



CARMEL VALLEY

NEIGHBORHOOD 10 PRECISE PLAN

PARDEE CONSTRUCTION COMPANY

CARMEL VALLEY

NEIGHBORHOOD 10 PRECISE PLAN

Applicant:

PARDEE CONSTRUCTION COMPANY
12760 High Bluff Drive, Suite 110
San Diego, California 92130

Prepared by:

T&B PLANNING CONSULTANTS
897 Oberlin Drive, Suite 208
San Diego, California 92121
(619) 546-8366

In Association with:

PROJECT DESIGN CONSULTANTS, RECON, URBAN SYSTEMS & ASSOCIATES
LEIGHTON & ASSOCIATES

*Originally Adopted on September 20, 1994
by City Council Resolution R-284659*

*Amendment No. 1 to the Precise Plan Incorporates
State Coastal Commission Recommendations
Approved on May 2, 1995
by City Council Resolution R-285687*

*Amendment No. 2 to the Precise Plan Adjusted the
Southerly Boundary of Development Area 9
Approved on July 16, 1996
by City Council Resolution R-287657*

*Amendment No. 3 to the Precise Plan
Added 128 Dwelling Units
Approved on June 30, 1997
by City Council Resolution R-288906*

TABLE OF CONTENTS

<u>SECTION</u>	<u>PAGE</u>
I. INTRODUCTION	1
A. PRECISE PLAN LOCATION	1
B. COMMUNITY PLAN BACKGROUND	1
1. Planning Background	1
2. Community Plan Context	4
3. Plan Process	5
C. PRECISE PLAN SETTING	5
1. Surrounding Projects	5
2. Site Analysis	7
3. Geology	7
4. Opportunities and Constraints	8
5. Key Development Factors	13
II. LAND USE ELEMENT	15
A. LAND USE SUMMARY	15
B. RESIDENTIAL LAND USE	18
1. Residential Land Use Categories/Development Areas	18
2. Density Transfer Mechanism	18
3. Housing Type/Density	22
4. Community Plan/Precise Plan Comparison	22
5. Balanced Community	23
C. COMMERCIAL LAND USE	23
1. Neighborhood Commercial Uses	23
2. Community and Regional Commercial Uses	24
III. NEIGHBORHOOD AND COMMUNITY FACILITIES	25
A. PARK FACILITIES	25
1. Elementary School/Neighborhood Park Complex	25
2. Community Park Facilities	26
B. OPEN SPACE	26
1. Overview	26
2. Open Space	26
a. Natural Open Space	26
b. Open Space Corridor/Road Undercrossings	27
c. Revegetated Slopes	28

<u>SECTION</u>	<u>PAGE</u>
d. Drainage Basins	28
3. Trails - Equestrian and Pedestrian	28
a. Equestrian Trail	29
b. Pedestrian Trail	29
4. Golf Course	30
IV. PUBLIC SERVICES AND FACILITIES ELEMENT	31
A. SCHOOLS	31
B. POLICE	32
C. FIRE	32
D. UTILITIES	32
1. Water	32
2. Sewer	32
3. Drainage	34
4. Power	36
E. LIBRARY	36
V. CIRCULATION	37
A. REGIONAL ACCESS	37
B. NEIGHBORHOOD STREET SYSTEM	37
1. Street Classifications	37
2. Street Improvements	39
C. STREET DESIGN	39
D. ALTERNATIVE TRANSPORTATION FACILITIES	42
1. Public Transit	42
2. Bikeways	42
3. Bicycle Parking Facilities	44
4. Pedestrian Travel	44
5. Equestrian Travel	45
E. PARKING	47
1. Residential	47
2. Commercial	47
F. SOUND ATTENUATION	47
VI. DESIGN GUIDELINES	48
A. INTRODUCTION	48
B. DESIGN OBJECTIVES	48
C. GRADING AND HILLSIDE STANDARDS	49
D. INTERFACE WITH NATURAL OPEN SPACE CORRIDORS	59

<u>SECTION</u>	<u>PAGE</u>
E. LANDSCAPE DESIGN	64
1. Roadway Landscape Design	66
2. Manufactured Slopes	68
3. Brush Management Program	70
F. SPECIAL EDGE CONDITIONS	74
G. ENTRIES	77
H. FENCING	80
I. RESIDENTIAL AREA	83
J. NEIGHBORHOOD COMMERCIAL CENTER	83
K. SCHOOL/NEIGHBORHOOD PARK COMPLEX	87
L. CONSERVATION PRACTICES	89
1. Energy Conservation	89
2. Water Conservation	90
3. Natural Open Space Conservation	90
4. Fire Protection	90
VII. PLAN IMPLEMENTATION	91
A. IMPLEMENTATION PROCESS	91
B. PRECISE PLAN AMENDMENTS	91
1. Formal Amendments	93
2. Amendment Criteria	93
C. PHYSICAL DEVELOPMENT CONTROLS	93
1. Planned District Ordinance	93
2. Complementary Approvals	96
D. DEVELOPMENT PHASING	96
E. PUBLIC FACILITIES FINANCING	99
F. MITIGATION, MONITORING AND REPORTING PROGRAM	100
1. Transportation/Traffic	101
2. Air Quality	101
3. Landform Alteration/Visual Quality	102
a. Landform Alteration	102
b. Visual Quality	102
4. Hydrology/Water Quality	104
a. Water Quality	104
b. Runoff/Erosion	104
5. Geology and Soils	106
a. Geological Conditions	106
b. Erosion	107
6. Noise	108

<u>SECTION</u>	<u>PAGE</u>
7. Paleontology	108
8. Biological Resources	109
9. Brush Management	111
10. Public Services	112
 VIII. CARMEL VALLEY COMMUNITY PLAN CONFORMANCE	 113
A. CARMEL VALLEY COMMUNITY PLAN GOALS	113
B. PRECISE PLAN DEVELOPMENT CRITERIA	114
 Appendix “A” - Development Suitability Analysis	 A-2
 Appendix “B” - Coastal Zone Standards	
A. PROTECTING WATER QUALITY IN COASTAL LAGOONS	B-1
B. CITY CLERK DOCUMENT NO. 00-17068	B-1

LIST OF FIGURES

<u>FIGURE</u>		<u>PAGE</u>
1	Location Map	2
2	1975 Adopted Land Use Plan	3
3	Surrounding Land Uses	6
4	Geology	9
5	Opportunities/Constraints	10
6	Slope Analysis	11
7	Ownership Patterns	12
8	Land Use Plan	17
9	Residential Land Use Plan	21
10	Water/Sewer Plan	33
11	Drainage/Power Easement	35
12	Regional Circulation	38
13	Roadway Cross-Sections	40
14	Roadway Cross-Sections	41
15A	Alternative Transportation Facilities	43
15B	Equestrian Trail (Plan View)	46
16	Development Impacts to Slopes of 25% and Greater	50
17	Grading Plan	53
18	Slope Treatment Map	55
19	Typical Slope Treatment - Concave Condition	56
20	Typical Slope Treatment - Convex Condition	57
21	Typical Slope Treatment - Straight Condition	58
22	Shaw Valley Cross-Section Index Map	60
23A	Shaw Valley Cross-Sections 1 & 2	61
23B	Shaw Valley Cross-Sections 3 & 4	62
23C	Shaw Valley Cross-Section 5	63
24	Landscape/Brush Management Plan	65
25	Typical Landscaping on Manufactured Slopes	71
26	Special Edge Conditions	75
27	Special Edge Conditions	76
28	Major Entry (Typical)	78
29	Minor Entry (Typical)	79
30	Wall & Fencing Plan	81
31	Typical Walls & Fences	82
32	Commercial Concept	85
33	School/Park Concept	88
34	Zoning Plan	92
35	Development Phasing Scenarios	97

FIGURE

PAGE

Appendix "A" -Development Suitability Analysis

A-1	Development Suitability Analysis	A-4
-----	--	-----

Map Pocket

Land Use Plan (200 scale)

LIST OF TABLES

<u>TABLE</u>		<u>PAGE</u>
I	Proposed Land Use Acreage Allocation	4
II	Precise Plan Land Use Acreage Allocation	16
III	Precise Plan Land Use Tabulation	19
IV	Community Plan/Precise Plan Comparison of Land Use Acreage and Dwelling Unit Allocation by Ownership	20
V	Schools Serving the Project Area	31
VI	Physical Development Controls	95
VII	Development Phasing Scenarios	99

I. INTRODUCTION

A. PRECISE PLAN LOCATION

Neighborhood 10 is located in the southeastern section of the Carmel Valley Community in the City of San Diego. The property is bordered on the north by Neighborhood 8, which is adjacent to Carmel Valley Road (SR-56). Directly to the west is Neighborhood 8A. South of the property is Los Peñasquitos Canyon Preserve. The eastern portion of the property borders the "future urbanizing" area. Figure 1, *Location Map*, and Figure 2, *1975 Adopted Land Use Plan for Neighborhood 10*, depict the precise location of Neighborhood 10.

B. COMMUNITY PLAN BACKGROUND

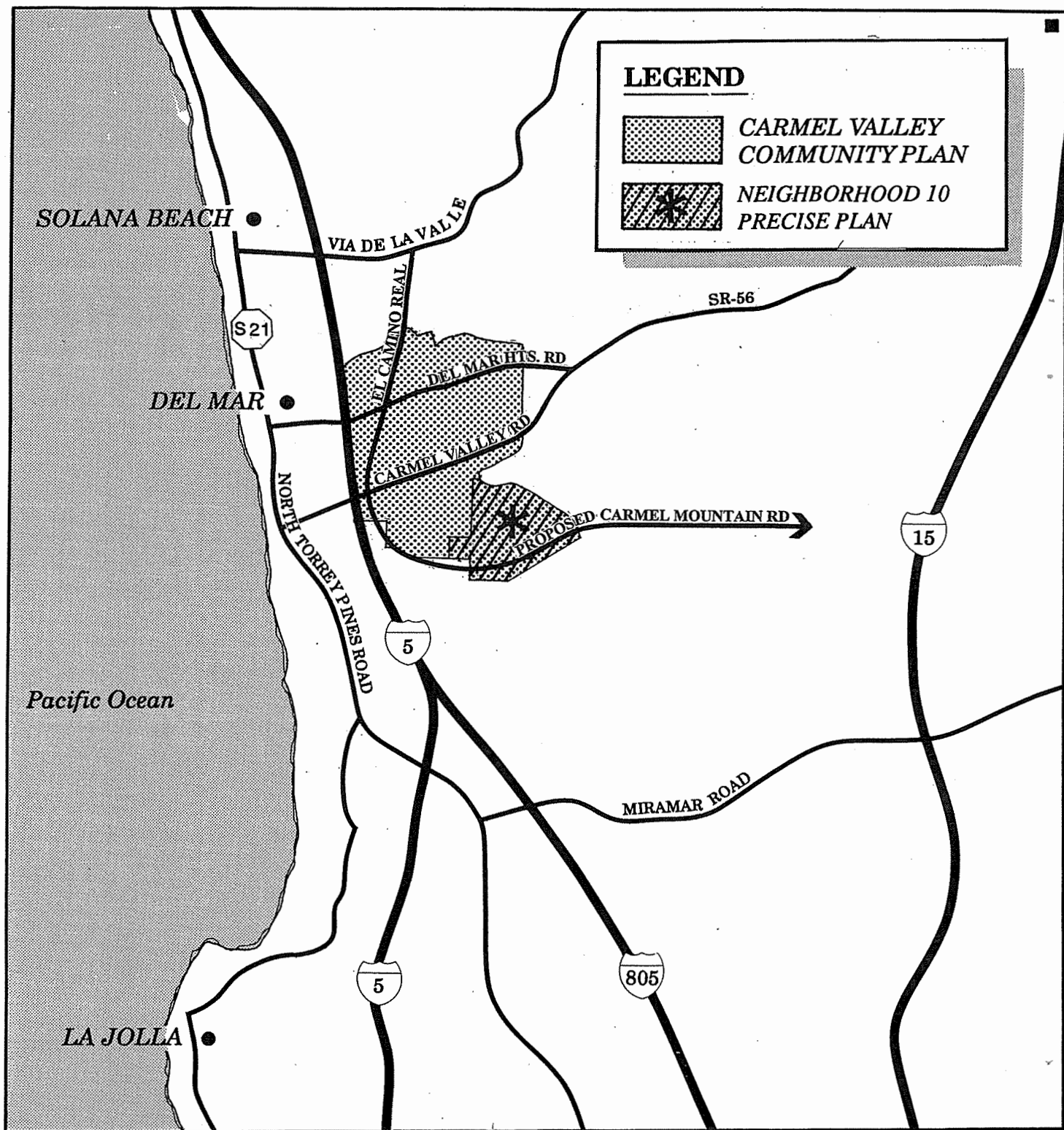
1. PLANNING BACKGROUND

On February 27, 1975, the San Diego City Council adopted the North City West Community Plan, now referred to as the *Carmel Valley Community Plan*. The plan outlines the conceptual development of Carmel Valley and calls for the orderly development of some 4,286 acres of land consisting of residential, commercial, industrial and public facility land uses. In order to facilitate this development, the plan calls for the adoption of Precise Plans for each development unit within the community. Each of these Precise Plans must specify development proposals while maintaining continuity with the Community Plan. To date, ten (10) such Precise Plans have been adopted.

On September 20, 1994, the San Diego City Council adopted this Carmel Valley Neighborhood 10 Precise Plan. The City Council specified, however, that the precise plan would not become effective until the California State Coastal Commission approved the coastal zoned portions of the plan (approximately 70 acres of the total 806.2-acre plan area are located within the coastal zone).

On February 9, 1995, the State Coastal Commission conducted a public hearing and approved the Neighborhood 10 Precise Plan, with minor changes to development criteria for the coastal zoned portions of the plan. On May 2, 1995, the City of San Diego approved Amendment No. 1 to the Precise Plan to incorporate recommendations of the Coastal Commission.

Amendment No. 2 to the Neighborhood 10 Precise Plan was approved in July 1996. This amendment adjusted the boundary between a residential development area and an abutting open space canyon to meet the objectives contained in the draft Multiple Species Conservation Program.



LOCATION MAP **PRECISE**

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

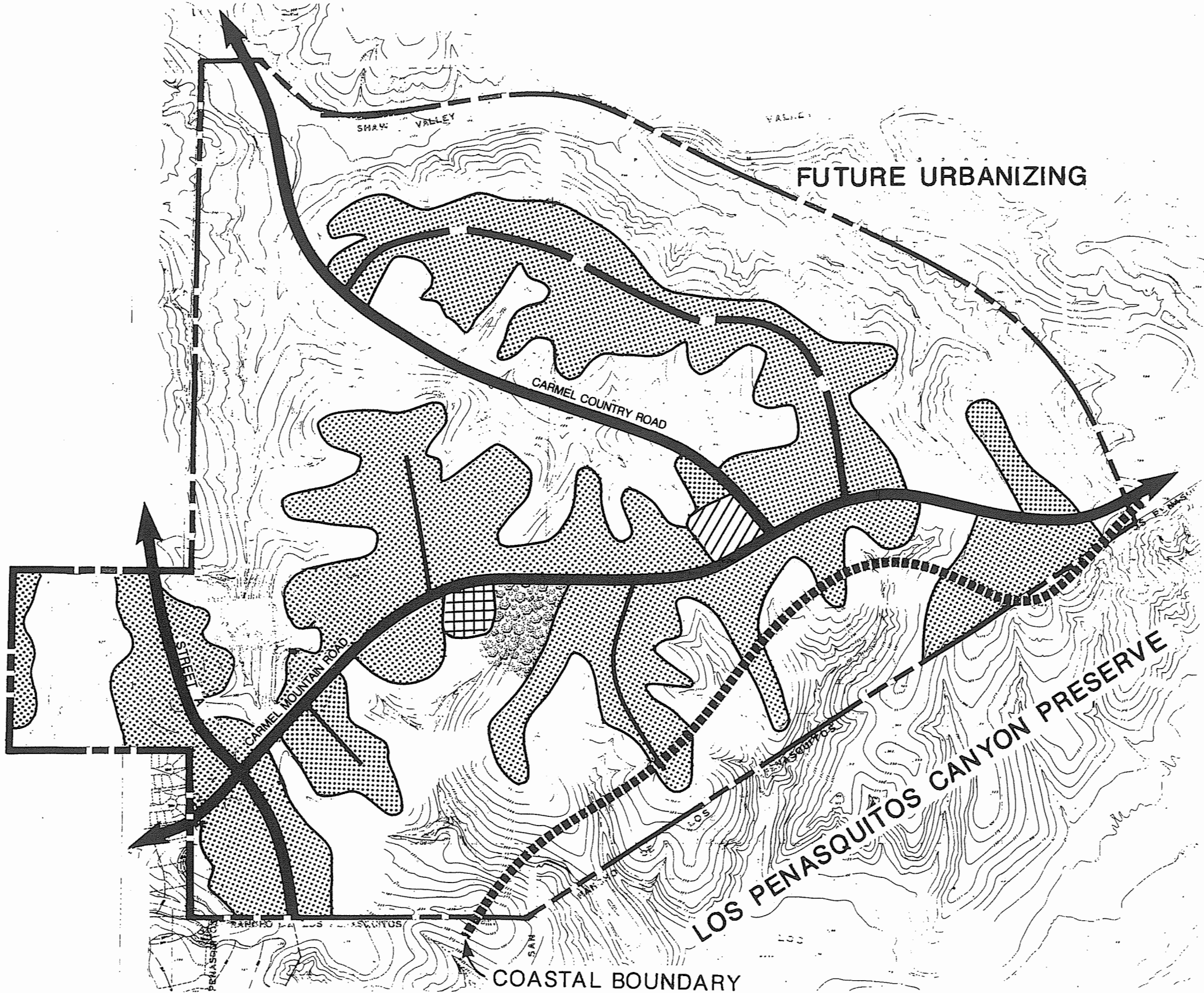
FIGURE 1



T&B Planning Consultants
 3242 HALLADAY, SUITE 100
 SANTA ANA, CALIF. 92705 (714) 662-2774
 5829 OBERLIN DRIVE, SUITE 208
 SAN DIEGO, CALIF. 92121 (619) 546-8366



1975 ADOPTED LAND USE PLAN



- VERY LOW DENSITY RESIDENTIAL
- NEIGHBORHOOD COMMERCIAL
- NEIGHBORHOOD PARK
- ELEMENTARY SCHOOL
- OPEN SPACE
- 4-LANE ARTERIAL ROAD
- 2-LANE COLLECTOR ROAD
- LOCAL STREET

PRECISE

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

FIGURE 2



T&B Planning Consultants

3242 HALLADAY, SUITE 100
SANTA ANA, CALIF. 92705 (714) 662-2774

5879 OBERLIN DRIVE, SUITE 200
SAN DIEGO, CALIF. 92121 (619) 546-8366



Amendment No. 3 transferred approximately 22.3 acres of open space within the Precise Plan to 19.5 acres of very low density residential and 2.8 acres of manufactured slopes. A total of 128 very low density residential units were added to Neighborhood 10. Additionally, Amendment No. 3 brought the Precise Plan up to date with regard to property ownership changes and tentative tract map approvals that occurred since original adoption of the Precise Plan in 1994.

2. COMMUNITY PLAN CONTEXT

Under the Carmel Valley Community Plan, Neighborhood 10 is situated within Phase 3, one of the last stages of residential development. The Carmel Valley Community Plan calls for the development in Neighborhood 10 to include very low density residential use which is characterized by detached single family residences, a neighborhood commercial center and an elementary school/neighborhood park complex. The Neighborhood 10 Precise Plan has been prepared in accordance with the Carmel Valley Community guidelines and will act as the development manual for the Precise Plan area. Section VIII of this Precise Plan discusses in further detail the Plan's conformance to the goals of the Community Plan.

Table I summarizes land use acreage allocation for Neighborhood 10 based upon the total approved dwelling units for the Carmel Valley Community. Per the land use analysis of the adopted Land Use Plan for Carmel Valley, Neighborhood 10 has been allocated a maximum total of 1,566 dwelling units. However, only 1,551 units will be constructed if a school is built within the Precise Plan area.

**TABLE I
PROPOSED LAND USE ACREAGE ALLOCATION**

PERCENT OF LAND USE	GROSS ACREAGE	TOTAL
Rural Residential (A-1-10 Zone)	15	2%
Very Low Density Residential	305	38%
Low Density Residential	14	2%
Neighborhood Commercial	4	1%
School/Park	15	2%
Major Roads	36	4%
Natural Open Space	358	44%
Manufactured Slopes	59	7%
TOTAL	806	100%

3. PLAN PROCESS

As addressed in Section VII, the Neighborhood 10 Precise Plan will function as an important component in the development implementation process and will constitute one step in a series of steps in City approval of the development. The adopted Precise Plan will serve as a basis for reviewing subsequent development plans, subdivisions and other discretionary permits.

Companion documents to the Neighborhood 10 Precise Plan, as amended in 1997, include its accompanying Supplemental Environmental Impact Report (SEIR) and the Planned District Ordinance (PDO). The SEIR cites the existing conditions in the Precise Plan area, anticipated impacts of development under the Precise Plan, and mitigation measures. The PDO establishes the procedures and standards for City review of the development plans.

C. PRECISE PLAN SETTING

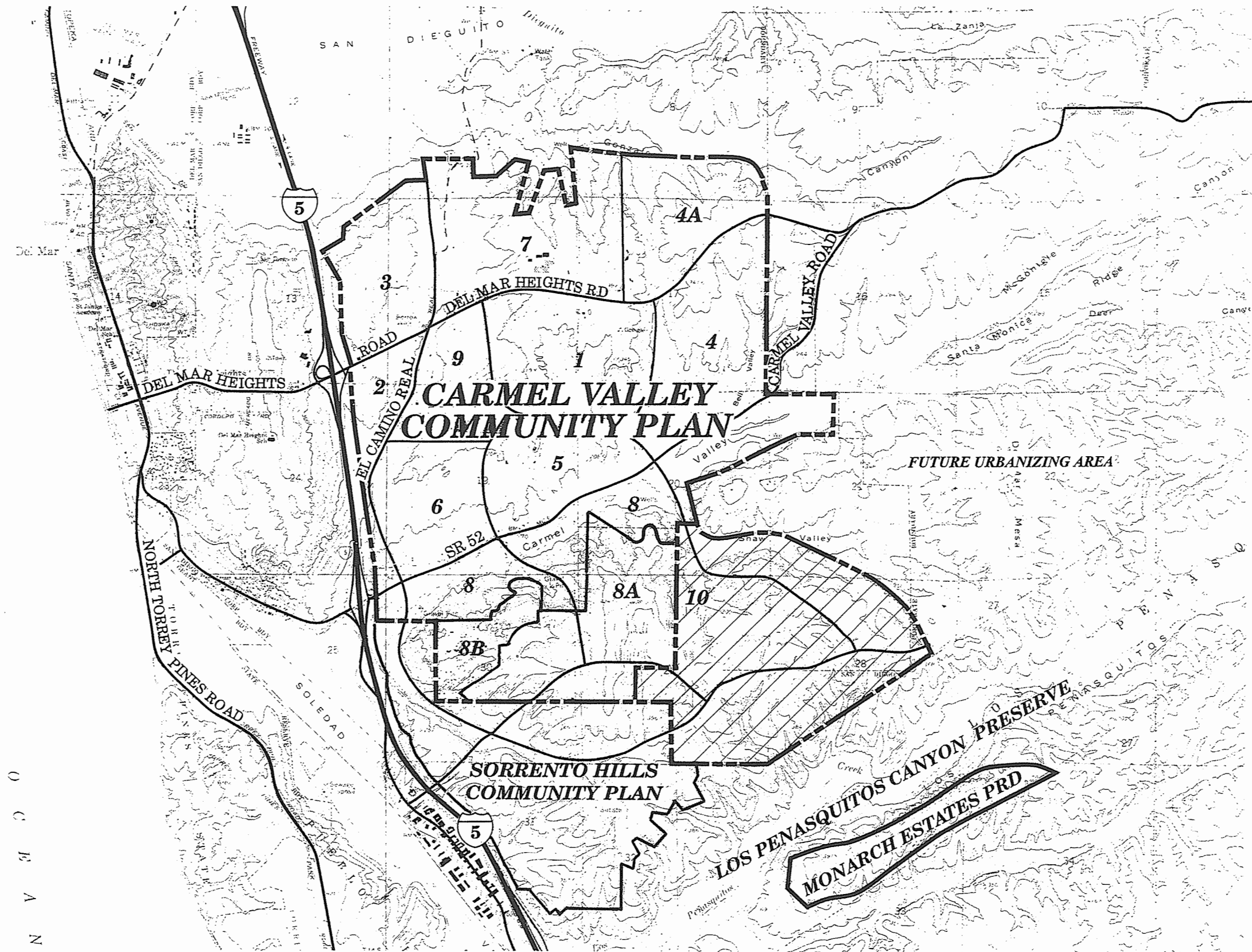
1. SURROUNDING PROJECTS

The Carmel Valley Community Planning Area is located in the City of San Diego, approximately 20 miles north of downtown San Diego. Neighborhood 10 is located in the southeastern portion of the Carmel Valley community. Neighborhood 8A borders the site on the west, with Carmel Valley Road to the northwest. A Precise Plan has not been approved for Neighborhood 8A. Neighborhood 10 forms a "peninsula" of Carmel Valley extending east from Neighborhood 8A (see Figure 3, *Surrounding Land Uses*).

The Los Peñasquitos Canyon Preserve adjoins the site along its southeastern border. To the southwest, between the southern boundary of Carmel Valley and Interstate 5, lies the Sorrento Hills Community Plan area.

The land north of the Los Peñasquitos Canyon Preserve and east of Neighborhood 10 is designated in the City's General Plan as a *Future Urbanizing Area (FUA)*. The *FUA* designation is an interim designation designed to prevent premature urbanization of areas until they are needed to accommodate the City's growth. According to the City's "Framework Plan" for the *North City FUA*, approximately half of all *FUA* land is to be retained in open space with most of the remainder in residential use with small mixed-use cores and employment centers. The portion of the *FUA* which lies adjacent to the Precise Plan area has been designated by the "Framework Plan" as part of Subarea V. It is planned for very low density development and open space because of its irregular and varied topography and high natural resource value.

An open space canyon on-site, just north of development areas 7 and 8, is planned to remain in open space as a part of the Neighborhood 10 Precise Plan Amendment Number 3. However, a golf course within the Bougainvillea Specific Plan, located directly to the north of Neighborhood 10, has been approved by the City Council. A portion of this approved golf course is actually located within the Neighborhood 10 Precise Plan boundary, in the open



SURROUNDING LAND USES

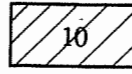

-  10 CARMEL VALLEY NEIGHBORHOOD 10
-  CARMEL VALLEY COMMUNITY PLAN BOUNDARY

FIGURE 3

space canyon just north of development areas 7 and 8. The portion located on-site is designated as open space in this Precise Plan.

2. SITE ANALYSIS

Neighborhood 10 consists of approximately 806.2 acres of land. Topographically, the area is characterized by eroded ridges and canyons ranging in elevations from 405 feet Above Mean Sea Level (AMSL) on a mesa top to 110 feet AMSL in a canyon. Distinctive land formations within the planning area include Carmel Valley and a few lateral canyons with steep canyon side slopes exceeding 25 percent gradients. Other land formations include mesas and more gently sloping land.

The project site is in terrain typical of the ridge-and-canyon area north of Los Peñasquitos Canyon. The central and eastern portion of the project area is a relatively flat mesa, whereas the western and southern portions of the site are characterized by steeply eroded gullies. Most of the more level areas on the site were recently used for agriculture with the native vegetation replaced by crops. Use of the site for cultivated crops has now ceased, and the former agriculture fields lie fallow. Grading and development is occurring in portions of Neighborhood 10 in accordance with approved tentative maps and grading permits.

The canyons and their slopes that have not been affected by past farming activities are covered with native coastal sage scrub and chaparral. Other types of vegetation that exist on-site include non-native grassland and scattered stands of eucalyptus trees. Southern willow scrub fills the channel at the bottom of the canyon which lies in the northwest corner of the site. This vegetation would qualify as a wetland under the City's Resource Protection Ordinance.

Several major drainage courses are present on the site. The majority of these drainage courses trend south-southeast, joining Los Peñasquitos Canyon off site. In the northerly portion of the site, a few major drainage courses trend north-northwest. The subject property is situated just north of Los Peñasquitos Canyon Preserve. The name Peñasquitos means "little cliffs," and little cliffs and steep slopes are prevalent in the area. The terrain is comprised of gently rolling hills, level mesa tops and steep canyons. Mesa edges are deeply eroded and interspersed with steep-side gullies and canyons.

Other existing features that affect the project include: 1) San Diego Gas & Electric electrical towers and easement located immediately off-site along the southern and southeastern property boundaries; 2) two wood-framed residences in the northwest corner of the property; and, 3) two windmills in the central portion of the site.

3. GEOLOGY

A preliminary geotechnical evaluation was performed by Leighton & Associates (May 1988) which indicates that there are no significant geologic constraints on the site that cannot be mitigated by proper planning, design and sound construction practices. Subsequent soil

studies were conducted for the western portion of the Precise Plan area by Pacific Soils in early 1993. Based on the site reconnaissances, the bedrock units underlying the site consist of the Eocene-aged Torrey Sandstone, Ardath Shale, Scripps Formation and the Pleistocene-aged Lindavista Formation. Surficial units noted mantling of these bedrock units: alluvium, colluvium and topsoil accumulations.

Several geomorphic features similar to landslide topography were noted on the site. These landforms are identified on Figure 4, *Geology*. Alternatively, these suspected features also may be attributable to the erosion characteristics of the underlying bedrock materials. Subsequent to the development of the project tentative map, an appropriate geotechnical investigation including subsurface exploration, laboratory testing and analysis should be performed to assess the deep-seated and surficial slope stability of the site.

In addition, the geotechnical report also should address the possible need for foundation setbacks from the anticipated canyon rim daylight areas.

No known active faults were found to transect the site; however, several lineaments were noted crossing the site and should be adequately investigated by a subsurface investigation. The possibility of severe ground shaking is considered high during the anticipated economic life of the proposed structures (50 to 100 years) and could originate from an earthquake emanating from the Coronado Banks Fault located offshore approximately 19 miles to the southwest of the site. Section VI discusses grading and design standards which will mitigate any geologic constraints.

4. OPPORTUNITIES AND CONSTRAINTS

Land use designations for Neighborhood 10 were based on a preliminary opportunities and constraints analysis of the property and as in Figure 5. The primary constraints of the site consist of topographical features and sensitive biological communities. The site consists of 334.43 acres of slopes with over 25% gradient (see Figure 6, *Slope Analysis*). Some of these slopes are covered with sensitive plant communities which include coastal sage scrub. These slopes are also associated with major drainage courses that trend toward Los Peñasquitos Canyon Preserve and Shaw Valley.

Another important site development constraint associated with Neighborhood 10 is the existing multiple ownership pattern. (See Figure 7, *Ownership Patterns*.) Twenty-five property owners with parcels ranging from one acre to over 400 acres comprise the Planning Area. As a result, issues such as the alignment of major roads along property boundaries, equitable distribution of development rights and the need to be consistent with the adopted Land Use Plan for Neighborhood 10 were considered.

Other constraints that occur on-site include the Coastal Zone boundary which extends into the property along the southeastern portion of the site. (See Figure 2.)

GEOLOGY

BEDROCK UNITS

- Tt TORREY SANDSTONE
- Ta ARDATH SHALE
- Tsc SCRIPPS FORMATION
- Qln LINDVISTA FORMATION

SURFICIAL UNITS

- Qal ALLUVIUM
- Qcol/sw COLLUVIUM / SLOPEWASH

MAP SYMBOLS

- APPROXIMATE LOCATION OF GEOLOGIC CONTACT, DOTTED WHERE BURLED
- UNNAMED FAULT MAPPED BY KENNEDY, 1975 (APPENDIX A, REFERENCE 12), DASHED WHERE APPROXIMATE, DOTTED WHERE BURLED, U INDICATES UPTHROWN, D INDICATES DOWNDROPPED
- APPROXIMATE LOCATION OF FAULT MAPPED DURING GEOLOGIC RECONNAISSANCE, QUERLED WHERE UNCERTAIN
- APPROXIMATE LOCATION OF LINEAMENT OBSERVED ON AERIAL PHOTOGRAPHS
- APPROXIMATE LOCATION OF POSSIBLE LANDSLIDE ARROWS INDICATE POSSIBLE DIRECTION OF MOVEMENT
- BEDDING ATTITUDE
- FAULT ATTITUDE
- JOINT ATTITUDE

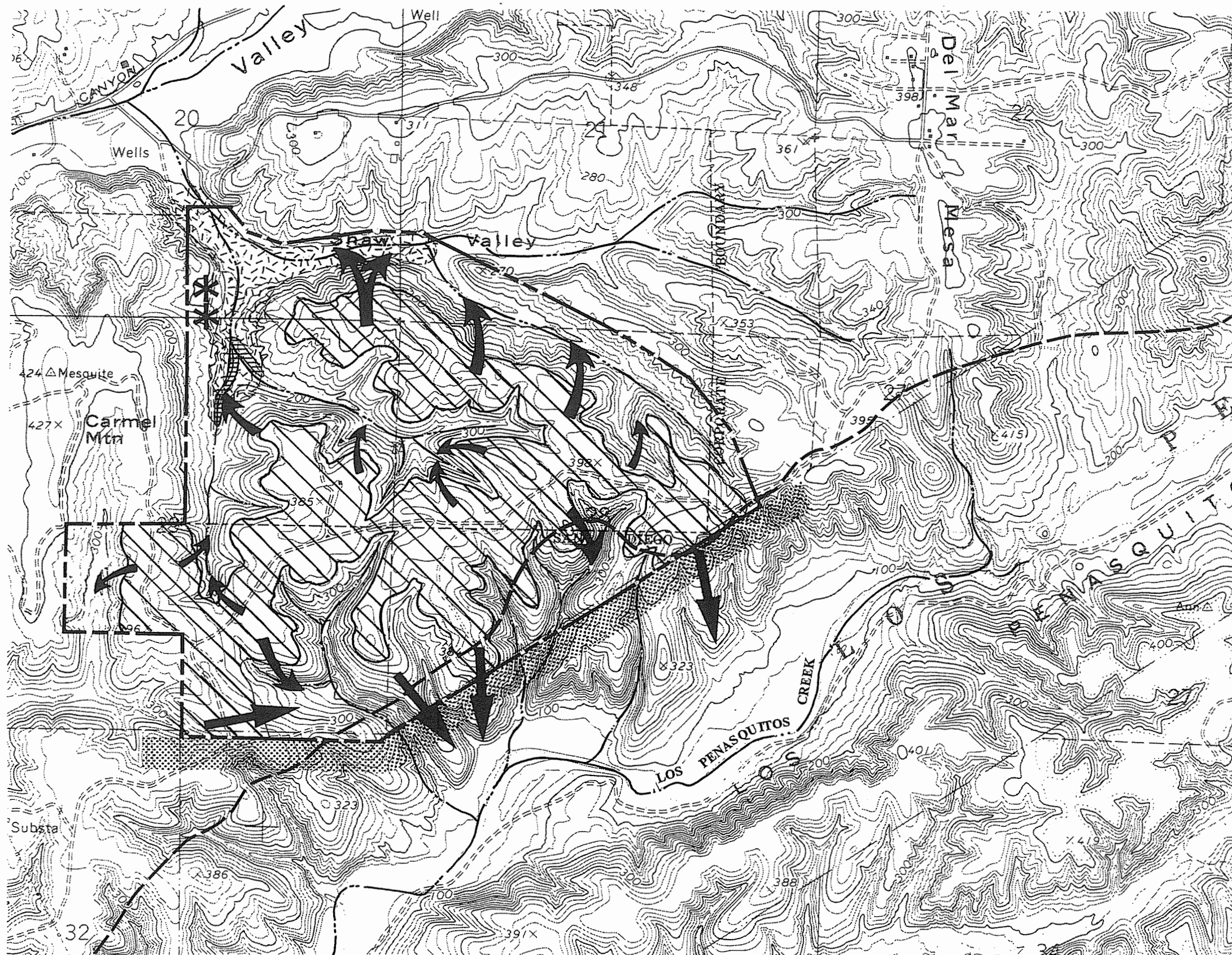


Source : Leighton & Associates


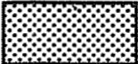


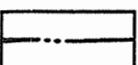
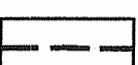
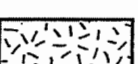
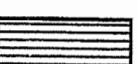
FIGURE 4

PRECISE

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10



OPPORTUNITIES / CONSTRAINTS

-  **EXISTING HOMES**
-  **SDG & E EASEMENT**
-  **MESA TOPS**
-  **CANYON VIEW**
-  **MAJOR DRAINAGE COURSE**
-  **COASTAL BOUNDARY**
-  **FLOOD PLAIN**
-  **WETLAND¹**

¹ As defined by the City's R.P.O.

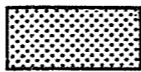
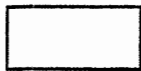
PRECISE

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

FIGURE 5



SLOPE ANALYSIS

	<i>SLOPES > 25%</i>	334.43 AC
	<i>SLOPES < 25%</i>	471.74 AC
TOTAL		806.17 AC

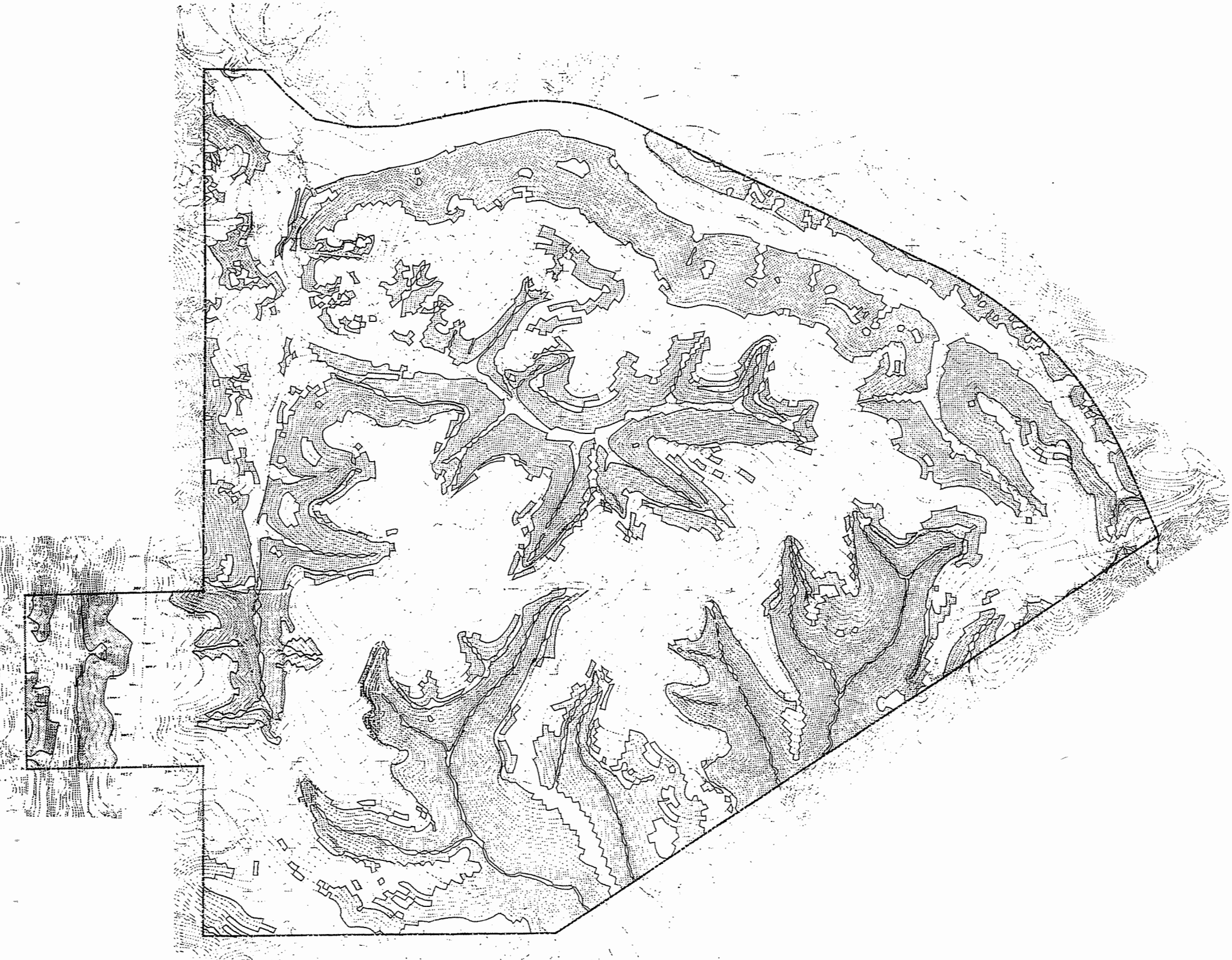


FIGURE 6

PRECISE

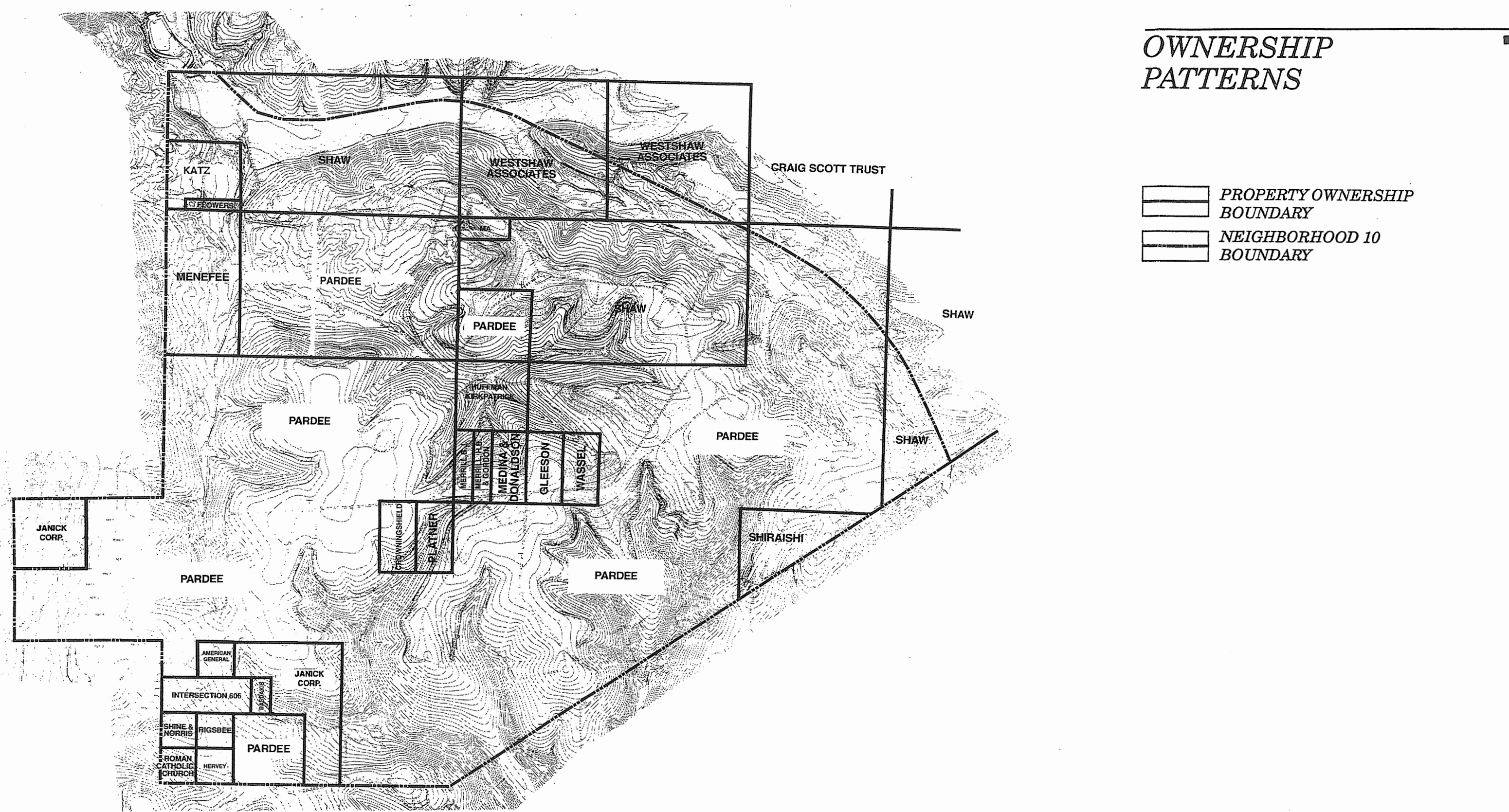
P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10



T&B Planning Consultants
3242 HALLADAY, SUITE 100
SANTA ANA, CALIF. 92705 (714) 662-2774
5879 OBERLIN DRIVE, SUITE 208
SAN DIEGO, CALIF. 92121 (619) 546-8366



OWNERSHIP PATTERNS



Opportunities exist to limit development to the mesa areas which will preserve most of the sensitive areas located within the Precise Plan area, while allowing for panoramic views. Since most of the mesa areas have been previously disturbed by agricultural activities, the proposed development in these areas would provide minimum disturbance to the existing slope and canyon areas. The amount of site grading would also be minimized by developing along the mesa.

As a result of development along the mesa, considerable view opportunities of canyons and natural open spaces will be available through the development of Neighborhood 10. Development south of Carmel Mountain Road will offer views of Los Peñasquitos Canyon Preserve and natural open spaces within lateral canyons. Development north of Carmel Mountain Road, but south of Carmel Country Road, will offer views into a major canyon which runs northwest to southeast through the center of the Precise Plan area. Development located north of Carmel Country Road will offer panoramic views into Shaw Valley and natural open spaces on-site and in the vicinity of the Precise Plan area and to the mountain ridges beyond.

5. KEY DEVELOPMENT FACTORS

There are several key development factors that have guided the preparation of the Precise Plan for Neighborhood 10. These factors are all integral to the Precise Plan and are uniquely interrelated.

- *The realignment of Carmel Country Road out of the canyon bottom and onto the mesa top.* This has effectively eliminated a major portion of Carmel Country Road that is "unloaded," that is, without adjoining development as well as preserving the significant drainage courses and sensitive open space and slope areas.
- *A balanced grading of cut and fill to minimize the need to import or export soil to or from Neighborhood 10.* This is essential for a viable development plan. The fill areas have been carefully selected to minimize their visual impacts, as well as impacts to sensitive biological resources on-site.
- *Recognition of natural steep slopes and biologically sensitive areas as community resources.* The development areas within Neighborhood 10 are designed to be consistent with the City of San Diego's Resource Protection Ordinance and City Council Policy 600-40, which are intended to protect and preserve environmentally sensitive lands.
- *Preservation of natural drainage courses.* All of the major natural drainage courses in Neighborhood 10 have been preserved. These include all of the major canyons as well as the lateral canyons of Los Peñasquitos Canyon Preserve. A contributing factor toward this end was the realignment of Carmel Country Road out of the canyon bottom.

- *Circulation linkages inside and out of Neighborhood 10.* Circulation linkages to areas outside of Neighborhood 10 (as previously defined in the Community Plan) have been provided. More importantly, no development within Neighborhood 10 has direct frontage on either of the neighborhood's major roads: Carmel Mountain Road or Carmel Country Road. All access to development is provided by local residential streets.

- *A Grading Plan which addresses the character of the site and the adopted Community Plan.* Topographically, Neighborhood 10 is characterized by many steep ridges and canyons supporting more gently sloping mesa tops. In attempting to achieve consistency with the adopted Community Plan, to address the steepness of the existing topography, and preserve the canyon bottoms, the Grading Plan proposes the placement of fill areas in steep canyon heads, thus requiring manufactured slopes in those canyon heads. However, the manufactured slopes are placed in areas minimized from view from Los Peñasquitos Canyon and contour grading techniques are used on all readily visible slopes in excess of ten (10) feet in height to blend in with the natural topography. Furthermore, filling the canyon heads enables the Precise Plan to maximize preservation of the sensitive biotic communities in the canyon bottoms, while accommodating the development permitted by the Community Plan.

- *Drainage/sewer laterals and detention basins into Los Peñasquitos Canyon.* Neighborhood 10 minimizes encroachment and grading within Los Peñasquitos Canyon by limiting the encroachment into the natural open space to detention basins and sewer laterals which connect with the main trunk line within Los Peñasquitos Canyon. These basins are required for flood control and sedimentation purposes. This Precise Plan allows for the provision of sewage pump stations as an alternative to the sewer laterals in Peñasquitos Canyon. The pump stations shall not be permitted in natural open space areas.

- *Achieving the total dwelling units allowed by the Community Plan and fair distribution within Neighborhood 10.* The Neighborhood 10 Precise Plan proposes a maximum of 1,566 residential dwelling units as addressed in the existing facilities benefit assessment for Carmel Valley although only 1,551 units will be built if a school site is located within Neighborhood 10. It is necessary to achieve the 1,551 dwelling units in order to fulfill the obligations in the existing Facilities Benefit Assessment (FBA) that have already been placed upon Carmel Valley South. In addition, the Neighborhood 10 Precise Plan attempts to achieve an equitable distribution of the 1,551 dwelling units, based on a pro-rata share, among the many property owners within Neighborhood 10.

II. LAND USE ELEMENT

This section outlines the type, location and acreage of various land uses within the Neighborhood 10 Precise Plan area. It should be emphasized that the land use element provides a quantitative or "structural" description of the Precise Plan. The design guidelines within this document provide a more qualitative aspect of the land use proposed for Neighborhood 10.

While the Precise Plan indicates specific site acreage and dwelling unit counts for each residential site, the site sizes and yields may be subject to modifications during precise engineering and design. Such modifications may be necessary because of adjustments in street alignments, grading and utility design, and/or lot configuration during engineering of development plans and subdivision maps.

A. LAND USE SUMMARY

Table II, *Precise Plan Land Use Acreage Allocation*, summarizes the Precise Plan area, while Figure 8 illustrates the Land Use Plan for Neighborhood 10. As depicted, the Precise Plan area will consist of rural residential, very low and low density residential development, a neighborhood commercial center, a school/park complex, streets and natural open space.

A maximum of 1,566 dwelling units are proposed in Neighborhood 10 which will support an estimated peak population of 4,119 based upon 1995 average household size of 2.53 persons per unit for Carmel Valley (Source: SANDAG Series 8 Population Forecast). However, the Precise Plan assumes that a school site will be provided within the Precise Plan area, thereby reducing the total residential units to 1,551 dwellings. If the school district elects not to construct school facilities in Neighborhood 10, then 15 dwelling units shall be permitted to develop on 5.0 acres in development area 3 for a maximum total of 1,566 units within the Precise Plan area. (See Figure 8, *Land Use Plan*.)

A natural trail will traverse from north to south in the southwestern portion of the Precise Plan area. As proposed by the Carmel Valley Community Plan, this trail will provide equestrian access. The trail will link to the proposed trail within the State Route 56 West, Carmel Valley Restoration and Enhancement Program (CVREP). A roadway undercrossing will be provided at Carmel Mountain Road, as shown in Figure 8, and will provide an open space linkage to Los Peñasquitos Canyon Preserve.

TABLE II
PRECISE PLAN LAND USE ACREAGE ALLOCATION

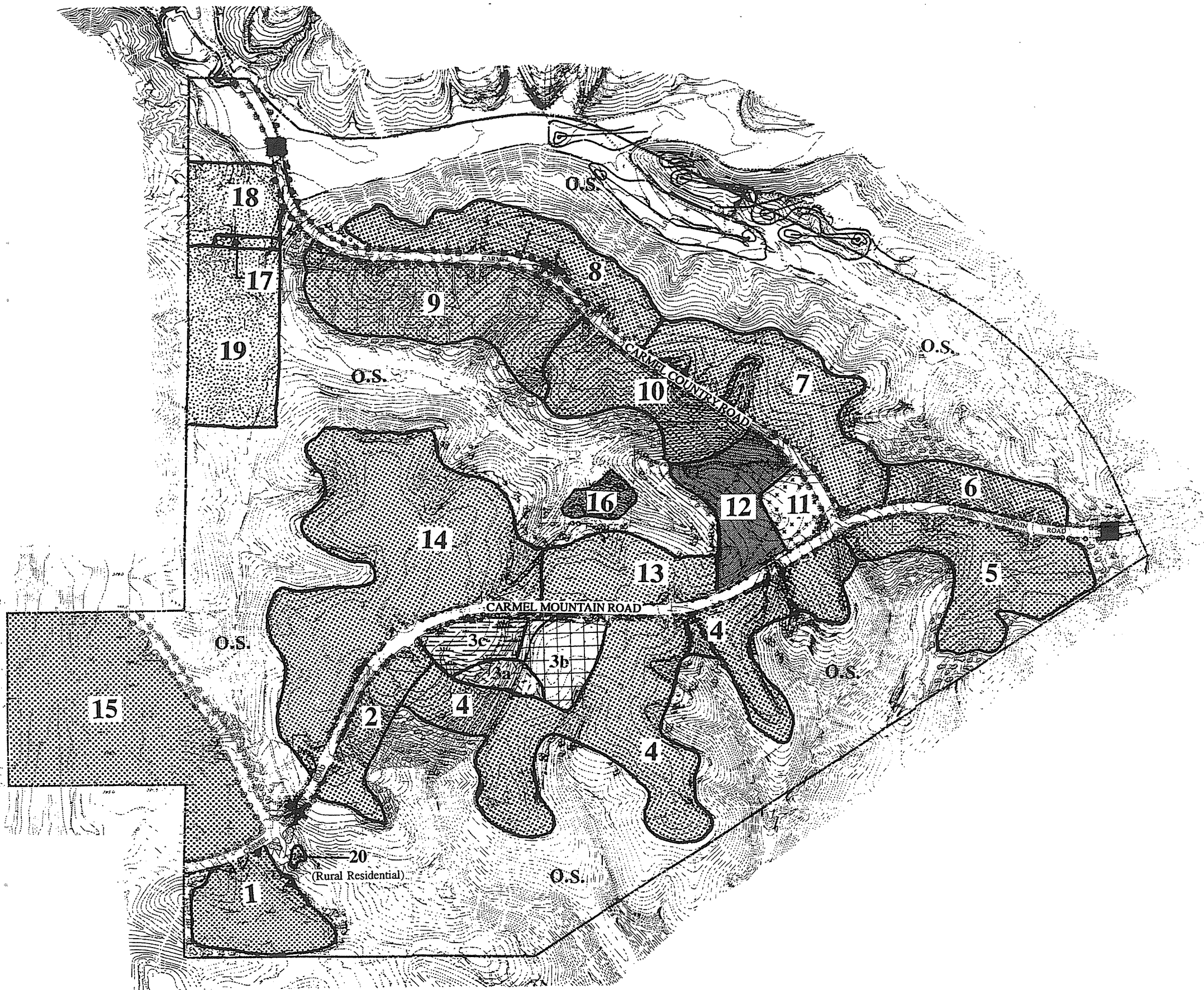
LAND USE	ACRES	Percent of Total Acreage
RURAL RESIDENTIAL (A-1-10)	15.3 ¹	1.9%
VERY LOW DENSITY RESIDENTIAL	304.2 ¹	37.7%
LOW DENSITY RESIDENTIAL	14.3 ¹	1.8%
TOTAL RESIDENTIAL	333.8²	41.4%
ELEMENTARY SCHOOL	3.0	.4%
ACTIVE PLAYFIELDS/JOINT USE	7.0	.9%
NEIGHBORHOOD PARK	5.0	.6%
NEIGHBORHOOD COMMERCIAL	4.0	.5%
TOTAL NEIGHBORHOOD/COMMUNITY FACILITIES	19.0	2.4%
NATURAL OPEN SPACE	358.1	44.4%
MANUFACTURED SLOPES	59.2 ³	7.3%
MAJOR ROADS	36.1 ³	4.5%
TOTAL	806.2	100.0%

¹ For residential uses, acreages include internal local and access roads, and internal slopes.

² Includes 11.0 acres of manufactured slopes within residential development areas.

³ Excludes 11.0 acres of manufactured slopes within residential development areas.

LAND USE PLAN



- VERY LOW DENSITY RESIDENTIAL (0-5 DU/AC)**
- LOW DENSITY (5-15 DU/AC)**
- RURAL RESIDENTIAL (UNDERLYING ZONING A-1-10)**
- NEIGHBORHOOD COMMERCIAL**
- ELEMENTARY SCHOOL**
- ACTIVE PLAYFIELD/ JOINT USE**
- NEIGHBORHOOD PARK**
- O.S. OPEN SPACE**
- ROAD UNDERCROSSING**
- BRIDGE**

PRECISE

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

FIGURE 8



T&B Planning Consultants
3242 HALLADAY, SUITE 100
SANTA ANA, CALIF. 92705 (714) 662-2774
5879 OBERLIN DRIVE, SUITE 208
SAN DIEGO, CALIF. 92121 (619) 546-8366



B. RESIDENTIAL LAND USE

1. RESIDENTIAL LAND USE CATEGORIES/DEVELOPMENT AREAS

The residential uses proposed in Neighborhood 10 correspond to the "very low density" (0-5 dwelling units per net residential acre [du/nr]) and "low" density (5-14 du/nra) categories in the *Carmel Valley Community Plan*. For certain properties which were designated solely as open space within the adopted Community Plan, the Neighborhood 10 Precise Plan proposes a designation of rural residential with permitted uses consistent with the underlying zone of A-1-10.

Neighborhood 10 has been divided into 20 development areas as depicted in Table III and on Figure 9, *Residential Land Use Plan*. Table III summarizes each development area according to gross acreage and dwelling unit allocation. (*Please note that development area numbers are not intended to correspond to the phasing of development.*) A total of 1,551 dwelling units will be provided with the buildout of Neighborhood 10, resulting in an overall density of 1.9 units per acre in the Precise Plan area, assuming that there will be a school site in development area 3. If a school is not constructed within the Neighborhood 10 Precise Plan area, then an additional 15 dwelling units may be constructed in development area 3, thereby raising the total permitted dwelling unit count to a maximum of 1,566 homes.

2. DENSITY TRANSFER MECHANISM

Density transfer(s) shall be allowed within Neighborhood 10; provided, however, that the following conditions are met: If the total number of allowed dwellings from one residential development area is not achieved due to topography constraints, Resource Protection Ordinance, or other development factors, the unused dwelling units may be transferred to another residential development area within Neighborhood 10 so long as the total number of units (1,551 units with a school site; 1,566 units without a school site) is not exceeded and the resultant development is consistent with the development standards of the underlying zone as set forth in the Planned District Ordinance (PDO). Table IV, *Community Plan / Precise Plan Comparison of Land Acreage and Dwelling Unit Allocation by Ownership*, indicates the maximum number of total dwelling units permitted per property owner (see the "Precise Plan" column under the "Dwelling Units" designation in Table IV).

TABLE III
PRECISE PLAN LAND USE TABULATION

DEVELOPMENT AREA	LAND USE	DEVELOPABLE ACRES ¹	DWELLING UNITS ²
1	Very Low Density Residential	13.6	73
2		9.9	59
4		53.0	220
5		18.7	130
6		8.6	55
7		25.6	122
8		19.0	65
9		25.6	136
10		15.7	62
13		12.4	61
14		52.6	235
15		49.5	217
12	Low Density Residential	12.3	98 ³
16		2.0	13
17	Rural Residential (Permitted to develop pursuant to underlying zone A-1-10)	0.5	1
18		6.1	1
19		8.5	2
20		0.2	1
TOTAL RESIDENTIAL		337.7	1,551
3a	Elementary School ⁴	3.0	--
3b	Active Playfields/Joint Use	7.0	--
3c	Neighborhood Park	5.0	--
11	Neighborhood Commercial	4.0	--
TOTAL NEIGHBORHOOD/COMMUNITY FACILITIES		19.0	--
--	Natural Open Space	358.1	--
--	Manufactured Slopes	59.2	--
--	Major Roads (Carmel Country Road, Carmel Mountain Road and Street A)	36.1	--
TOTAL		806.2	1,551

¹ For residential uses, acreage includes internal local and access roads, and internal slopes.

² The actual yield per development area by the Tentative Map will be permitted to differ from these numbers, provided the conditions of the underlying zone and the density transfer mechanism are met.

³ Although only 98 dwellings are allocated to development area 12, a maximum of 189 dwelling units may be constructed within development area 12 via the use of the density transfer mechanism permitted by this Precise Plan.

⁴ If elementary school is not constructed, 15 dwelling units will be constructed on 5.0 acres in development area 3 for a maximum Precise Plan total of 1,566 dwelling units.

TABLE IV
COMMUNITY PLAN/PRECISE PLAN COMPARISON OF LAND USE ACREAGE
AND DWELLING UNIT ALLOCATION BY OWNERSHIP

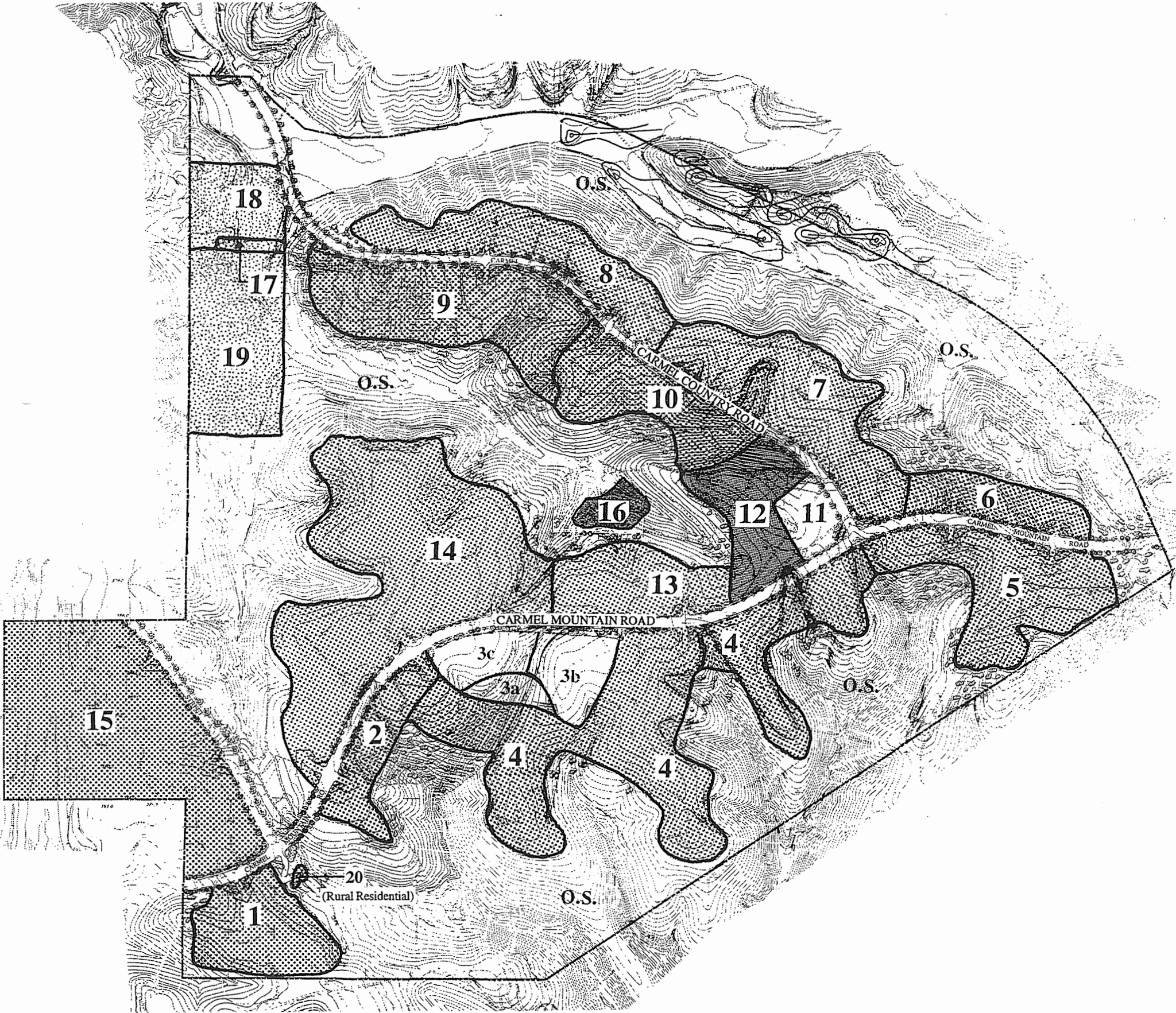
OWNERSHIP	TOTAL GROSS ACRES	RESIDENTIAL		COMMERCIAL		SCHOOL		PARK		ROADS**		DWELLING UNITS	
		COMMUNITY PLAN	PRECISE PLAN	COMMUNITY PLAN	PRECISE PLAN	COMMUNITY PLAN	PRECISE PLAN	COMMUNITY PLAN	PRECISE PLAN	COMMUNITY PLAN	PRECISE PLAN	COMMUNITY PLAN	PRECISE PLAN
KATZ	9.00	--	--	--	--	--	--	--	--	1.0	.12	--	1(A-1-10)
FLOWERS	1.00	--	--	--	--	--	--	--	--	.1	--	--	1(A-1-10)
MENEFEE	20.21	--	--	--	--	--	--	--	--	.1	--	--	2(A-1-10)
PARDEE CONSTRUCTION COMPANY	508.26	209.43	226.69	2.1	4.0	3.0	.18	6.8	6.78	21.66	18.97	975	1,148
WESTSHAW ASSOCIATES	33.56	1.4	2.18	--	--	--	--	--	--	--	--	6	7
MA	2.00	2.0	1.59	--	--	--	--	--	--	--	.41	9	8
SHAW	133.90	42.3	46.61	--	--	--	--	--	--	2.2	10.16	185	180
B.J. MERRILL	2.35	1.4	1.41	--	--	--	--	--	--	.1	.19	6	7
H.B. MERRILL/GORDON	2.35	2.0	1.65	--	--	--	--	--	--	.3	.18	9	8
MEDINA/DONALDSON	4.80	3.4	3.91	--	--	--	--	--	--	.5	.39	15	19
GLEESON	4.70	3.9	3.26	--	--	--	--	--	--	.6	.35	17	16
WASELL	4.70	1.7	2.22	1.9	--	--	--	--	--	.8	.65	7	11
CROWNINGSHIELD	5.00	3.3	--	--	--	--	1.06	.9	2.68	.8	.53	14	14
PLATNER	5.00	.1	--	--	--	--	1.76	4.3	2.54	.6	.37	1	1
SHIRAISHI	12.98	7.1	3.63	--	--	--	--	--	--	--	--	31	18
JANICK CORPORATION	27.50	9.74	12.24	--	--	--	--	--	--	.01	.97	41	51
BASDAKIS	1.25	.2	.2	--	--	--	--	--	--	--	--	1	1
INTERSECTION 606	5.88	4.74	3.40	--	--	--	--	--	--	1.5	1.78	28	17
RIGSBEE	2.28	2.1	2.28	--	--	--	--	--	--	.1	--	9	11
SHINE/NORRIS	2.34	2.1	1.90	--	--	--	--	--	--	.2	.11	9	9
HERVEY	2.13	2.1	2.05	--	--	--	--	--	--	--	--	9	10
ROMAN CATHOLIC CHURCH	2.50	2.5	1.76	--	--	--	--	--	--	--	--	11	9
HUFFMAN/KIRKPATRICK	10.00	.7	2.02	--	--	--	--	--	--	--	--	3	13
AMERICAN GENERAL	2.48	1.3	0.86	--	--	--	--	--	--	1.18	.85	7	4
GRAND TOTAL	806.17	303.51	319.85	4.00	4.00	3.00	3.00	12.00	12.00	31.75	36.10	1,393	1,566

* THE ACREAGE IN THIS TABLE, FOR BOTH THE COMMUNITY PLAN AND PRECISE PLAN, IS USABLE ACREAGE AND DOES NOT INCLUDE MANUFACTURED SLOPES.

** ROAD ACREAGE CALCULATION INCLUDES ONLY CARMEL COUNTRY ROAD, CARMEL MOUNTAIN ROAD & STREET A.

NOTE: DUE TO VARIANCES WITH THE ENLARGEMENT PROCESS, PLANIMETER ERROR, ASSESSOR'S PARCEL MAPS AND BOUNDARY SHIFTS, INACCURACIES OF ACREAGES FOR INDIVIDUAL PROPERTY OWNERS MAY BE REVEALED DURING FINAL ENGINEERING AND METES & BOUNDS SURVEYS.

RESIDENTIAL LAND USE PLAN



- VERY LOW DENSITY
RESIDENTIAL (0-5 DU/AC)**
- LOW DENSITY
(5-15 DU/AC)**
- RURAL RESIDENTIAL
(UNDERLYING ZONING A-1-10)**

PRECISE

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

3. HOUSING TYPE/DENSITY

The "rural residential" and "very low" density (0-5 dwelling units per acre) categories are proposed for residential development areas 1, 2, 4, 5, 6, 7, 8, 9, 10, 13, 14, 15, 17, 18, 19, and 20 (see Table III). These areas shall develop with single-family detached residential units. The "low density" category (5-15 dwelling units per acre) is proposed for residential development areas 12 and 16. The "low" density category in the Carmel Valley Community Plan permits densities of 5 to 14 du/ac and is characterized by an average density of 10 du/ac. However, the Planned District Ordinance (PDO) for Carmel Valley permits a density up to 15 du/ac in the MF-1 Zone. Development areas 12 and 16 shall be permitted to develop single-family detached residential on small lots, as well as townhomes and other low density attached units. Development areas 17-19 are considered important in the health and movement of native wildlife through the area. Consideration should be given to acquisition of these properties for that purpose.

Those areas designated as "rural residential" shall be permitted to develop according to the underlying zone of A-1-10. Any future development proposals within these areas will require the processing of a development permit in accordance with the Carmel Valley PDO and may also require additional environmental review. In addition, any future development proposals are subject to the development and design guidelines as outlined within the Neighborhood 10 Precise Plan.

4. COMMUNITY PLAN/PRECISE PLAN COMPARISON

The Neighborhood 10 Precise Plan attempts to achieve an equitable distribution of development rights for the many property owners. A 200-foot scale enlargement of the adopted Community Plan for Neighborhood 10 was used along with current ownership boundaries based upon Assessor's Parcel Maps to estimate individual property owner development opportunities. However, the accuracy of these estimates are subject to the variances of the enlargement process, Neighborhood 10 boundary adjustments and Assessor's Parcel Map boundaries. Future evaluations using precise engineering techniques with Metes & Bounds boundary surveys may reveal different conclusions.

Table IV summarizes a comparison of the adopted Community Plan and proposed Precise Plan in relation to development acreage and dwelling unit allocation by ownership (as of February 1997). In reviewing Table IV, it should be noted that only one dwelling unit is permitted for every ten (10) acres or portion thereof on Rural Residential parcels. In addition, it is anticipated that some dwelling units may be transferred into development area 12 from other development areas within Neighborhood 10 to allow for the construction of multi-family housing. This is permitted provided the transfer of units is accomplished per the density transfer mechanism contained in Section II.B.2. in this Precise Plan.

5. BALANCED COMMUNITY

The Carmel Valley Community Plan calls for the establishment of a balanced community housing program consistent with Council Policy 600-19. This means that a range of housing unit types and prices should be available in the community, suitable to households at a variety of income levels.

The Community Plan provides for this balance by correlating income levels to proposed housing categories and locations. The very low density single family category in the Community Plan allows 0-5 dwelling units per net residential acre (du/nra) and is designated for middle income families and up; this is the housing category included in the Neighborhood 10 Precise Plan area. Therefore, only market rate units will be required to be provided in Neighborhood 10. On the other hand, the Community Plan designates portions of the proposed low-medium density (15-29 du/nra) and medium density (30-44 du/nra) units for low and moderate income households; these units are planned in Neighborhood 9 near the Town Center, outside of the Neighborhood 10 Precise Plan area.

The City of San Diego is presently considering adoption of an Inclusionary Housing Program. The intent of this program is to ensure that a variety of housing types are made available within the City, with special emphasis given to providing housing for minorities and housing that is affordable to low and moderate income households. Neighborhood 10 will comply with the requirements of the City of San Diego Inclusionary Zoning Ordinance when the ordinance becomes effective, but only to the extent the ordinance is applicable by law to any phase, element, or pending application in effect at the time the tentative map is approved.

An effective affirmative marketing plan will be utilized in conjunction with all residential projects. The affirmative action program of the San Diego Building Industry Association (BIA) or equivalent should be employed in order to ensure affirmative marketing of sale and rental units. The objective of the program should be to establish a racially balanced neighborhood through advertising and other methods, intended to inform minority and majority households that the Neighborhood 10 housing is available on an equal opportunity basis.

C. COMMERCIAL LAND USE

1. NEIGHBORHOOD COMMERCIAL USES

In accordance with the Community Plan, a 4.0-acre neighborhood commercial center will be provided in Neighborhood 10 in development area 11. This neighborhood commercial center will respond to the daily convenience needs of the residents within the neighborhood. Typical uses permitted could include a small supermarket or convenience food store, drugstore, small restaurant, hardware stores, laundromats and cleaners, beauty and barber

shops and other miscellaneous services stores. As shown in Figure 8, the neighborhood commercial center can be accessible via Carmel Mountain Road and/or Carmel Country Road.

2. COMMUNITY AND REGIONAL COMMERCIAL USES

The nearest community commercial uses to Neighborhood 10 residents is the Town Center located south of Del Mar Heights Road and east of El Camino Real approximately two miles to the north. The Town Center contains major commercial development for the Carmel Valley community. Also, a neighborhood commercial center is located in Neighborhood 6, and, to the south of the community, Sorrento Hills is planning an approximate 21-acre neighborhood commercial center. University Town Center is the nearest regional commercial center to the planning area and is conveniently located approximately six miles to the south, just east of Interstate 5.

III. NEIGHBORHOOD AND COMMUNITY FACILITIES

A. PARK FACILITIES

1. ELEMENTARY SCHOOL/NEIGHBORHOOD PARK COMPLEX

A combination neighborhood park and school site is proposed in the western portion of the Planning Area on the south side of Carmel Mountain Road. In accordance with the Carmel Valley Community Plan, Neighborhood 10 will provide an elementary school/neighborhood park complex to the surrounding community.

Neighborhood parks are designed to serve the local park needs of surrounding pedestrian oriented population. Typically, a neighborhood park should contain a minimum of 5 usable acres when located adjacent to an elementary school and 10 acres when not so located. The park would provide a play area, multi-purpose courts, picnic facilities, open space and lawn areas to serve resident population from 3,500 to 5,000 persons.

Entrance into the school/park complex will be located on a local interior street and not on Carmel Mountain Road. The complex, which will be below the grade of Carmel Mountain Road, will have sufficient landscape buffering and an attractive sound attenuation wall along the northern boundary. Figure 32A, *Typical Walls and Fences*, includes examples of typical walls that are suitable for sound attenuation purposes, although the height of the wall may vary depending upon the amount of mitigation required.

The City and the school district have developed and are currently implementing a concept in which schools and parks share available field facilities. In addition, the City would propose to acquire the non-building area (field facilities) of a school/park complex, while the school district would acquire only the property on which the school buildings would be placed.

The Neighborhood 10 Precise Plan has been prepared in accordance with this concept. This reflects the joint use concept for the site and is related to the treatment in the City Facilities Benefit Assessment Program and the School Facilities Financing Authority, which are the entities for financing the site.

Land use designation for the site will, at a minimum, be as follows:

- 3.0 usable acres for the elementary school
- 7.0 usable acres for the active playfields/joint use
- 5.0 usable acres for the neighborhood park
- 15.0 total usable acres

Should the school district determine that the school site will not be needed to serve the anticipated population, a minimum of 10.0 usable acres will have to be set aside for neighborhood park use. The remaining 5.0 acres can be developed with up to 15 single family residences within the 0-5 du/ac density so long as the total approved dwelling units (i.e., 1,566 dwelling units maximum) for Neighborhood 10 is not exceeded.

2. COMMUNITY PARK FACILITIES

The Carmel Valley Community Plan proposes a community park to be located in the southern portion of the community. The Neighborhood 8A Precise Plan includes a 20-acre community park site to meet the recreational needs of the area. The park site will be approximately one mile from the Neighborhood 10 Planning Area.

Community parks typically provide a wide range of facilities including athletic fields and multi-purpose courts, picnic facilities, a variety of play areas, a recreation center building and lawn areas. Community parks are intended to serve between 18,000 and 25,000 residents and should be located within a 1-1/2 mile distance from one another.

B. OPEN SPACE

1. OVERVIEW

The Neighborhood 10 Precise Plan proposes a significant amount of open space, as shown in Figure 8. Open space defines the character of Neighborhood 10 and provides the community with a valuable asset that not only preserves and enhances natural resources, but also provides a positive psychological benefit to the area-wide residents. Approximately 44.4% or 358.1 acres of the Precise Plan area will be preserved as natural open space. In addition, another 7.3% or 59.2 acres will be devoted to manufactured slopes. These slopes will be landscaped with drought tolerant, native, and/or ornamental plant species, as appropriate to the particular slope condition and location. Much of the open space area represents "*sensitive lands*" as defined by the City's Resource Protection Ordinance (RPO). A portion of the open space is reflected by revegetated manufactured slope areas which will be graded and landscaped to blend into the existing natural open space.

2. OPEN SPACE

a. NATURAL OPEN SPACE

The Neighborhood 10 Precise Plan proposes 358.1 acres to be preserved as natural open space, exclusive of manufactured slopes. This area includes finger canyons, canyon slopes and basins, previously farmed areas and "*sensitive lands*" as defined by the RPO. The natural open space not only provides significant view opportunities from the mesa tops, but is also visible from canyon basins, major roadways, and areas outside of Neighborhood 10.

The potential for views of development from Los Peñasquitos Canyon and Shaw Valley are limited due to the considerable distance and height of existing topography. A detailed cross-section analysis was performed which depicted view opportunities from the center-line of major canyons into proposed development areas and revealed that minimal development will be noticeable. In addition, several field surveys involving flagging of future development areas and site visits with City staff concluded that minimal intrusion of noticeable housing will take place from major canyon areas (see Figures 23, 24A, 24B, and 24C).

It is anticipated that preservation and maintenance of this open space area will be in the form of fee ownership by the City, open space easement to the City or some other similar option.

b. OPEN SPACE CORRIDOR/ROAD UNDERCROSSINGS

Two significant open space corridors are located in the Neighborhood 10 Precise Plan area. One of the open space corridors is located in the western portion of the project between development areas 1 & 2 and development areas 14 & 15 and runs through the canyon bordering Carmel Valley Neighborhood 8A to the mouth of Shaw Valley. The second corridor will be located on the far eastern portion of the property and will run along the borders between Neighborhood 10 and the adjacent Future Urbanizing Area to the east and north. Both corridors will serve to connect Los Peñasquitos Canyon Preserve to the south of the Precise Plan area with the Carmel Valley Restoration and Enhancement Project (CVREP) to the north, thereby allowing for uninterrupted wildlife movement in the region.

Several underpasses and, provided that adequate funding sources are available, one or more bridges are planned in Neighborhood 10 to allow wildlife to travel under the roads and between the on-site ravines easily and safely. A bridge will be constructed with a 90-foot-span and with a minimum height clearance of 20 feet at Carmel Mountain Road and the western open space corridor. Funding for the bridge is anticipated to be available via the Carmel Valley South FBA. If public funding for the bridge is not available, then three parallel arch pipe style culverts will be constructed at this location under Carmel Mountain Road. The western open space corridor at Carmel Mountain Road shall be a minimum of 400 feet in width for a maximum length of 500 feet, before gradually widening to an optimum width of 1,000 feet.

An undercrossing shall be constructed under Carmel Mountain Road at the open space corridor, just east of Planning Areas 5 & 6. This undercrossing shall consist of one (or more, if feasible) arch pipe style culvert(s). The culvert(s) will be approximately 15 feet in height by 30 feet in width. In addition, either two parallel arch pipe style culverts, each measuring 15 feet in height by 30 feet in width, or a bridge if additional funding is available, shall be constructed under Carmel Country Road, at the north end of the Precise Plan area, near development areas 8 and 18. The actual length of the each undercrossing shall be determined concurrently with

preparation of detailed construction documents for Carmel Mountain Road. Equestrian/hiking trails are compatible with the open space designation in the precise plan area, provided that such trails are limited to areas already informally in use for such purposes or, if new alignments are proposed, that appropriate environmental analysis and mitigation be provided for those new alignments.

c. REVEGETATED SLOPES

Approximately 59.2 acres of the designated open space area will consist of revegetated manufactured slope areas. Where these slopes have high visibility, the majority of the slopes will be contour graded per the standards set forth in Section IV.C of this Precise Plan. Slope gradients will vary from 2:1 to 4:1 with average overall gradients of 2-1/2:1. In addition, another approximately 37.6 acres of manufactured slopes will be created within residential development areas. Specific grading, hillside and landscape guidelines are being proposed to prevent erosion and insure the selection of proper slope plantings. Section IV.C, *Grading and Hillside Standards*, provides guidelines for proposed development in areas with slopes of 25% or greater. The landscape design section of this Precise Plan also addresses transition planting guidelines between natural open space and development areas.

d. DRAINAGE BASINS

Detention basins and associated drainage facilities such as pipelines are permitted uses in all areas designated as open space in the Neighborhood 10 Precise Plan. Pump stations shall not be permitted within natural open space areas. During project construction, the detention basins would be inspected after major storms, then cleaned and maintained as needed by the developer for a one- to two-year period. Maintenance may consist of removing settled material from the basins after major storm events and shall be the responsibility of the developer(s). Following the developer maintenance period, if the system has a record of satisfactory performance as determined by the City Engineer, ownership of the facilities shall be accepted by the City of San Diego. At that time, maintenance shall be conducted by the City's streets division or other appropriate City division, with funds collected under the storm drainage fee program.

3. TRAILS - EQUESTRIAN AND PEDESTRIAN

Although the Neighborhood 10 Precise Plan does not propose active recreational uses within open space areas, pedestrian and equestrian trails are permitted uses in all areas designated as open space. In fact, informal horse riding trails exist and are being used in the Neighborhood 10 canyon areas. It is the intent of this Precise Plan to allow for the continued use of the existing informal trail connection through the westerly canyon on-site. A structural connection (i.e., a bridge or arch pipe style undercrossing) to Los Peñasquitos Canyon Preserve under Carmel Mountain Road is proposed as part of this Precise Plan. The existing informal trails shall be modified as necessary to enable horses to cross under Carmel Mountain Road safely via means of the planned structural connection.

a. EQUESTRIAN TRAIL

An east-west equestrian trail is located within Neighborhood 8 and proposed in the EIR for State Route 56 West, Carmel Valley Restoration and Enhancement Program (CVREP). The trail is planned to run east of Carmel Country Road along the north-facing slopes. In the 1975 *Plan for Equestrian Trails and Facilities* (City of San Diego), the "connector trail" is proposed to extend eastward to Black Mountain Park. Neighborhood 10 proposes an informal linkage to this trail via the north-south trails discussed below.

The 1975 plan for equestrian trails also called for the "El Camino Real" trail to extend from San Clemente Canyon Park, now Marian Bear Memorial Park south of University City, northward to Los Peñasquitos Canyon Preserve, then north through the middle of Carmel Valley, to the San Dieguito River Valley. This "major trail" was not included in the *Carmel Valley Community Plan* nor in subsequent development plans.

Three joint use equestrian/hiking trails are proposed in Neighborhood 10 as shown on Figure 15A, *Alternative Transportation Facilities*. The Western Trail, in a north-south alignment, is proposed to link the Peñasquitos Canyon Preserve with a trail in the adjacent Neighborhood 8 planning area. The Western Trail will pass under a bridge at Carmel Mountain Road and utilize a culvert to pass under Carmel Country Road. A smaller east-west aligned natural trail will intersect the Western Trail and provide access to the Central Canyon area. This trail is proposed as a cul-de-sac. A third trail is proposed in Shaw Valley. It is proposed that the Shaw Valley Trail link Peñasquitos Canyon with the Western Trail, connecting with the latter trail near the culvert undercrossing at Carmel Country Road. The Shaw Valley Trail will utilize a culvert to cross under Carmel Mountain Road east of Planning Areas 5 and 6.

Only the Shaw Valley Trail is proposed to be improved meeting design criteria of the City's adopted "*A Plan For Equestrian Trails and Facilities*." Trail improvements could either be funded by an assessment district or as a community Facilities Benefit Assessment (FBA) project. Trail improvements will require evaluation for potential environmental impacts.

b. PEDESTRIAN TRAIL

Pedestrian trails are proposed within Neighborhood 10 to traverse east-west through the central canyon and to span the western project perimeter between Carmel Country Road and Street A (Figure 16A). Public access to natural open space areas will be limited from residential development areas within Neighborhood 10 to a series of trails to be constructed on existing dirt roads that traverse the western half

of the Precise Plan area. These trail locations must be approved by the City Park and Recreation Department.

Pedestrian circulation will be possible on sidewalks along local residential streets, as well as along both sides of Carmel Country Road, Street A, and Carmel Mountain Road. These sidewalks will be designed and maintained per the City of San Diego as discussed in the circulation element of this Precise Plan.

4. GOLF COURSE

An open space canyon on-site, just north of development areas 7 and 8, is planned to remain in open space as a part of this Precise Plan. However, a golf course within the Bougainvillea Specific Plan, located directly to the north of Neighborhood 10, has been approved by the City Council. A portion of this approved golf course is actually located within the Neighborhood 10 Precise Plan boundary, in the open space canyon just north of development areas 7 and 8. The portion located on-site is designated as open space in this Precise Plan.

IV. PUBLIC SERVICES AND FACILITIES ELEMENT

A. SCHOOLS

Del Mar Union School District and San Dieguito Union High School District service this part of the Carmel Valley area. Currently, the schools that would serve the project site have sufficient capacity to serve the existing enrollment (Table V). Torrey Pines High School in Neighborhood 7 would serve the high school age population of the project site. Junior high students would attend Earl Warren Junior High School in Solana Beach. There are local elementary schools in Neighborhood 1 and Neighborhood 5. In addition, a junior high school is planned to open in 1999 within Neighborhood 9.

Based on generation factors provided by the school districts (1997), Neighborhood 10 at build-out will generate approximately 632 elementary school students, 158 junior high school students, and 316 senior high school students.

The Carmel Valley Community Plan proposes an elementary school site adjacent to a neighborhood park within Neighborhood 10. The proposed 15.0-acre school/park complex will be located on Carmel Mountain Road which will provide elementary school facilities within a mile of the residential community. Should the school district determine that future populations do not warrant the need for this school site, it should be developed as a 10.0-acre park site with the remaining 5.0 acres developed with residential uses.

TABLE V
SCHOOLS SERVING THE PROJECT AREA

SCHOOL	GRADES	ENROLLMENT	CAPACITY
Torrey Pines High School	9-12	2,230 ¹	2,188
Earl Warren Junior High School	7-8	1,007 ¹	1,145
Del Mar Union Elementary Schools	K-6	1,701 ²	1,563 ³

Source: RECON

¹ 1996 enrollment figure

² 1997 enrollment figure

³ 1996-97 school year capacity for the three district elementary schools: Del Mar Heights Elementary, Del Mar Hills Elementary, and Carmel Del Mar Elementary Schools. The capacity of the elementary schools is currently undergoing revision by the district.

B. POLICE

Police protection for the Planning Area will be provided by the Northern Division of the San Diego Police Department. The nearest station is located at: 4275 Eastgate Mall, University City.

C. FIRE

The San Diego Fire Department serves the Carmel Valley community. It is anticipated that the Planning Area will be served by Station No. 24 which opened for operation in May, 1993. Station No. 24 is located at 13077 Hartfield Avenue in Neighborhood 7. The City also provides automatic aid agreements with other fire departments in the area such as the San Diego Fire Department Station 41 located at 4914 Carroll Canyon Road, Del Mar Fire Station 1 located at 2200 Jimmy Durante Boulevard and San Diego Fire Department Station 35 located at 4285 Eastgate Mall.

D. UTILITIES

The following utilities will be operated by public and semi-public agencies in Neighborhood 10:

1. WATER

The North City West Domestic Water System Master Plan prepared by Lowry and Associates dated June 1980 called for a 30" transmission main (Green Valley Pipeline) connection between the 30" Del Mar Heights Pipeline and the 51" Miramar Pipeline. Construction of the Green Valley Pipeline has been completed. The studies conducted for this area have concluded that a water main in Carmel Mountain Road and a water distribution main in Carmel Country Road are needed to provide water to that area. The sizes of these mains would be determined in later studies. (See Figure 10, *Water / Sewer Plan.*)

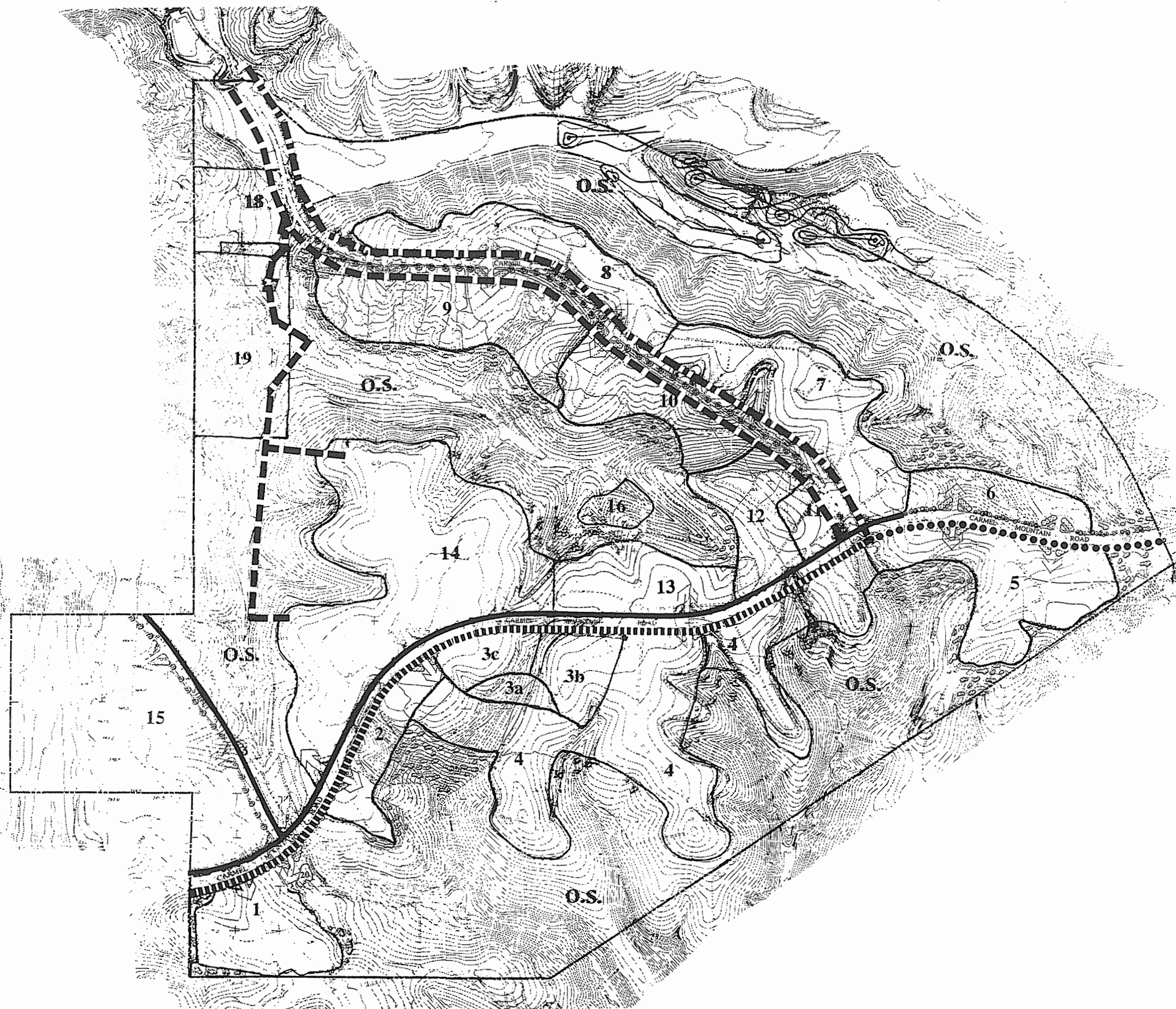
A water system master plan for Neighborhood 10 will be prepared as a condition of approval for the first tentative map filed.



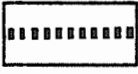
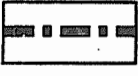

2. SEWER

Carmel Valley Neighborhood 10 is within the service area of the Metropolitan Sewage System (Metro). The site flows by gravity within three major basins that ultimately feed to Pump Station 65.

The area north of Carmel Mountain flows northerly into the Carmel Valley Trunk Sewer by a main in Carmel Country Road and a main through a tributary to Shaw Valley. Figure 10 indicates the proposed sewer system. This Precise Plan proposes that flows from the

WATER / SEWER PLAN



-  PROPOSED SEWER MAIN
(SIZE TO BE DETERMINED)
-  PROPOSED WATER MAIN
-  PROPOSED WATER MAIN
-  PROPOSED WATER MAIN
-  PROPOSED SEWER MAINS

NOTES

1. Master Water and Sewer Plans will be required prior to the Final Map to determine the appropriate sizes of facilities needed.
2. The Sewer Plan will use pump stations to pump sewage for some of the residential areas located south of Carmel Mountain Road uphill to the Carmel Mountain Road Trunk Line. Sewer pump stations will be provided in graded areas, not in natural open space areas.
3. The ultimate sizes of water mains will be determined by the conclusions of the 610/712 Water Study

FIGURE 10

PRECISE

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10



T&B Planning Consultants
3242 HALLADAY, SUITE 100
SANTA ANA, CALIF. 92705 (714) 462-2774
5879 OBERLIN DRIVE, SUITE 208
SAN DIEGO, CALIF. 92121 (619) 546-8366



southerly portion of the site will connect to the Carmel Mountain Road sewer main via the use of force mains and strategically located pump stations. The Penasquitos Trunk Sewer is approaching its ultimate capacity. Although no connections from this development to the Penasquitos Trunk Sewer are planned at this time, if in the future any wastewater from this development flows into the Penasquitos Trunk Sewer then connections may be limited. Development area 20 may develop with one residence on a septic system (subject to the necessary City and Health Department regulations and approvals), or use a pump station to tie into the local sewer system planned in development area 1.

The City of San Diego Engineering and Capital Projects has monitored flows in the Peñasquitos Trunk Sewer and included a parallel main in the Capital Improvement Program (CIP). The proposed trunk main is currently under construction and consists of a new pump station located in the area immediately east of I-15 and north of Scripps Parkway, and a pipeline extending from the new pump station to the North City Water Reclamation Plant. The pump station is expected to be completed in the fall of 1997, and completion of the trunk main is anticipated by late 1997 or early 1998. At this time, planning is underway for a sewer trunk main in Mira Mesa Boulevard; this trunk main has been included in the CIP.

3. DRAINAGE

Storm drain runoff will be collected in standard inlet facilities and conveyed by pipes principally located in streets and generally paralleling the sewer system. Outfalls are into Shaw Valley, a tributary to Carmel Creek to the north, and a series of tributary canyons to Peñasquitos Creek to the south. Figure 11, *Major Drainage Course/Power Easement*, indicates the natural drainage basins within Neighborhood 10.

As stated in the approved drainage study for Carmel Valley, prior to or as part of recordation of the first Final Map in the Neighborhood 10 Precise Plan area, a Master Drainage Plan for the Precise Plan area shall be prepared that will address the facilities required to control runoff and mitigate potential erosion or siltation problems. The Master Drainage Plan will demonstrate that any development proposed in coastal lagoon watersheds will not increase the rate of runoff reaching the lagoon in a ten (10) year design storm.

According to the Clean Water Act, "best management practices" (BMPs) to control pollutants and sediment from entering storm water runoff are required for the Precise Plan area during construction. The Precise Plan provides source control BMPs by requiring landscaping of all manufactured slopes and street rights-of-way to prevent erosion and by incorporation of a grading/drainage concept which directs water away from easily erodible areas and into a drainage system designed to safely handle the storm water runoff. Additionally, two detention basins and two desilting basins will be located at drainage confluence points to intercept stormwater runoff from developed areas.

DRAINAGE / POWER EASEMENT

-  DRAINAGE COURSE
-  SAN DIEGO GAS & ELECTRIC EASEMENT

Note : Arrows indicate direction of flow

FIGURE 11



T&B Planning Consultants

3242 HALLADAY, SUITE 100
SANTA ANA, CALIF. 92705 (714) 662-2774

5879 OBERLIN DRIVE, SUITE 208
SAN DIEGO, CALIF. 92121 (619) 546-8366



PRECISE

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

Any other applicable source control or BMP which may be implemented on a City-wide basis in conjunction with the City's Municipal National Pollutant Discharge Elimination System permit (Permit No. A018758) and State Regional Water Quality Control Board Order Number 90-42 shall be incorporated into the Precise Plan. The City Manager shall verify that the mitigation measures contained in this Precise Plan regarding storm water and drainage management and mitigation of urban runoff flows are conditions for the approval of all subsequent Tentative Maps within the Neighborhood 10 Precise Plan area.

4. POWER

Power lines and service will be provided by San Diego Gas and Electric (SDG&E). Proposed lines, which will be undergrounded, are planned to run parallel to the future extension of Carmel Country Road and Carmel Mountain Road, south of Carmel Valley. SDG&E plans also include a substation to be located in Neighborhood 7.

A 300-foot SDG&E easement located south of Neighborhood 10 will not serve as a power source for the Precise Plan area. Adequate power sources will be provided via the extension and improvement of Carmel Country and Carmel Mountain Roads into Neighborhood 10.

E. LIBRARY

The Carmel Valley Branch Library lies within the Carmel Valley Town Center within Neighborhood 9. The library opened on July 10, 1993, and is located at 3919 Townsgate Drive. The library contains a multi-media area, separate adult and children's areas, a reading room with a fireplace, various offices and a community room with a patio.

V. CIRCULATION

A. REGIONAL ACCESS

Regional access to the central and southern portions of the Carmel Valley Community Plan Area will be provided by the Interstate 5/Route 56 Freeway Interchange. The I-5/SR-56 Interchange is currently under construction and is scheduled for completion in April of 1998. Figure 12 indicates the regional circulation system serving Neighborhood 10 and the average daily traffic (ADT) at buildout of the Planning Area. Interstate 5 provides access from Carmel Valley to the San Diego Metropolitan Area. Carmel Valley Road, and SR 56, provides access from within Carmel Valley to Interstate 5.

B. NEIGHBORHOOD STREET SYSTEM

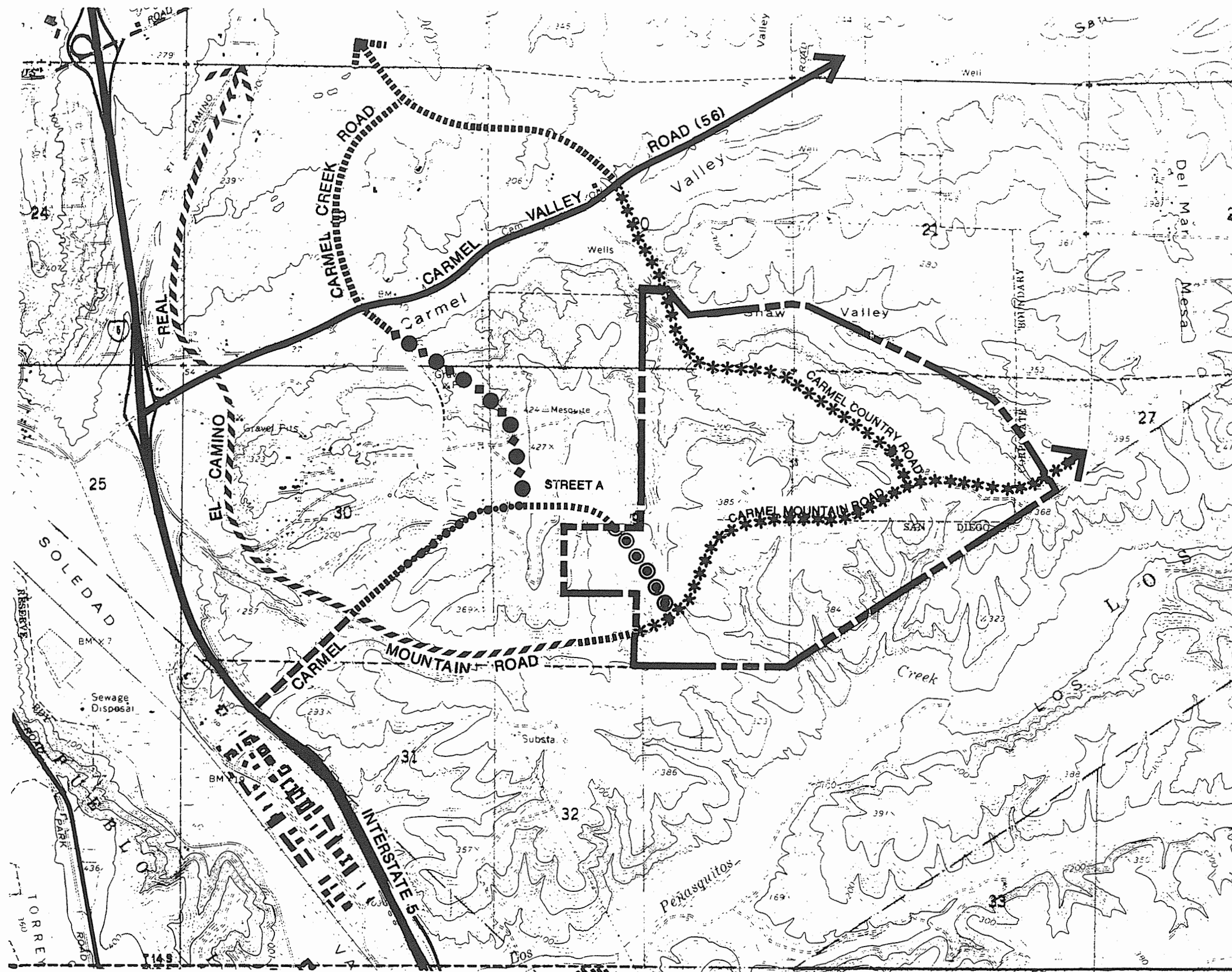
1. STREET CLASSIFICATIONS

The street classifications proposed for the Neighborhood 10 Precise Plan area are depicted in Figure 12, Regional Circulation, and are described as follows:




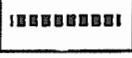
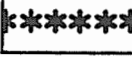



- ***Carmel Country Road:*** Planned as a modified four-lane collector with a right-of-way width of 98 feet and a minimum design speed of 35 mph.
- ***Carmel Mountain Road:*** Planned as a modified four-lane collector with a right-of-way width of 98 feet and a minimum design speed of 45 mph.
- ***Street A:*** Planned as a modified two-lane collector with a right-of-way width of 92 feet and a minimum design speed of 35 mph.
- ***Residential Local:*** Planned as two-lane roadways with rights-of-way widths ranging from 43 feet to 60 feet.

The most current adopted street design requirements set forth by the City of San Diego will be adhered to in the design of the Neighborhood 10 street system.

The phasing of the major streets within Neighborhood 10 is discussed in Section VII.D, Development Phasing. The phasing of all other interior collector and local streets will be directly tied to the development phasing, also discussed in Section VII.



REGIONAL CIRCULATION

-  **FREEWAY**
-  **6 LANE PRIMARY ARTERIAL**
(122' R.O.W.)
-  **6 LANE MAJOR**
(122' R.O.W.)
-  **4 LANE MAJOR**
(98' R.O.W.)
-  **4 LANE COLLECTOR**
(98' R.O.W.)
-  **MODIFIED 2 LANE COLLECTOR**
(92' R.O.W.)
-  **MODIFIED 2 LANE COLLECTOR**
(74' R.O.W.)
-  **MODIFIED 3 LANE COLLECTOR**
(86' R.O.W.)

PRECISE

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

FIGURE 12



T&B Planning Consultants
3242 HALLADAY, SUITE 100
SANTA ANA, CALIF. 92705 (714) 662-2774
5675 OBERLIN DRIVE, SUITE 200
SAN DIEGO, CALIF. 92121 (619) 546-8366

2. STREET IMPROVEMENTS

Carmel Mountain Road, Carmel Country Road, and Street A may be designed and constructed as community-wide facilities using a mechanism such as a facility benefit district. Because these roads benefit the entire Neighborhood 10 community, as well as the surrounding region as a whole, the fees necessary to design and construct these streets are included within the Carmel Valley Public Facilities Financing Plan (PFFP) . The actual financing mechanism to be used for construction of Carmel Mountain Road, Carmel Country Road, and Street A are included in the PFFP .

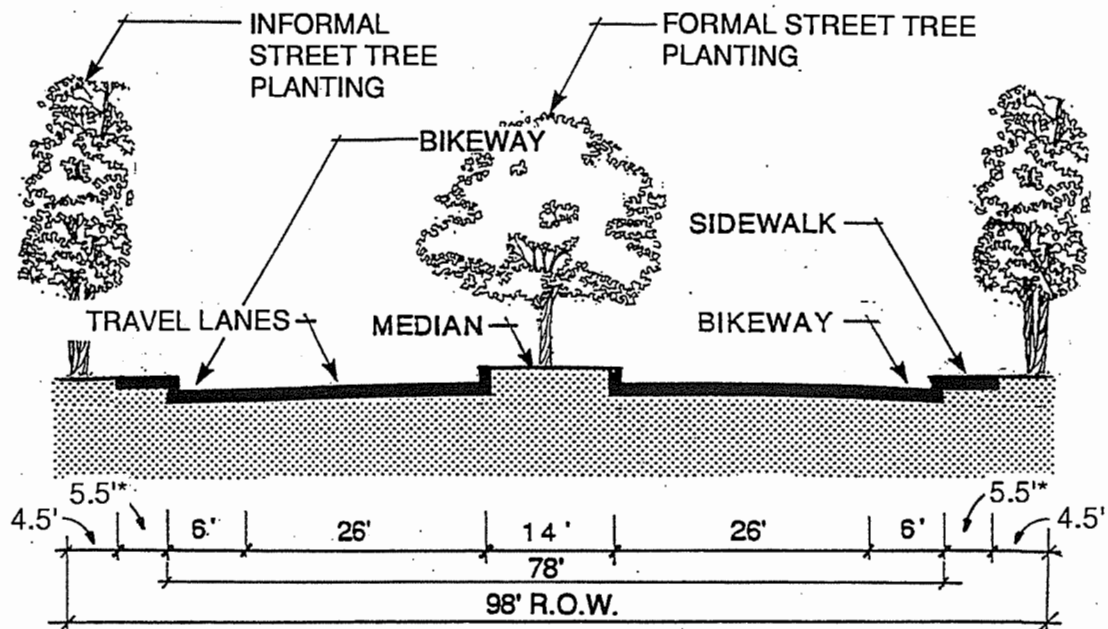
Residential local streets within each development area shall be the responsibility of the developer/builder to design and construct. All streets within the Neighborhood 10 Precise Plan area that are constructed to City standards will be dedicated to and maintained by the City of San Diego as public streets.

C. STREET DESIGN

All Neighborhood 10 proposed street designs shall conform to City Document No. 769635, dated October 28, 1987, or subsequent updates, for standards for street rights-of-way and improvements installed therein.

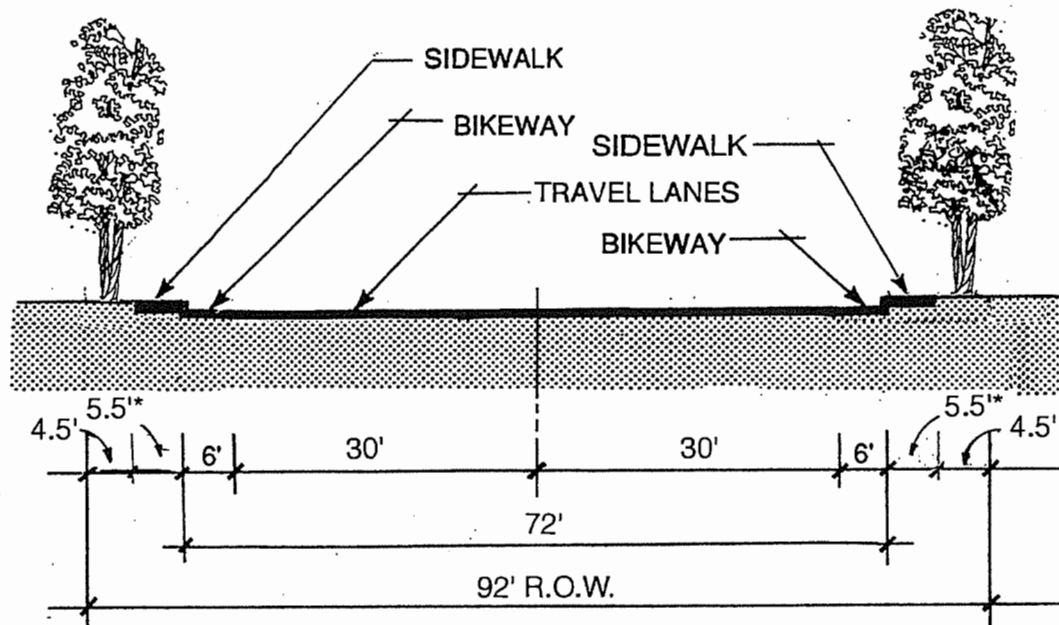
Typical street sections for local streets, Carmel Country Road and Carmel Mountain Road, are shown in Figures 13, 14, and 15. The landscape design section discusses the proposed parkway plant palette.

Bicycle travel is accommodated in a marked lane next to the curb on the main thoroughfares, while pedestrians are accommodated by sidewalks. Sidewalks on Carmel Mountain Road, Carmel Country Road, and Street A , shall be constructed directly adjacent to the curb. Sidewalks on local streets may be either directly adjacent to the curb or noncontiguous with the curb.



4 LANE COLLECTOR

CARMEL MOUNTAIN ROAD AND CARMEL COUNTRY ROAD



MODIFIED 2 LANE COLLECTOR

STREET "A"

*AS MEASURED FROM FACE OF CURB

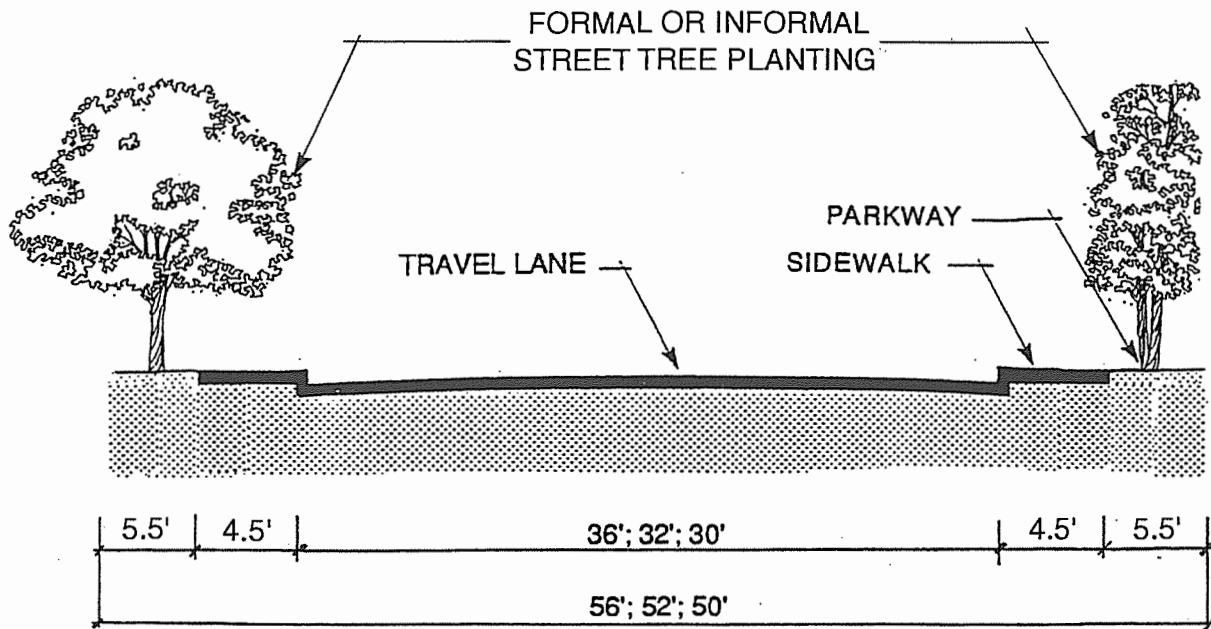
ROADWAY CROSS SECTIONS
PRECISE

FIGURE 13

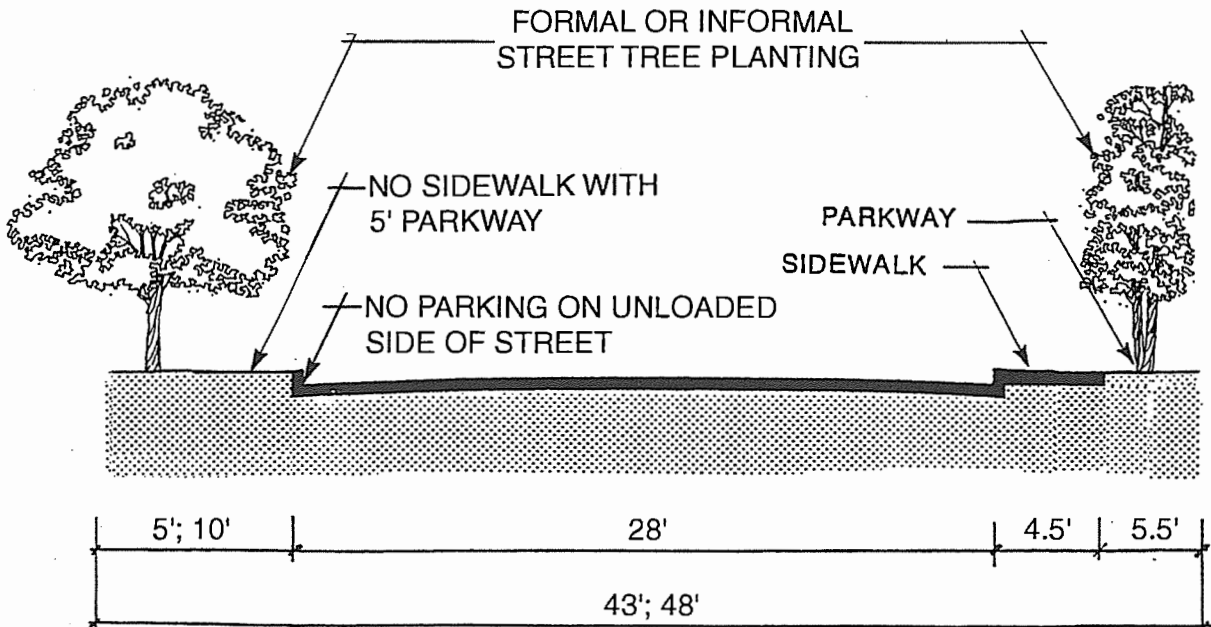


T&B Planning Consultants
 3242 HALLADAY, SUITE 100
 SANTA ANA, CALIF. 92705 (714) 663-2774
 5879 OBERLIN DRIVE, SUITE 208
 SAN DIEGO, CALIF. 92121 (619) 546-8366

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10



RESIDENTIAL LOCAL STREET DOUBLE LOADED



RESIDENTIAL LOCAL STREET SINGLE LOADED

ROADWAY CROSS SECTIONS **PRECISE**

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

FIGURE 14

T&B Planning Consultants
3242 HALLADAY, SUITE 100
SANTA ANA, CALIF. 92705 (714) 662-2774
5479 OBERLIN DRIVE, SUITE 208
SAN DIEGO, CALIF. 92121 (619) 546-8366

D. ALTERNATIVE TRANSPORTATION FACILITIES

Practical alternatives to private automobile travel are stressed in the Carmel Valley Community Plan. Public transit, bicycle travel and pedestrian circulation systems are proposed for the community. Neighborhood 10 reflects these Community Plan objectives and provides for neighborhood transit, bicycle, equestrian and pedestrian alternatives related to the community circulation network. (Please see Figure 15A, *Alternative Transportation Facilities*.)

1. PUBLIC TRANSIT

Public transit will be provided by Metropolitan Transit Development Board (MTDB). The proposed Commuter Express Route 960 will be an express route linking Carmel Valley and North University City with Center City during the am/pm rush hours only. Commuter Express Route 960 is planned to include a stop at the Del Mar Highlands Town Center located at the intersection of Del Mar Heights Road and El Camino Real, approximately two miles north of Neighborhood 10.

Currently, Caltrans has plans for two Park-N-Ride facilities within Carmel Valley. One facility is to be located at the corner of El Camino Real and Townsgate Drive as designated by Caltrans. The facility will be approximately one acre in size and provide approximately 110 parking spaces. This location provides direct proximity to bus service on El Camino Real and is away from conflicting facilities such as the shopping center's parking areas. The other facility will be located at the Route 56 and I-5 Interchange or the I-5 and Carmel Valley Road Interchange.

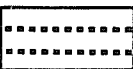
There is an existing transit center located in the Town Center in the Neighborhood 9 Precise Plan, adjacent to El Camino Real. The center contains a bus shelter, as well as stalls for buses.

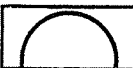
Both the Park-N-Ride facilities and the Transit Center are located adjacent to major pedestrian and bicycle routes. Bicycle parking facilities should be located at these facilities.

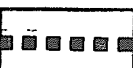
2. BIKEWAYS

Bicycle lanes will be provided within the Neighborhood 10 Precise Plan area and will connect to the bikeway network in neighboring communities. On-street bicycle lanes in Neighborhood 10 will follow Carmel Country Road, Carmel Mountain Road and Street A. Thus, bicycle access to the neighborhood commercial center and elementary school/park complex will be made possible. Bicycle movement will also be feasible along residential streets, although marked lanes will not be provided in these locations.


ALTERNATIVE TRANSPORTATION FACILITIES

- 


ON-STREET BIKE LANE &
CONTIGUOUS SIDEWALK
- 

ROAD UNDERCROSSING
- 

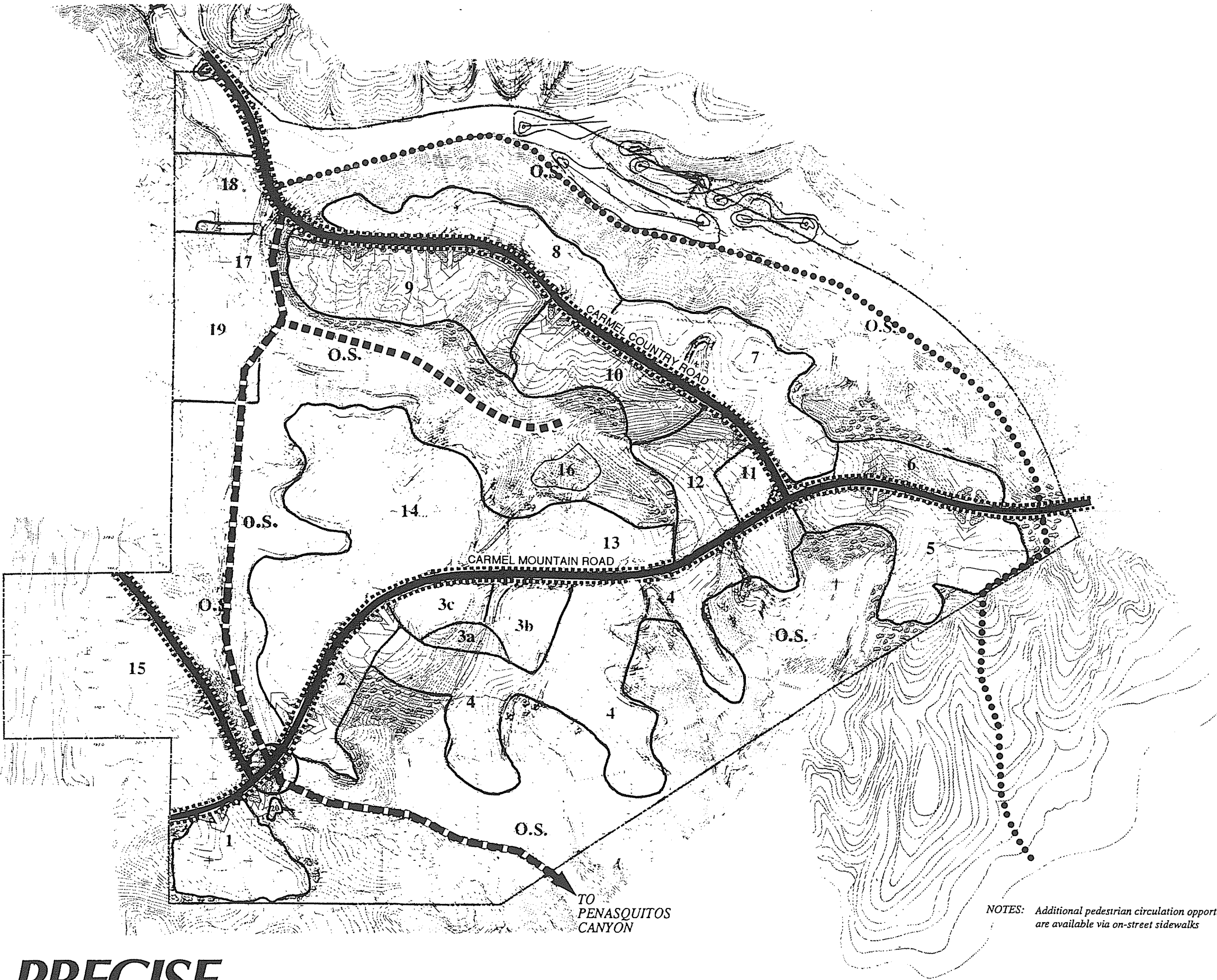
EXISTING NATURAL TRAILS
- JOINT USE EQUESTRIAN /
HIKING TRAILS



WESTERN TRAIL



SHAW VALLEY TRAIL
*Precise alignment of shaw valley trail has not been
determined. When precise alignment is concluded,
environmental documentation must be completed.*



NOTES: Additional pedestrian circulation opportunities are available via on-street sidewalks

PRECISE

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

Bicycle lanes within Neighborhood 10 will include the following components:

- Marked bicycle lanes in conformance with City of San Diego striping and width requirements within the rights-of-way along Carmel Country Road, Carmel Mountain Road, and Street A.
- Bicycle sensitive traffic signals at major entry points or where deemed necessary by City standards.

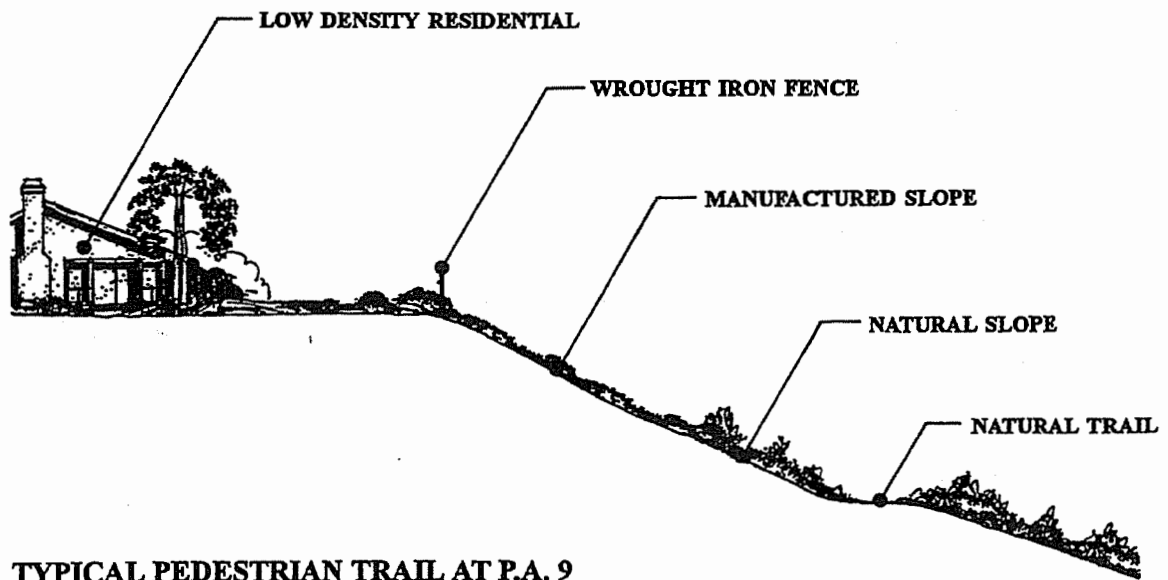
3. BICYCLE PARKING FACILITIES

Bicycle parking facilities consist of bicycle racks. Bicycle racks should not require the use of chains or cables to secure them as chains and cables are easily cut by thieves using bolt cutters. It is recommended that bicycle rack design encourage the use of U-shaped high security locks to lock bicycles.

4. PEDESTRIAN TRAVEL

The pedestrian path system for Neighborhood 10 will include the following elements:

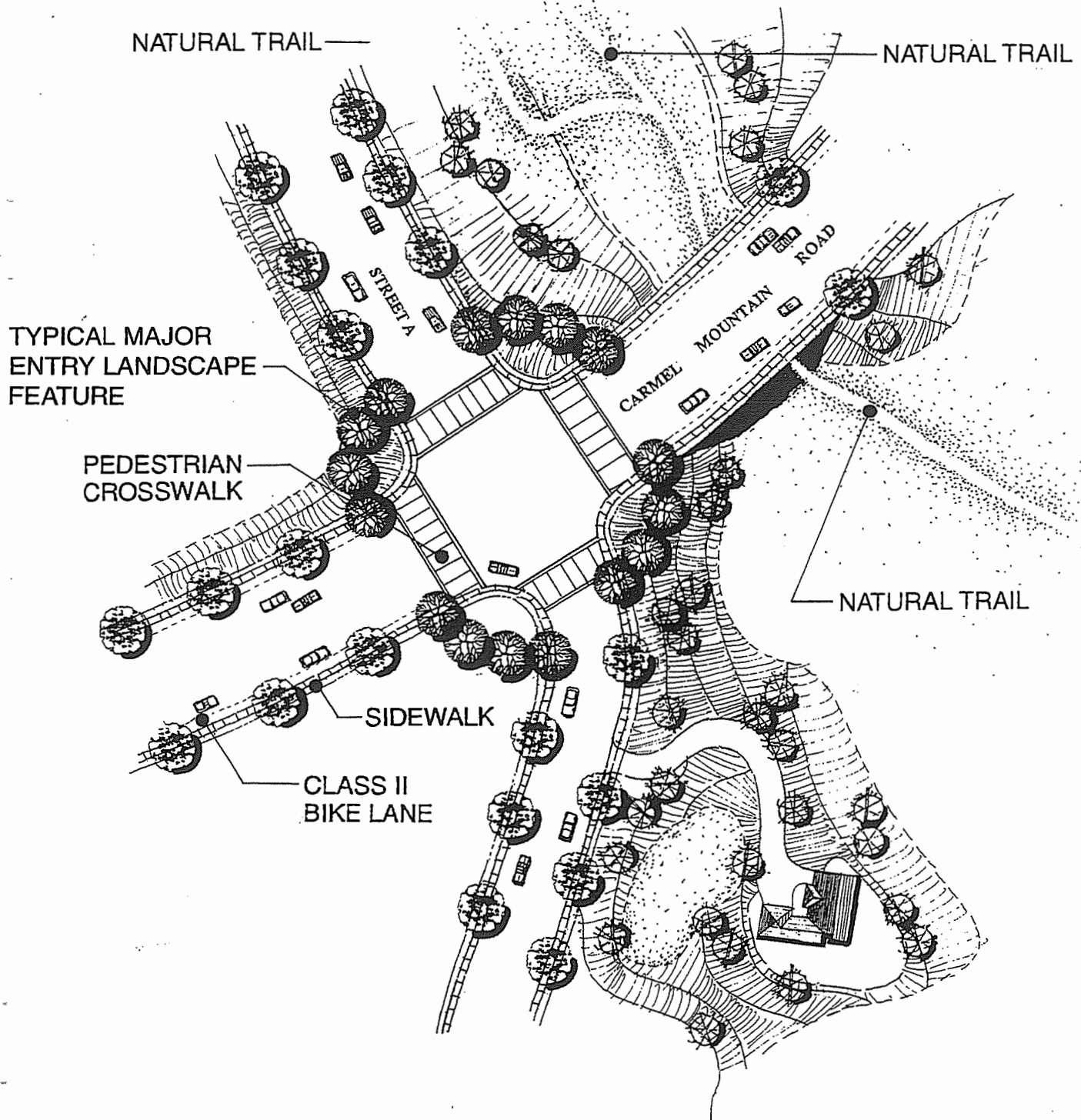
- Sidewalks along local residential streets, designed to City of San Diego requirements.
- Traffic signals along Carmel Country Road and Carmel Mountain Road at major entrances to Neighborhood 10 where neighborhood and community-wide pedestrian systems intersect.
- Existing natural trails that traverse through the central east-west canyon and run north-south parallel to the western project perimeter will accommodate pedestrians (see Figure 15A for trail locations). The illustration (below) depicts the relationship of an existing trail and the "very low density" residential development planned in development area 9.



TYPICAL PEDESTRIAN TRAIL AT P.A. 9

5. EQUESTRIAN TRAVEL

One improved and two natural trails are proposed in Neighborhood 10 (Figure 16A). The one proposed improved trail is the Shaw Valley Trail, and the two natural trails are the Western Trail and the existing natural trail, as shown on Figure 16A. A plan view of the bridge undercrossing at Carmel Mountain Road for the Western Trail is illustrated in Figure 15B, *Equestrian Trail (Plan View)*.



EQUESTRIAN TRAIL (PLAN VIEW)

PRECISE

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

FIGURE 15B



T&B Planning Consultants
3242 HALLADAY, SUITE 100
SANTA ANA, CALIF. 92705 (714) 662-2774
5879 OBERLIN DRIVE, SUITE 208
SAN DIEGO, CALIF. 92121 (619) 546-8366



E. PARKING

Standards for off-street parking are incorporated into the zoning regulations for each type of use in the Planned District Ordinance. Shared parking should be considered in the park/school complex. No parking will be permitted on Carmel Mountain Road, Carmel Country Road, or Street A, because the on-street areas next to the curbs are reserved for bicycle lanes.

1. RESIDENTIAL

Each residential project shall provide adequate off-street parking, as required by the Carmel Valley Planned District Ordinance.

2. COMMERCIAL

- Parking areas shall be integrated into the overall design of the projects they serve.
- Unobstructed and adequate maneuvering aisles or turn-around areas shall be provided to ensure that all vehicles shall enter the street or highway in a forward manner.
- Where parking areas are visible from a street, there shall be a landscaped area between the curb and parking area planted with trees, shrubs and ground cover that will significantly soften the view of such areas.
- Conflicts between automobiles, service vehicles, and pedestrians within the site shall be minimized.

F. SOUND ATTENUATION

Portions of Neighborhood 10 adjacent to Carmel Mountain Road exceed the City's criteria of 65 CNEL exterior and 45 CNEL interior for maximum acceptable noise levels for residential uses. Areas that are subject to future exterior noise levels in excess of City standards shall require that mitigation measures such as increased building setbacks, sound attenuation walls, berms, or combination thereof, be implemented in these areas to mitigate noise levels to acceptable levels. An acoustical analysis shall be required for all future tentative maps identified in the accompanying EIR as having the potential for exposure to adverse noise levels. Any mitigation recommendations contained in the acoustical analysis shall be required as conditions of approval for all tentative maps and planned development permits so affected.

VI. DESIGN GUIDELINES

A. INTRODUCTION

The purpose of this community design element is to provide designers, developers and review agencies with basic design objectives and guidelines for the development of Neighborhood 10. The overall goal is to create a strong sense of community identity while contributing to the identity of Carmel Valley as a whole. A functional and aesthetically pleasing development should result with adherence to the Design Guidelines set forth in this design element.

These Design Guidelines are not intended to restrict the creativity of the designers of Neighborhood 10. Rather, they are formulated to guide the designer while allowing flexibility. Particular architectural, site planning or landscaping solutions or styles will not be recommended. Instead, general planning concerns will be addressed (with occasional possible solutions suggested) that should be considered in the development of Neighborhood 10. Proposals presented here are conceptual and will be refined and modified in accordance with the approved objectives and guidelines during the development plan stage and subsequent stages of development of Neighborhood 10.

B. DESIGN OBJECTIVES

The following general principles and objectives should be considered in the development of Neighborhood 10. The plan should:

- Create a neighborhood identity while maintaining an overall unity in Carmel Valley.
- Create a development that responds to the physical and visual features of the Precise Plan area.
- Preserve key environmental features.
- Maximize opportunities for views.
- Mitigate significant impacts to Los Peñasquitos Canyon Preserve.
- Conform to the functional requirements described in the land use and circulation elements.
- Incorporate conservation practices into the design and maintenance of buildings and spaces.

C. GRADING AND HILLSIDE STANDARDS



The Neighborhood 10 Precise Plan focuses on the retention of basic landforms. The plan is directed toward the development of the mesa and moderate filling of certain canyon areas in order to preserve intervening canyon slopes in their natural state to the maximum extent feasible.

To provide guidance to City staff in implementing the Neighborhood 10 Precise Plan and subsequent tentative and final maps, the following guidelines have been developed. The intent of these guidelines is to provide direction for proposed development in areas with slopes of 25% or greater located within the Neighborhood 10 Precise Plan area. This Precise Plan has been designed to minimize development encroachment into slope areas of 25 percent or greater. Figure 16 depicts the locations of on-site slopes in excess of 25 percent and indicates where planned development within the Neighborhood 10 Precise Plan area will impact these areas.

- Residential development shall be sited on the least sensitive portion of the site to preserve the on-site natural landforms and biological resources while also accommodating residential lots, driveways and roads.
- Residential development shall be located to minimize the disturbance of areas containing sensitive habitat.
- View opportunities should be maximized through selective grading of development areas.
- To the maximum extent feasible, along canyon rims, cut to daylight lines in order to minimize fills on canyon slopes.
- Grading should minimize surface drainage to natural slopes.
- Direct and indirect grading and drainage impacts to Los Peñasquitos Canyon Preserve shall be avoided.
- The major canyons on-site shall be preserved by concentrating the majority of development on the ridgelines and mesas in the Precise Plan area. Grading of these ridgelines will be permitted to accommodate residential lots and drives, as well as local streets and Community Plan designated roads.
- Manufactured slope gradients should be variable, but in no case shall the gradient exceed a 2:1 horizontal to vertical relationship; provided, however, that slopes less than ten feet (10') high shall not exceed a gradient of 1 1/2:1, subject to applicable City ordinances.

DEVELOPMENT IMPACTS to SLOPES 25% & GREATER



-  DEVELOPMENT IMPACTS TO SLOPES 25% AND GREATER
-  EXISTING SLOPES 25% AND GREATER

PRECISE

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

FIGURE 16



- Manufactured slopes (either internal or external to residential development areas) which are readily visible from Carmel Mountain Road, Carmel Country Road, Street A, or Los Peñasquitos Canyon Preserve that are over ten feet (10') in height shall be contour graded, as feasible. Manufactured slopes located internal to residential development areas within the Precise Plan area which are not readily visible from Carmel Mountain Road, Carmel Country Road, Street A or Los Peñasquitos Canyon Preserve and manufactured slopes that are ten feet (10') or less in height shall not be required to be contour graded. Harsh, easily eroded forms and high, steep banks shall be avoided in favor of smooth, rounded ground forms that blend into the natural topography. Slopes may be contour or landform graded both vertically and horizontally to create undulating, natural-appearing slopes. In addition, gradients on manufactured slopes greater than ten feet (10') in height should typically vary, on average, between 2:1 and 4:1 gradients.
- In those instances where manufactured slopes exceed ten feet (10') in height, a detailed landscape and maintenance plan shall accompany future improvement plans. These plans shall ensure natural-appearing slope coverage by vegetation within a practicable time frame, as recommended by the landscape architect and approved by the Park and Recreation and Development Services Department.
- All manufactured slopes adjacent to natural open space or in areas with high community or neighborhood visibility shall be contour graded to produce a natural appearance and shall be blended to meet the native terrain.
- Transitional slopes should be planted to enhance the blending between manufactured and natural slopes.
- Designated open space and sensitive resource areas shall be protected from encroachment by sensitive grading techniques, such as contour grading.
- Hillside stability (both during and after construction) should be assured by recognizing soil characteristics, hydrology, and the steepness of the terrain. The project should incorporate measures to assure hillside stability as appropriate.
- Grading should be limited to the dry months or special construction methods should be utilized to minimize erosion and siltation problems during construction.
- No off-site grading shall be permitted in Los Peñasquitos Canyon Preserve.
- Landscaping plans should not require excessive irrigation needs.
- All manufactured slopes shall be planted with erosion control, fire resistant, and self-sufficient plantings.

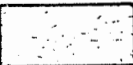
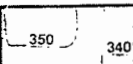

- Those portions of the site which are impacted by development should be replanted with self-sufficient trees, shrubs, and groundcover that is compatible with existing surrounding vegetation.
- All manufactured slopes shall be landscaped and irrigated to ensure adequate plant establishment and slope stability, reduce erosion, and enhance their visual appearance.
- Existing vegetation which is to remain on-site is encouraged to be incorporated into public common areas and manufactured slopes which abut natural open space. Indigenous plant materials should be preserved in these areas and supplemented by drought-tolerant and fire-resistant plant materials, provided such plantings comply with the guidelines issued by the City of San Diego Fire Department for brush management.
- Surface water crossing slope banks shall be reduced by providing drainage swales at the top of the banks.
- Development encroachment into slope areas of 25 percent or greater is restricted to the areas defined in the Grading Plan in this Precise Plan (see Figure 17).
- Encroachments into slopes of 25 percent and greater and into areas which possess environmentally sensitive habitats shall be required to conform to the Neighborhood 10 Precise Plan Land Use Plan which has been deemed by the City of San Diego as being consistent with San Diego City Council Policy 600-40.
- The scale and character of the residential buildings on-site should match the scale and character of the terrain and surrounding development. To avoid a monotonous appearance, however, residential building heights, colors and floor plans should be varied occasionally.
- Applicants for coastal development permits for projects located in the watershed of Los Peñasquitos Lagoon shall, in addition to meeting all other requirements of the North City segment of the City of San Diego Local Coastal Program, contribute a Los Peñasquitos watershed restoration and enhancement fee to the Los Peñasquitos Lagoon Fund for restoration of the Los Peñasquitos Lagoon and its watershed. This fee is calculated based on newly graded areas and creation of impervious areas which occur within the identified watershed boundary of Los Peñasquitos Lagoon.
- Grading shall be cut to daylight lines along canyon rims in order to minimize fills on canyon slopes.
- For areas within the Coastal Zone, development shall comply with the Coastal Zone Regulations of the HR (Hillside Review) Overlay Zone in effect on February 9, 1995, and steep slope/open space areas remaining undisturbed shall be conveyed to the City of San Diego as permanent open space.

VI. DESIGN GUIDELINES

CARMEL VALLEY - NEIGHBORHOOD 10 PRECISE PLAN

GRADING PLAN



-  NATURAL CONTOURS
-  REVISED CONTOURS
-  MANUFACTURED SLOPES

PRECISE

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

FIGURE 17



T&B Planning Consultants
3242 HALLADAY, SUITE 100
SANTA ANA, CALIF. 92705 (714) 662-2774
5879 OBERLIN DRIVE, SUITE 208
SAN DIEGO, CALIF. 92121 (619) 546-8366





- For areas within the Coastal Zone, development shall comply with the Erosion Control Measures for North City Areas Draining into Los Peñasquitos or San Dieguito Lagoons, as required by Section 62.0419 of the Municipal Code and City Clerk Document No. 00-17068 in effect on February 9, 1995 (see Appendix 'B').
- To the extent applicable, all new development within the Coastal Zone shall be designed to be consistent with multi-species and multi-habitat preservation goals and requirements as established in the statewide Natural Communities Conservation Planning (NCCP) Program, shall comply with the City of San Diego MSCP Interim Habitat Loss Permit Process, or shall obtain an incidental take permit under Section 4d, Section 7, or Section 10a of the Endangered Species Act related to the California gnatcatcher. Compliance with these goals and requirements shall be implemented in consultation with the United States Fish and Wildlife Service and California Department of Fish and Game.

These concepts and objectives have been reflected in the grading approach for Neighborhood 10 (See Figure 17, *Grading Plan*). Major canyon slopes and natural vegetation are retained in their natural state to the maximum extent feasible. Within development areas, cutting generally occurs along the ridges and higher elevations.

In developing the grading plan for the Neighborhood 10 Precise Plan area, there are two types of typical slope conditions: concave conditions and straight conditions. The locations of these slopes are illustrated in Figure 18, *Slope Treatment Map*. Almost all of the on-site manufactured slopes exceeding ten (10) feet in height shall be contoured graded in a fashion similar to the slope depicted in Figure 19, *Typical Slope Treatment - Concave Condition*. Figure 20 illustrates a typical slope treatment for convex slopes. Although contour grading is strongly encouraged, in a few select cases in Neighborhood 10, straight uncontoured manufactured slopes are planned, typically in areas where the slopes are not visible from public roads (see Figure 21, *Typical Slope Treatment - Straight Condition*).

The southeastern portion of the property is within the Coastal Zone and is adjacent to the Los Peñasquitos Canyon Preserve. As noted in Figure 2, *1975 Adopted Land Use Plan*, only a minor portion of this area is proposed for development and is consistent with the criteria of the Local Coastal Plan.

SLOPE TREATMENT MAP

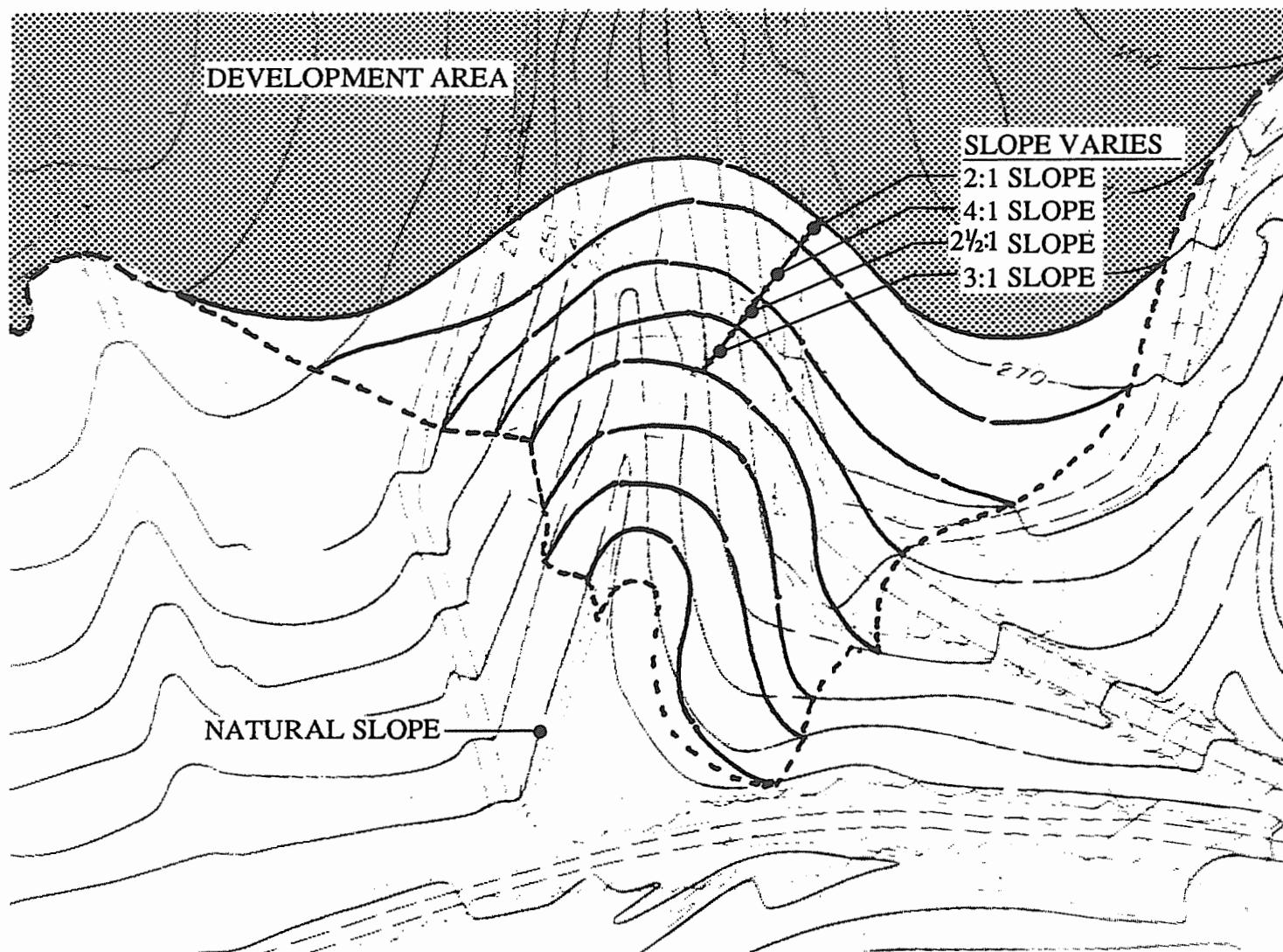
-  REQUIRES UNDULATION AND VARIABLE SLOPE RATIO
-  STANDARD 2:1 SLOPE



NOTE: SLOPES NOT IDENTIFIED ABOVE ARE VISUALLY NOT SIGNIFICANT OR ARE VISIBLE FROM WITHIN THE PROJECT ONLY.

PRECISE

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10



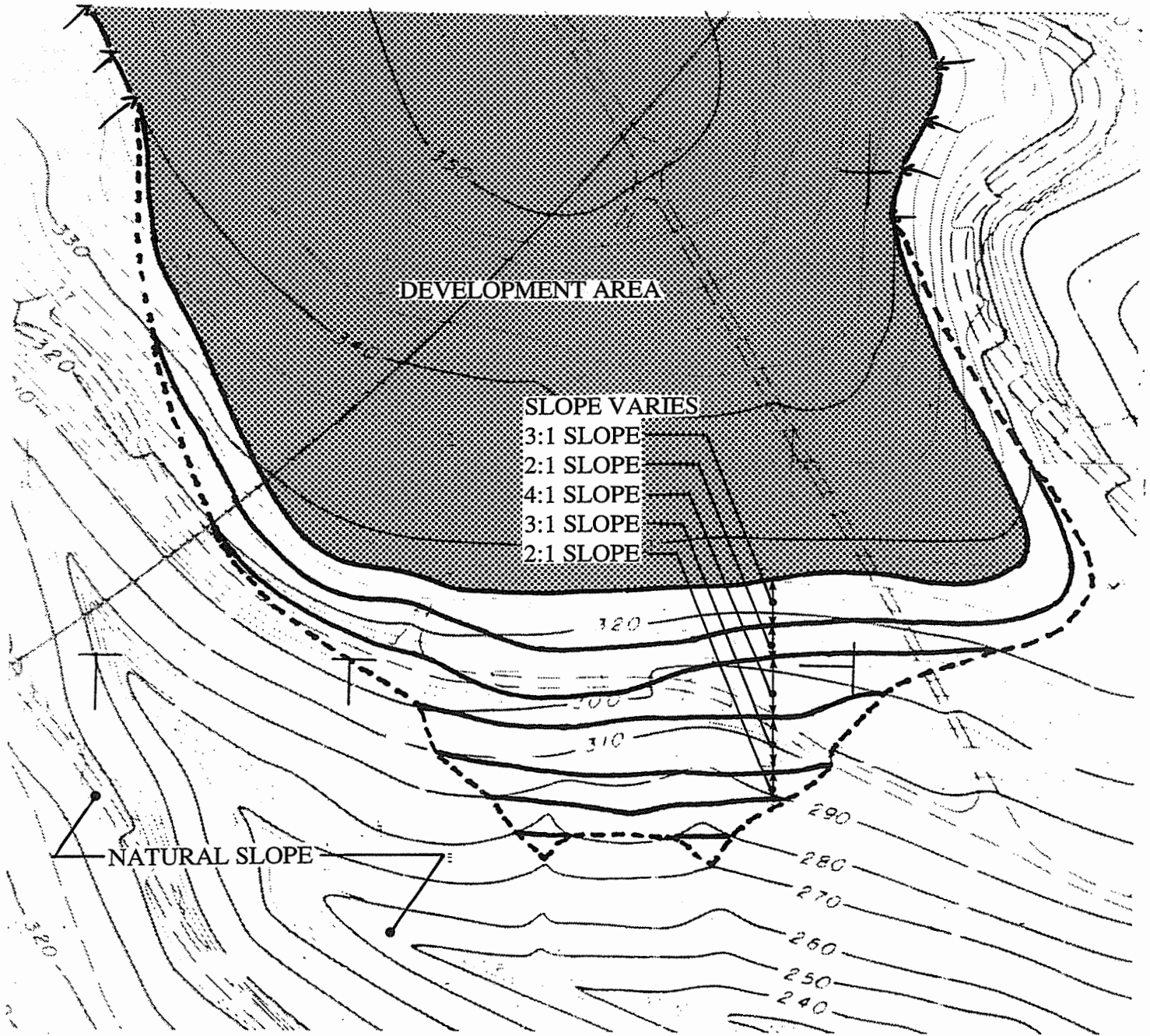
Note: This graphic depicts a typical slope condition. The actual grading, design and configuration of the slope will vary from this typical condition depending upon actual field conditions. Final design of slope will be determined at the Final Map stage.

TYPICAL SLOPE TREATMENT - *CONCAVE CONDITION* ***PRECISE***

P • L • A • N ■ ***CARMEL VALLEY/NEIGHBORHOOD 10***

FIGURE 19





Note: This graphic depicts a typical slope condition. The actual grading, design and configuration of the slope will vary from this typical condition depending upon actual field conditions. Final design of slope will be determined at the Final Map stage

TYPICAL SLOPE TREATMENT - CONVEX CONDITION

PRECISE

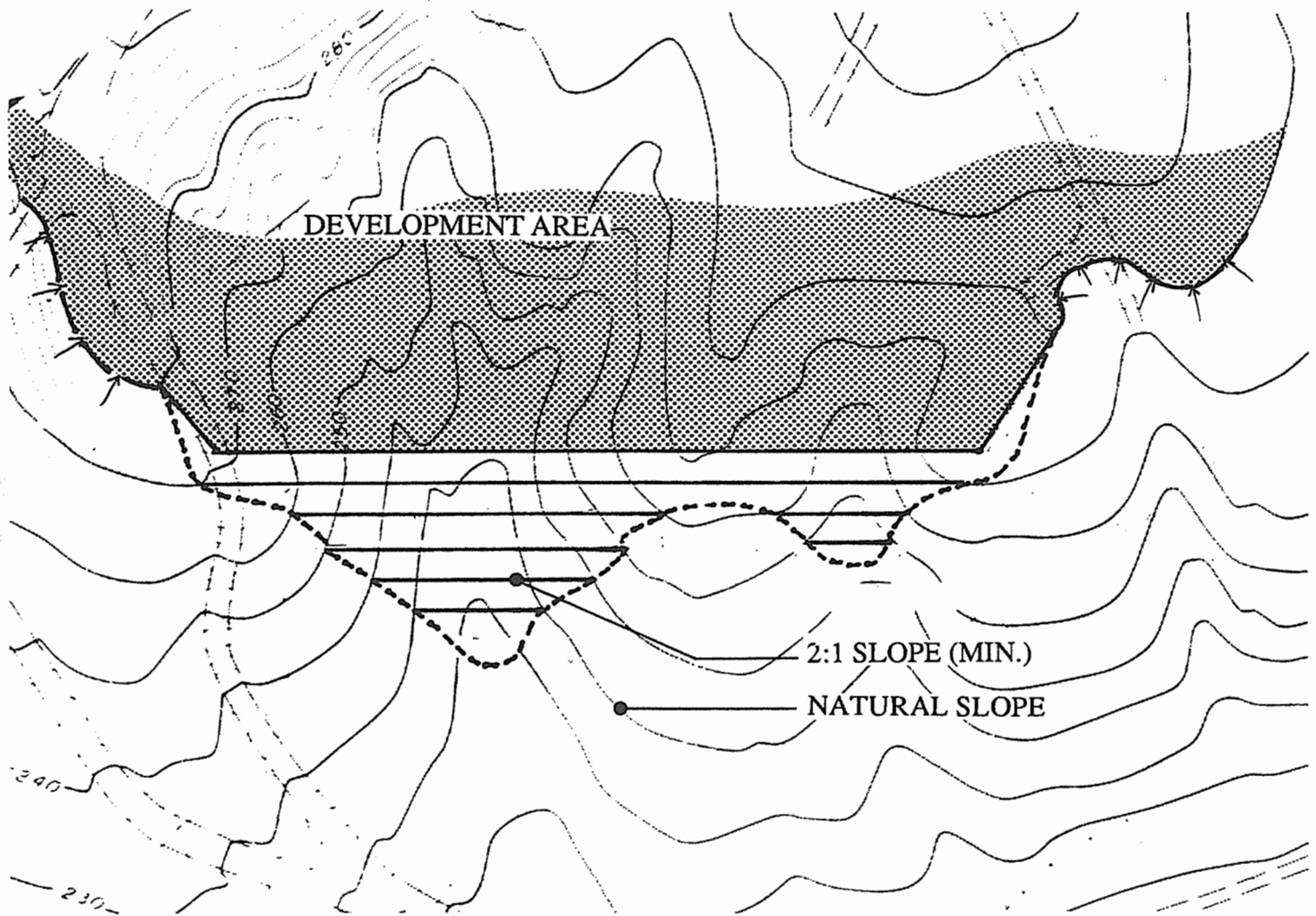
P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

FIGURE 20



T&B Planning Consultants
3242 HALLADAY, SUITE 100
SANTA ANA, CALIF. 92705 (714) 662-2774
5879 OBERLIN DRIVE, SUITE 200
SAN DIEGO, CALIF. 92121 (619) 546-8366





Note: This graphic depicts a typical slope condition. The actual grading, design and configuration of the slope will vary from this typical condition depending upon actual field conditions. Final design of slope will be determined at the Final Map stage

TYPICAL SLOPE TREATMENT - STRAIGHT CONDITION **PRECISE**

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

FIGURE 21



T&B Planning Consultants
3242 HALLADAY, SUITE 100
SANTA ANA, CALIF. 92705 (714) 662-2774
5879 OBERLIN DRIVE, SUITE 208
SAN DIEGO, CALIF. 92121 (619) 546-8366



D. INTERFACE WITH NATURAL OPEN SPACE CORRIDORS

Los Peñasquitos Canyon Preserve is a major open space resource within the City of San Diego which is located to the south of the Planning Area. Shaw Valley is another open space corridor to the north of the Planning Area. As such, it is necessary that adjoining development be sensitive to its impact on these open space corridors. It is important that development of the lots abutting the open space do not create a "wall" of buildings and fences. However, access to the open space canyon areas, especially into Los Peñasquitos Canyon, should be restricted except at locations approved by the Park and Recreation Department.

■ ANALYSIS OF VISUAL IMPACTS

Because of the distance between Neighborhood 10 development and the open space corridors, development visibility from the canyons to the Precise Plan area will be minimal. Figure 22, *Shaw Valley Cross-Sections Key Map*, and Figures 23A, 23B, and 23C, *Shaw Valley Cross-Sections*, illustrate that project development will not be visible from the Shaw Valley open space corridor. In addition, implementation of the Land Use Plan for Neighborhood 10 will result in only minimal visual impacts to the Los Peñasquitos Canyon Preserve. The planned housing in the Precise Plan area will not be visible from the bottom of the canyon. However, sensitive fence design is still important as it may be visible from other development within Neighborhood 10.

The following is a list of specific design guidelines to help mitigate potential visual impacts from these open space corridors.

■ RIDGE TOP DEVELOPMENT GUIDELINES

- All buildings shall not exceed thirty-five feet (35') in height.
- Buildings along the open space corridors shall be set back a minimum of thirty feet (30') from the top of slope or as required for brush management.
- Fencing along property boundaries facing the open space corridors shall be designed and constructed of materials that are compatible with the open space corridors, and shall be installed by the developer prior to the occupancy of the units in order to ensure uniformity. For additional guidance, refer to Section VI.G, Fencing.
- Fences and walls shall not exceed six feet (6') in height; provided, however, that retaining walls only may exceed six feet (6') in height if a discretionary permit is obtained from the City and the wall(s) comply(ies) with the City's Municipal Code Ordinance, Section 101.0615, *Fences*.

SHAW VALLEY CROSS SECTION INDEX MAP

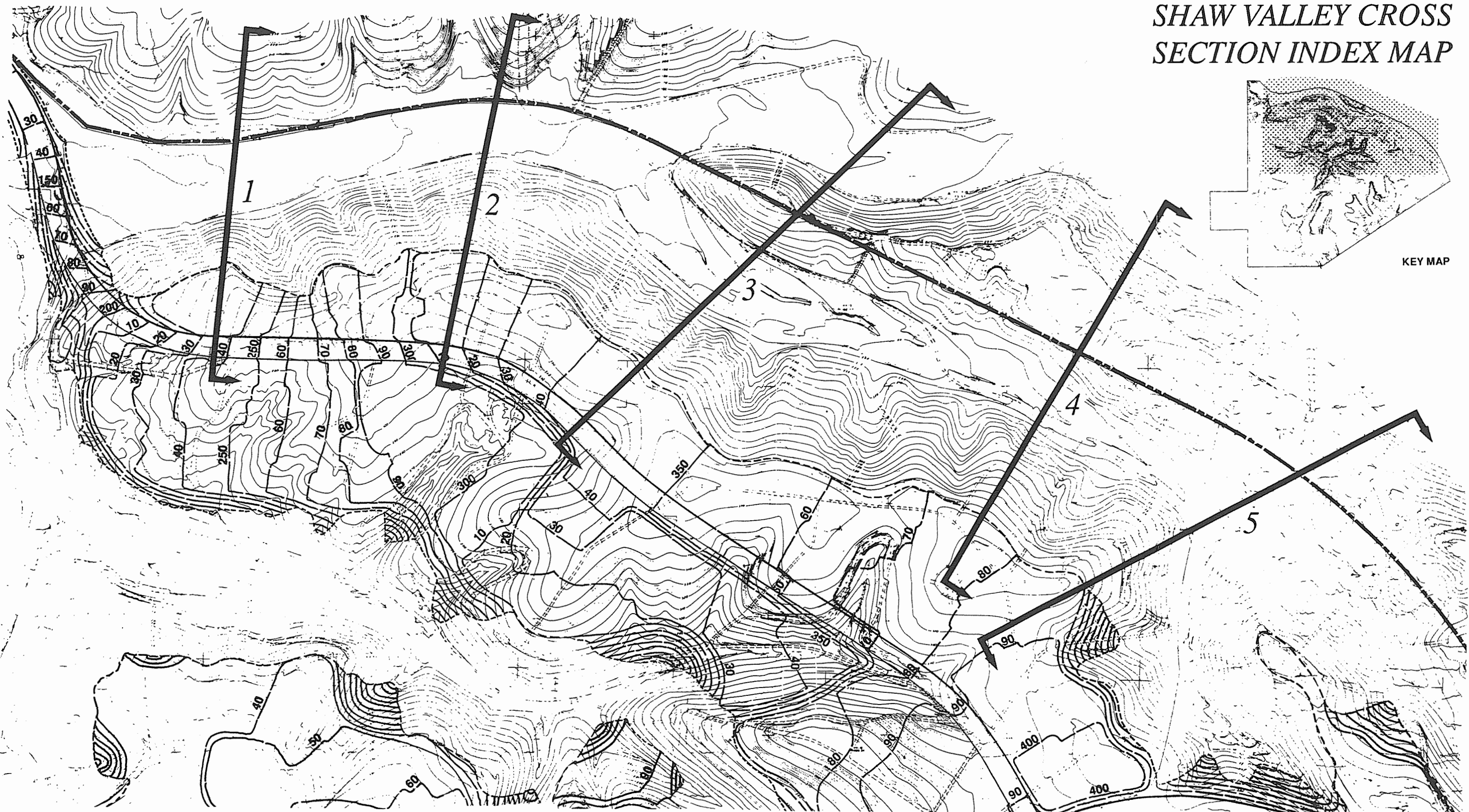
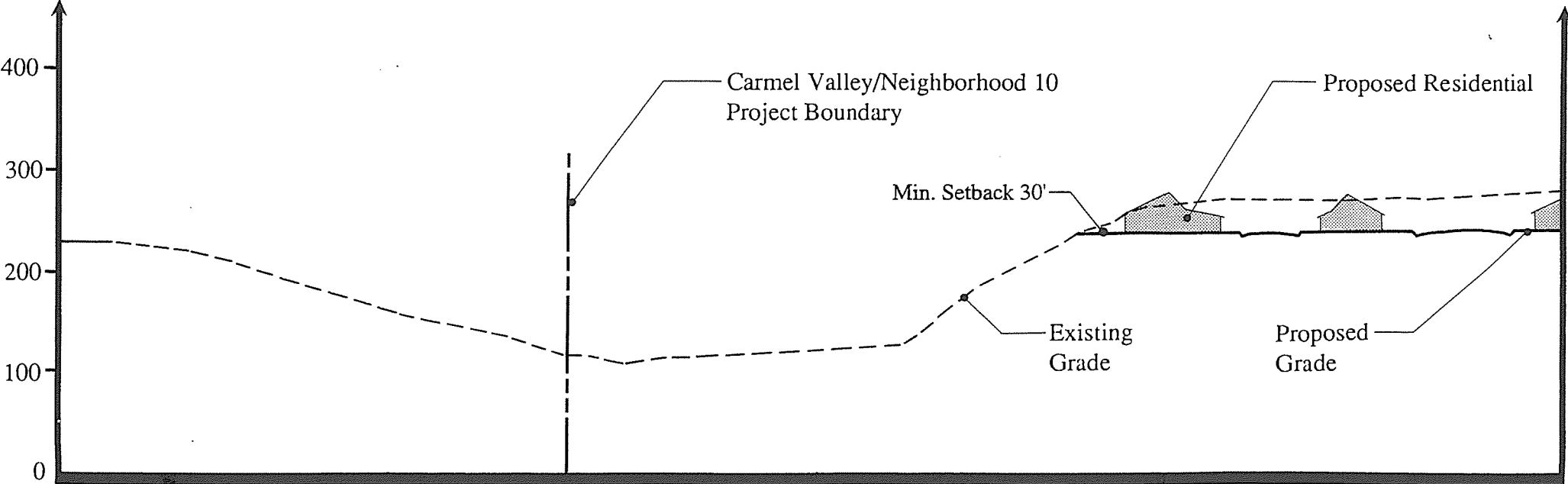
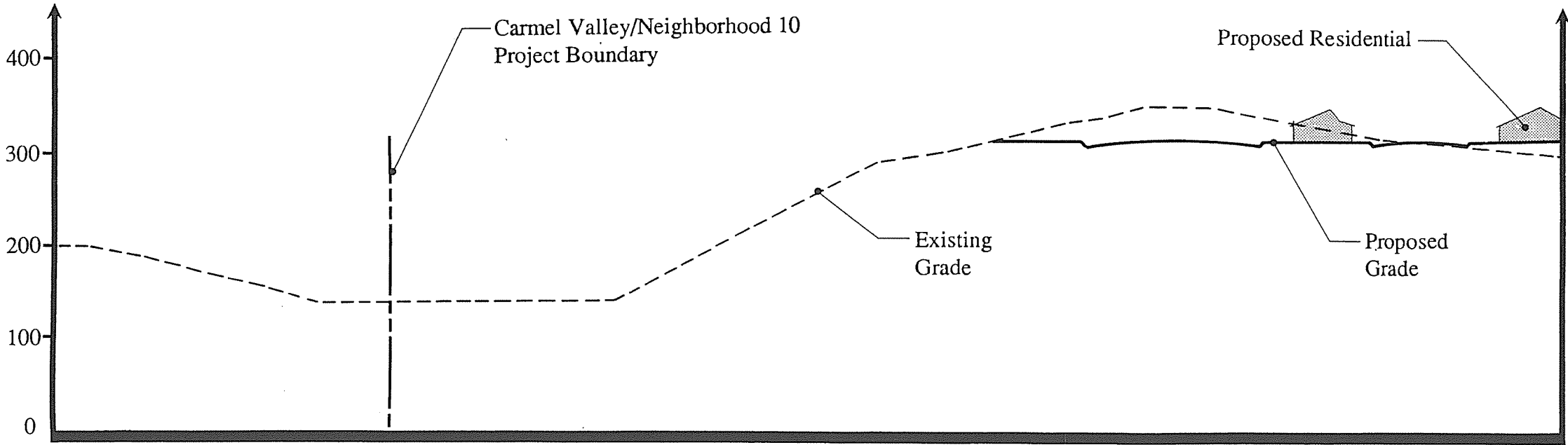


FIGURE 22



SECTION - 1



SECTION - 2

Note:
These cross-sections are not intended to indicate that the homes planned for construction in Neighborhood 10 are not visible from Shaw Valley. However, views of dwelling units on mesa top from Shaw Valley are expected to be minimal.

PRECISE

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

Note: For Approximate Locations of Cross sections Refer to Figure 23.

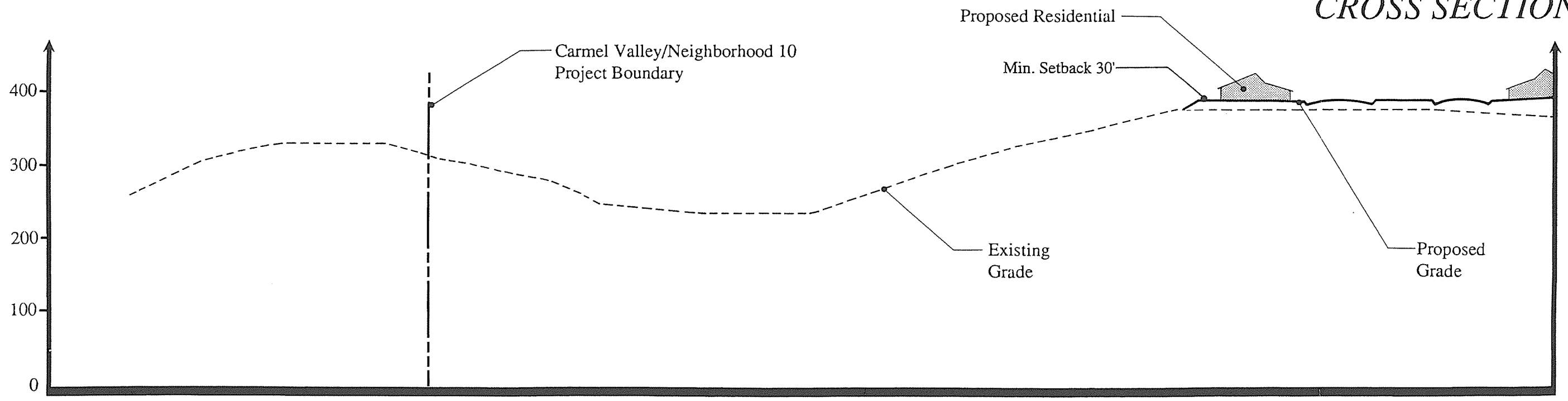


P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

Note: For Approximate Locations of Cross sections Refer to Figure 23.

T&B Planning Consultants
3242 MALLADAY, SUITE 100
SANTA ANA, CALIF. 92705 (714) 662-2774
5879 OBERLIN DRIVE, SUITE 208
SAN DIEGO, CALIF. 92121 (619) 546-8366

SHAW VALLEY CROSS SECTION 5



SECTION - 5

Note:
These cross-sections are not intended to indicate that the homes planned for construction in Neighborhood 10 are not visible from Shaw Valley. However, views of dwelling units on mesa top from Shaw Valley are expected to be minimal.

Note: For Approximate Location of Cross section Refer to Figure 23.

- Manufactured slopes abutting the open space corridors shall strictly adhere to the grading, landscaping and brush management objectives outlined in this Precise Plan.
- A discussion of Brush Management techniques for fire hazards is further discussed in this Precise Plan.

E. LANDSCAPE DESIGN

▣ OVERALL OBJECTIVES

A conceptual landscape plan is depicted in Figure 24, *Landscape / Brush Management Plan*, for easy reference.

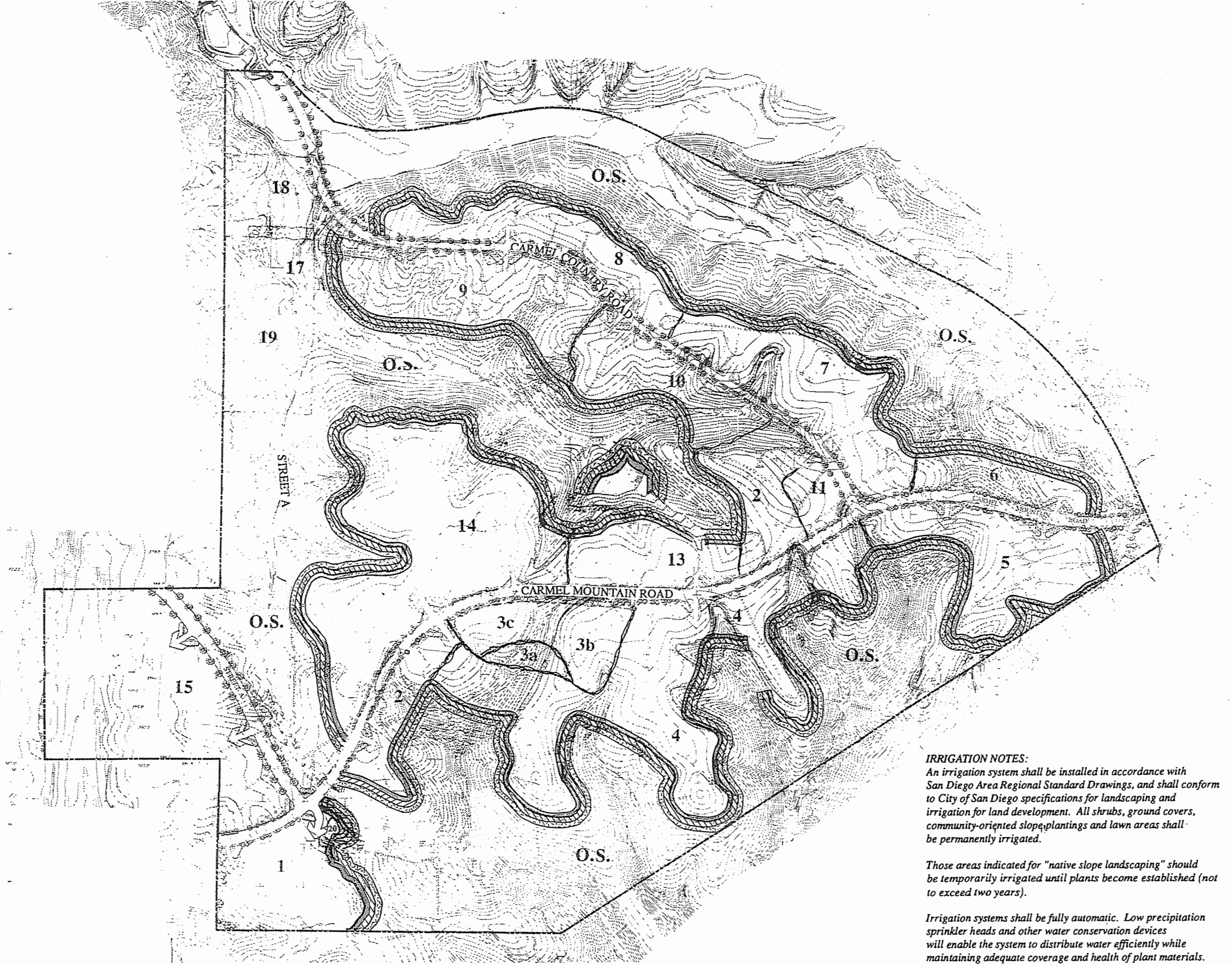
The overall landscape design concept is based on the following objectives:

- Require low maintenance, drought-tolerant plant material.
- Require visually appropriate plant material (e.g., indigenous species next to natural open space).
- Utilize plant material to create unit identity while maintaining a unified theme throughout Neighborhood 10.
- Accent desirable elements and delineate entries using landscaping as an objective.
- Provide transitional landscaping at the interface between graded and natural areas.
- Transitional plant materials should be native and/or drought-tolerant species which require low maintenance, are fire retardant and are effective in erosion control.

All plant materials to be utilized in public areas and on graded slopes shall be subject to review and approval by the San Diego Parks and Recreation Department, Open Space Division and the Development Services Department.

In order to insure the proper integration of landscape treatment within Neighborhood 10, separate landscape standards are established for different landscape areas: (1) landscaping of streetscapes; (2) landscape treatment of manufactured slopes; and (3) Brush Management Program. The landscaped areas for streetscapes should be consistent with

LANDSCAPE / BRUSH MANAGEMENT PLAN



- ALEPPO PINE, BRISBANE BOX (PARKWAY OF CARMEL MOUNTAIN ROAD)
- LONDON PLANE TREE, TORREY PINE, HOLLY OAK (PARKWAY OF CARMEL COUNTRY ROAD)
- GROUNDCOVER (MANUFACTURED SLOPES)
- NEIGHBORHOOD ENTRY
- SCHOOL / PARK ENTRY
- COMMERCIAL ENTRY

BRUSH MANAGEMENT LEGEND

- ZONE 1 - 30' - 40'
- ZONE 2 - 40'
- ZONE 3 - 30' - 40'

IRRIGATION NOTES:
 An irrigation system shall be installed in accordance with San Diego Area Regional Standard Drawings, and shall conform to City of San Diego specifications for landscaping and irrigation for land development. All shrubs, ground covers, community-oriented slope plantings and lawn areas shall be permanently irrigated.

Those areas indicated for "native slope landscaping" should be temporarily irrigated until plants become established (not to exceed two years).

Irrigation systems shall be fully automatic. Low precipitation sprinkler heads and other water conservation devices will enable the system to distribute water efficiently while maintaining adequate coverage and health of plant materials.

PRECISE

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

FIGURE 24



T&B Planning Consultants

3242 HALLADAY, SUITE 100
 SANTA ANA, CALIF. 92705 (714) 662-2774

5879 OBERLIN DRIVE, SUITE 208
 SAN DIEGO, CALIF. 92121 (619) 546-8366



the Recommendations for Street and Parkway Improvements Manual prepared for Carmel Valley Neighborhoods 2, 3, 4a, 7, and 9. In addition, all landscaped areas should be consistent with the City of San Diego's Landscape Technical Manual.

1. ROADWAY LANDSCAPE DESIGN

The rights-of-way and adjacent slopes for Carmel Country Road and Carmel Mountain Road should be designed similar to that of other community-oriented streets. Design solutions should visually edit out traffic and mitigate traffic noise to the extent feasible. Trees allowed in street right-of-ways are discussed below under *Street Trees*. Figures 13 and 14, *Roadway Cross-Sections*, illustrate typical roadway landscape sections for streets in the Precise Plan area.

The following streetscape guidelines shall be applied to future development plans for Neighborhood 10:

- Low maintenance, drought-tolerant plant material should be used within the public right-of-way.
- Medians should consist of trees, low shrubs and ground cover. (Per City of San Diego Landscape Guidelines.)
- Vines and/or espaliers should be planted periodically along the length of walls and fences that front on Carmel Mountain Road, Carmel Country Road, or Street "A" to provide visual interest and to visually soften the wall/fence surfaces.
- Signage within Neighborhood 10 shall comply with the Carmel Valley Sign Program. If this program is not in place at the time the first tentative map is processed, an interim signage plan shall be approved for the neighborhood in association with the processing of the first tentative map and development plan.
- All walls and fencing visible from a public street shall comply with the wall and fencing guidelines outlined in this plan.

a. STREET TREES

■ Carmel Mountain Road: Suggested Plant Palette

● Median:

Liquidambar styraciflua
Cupania anacardioides
Eucalyptus ficifolia

Sweet Gum
 Carrot Wood
 Red-Flowering Gum

VI. DESIGN GUIDELINES

CARMEL VALLEY - NEIGHBORHOOD 10 PRECISE PLAN

- Parkway:

Pinus halepensis
Tristania conferta

Aleppo Pine
Brisbane Box

■ **Carmel Country Road: Suggested Plant Palette**

- Median:

Same as Carmel Mountain Road

- Parkway:

Platanus acerifolia
Pinus torreyana
Quercus ilex

London Plane Tree
Torrey Pine
Holly Oak

- Local Streets: Suggested Plant Palette

Pinus canariensis
Quercus ilex

Canary Island Pine
Holly Oak

- Accent Trees: Suggested Plant Palette

Albizzia julibrissin
Arbutus unedo
Bauhinia blakeana
Chorisia speciosa
Jacaranda acutifolia
Magnolia grandiflora
Pyrus 'Bradfordi'

Mimosa Tree
Strawberry Tree
Hong Kong Orchid Tree
Floss Silk Tree
N.C.N.
Southern Magnolia
Bradford Pear

b. SHRUBS: Suggested Plant Palette

Carissa grandiflora
Cistus species
Cotoneaster parneyii
Grevillea 'Noellii'
Heteromeles arbutifolia
Hypericum calycinum
Prunus illicifolia
Prunus lyonii
Raphiolepis indica
Rhamnus californica
Rhus integrifolia

Natal Plum
Rockrose
Red Clusterberry
N.C.N.
Toyon
Aaron's Beard
Hollyleaf Cherry
Catalina Cherry
Raphiolepis
Coffeeberry
Lemonade Berry

VI. DESIGN GUIDELINES

CARMEL VALLEY - NEIGHBORHOOD 10 PRECISE PLAN

Rhus laurina
Dodonea viscosa
Escallonia
Nerium oleander
Photinia fraseri
Pittosporum
Xylosma congestum
Bougainvillea

Laurel Sumac
 Hopseed Bush
 N.C.N.
 N.C.N.
 N.C.N.
 Mock Orange
 Xylosma
 N.C.N.

- Ground Cover:

Delosperma 'Alba'
Lonicera japonica
Rosemarinus officinalis 'Prostratus'

White Trailing Ice Plant
 Japanese Honeysuckle
 Dwarf Rosemary

2. MANUFACTURED SLOPES

Manufactured slopes over ten feet (10'), and exposed to public view within Neighborhood 10, are required to be contour graded and landscaped with native and naturalized plant species. Naturalized species should be included for higher initial plant cover only in areas that do not abut natural open space, and should not inhibit or replace natives in the planting design. Manufactured slopes adjacent to natural open space are identified on the Landscape Plan and will involve careful recontouring to create a transitional landform between the graded slopes and the natural topography. Plantings on these slopes shall reflect the natural vegetation and accommodate required brush management. A plant palette should be selected that will be visually compatible with the surrounding natural landscape and allow for transitional slope coverage, natural ecological succession and, ultimately, a mature climax plant development which integrates with the undisturbed natural areas. The plant list below reflects these objectives and identifies a list of plants from which a desired landscape palette for manufactured slopes can be chosen, however, invasive species will not be permitted on slopes adjacent to natural open space.

All manufactured slopes within open space areas that are planned for future ownership by the City of San Diego shall be planted with native plant materials, installed with temporary irrigation systems and maintained by the developer. The manufactured slopes will not be accepted into the City's inventory until the plants are established and the temporary irrigation system is removed.

All manufactured slopes shall be consistent with Section VI., Brush Management Program, and Section VII., Revegetation Guidelines, of the City of San Diego Landscape Technical Manual and, no plantings shall be done without prior approval of the City's Park and Recreation Department and the Development Services Department. The appropriate developer will be responsible for landscape maintenance for an initial two-year period or until accepted by the Park and Recreation Department, should it be determined the landscaping has not met the success criteria in that two-year period.

VI. DESIGN GUIDELINES

CARMEL VALLEY - NEIGHBORHOOD 10 PRECISE PLAN

a. TREES: Minimum 15 gallons.

Pinus halepensis
Tristania conferta

Aleppo Pine
Brisbane Box

b. SHRUBS:

Abelia grandiflora (Dwarf Var.)
Acacia cultriformis
Carissa grandiflora
Ceanothus griseus horixontalis
Cistus purpureus
Coprosma repens
Cotoneaster microphyllus
Cotoneaster lacteus
Dodonaea viscosa
Echium fastuosum
Elaeagnus pungens
Grevillea noelli
Heteromeles arbutifolia
Melaleuca nesophila
Myoporum laetum
Nerium oleander
Nerium oleander
Photinia fraseri
Prunus caroliniana

Glossy Abelia
Knife Acacia
Natal Plum
Carmel Creeper
Orchid Rockrose
Mirror Plant
Rockspray Cotoneaster
Parney Cotoneaster
Hopseed Bush
Pride of Madeira
Silver Berry
Grevillea
Toyon
Pink Melaleuca
Myoporum
Oleander
Dwarf Oleander
N.C.N.
Carolina Laurel

c. HYDROSEED MIX FOR TRANSITIONAL SLOPES:

Artemisia californica

Coastal Sagebrush

Encelia californica
Eriogonum fasciculatum
Eschscholzia californica
Heteromeles arbutifolia
Lotus scoparius
Lupinus bicolor
Lupinus succulentus
Mimulus puniceus
Plantago insularis
Rhus Laurina
Rhus integrefolia
Salvia apiana
Salvia mellifera

Bush Sunflower
California Buckwheat
California Poppy**
Toyon*
Deerweed
Pygmy-Leaved Lupine**
Arroyo Lupine**
Mission Red Monkeyflower
Plantain**
Laurel Sumac*
Lemonadeberry
White Sage
Black Sage

* Not to be applied in brush management areas.

** Nurse crops.

VI. DESIGN GUIDELINES

CARMEL VALLEY - NEIGHBORHOOD 10 PRECISE PLAN

A refined, manicured, smooth slope outline should be avoided by the use of plant species similar in appearance to those on adjacent natural slopes. Such an approach will create a sense of undulation and visual relief along the vertical planes consistent with the characteristics of the surrounding hillsides. See Figure 25, *Typical Landscaping on Manufactured Slopes*.

3. BRUSH MANAGEMENT PROGRAM

All development within Neighborhood 10 will comply with the Uniform Fire Code and Section 6, Brush Management of the City of San Diego's *Landscape Technical Manual* as approved by the City Council.

As stated in the City's Brush Management Program:

"Fire safety in the landscape is achieved by reducing the readily flammable fuel adjacent to structures. This can be accomplished by pruning and thinning of native plants, revegetation with low fuel volume plantings or a combination of the two. Implementing brush management in an environmentally appropriate manner requires a gradual reduction in the amount and continuity of highly flammable fuel while maintaining plant coverage for soil protection. Such a transition will minimize the visual, biological and erosion impacts while reducing the risks of wildfires."

In addition, the City's program discusses the creation of three "Brush Management Zones" which allow for a transition of flammable plant materials. These transitional zones provide a buffer of 50 to 110 feet between structures and undisturbed native vegetation.

The City's Brush Management Program further states that:

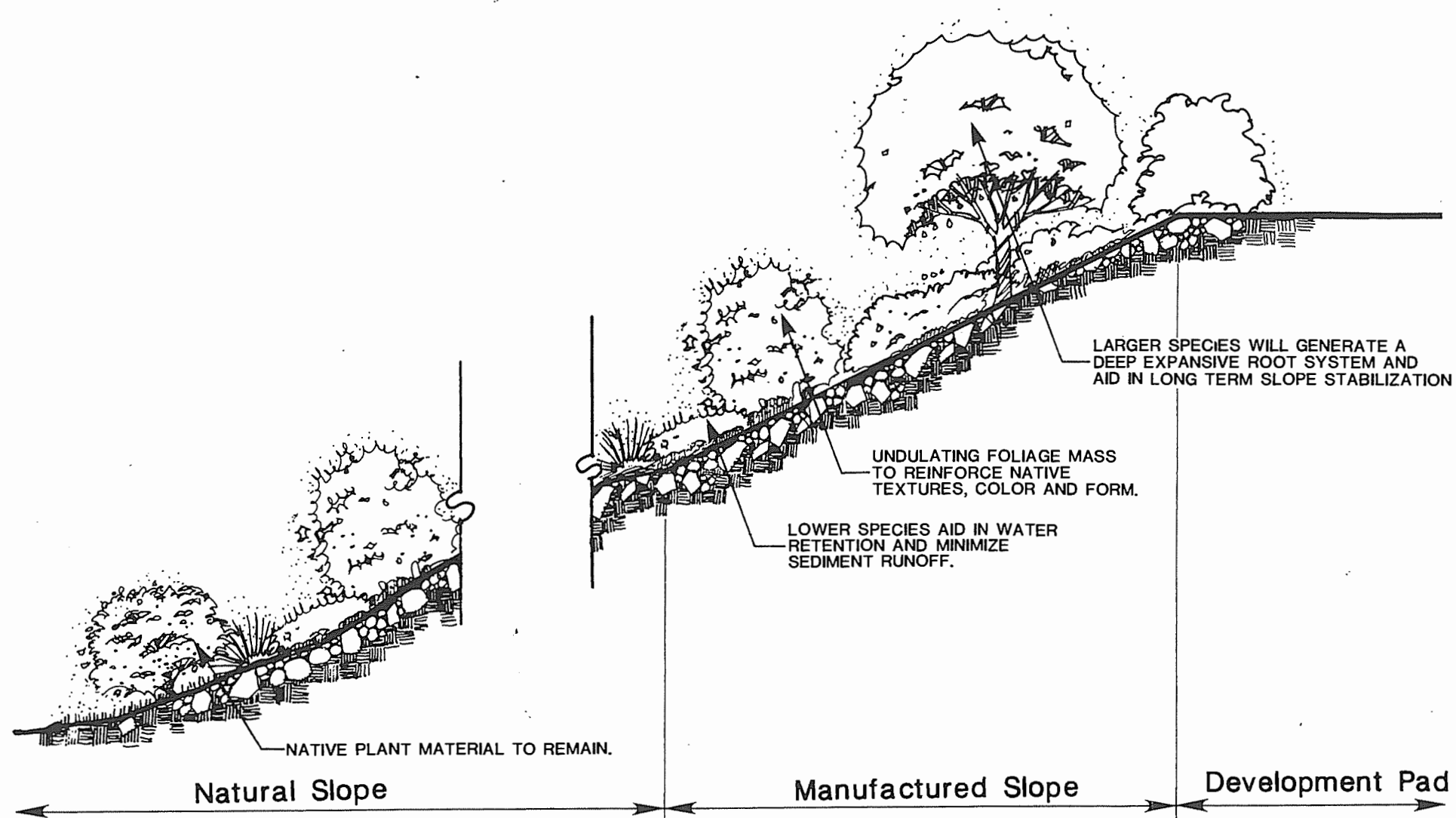
"Deviations from the guidelines and requirements of this program may be considered in environmentally sensitive areas and must be approved by the Fire Chief and the Planning Director. Approval by the Park and Recreation Director shall also be required for sensitive areas in public ownership."

The Brush Management Program proposed for Neighborhood 10 shall incorporate the three zones as stated above. Zone 1 shall be 30-40 feet (30 feet when architectural features use fire-resistive materials), Zone 2, shall be 40 feet and Zone 3 shall be 30-40 feet. It is anticipated that maintenance of property situated within Brush Management Zone 1 will be the responsibility of the private property owner of the lot(s). Those portions of the Precise Plan area that are located within Brush Management Zones 2 and 3 will be maintained by the Landscape Maintenance District or Homeowners Association. The three Brush Management Zones are discussed below and shall be prepared and maintained per the requirements of the City of San Diego Landscape Technical Manual, Brush Management Program, Section Six. (See Figure 24.)

VI. DESIGN GUIDELINES

CARMEL VALLEY - NEIGHBORHOOD 10 PRECISE PLAN

TYPICAL LANDSCAPING ON MANUFACTURED SLOPES



PRECISE

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

FIGURE 25

T&B Planning Consultants
3242 HALLADAY, SUITE 100
SANTA ANA, CALIF. 92705 (714) 662-2774
5879 OBERLIN DRIVE, SUITE 208
SAN DIEGO, CALIF. 92121 (619) 546-8366



☐ ZONE 1

Zone 1 consists of plantings adjacent to structures. While these plantings typically consist of irrigated, ornamental non-native species, native plants may also be used. When used, native plants should be able to survive with summer water. In the "High" Fire Hazard Severity Class, Zone 1 can be reduced from 40 feet to 30 feet through the use of fire-resistive architectural features and building design.

☐ ZONE 2

Zone 2 can be implemented in a variety of ways, the simplest being the selective thinning and pruning of the native plants. Long-term ongoing thinning cost may be reduced by the introduction of low growing fire retardant shrubs and ground covers that are visually and horticulturally compatible with the native vegetation. Zone 2 plantings can also be established in disturbed areas that have been cleared of native vegetation by replanting appropriate non-native plant materials. Plantings in Zone 2 may be established by hydroseeding, if desired.

● **REQUIREMENTS**

- 1) Individual non-irrigated plant groupings over 18 inches in height may be retained provided they do not exceed 400 square feet in area and their combined coverage does not exceed 30 percent of the total Zone 2 area.
- 2) Shrubs in new plantings shall have an average maximum mature height of 24 inches or less, except as noted above in Item "1)". Trees, large trees and form shrubs (e.g. Oaks, Sumac, Toyon) which are being retained shall be pruned to provide clearance of three times the height of the understory plant materials or six feet (6'), whichever is higher.
- 3) Irrigated plantings on slopes with a gradient of 4:1 shall satisfy the following performance criteria:
 - a) Spray heads (except micros) shall not be used on slopes above Zone 3. Overspray and runoff affecting Zone 3 shall be avoided.
 - b) Fifty to seventy percent of the area shall be planted with deep rooting (i.e. over 4 feet in average depth), spreading vines and shrubs with low fuel volume and low to moderate fire retardance.
 - c) Thirty to fifty percent of the area shall be planted with ground covers with moderate to high fire retardance.

- 4) Except as noted above, new non-irrigated plantings in Zone 2 shall satisfy the following performance criteria:
 - a) Plants shall have a low growing spreading habit.
 - b) Plants shall be self regenerating, drought-resistant and effective in erosion control and slope stabilization.
 - c) Plants shall be native in all areas adjacent to MSCP open space.
- 5) Non-native plans shall only be permitted in Zone 1 and on interior slopes not located adjacent to natural open space.

- **MAINTENANCE**

Seasonal maintenance in Zone 2 should include removal of dead woody plants, eradication of weedy species and periodic pruning and thinning of trees and shrubs. Native shrubs should be pruned in the fall after the major plant growth occurs. Well pruned healthy shrubs should typically require several years to build up excessive live and dead fuel. On slopes, all drainage devices must be kept clear. Reinspect after each major storm since minor soil slips can block drains. Various ground covers (e.g. ivy) should be periodically sheared and thatch removed (grasses and some ice plants). Diseased and dead wood should be pruned from trees. Fertilizing trees and shrubs is not typically recommended as this may stimulate excessive growth. Maintenance shall be done on a seasonal basis and shall be the responsibility of a landscape maintenance association or similar mechanism.

- **ZONE 3**

Zone 3 is the first line of defense for fire safety and involves the selective thinning and pruning of native vegetation in a way that preserves the natural appearance of the area while reducing the fuel load.

- **REQUIREMENTS**

- 1) Individual non-irrigated plant groupings over 18 inches in height may be retained provided they do not exceed 650 square feet in area.
- 2) The combined coverage of individual non-irrigated plant groupings over 18 inches in height may vary from 40 percent immediately adjacent to Zone 2 to 60 percent at the outer limits of Zone 3.
- 3) Except as noted above, new plantings shall satisfy the following performance criteria:
 - a) Plants shall have an average maximum mature height of 36 inches or less.

- b) Plants shall be effective in erosion control, slope stabilization and capable of self propagation without supplemental waterings.
- 4) Removal of highly flammable plants such as sages (white, black, purple), buckwheat, Chamise and California sagebrush shall be allowed only in conjunction with revegetation as stated in Item "3)" on the previous page.

- **EXCEPTIONS**

- 1) San Diego Barrel Cactus and Pygmy Spikemoss shall be left undisturbed by brush management thinning or pruning.

- **MAINTENANCE**

Ongoing selective thinning and pruning as previously discussed must be done to reduce the amount and continuity of fuel. Ideally, this thinning and pruning is accomplished incrementally each year by removing the oldest and most flammable vegetation while retaining the younger less flammable vegetation. Periodic maintenance shall be performed with yearly inspections.

Refer to the City of San Diego Landscape Technical Manual for specifications on: revegetation and slope planting, irrigation, design standards and installation standards.

F. SPECIAL EDGE CONDITIONS

The Neighborhood 10 Precise Plan has been developed with special care to insure compatibility between adjacent land uses. In particular, there are four conditions that warrant special edge treatments. These conditions are depicted in Figures 26 and 27, *Special Edge Conditions*.

- **SECTION A**

Section A in Figure 26 is concerned with the relationship of the community park site in development area 3 to the adjacent single family homes planned in development area 4. A minimum five (5) foot high wall, fence or wall/fence combination shall be required to protect the privacy of the residences which adjoin the park site.

- **SECTION B**

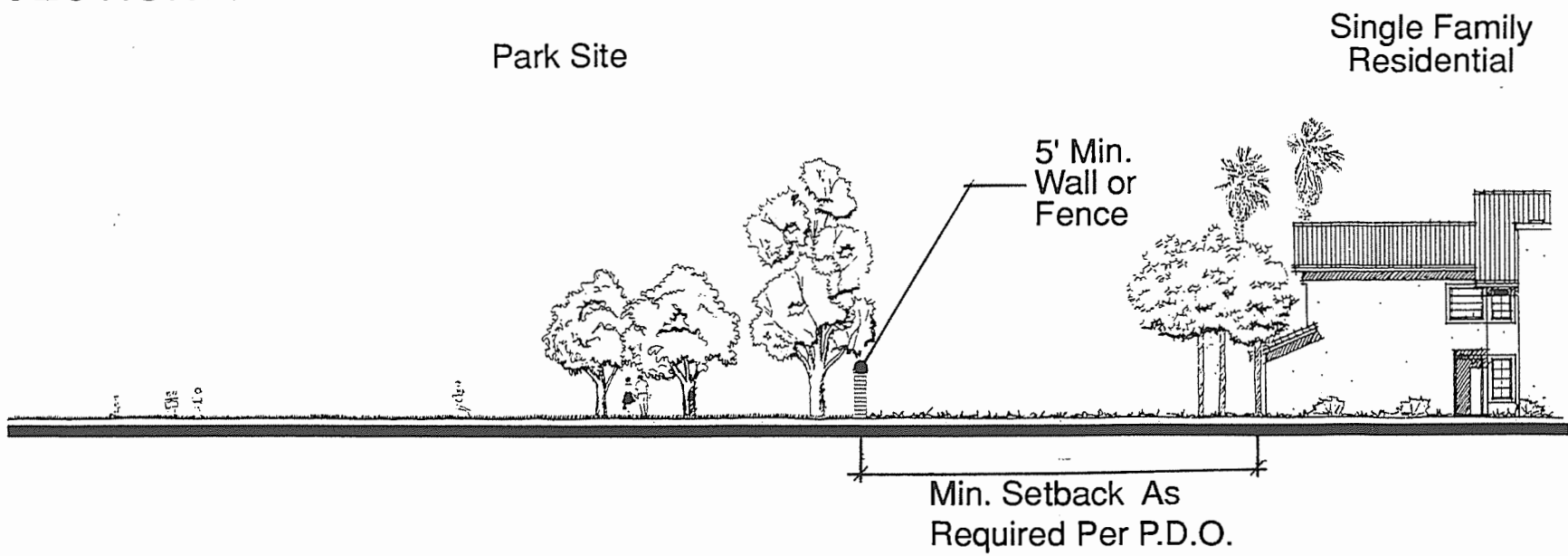
Section B depicts the relationship between the natural open space on either side of Carmel Mountain Road and the single family uses in development areas 1, 2, 14, and 15. At its narrowest juncture, adjacent to the road undercrossing at Carmel Mountain Road, the natural open space will have a minimum width of 200 feet. As the distance from Carmel Mountain Road increases, the open space corridor will widen and expand. A natural trail (shown in Figure 26, Section B) will traverse

VI. DESIGN GUIDELINES

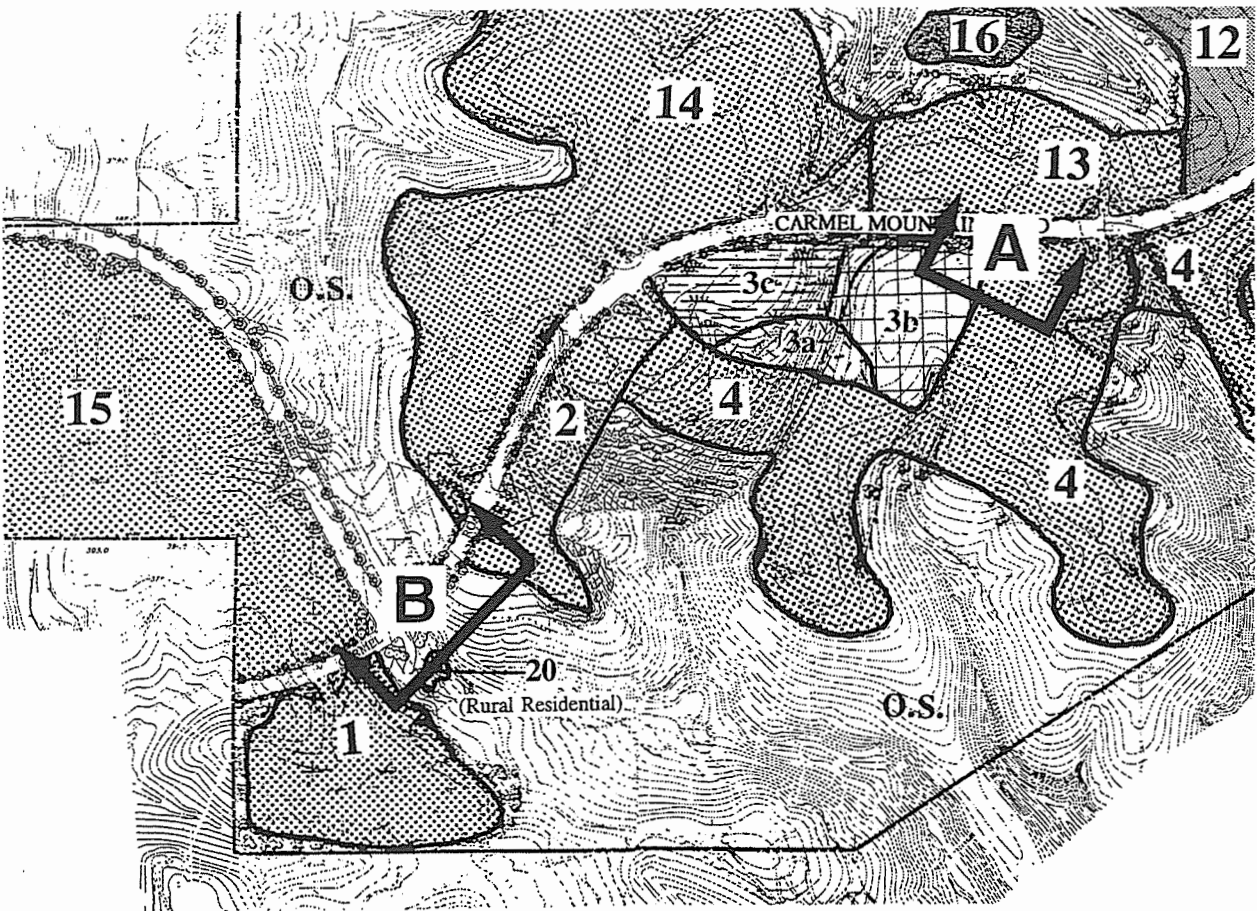
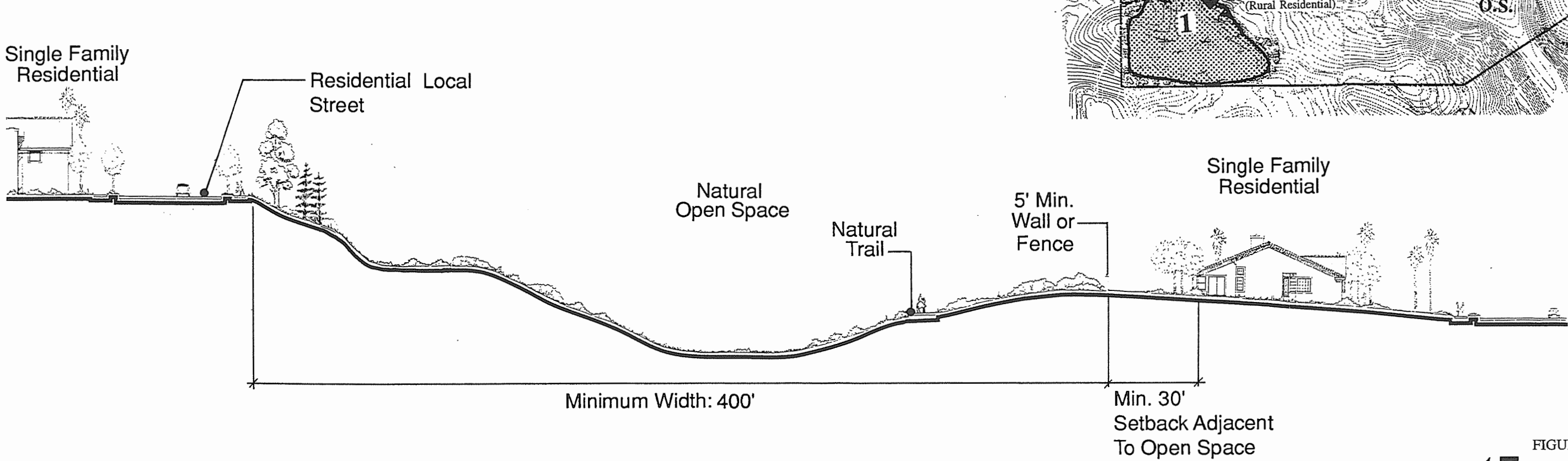
CARMEL VALLEY - NEIGHBORHOOD 10 PRECISE PLAN

SPECIAL EDGE CONDITIONS

SECTION A



SECTION B



PRECISE

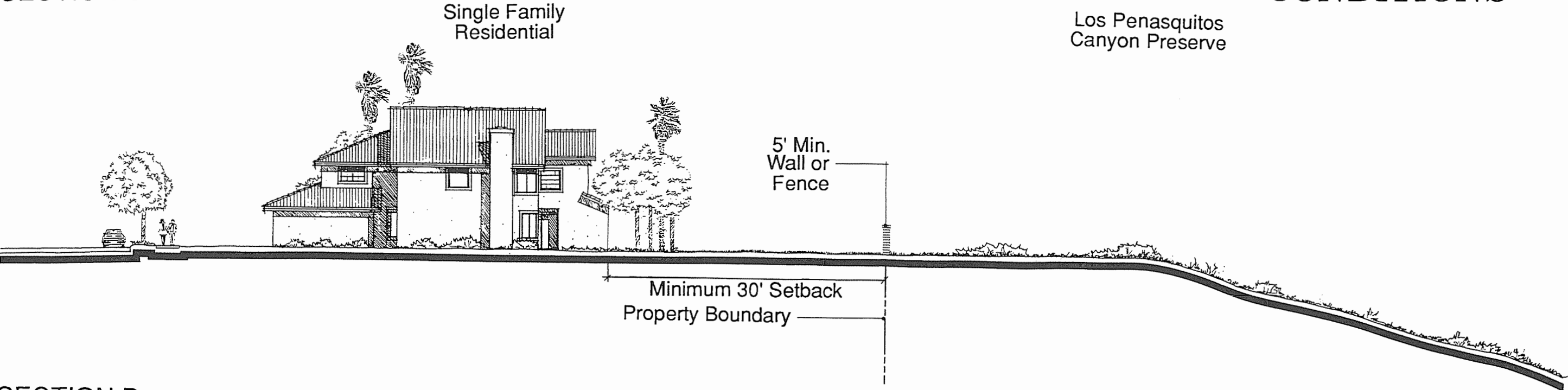
P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

FIGURE 26

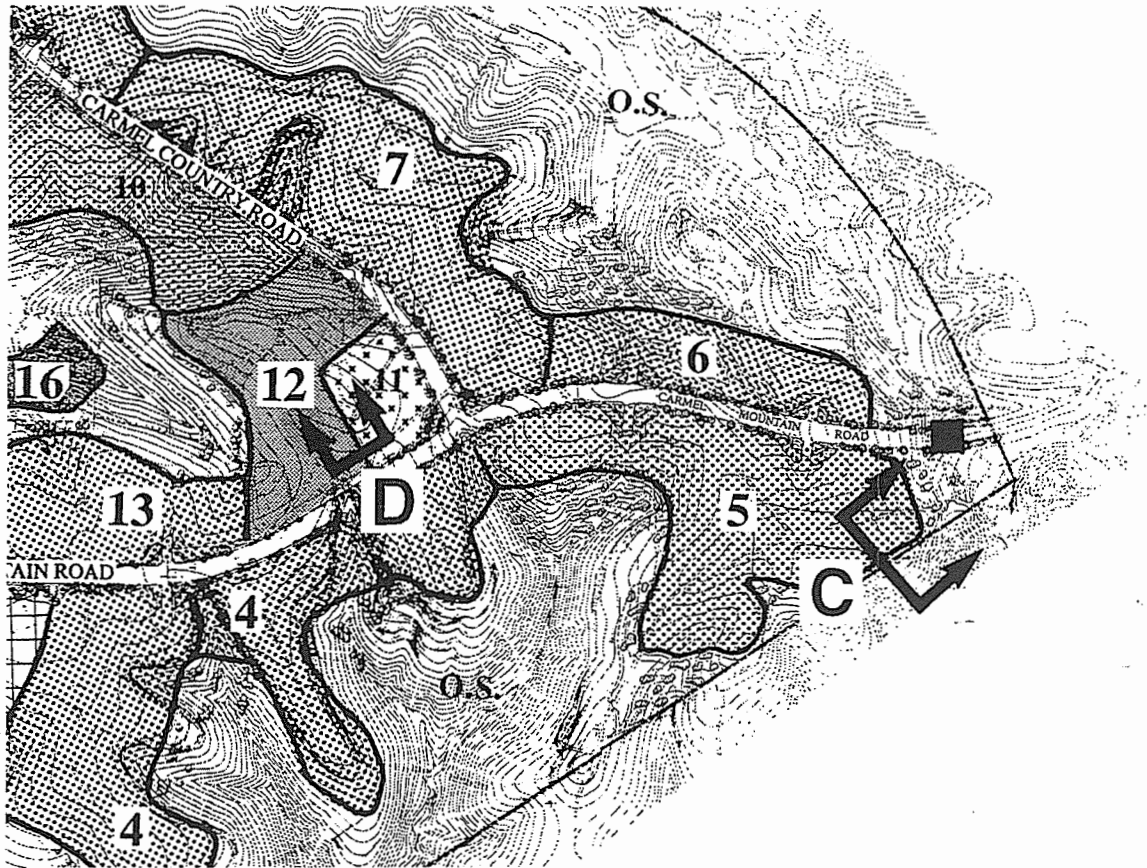
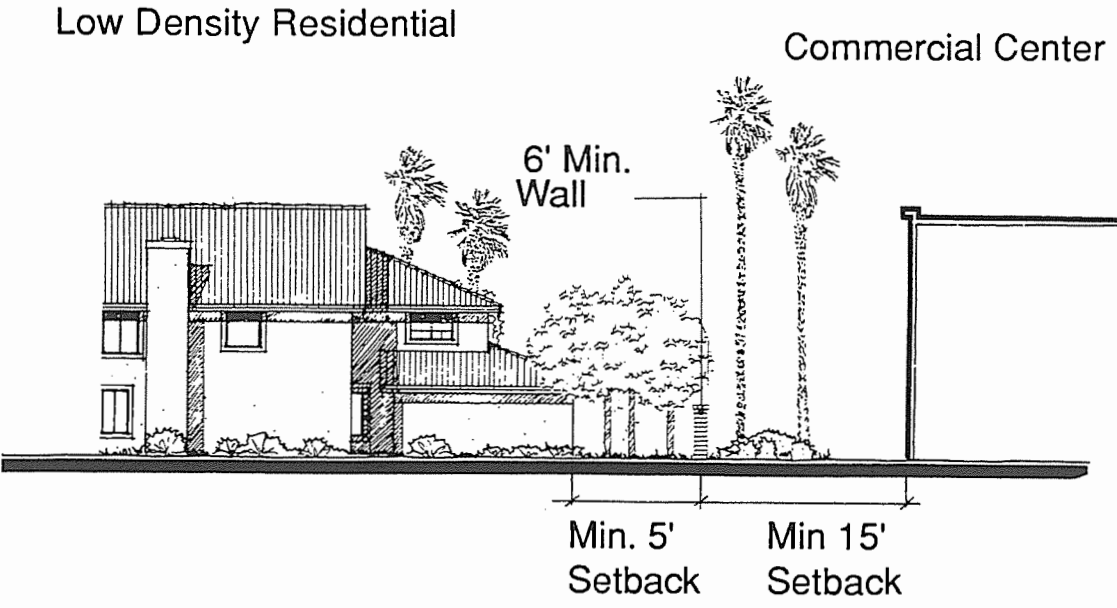
T&B Planning Consultants
3242 HALLADAY, SUITE 100
SANTA ANA, CALIF. 92705 (714) 662-2774
5879 OBERLIN DRIVE, SUITE 208
SAN DIEGO, CALIF. 92121 (619) 546-8366

Not To Scale

SECTION C



SECTION D



PRECISE

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

through this open space corridor and pass underneath Carmel Mountain Road through the road undercrossing. In those areas where the side or rear yards of residential lots abut the natural open space, a minimum five (5) foot high wall or fence shall be required to discourage intrusion into the open space by humans and domestic animals. A building setback of at least thirty feet (30') for homes adjacent to natural open space shall be required for fuel modification purposes.

▣ SECTION C

Figure 27 shows the relationship between single family homes and Los Peñasquitos Canyon Preserve in Section C. This edge condition will occur in development area 5 only. A minimum five (5) foot high wall, fence or wall/fence combination shall be erected on lots adjacent to Los Peñasquitos Canyon Preserve. In addition, a building setback of at least thirty feet (30') shall be required on these lots for fuel modification purposes.

▣ SECTION D

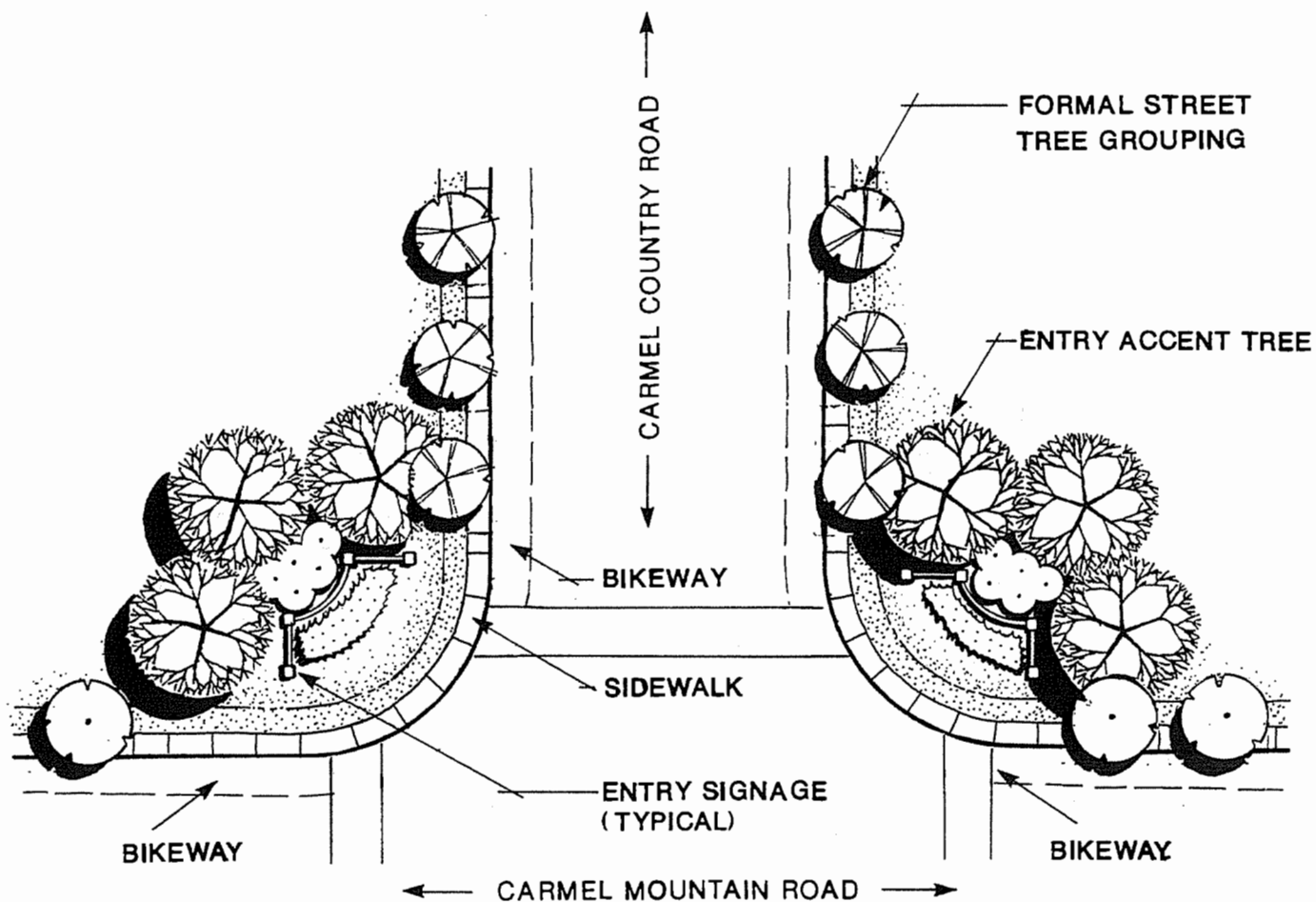
The edge treatment between the neighborhood commercial center in development area 11 and the low density housing in development area 12 is depicted in Section D in Figure 27. All commercial buildings must be set back at least fifteen feet (15') from the low density residential property line. A six foot (6') high solid masonry wall must be erected by the commercial developer on the property line to help screen the commercial center from the adjoining low density residential uses.; provided, however, that within any required front yard or street side yard such wall may be reduced in height to three feet (3').

G. ENTRIES

Entries into Neighborhood 10 should utilize certain community design elements (such as the community logo, compatible materials and coordinated landscaping) to impart unity and character. All entries should be consistent with the Carmel Valley Signage Guidelines and Criteria manual.

Neighborhood 10 will have major entries located along collectors and major arterials and several designated entries into individual neighborhoods. Figure 28, *Major Entry*, illustrates a typical major entry, and Figure 29, *Minor Entry*, illustrates a typical neighborhood entry.

Entry signs will be maintained by a homeowner's association.



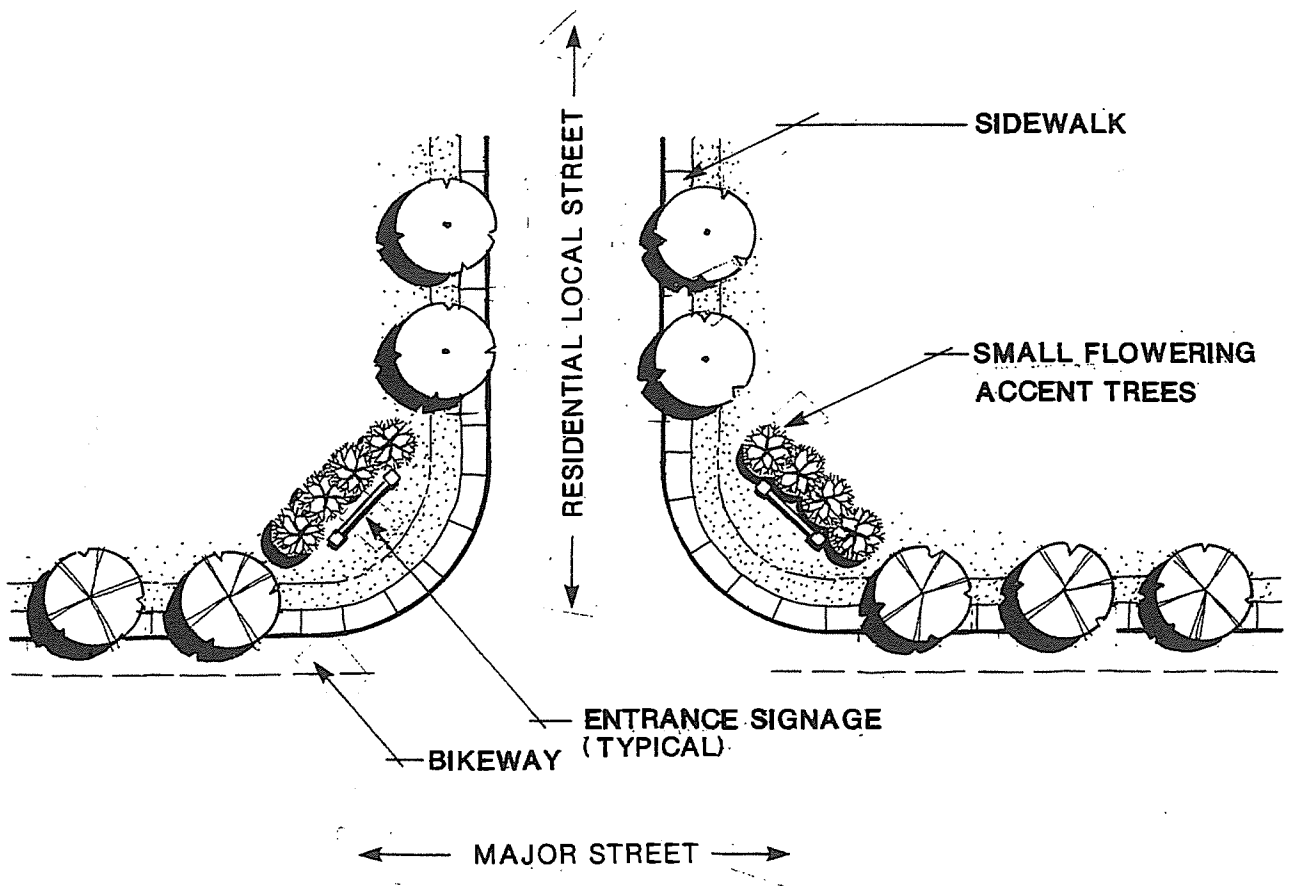
MAJOR ENTRY (Typical)
PRECISE
 P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

FIGURE 28



T&B Planning Consultants
 3242 HALLADAY, SUITE 100
 SANTA ANA, CALIF. 92705 (714) 662-2774
 5879 OBERLIN DRIVE, SUITE 200
 SAN DIEGO, CALIF. 92121 (619) 546-8366





MINOR ENTRY (Typical) **PRECISE**

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

FIGURE 29



H. FENCING

It is a goal of this plan that all walls and fencing within Neighborhood 10 exceed the minimum requirements of the City-wide Fence Ordinance. To accomplish this goal, a comprehensive plan is proposed for highly visible walls within the Neighborhood 10 community (see Figure 30, *Wall and Fencing Plan*). Three major categories of community walls are discussed below and illustrated on Figure 31, *Typical Walls and Fences*. Long, linear walls should be articulated with offsets and projections at periodic intervals.

■ PROJECT BOUNDARY WALLS

Project boundary walls are used along the perimeter of a project or residential lot to provide separation from an adjacent project, lot or interior residential street. These walls shall have a maximum height of six feet (6'). Project boundary walls will be consistent within each project area and will be attractive throughout the community.

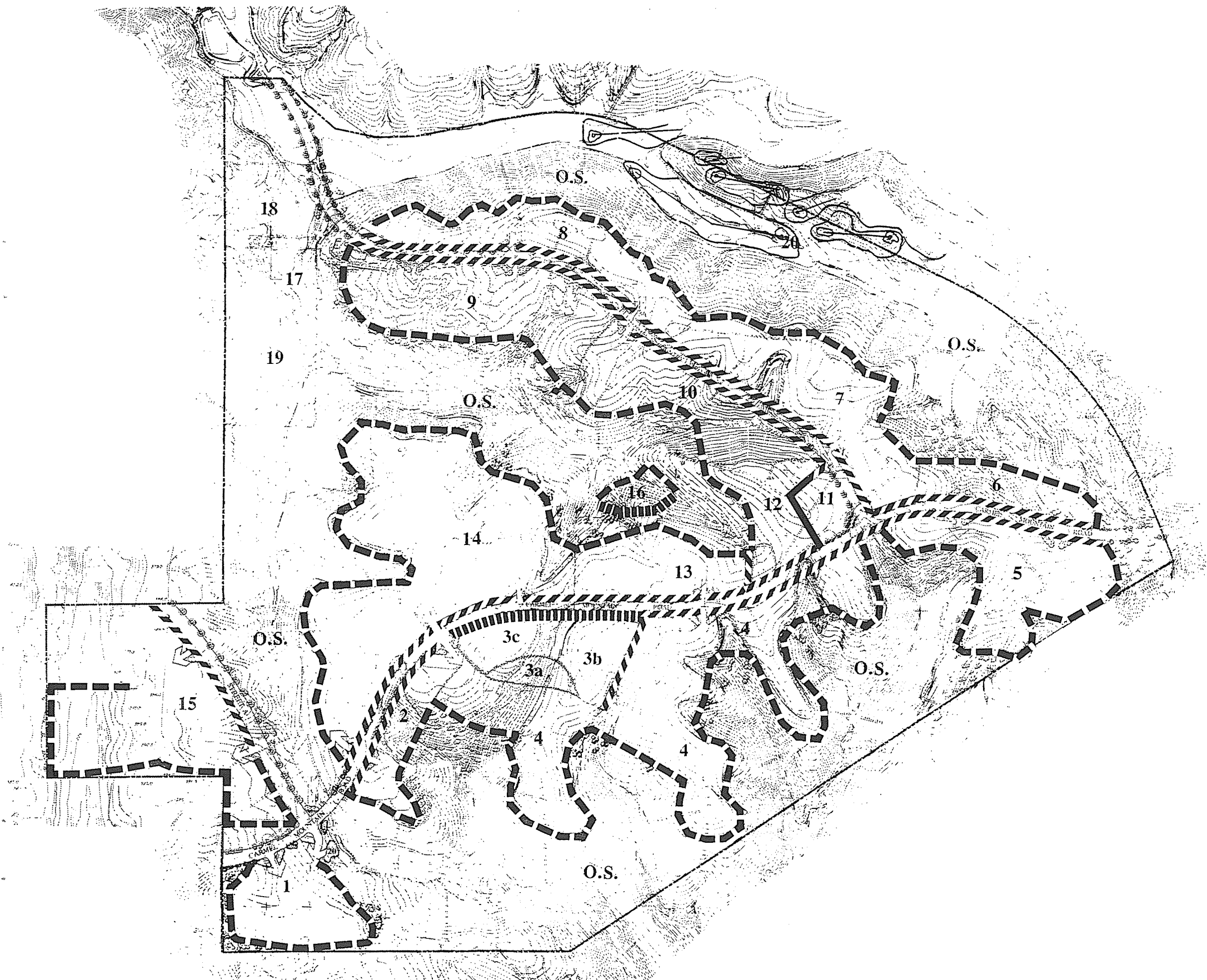
■ PERIMETER WALLS AND FENCES ON MAJOR STREETS

Perimeter walls are used for aesthetic purposes around projects, as well as a means to reduce the noise level of vehicular traffic along Carmel Mountain and Carmel Country Roads. A combination of slopes, landscaping between the walls, sidewalks and building setbacks should be incorporated to complement the overall landscape character and adjacent building architecture. These walls will be solid where sound attenuation is necessary and may be solid and/or open where noise attenuation is not required. The maximum height of walls and fences shall be six feet (6'), unless a higher barrier is required for sound attenuation purposes. All perimeter walls and fencing shall comply with the City's Municipal Code Ordinance, Section 101.0620, *Fences*. All perimeter walls shall be attractive and compatible with the community design. Refer to the landscape design section for suggested plant material within the parkway.

■ PERIMETER WALLS AND FENCES ADJACENT TO OPEN SPACE

In order to avoid visual impacts from canyon views into Neighborhood 10, and from adjacent lateral canyons, it is necessary to provide uniform fencing for all mesas. Fences abutting canyons and open space areas should, to the extent feasible, be constructed to be at least 40% open and designed and constructed of materials compatible with open space. Solid walls are permitted if sound attenuation is required per the Neighborhood 10 EIR. View fencing such as wrought iron or tubular steel will be encouraged, and solid walls will be discouraged. To ensure that these walls and fencing will be consistent with each project area and throughout the community, they shall be installed by the developer prior to occupancy of the units. Perimeter walls and fences shall have a maximum height of six feet (6') and shall be earthtone in color. The use of white or other brightly colored fencing is not permitted adjacent to open space areas.

WALL & FENCING PLAN



LEGEND

	WROUGHT IRON
	PILASTERS WITH WROUGHT IRON
	SOLID WALL (NO SLURRY COAT)
	SOLID WALL WITH SLURRY COAT (PAINTED)
	NO FENCING REQUIRED

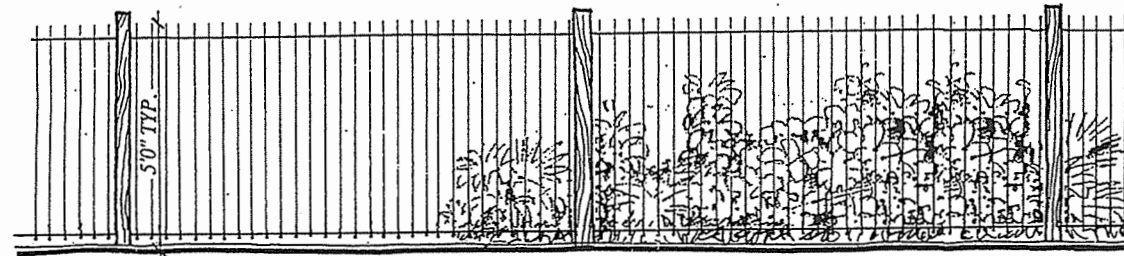
PRECISE

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

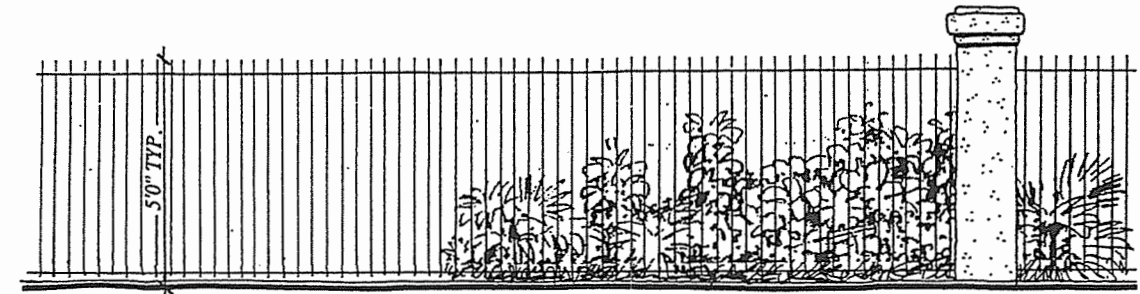
FIGURE 30

T&B Planning Consultants
3242 HALLADAY, SUITE 100
SANTA ANA, CALIF. 92705 (714) 662-2774
5879 OBERLIN DRIVE, SUITE 208
SAN DIEGO, CALIF. 92121 (619) 546-8366

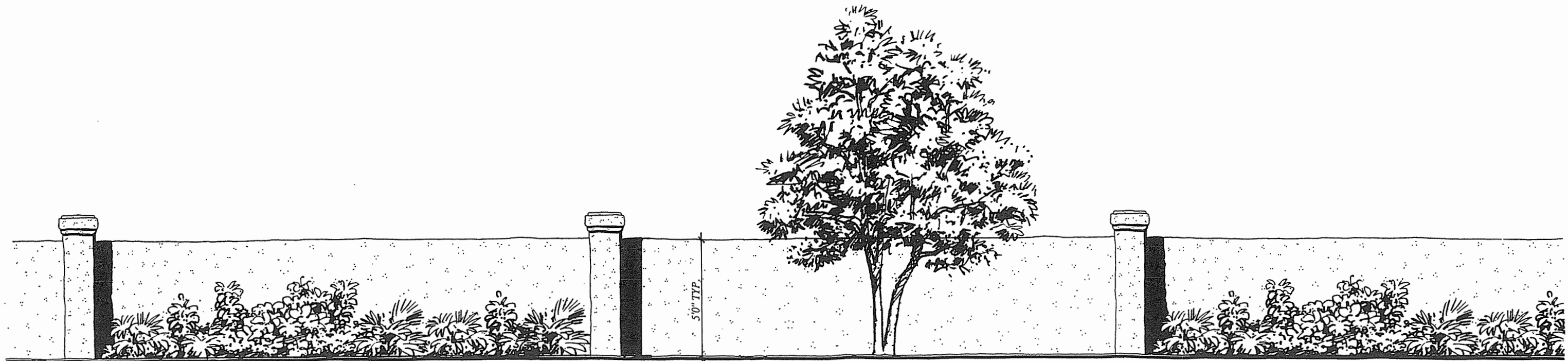
TYPICAL WALLS & FENCES



WROUGHT IRON WITH WOOD POSTS



WROUGHT IRON WITH PILASTERS



SOLID WALL

PRECISE

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

FIGURE 31



■ General Guidelines for Walls and Fences

The following general guidelines have been established for all community walls and fences within the Precise Plan area:

- Accepted materials for walls and fences include wood, stucco, slump block, wrought iron or tubular steel, stone and transparent (glass, plexi-glass, etc.).
- Walls and fences shall be maintained by a homeowner's association, or by means other than a landscape maintenance district.

I. RESIDENTIAL AREA

This Precise Plan recommends a maximum total of 1,551 dwellings if an elementary school is constructed within the Precise Plan area and a maximum of 1,566 units if no school is built. The following guidelines should be considered in the design, review and approval of subdivision maps and development plans for Neighborhood 10.

- Residential areas should create an identity through compatible design treatments, distinctive neighborhood entrances and delineation of project boundaries.
- Unit placement should consider maximizing views.
- Encourage curvilinear street patterns to conform with the existing topography, satisfy city standards and policies and provide visual interest.
- Establish an architectural theme within each residential neighborhood development or cluster.
- Architectural material and colors should be compatible with adjacent dwelling units within the development.
- Architectural interest and relief should be incorporated on those elevations of high visibility. This can be achieved through the use of horizontal offsets, vertical interest, pop-outs, overhangs, applied trim and recesses.
- Residential development located adjacent to commercially-designated property shall be provided with a six (6) foot solid masonry wall along all property lines which abut commercially-zoned land; provided, however, that within any required front yard or street side yard such wall may be reduced in height to three feet (3').

J. NEIGHBORHOOD COMMERCIAL CENTER

The Neighborhood Commercial Center is intended to accommodate shopping facilities that will provide convenience goods and services for the surrounding residential neighborhood.

Permitted uses within the center must co-exist with the neighborhood in an atmosphere of compatibility and restraint.

The center is proposed for development under the Neighborhood Commercial (NC) zone of the Carmel Valley Planned District Ordinance. The NC zone incorporates the standards set out in the City's Neighborhood Commercial (NC) zone. This includes permitted uses, property development regulations and off-street parking requirements. The development standards for the zone are designed to minimize possible conflicts with adjacent residential uses and to provide commercial services at a neighborhood scale.

Figure 32, *Commercial Concept*, illustrates a conceptual land use plan for the neighborhood commercial center. The commercial uses would be located at the intersection of Carmel Country Road and Carmel Mountain Road to maximize their exposure and facilitate ingress and egress.

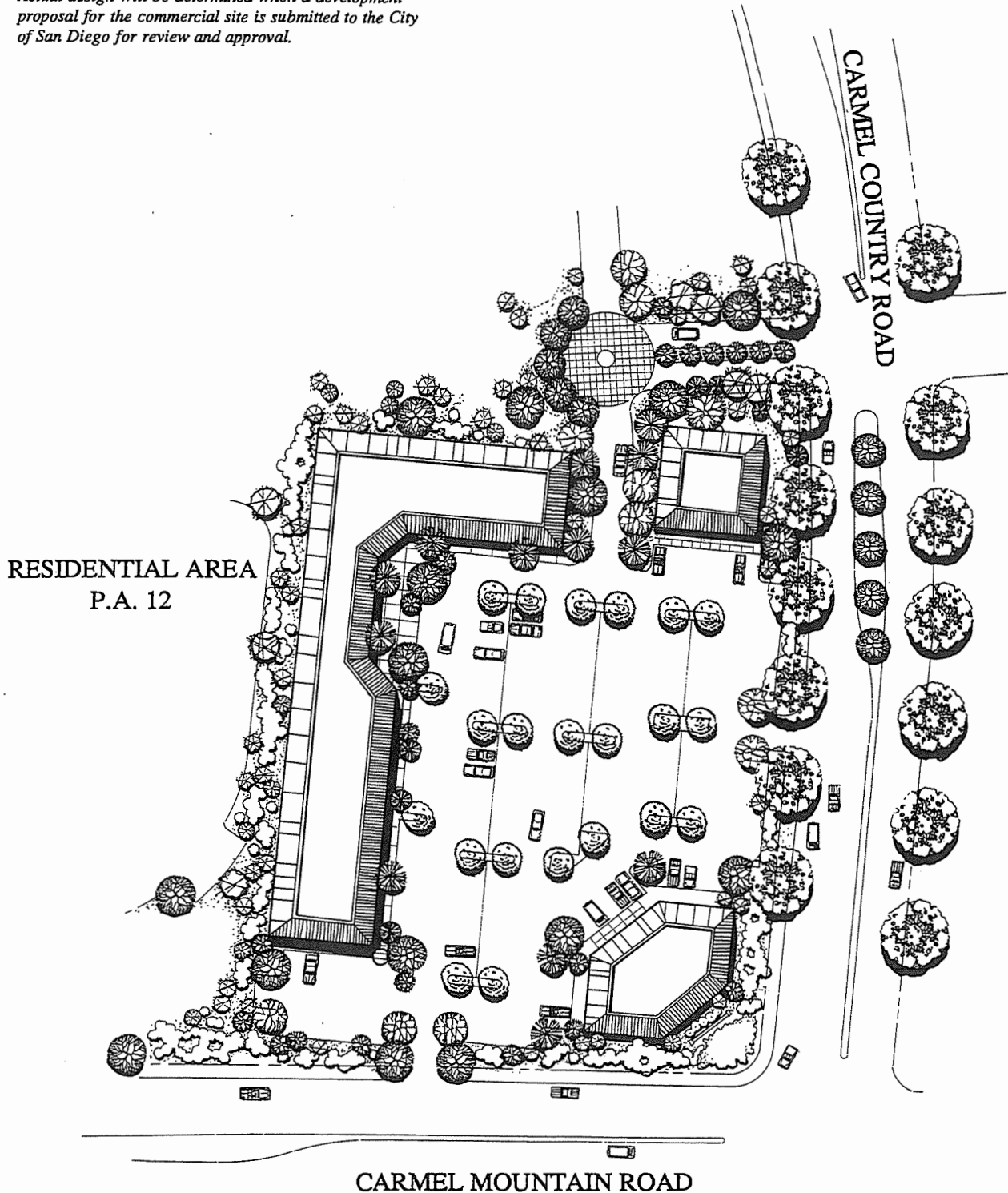
In addition to the zoning standards, the following design guidelines shall be considered in the development of the neighborhood commercial center.

- A six-foot (6') solid masonry wall shall be constructed along property lines common to both residential and commercial uses, and a six foot (6') landscaped strip shall be provided between the wall and any commercial uses including driveways, storage areas or parking.
- Impacts to adjacent residential areas shall be minimized by avoiding spillover lighting and carefully siting driveway entrances. Hours of operation and for delivery shall be determined at the building permit stage.
- The architectural design of the front, back and sides of all commercial buildings shall incorporate various design techniques which could include variation in facade treatment, varying roof and parapet heights, use of insets and texturing and other treatments that will enhance the center's appearance for the adjoining residents, as well as travellers on the surrounding roadways.
- The bulk and scale of the buildings, as well as the basic color palette of the project shall be appropriate to the commercial development and compatible with the surrounding development.
- Roofing materials and colors shall be selected that complement the visual quality of the adjacent residential area. All rooftop equipment shall be screened from view, and all building designs shall reflect a sensitivity to views from all off-site angles due to the visibility of the site from major transportation routes, as well as the surrounding residential area.
- Vehicular access to the site shall be provided in a manner that does not impact traffic flows along Carmel Country Road and Carmel Mountain Road.

VI. DESIGN GUIDELINES

CARMEL VALLEY - NEIGHBORHOOD 10 PRECISE PLAN

Note: This exhibit is provided for illustrative purposes only. Actual design will be determined when a development proposal for the commercial site is submitted to the City of San Diego for review and approval.



COMMERCIAL CONCEPT PRECISE

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

FIGURE 32



T&B Planning Consultants
3242 HALLADAY, SUITE 200
SANTA ANA, CALIF. 92705 (714) 662-2774
5879 OBERLIN DRIVE, SUITE 208
SAN DIEGO, CALIF. 92121 (619) 546-8366



- Joint access from Carmel Country Road into the neighborhood commercial center and the low density residential development shall be considered in order to minimize street intersections and median breaks on Carmel Country Road.
- A comprehensive landscape plan shall be provided that uses trees and shrubs not only to enhance the visual appearance of the parking area and the front of the center, but also to provide screening around the back and sides of the buildings to reduce the center's visibility from the surrounding area.
- Accommodate non-motorized transportation by providing on-site bicycle racks and, if required, a curb-side transit stop with sheltered seating areas to provide protection from inclement weather.
- Consider crime preventive design and ease of surveillance in site planning and access design.

■ **SIGNAGE**

- All signs shall be consistent with the sign program in place for this community at the time the development plan for this center is processed.
- Signs should be subtle, tasteful, imaginative and an integral part of the building and site design concepts.
- Roof and pole mounted signs are not permitted.
- Signs shall require a minimum of maintenance.

■ **EXTERIOR LIGHTING**

- Site lighting should be installed for security and safety purposes.
- Lighting shall be indirect and subtle. Overhead pole mounted down lighting is encouraged.
- Parking areas, access drives, internal vehicular circulation and outdoor pedestrian use areas shall have sufficient illumination for safety and security.
- Service area lighting shall be contained within the service yard boundaries and enclosure walls. No light spillover should be allowed.
- Architectural lighting should be indirect, such as soffit lighting, or shall incorporate a full cut-off shield type fixture.

- Architectural overhead down lighting is encouraged. Lighting should articulate and animate the building design.

K. SCHOOL/NEIGHBORHOOD PARK COMPLEX

As previously mentioned, Neighborhood 10 is designated on the Carmel Valley Community Plan to include an elementary school/neighborhood park complex. At present, no specific plans have been prepared. However, the overall concept of the complex should be designed in a joint effort with the School District, City of San Diego and the surrounding community. A conceptual illustration of the school/park is depicted in Figure 33, *School/Park Concept*. This exhibit is included in this Precise Plan to demonstrate that the school building and associated facilities, as well as the athletic playing fields and the tot lot, can be accommodated on the designated site. Figure 33 is provided for illustrative purposes only and is not intended to represent a final site design.

Actual design of the school/park will be determined in conjunction with the Del Mar Union Elementary School District and the City Parks and Recreation Department at the appropriate time.

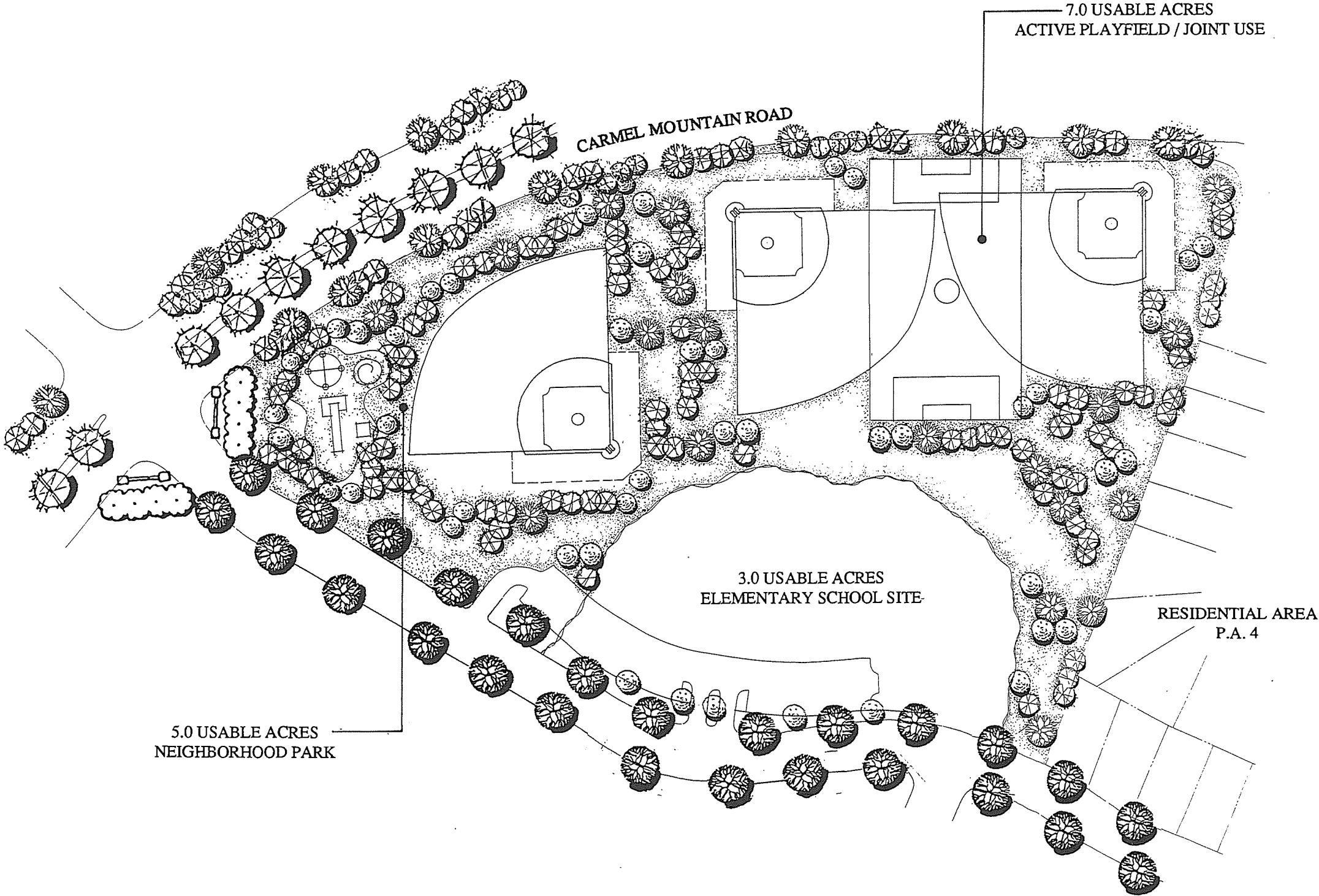
The Precise Plan for Neighborhood 10 proposes the location of the school/park site to be in the same location as shown in the adopted Community Plan. The following guidelines shall be incorporated into the school/park complex:

- Three usable acres minimum set aside for the elementary school, pad and building area.
- Seven usable acres minimum developed as an active playfield adjacent to the school site.
- Five usable acres minimum developed as a neighborhood park. There should be consideration given to a design that includes more passive activities.
- Both the active playfield and neighborhood park should contain amenities agreed upon by the Parks and Recreation Department, local school district and local community representatives. These areas will all be available to the school and community as joint use facilities pursuant to the Community Plan.
- Create the park-school complex as a focal point for the neighborhood.
- Link the park and school facilities functionally and aesthetically, facilitating joint usage.
- Provide for adequate neighborhood and community access via auto, bicycle and foot.
- Provide adequate on-site parking to avoid the need to park on adjoining streets.

VI. DESIGN GUIDELINES

CARMEL VALLEY - NEIGHBORHOOD 10 PRECISE PLAN

SCHOOL / PARK CONCEPT



Note: This exhibit is provided for illustrative purposes only. Actual design of the joint-use school and park will be determined if and when the Del Mar Union Elementary School District and the City of San Diego have committed to constructing a school and park facility within the Neighborhood 10 Precise Plan area.

PRECISE

P • L • A • N ■ CARMEL VALLEY/NEIGHBORHOOD 10

FIGURE 33



T&B Planning Consultants

3242 HALLADAY, SUITE 100
 SANTA ANA, CALIF. 92705 (714) 662-2774
 5879 OBERLIN DRIVE, SUITE 208
 SAN DIEGO, CALIF. 92121 (619) 546-8366



- Consider the principles of crime preventive design in site planning and facilities design. Where possible, encourage visual surveillance of the complex by passing motorists.
- Design the recreational facilities, paths and landscaping to take advantage of views. Select and locate plant materials to frame or enhance, not obscure, off-site views.
- Protect the school/park complex from excessive noise from Carmel Mountain Road through the construction of a solid masonry wall, if necessary. If there is a potential for adverse noise impacts to the school/park complex resulting from vehicular traffic on Carmel Mountain Road, noise mitigation measures will be required to reduce noise levels on the school/park site to acceptable City standards. Mitigation measures could include berms or solid masonry walls.
- Utilize landscaping, slopes and berms to screen parking areas and enhance the appearance of manufactured slopes.
- Provide sufficient on-site lighting for safety and security purposes. At the same time, avoid spillover lighting into nearby residential projects.
- The school should be designed to facilitate the use of meeting rooms and recreational facilities during off-session hours by the general public.
- Utilize a plant palette which incorporates plant materials from the parkway landscape treatment, to bring further visual coherence to the neighborhood. An informal landscaping pattern emphasizing planting clusters for decorative accents and screening is proposed. Extensive use of deciduous canopy trees along pathways and in seating areas is encouraged.
- Install drainage facilities as required to provide drainage in the direction of the natural flow.

L. CONSERVATION PRACTICES

1. ENERGY CONSERVATION

Thoughtful site design and quality architectural construction can conserve energy. At a minimum, development within Neighborhood 10 will comply with energy standards required by The California Administrative Code, Title 24 (Energy Conservation in New Building Construction) and solar energy issues as required by the City, in accordance with the State Subdivision Map Act, Section 66473.1. Long-term costs should be considered and given a higher priority than short-term installation costs. The following energy conservation guidelines should be incorporated:

- Buildings should be sited according to passive solar energy concepts.

- Whenever possible, buildings should incorporate either a passive or active solar energy system to help reduce heating and air conditioning costs.
- Proper insulation of walls, ceiling and floors will also reduce energy costs.
- Deciduous trees with dense foliage are recommended on the south and west faces of buildings to provide summer shade and winter sun.

2. WATER CONSERVATION

The landscape design section describes landscape and irrigation design guidelines to conserve water. Drought tolerant and native vegetation should be incorporated into the plant palette on all revegetated slopes adjacent to natural open space areas. In addition, all water reclamation and conservation methods required by state law shall be applied.

3. NATURAL OPEN SPACE CONSERVATION

During design, construction and maintenance of Neighborhood 10 developments, areas designated as natural open space should be left as intact as possible. Placement of fill should be minimized, and flattening of native vegetation by vehicles should not be permitted. During construction, the developer shall be responsible for preventing off-road vehicle intrusion into natural open space areas from the subdivisions. Also, trails should be carefully designed to discourage use by off-road vehicles.

4. FIRE PROTECTION

Dense native vegetation on hillsides adjacent to development poses a fire hazard, especially in the dry season. A brush management program has been established to address this issue as described in Section VI.

VII. PLAN IMPLEMENTATION

A. IMPLEMENTATION PROCESS

The Neighborhood 10 Precise Plan is one of several steps in securing City approval of development within the Precise Plan area. While based on the Carmel Valley Community Plan, the adopted Precise Plan itself becomes the basis for reviewing subsequent development plans, subdivisions and other permits.

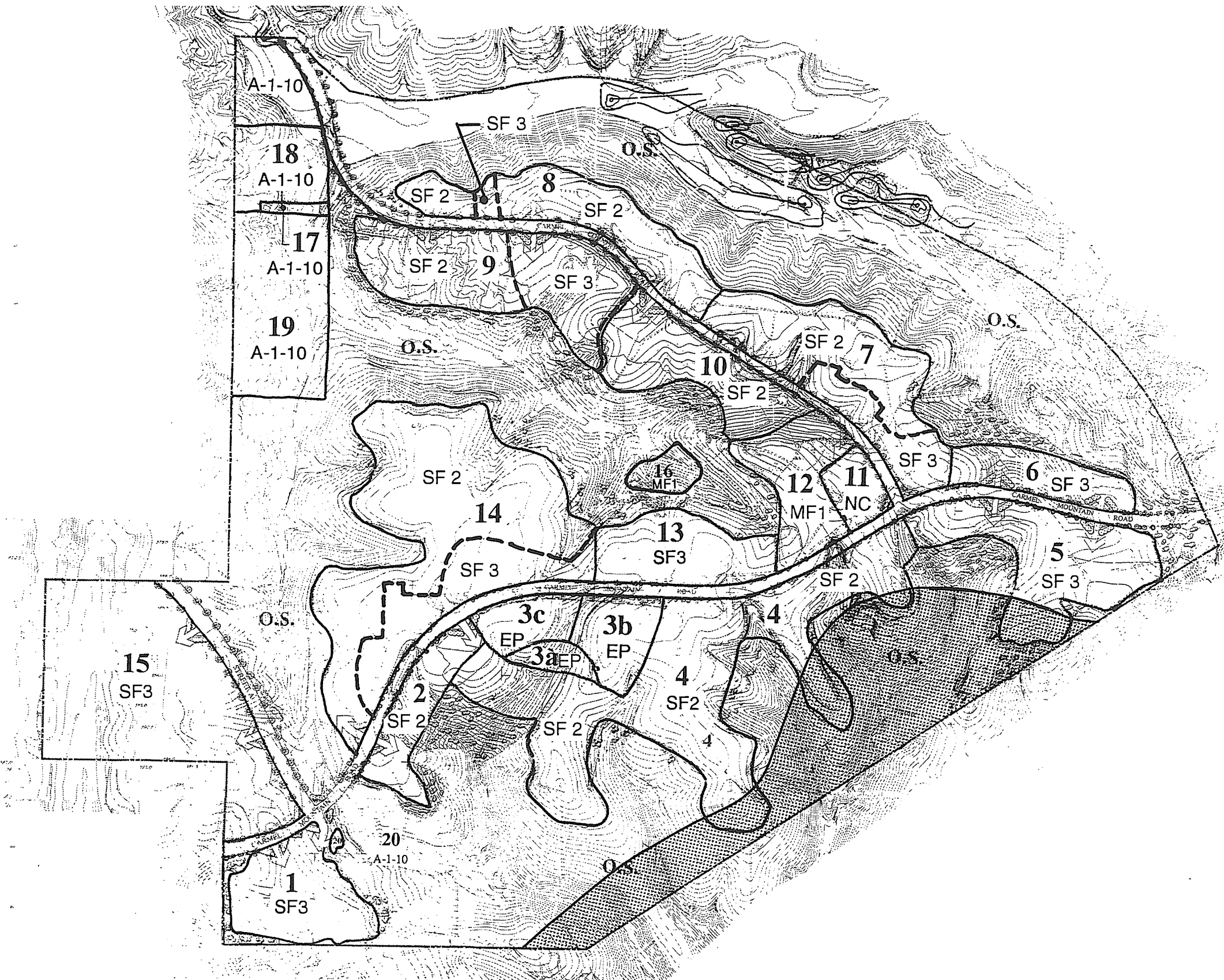
Companion documents to the Precise Plan include the Planned District Ordinance and associated Neighborhood 10 Environmental Impact Reports (EIR). The ordinance prescribes the procedures and standards for City review of development plans and establishes zoning controls. The EIR (DEP No. 91-0834 and LDR Nos. 96-0736 and 96-0737) cites the existing conditions in the Precise Plan area, anticipated impacts of development under the Precise Plan, and mitigation measures.

The Public Facilities Financing Plan and the School Facilities Master Plan area are also applicable to the implementation of the Precise Plan. The Financing Plan provides for phased financing and development of the public infrastructure serving Neighborhood 10. The Carmel Valley Transportation Phasing Plan limits the issuance of building permits to the funding and availability of transportation improvements. The School Facilities Master Plan deals with the future provision of public schools.

A small portion of the site along the southeastern border will require a Coastal Development Permit (see Figure 34, *Zoning Plan*).

B. PRECISE PLAN AMENDMENTS

Any modifications to this Precise Plan shall take place in accordance with the process described in this section. Amendment proposals are divided into two basic categories: Administrative and Formal. All amendments must be consistent with the goals and objectives of the General Plan, the Carmel Valley Community Plan, the Carmel Valley Planned District Ordinance and the Carmel Valley Facilities Benefit Assessment, the Master School Facilities Agreement and the general goals of this Precise Plan. Any amendments which affect properties within the Coastal Zone must also be consistent with the Coastal Act and with the goals and objectives of the City of San Diego certified Local Coastal Program, and are not effective within the Coastal Zone until certified by the Coastal Commission. In addition, proposed amendments should follow the criteria described below:



- A-1-10 RURAL RESIDENTIAL
- SF2 VERY LOW DENSITY RESIDENTIAL
- SF3 VERY LOW DENSITY RESIDENTIAL
- MF1 LOW DENSITY RESIDENTIAL
- NC NEIGHBORHOOD COMMERCIAL CENTER
- EP ELEMENTARY SCHOOL, NEIGHBORHOOD PARK
- OS OPEN SPACE PRESERVE
- COASTAL ZONE

1. FORMAL AMENDMENTS

All Precise Plan modifications which do not meet the definitions of an Administrative Amendment noted in this section shall be deemed to require a Formal Amendment of this Precise Plan. All Formal Amendments shall require additional environmental review and shall be reviewed for approval by the Planning Commission and the City Council.

2. AMENDMENT CRITERIA

All amendments to the Precise Plan shall reflect the same comprehensive analysis which has been undertaken in the adoption of the Precise Plan. All formal amendments will require additional environmental review. The applicant shall satisfy the following minimum criteria:

- a. Demonstrate that the proposed amendment meets the goals and objectives of the Precise Plan and the Carmel Valley Community Plan.
- b. Ensure that any impacts to the Precise Plan, resulting from the amendment, shall be mitigated unless a statement of overriding considerations is adopted.
- c. Update Precise Plan technical studies (e.g., traffic, biology, archaeology) and/or provide additional environmental studies upon determination of the Development Services Department or City Engineer.
- d. Provide revised Precise Plan text and maps, as necessary.

C. PHYSICAL DEVELOPMENT CONTROLS

1. PLANNED DISTRICT ORDINANCE

"The public health, safety and welfare necessitate distinctive development controls and requirements for capital improvements and public facilities in order to systematically implement the phased growth of Carmel Valley." In keeping with this statement, the Carmel Valley Planned District Ordinance establishes procedures, standards and zoning controls for City review of any development, building or construction within the Carmel Valley Planning Area.

The Neighborhood 10 Precise Plan has been prepared in conformance with the Planned District Ordinance to facilitate the implementation of subsequent development plans in the Precise Plan area. The implementation mechanisms as described above include:

- Zoning to identify permitted uses and accompanying development regulations for designated areas. Table VI assigns land uses delineated in the Land Use Element of the Precise Plan to zones specified in the Planned District Ordinance, while Figure 34, *Zoning Plan*, depicts the proposed zoning locations for the Precise Plan area.

- Development plans, including site layouts and landscaping plans. Planning Commission approval of development plans insures conformance with the Design Element of the Neighborhood 10 Precise Plan.
- Tentative Subdivision Maps, including street alignments, grading and easements. Tentative maps are reviewed and recommended for approval by the Planning Commission in substantial conformance with the street design guidelines, grading concepts and drainage proposals outlined in Sections V and VI of the Precise Plan.
- A Planned District Permit for development in all zones located within the Neighborhood 10 Precise Plan.

In addition, the following conditions shall be met during the development approval process:

- All tentative maps for single family residential projects in Neighborhood 10 shall be accompanied by a development plan. The development plan shall be consistent with the P.D.O.
- Prior to the approval of a final map or issuance of a Building Permit, compliance with the terms of the adopted Carmel Valley School Facilities Master Plan must be demonstrated.
- A conceptual landscaping plan must accompany each Neighborhood 10 development plan and provide for the stabilization of all graded slopes.
- Approval of the first tentative map in the neighborhood shall be subject to the approval of a comprehensive drainage plan for the entire Neighborhood 10 area by the City Engineer prior to recordation of a final map.
- Prior to the recordation of any final map in Neighborhood 10, approval shall be given to a sign plan for Neighborhood 10, if one has not yet been established for the community.

TABLE VI
PHYSICAL DEVELOPMENT CONTROLS*

PRECISE PLAN LAND USE CATEGORY	P.D.O. ZONING*	ZONING DESCRIPTION	PERMITTED USES
Rural Residential	A-1-10	Minimum Lot Size: 10 Acres (Existing lots less than 10 acres in size are non-conforming.)	All uses permitted in Section 101.0404 of the City's Municipal Code including: <ul style="list-style-type: none"> • Single-family dwellings • Churches • Private stables (includes commercial stables) • Agricultural uses • Public utility substations
Very Low Density Residential	SF2 SF3	Single Family Detached Minimum Lot Size: 4,500 s.f. Single Family Detached Minimum Lot Size: 3,000 s.f.	All uses permitted in Section 101.0407 of the City's Municipal Code including: <ul style="list-style-type: none"> • One-family dwellings • Public parks and playgrounds • Temporary real estate sales offices and model homes
Low Density Residential	MF1	Single Family Detached Homes, Townhomes, or other Low Density Attached Units. Density of 7-15 du/net ac	All uses permitted in Section 101.0410 of the City's Municipal Code including: <ul style="list-style-type: none"> • One-family dwellings • Two-family dwellings • Apartment houses, excluding premises designed or used for the temporary residence of persons for less than one week • Schools • Churches and temples • Temporary real estate sales offices and model homes • Public parks and playgrounds
Neighborhood Commercial Center	NC	Uses allowed by CN Zone (Neighborhood Commercial Zone) in City Code	All uses permitted in Section 101.0426 of the City's Municipal Code.
Elementary School, Neighborhood Park	EP	Educational and Park Area	<ul style="list-style-type: none"> • Elementary, junior high and senior high schools • Neighborhood and community parks
Open Space	OS	Open Space and compatible uses.	<ul style="list-style-type: none"> • Open space, including natural and equestrian/pedestrian trail uses

* See Carmel Valley Planned District Ordinance (PDO) for specific zoning regulations. The A-1-10 PDO Zone is the same as the City-wide A-1-10 Zone (see Section 101.0404 of the San Diego Municipal Code).

VII. PLAN IMPLEMENTATION
CARMEL VALLEY - NEIGHBORHOOD 10 PRECISE PLAN

2. COMPLEMENTARY APPROVALS

All subdivisions, rezonings and other discretionary acts required for the physical implementation of the Precise Plan area are subject to environmental review under the provisions of the California Environmental Quality Act (CEQA) and the City Municipal Code Ordinance. Projects should be reviewed for compliance with the associated Carmel Valley/Neighborhood 10 Precise Plan Environmental Impact Reports accompanying this document.

Concurrent with adoption of the Neighborhood 10 Precise Plan, the entire Neighborhood 10 project shall be in conformance with the City's Resource Protection Ordinance (RPO). For all subsequent development proposals which demonstrate consistency with this Precise Plan, compliance with Council Policy 600-40 shall be demonstrated.

In addition, prior to recordation of any map, compliance with the terms of the adopted Carmel Valley School Facilities Master Plan must be demonstrated during the development approval process.

D. DEVELOPMENT PHASING

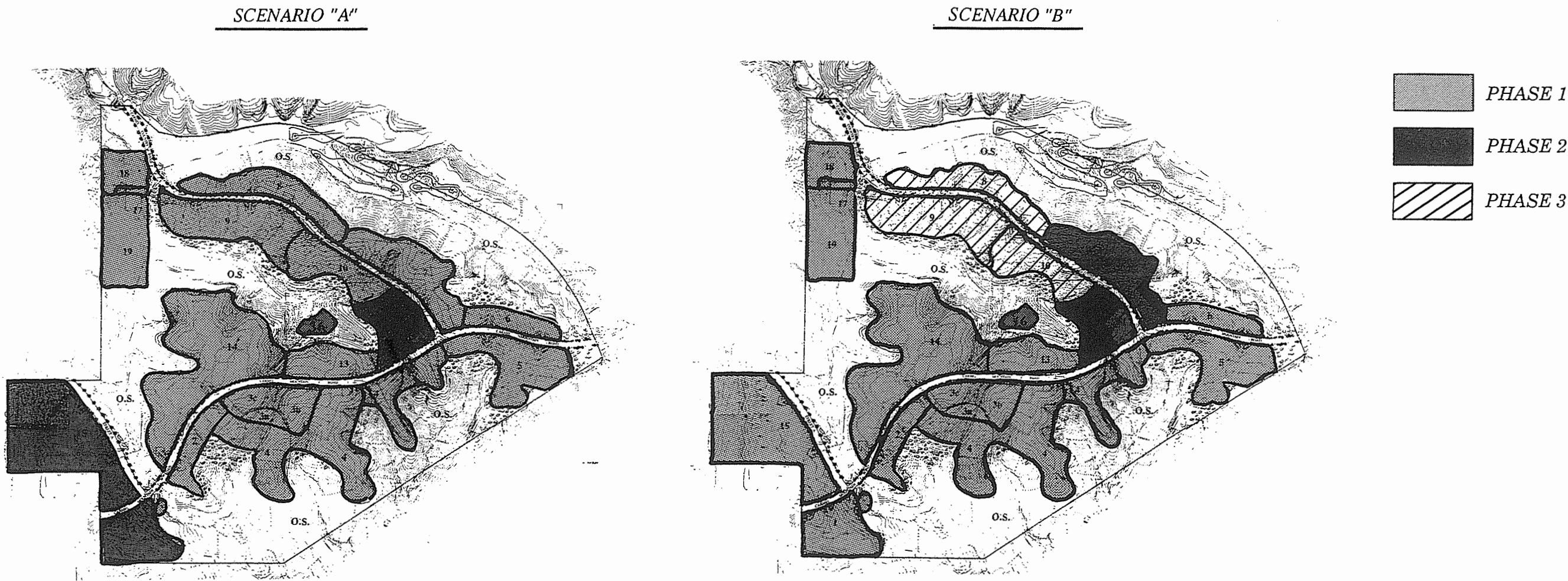
Phasing of development within Neighborhood 10 will be directly related to the provision of major streets and other elements of the community infrastructure proposed for development within an adopted Public Facilities Financing Plan (PFFP) for Carmel Valley. Such a Financial Plan must be prepared for this area (as well as others) in Carmel Valley, as explained below.

Carmel Valley is designated as a new community in the planned urbanizing area by the City of San Diego *Progress Guide and General Plan*. Under this designation and City Policy 600-28, a PFFP must be adopted prior to map recordation for Neighborhood 10. The purpose of the phasing program is to coordinate the timing and level of public facilities, and the sequence and amount of residential development.

Because the development of Neighborhood 10 is directly related to the improvements and future extensions of Carmel Mountain Road, Carmel Country Road and Street A, three logical phasing scenarios are presented for the buildout of Neighborhood 10. Scenarios A and B are depicted in Figure 35, *Development Phasing Scenarios*.

- **SCENARIO A:** Scenario A Phasing Plan assumes the extension of Carmel Country Road from the north through the project site with construction of portions of Carmel Mountain Road in Phase 1. Carmel Mountain Road and Street A may be completed in either Phase 1 or 2, depending upon traffic needs and demands. (See Table VII, below, and Figure 35.)

DEVELOPMENT PHASING SCENARIOS



* NOTE: ALTERNATIVE "C" (NOT SHOWN) ASSUMES THE SIMULTANEOUS EXTENSIONS OF PORTIONS OF CARMEL COUNTY ROAD AND CARMEL MOUNTAIN ROAD. THE PHASING OF EACH DEVELOPMENT AREA WILL BE DETERMINED AT EITHER THE TENTATIVE OR FINAL MAP LEVEL.

- **SCENARIO B:** Scenario B Phasing Plan assumes the extension of Carmel Mountain Road from the western Precise Plan boundary, east to the eastern Precise Plan boundary. This will occur in the first phase of the project. Street A will be constructed at this time to provide access directly from the on-site portion of Carmel Mountain Road to Carmel Creek Road (an off-site street that will feed into proposed Route 56). Carmel Country Road will be constructed in Phases 2 and 3. (See Table VII and Figures 12 and 36.)

■ **PROVISION OF PUBLIC FACILITIES**

- Street, utilities and drainage facilities will be constructed concurrently with residential development, ensuring sufficient capacity to meet residents' requirements.
 - Construction of the school, neighborhood park, neighborhood commercial center and equestrian/pedestrian trails will occur when adequate demand warrants. This is likely to occur in Phase 1 of Scenarios A or B; however, the school should be developed when required and the park complex should be developed as early as feasible.
 - Community-level facilities outside Neighborhood 10 will be built when the service area is sufficient, with fees or assessments collected as residential construction progresses. A library and fire station have already been constructed.
- **SCENARIO C:** Scenario C Phasing Plan assumes the simultaneous extension of Carmel Country Road from the north and Carmel Mountain Road from the west through the Precise Plan area. (Not shown in Table VII, below). The exact buildout of each development area will be determined at either the tentative or final map level. Street A will be constructed as necessary to ensure adequate levels of circulation movement.

Because of the nature of the ownership patterns within the Neighborhood 10 Precise Plan area, as well as the ability of the various property owners to begin development and install the necessary infrastructure improvements to support the planned development, areas slated for development in the first phase (i.e., Phase 1) of Scenarios A and B may, in fact, occur later than development slated to occur in later phases (i.e., Phase 2 and/or Phase 3), in which case, the development planned for areas in Phase 2 and/or Phase 3 may be developed prior to Phase 1 development. Such phasing is feasible and consistent with this Precise Plan. The exact phasing of the infrastructure necessary to support development in Neighborhood 10 will be specifically defined in the PFFP for Carmel Valley South.

**TABLE VII
DEVELOPMENT PHASING SCENARIOS**

	DEVELOPMENT AREAS	TOTAL DWELLING UNITS	TOTAL COMMERCIAL ACREAGE
SCENARIO A			
Phase 1	2, 4, 5-10, 13, 14, 16, 17-20, 3 ¹ (school/park)	1,150 SF DU 13 MF DU	---
Phase 2	1, 12, 15, 11 ¹	290 SF DU 98 MF DU	4.0 acres ¹
TOTAL		1,551 DU	4.0 ACRES
SCENARIO B			
Phase 1	1, 2, 4-6, 13-20; 3 ¹ (school/park)	1,055 SF DU 13 MF DU	---
Phase 2	7, 11 ¹ , 12,	122 SF DU 98 MF DU	4.0 Acres*
Phase 3	8, 9, 10	263 SF DU	---
TOTAL		1,551¹ DU	4.0 ACRES

E. PUBLIC FACILITIES FINANCING

On May 28, 1996, the City Council adopted a Public Facilities Financing Plan for the Carmel Valley Community Plan Area which includes areas north and south of Carmel Valley Road. This financing plan provides for public facilities through a Facilities Benefit Assessment (FBA) program. The facilities identified and financed are community parks, Park-N-Ride facilities, library, fire station, neighborhood parks, sewage pump station, water line and major road system.

The possible financing methodologies for Neighborhood 10 are summarized as follows:

- ***Facilities Benefit Assessment (FBA)*** against dwelling units or the equivalent within the Precise Plan area for public facilities and services, such as the 24-inch regional water line and the Neighborhood 10 park and transportation improvements

¹ Table VII depicts possible phasing scenarios for development of the school/park & neighborhood commercial center in Neighborhood 10. However, actual construction of the school/park & commercial center will occur when adequate demand warrants.

of a community-wide benefit. Other facilities and services include this community's fair share of a library, a fire station, public transit, traffic signals, the school/park complex, and provision of an open space linkage on-site with a crossing constructed under Carmel Mountain Road to facilitate wildlife movement between Peñasquitos Canyon and Carmel Valley. In-lieu credits for construction of facilities are optional and encouraged with a reimbursement agreement.

- ***Standard Subdivision Agreements*** to finance on and off-site improvements under the conventional subdivision process.
- ***Reimbursement Agreements*** between developers and the City for the construction of improvements for community-wide benefit or neighborhood-wide benefit.
- ***School Financing*** as available and approved by the governing school district through a joint powers agreement or existing Mello-Roos.
- ***Development Agreements*** to provide for the payment of fees to the City under the Facilities Benefit Assessment and the School Facilities Master Plan, with such fees to be used by the City or school districts for construction of necessary facilities.

Use of an assessment district created under the Improvement Acts of 1913/1915 is optional. This district could be applied to the Precise Plan area and utilized to finance such facilities as major utilities and perimeter major streets.

The Neighborhood 10 Precise Plan area should be added to the Carmel Valley community lighting and open space maintenance district. The district should maintain and/or operate the following:

- Natural open space areas and open space linkages other than those areas to be maintained by private property owners or homeowners' association.
- Street medians and parkways of major streets and selected portions of neighborhood collector street parkways.
- The equestrian trail and other informal pedestrian paths within natural open space areas.

The neighborhood park and playground portion of the elementary school site should be maintained through the City and school district joint use agreement operating budget.

F. MITIGATION, MONITORING AND REPORTING PROGRAM

The California Environmental Quality Act (CEQA), Section 21081.6, requires that a mitigation monitoring and reporting program be adopted upon certification of an

environmental impact report (EIR) in order to ensure that the mitigation measures are implemented. The mitigation monitoring and reporting program specifies what the mitigation is, the entity responsible for monitoring the program, and when in the process it should be accomplished.

The mitigation monitoring and reporting program for the Neighborhood 10 Precise Plan Amendment is under the jurisdiction of the City of San Diego and other agencies as specified below. The following is a description of the mitigation monitoring and reporting program to be completed for the project. Tables and figures from the EIR for the project (State Clearinghouse No. 97-011032) are referenced in the following text.

1) TRANSPORTATION/TRAFFIC

The following mitigation measures have been included in the proposed precise plan amendment:

- a. Provide traffic signals at the intersections of Carmel Mountain Road and Carmel Country Road; Carmel Country Road and the commercial center/Area 7 access street; Carmel Mountain Road and the school site access street; and Carmel Mountain Road and Street "A."
- b. For Carmel Mountain Road, construct as a four-lane collector from the western project boundary to Street "A"; construct as a four-lane collector from Street "A" to Carmel Country Road; and construct as a two-lane collector from Carmel Country Road to the eastern project boundary.
- c. For Carmel Country Road, construct as a four-lane collector from Carmel Mountain Road to SR-56.
- d. For Street "A," construct as a two-lane collector from Carmel Mountain Road to the northern project boundary.

2) AIR QUALITY

The amended precise plan would implement a number of regional air pollution control tactics; these measures would reduce direct impacts to air quality to below a level of significance for the amended precise plan. These measures would not reduce cumulative impacts to air quality to below a level of significance.

Direct impacts to air quality from the amended precise plan would be reduced by the incorporation of appropriate tactics as listed in the RAQS and Transportation Control Measures (TCM) Plan. These would include provision of sidewalks along all major and local streets to facilitate pedestrian movement and bicycle lanes along Carmel Country Road, Carmel Mountain Road, and Street "A." The pedestrian and bike lane system would

provide access to the neighborhood commercial center, the school/park area, and the existing transit center located in the North Town Center in Neighborhood 9. The major streets within the precise plan area would also be designed to allow for the construction of bus stops, as needed by the Metropolitan Transit Development Board. In addition, implementation of the transportation improvement phasing program detailed in the Transportation section of this EIR would facilitate traffic flow at projected buildout in the area.

Prior to approval of the amended precise plan, the City Development Services, Transportation Development shall verify that the sidewalks, bicycle lanes, and room for bus stops, as needed, are incorporated in appropriate sections of the amended precise plan. Additionally, the road system and other traffic improvements identified in Chapter 4B, Transportation/Traffic of the SEIR, will conform with the recommended transportation improvement phasing program.

3) LANDFORM ALTERATION/VISUAL QUALITY

Landform Alteration

The adopted Neighborhood 10 Precise Plan development guidelines are unaltered by Precise Plan Amendment No. 3. Therefore, it shall be a requirement of the amended vesting tentative maps that all manufactured slopes greater than 10 feet in height be contour graded as shown in the adopted precise plan. The grading guidelines include cutting to the daylight line along the canyon rims to minimize fill on canyon slopes and minimizing the heights of manufactured slopes during engineering design. Final design of each manufactured slope will be determined at the final map stage. The landform alteration impacts would not be reduced to below a level of significance.

Prior to approval of a grading and landscape plan for the two amended VTMs and other future subdivision maps, the City Development Services, Development Coordination shall verify that these conditions have been met. Prior to approval of the final grading of any subsequent final map of the site, the City Development Services, Development Coordination shall verify that these requirements are shown on the VTMs. The City Development Services, Environmental Analysis Section shall be kept informed in writing of the Development Coordination approvals.

Visual Quality

The approved precise plan contains development guidelines and landscaping recommendations which would partially reduce the significant visual impacts from the proposed development which would result from the precise plan amendment. The visual effect of the additional grading necessary to implement the precise plan amendment would be further reduced by incorporation of the contour grading as shown in the approved precise plan; however, because the additional 22.3 acres of development would increase the impact

already considered significant, these impacts would not be reduced to a level less than significant.

Specific design guidelines and objectives stated in the approved precise plan and amended precise plan which would reduce the visual quality impacts are:

- a. To the maximum extent feasible, grading shall cut to daylight lines along canyon rims in order to minimize fills on canyon slopes.
- b. Manufactured slope gradients adjacent to natural open space shall be variable for slopes greater than 10 feet in height, but in no case shall the gradient exceed a 2:1 gradient. Internal sideyard slopes less than five feet in height may be constructed at 1.5:1 maximum.
- c. No off-site grading shall be permitted in Los Peñasquitos Canyon Preserve.
- d. In engineering design, the heights of manufactured slopes shall be minimized. For manufactured slopes in excess of 10 feet in height, which are identified in the EIR section, slopes shall be blended, tops of slope banks shall be rounded, and contour or landform grading shall be utilized. Landform grading shall incorporate variable 2:1 and 4:1 slope gradients in order to produce a natural undulating terrain.
- e. For slopes that exceed 10 feet in height, a detailed landscape and maintenance plan shall accompany future grading plans. These plans shall ensure natural appearing slope coverage by vegetation within a practical time frame, as recommended by the landscape architect and approved by the Park and Recreation Department and Development Services.
- f. All manufactured slopes adjacent to natural open space or in areas with high community or neighborhood visibility shall be contour graded to produce a more natural appearance and shall be blended to meet the native terrain.
- h. Sensitive grading techniques shall be implemented to protect designated open space areas and sensitive resource areas from encroachment.
- i. All manufactured slopes shall be landscaped and irrigated to ensure adequate plant establishment and slope stability, reduce erosion, and enhance their visual appearance. In addition, grading should be limited to the dry months or special construction methods should be utilized to minimize erosion and siltation problems during construction.

Prior to issuance of a grading permit and landscape plan for any tentative subdivision map or VTM within the precise plan area, the City Development Services, Development Coordination shall verify that the above conditions are shown on the grading plans. Prior

to issuance of the grading permit of any tentative map or VTM within the precise plan area, the City Development Services, Development Coordination shall verify that these requirements are shown on the tentative map or VTM. The City Development Services, Environmental Analysis Section shall be kept informed by the applicant in writing of the Development Coordination approvals.

4) HYDROLOGY AND WATER QUALITY

Water Quality

The following measures to mitigate direct impacts to below a significant level and lessen cumulative water quality impacts (but not to below a level of significance) are incorporated into the proposed precise plan amendment and shall be a condition of the Neighborhood 10 North and Parkview East amended VTMs, subject to approval by the City Engineer.

- a. The Neighborhood 10 Precise Plan master drainage plan shall be amended to demonstrate that the development of an additional 22.3 acres in coastal lagoon watersheds would not increase the rate of runoff reaching the lagoon in a 10-year design storm.
- b. Per the Clean Water Act, BMPs to control pollutants and sediment from entering stormwater runoff are required for the precise plan area. The precise plan amendment shall be consistent with the approved plan and provide source control BMPs via landscaping of all slopes and street rights-of-way to prevent erosion and a grading/drainage concept that directs water away from easily erodible areas and into a drainage system designed to safely handle the stormwater runoff. Additionally, desilting/water quality basins shall be provided at strategic locations within the precise plan area. Any other applicable source control or BMPs which may be implemented on a city-wide basis in conjunction with the City's Municipal NPDES permit (Permit No. CA 018758) and State Regional Water Quality Control Board Order Number 90-42, if implemented prior to the adoption of the precise plan, shall be incorporated into the precise plan.

The City Development Services, Development Coordination shall verify that the above mitigation measures are conditions for the approval of all subsequent tentative maps and VTMs within Neighborhood 10.

Runoff and Erosion

Several measures included in the approved precise plan shall be included as conditions of the amended VTMs to reduce peak flow, runoff velocity, erosion, and downstream siltation during grading and construction phases.

- a. Direct surface drainage to natural slopes and manufactured slopes shall be minimized by (a) grading away from slopes, (b) providing drainage swales at tops or toes of manufactured slopes, where appropriate, and (c) providing an underground drainage system.
- b. Direct and indirect grading and drainage impacts to Los Peñasquitos Canyon Preserve shall be minimized through the design provided in the precise plan. Specifically, the plans provide for underground and surface drainage facilities for all the developed areas. All stormwater runoff from developed areas shall first be directed through desilting basins to a stable discharge point to minimize impacts to the Preserve.
- c. Grading shall be limited to the dry months (March 15-October 15) and special construction methods shall be used to minimize erosion and siltation problems during construction, except that grading may continue during the rainy season, provided adequate erosion-control measures are employed to the satisfaction of the City Engineer. These measures may include use of sand and gravel bags, hay bales, silt fences, and temporary desilting basins.
- d. All manufactured slopes shall be landscaped and irrigated to ensure slope stability, reduce erosion, and enhance visual appearance within 90 days of their creation. Temporary slope erosion-control measures, such as hydroseeding, and slope stability measures will be undertaken.
- e. Surface water crossing slope banks shall be reduced by providing drainage swales above the banks.
- f. To reduce the loading of nutrients in urban runoff, landscape design shall incorporate the use of low-water requirement vegetation.
- g. Slope planting species shall be chosen for low fertilization requirements, and fertilization shall be discontinued one year after planting for naturalized areas adjacent to open space.
- h. All manufactured slopes shall be maintained per Section 7.3, Maintenance Requirements, of the City of San Diego Landscape Technical Manual, requiring permanent (or temporary per City direction) irrigation systems to be inspected on a regular basis and properly maintained.
- i. Pollution control devices and best management practices (BMPs), as specified by the City Development Services, shall be utilized in designing the drainage and detention/desilting system in response to NPDES requirements. Examples include grass-lined swales and French drains.

5) GEOLOGY AND SOILS

Geologic Conditions

The following recommendations for mitigation of potential impacts are made in the 1988 and 1992 Leighton and Associates reports, the 1993 Pacific Soils Engineering, Inc., report, and the 1995, 1996, and 1997 Geocon Incorporated reports. Implementation of these mitigation measures would reduce impacts associated with potential geologic hazards to below a level of significance. A summary of these measures follows.

- a. Prior to grading permit issuance for the proposed development, further geotechnical studies that include detailed subsurface exploration and laboratory testing shall be performed to provide specific design recommendations for earthwork, foundations, and other geotechnical and construction considerations. The studies shall be submitted to and approved by the City Engineer.
- b. Because of the potential landslide hazards associated with the Scripps Formation and Ardath Shale, it is recommended that grading on these slopes be mapped and monitored by an engineering geologist. Mitigation measures for detected landslides and/or other adverse geologic hazards shall include the construction of earth fill buttresses and other remedial grading measures, where warranted. In addition, the studies prepared under item a above shall also address the need for foundation setbacks from the canyon rim daylight areas.
- c. The lineaments and suspected faults identified on the site shall be adequately investigated by subsurface trenching to delineate their relative activity, extent across the site, and impact on the proposed development. If warranted, appropriate building set back recommendations shall be provided.
- d. Prior to construction in areas with alluvial deposits, removal and recompaction and/or placement of surcharge fills and subdrains shall be implemented as necessary.
- e. Since the depth of the static groundwater table is unknown, a qualified engineer shall analyze the hydraulic parameters of the low-lying areas of the site and vicinity.
- f. A geotechnical investigation shall be performed to assess the deep-seated and surficial slope stability of the site. If landslide hazards are encountered, future grading plans shall be revised accordingly, even if substantial changes are entailed.

Actual development under the precise plan shall be preceded by the submittal and review of tentative maps or VTMs and development plans that shall show more precise details of grading and construction. Geological reports based on more detailed plans are required under the City's Grading Ordinance. The recommendations above, with additional ones as

appropriate, shall be implemented through requirements at the tentative map or VTM stage of development.

Prior to approval of a tentative map or VTM, the City Development Services, Development Coordination shall ensure that all additional studies have been completed and approved by the City Engineer, and appropriate mitigation measures have been made a condition of the tentative map or VTM. Implementation of these measures would reduce the geology/soils and erosion impacts to below a level of significance.

Erosion

Mitigation measures were recommended in the geotechnical reports and incorporated into the adopted precise plan. These measures also apply to the precise plan amendment and shall be incorporated into all subsequent tentative maps and VTMs, as appropriate. These measures would reduce the impact of erosion to below a level of significance. They include:

- a. Prior to grading permit issuance, a project-specific landscaping plan shall be prepared and approved by the Development Services, Landscape Section. The landscape plan shall be consistent with the City's Landscape Technical Manual. Temporary erosion-control measures to be implemented during construction include planting on disturbed and manufactured slopes within 90 days. Permanent erosion-control measures shall include complete landscaping with drought-tolerant, slope-stabilizing vegetation.
- b. Actual development under the precise plan amendment shall be preceded by the submittal and review of tentative maps or VTMs and development plans that shall show more precise details of grading and construction, including the location of the settlement and detention basins. The above mitigation measures shall be implemented as requirements for development at the tentative map or VTM stage through the City's review of more detailed development plans.

6) NOISE

No additional mitigation measures are recommended from those identified in the EIR prepared for the adopted precise plan (1994). These measures included barriers along portions of Camel Mountain Road which reduce potential noise impacts to below a level of significance.

7) PALEONTOLOGY

The approved precise plan requires that all future tentative maps and VTMs approved include a condition for the implementation of a monitoring and salvage program for the recovery of paleontological resources during development. A program for the recovery of paleontological resources during grading and earthwork shall be implemented. This program would reduce impacts to paleontological resources to below a level of significance and will include the following steps:

- a. Prior to recordation of a final major issuance of a grading permit, written verification that a qualified paleontologist and/or paleontological monitor has been retained to implement a paleontological monitoring program shall be provided to the City. Verification shall be in the form of a letter from the project applicant to the principal planner of the Environmental Analysis Section (EAS) of Development Services. A qualified paleontologist and/or paleontological monitor shall be retained to implement the monitoring program. A qualified paleontologist is defined as an individual with a Ph.D. or master's degree in paleontology or geology who is a recognized expert in the application of paleontological procedures and techniques such as screen washing of materials and identification of fossil deposits. A paleontological monitor is defined as an individual who has experience in the collection and salvage of fossil materials and who is working under the direction of a qualified paleontologist. All persons involved in the paleontological monitoring program shall be approved by EAS prior to any preconstruction meetings.
- b. The qualified paleontologist shall attend any preconstruction meetings to consult with the excavation contractor. The project applicant shall notify EAS of any preconstruction meeting dates, and of the start and end of construction. The requirement for paleontological monitoring shall be noted on the construction plans. The paleontologist's duties shall include monitoring, salvaging, preparing materials for deposit at a scientific institution that houses paleontological collections, and preparing a results report. These duties are defined as follows:
 1. **Monitoring.** The paleontologist or paleontological monitor shall be on-site during the original cutting of previously undisturbed areas of the sensitive formation to inspect for well-preserved fossils. The paleontologist shall work with the contractor and EAS to determine the monitoring locations and the amount of time necessary to ensure adequate monitoring of the project.

2. **Salvaging.** In the event that well-preserved fossils are found, the paleontologist shall have the authority to divert, direct, or temporarily halt construction activities in the area of discovery to allow recovery of fossil remains in a timely manner. Recovery is anticipated to take from one hour to a maximum of two days. At the time of discovery, the paleontologist shall contact the Environmental Analysis Section of the City of San Diego Development Services. EAS must concur with the salvaging methods before construction is allowed to resume.
3. **Preparation.** Fossil remains shall be cleaned, sorted, cataloged, and then deposited in a scientific institution that houses paleontological collections (such as the San Diego Natural History Museum).

8) BIOLOGICAL RESOURCES

Implementation of the precise plan amendment as proposed would result in the direct loss of high quality Diegan coastal sage scrub habitat. Prior to implementation of the precise plan amendment, and impacts to coastal sage scrub, the applicant must comply with the requirements of the Endangered Species Act under Section 4(d) or the Natural Community Conservation Plan/Multiple Species Conservation Program (NCCP/MSCP) as detailed in the mitigation measures below. The applicant intends to process a multiple projects 4(d) Interim Habitat Loss Permit which includes Neighborhood 10 Precise Plan Amendment, Del Mar Highlands Estates, Carmel Valley Neighborhood 10 Sewer Easement and School/Park site, and Carmel Valley Neighborhood 8C Precise Plan. The Interim Habitat Loss Findings were distributed to the public and wildlife agencies for a 45-day review period on February 28, 1997, consistent with the City's NCCP Process Guidelines. Public review will end on April 24, 1997. The projects are all on different expedited processing schedules and will be considered individually by the Planning Commission and City Council as part of the discretionary process for each project.

The applicant has also agreed to implement the following mitigation measures which would reduce biological impacts associated with the precise plan amendment to below a level of significance. These measures are part of the broader mitigation requirements outlined in the 4(d) Interim Habitat Loss Permit Findings that address Neighborhood 10 Process Plan as well as Del Mar Highland Estates and Carmel Valley Neighborhood 8C.

- a. Mitigation for the impacts to 12.8 acres of high quality coastal sage scrub, 4.5 acres of disturbed coastal sage scrub, 1.3 acres of coastal sage scrub/native grasslands, 0.8 acre of coastal sage scrub/annual grassland, and 0.3 acre of southern willow scrub would be accomplished by the applicant's contribution to the City of San Diego for the acquisition of a corresponding portion of an 80-acre off-site property, known as Mesa Top, within the Neighborhood 8A precise plan area. Neighborhood 8A lies to the west of Neighborhood 10. The entirety of this off-site property consists of very high quality coastal sage scrub vegetation and southern maritime chaparral with numerous sensitive plant and animal species and is an integral component of the Draft MSCP as a part of the Carmel Mountain biological core area. The City of San

Diego considers the Mesa Top parcel to be a critical acquisition parcel and the monetary contribution would greatly improve the City's ability to complete the acquisition in a timely manner.

- b. The on-site revegetation of approximately 37 acres of the manufactured slopes adjacent to natural open space resulting from the precise plan amendment adjacent to natural open space will also be required. As required in the previously approved revegetation plan for Neighborhood 10 (City of San Diego 1993b) the revegetation will utilize coastal sage scrub plant species suitable for use as habitat for the gnatcatcher and other species which occur in coastal sage scrub. Maintenance and monitoring of the revegetated slopes will be conducted to ensure habitat establishment and determine compliance with success criteria set forth in the revegetation plan. Implementation of the revegetation plan and the maintenance and monitoring of the revegetated slopes has been assured through the posting of a bond(s) prior to issuance of the grading and Interim Habitat Loss Permits.
- c. Approval of the precise plan will also require the implementation of a brush management plan through the master homeowner's associations for the proposed amended VTMs and any future VTMs that minimizes impacts to the habitat quality of the native vegetation nearest development areas which must be thinned for fire protection purposes. The native vegetation will be trimmed by hand rather than removed altogether, and fuel build-up in the brush management zone surrounding development areas will be reduced by hand clearing away of dead branches and undergrowth rather than periodic discing or grading. To further minimize potential impacts to sensitive plant and wildlife species, brush management activities will not be conducted during the breeding season (between March 15 and August 15). A qualified biologist shall be on-site during brush management activities to ensure that, to the fullest extent possible, brush management activities avoid impacts to sensitive plant species and will not directly impact sensitive wildlife species. In Zone 2, 30 percent native plant cover over 18 inches high shall be retained, and in Zone 3, a 40-60 percent gradient of native plant cover over 18 inches high shall be retained. No non-native planting or irrigation shall be used or allowed in Zones 2 or 3. These measures will be noted on the grading plan prior to the issuance of the grading and interim habitat loss permits. The ongoing brush management requirement and continued funding for the biologist will be assured through the future homeowner's association agreement which shall be established by the applicant. Ashy spike-moss and San Diego barrel cactus will not be removed while thinning for Zones 2 and 3.
- d. Staking and monitoring of grading activities for the precise plan amendment by a qualified biologist will also be implemented prior to and during the grading operations to ensure no unanticipated impacts to sensitive habitats or species occur within the areas shown for permanent open space. Prior to clearing or grading of the precise plan, an Interim Habitat Loss Permit would need to be obtained.

VII. PLAN IMPLEMENTATION
CARMEL VALLEY - NEIGHBORHOOD 10 PRECISE PLAN

- e. No clearing or grading of native habitat shall occur during the gnatcatcher breeding season (March 1-August 15).
- f. The salvage and transplanting of San Diego barrel cactus within the areas to be disturbed by grading per the precise plan amendment shall be required prior to the issuance of a grading permit.
- g. Lighting at perimeter lots adjacent to the open space shall be selectively placed, shielded, and directed away from that habitat.
- h. Fencing along property boundaries facing the open space corridors shall be designed and constructed of materials that are compatible with the open space corridors and shall be installed by the developer prior to the occupancy of the units in order to ensure uniformity.

9) BRUSH MANAGEMENT

The brush management mitigation measures associated with the precise plan amendment for brush management are described below:

- a. Any brushing activity that would impact coastal sage scrub habitat shall not occur during the breeding season (March 1-August 15) to avoid impacts to breeding California gnatcatchers. The biologist on duty for the applicant/developer shall verify this condition through written notification of the Development Services Director. This condition shall be included on all future TMs, VTMs, and grading and landscaping plans.
- b. Approval of the precise plan shall require that the proposed amended VTMs and future VTMs minimize impacts to native vegetation nearest development areas which must be thinned for fire protection purposes. This condition shall be included on all future TMs, VTMs, and grading and landscaping plans. The native vegetation will be trimmed by hand rather than removed altogether, and fuel build-up in the brush management zone surrounding development areas will be reduced by hand clearing away of dead branches and undergrowth rather than periodic discing or grading. To further minimize potential impacts to sensitive plant and wildlife species, brush management activities will not occur during the non-breeding season (between March 1 and August 15). The duty of the biologist will be to ensure that brush management activities avoid impacts to sensitive plant species (i.e., ashly spike-moss and barrel cactus) and will not directly impact sensitive wildlife species. In Zone 2, 30 percent native plant cover over 18 inches high shall be retained, and in Zone 3, a 40-60 percent gradient of native plant cover over 18 inches high shall be retained. No non-native planting or irrigation shall be used or allowed in Zone 2 or 3. These measures will be noted on the grading plan prior to the issuance of the grading and Interim Habitat Loss Permits. The ongoing brush management requirement and continued funding for the biologist will be assured through the future homeowner's association agreement which shall be established by the

applicant. Brush management in revegetated areas shall commence at the end of the revegetation monitoring period, or as specified by the revegetation biologist.

10) PUBLIC SERVICES

Schools

Currently a Mello-Roos District has been formed pursuant to the requirements of the approved precise plan and participation in the Mello-Roos District shall be included in the currently proposed precise plan amendment. The City of San Diego Development Services Department shall assure this is in the precise plan amendment prior to plan approval.

Participation in the established Mello-Roos District would mitigate the amended precise plan direct impacts on educational services to a level less than significant. The project's contribution to the cumulative impact would be mitigated upon provision of adequate facilities, as defined by the General Plan, to accommodate the students.

VIII. CARMEL VALLEY COMMUNITY PLAN CONFORMANCE

Neighborhood 10 is planned based upon the goals and proposals set forth in the Carmel Valley Community Plan. This chapter addresses the conformance of the Neighborhood 10 Precise Plan to the Community Plan on a general or conceptual basis.

A. CARMEL VALLEY COMMUNITY PLAN GOALS

1. *"To establish a physical, social and economically balanced community."*

Neighborhood 10 will contain housing in the rural residential, very low and low density ranges as specified in the Community Plan. A variety of product types and prices will be available. An internal transportation system linked to the community-wide network will ensure mobility and access to all parts of the neighborhood and the community.

2. *"To establish self-containment and feeling of community identity among the future residents of Carmel Valley."*

Neighborhood 10 will be an identifiable development unit of Carmel Valley. The Neighborhood Commercial Center and school/neighborhood park complex will provide a sense of community identity.

3. *"To preserve the natural environment."*

Of the total 806.2 acres contained in the Neighborhood 10 Precise Plan area, approximately 358.1 acres (44.4%) will be preserved as natural open space. Residential development is concentrated in the developable portions of the plan area. The Precise Plan will preserve the open space areas in their natural state and minimize visual impacts of project development.

4. *"To establish a balanced transportation system which is used as a tool for shaping the urban environment."*

Carmel Country Road, Carmel Mountain Road and Street A will provide public access from Neighborhood 10 to the entire Carmel Valley community. The internal collector street system will provide a visually enhanced streetscene. The bicycle/pedestrian trail system will provide access from Neighborhood 10 to major regional open space systems and community facilities.

5. *"To establish a realistic phasing of development within the community based on maximum utilization of the privately financed public facilities."*

The Precise Plan provides for the installation of public facilities by property owners as required for residential development. Financing of an adequate circulation system, and necessary public facilities will be detailed in the Public Facilities Financing Plan.

B. PRECISE PLAN DEVELOPMENT CRITERIA

The Carmel Valley Community Plan provides guidelines for the contents and preparation of Precise Plans for development units. These guidelines are restated below, followed by a brief discussion of compliance by this Precise Plan.

1. The development unit Precise Plan *"must be in general conformance with the Carmel Valley Community Plan objectives and proposals in terms of overall density, neighborhood concepts, major open space delineation and major and collector street patterns."*

As indicated in maps and text, the Precise Plan is in substantial conformance with the objectives and proposals of the Carmel Valley Community Plan.

2. The Precise Plan must *"illustrate the complete circulation system, including local streets and transit, and further indicate how the system will relate to the total Carmel Valley Circulation System."*

Section V describes the complete circulation network, including the neighborhood street system, street design, alternative transportation facilities and parking. Ties to the total Carmel Valley System also are discussed.

3. The Precise Plan must *"illustrate a system of separate bicycle and pedestrian pathways linking the neighborhood center with the residential areas and open space system and also illustrate how these pathways can link to the town center."*

Section V and Figure 16A depict the bicycle and pedestrian pathways within the Precise Plan area. Class II bicycle lanes in Neighborhood 10 will follow Carmel Country Road, Carmel Mountain Road, and Street A, providing access to other neighborhoods within Carmel Valley.

4. The Precise Plan must *"contain data describing the housing balance projected regarding the quantity and/or proportion of low and moderate income housing, as well as a plan describing efforts to be made to maintain an ethnic and racial balance."*

Section II addresses residential location and mix, as well as efforts to contribute to housing community-wide.

5. The Precise Plan must *"contain a detailed design plan for the layout of the neighborhood center including shopping area and uses, neighborhood school and park; the City and local school district must agree to the sites and design of the facility."*

Section VI provides necessary design guidelines for the neighborhood commercial center and school/neighborhood park complex.

6. The Precise Plan must *"illustrate the timing of necessary public facilities through the Assessment District and fees approach to serve the development."*

Section VII discusses the phasing and financing of public facilities to be provided through the Public Facilities Financing Plan.

7. The Precise Plan must *"contain an environmental impact statement."*

EIR (DEP No. 91-0803) and Supplemental EIR (LDR Nos. 96-0736 and 96-0737) for Neighborhood 10 accompanies this document.

NEIGHBORHOOD 10

APPENDIX "A"

DEVELOPMENT SUITABILITY ANALYSIS

APPENDIX A

The placement of the additional 128 dwelling units in this Precise Plan Amendment has been directed by a Development Suitability Analysis which has been prepared for all of the property within the Precise Plan which has not already been approved for development. The Development Suitability Analysis identifies areas of high, medium and low development potential.

In the preparation of the original approved Precise Plan, all of the areas of high development potential were approved for development, and most of the area of medium and low development potential were preserved as open space. In determining the most appropriate location for the additional 128 dwelling units in this Precise Plan Amendment, those remaining areas of neighborhood with the highest development were selected.

DEVELOPMENT SUITABILITY ANALYSIS

NEIGHBORHOOD 10 PRECISE PLAN AMENDMENT

An Amendment to the Neighborhood 10 Precise Plan has been proposed to add 128 detached single family dwelling units. This Development Suitability Analysis has been prepared to identify the most appropriate location to place those additional dwelling units.

This analysis evaluates those portions of Neighborhood 10 that have not already been approved for development, and ranks those areas as either medium or low development potential. In the preparation of the original Neighborhood 10 Precise Plan, most development was approved in areas of high development potential. This Development Suitability Analysis evaluates those areas remaining and ranks their relative appropriateness for development.

The relative suitability of the remaining property for development is based upon environmental resources including wetlands, wetland buffers, flood plains or major drainage courses, hillsides, biologically sensitive lands and significant prehistoric and historic sites. These resources are graphically displayed on Figures 5 and 6 of the Precise Plan. Relative suitability of the remaining property for development is also based upon factors such as visual resources, topography, public facilities needs, public safety issues and adjacent land uses.

Please refer to the attached figure defining the suitability conclusions. The analysis concludes there are two categories of development suitability: medium and low. That is because most of the area in Neighborhood 10 not proposed for development in the original approved Precise Plan contains slopes greater than 25% and/or some sensitive environmental resources. However, it is possible to rank the relative appropriateness of the remaining property.

The areas on the attached figure ranked with low development potential have resulted from the presence of most of the following circumstances:

- (1) Major drainage course
- (2) Floodplain
- (3) Wetland (as defined by R.P.O.)
- (4) Neighborhood 10 wildlife corridor as defined in Precise Plan
- (5) Visibility from Penasquitos Canyon Preserve
- (6) Agreed upon canyon width with US Fish and Wildlife Service
- (7) Some presence of 25% hillside slope
- (8) Presence of sensitive native biological resources
- (9) Difficulty to serve with roads and or utilities

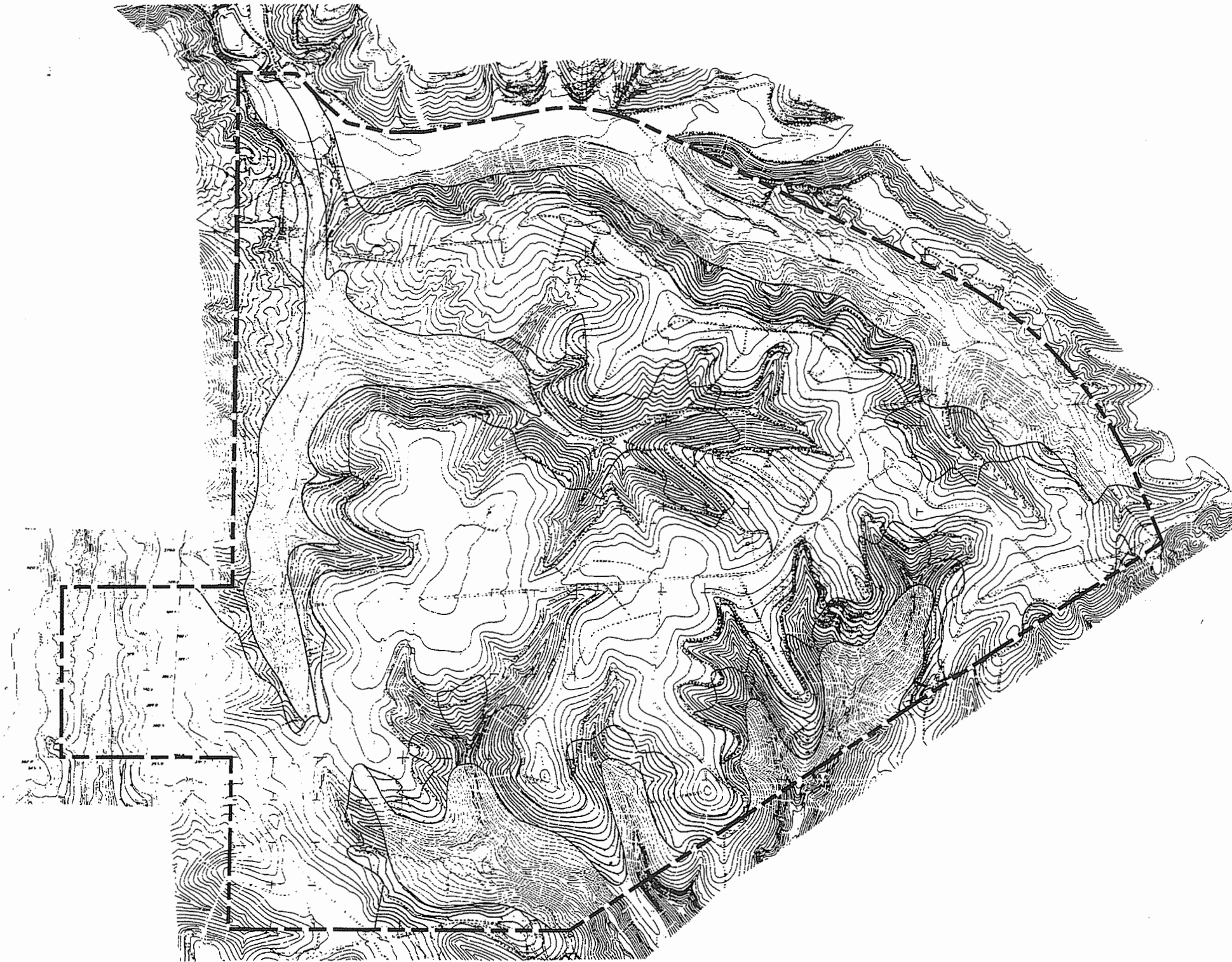
The areas on the attached figure ranked with medium development potential have resulted from all of the following circumstances:

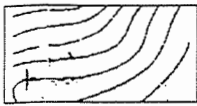
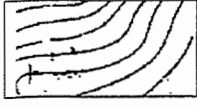
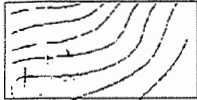
- (1) Absence of major drainage courses
- (2) Absence of floodplain
- (3) Absence of wetland or wetland buffer
- (4) Absence of wildlife corridor as defined in Precise Plan
- (5) No visibility from major portion of Penasquitos Canyon Reserve
- (6) Not part of agreed upon canyon width with US Fish and Wildlife Service
- (7) Ability to serve with ease with roads and utilities
- (8) Appropriate adjacent land uses (similar detached single family residential)

It should be noted that the areas ranked with moderate development potential did not receive a ranking of high development potential because of the presence in all cases of some 25% hillside slope and native biological resources.

All of the 128 additional dwelling units proposed in this Precise Plan Amendment have been placed in the areas defined as medium development potential. They are the most appropriate and logical areas for the additional dwelling units.

DEVELOPMENT SUITABILITY ANALYSIS



-  APPROVED FOR DEVELOPMENT
-  MODERATE DEVELOPMENT POTENTIAL
-  LOW DEVELOPMENT POTENTIAL

PRECISE

P . L . A . N ■ CARMEL VALLEY/NEIGHBORHOOD 10

NEIGHBORHOOD 10

APPENDIX "B"

COASTAL ZONE STANDARDS

**§ 62.0419 in the City of San Diego Municipal Code
and
City Clerk Document No. 00-17068**

A. PROTECTING WATER QUALITY IN COASTAL LAGOONS

(See §62.0419 in the City of San Diego Municipal Code)

In addition to compliance with other requirements of this Division, grading for properties within the Coastal Zone which drain into Los Penasquitos Lagoon or San Dieguito Lagoon requires compliance with erosion control measures specified within the document, "Erosion Control Measures for North City Areas Draining to Los Penasquitos or San Dieguito Lagoons," on file in the office of the City Clerk as Document No. OO-17068.

(Amended 11-23-92 by O-17864 N.S.)

B. CITY CLERK DOCUMENT NO. 00-17068

1. EROSION CONTROL MEASURES FOR NORTH CITY AREAS DRAINING INTO LOS PENASQUITOS OR SAN DIEGUITO LAGOONS

Land development for properties within the Coastal Zone which drain into Los Penasquitos Lagoon or San Dieguito Lagoon shall comply with the following erosion control measures.

- a. A grading plan that incorporates runoff and erosion control procedures to be utilized during all phases of project development shall be prepared and submitted concurrently with subdivision improvement plans or planned unit development plans where such development is proposed to occur on lands that will be graded or filled. Such a plan shall be prepared by a registered civil engineer and shall be designed to assure that there will be no increase in the peak runoff rate from the fully developed site over the greatest discharge that would occur from the existing undeveloped site as a result of the intensity of rainfall expected during a six-hour period every ten years (the "six-hour, ten year design storm"). Runoff control shall be accomplished by establishing on-site or at suitable nearby locations catchment basins, detention basins, and siltation traps along with energy dissipating measures at the terminus of storm drains, or other similar means of equal or greater effectiveness.
- b. Sediment basins (debris basins, desilting basins, or silt traps) shall be installed in conjunction with the initial grading operations and maintained throughout the development process as necessary to remove sediment from runoff waters draining from the land undergoing development. Areas disturbed but not complete prior to November 15, including graded pads and stockpiles shall be suitably prepared to prevent excessive soil loss during the late fall and winter seasons. All graded slopes shall be stabilized prior to November 15, by means of native vegetation, if feasible, or by other suitable means. The use of vegetation as a means to control site erosion shall be accomplished pursuant to plans and specifications prepared by a licensed

APPENDIX B - COASTAL ZONE STANDARDS

CARMEL VALLEY - NEIGHBORHOOD 10 PRECISE PLAN

landscape architect or other qualified professional. Erosion control using vegetation may include but is not limited to, seeding, mulching, fertilization, and irrigation within sufficient time prior to November 15 to provide landscape coverage that is adequate to achieve the provisions of this policy. Temporary erosion control measures shall include the use of berms, interceptor ditches, sandbagging, hay bales, filtered inlets, debris basins, silt traps, or other similar means of equal or greater effectiveness. From November 15 to March 31, grading may be permitted provided the applicant conforms to the requirements of subsection C and submits monthly documentation within two weeks following the end of the preceding month to the City Engineer of the condition of the erosion control procedures for graded pads, slopes and stockpiles whenever precipitation during the month exceeds two (2) inches.

- c. From November 15 to March 31, grading may occur in phased increments as determined by the City Engineer provided all of the following requirements have been met:
- 1) The increments shall be limited to those areas that have been prepared to control the effects of soil erosion. Control measures, such as sedimentation basins, detention basins and other facilities, shall be scheduled and placed in a sequence that shall minimize and control the off site transportation of sediments. Such erosion control measures shall be installed for such increments prior to commencing any grading that would be performed between the period between November 15 and March 31.
 - 2) Detention basins and other control measures employed shall be designed to assure that there will be no increase in the peak runoff rate from the fully developed site over the greatest discharge that would occur from the existing undeveloped site as a result of the intensity of rainfall expected during a six-hour period once every ten years (the "six-hour, ten-year" design storm).
 - 3) The applicant shall post a bond, for such areas to be graded, which shall remain in force and effect for one year after acceptance by the City. The bond shall be sufficient to cover the costs of any remedial grading and replanting of vegetation, including any restoration of lagoon, wetland, or other environmentally sensitive habitat areas adversely effected by the failure of the erosion control measures required pursuant to subsection C.2 above, as determined by the City Engineer. The bond will inure to the benefit of the City in case of noncompliance as determined by the City Engineer.
 - 4) The applicant agrees to provide daily documentation to the City Engineer of the condition of the erosion control procedures for any 24-hour period in which precipitation exceeds 0.25 inches. Such documentation shall be provided within five working days of said 24-hour period. Failure to provide

such documentation of the occurrence of any significant discharge of sediments or silts in violation of this policy shall constitute automatic grounds for suspension of the applicants grading permits during the period of November 15 to March 31.

2. EROSION CONTROL MONITORING PROGRAM FOR NORTH CITY AREAS DRAINING INTO LOS PENASQUITOS OR SAN DIEGUITO LAGOONS

- a. Overall field review of grading operations will be performed by the City Resident Engineer on each grading project in the Coastal Zone.
- b. Field review of erosion control devices, sedimentation basins, detention basins, and landscaping will be made by the City Engineer prior to the advent of the rainy season, and throughout the rainy season as necessary to monitor grading operations phased between November 15 and March 31. The City Engineer shall prepare a periodical report documenting the compliance of all individual projects with the grading and erosion control requirements. The report will be completed as of November 15 of each year.
- c. The City Engineer will periodically review and prepare a report on the effectiveness of the runoff and erosion control measures established for the North City areas within the Coastal Zone that drain into Los Penasquitos or San Dieguito Lagoons. The initial report shall be completed within two years following the adoption of the erosion control measures and thereafter six months prior to any scheduled review by the California Coastal Commission of the Local Coastal Program for the City of San Diego. A copy of the report shall be submitted to the Executive Director of the Coastal Commission.

