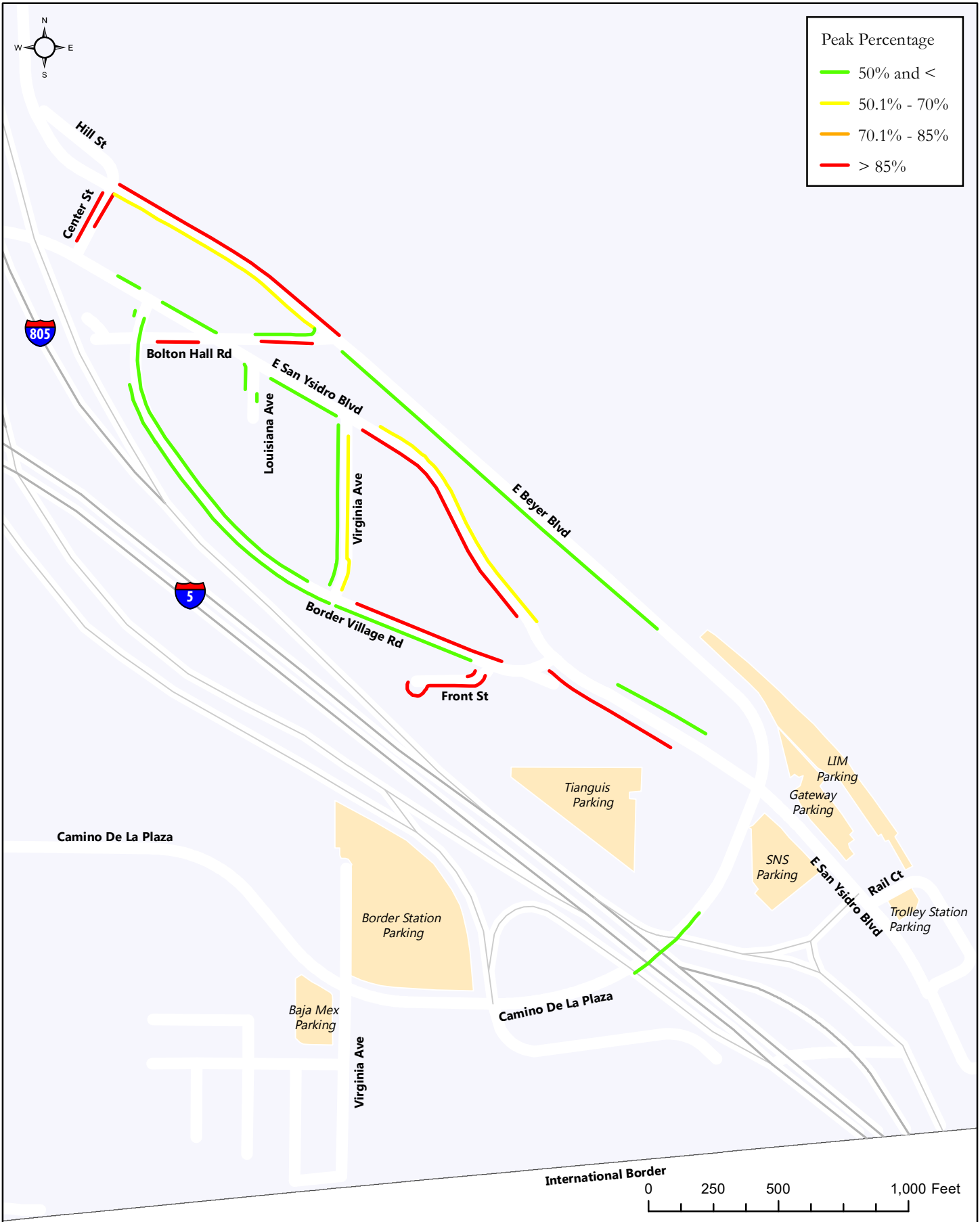
Additionally, on-street parking fronting commercial and retail land uses were observed with similar occupancy percentages when compared to the previous period (afternoon).

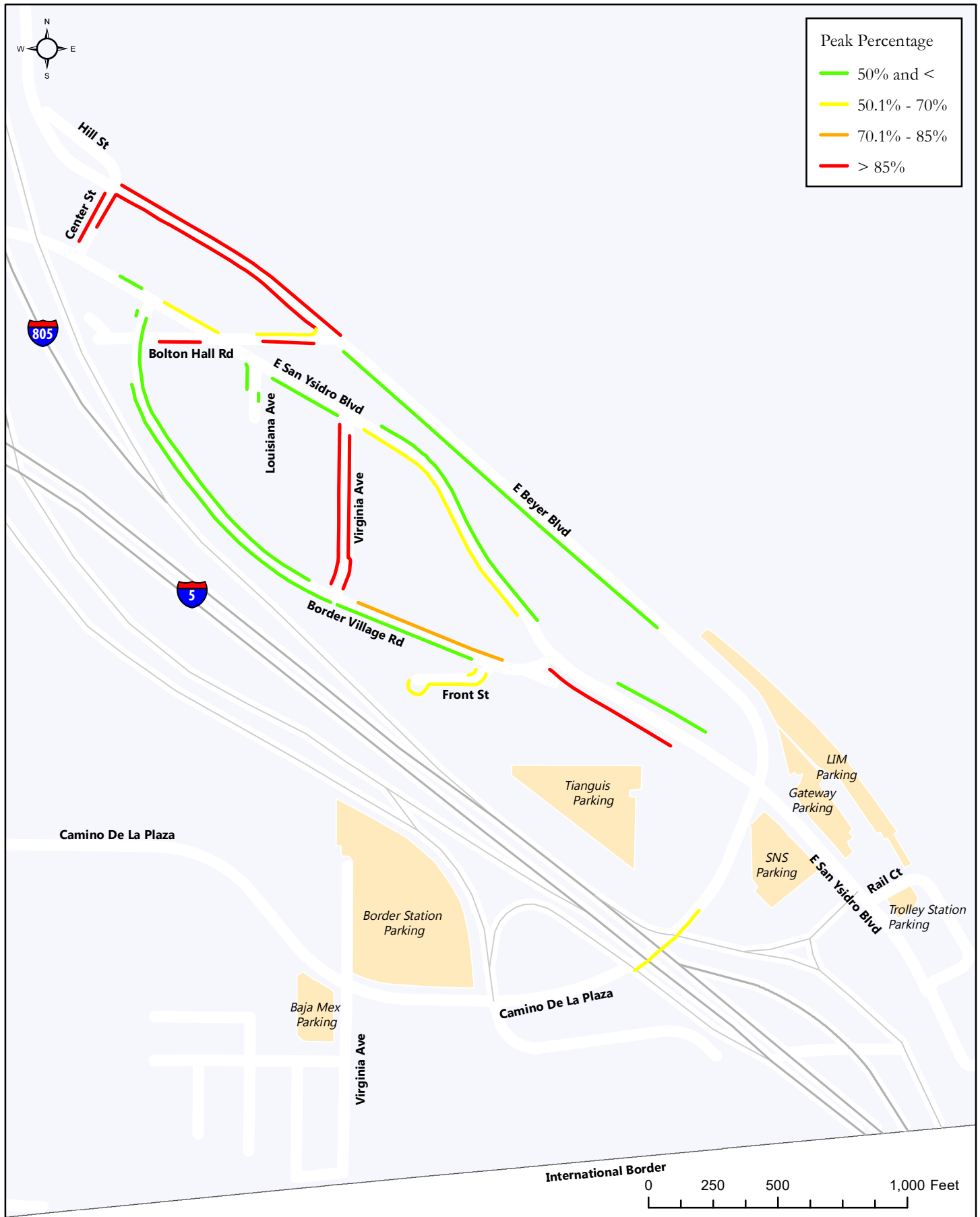


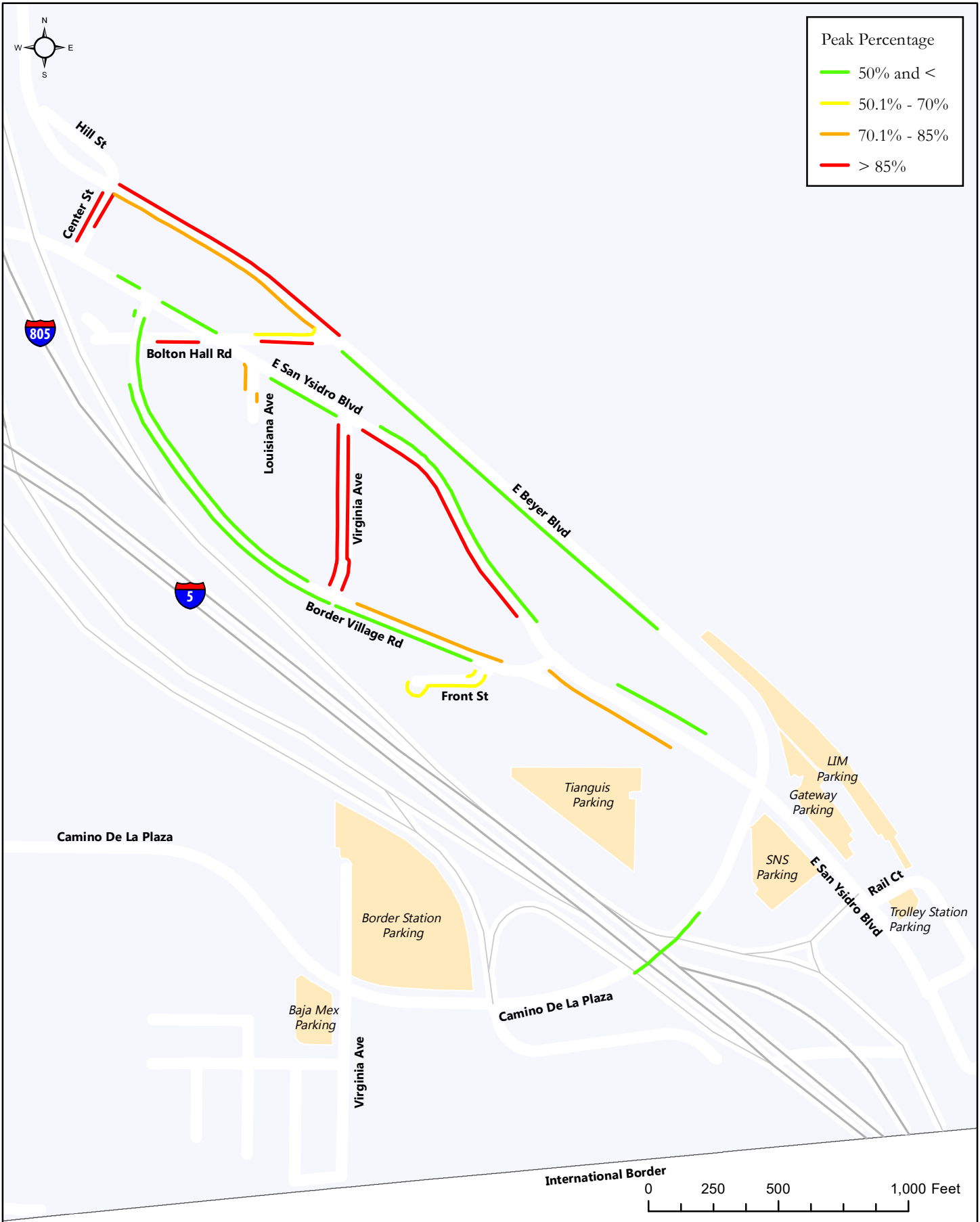


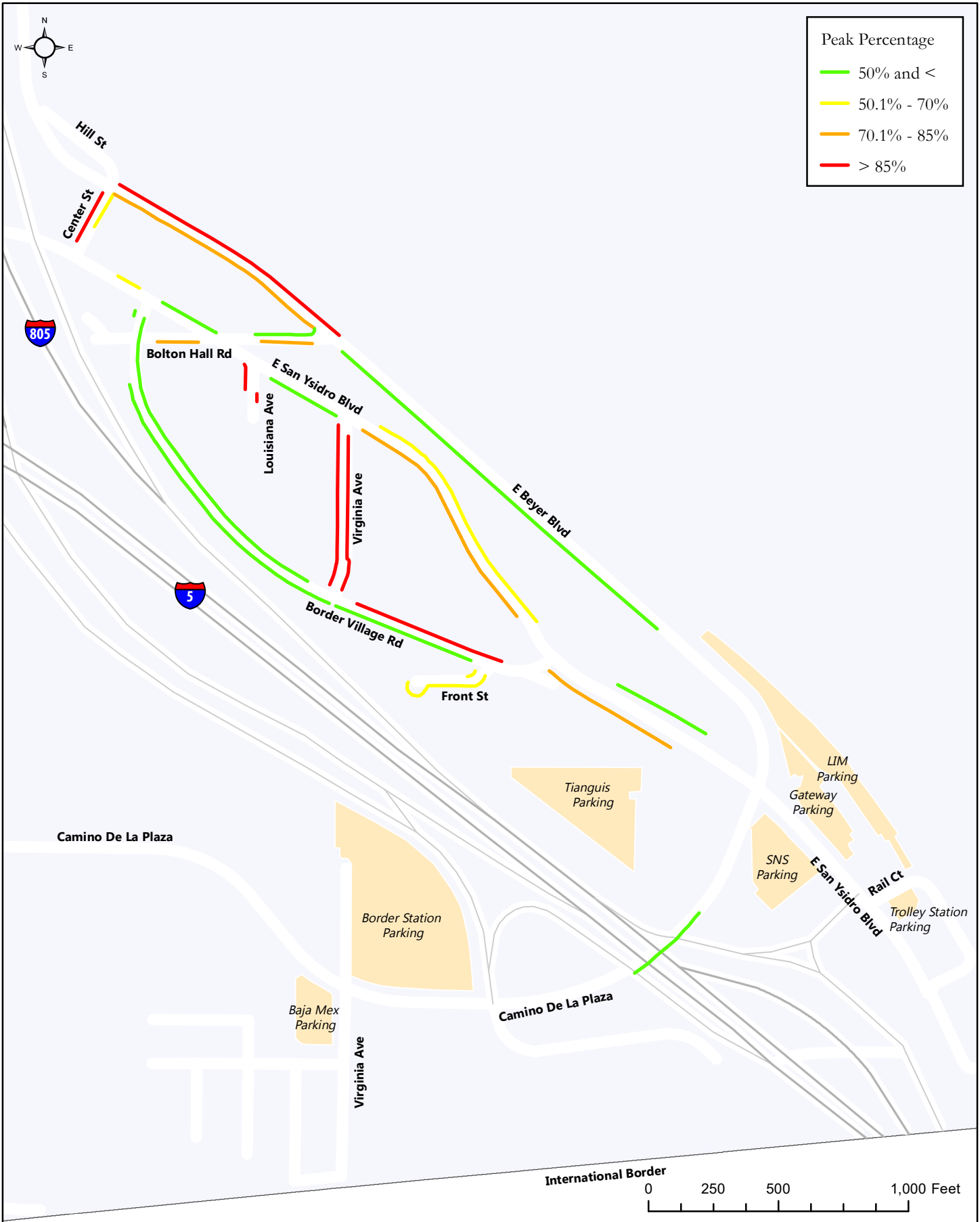


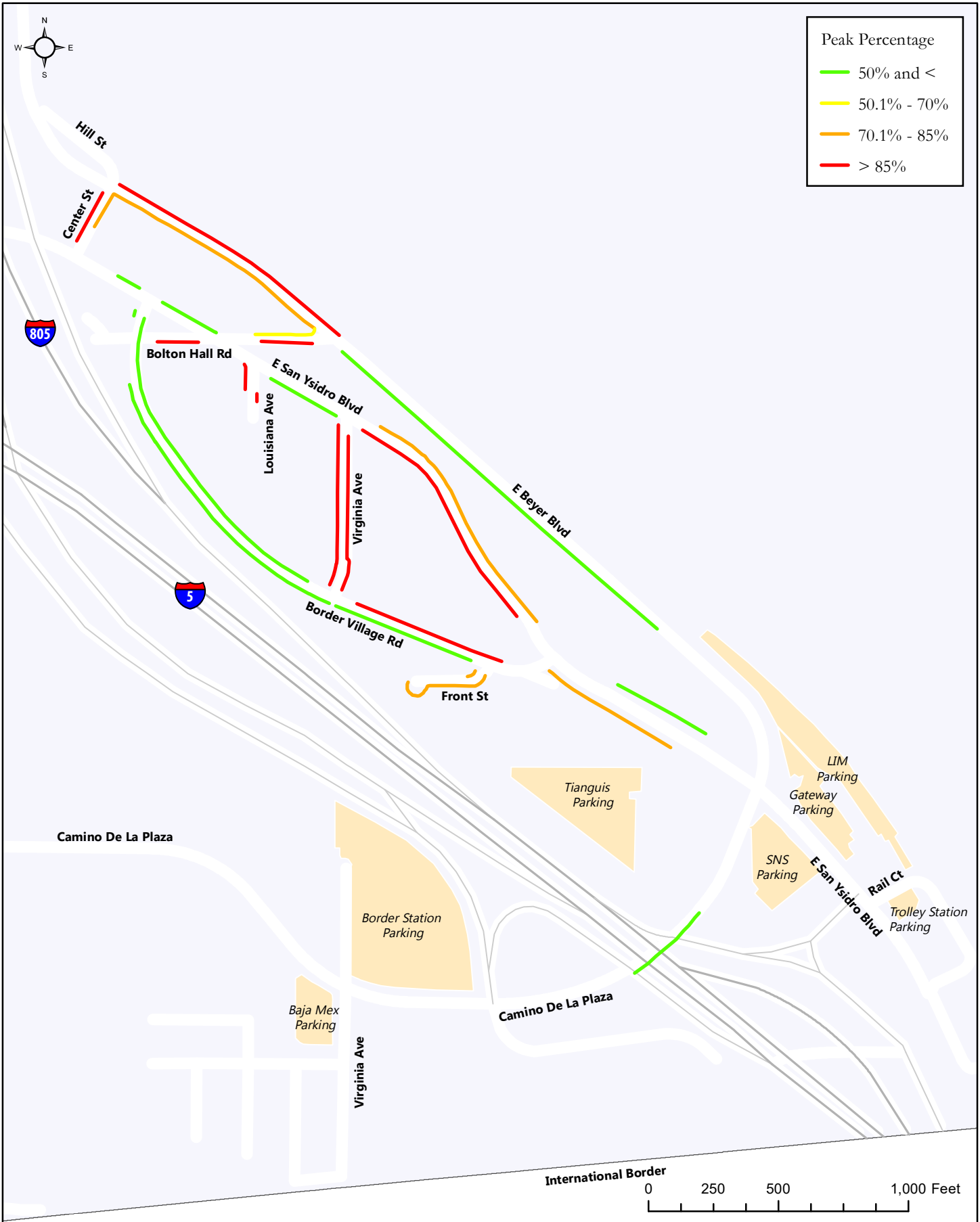


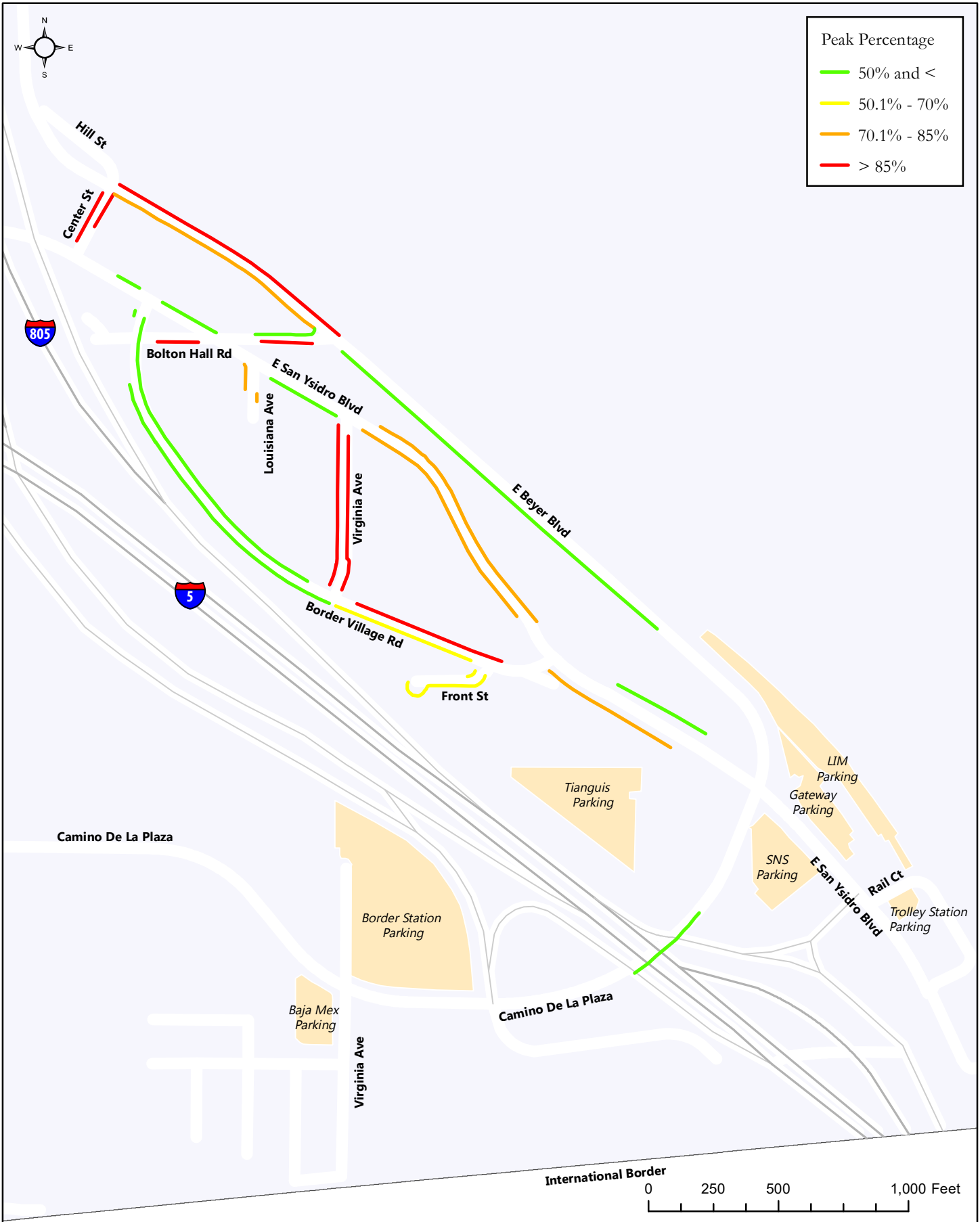


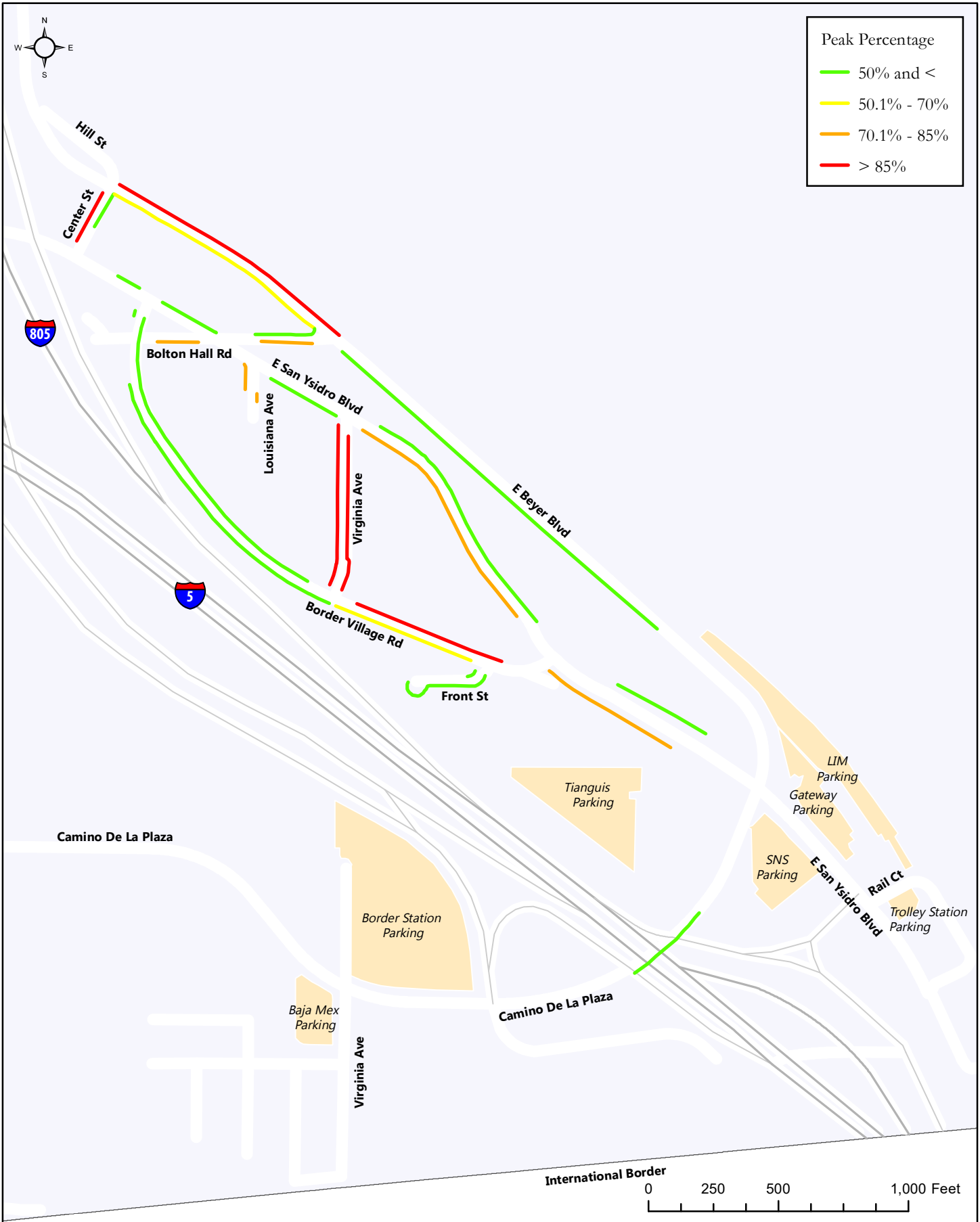


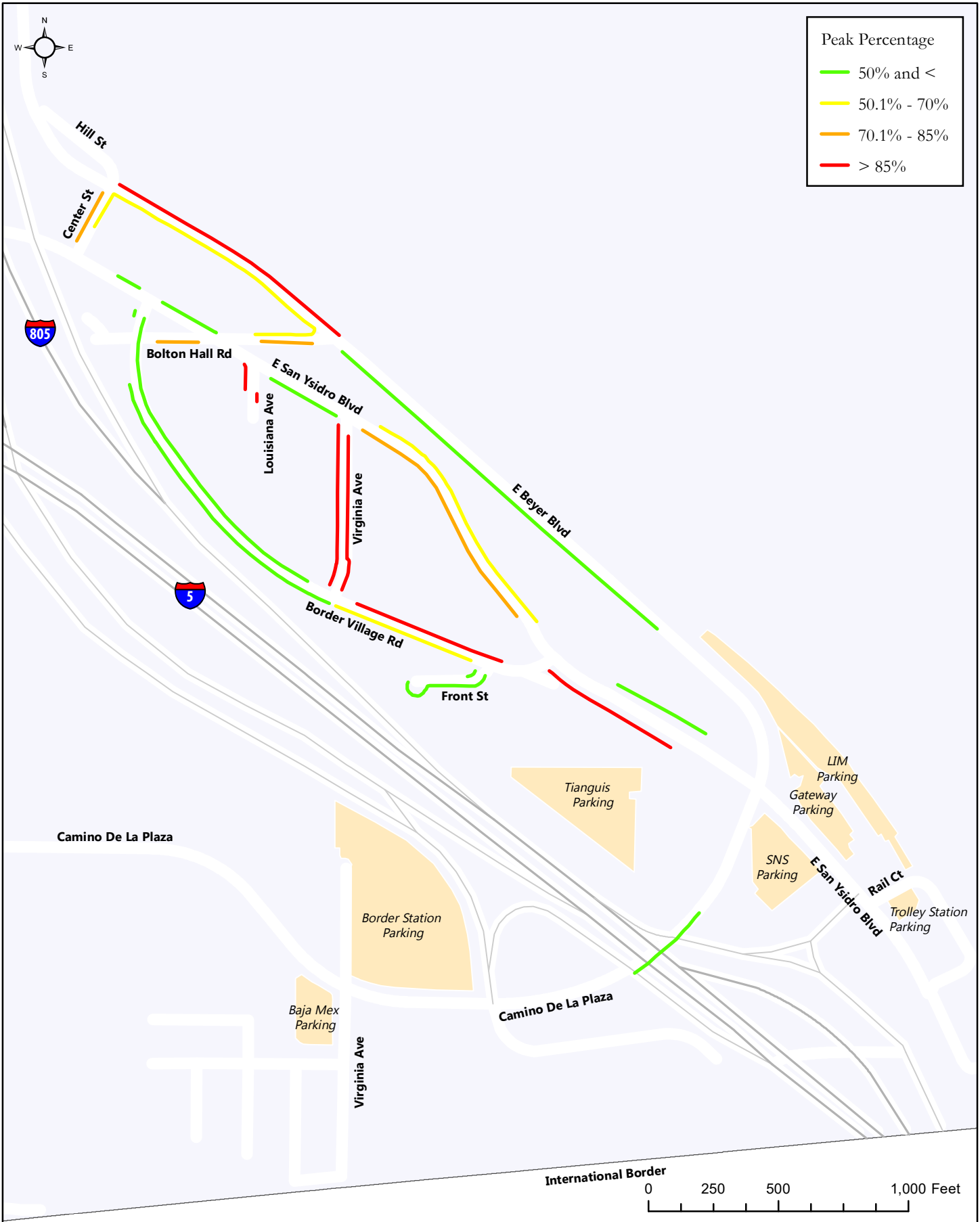


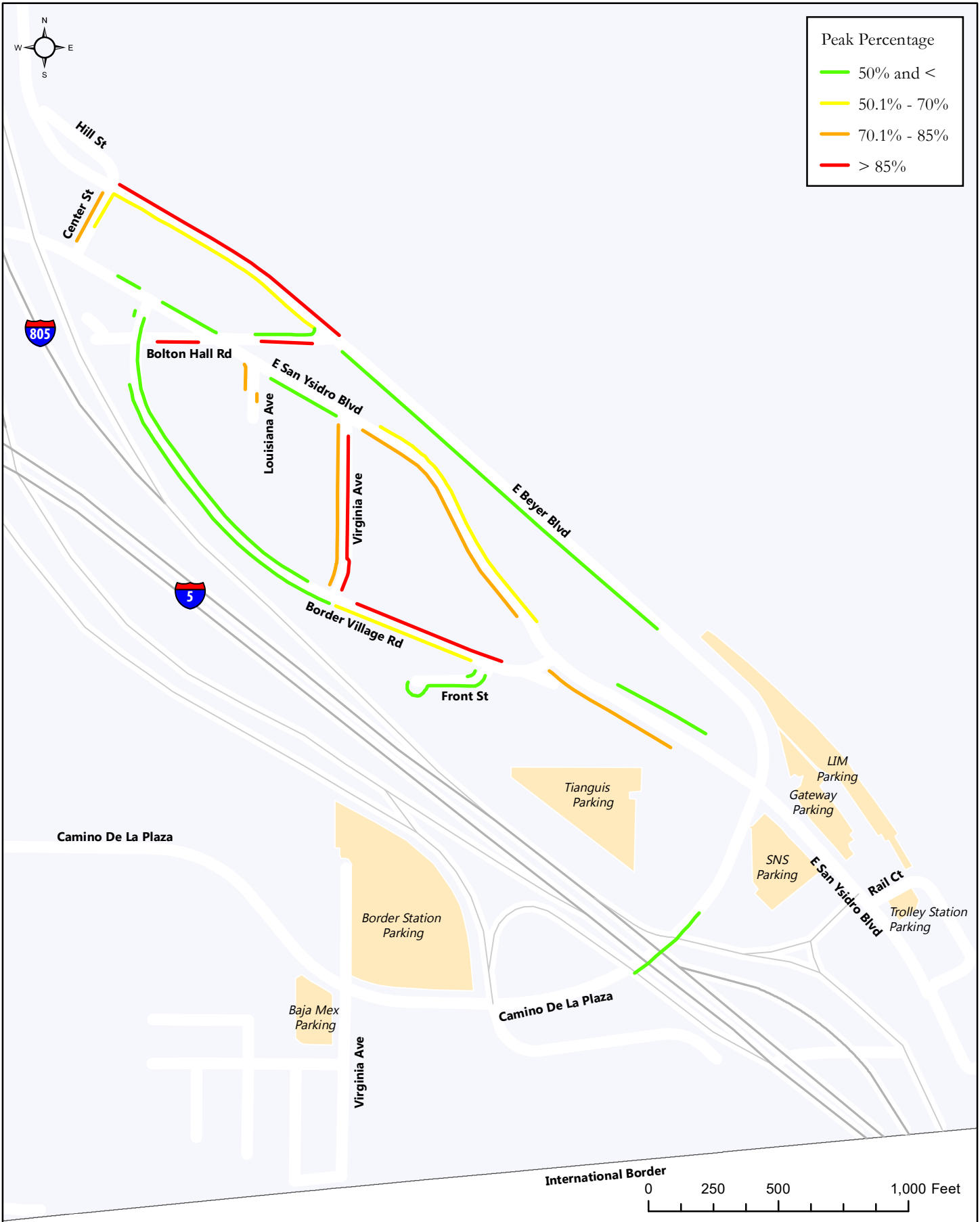


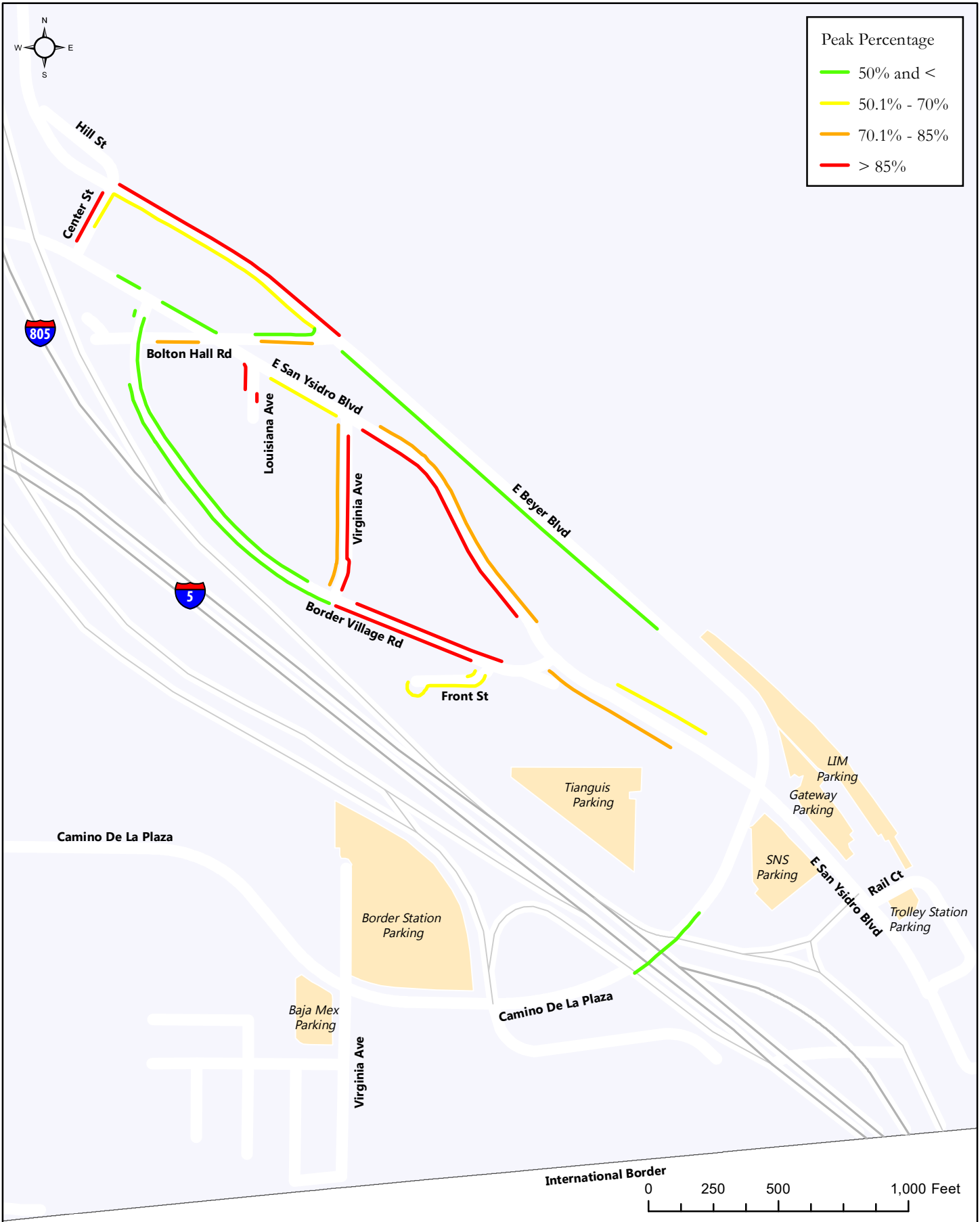


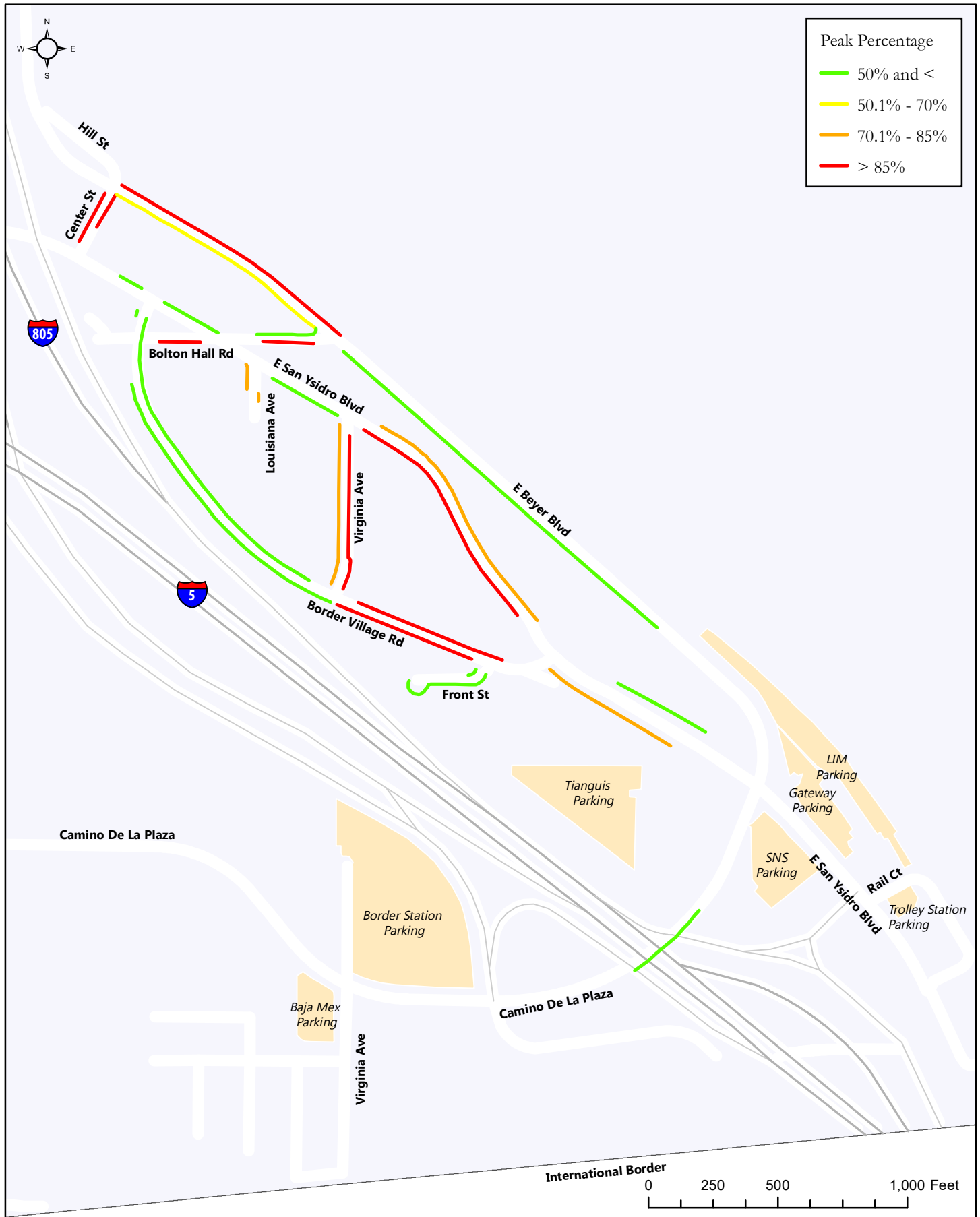












San Ysidro Parking District Project
 Parking Study

*Sunday Occupancy Percentages
 4PM - 6PM*



Appendix C - High Parking Duration



Segment	Roadway	Segment	Side	SAT	SUN	Parked All Weekend
				9-10 HRS	9-10 HRS	
S1	Camino De La Plaza	Interstate 5 southbound ramps and E. San Ysidro Boulevard	East Side	0	0	-
S3	E. Beyer Boulevard	E. San Ysidro Boulevard and Center Street	North Side	14	16	11
S4	Center Street	E. Beyer Boulevard and E. San Ysidro Boulevard	West Side	5	5	5
S5	Center Street	E. Beyer Boulevard and E. San Ysidro Boulevard	East Side	2	1	1
S6	E. Beyer Boulevard	Center Street and Bolton Hall Road	South Side	11	13	9
S7	E. Beyer Boulevard	Bolton Hall Road and E. San Ysidro Boulevard	South Side	2	8	2
S8	E. San Ysidro Boulevard	E. Beyer Boulevard and Border Village Road (south)	North Side	1	2	-
S9	E. San Ysidro Boulevard	Border Village Road (south) and Virginia Avenue	North Side	11	11	7
S10	E. San Ysidro Boulevard	Virginia Avenue and Border Village Road (north)	North Side	0	0	-
S11	E. San Ysidro Boulevard	Border Village Road (north) and Center Street	North Side	0	1	-
S14	Louisiana Avenue	Cul-de-Sac	Both Sides	0	3	-
S15	E. San Ysidro Boulevard	Louisiana Avenue and Virginia Avenue	South Side	0	0	-
S16	E. San Ysidro Boulevard	Virginia Avenue and Border Village Road (south)	South Side	3	7	2
S17	E. San Ysidro Boulevard	Border Village Road (south) and E. Beyer Boulevard	South Side	5	5	3
S18	Border Village Road	E. San Ysidro Boulevard (south) and Virginia Avenue	North Side	6	12	2
S19	Border Village Road	Virginia Avenue and E. San Ysidro Boulevard (north)	North Side	2	1	1
S20	Bolton Hall Road	Border Village Road and E. Beyer Boulevard	South Side	7	6	6
S21	Bolton Hall Road	E. Beyer Boulevard and Border Village Road	North Side	4	5	3
S22	Border Village Road	E. San Ysidro Boulevard (north) and Virginia Avenue	South Side	2	5	2
S23	Border Village Road	Virginia Avenue and E. San Ysidro Boulevard (south)	South Side	0	4	-
S24	Front Street	Cul-de-Sac	Both Sides	7	8	6
S26	Virginia Avenue	E. San Ysidro Boulevard and Border Village Road	West Side	15	14	9
S27	Virginia Avenue	Border Village Road and E. San Ysidro Boulevard	East Side	11	11	7
				108	138	76

Source: CR Associates (2022)