



THE CITY OF SAN DIEGO

Report to the Planning Commission

DATE ISSUED: April 9, 2020 REPORT NO. PC-20-023

HEARING DATE: April 16, 2020

SUBJECT: 4TH AVENUE APARTMENTS, Process Five Decision

PROJECT NUMBER: [588751](#)

REFERENCE: [Item 203, City Council Meeting on March 9, 2020](#)

OWNER/APPLICANT: Fourth and Laurel, a California limited partnership, Owner / Richard Simis, Applicant

SUMMARY

Issue: Should the Planning Commission recommend to City Council to approve an override of the determination of inconsistency with the Airport Land Use Compatibility Plan for San Diego International Airport with respect to residential density for the construction of a 36-unit, mixed-use residential development on a vacant lot located at 2426 4th Avenue on 4th Avenue within the Uptown Community Planning Area?

Staff Recommendation: Recommend to City Council to **Approve** Site Development Permit No. 2243451.

Community Planning Group Recommendation: On October 2, 2018, the Uptown Planners voted to 15-1-1 to recommend approval of the Project with no conditions (Attachment 7).

Environmental Review: The project required the preparation of a Mitigated Negative Declaration in accordance with the California Environmental Quality Act (CEQA) for potential noise (construction-related) impacts. On March 9, 2020, City Council voted 9-0 to adopt [Mitigated Negative Declaration No. 588751](#) and the Mitigation, Monitoring, and Reporting Program. The scope of the subject hearing only includes the project, and not the environmental determination.

Fiscal Impact Statement: All costs associated with the processing of this application are recovered through a deposit account funded by the applicant.

Code Enforcement Impact: None with this action.

Housing Impact Statement: The Uptown Community Plan designates the project site Community Commercial with a residential density of 0 to 109 dwelling units per acre (du/ac). The total site acreage is 0.23 acres and would allow for a maximum of 26 dwelling units. The proposed project would construct a new mixed-use building containing 36 multi-family residential units on a vacant lot located at 2426 4th Avenue. The project would provide four “very low-income household” residential units. The four affordable housing units will be income-restricted dwelling units affordable to households with a combined annual gross income at or below 50 percent of the Area Median Income (AMI), subject to an affordable housing agreement with the San Diego Housing Commission. The inclusion of very low-income affordable units allows for a housing density bonus of 50 percent through the Affordable Housing Regulations. The proposed project’s use of the Affordable Housing Regulation’s density bonus results in 10 additional units for a total of 36 residential units, including the affordable units, on the project site.

BACKGROUND

The 4th Avenue Apartments project (Project) is a proposed 36-dwelling unit mixed-use development, including four very low-income dwelling units, on a vacant lot located at 2426 4th Avenue within the [Uptown Community Plan](#) area (Attachment 1). The 0.23-acre Project site is zoned CC-3-9, a Community Commercial zone, which allows a density of one dwelling unit per 400 square feet. The community plan designates the site Community Commercial at a density range between 0 to 109 dwelling units per acre (du/ac) (Attachment 2). The site is also located in the Very High Fire Hazard Severity Overlay Zone, Community Plan Implementation Overlay Zone (CPIOZ-A), Residential Tandem Parking Overlay Zone, the Transit Area Overlay Zone, the Transit Priority Area, Airport Influence Area for San Diego International Airport (SDIA) – Review Area 1, SDIA 60-65 Community Noise Equivalent Level (CNEL), SDIA Safety Zone 3NE, Federal Aviation Administration (FAA) Part 77 Notification Area for SDIA and North Island Naval Air Station (NAS), Airport Environs Overlay Zone (AEOZ), and the Airport Approach Overlay Zone (AAOZ) for SDIA.

On May 16, 2018, the applicant submitted an application for a Process Two Neighborhood Development Permit (NDP) for the construction of a mixed-use development consisting of 15 percent very-low income dwelling units, 50 percent density bonus and requests for deviations and development incentives in accordance with the [Affordable Housing Regulations](#) and the [Affordable Housing, In-Fill Projects, and Sustainable Buildings Development Regulations](#). During the review of the Project, City staff confirmed the Project does not require a NDP as the Project’s requested deviations from the development standards do not exceed the maximum allowable development incentives. However, City staff determined the Project exceeds the maximum allowable residential density of 22 units per the residential factor established by the [Airport Land Use Compatibility Plan \(ALUCP\) for San Diego International Airport \(SDIA\)](#) for a mixed-use project, and the applicant requested to proceed rather than redesigning the Project.

The Project was submitted to the San Diego County Regional Airport Authority (SDCRAA), serving as the Airport Land Use Commission (ALUC) for a determination of consistency. On October 4, 2018, the ALUC determined that the proposed 36 residential units exceeds the maximum compatible density of 22 units and is not compatible with the mixed-use project compatibility criteria of the

SDIA ALUCP. The ALUCP was adopted by the ALUC and is intended to provide for the orderly development of the SDIA and the area surrounding the SDIA and protect public health, safety and welfare in areas around the SDIA. Pursuant to the [California Public Utilities Code \(CPUC\) Section 21675.1\(d\)](#), the City Council has the authority to overrule the ALUC's determination. Accordingly, the applicant requested an Overrule Hearing before the City Council. The [California Public Utilities Code \(CPUC\) Section 21676.5\(a\)](#) requires two hearings for any decision to overrule a determination of inconsistency, with the initial hearing to consider a proposed decision whether or not to overrule, and a second hearing for a final decision whether to overrule the ALUC. On March 9, 2020, the City Council voted 9-0 to adopt Mitigated Negative Declaration No. 588751 and the Mitigation, Monitoring, and Reporting Program ([Resolution No. 312873](#)), and to propose to overrule, at a second public hearing in accordance with the CPUC section 21676.5(a) and [San Diego Municipal Code \(SDMC\) Section 132.1555](#), the ALUC's determination of inconsistency ([Resolution No. 312874](#)) (Attachment 9).

The applicant is also requesting a Process Five, Site Development Permit (SDP), pursuant to [SDMC Sections 132.0310](#) and [126.0502\(e\)\(2\)](#), for an override of the determination of inconsistency with the land use recommendations of the SDIA ALUCP made by City staff. In accordance with [SDMC 112.0509\(b\)](#), the SDP application is before the Planning Commission for a recommendation to City Council, prior to the second City Council hearing for a final decision on the Project, including the overrule request and SDP in accordance with CPUC Section 21676.5(a) and [SDMC Section 132.1555](#).

Site Details: The Project site was previously developed with a warehouse structure and demolished in 2005. The site slopes to the south with elevations ranging from 237 feet above mean sea level (AMSL) at the north to approximately 233 feet AMSL to the south. The site is located within an urbanized community surrounded by one- to five-story residential and commercial buildings, including multi-family apartments and commercial establishments to the south, apartments and cottages to the west, commercial/office uses to the north, and high density residential mixed-use development to the east. Balboa Park is located two blocks further east (Attachment 3).



4th Avenue Apartments Project Site

DISCUSSION

Project Description: The Project proposes the construction of a six-story, mixed-use building approximately 73 feet tall (above ground level) with 36 dwelling units, an approximately 1,174 square-foot retail suite, parking, and associated site improvements. The Project site could accommodate a maximum of 26 dwelling units based on the underlying zone and community plan land use designation. The Project includes four very low-income dwelling units, which equates to 15 percent of the base units allowed per the underlying zone, affordable to households with a combined annual gross income at or below 50 percent of the Area Median Income (AMI), subject to

an affordable housing agreement with the San Diego Housing Commission. Pursuant to the City's Affordable Housing Regulations, the inclusion of 15 percent very low-income affordable units allows for a housing density bonus of 50 percent, or up to 39 dwelling units, and five development incentives. Consistent with these regulations, the Project proposes a total of 36 dwelling units and five allowable incentives.

The residential component would be located within five levels over a concrete podium with one level of parking at ground level and one subterranean parking level with a total residential building area of 38,132 square feet. The residential product mix would include 18 studios, 2 loft units, 10 one-bedroom units, and 6 two-bedroom units for a total of 36 dwelling units ranging in size from 549 square feet to 1,178 square feet. The commercial



component would consist of a single 1,174 square-foot retail suite with access provided at the ground level from 4th Avenue. Proposed on-site amenities include a ground level lobby, 1,128 square feet of total common open space area, including 539 square feet located on the second level, and 589 square feet with barbecue and seating areas located on the sixth level, as well as tenant storage located on the first level.

The Project would provide 21 secured parking spaces, 4 motorcycle spaces, 15 bicycle spaces, and 3 electrical vehicle spaces for the residential units. Three of the 24 automobile parking stalls within the parking structure would be reserved for the retail suite. All the residential parking spaces would be covered podium areas underneath the residential component. Access to the Project site would be provided via a gated garage entrance along the 4th Avenue frontage. The driveway would serve to access the parking areas for both the commercial and the residential components of the Project. The Project also proposes on-site infrastructure improvements such as connections to off-site utilities (existing 12-inch water line and 12-inch sewer line) in 4th Avenue adjacent to the site. In addition, connections to the storm drain system would also be provided consistent with the City's Storm Water Regulations and be directed into appropriate storm drain systems designated to carry surface runoff that has been reviewed and accepted by City Engineering staff.

The Project's landscape design concept includes a softscape comprised of a drought tolerant plant palette and an automatic drip irrigation system. Most of the frontage along 4th Avenue would be landscaped, except for the entrance to the parking. At the street level, the Project will use trees to line 4th Avenue, along with shrub and ground covers to buffer the street impact. All tree and shrub sizes and quantities would meet or exceed all City of San Diego requirements. Furthermore, all landscape areas would be maintained by the Property Association.

Project-Related Issues:

FAA Determination - The Project is located within the Airport Influence Area for SDIA (Review Area 1), FAA Part 77 Notification Area for SDIA and North Island NAS, and the Airport Approach Overlay Zone. On July 18, 2017, and extended on February 7, 2019, the FAA issued a determination of no hazard to air navigation (Aeronautical Studies No. 2017-AWP-1060-OE, 2017-AWP-1331-OE, 2017-AWP-1332-OE, 2017-AWP-1333-OE, 2017-AWP-1341-OE, 2017-AWP-1342-OE, 2017-AWP-1343-OE, 2017-AWP-1344-OE). On November 6, 2018, the FAA issued a determination of no hazard to air navigation for the temporary crane structure (Aeronautical Study No. 2018-AWP-13477-OE). The determinations were issued with the condition that the structures be marked/lighted in accordance with FAA requirements.

The project has been conditioned to require the structures to be marked and lit according to FAA procedures. The Project has also been conditioned to record an aviation easement for airspace with the County Recorder (Attachment 6, Conditions 18-20). The aviation easement would also satisfy the ALUCP's overflight notification requirement that the Project is located in an area with the overflight of aircraft.

ALUC Determination - Per the CPUC, the ALUC is required to prepare an ALUCP for each airport in the County. These plans are meant to promote compatibility between the airports and the surrounding land uses in order to provide for the orderly development and protect the public health safety and welfare of the area. The compatibility policies and standards pertain to four airport-related factors: noise, safety, airspace protection and overflight. ALUC reviews projects for consistency with these plans. The reviews are triggered by projects that are within the Airport Influence Area, and propose a change of use, or increase intensity or building height. The ALUCP for SDIA limits residential density and non-residential intensity for proposed uses by safety zone and neighborhood based on the average existing development intensity and density. Residential uses are measured by dwelling units per acre and commercial uses are measured by people per acre. For mixed-use development, the ALUCP allows for residential density to be converted to intensity measured by people per acre, based on a people per household factor in the ALUCP, which is then added to the commercial intensity for the total project intensity. A maximum of 50 percent of the maximum site intensity criteria ([Policy S.8](#)) may be applied for residential portion of a mixed-use development. The residential portion of the Project exceeds that threshold.

The Project is in the ALUCP's Uptown Safety Zone 3NE. The overall ALUCP people per acre intensity for that zone is 278 people per acre, which equates to 64 people for the 0.23-acre site. The residential portion of the Project cannot exceed 50 percent, which is 32 people (64/2). The ALUCP allows a maximum of 22 dwelling units, using the ALUCP factor of 1.48 people per household (32/1.48). The Project proposes 36 dwelling units, which exceeds the maximum compatible threshold of 22 dwelling units, as determined by the ALUC at their [October 4, 2018 meeting](#) (Attachment 8).



The other 32 people that comprise the nonresidential half of the mixed-use project is multiplied by the occupancy factor in the ALUCP to determine the maximum compatible commercial use. Retail sales has an occupancy factor of 170 square feet per person, which yields a total maximum area of 5,440 square feet as compatible with the ALUCP (32x170 = 5,440). The Project's commercial component of approximately 1,174 is significantly less than the compatibility threshold. The Project would have a total intensity of 60 people including 53 residents (36 units x 1.48) and 7 retail space occupants (1,174 square feet/170 square feet/person), which does not exceed the 64 people ALUCP limitation for the site.

California Public Utilities Code (CPUC) section 21670 directs Caltrans to prepare an Airport Land Use Planning Handbook (Handbook) to provide guidance to local ALUCs when preparing an ALUCP. Within highly density areas with high-rise and mid-rise buildings, the Handbook allows for infill development at the average density and intensity of existing uses. The Project's proposed land use intensity is consistent with the existing average intensity for the high density residential and commercial mixed-use developments in the immediate area as shown in the following table and Attachment 9. The existing office, residential and mixed-use higher intensity developments shown in the table have an average of 351 people per acre. The Project would have an intensity of 261 people per acre (60 people/0.23 acres); and therefore, the Project is consistent with the Handbook and CPUC section 21670.

Comparison between Existing Development and the Project							
Name	Address	Lot Size	Stories	Use	Non-Res Floor Area	Units	People per Acre
5 th & Laurel Financial Centre	2550 5 th Ave	1.38 Acres	18	Office	160,000	0	539
5 th & Laurel Corporate Center	2445 5 th Ave	0.36 Acres	6	Office	40,000	0	517
Imperial Towner Apartments	2399 5 th Ave	0.46 Acres	14	Mixed-Use	13,000	89	420
4th Avenue Apartments	2455 4th Ave	0.23	6	Mixed-Use	1,174	36	261
2400 6 th Apartments	2400 6 th Ave	0.35 Acres	11	Multi-Family	0	60	254
Laurel Bay	2400 5 th Ave	1.38 Acres	5	Mixed-Use	18,000	150	238
Park Laurel	2515 5 th Ave	1.34 Acres	14	Mixed-Use	10,000	94	139

AEOZ Overlay Zone – The Project site is located within the Airport Environs Overlay Zone (AEOZ). The purpose of the AEOZ is “to provide supplemental regulations for property surrounding SDIA.” The intent of the regulations is to ensure land uses are compatible with airport operations by implementing the ALUCP for SDIA, provide a mechanism to inform property owners of noise impacts

and safety hazards associated with their property's proximity to aircraft operations, and to ensure California Administrative Code Title 21 provisions for incompatible land use are satisfied. Although the ALUCP for SDIA identifies residential and commercial uses as compatible uses for the site of the proposed development Project, as explained above, the maximum compatible number of units for a mixed-use development on the 0.23-acre project site is 22 based on the intensity factor established by the ALUCP. The applicant is requesting an override of the determination of inconsistency with respect to Project's density pursuant to SDMC Section 132.0310 to allow the development of a 36-unit mixed-use development at the subject location. The City Council, may, by a two-thirds vote, override the determination of inconsistency with the ALUCP and determine that the proposed use meets the intent of the ALUCP if the City Council makes the required findings to approve a Site Development Permit set forth in SDMC section 126.0505 and the supplemental findings set forth in SDMC section 132.0310(b). Staff has provided draft Site Development Permit resolution with findings supporting City Council override of the determination of inconsistency with the SDIA ALUCP (Attachment 5).

Requested Incentives – By including 15 percent of the base units for very low-income households, the Project is allowed five development incentives under the provisions of the Affordable Housing Regulations. Pursuant to [SDMC Section 143.0740](#), an incentive can mean a deviation to a development regulation. The applicant is requesting that five incentives be used for five deviations for the Project. The SDMC further states that: "Upon an applicant's request, development that meets the applicable requirements of Section 143.0720 shall be entitled to incentives pursuant to Section 143.0740 unless the City makes a written finding of denial based on substantial evidence, of any of the following:

- (A) The incentive is not required in order to provide for affordable housing costs, as defined in California Health and Safety Code Sections 50052.5 and 50053;
- (B) The incentive would have a specific adverse impact upon public health and safety as defined in Government Code section 65589.5, the physical environment, including environmentally sensitive lands, or on any real property that is listed in the California Register of Historical Resources and for which there is no feasible method to satisfactorily mitigate or avoid the specific adverse impact without rendering the development unaffordable to low income and moderate-income households;
- (C) The incentive would be contrary to state or federal law. Requested incentives shall be analyzed in compliance with the California Environmental Quality Act as set forth in Chapter 12, Article 8, and no incentive shall be granted without such compliance.

The incentives must be granted unless the City makes any one of the above findings based upon substantial evidence. The Project requests incentives for five deviations from the development regulations, which are summarized in the following table:

Incentives		
SDMC Section	Required/Allowed	Proposed
Maximum Building Height specified by the AAOZ Section 132.0205	275 feet	Allow the maximum building height to exceed the AAOZ height limit by 35.85 feet.
Maximum Building Height specified by the CPIOZ-A Section 132.1401, Diagram 132.14K	65 feet above ground level	Allow the maximum building height to exceed the CPIOZ height limit by 8.15 feet.
Ground Floor Restriction Section 131.0540(c)(1)	Commercial uses along front half of property	Lobby area (accessory residential use) within front 30-feet of the property
Private Exterior Open Space Section 131.0455(d)	All residential units provide a minimum area of 50 square feet of private exterior open space	Five dwelling units without private exterior open space
Side and Rear Yard Setbacks Section SDMC 131.0543(b)	Structure must be placed at zero- or 10-foot Side and Rear Yard Setbacks	1.625 feet Side Yard Setback 1.5 feet Rear Yard Setback

Staff has reviewed each of the requested incentives as they relate to the proposed design of the Project, the property configuration, the surrounding development, and the affordable housing regulations. Staff has determined the requested incentives are consistent with the intent of the State's Density Bonus Law as detailed below.

AAOZ Height – The project is requesting an incentive to allow the building height to exceed the AAOZ by approximately 35 feet. The Project site is within the AAOZ 250-300-foot overlay zone. The AAOZ limits the Project to 275 feet Above Mean Sea Level (AMSL) at the northern portion of the Project along Fourth Avenue. The proposed maximum building height is approximately 310 feet AMSL at its highest point along Fourth Avenue. The proposed building at its highest point would exceed the AAOZ by approximately 35 feet. Due to the need for off-street parking and the incorporation of ground-level commercial space, an additional floor of apartment units is necessary to make the Project financially viable according to the applicant.

The ALUCP for SDIA was updated in 2014 and it replaced the AAOZ with the Threshold Sitting Surface (TSS) as the planning tool to limit building heights with the approach area to SDIA. The TSS is geographically smaller than the AAOZ and does not contain the same height limits as the AAOZ. While ALUC determined that the geographic extent and height limitations used by AAOZ exceeds what is needed to protect airspace for the runway approach area, the AAOZ has not been amended yet to reflect the TSS.

The Project is also subject to the Federal Aviation Regulations (FAR), Part 77 which requires that projects submit application to the Federal Aviation Administration (FAA) to determine if the proposed buildings would be a hazard to air navigation. The City and the ALUCP also require that development projects obtain a “no hazard” to air navigation determination from the FAA. The Project received a determination of no hazard from the FAA on July 18, 2017 and an extension of the determination on February 7, 2019. The proposed Project is consistent with the ALUCP for building height, is located outside of the TSS area, and has received a determination of no hazard to air navigation from the FAA.

CPIOZ Height: The Project is requesting an incentive to allow the building height to exceed the 65-foot height limit of the Uptown CPIOZ-Type A by approximately 8 feet. The proposed Project site is located within the CPIOZ - Type A - Building Heights (UCP, Figure 4-12). The CPIOZ identifies areas within the community where ministerial approval is granted for proposed development with buildings or structures that do not exceed 65 feet in Hillcrest and Bankers Hill/Park West. Proposed developments that exceed the height limitations may be approved to the maximum allowed floor area of the base zone for zones without a maximum height limit if they comply with the applicable regulations of the Municipal Code and are consistent with the applicable policies in the General Plan and Uptown Community Plan. The Uptown CPIOZ - Type A height requirement is a development standard. Therefore, an affordable housing density bonus project may request an incentive to deviate from this standard. The CC-3-9 zone does not have a maximum height structure per Table 131-05e of the SDMC. Additionally, the Project’s proposed floor area of 39,468 square feet is less than the maximum 40,240 square feet allowed in the CC-3-9 Zone. Additionally, as stated above, the FAA has issued a determination of no hazard for the proposed structure height. The additional level of apartment units is necessary to make the Project financially feasible according to the applicant.

Ground Floor Restriction – An incentive is requested to allow an accessory use to the residential development within the front 30 feet of the lot. The Project provides access to the residential component of the development through a lobby located within the portion of the building fronting on 4th Avenue, and within the front 30 feet of the lot. The main purpose of this regulation is to prevent a dwelling unit within the front of the lot. The Project therefore as designed meets the purpose and intent of the regulations by not having a dwelling unit at the ground floor along the street frontage.

Private Exterior Open Space – An incentive is requested to eliminate private exterior open space for five dwelling units. The incentive is needed for two purposes: (1) to maintain SDGE’s 30-foot minimum clearance requirement above the underground electrical vault which is located directly in front of two dwelling units that are on the second and third floors along the street front. The clearance is necessary for SDGE to access the vault by lifting the vault lid with crane equipment if necessary; and (2) to maintain the openness of the ground floor commercial space and building lobby entrance to the sky by not including private exterior spaces (balconies) on the three second floor dwelling units along the street corridor. The second-floor elevation is at the minimum permitted along the street and is meant to maximize the floor levels so as to achieve the quantity of dwelling units proposed for the Project. The balconies required would protrude above the entrances and portion of the commercial space storefront, contributing to an “enclosed” feel of the spaces and diminish natural lighting. The omission of the five balconies would still be consistent with the

Community Plan's Urban Design Element development form policies since it would result in visual variety and interest to the building façade.

Side and Rear Yard Setbacks – An incentive is requested to allow side yard setbacks of 1.625 feet and a rear yard setback of 1.5 feet where either 10-foot minimum or zero-foot option is required. Pursuant to SDMC 131.0543(b)(1), in zones that require a 10-foot minimum side or rear setback and provide the option for no side or rear setbacks, the structure shall either be placed at the property line or shall be setback at least 10 feet. This incentive is requested to allow the Project to provide the necessary shoring of the site perimeter, in order to build the subterranean parking level and provide the bearing walls for the habitable structure above. The setback is sought to facilitate construction of the development as proposed with minimal deviation to the zero-foot setback option.

Community Plan Analysis:

The proposed Project is consistent with the goals and policies of the General Plan and Uptown Community Plan. The Project proposes a high-density mixed-use development along a mixed-use transit corridor with high frequency bus service consistent with the General Plan City of Villages strategy. The Project site has a land use designation of Community Commercial (0-109 Du/Ac) in the Uptown Community Plan, and the proposed Project incorporates ground floor retail uses along 4th Avenue consistent with the uses allowed in the Community Commercial land use designation. The proposed Project site is located within the Community Plan's Neighborhood Village, which promotes very-high residential densities along major commercial transit mixed-use corridors. Consistent with the policies of the Community Plan Land Use Element, the proposed Project concentrates high density housing on upper floors as part of mixed-use development in commercial areas and the development is located near transit on a higher volume traffic corridor. The proposed Project would locate higher density residential development in an area that is situated to promote a safer and livelier commercial district along 4th Avenue.

The proposed Project is located within the SDIA influence area and would be exposed to elevated noise levels from nearby roadways and from its proximity to SDIA. The proposed Project is consistent with the General Plan Noise Element by allowing for mixed-use development in an area with existing residential uses and where the Community Plan and the Airport Land Use Compatibility Plan allow future residential uses. The Project site would further implement the Noise Element by incorporating noise attenuation measures into the Project's design that ensure interior noise levels do not exceed 45 dBA CNEL.

The proposed Project would help implement the policies of the Community Plan and Housing Element by providing both affordable and market rate housing integrated with commercial development. The proposed Project site is a 0.23-acre vacant lot that would allow for a maximum of 26 dwelling units. The proposed Project would construct a new mixed-use building containing 36 multi-family residential units on a vacant lot. The proposed Project would include four "very low-income household" residential units and would use the density bonus provisions of the Affordable Housing Regulations to develop an additional 10 units for a total of 36 (including the affordable units) residential units on the Project site.

The proposed Project would help implement the goals and policies of the Uptown Community Plan Urban Design Element by providing a mixed-use development with pedestrian-oriented building frontages that contribute to a vibrant public realm. Consistent with the Streetscape and Public Realm policies of the Urban Design Element, the proposed Project would include non-contiguous sidewalks with landscaped parkways on 4th Avenue to provide a buffer between pedestrians and the street edge. The proposed Project implements the Street Tree Plan by incorporating the Jacaranda within the parkway to provide shade and add visual interest to the public realm.

The proposed Project's design would help implement the Development Form policies of the Urban Design Element. The ground level uses combine changes in depth with a change in material, which contribute to a pedestrian scale environment. The Project maintains a consistent street wall along 4th Avenue and provides building entrances that are level with the adjacent public sidewalk. The Project incorporates façade articulation using balconies and windows along 4th Avenue that add visual variety and interest to the building façade.

As stated above, the Project is requesting an incentive to exceed the 65-foot height limit established by the CPIOZ. Proposed development projects that exceed the 65-foot height limitation may be approved to the maximum allowed floor area of the base zone for zones without a maximum height limit if they comply with the applicable regulations of the Municipal code and are consistent with the applicable policies in the General Plan and Uptown Community Plan. The Project proposes an approximately 73.15-foot tall structure within a zone that does not have a maximum height limit. The Project's proposed floor area of 39,468 square feet is less than the maximum 40,240 square-foot floor area allowed in the CC-3-9 Zone. Furthermore, the proposed Project does not exceed the maximum height limits established by the Airport Land Use Compatibility Plan and the Federal Aviation Administration for the site. Consistent with the General Plan and Community Plan Urban Design Elements, the proposed Project would create street frontages with architectural interest by maintaining the existing setback pattern along 4th avenue and locating transparent features, including balconies and windows, facing the street. The proposed Project avoids repeating the same wall surface and incorporates façade articulation through the use of balconies and windows consistent with the policies of the Urban Design Element.

Conclusion:

The restriction of residential development based on the 50 percent maximum site intensity that is applied to residential use for a mixed-use development per the ALUCP results in under-utilization of land and the inability of the Project to maximize density at a location along a major commercial transit mixed-use corridor intended for high residential density. The Project would have a total intensity of 60 people which does not exceed the 64 people ALUCP limitation for the site. Furthermore, the Project's proposed land use intensity is consistent with the average intensity of the existing high intensity office, residential, and commercial mixed-use developments in the immediate area. Staff has provided draft findings to support approval of the project and draft conditions of approval. Staff recommends the Planning Commission recommend to the City Council to override the inconsistency determination and approve the Project as proposed.

ALTERNATIVES

1. Recommend to City Council to Approve Site Development Permit No. 2243451, with modifications.
2. Recommend to City Council to Deny Site Development Permit No. 2243451, if the findings required to approve the Project cannot be affirmed.

Respectfully submitted,



PJ Fitzgerald
Assistant Deputy Director
Development Services Department

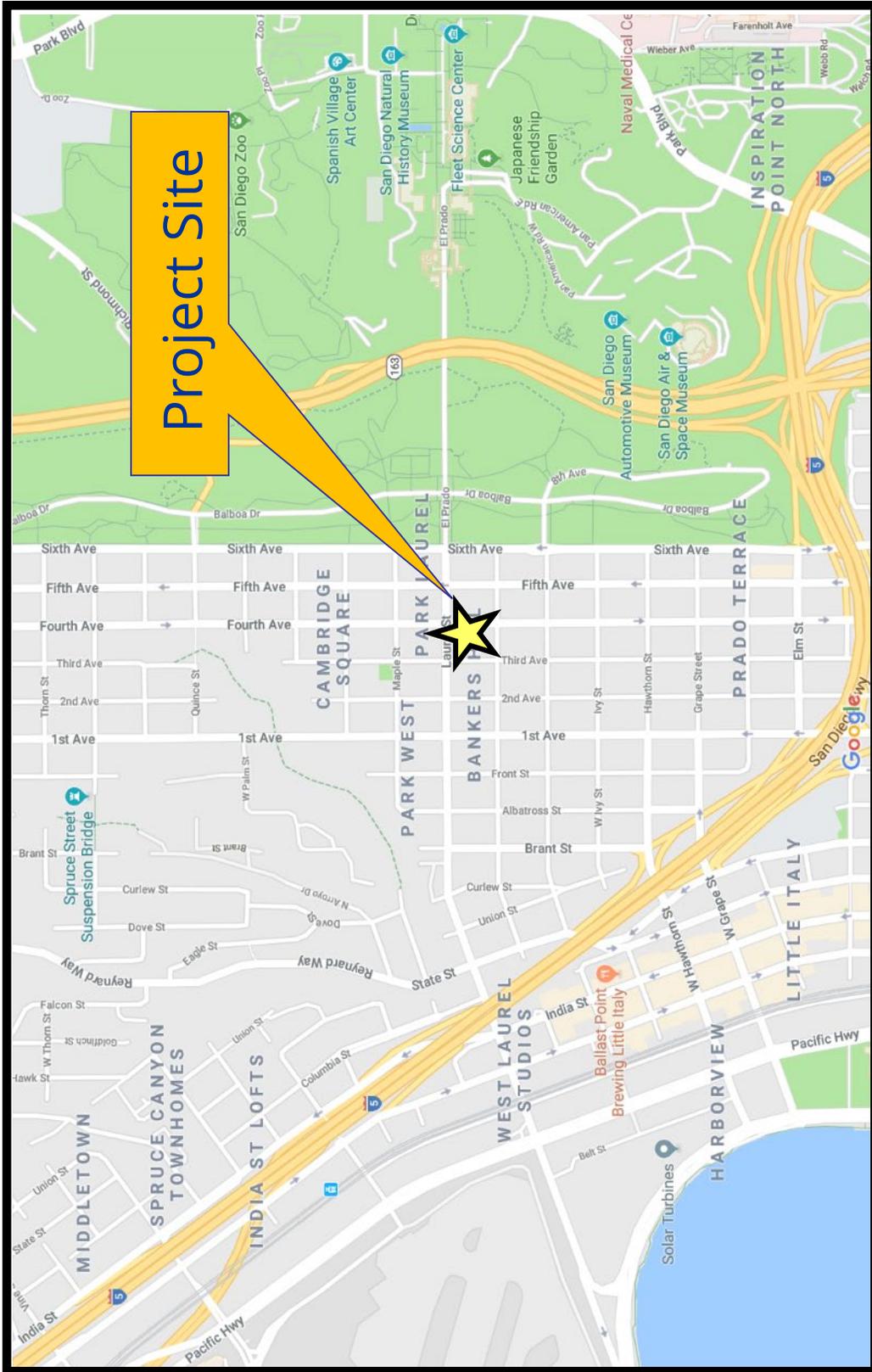
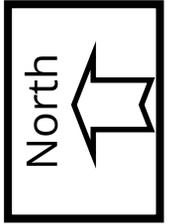


Firouzeh Tirandazi
Development Project Manager
Development Services Department

FITZGERALD/FT

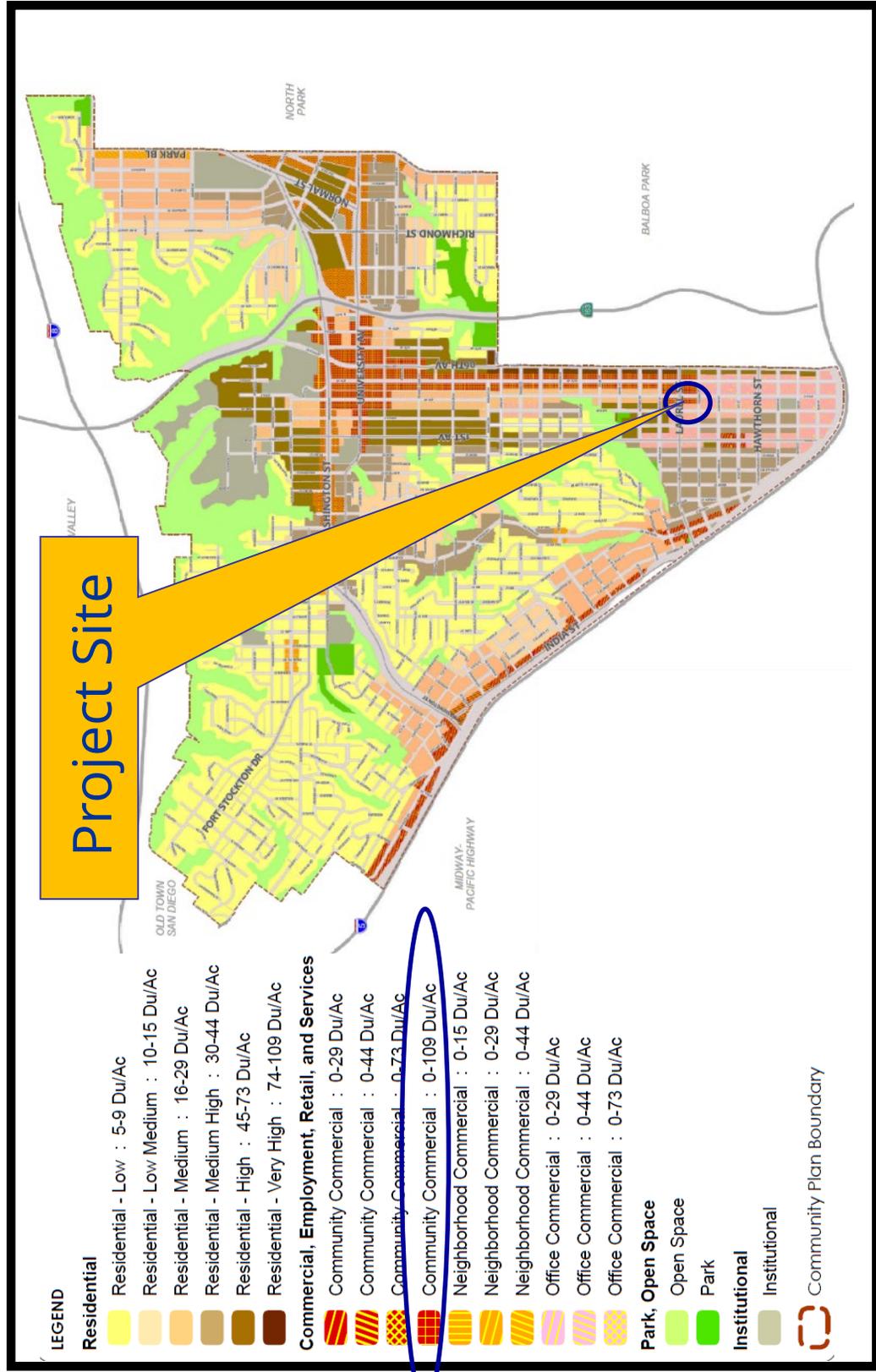
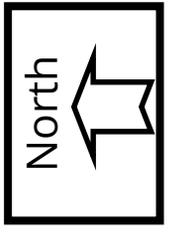
Attachments:

1. Project Location Map
2. Community Plan Land Use Map
3. Aerial and Site Photographs
4. Project Data Sheet
5. Draft Permit Resolution with Findings
6. Draft Permit with Conditions
7. Community Planning Group Recommendation
8. ALUC Resolution
9. City Council Resolution No. 312874
10. Development Comparison Exhibit
11. Ownership Disclosure Statement
12. Project Plans



Location Map
4TH AVENUE APARTMENTS – Project 588751
2426 4th Avenue





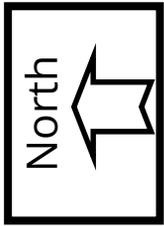
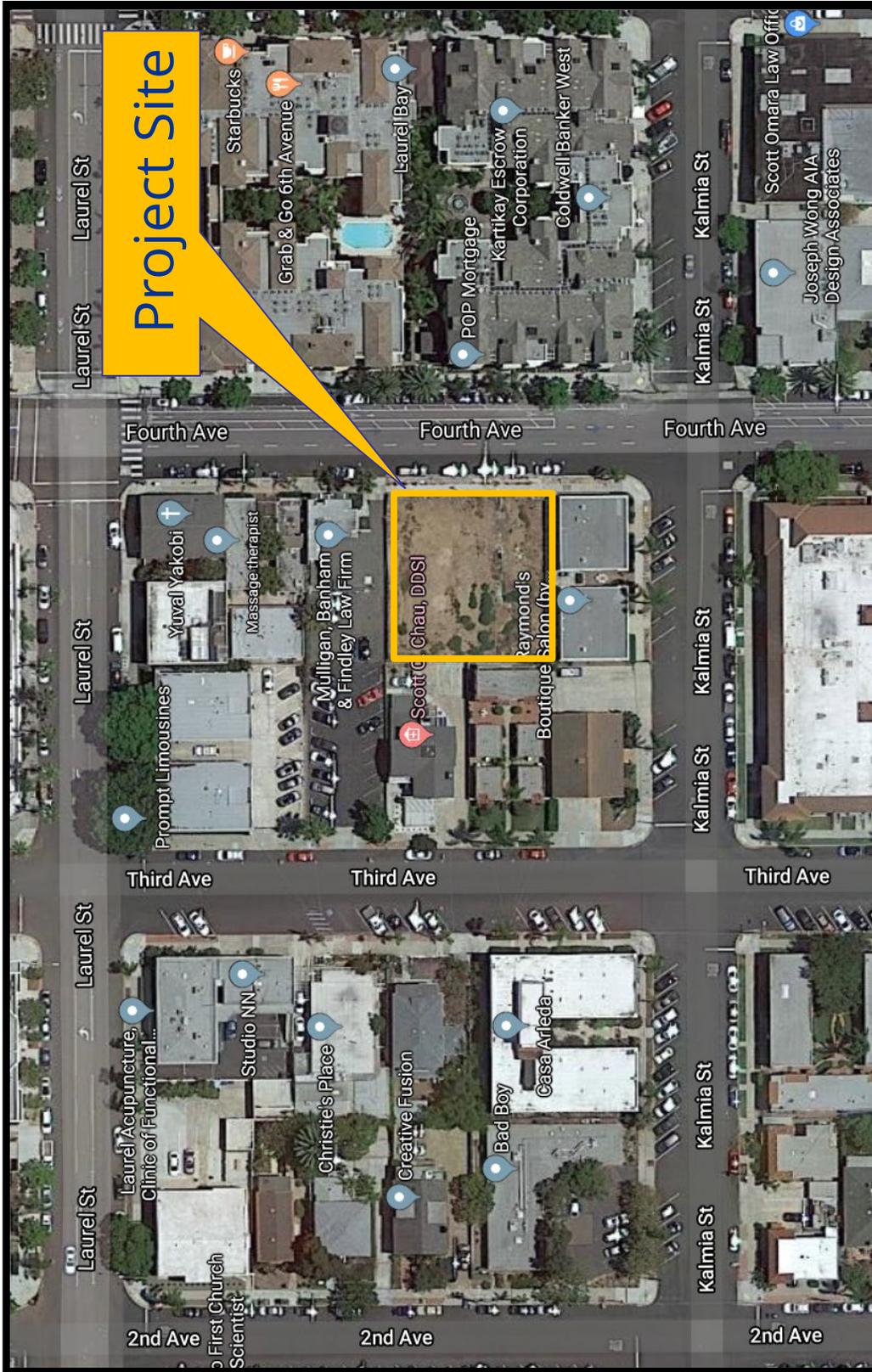
Project Site

- LEGEND**
- Residential**
 - Residential - Low : 5-9 Du/Ac
 - Residential - Low Medium : 10-15 Du/Ac
 - Residential - Medium : 16-29 Du/Ac
 - Residential - Medium High : 30-44 Du/Ac
 - Residential - High : 45-73 Du/Ac
 - Residential - Very High : 74-109 Du/Ac
 - Commercial, Employment, Retail, and Services**
 - Community Commercial : 0-29 Du/Ac
 - Community Commercial : 0-44 Du/Ac
 - Community Commercial : 0-73 Du/Ac
 - Community Commercial : 0-109 Du/Ac
 - Neighborhood Commercial : 0-15 Du/Ac
 - Neighborhood Commercial : 0-29 Du/Ac
 - Neighborhood Commercial : 0-44 Du/Ac
 - Office Commercial : 0-29 Du/Ac
 - Office Commercial : 0-44 Du/Ac
 - Office Commercial : 0-73 Du/Ac
 - Park, Open Space**
 - Open Space
 - Park
 - Institutional**
 - Institutional
 - Community Plan Boundary

Community Land Use Map (Uptown)

4TH AVENUE APARTMENTS – Project 588751
2426 4th Avenue





Aerial Photograph
4TH AVENUE APARTMENTS – Project 588751
2426 4th Avenue

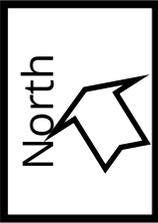




Project Site

Site Photograph No. 1
4TH AVENUE APARTMENTS – Project 588751
2426 4th Avenue





Site Photograph No. 2

4TH AVENUE APARTMENTS – Project 588751

2426 4th Avenue

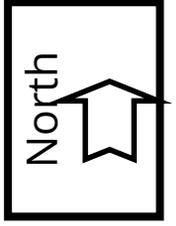




Project Site

Site Photograph No. 3
4TH AVENUE APARTMENTS – Project 588751
2426 4th Avenue





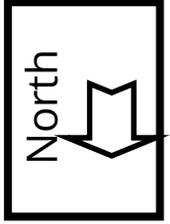
Project Site

Site Photograph No. 4

4TH AVENUE APARTMENTS – Project 588751

2426 4th Avenue





Site Photograph No. 5 (view from 3rd Ave)

4TH AVENUE APARTMENTS – Project 588751
2426 4th Avenue



Project Site



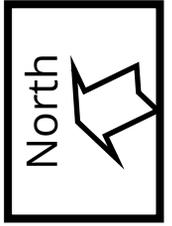
Project Site

Site Photograph No. 6 (4th & Kalmia)

4TH AVENUE APARTMENTS – Project 588751

2426 4th Avenue





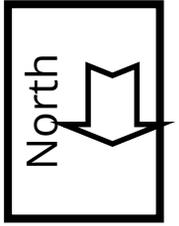
Project Site

Site Photograph No. 7

4TH AVENUE APARTMENTS – Project 588751

2426 4th Avenue



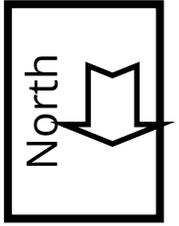


Site Photograph No. 8 (4th & Laurel)

4TH AVENUE APARTMENTS – Project 588751

2426 4th Avenue



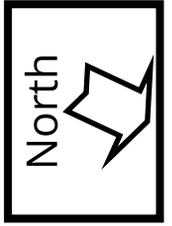


Site Photograph No. 9 (4th & Kalmia)

4TH AVENUE APARTMENTS – Project 588751

2426 4th Avenue





Project Site

Site Photograph No. 10

4TH AVENUE APARTMENTS – Project 588751

2426 4th Avenue



PROJECT DATA SHEET		
PROJECT NAME:	4 th Avenue Apartments	
PROJECT DESCRIPTION:	Construction of a 36-unit mixed-use development on an approximately 0.23-acre vacant site located on 4 th Avenue between Laurel and Kalmia Streets.	
COMMUNITY PLAN AREA:	Uptown	
DISCRETIONARY ACTIONS:	Process 5 Site Development Permit for SDIA ALUCP Override	
COMMUNITY PLAN LAND USE DESIGNATION:	Community Commercial (0-109 du/ac)	
<u>ZONING INFORMATION:</u>		
ZONE: CC-3-9 HEIGHT LIMIT: None; CPIOZ 65-foot limit LOT SIZE: 0.23 acre FLOOR AREA RATIO: 3.0 plus up to 1.0 for underground parking bonus LOT COVERAGE: 35% minimum FRONT SETBACK: 0 feet SIDE SETBACK: 10 feet minimum, 0-foot option STREETSIDE SETBACK: N/A REAR SETBACK: 10 feet minimum, 0-foot option PARKING: 24 spaces		
<u>ADJACENT PROPERTIES:</u>	LAND USE DESIGNATION & ZONE	EXISTING LAND USE
NORTH:	Community Commercial, CC-3-9 Zone	Commercial office
SOUTH:	Community Commercial, CC-3-9 Zone	Commercial / residential
EAST:	Community Commercial, CC-3-9 Zone	High density residential mixed-use development
WEST:	Office Commercial, CC-3-4 and CC 3-6 Zones	Commercial office / residential
DEVIATIONS:	Five allowable incentives in accordance with the Affordable Housing Regulations: AAOZ Height, CPIOZ Height, Ground Floor Restriction, Private Exterior Open Space, and Setbacks.	
COMMUNITY PLANNING GROUP RECOMMENDATION:	On October 2, 2018, the Uptown Planners voted to 15-1-1 to recommend approval of the Project with no conditions	

CITY COUNCIL RESOLUTION NO. _____
SITE DEVELOPMENT PERMIT NO. 2243451
4th AVENUE APARTMENTS - PROJECT NO. 588751 [MMRP]

WHEREAS, Fourth and Laurel, a California limited partnership, Owner, and Richard Simis, Permittee, filed an application with the City of San Diego for a Site Development Permit to construct a 36-unit mixed-use development known as the 4th Avenue Apartments project (Project), located at 2426 4th Avenue, between Laurel Street and Kalmia Street, within the Uptown Community Plan area and legally described as Lots "H" and "I" in Block 280 of Horton's Addition, in the City of San Diego, County of San Diego, State of California, According to Map thereof by LL. Lockling, Filed in the Office of the County Recorder of San Diego County;

WHEREAS, the site is in the CC-3-9 Zone, and the Very High Fire Hazard Severity Overlay Zone, Community Plan Implementation Overlay Zone (CPIOZ A), Residential Tandem Parking Overlay Zone, Transit Priority Area, Airport Influence Area for San Diego International Airport (SDIA) – Review Area 1, SDIA 60-65 Community Noise Equivalent Level (CNEL), SDIA Safety Zone 3NE, Federal Aviation Administration (FAA) Part 77 Notification Area for SDIA and North Island Naval Air Station (NAS), the Airport Environs Overlay Zone, and the Airport Approach Overlay Zone (AAOZ) for SDIA; and

WHEREAS, the Project must be submitted to the San Diego County Regional Airport Authority (SDCRAA), serving as the Airport Land Use Commission (ALUC), for a consistency determination with SDIA Airport Land Use Compatibility Plan (ALUCP) because of the Project's proposed density and location within SDIA-Review Area 1; and

WHEREAS, at its meeting on October 4, 2018, the SDCRAA, acting in its capacity as the ALUC, reviewed the Project and determined it inconsistent with the ALUCP because it exceeds the ALUCP's

allowable intensity factor for mixed use development with respect to residential density for the area in which it is located, Safety Zone 3 Northeast (3NE); and

WHEREAS, on March 9, 2020, pursuant to the California Public Utilities Code, the City Council voted 9-0 to adopt Mitigated Negative Declaration No. 588751 and the Mitigation Monitoring and Report Program (Resolution No. 312873) and to propose to overrule the SDCRAA's determination of inconsistency (Resolution No. 312874); and

WHEREAS, City Staff determined the Project exceeds the maximum allowable residential density per the residential factor established by the ALUCP for a mixed-use project, and a City Council override of this inconsistency determination is required; and

WHEREAS, the Project requires a Site Development Permit for the City Council to override the determination of inconsistency with the intensity of the development within Safety Zone 3NE of the ALUCP for SDIA in accordance with SDMC sections 126.0502(e) and 132.0310; and

WHEREAS, on April 16, 2019, the Planning Commission of the City of San Diego considered Site Development Permit No. 2243451, and pursuant to Resolution No. _____ the Planning Commission voted to recommend _____ of the Permit; and

WHEREAS, under Charter section 280(a)(2) this resolution is not subject to veto by the Mayor because this matter requires the City Council to act as a quasi-judicial body and where a public hearing was required by law implicating due process rights of individuals affected by the decision and where the Council was required by law to consider evidence at the hearing and to make legal findings based on the evidence presented; and

WHEREAS, the Project is within the scope of Mitigated Negative Declaration No. 588751 adopted by the City of San Diego on March 9, 2020, and pursuant to CEQA Guidelines Section 15162,

there is no substantial change in circumstance, project changes, or new information that warrant additional environmental review or the preparation of a new environmental document; and

WHEREAS, pursuant to SDMC Section 132.1555, any associated development permit shall be consolidated with the City Council hearing regarding the overrule and the matter was set for public hearing on _____, testimony having been heard, evidence having been submitted, and the City Council having fully considered the matter and being fully advised concerning the same; NOW, THEREFORE,

BE IT RESOLVED, by the Council of the City of San Diego, that it adopts the following findings with respect to Site Development Permit No. 2243451:

A. SITE DEVELOPMENT PERMIT [SDMC Section 126.0504(a)]

1. The proposed development will not adversely affect the applicable land use plan.

The application for the 4th Avenue Apartments development project (Project) is for a residential and commercial mixed-use development with a mix of affordable and market rate dwelling units. The Uptown Community Plan designates the 0.23-acre development site for Community Commercial which allows up to 109 dwelling units per acre as part of a mixed-use development, and is zoned CC-3-9, allowing 26 dwelling units on the site. The Project is proposing to designate 15 percent of the allowed number of dwelling units, as affordable housing of a very low-income level designation. Under the City's Affordable Housing Regulations, this designation permits an increase of 50 percent of the allowed dwelling units for an additional 13 dwelling units, totaling 39 dwelling units; however, the Project is only proposing 36 dwelling units. In total, the Project will include 36 dwelling units as well as approximately 1,174 square feet of ground floor commercial uses along Fourth Avenue.

The Affordable Housing Regulations allow the Project to incorporate up to five development incentives, as described in Table 143-07A of the San Diego Municipal Code. The Project is requesting the following deviations as its five incentives. The incentives will allow the Density Bonus units to be constructed on-site with no deviations required to the Land Development Code. The incentives are as follows:

Incentives		
SDMC Section	Required/Allowed	Proposed
Maximum Building Height specified by the AAOZ Section 132.0205	275 feet	Allow the maximum building height to exceed the AAOZ height limit by 35.85 feet.
Maximum Building Height specified by the CPIOZ Section 132.1401, Diagram 132.14K	65 feet above ground level	Allow the maximum building height to exceed the CPIOZ height limit by 8.15 feet.
Ground Floor Restriction Section 131.0540(c)(1)	Commercial uses along front half of property	Lobby area (accessory residential use) within front 30-feet of the property
Private Exterior Open Space Section 131.0455(d)	All residential units provide a minimum area of 50 square feet of private exterior open space	Five dwelling units without private exterior open space
Side and Rear Yard Setbacks Section 131.0543(b)	Structure must be placed at zero- or 10-foot Side and Rear Yard Setbacks	1.625 feet Side Yard Setback 1.5 feet Rear Yard Setback

The proposed Project would implement the goals and policies of the General Plan and Uptown Community Plan by providing a mixed-use development with affordable units along a high frequency transit corridor. The General Plan’s City of Villages strategy focuses new development within corridors and villages served by high frequency transit. The proposed Project is also implementing the City’s Climate Action Plan by placing new mixed-use development within a Transit Priority Area. The Community Plan’s Land Use Element and Urban Design Element contains policies to develop high density mixed-use projects on commercial mixed-use transit corridors with pedestrian-oriented building frontages that contribute to a vibrant public realm. The proposed Project would be consistent with the streetscape and public realm policies of the Urban Design Element. The proposed Project would include non-contiguous sidewalks with landscaped parkways on Fourth Avenue to provide a buffer between pedestrians and the street edge. The proposed Project implements the Community Plan’s street tree plan by incorporating the Jacaranda, three 24-inch box, within the parkway to provide shade and add visual interest to the public realm.

The proposed Project’s design would help implement the development form policies of the Urban Design Element. The ground level uses combine changes in depth with a change in material, which contribute to a pedestrian scale environment. The Project maintains a consistent street wall along Fourth Avenue and provides building entrances that are level with the adjacent public sidewalk. The Project incorporates façade articulation using balconies and windows along Fourth Avenue that add visual variety and interest to the building façade.

The proposed Project is located along Fourth Avenue which is identified in the Community Plan as a Class II bicycle route, set to become designated a Class IV cycle track. A goal of the Mobility Element of the General Plan is to encourage bicycling as a viable travel choice within the City. The Project would assist in attaining this goal by providing 15 onsite bicycle parking spaces. The Community Plan's Mobility Element contains recommendations to place off-street parking underground and/or screened from the public right-of-way and adjacent residences. The proposed Project would implement this recommendation by enclosing all off-street parking within one level of at grade and one level of underground parking.

Policy NE-A.4 of the Noise Element of the General Plan requires an acoustical study consistent with Acoustical Study Guidelines (Table NE-4 of the General Plan) for proposed developments in areas where the existing or future noise level exceeds or would exceed the "compatible" noise level thresholds as indicated on the Land Use-Noise Compatibility Guidelines (Table NE-3 of the General Plan), so that noise mitigation measures can be included in a development project's design to meet noise guidelines. According to Table NE-3, a proposed mixed-use project within an exterior noise exposure level of 60 dB and up to 70 decibel (dB) Community Noise Equivalent Level (CNEL) would be "conditionally compatible" and must attenuate exterior noise to an indoor noise level of 45 dB CNEL.

An acoustical report prepared for the proposed Project evaluated traffic levels on Fourth Avenue and noise aircraft associated with the San Diego International Airport in relation to the proposed Project. The acoustical report indicates that the building face of the proposed Project would be exposed to a traffic noise level of 65.0 dB CNEL and that ultimate noise level from aircraft are estimated at 62 dB CNEL. The result is a combined noise level of 67 dB CNEL. According to Table NE-3 of the General Plan, proposed projects having a noise level of 65-70 dB CNEL are "conditionally compatible" and must be capable of attenuating exterior noise to an indoor level of 45 dB CNEL. Based on calculations performed as part of the noise study, a minimum noise reduction of 22dB would be achieved through provision of mechanical ventilation and acoustical treatments. Therefore, the proposed Project would meet the dB 45 CNEL interior noise level. The proposed Project will also be providing an avigation easement regarding the overflight of aircraft to the San Diego County Regional Airport Authority as a condition of approval as required by the Noise Element of the General Plan, and the ALUCP for SDIA.

The design considerations incorporated into the Project make it consistent with the General Plan and the Uptown Community Plan; and therefore, the Project will not adversely affect the applicable land use plan.

2. The proposed development will not be detrimental to the public health, safety, and welfare.

The Project site currently is a vacant lot with no structures, and it is located in a developed urban neighborhood served by existing streets, sidewalks and public utilities, including water, sewer, gas, and electric. The Project will have adequate levels of essential public services such as police, fire, and emergency medical services, including access to two major hospitals in the Medical Complex neighborhood directly to the north.

The Project will not be detrimental to public health, safety and welfare because the permit controlling the development and use of the site requires compliance with the City codes, policies, regulations and other regional, state, and federal regulations, except where design incentives are identified in this permit. Construction plans will be reviewed by City staff to ensure they comply with all building code regulations. The Project will be inspected by certified building and engineering inspectors to assure construction is in accordance with the approved plans and regulations.

The Project is required to comply with seismic requirements of the California Building Code. Implementation of proper engineering design and utilization of standard construction practices (including recommendations contained within the Geotechnical Investigation), verified at the building permit stage, would ensure that the potential for impacts from regional geologic hazards, including fault rupture, would be less than significant.

The Project site is located approximately 0.9 miles from the San Diego International Airport (SDIA) and is within the Airport Approach Overlay Zone (AAOZ) contained within Chapter 13, Article 2, Division 15 of the San Diego Municipal Code. The Project is seeking an affordable housing incentive to deviate from the AAOZ by 35 feet. In 1984, the City adopted the AAOZ as a planning tool to restrict building heights primarily for the approach and departure areas for SDIA. The AAOZ does not implement federal aviation regulations or Airport Land Use Commission policies for airspace protection.

In 1990, the San Diego Association of Governments (SANDAG) as the Airport Land Use Commission (ALUC) for San Diego County adopted the Comprehensive Land Use Plan (CLUP) for SDIA and incorporated the AAOZ into the plan. In 2014, the San Diego County Regional Airport Authority as the ALUC, adopted the Airport Land Use Compatibility Plan (ALUCP) for SDIA which replaced the CLUP. The ALUCP is intended to provide for the orderly development of the Airport and the area surrounding the Airport and protect public health, safety and welfare in areas around the Airport.

The ALUCP replaced the AAOZ with the Threshold Sitting Surface (TSS) as the planning tool to limit building heights needed to protect airspace. The TSS geographically is smaller than the AAOZ and does not contain the same height limits as the AAOZ. The Project site is outside of the TSS area and is therefore consistent with the ALUCP for airspace protection. The proposed Project is seeking an overrule of the ALUC's ruling of inconsistency based on the ALUCP's limitation on residential density.

The Project is also subject to the Federal Aviation Regulations (FAR), Part 77 which requires that projects submit application to the Federal Aviation Administration (FAA) to determine if the proposed buildings would be a hazard to air navigation. The City and the ALUCP also require that projects obtain a "no hazard" determination from the FAA. The Project received a determination of no hazard from the FAA on July 18, 2017 and an extension of the determination on February 7, 2019. The proposed Project is consistent with the ALUCP for building height and has received a determination of no hazard from the FAA.

All Uniform Building, Fire Plumbing, Electrical and Mechanical Code regulations and permitting requirements governing the construction and continued operation of the

development apply to this Project. Therefore, as proposed, the Project will not be detrimental to the public health, safety, or welfare.

3. The proposed development will comply with the applicable regulations of the Land Development Code, including any allowable deviations pursuant to the Land Development Code.

The Project will comply with the applicable regulations of the Land Development Code for a mixed-use residential and commercial retail space with approval of the SDP for the proposed Project. The CC-3-9 zone allows for commercial and residential mixed-use development. This Project will provide commercial retail and residential on the bottom floor within the commercial zone with residential above. The applicant has requested a density bonus in accordance with the City's Affordable Housing Regulations. In exchange for restricting 15 percent of the allowed 26 dwelling units (four units) as affordable to very-low income households, the applicant is eligible for a 50 percent density bonus, for a total of 39 dwelling units, however, the Project is only proposing 36 dwelling units. In total, the Project will include 36 residential dwelling units as well as 1,174 square feet of ground floor commercial uses along Fourth Avenue.

The Affordable Housing Regulations allow the proposed 36-unit residential mixed-use Project to incorporate up to five development incentives, as described in Table 143-07A of the San Diego Municipal Code. Pursuant to SDMC Section 143.0740, an incentive can mean a deviation to a development regulation. The applicant is requesting that five incentives be used for the following five deviations. The incentives will allow the Density Bonus units to be constructed on-site with no deviations required to the Land Development Code. The incentives are as follows:

Incentives		
SDMC Section	Required/Allowed	Proposed
Maximum Building Height specified by the AAOZ Section 132.0205	275 feet	Allow the maximum building height to exceed the AAOZ height limit by 35.85 feet.
Maximum Building Height specified by the CPIOZ-A Section 132.1401, Diagram 132.14K	65 feet above ground level	Allow the maximum building height to exceed the CPIOZ height limit by 8.15 feet.
Ground Floor Restriction Section 131.0540(c)(1)	Commercial uses along front half of property	Lobby area (accessory residential use) within front 30-feet of the property
Private Exterior Open Space Section 131.0455(d)	All residential units provide a minimum area of 50 square feet of private exterior open space	Five dwelling units without private exterior open space
Side and Rear Yard Setbacks Section SDMC 131.0543(b)	Structure must be placed at zero- or 10-foot Side and Rear Yard Setbacks	1.625 feet Side Yard Setback 1.5 feet Rear Yard Setback

The first incentive pertains to the building's height as measured from above mean sea level. The Airport Approach Overlay Zone (AAOZ) limits Project to 275 feet Above Mean Sea Level (AMSL) at the northern portion of the Project along Fourth Avenue. The proposed maximum building height is approximately 310 feet AMSL at its highest point along Fourth Avenue. The proposed building at its highest point would exceed the AAOZ by approximately 35 feet. Due to the need for off-street parking and the incorporation of ground-level commercial space, an additional floor of apartment units is necessary to make the Project financially viable according to the applicant.

The ALUCP for San Diego International Airport (SDIA) was updated in 2014 and it replaced the AAOZ with the Threshold Sitting Surface (TSS) as the planning tool to limit building heights with the approach area to SDIA. The TSS is geographically smaller than the AAOZ and does not contain the same height limits as the AAOZ. The proposed Project is seeking an overrule of the ALUCP's ruling of inconsistency based on the ALUCP's limitation on intensity of the development which is separate from building height.

The Project is also subject to the Federal Aviation Regulations (FAR), Part 77 which requires that projects submit application to the Federal Aviation Administration (FAA) to determine if the proposed buildings would be a hazard to air navigation. The City and the ALUCP also require that development projects obtain a "no hazard" to air navigation determination from the FAA. The Project received a determination of no hazard from the FAA on July 18, 2017 and an extension of the determination on February 7, 2019. The proposed Project is consistent with the ALUCP for building height, is located outside of the TSS area, and has received a determination of no hazard to air navigation from the FAA.

The second incentive also pertains to the building's height as measured from above ground level. The Community Plan Implementation Overlay Zone (CPIOZ) Type A limits maximum building height at 65 feet above ground level. Proposed projects that exceed the height limitations may be approved to the maximum allowed floor area of the base zone for zones without a maximum height limit if they comply with the applicable regulations of the Municipal Code and are consistent with the applicable policies in the General Plan and Uptown Community Plan. The proposed building height is 73.15 feet, or 8.15 feet more than regulated by the CPIOZ. The CC-3-9 base zone does not limit building heights. The Project's proposed floor area of 39,468 square feet is less than the maximum 40,240 square feet allowed in the CC-3-9 Zone, and the Project complies with the application regulations of the Municipal Code and is consistent with the applicable policies in the General Plan and Uptown Community Plan.

The third incentive is seeking to place a "residential" use at the front 30 feet of the lot. The use proposed would be a lobby, for the use of the tenants to access the public right-of-way. The main purpose of this regulation is to prevent a dwelling unit to be located at the front of the lot, however, this space is being used as a means to connect the dwelling units that occur above, from the second floor and up, to the public right-of-way thus meeting the purpose and intent of not having a permanent dwelling unit at the ground floor along the street frontage.

The fourth incentive is to allow for the omission of private exterior spaces (balconies) for five dwelling units where all units are to have this space. The incentive is needed for two purposes: (1) to maintain SDGE's 30-foot minimum clearance requirement above the underground electrical vault which is located directly in front of two dwelling units that are on the second and third floor along the street front. The clearance is necessary for SDGE to access the vault by lifting the vault lid with crane equipment if necessary; and (2) to maintain the openness of the ground floor commercial space and building lobby entrance, to the sky by not including private exterior spaces (balconies) on the three second floor dwelling units along the street corridor. The second-floor elevation is at the minimum permitted along the street and is meant to maximize the floor levels so as to achieve the quantity of dwelling units proposed for the Project. The balconies required would project above the entrances and portion of the commercial space storefront, contributing to an "enclosed" feel of the spaces and diminish natural lighting. The omission of the five balconies would still be consistent with the Community Plan's Urban Design Element development form policies since it would result in visual variety and interest to the building façade.

The fifth incentive is to have side and rear yard setbacks that are neither the zero-foot or 10-foot required by the Land Development Code. Instead, the Project proposes a 1.5-foot setback at the rear and a setback of 1.625 feet on each side. This incentive is being sought to be able to provide the necessary shoring of the site perimeter, in order to build the subterranean parking level and provide the bearing walls for the habitable structure above. The proposed setbacks are essentially sought to facilitate construction of the development as proposed with minimal deviation, so the design meets the intent of the regulation.

Based on the above, the proposed development will comply with the regulations of the Land Development Code, including the five Affordable Housing Density Bonus incentives, and Federal Aviation Regulation, Part 77.

B. SITE DEVELOPMENT PERMIT - SUPPLEMENTAL FINDINGS [SDMC Section 132.0310(B)]

1. The proposed development will not be detrimental to the public health, safety, and welfare.

The Project proposes a residential and commercial mixed-use development consisting of 36 dwelling units, including four very-low income dwelling units, and an approximately 1,174 square-foot retail suite, parking and associated site improvements, with five development incentives in accordance with the Affordable Housing Regulations. The Project is a permitted use that is consistent with the Uptown Community Plan's Community Commercial land use designation, which allows up to 109 dwelling units per acre as part of a mixed-use development, the underlying CC-3-9 Zone, and the City's Affordable Housing Regulations. The Project site is located within the Uptown Community Plan's Neighborhood Village, which promotes very-high residential densities and non-residential intensities along major commercial mixed-use transit corridors and proposes a development intensity similar to existing office and mixed-use developments in the area.

The Project site is located approximately 0.9 miles from the San Diego International Airport (SDIA). Project is subject to the policies and criteria of the Airport Land Use Compatibility

Plan (ALUCP) for SDIA. In 2014, the SDCRAA as the ALUC, adopted a comprehensive update to the ALUCP for SDIA. The ALUCP is intended to provide for the orderly development of the Airport and the area surrounding the Airport and protect public health, safety and welfare in areas around the Airport.

For airspace protection, the ALUCP uses Threshold Sitting Surface (TSS) as the planning tool to limit building heights. The Project site is outside of the TSS area and is therefore consistent with the ALUCP for airspace protection. The Project is also subject to the Federal Aviation Regulations (FAR), Part 77 which requires that projects submit application to the Federal Aviation Administration (FAA) to determine if the proposed buildings would be a hazard to air navigation. The City and the ALUCP also require that projects obtain a “no hazard” to air navigation determination from the FAA. The Project received a determination of no hazard to air navigation from the FAA on July 18, 2017 and an extension of the determination on February 7, 2019. The proposed Project is consistent with the ALUCP for building height and has received a determination of no hazard from the FAA. Furthermore, the Project has been reviewed pursuant to the California Environmental Quality Act, and the environmental analysis determined the proposed project could have a significant environmental effect with regard to Noise (construction-related). However, potential impacts would be mitigated to below a level of significance with specific mitigation measures identified in the Project’s Mitigation, Monitoring, and Reporting Program. Therefore, the proposed development will not be detrimental to the public health, safety, and welfare.

2. The proposed development will minimize the public’s exposure to excessive noise and safety hazards to the extent feasible.

The Project will minimize the public’s exposure to excessive noise and safety hazards to the extent feasible. The Project is located within the 60-65 decibel Community Noise Equivalent Level (dB CNEL) noise contour. The ALUCP for SDIA identifies commercial uses located within the 60-65 dB CNEL noise contour as compatible with airport uses, and identifies residential uses located within the 60-65 dB CNEL noise contour as conditionally compatible with airport uses, provided that the residences are sound attenuated to 45 dB CNEL interior noise level. The ALUCP for SDIA contains policies to ensure that noise attenuation is required to ensure an interior noise level of 45 dB CNEL for developments exposed to exterior aircraft noise levels 60 dB CNEL or greater. An acoustical report prepared for the Project evaluated traffic levels on Fourth Avenue and aircraft noise associated with the San Diego International Airport in relation to the proposed Project. The acoustical report indicates that the building face of the proposed Project would be exposed to a traffic noise level of 65.0 dB CNEL and that ultimate noise level from aircraft are estimated at 62 dB CNEL. The result is a combined noise level of 67 dB CNEL.

According to Table NE-3 of the General Plan, proposed projects having a noise level of 65-70 dB CNEL are “conditionally compatible” and must be capable of attenuating exterior noise to an indoor level of 45 dB CNEL. Based on calculations performed as part of the noise study, a minimum noise reduction of 22dB would be achieved through provision of mechanical ventilation and acoustical treatments. Therefore, the proposed Project would meet the dB 45 CNEL interior noise level. The proposed Project will also be providing an aviation easement

to the San Diego County Regional Airport Authority as a condition of approval as required by the Noise Element of the General Plan, and the ALUCP for SDIA.

3. The proposed development will meet the purpose and intent of the California Public Utilities Code Section 21670.

The purpose and intent of the California Public Utilities Code section 21670 is to “provide for the orderly development of each public use airport in the state and the area surrounding these airports so as to promote the overall goals and objectives of the California airport noise standards adopted pursuant to Section 21669 and to prevent the creation of new noise and safety problems.” In addition, the purpose and intent of the section is also “to protect public health, safety, and welfare by ensuring the orderly expansion of airports and the adoption of land use measures that minimize the public’s exposure to excessive noise and safety hazards within areas located around public airports to the extent that these areas are not already devoted to incompatible uses.”

The proposed Project received a “no hazard” to air navigation determination from the FAA and is outside of Threshold Siting Surface used by the ALUCP for SDIA to limit building heights and protect airspace within the runway approach area; and therefore, the proposed development would not impact existing or an expansion of future airport operations.

An acoustical report prepared for the Project evaluated traffic levels on Fourth Avenue and aircraft noise associated with the San Diego International Airport in relation to the proposed Project. The acoustical report indicates that the building face of the proposed Project would be exposed to a traffic noise level of 65.0 dB CNEL and that ultimate noise level from aircraft are estimated at 62 dB CNEL. The result is a combined noise level of 67 dB CNEL. Based on calculations performed as part of the noise study, a minimum noise reduction of 22dB would be achieved through provision of mechanical ventilation and acoustical treatments. the Project would have an interior noise level that would not exceed 45 dB CNEL for habitable areas consistent with the ALUCP for SDIA; and therefore, the proposed development would not expose the people living or working in the building to excessive noise hazards.

The proposed Project site is in a neighborhood with existing higher density multifamily and commercial uses. The proposed residential commercial mixed-use Project is consistent with the adopted community plan for land use. The ALUCP for SDIA identifies residential and commercial uses as compatible uses for the site of the proposed Project; and therefore, the Project uses are consistent with the ALUCP for SDIA.

The ALUCP for SDIA limits density and intensity for proposed uses by safety zone and neighborhood based on the average existing development intensity and density. Residential uses are measured by dwelling units per acre and commercial uses are measured by people per acre. For mixed-use development, the ALUCP allows for residential density to be converted to intensity measured by people per acre, based on a people per household factor in the ALUCP, which is then added to the commercial intensity for the total intensity. A maximum of 50 percent of the maximum site intensity may be applied for residential uses. The Project exceeds the 50 percent threshold.

The Project is located in the ALUCP's Uptown Safety Zone 3NE. The overall ALUCP people per acre intensity for that zone location is 278 people per acre, which equates to 64 people for the 0.23-acre site. The residential portion of this Project cannot exceed 50 percent of the 64 people, which is 32 people (64/2). The maximum compatible number of units would be 22, using the ALUCP factor of 1.48 people per household (32/1.48).

The other 32 people that comprise the nonresidential half of the mixed-use Project is multiplied by the occupancy factor in the ALUCP to determine the maximum compatible commercial use. Retail sales has an occupancy factor of 170 square feet per person, which yields a total maximum area of 5,440 square feet as compatible with the ALUCP (32x170 = 5,440). The nonresidential component of the Project, consisting of an approximately 1,174 square feet, is significantly below the compatibility threshold. The Project would have a total intensity of 60 people (53 residents and 7 retail space occupants) which does not exceed the 64 people ALUCP limitation for the site.

California Public Utilities Code section 21670 directs Caltrans to prepare an Airport Land Use Planning Handbook (Handbook) to provide guidance to local ALUCs when preparing an ALUCP. Within highly density areas with high-rise and mid-rise buildings, the Handbook allows for infill development at the average density and intensity of existing uses. The Project's proposed land use intensity is consistent with the existing average intensity for the high density residential and commercial mixed-use developments in the immediate area as shown in the following figure. The existing office and mixed-use higher intensity developments show in the figure have an average of 351 people per acre. The Project would have an intensity of 261 people per acre; and therefore, the Project is consistent with the Handbook and CPUC section 21670.

Comparison between Existing Development and the Project							
Name	Address	Lot Size	Stories	Use	Non-Res Floor Area	Units	People per Acre
5 th & Laurel Financial Centre	2550 5 th Ave	1.38 Acres	18	Office	160,000	0	539
5 th & Laurel Corporate Center	2445 5 th Ave	0.36 Acres	6	Office	40,000	0	517
Imperial Towner Apartments	2399 5 th Ave	0.46 Acres	14	Mixed-Use	13,000	89	420
4th Avenue Apartments	2455 4th Ave	0.23	6	Mixed-Use	1,174	36	261
2400 6 th Apartments	2400 6 th Ave	0.35 Acres	11	Multi-Family	0	60	254
Laurel Bay	2400 5 th Ave	1.38 Acres	5	Mixed-Use	18,000	150	238
Park Laurel	2515 5 th Ave	1.34 Acres	14	Mixed-Use	10,000	94	139

The restriction of residential development based on a people per household factor and the 50 percent maximum site intensity that is applied to residential use for a mixed-use development per the ALUCP results in under-utilization of land and the inability of the Project to maximize density at a location along a major commercial mixed-use transit corridor intended for high residential density. Furthermore, the Project's proposed land use intensity is consistent with existing high intensity office, residential, and commercial mixed-use developments in the immediate area. Based on the above, and considering the consistency and compatibility of the Project with the land use designation per the Uptown Community Plan, and the City's Affordable Housing regulations, the Project's intensity below the 64 people ALUCP limitation for the site, and below the average existing intensity of the high intensity development in the immediate area, and the Project's location in a highly urban area already devoted to similar land uses, the Project meets the intent of CPUC section 21670.

The above findings are supported by the minutes, maps and exhibits, all of which are incorporated herein by this reference.

BE IT FURTHER RESOLVED, that, based on the findings hereinbefore adopted by the City Council, Site Development Permit No. 2243451 is granted to Fourth and Laurel, a California limited partnership, Owner, and Richard Simis, Permittee, under the terms and conditions set forth in the attached permit which is made a part of this resolution.

APPROVED: MARA W. ELLIOTT, CITY ATTORNEY

By _____
NAME
Deputy City Attorney

ATTY/SEC. INITIALS

DATE

R- INSERT

RECORDING REQUESTED BY
CITY OF SAN DIEGO
DEVELOPMENT SERVICES
PERMIT INTAKE, MAIL STATION
501

WHEN RECORDED MAIL TO
CITY CLERK
MAIL STATION 2A

INTERNAL ORDER NUMBER: 24007643

SPACE ABOVE THIS LINE FOR RECORDER'S USE

SITE DEVELOPMENT PERMIT NO. 2243451
4th AVENUE APARTMENTS - PROJECT NO. 588751 [MMRP]
CITY COUNCIL

This Site Development Permit No. 2243451 is granted by the City Council of the City of San Diego to Fourth and Laurel, a California limited partnership, Owner, and Richard Simis, Permittee, pursuant to San Diego Municipal Code [SDMC] sections 126.0505 and 132.0310(b). The 0.23-acre vacant lot is located at 2426 4th Avenue in the CC-3-9 Zone of the Uptown Community Plan area. The site is also located in the Very High Fire Hazard Severity Overlay Zone, Community Plan Implementation Overlay Zone (CPIOZ-A), Residential Tandem Parking Overlay Zone, Transit Priority Area, Airport Influence Area for San Diego International Airport (SDIA) – Review Area 1, SDIA 60-65 Community Noise Equivalent Level (CNEL), SDIA Safety Zone 3NE, Federal Aviation Administration (FAA) Part 77 Notification Area for SDIA and North Island Naval Air Station (NAS), Airport Environs Overlay Zone, and the Airport Approach Overlay Zone (AAOZ) for SDIA. The project site is legally described as: Lot "H" and "I" in Block 280 of Horton's Addition, in the City of San Diego, County of San Diego, State of California, According to Map thereof by LL. Lockling, Filed in the Office of the County Recorder of San Diego County.

Subject to the terms and conditions set forth in this Permit, permission is granted to Owner and Permittee to construct a six-story, 36-dwelling unit mixed-use development described and identified by size, dimension, quantity, type, and location on the approved exhibits [Exhibit "A"] dated _____, on file in the Development Services Department.

The project shall include:

- a. Construction of a six-story, 73.15-foot tall (above ground level), approximately 39,468-square-foot mixed-use building containing 36 dwelling units, including four units affordable to very low-income households, a 1,174-square-foot retail suite on the first floor, lobby area, and parking (one level at grade and one subterranean level);
- b. Approval of the following five development incentives:
 - 1. Allow a structure height of 73.15 feet, where 65 feet is allowed by right within the Community Plan Implementation Overlay Zone – A per SDMC Section 132.1401, Diagram 132.14K;

2. Allow residential use within the front 30 feet of the lot where residential use and residential parking are prohibited on the ground floor in the front half of the lot per SDMC Section 131.0540(c)(1);
 3. Allow five dwelling units without usable, private exterior open space where each dwelling unit is required to provide a minimum area of 50 square feet per SDMC Section 131.0455(d);
 4. Allow side yard setbacks of 1.625 feet along both property side yards, and a rear-yard setback of 1.5 feet, where either a 0-foot or 10-foot minimum setback is required per SDMC Section 131.0543(b); and
 5. Allow a maximum structure height at 310.85 feet Above Mean Sea Level (AMSL), exceeding the AAOZ surface by 35.85 feet per SDMC Section 132.0205, which overlays the project site.
- c. Landscaping (planting, irrigation and landscape related improvements);
 - d. Off-street parking; and
 - e. Public and private accessory improvements determined by the Development Services Department to be consistent with the land use and development standards for this site in accordance with the adopted community plan, the California Environmental Quality Act [CEQA] and the CEQA Guidelines, the City Engineer's requirements, zoning regulations, conditions of this Permit, and any other applicable regulations of the SDMC.

STANDARD REQUIREMENTS:

1. This permit must be utilized within thirty-six (36) months after the date on which all rights of appeal have expired. If this permit is not utilized in accordance with Chapter 12, Article 6, Division 1 of the SDMC within the 36-month period, this permit shall be void unless an Extension of Time has been granted. Any such Extension of Time must meet all SDMC requirements and applicable guidelines in effect at the time the extension is considered by the appropriate decision maker. This permit must be utilized by _____ [3 years, including the appeal time].
2. No permit for the construction, occupancy, or operation of any facility or improvement described herein shall be granted, nor shall any activity authorized by this Permit be conducted on the premises until:
 - a. The Owner/Permittee signs and returns the Permit to the Development Services Department; and
 - b. The Permit is recorded in the Office of the San Diego County Recorder.
3. While this Permit is in effect, the subject property shall be used only for the purposes and under the terms and conditions set forth in this Permit unless otherwise authorized by the appropriate City decision maker.

4. This Permit is a covenant running with the subject property and all of the requirements and conditions of this Permit and related documents shall be binding upon the Owner/Permittee and any successor(s) in interest.
 5. The continued use of this Permit shall be subject to the regulations of this and any other applicable governmental agency.
 6. Issuance of this Permit by the City of San Diego does not authorize the Owner/Permittee for this Permit to violate any Federal, State or City laws, ordinances, regulations or policies including, but not limited to, the Endangered Species Act of 1973 [ESA] and any amendments thereto (16 U.S.C. § 1531 et seq.).
 7. The Owner/Permittee shall secure all necessary building permits. The Owner/Permittee is informed that to secure these permits, substantial building modifications and site improvements may be required to comply with applicable building, fire, mechanical, and plumbing codes, and State and Federal disability access laws.
 8. Construction plans shall be in substantial conformity to Exhibit "A." Changes, modifications, or alterations to the construction plans are prohibited unless appropriate application(s) or amendment(s) to this Permit have been granted.
 9. All of the conditions contained in this Permit have been considered and were determined necessary to make the findings required for approval of this Permit. The Permit holder is required to comply with each and every condition in order to maintain the entitlements that are granted by this Permit.
- If any condition of this Permit, on a legal challenge by the Owner/Permittee of this Permit, is found or held by a court of competent jurisdiction to be invalid, unenforceable, or unreasonable, this Permit shall be void. However, in such an event, the Owner/Permittee shall have the right, by paying applicable processing fees, to bring a request for a new permit without the "invalid" conditions(s) back to the discretionary body which approved the Permit for a determination by that body as to whether all of the findings necessary for the issuance of the proposed permit can still be made in the absence of the "invalid" condition(s). Such hearing shall be a hearing de novo, and the discretionary body shall have the absolute right to approve, disapprove, or modify the proposed permit and the condition(s) contained therein.
10. The Owner/Permittee shall defend, indemnify, and hold harmless the City, its agents, officers, and employees from any and all claims, actions, proceedings, damages, judgments, or costs, including attorney's fees, against the City or its agents, officers, or employees, relating to the issuance of this permit including, but not limited to, any action to attack, set aside, void, challenge, or annul this development approval and any environmental document or decision. The City will promptly notify Owner/Permittee of any claim, action, or proceeding and, if the City should fail to cooperate fully in the defense, the Owner/Permittee shall not thereafter be responsible to defend, indemnify, and hold harmless the City or its agents, officers, and employees. The City may elect to conduct its own defense, participate in its own defense, or obtain independent legal counsel in defense of any claim related to this indemnification. In the event of such election, Owner/Permittee

shall pay all of the costs related thereto, including without limitation reasonable attorney's fees and costs. In the event of a disagreement between the City and Owner/Permittee regarding litigation issues, the City shall have the authority to control the litigation and make litigation related decisions, including, but not limited to, settlement or other disposition of the matter. However, the Owner/Permittee shall not be required to pay or perform any settlement unless such settlement is approved by Owner/Permittee.

ENVIRONMENTAL/MITIGATION REQUIREMENTS:

11. Mitigation requirements in the Mitigation, Monitoring, and Reporting Program [MMRP] shall apply to this Permit. These MMRP conditions are hereby incorporated into this Permit by reference.

12. The mitigation measures specified in the MMRP and outlined in MITIGATED NEGATIVE DECLARATION NO. 588751, shall be noted on the construction plans and specifications under the heading ENVIRONMENTAL MITIGATION REQUIREMENTS.

13. The Owner/Permittee shall comply with the MMRP as specified in MITIGATED NEGATIVE DECLARATION NO. 587751 to the satisfaction of the Development Services Department and the City Engineer. Prior to issuance of any construction permit, all conditions of the MMRP shall be adhered to, to the satisfaction of the City Engineer. All mitigation measures described in the MMRP shall be implemented for the following issue areas:

- Noise (construction-related)

CLIMATE ACTION PLAN REQUIREMENTS:

14. Owner/Permittee shall comply with the Climate Action Plan (CAP) Consistency Checklist stamped as Exhibit "A." Prior to issuance of any construction permit, all CAP strategies shall be noted within the first three (3) sheets of the construction plans under the heading "Climate Action Plan Requirements" and shall be enforced and implemented to the satisfaction of the Development Services Department.

AFFORDABLE HOUSING REQUIREMENTS:

15. Prior to issuance of any construction permit for the building associated with this Project, the Owner/Permittee shall demonstrate compliance with the provisions of the Affordable Housing Density Bonus Regulations of Chapter 14, Article 3, Division 7 of the San Diego Municipal Code. The Owner/Permittee shall enter into a written Agreement with the San Diego Housing Commission which shall be drafted and approved by the San Diego Housing Commission, executed by the Owner/Permittee, and secured by a deed of trust which incorporates applicable affordability conditions consistent with the San Diego Municipal Code. The Agreement will specify that in exchange for the City's approval of the Project, which contains a 39% density bonus (10 units in addition to what is permitted by the underlying zoning regulations), alone or in conjunction with any incentives or concessions granted as part of Project approval, the Owner/Permittee shall provide four affordable density bonus units with rents of no more than 30% of 50% of AMI for no fewer than 55 years.

16. The Agreement referenced in the preceding paragraph will satisfy the requirements of San Diego Municipal Code section 143.1303(g) and therefore, exempts the Project from the Inclusionary Affordable Housing Regulations in Chapter 14, Article 2, Division 13 of the San Diego Municipal Code.

AIRPORT REQUIREMENTS:

17. Prior to issuance of building permits, the Owner/Permittee shall provide a valid "Determination of No Hazard to Air Navigation" issued by the Federal Aviation Administration [FAA], for the building and temporary construction crane.

18. Owner/Permittee shall assure the structure is marked and lighted in accordance with Determinations of No Hazard to Air Navigation for the building and a Determination of No Hazard to Air Navigation for temporary crane structure.

19. Prior to the issuance of any construction permit for the building, the Owner/Permittee shall grant an aviation easement to the San Diego County Regional Airport Authority as the operator of the San Diego International Airport for the purposes of aircraft operations, noise and other effects caused by the operation of aircraft, and for structure height if the same would interfere with the intended use of the easement. The Owner/Permittee shall use the aviation easement form provided by the San Diego County Regional Airport Authority.

20. Prior to submitting building plans to the City for review, the Owner/Permittee shall place a note on all building plans indicating that an aviation easement has been granted across the property to the airport operator. The note shall include the County Recorder's recording number for the aviation easement.

ENGINEERING REQUIREMENTS:

21. Prior to the issuance of any construction permit for the building, the Owner/Permittee shall obtain a bonded grading permit for the grading proposed for this project. All grading shall conform to requirements in accordance with the City of San Diego Municipal Code in a manner satisfactory to the City Engineer.

22. The project proposes to export 4,710 cubic yards of material from the project site. All excavated material listed to be exported, shall be exported to a legal disposal site in accordance with the Standard Specifications for Public Works Construction (the "Green Book"), 2015 edition and Regional Supplement Amendments adopted by Regional Standards Committee.

23. The drainage system for this project is private and will be subject to approval by the City Engineer.

24. Prior to the issuance of any construction permit for the building, the Owner/Permittee shall assure by permit and bond replacement of the damaged curb and gutter with new City standard curb and gutter, adjacent to the project site on Fourth Avenue, satisfactory to the City Engineer.

25. Prior to the issuance of any construction permit for the building, the Owner/Permittee shall obtain an Encroachment Maintenance Removal Agreement for the projecting balconies and SDG&E vault located in the Fourth Avenue right-of-way, satisfactory to the City Engineer.

26. Prior to the issuance of any construction permit for the building, the Owner/Permittee shall obtain an Encroachment Maintenance Removal Agreement for the curb outlet, landscape, irrigation and any temporary shoring system located in the Fourth Avenue right-of-way, satisfactory to the City Engineer.

27. Prior to the issuance of any construction permit for the building, the Owner/Permittee shall assure, by permit and bond, the construction of a new City standard 20-foot driveway, adjacent to the site on Fourth Avenue, satisfactory to the City Engineer.

28. Prior to the issuance of any construction permit for the building, the Owner/Permittee shall assure, by permit and bond, the reconstruction of the damaged/nonconforming portions of the existing sidewalk with the same scoring pattern City Standard Sidewalk, adjacent to the site on Fourth Avenue, satisfactory to the City Engineer.

29. Prior to the issuance of any construction permit for the building, the Owner/Permittee shall assure, by permit and bond, the removal of all private existing encroachments such as walls and fences in the right-of-way adjacent to the site on Fourth Avenue, satisfactory to the City Engineer.

30. Prior to the issuance of any construction permit, the applicant shall incorporate any construction Best Management Practices necessary to comply with Chapter 14, Article 2, Division 1 (Grading Regulations) of the Municipal Code, into the construction plans or specifications.

31. Prior to the issuance of any construction permit, the Owner/Permittee shall submit a Water Pollution Control Plan (WPCP). The WPCP shall be prepared in accordance with the guidelines in Part 2 Construction BMP Standards Chapter 4 of the City's Storm Water Standards.

GEOLOGY REQUIREMENTS:

32. Prior to the issuance of any construction permit (either for grading or building), the Owner/Permittee shall submit a geotechnical investigation report prepared in accordance with the City's "Guidelines for Geotechnical Reports" that specifically addressed the proposed construction plans. The geotechnical investigation report shall be reviewed for adequacy by the Geology Section of Development Services prior to the issuance of any construction permit.

LANDSCAPE REQUIREMENTS:

33. Prior to issuance of any construction permit for grading, the Owner/Permittee shall submit complete construction documents for the revegetation and hydro-seeding of all disturbed land in accordance with the City of San Diego Landscape Standards, Storm water Design Manual, and to the satisfaction of the Development Services Department. All plans shall be in substantial conformance to this permit (including Environmental conditions) and Exhibit "A", on file in the Development Services Department.

34. Prior to issuance of any construction permit for right-of-way improvements, the Owner/Permittee shall submit complete landscape construction documents for right-of-way improvements to the Development Services Department for approval. Improvement plans shall show, label, and dimension a 40-square-foot area around each tree which is unencumbered by utilities. Driveways, utilities, drains, water and sewer laterals shall be designed so as not to prohibit the placement of street trees.

35. Prior to issuance of any construction permit for the building (including shell), the Owner/Permittee shall submit complete landscape and irrigation construction documents, which are consistent with the Landscape Standards, to the Development Services Department for approval. The construction documents shall be in substantial conformance with Exhibit "A", Landscape Development Plan, on file in the Development Services Department. Construction plans shall provide a 40-square-foot area around each tree that is unencumbered by hardscape and utilities unless otherwise approved per §142.0403(b)5.

36. In the event that a foundation only permit is requested by the Owner/Permittee, a site plan or staking layout plan, shall be submitted to the Development Services Department identifying all landscape areas consistent with Exhibit "A", Landscape Development Plan, on file in the Development Services Department. These landscape areas shall be clearly identified with a distinct symbol, noted with dimensions, and labeled as 'landscaping area'.

37. The Owner/Permittee shall be responsible for the maintenance of all landscape improvements shown on the approved plans, including in the right-of-way, unless long-term maintenance of said landscaping will be the responsibility of another entity approved by the Development Services Department. All required landscape shall be maintained consistent with the Landscape Standards in a disease, weed, and litter free condition at all times. Severe pruning or "topping" of trees is not permitted.

38. If any required landscape (including existing or new plantings, hardscape, landscape features, etc.) indicated on the approved construction documents is damaged or removed during demolition or construction, the Owner/Permittee shall repair and/or replace in kind and equivalent size per the approved documents to the satisfaction of the Development Services Department within 30 days of damage or Certificate of Occupancy.

PLANNING/DESIGN REQUIREMENTS:

39. All automobile, motorcycle and bicycle parking spaces must be constructed in accordance with the requirements of the SDMC. All on-site parking stalls and aisle widths shall be in compliance with requirements of the City's Land Development Code and shall not be converted and/or utilized for any other purpose, unless otherwise authorized in writing by the appropriate City decision maker in accordance with the SDMC.

40. Prior to issuance of any construction permit for the building, the Owner/Permittee shall submit an exterior to interior noise analysis identifying appropriate sound transmission reduction measures necessary to achieve an interior noise level that would not exceed 45 dBA.

41. A topographical survey conforming to the provisions of the SDMC may be required if it is determined, during construction, that there may be a conflict between the building(s) under construction and a condition of this Permit or a regulation of the underlying zone. The cost of any such survey shall be borne by the Owner/Permittee.
42. All signs associated with this development shall be consistent with sign criteria established by either the approved Exhibit "A" or City-wide sign regulations.
43. All private outdoor lighting shall be shaded and adjusted to fall on the same premises where such lights are located and in accordance with the applicable regulations in the SDMC.

PUBLIC UTILITIES DEPARTMENT REQUIREMENTS:

44. Prior to the issuance of any construction permit for the building, the Owner/Permittee shall apply for a plumbing permit for the installation of appropriate above ground private back flow prevention device(s), on each water service (domestic, fire and irrigation), in a manner satisfactory to the Public Utilities Director and the City Engineer. BFPD's are typically located on private property, in line with the service and immediately adjacent to the right-of-way. The Public Utilities Department will not permit the required BFPDs to be located below grade or within the structure.
45. Prior to the issuance of any construction permit for the building, the Owner/Permittee shall assure, by permit and bond, the design and construction of any new water and sewer service(s) outside of any driveway, and the disconnection at the water main of the existing unused water service adjacent to the project site, in a manner satisfactory to the Public Utilities Director and the City Engineer.
46. Prior to issuance of any construction permit for the building, provide an Encroachment Maintenance and Removal Agreement for, and prior to, proposed improvements of any kind, including utilities, landscaping, enriched paving, and electrical conduits to be installed within the public right-of-way.
47. All proposed private sewer facilities located within a single lot are to be designed to meet the requirements of the California Plumbing Code and will be reviewed as part of the building permit plan check.
48. All proposed public water and sewer facilities must be designed and constructed in accordance with established criteria in the most current edition of the City of San Diego Water and Sewer Facility Design Guidelines and City regulations, standards and practices pertaining thereto.
49. All on-site water and sewer facilities shall be private.
50. No trees or shrubs exceeding three feet in height at maturity shall be installed within ten feet of any sewer and five feet of any water facilities.

INFORMATION ONLY:

- The issuance of this discretionary permit alone does not allow the immediate commencement or continued operation of the proposed use on site. Any operation allowed by this discretionary permit may only begin or recommence after all conditions listed on this permit are fully completed and all required ministerial permits have been issued and received final inspection.
- Any party on whom fees, dedications, reservations, or other exactions have been imposed as conditions of approval of this Permit, may protest the imposition within ninety days of the approval of this development permit by filing a written protest with the City Clerk pursuant to California Government Code-section 66020.
- This development may be subject to impact fees at the time of construction permit issuance.

APPROVED by the City Council of the City of San Diego on _____ and Resolution No. _____.

DRAFT

Permit Type/PTS Approval No.: SDP No. 2243451
Date of Approval: _____

AUTHENTICATED BY THE CITY OF SAN DIEGO DEVELOPMENT SERVICES DEPARTMENT

Firouzeh Tirandazi
Development Project Manager

**NOTE: Notary acknowledgment
must be attached per Civil Code
section 1189 et seq.**

The undersigned Owner/Permittee, by execution hereof, agrees to each and every condition of this Permit and promises to perform each and every obligation of Owner/Permittee hereunder.

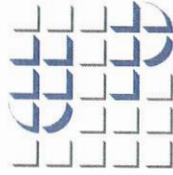
FOURTH AND LAUREL, LP
a California limited partnership
Owner

By _____
Richard Simis
Manager

RICHARD SIMIS
Permittee

By _____
Richard Simis

**NOTE: Notary acknowledgments
must be attached per Civil Code
section 1189 et seq.**



UPTOWN PLANNERS
Uptown Community Planning Group

October 2, 2018
FINAL MINUTES

Call to order by Chair Wilson at 6:05.

In attendance: William Smith, Jr., Jay Newington, Jennifer Pesqueira, Bill Ellig, Amie Hayes, Ken Tablang, Tim Gahagan, Mat Wahlstrom, Michael Brennan, Leo Wilson, Soheil Nakhshab, Stuart McGraw, Tom Mullaney (arrived during Item III.), Bob Daniel, Roy Dahl, Dennis Seisun, Kyle Schertzing.

Absent: none

Board Meeting: Parliamentary Items/Reports:

Introductions

Adoption of Agenda and Rules of Order: Motion by Wahlstrom, seconded by Dahl, to approve the agenda; approved by unanimous voice vote

Approval of Minutes: Motion to approve September 4, 2018 minutes approved by unanimous vote.

Treasurer's Report: balance remained the same as prior month

Chair/CPC Report – none

Public Communication:

Melissa L. from University Heights stated that October is breast cancer awareness month;

Roy McMakin spoke on behalf of the recently formed Bankers Hill 150 group, which was formed to encourage good planning in Bankers Hill and encourage the retention of an approximately 150-foot height limit on buildings in Bankers Hill;

Patrick Santana spoke regarding the need for more density and housing in Uptown;

Ian Epley also spoke regarding the need for more development, and questioned the amount of historic preservation that was being proposed;

Chris Duggan, the Director of Local Government Affairs for the California Restaurant Association, introduced himself and spoke briefly on issues impacting the businesses he represented;

Mat Wahlstrom pointed out the Deco (now Discover) bike stations often did not have bicycles docked in them, and inquired if the stations could be removed.

Leo Wilson announced that the Metro San Diego Community Development Corporation and St. Paul's Cathedral were cosponsoring a series of public meetings to discuss how St. Paul's Cathedral should utilize the ground floor public space that would be created as part of the 6th & Olive project.

Representatives of Elected Officials:

The following item was continued from the September 4, 2018 Uptown Planners meeting:

- 1. REQUEST FOR LETTER OF SUPPORT FOR STYROFOAM/SINGLE USE PLASTIC BAN– Action Item** --Brian Elliott, representing Councilmember Chris Ward, will provide information on the proposal to ban the sale and distribution of polystyrene foam (Styrofoam) products, and place restrictions on single use plastics in the City of San Diego. The proposal was approved by a 3-2 vote of the San Diego City Council Rules Committee in July 2018, and will be voted on by the full City Council later in 2018. The item was first heard by Uptown Planners at its September 5, 2018 meeting.

Public Comment

Chris Duggan, of the California Restaurant Association, spoke in opposition to the proposed Styrofoam/Single Plastic Use Ban; stating its costs would be exorbitant and that there was not a need to ban Styrofoam as it could be recycled. Duggan stated that restaurants were already faced with increased minimum wage and other costs, and the additional cost of replacing Styrofoam and single use plastic would impose an undue financial burden on local businesses.

Brian Elliott, who made the presentation about the proposed ban at the August meeting, was not in attendance at the October meeting.

Roy McMakin, Patrick Santana and Ian Epley spoke in favor of the proposed ban.

Board Comment

Gahagan expressed disappointed that a representative from the city council office was not present to answer questions, and thought the proposed ban focused on only a small part of the plastic waste problem. McGraw expressed concerns over the impact on small business; Wilson did also, although indicating he was supportive of the ban. Dahl restated his question from the last meeting -- how much would alternative products cost as opposed to products being banned: Duggan estimated they would cost 3 times to 10 times as much.

Ellig expressed support for the ban, which he stated had been done successfully elsewhere, but still had unanswered questions. Seisun supported the ban, as it was the only way to phase out the products in question. Daniel could not support the ban in its present form, particularly if Styrofoam was recyclable. Pesqueira indicated that at one particular restaurant, it was estimated ban would result in an additional \$500,000 in costs. She was concerned over the added costs and the impact to her businesses customers. Smith indicated he opposed the ban.

There was no motion as the board felt it needed more information before it could vote. .

Zach Bunshaft, representing Congressmember Susan Davis spoke about her activities in Congress, and passed out copies of the Congress member's newsletter.

Consent Agenda: No items placed on consent.

Information Item: Planning

- 1. CALTRANS UPDATE REGARDING SR-163 BETWEEN ROBINSON STREET & INTERSTATE 8 –** Hayden Manning, CALTRANS – will discuss maintenance and current and planned improvements taking place along Interstate 8 between SR-163 and Robinson Street; as well as issues involving trash and litter removal, and the area's homeless population. Gustavo Delarda presents the various projects and safety improvements going on in Cal Trans district 11. Many of these projects are paid for by SB1.

Presentation by Gustavo Delarda and Hayden Manning, representing CALTRANS, about current construction work and planned improvements along the SR-163 corridor; which would be done in phases over the next several years.

Public Comment

Ian Epley expressed concern over the congestion that the project would cause at interchanges along SR-163, and the adjacent areas of Washington Street and University Avenue. Sol Schumer inquired about the plan to place a high friction surface paving on State Route 163, to prevent skidding. Schumer also suggested improvements to the Caltrans website. Melissa L. from University Heights inquired about the plans for a detour when Taylor Street was closed, and how it would affect the flow of traffic.

Board Comment

Board member comment including inquires about how funding was being allocated, and how much of SB1 money was being used for transit. The issue of graffiti removal and homeless encampments on Caltrans property were also raised. Dahl pointed out there was no way traffic going north on SR-163 can exit west on Robinson Street, likewise there was an issue of access off Taylor Street to westbound I-8. Also concerns were raised about the impact of metering on traffic congestion on Washington Street and University Avenue traffic. In the future autonomous vehicles would be tested on several roads in San Diego County. Sand and debris from construction activity had been experienced on the roadway, and questions were raised about lights being on out section SR-163.

Action Items: Projects

1. 2426 FOURTH AVENUE APARTMENTS NDP – Process Two – Bankers Hill/Park West –

Neighborhood Development Permit for the construction of a six-story, 48,573 sq. ft. mixed use development, with 36 apartment units over 9,890 sq. ft. underground parking garage on an existing vacant lot (10% Affordable Units @ 65 AMI) located at 2426 Fourth Avenue. The 0.23 acre site is located in the CC-3-9 zone; SDIA 77, AIA, AAOZ.

Luis Arellano made the presentation on behalf of the applicant, pointed out project would provide workforce housing in an urban neighborhood near Downtown.

Public Comment

Ian Epley and Roy McMakin supported the project, and spoke in favor of the smaller unit mix, which would provide additional affordable housing. Carol Emerick questioned the height of the building, and was informed it was 68', which was about the height of the condo complex on the east side of Fourth Avenue across the street. Sol Schumer inquired if there would be any retail; the response was that retail would be included, and two parking spaces would be created for commercial users. Brer Marsh spoke in favor of the project.

Board Comment

During board comment Ellig clarified that the rent for the studio units would range from \$1800-\$2200 a month. Dahl and Gahagan inquired about parking; pointing out there was a shortage of parking in the neighborhood the project was located. Tablang also expressed concern about the lack of street parking in the neighborhood.

The applicant indicated it had received a negative recommendation from the Airport Authority stating the project did not comply with the Airport Land Use Plan; this decision could be overridden by the City Council by a supra majority vote. Wilson pointed out the project likely also was in violation of the City's Airport Approach Overlay Ordinance ("AAOZ"), which does not have an exception clause. That issue would be determined later on in the approval process; Wilson supported the project as long as it complied with airport safety regulations.

Brennan supported the project. Nakhshab supported the project, and believed the City Council should override the Airport Authority decision. Hayes supported the project, and appreciated it

supplied workforce housing. Smith supported adding density and supported the project. Schertzing supported the project, and suggested high quality materials and sustainability components. Mullaney pointed out with the elevator shaft, the project is 75-feet in height, and expressed concern over parking.

Motion by Nakhshab, seconded by Daniel, to support the project as proposed; Motion passed by a 15-1-1 vote, Dahl voting against the motion because of insufficient parking, and non-voting chair Wilson abstaining.

2. 1603 LINWOOD STREET ROW VACATION (“STATE STREET – ROW VACATION”) – Process Five – Mission Hills – Right of Way street vacation; would vacate an unimproved section of State Street that separates the owner’s lots at 1603 Linwood Street.

Scott Case, the applicant, made a brief presentation regarding the reason he sought a street vacation. He had previously submitted a letter in support of his request for a street vacation, which had been distributed to the board prior to the meeting. The area being vacated is a paper street, surrounded on three sides by applicant’s property. It has been maintained as a landscaped area associated with his property for over 30-years. City staff has indicated that are not opposed to the paper street being vacated.

There was no public comment, and after board discussion the following motion was passed.

Motion by Nakhshab, seconded by Daniel, to approve the street vacation request as presented; Motion passed by a 16-0-1 vote; with non-voting chair Wilson abstaining.

Action Items: Planning

1. Proposed Amendments to the City of San Diego’s Historic District Formation Process –

The proposed amendments would provide for increased property owner noticing, and change the appeal process for a Historic Resources Board historic district determination. At present, the City Council hears appeals of HRB approvals of potential historic districts. The amendments would shift the hearing of appeals to the city’s Planning Commission. If a proposed historic district has the support of the majority of property owners, and is approved by the HRB, it would be subject to appellate review by the Planning Commission. If a proposed historic district does not have support of the majority of the proposed district’s property owners, it can only be approved by the HRB if it is “exceptional important” historically; and would be subject to a de novo hearing upon appeal to the Planning Commission.

The Uptown Planners Historic Resources Subcommittee unanimously approved the following motions regarding the proposed amendments:

- 1) The HRC/Joint NP Subcommittee approves the historic district amendments proposed with the strong exception to change of the decision maker, and for any appeal to trigger a de novo hearing. Motion by M. Wahlstrom, second J. Newington. Vote: 8-0.
- 2) The HRC/Joint NP Subcommittee supports HRB guideline amendments related to increased noticing for the establishment of new Historical Districts. Motion by D. Carlson, second by R. Dahl. Vote: 8-0.
- 3) The HRC/Joint NP Subcommittee does not support amendments that will weaken the current historical district ordinance including changing the decision maker to the Planning Commission and for any appeal to trigger a de novo hearing. Motion by D. Carlson, second by M. Wahlstrom. Vote: 8-0.

Subcommittee Chair Hayes presented the background of the proposed to amendments to the HRB guidelines for forming historic districts and moved the subcommittee motions:

Public Comment

Barry Hager, of Mission Hills Heritage, spoke in favor of the recommendations. Sharon Gehl, Ian Epley and Clint Epley spoke against the recommendations; opposing historic districts being formed without the support of a majority of the property owners. Mark Hagler, a Upas Street property owner, spoke against the recommendations. Brer Marsh spoke in favor of the recommendations.

Board Comment

Mullaney, Dahl and Wahlstrom spoke in support of the subcommittee motions. Gahagan and McGraw supported historic districts if the majority of the property owners in the proposed district supported their creation. Wilson and Selsun spoke against the Planning Commission making the final decision regarding the establishment of a historic district; which they felt should remain with the City Council. Smith opposed establishing historic districts; believing it should be an individual property owner decision.

Motion One

Motion by Dahl, seconded by Wahlstrom, to support the HRC/Joint NP Subcommittee motion supporting HRB guideline amendments related to increased noticing for the establishment of new historic districts; Motion passed by a 15-1-1 vote; with McGraw voting against, and non-voting chair Wilson abstaining.

Motion Two

Motion by Wahlstrom, seconded by Newington, to support The HRC/Joint NP Subcommittee motion opposing the amendment changing the decision maker to the Planning Commission (instead of the City Council) and for appeals to the City Council to trigger a de novo review; Motion passed by a 14-2-1, with Mullaney and Dahl against, and non-voting chair Wilson abstaining. Mullaney voting against because he felt the question should not have been called and there should have been further decision of the motion.

Letters of Support -- none

Planning Staff/ Community Reports

1. **Historic Resources Subcommittee** – Action Item – Amie Hayes -- The subcommittee met on August 2, 2018. It discussed the proposed Park Boulevard Residential Historic District, and reviewed the proposed district's context statement, boundaries, and reasons for designation. Board members from the Greater North Park Planning Group were in attendance. The subcommittee passed the following motion:

The HRC/Joint NP Subcommittee endorses the May 30, 2018 Memorandum to the HRB Policy Subcommittee, with subject as Park Boulevard Residential Historic District, regarding establishment of the Park Blvd. Historic District as presented: Motion by Wahlstrom; second Carlson. Motion passed 8 -0.

Subcommittee Chair Hayes presented the background and context of the proposed Park Boulevard Residential Historic District.

Public Comment

Ian Epley spoke against the proposed historic district. Mark Hagler, a Upas Street property owner, spoke against the district. Also opposing were Clint Daniel and Patrick Santana, who stated the proposed district was along a transit corridor that need to have more housing. Patrick Santana questioned if the architectural styles in the proposed district were consistent. Sharon Gehl opposed the district, and did not find the buildings in it to have special qualities justifying a historic district.

Barry Hager supported the creation of the historic district, as did Brer Marsh, who felt the architecture of the proposed district's buildings was unique and needed to be preserved. Lu Rehling supported the proposed district, and inquired about the polling procedure.

Board Action

Given the late hour, the board decided to continue the item to a future meeting; Motion to continue item made by Wahlstrom, seconded by Newington; passed by a unanimous vote.

Adjournment at 9:13 by voice vote

Respectfully submitted,

Michael Brennan, Secretary
(Finalized by Leo Wilson)

RESOLUTION NO. 2018-0001 ALUC

A RESOLUTION OF THE AIRPORT LAND USE COMMISSION FOR SAN DIEGO COUNTY, MAKING A DETERMINATION THAT THE PROPOSED PROJECT: CONSTRUCTION OF 36 ATTACHED RESIDENTIAL UNITS WITH LEASABLE COMMERCIAL SPACE AT 2426 4TH AVENUE, CITY OF SAN DIEGO, IS NOT CONSISTENT WITH THE SAN DIEGO INTERNATIONAL AIRPORT - AIRPORT LAND USE COMPATIBILITY PLAN

WHEREAS, the Board of the San Diego County Regional Airport Authority, acting in its capacity as the Airport Land Use Commission (ALUC) for San Diego County, pursuant to §21670.3 of the California Public Utilities Code, was requested by the City of San Diego to determine the consistency of a proposed project: Construction of 36 Attached Residential Units with Leasable Commercial Space at 2426 4th Avenue, City of San Diego, which is located within the Airport Influence Area (AIA) for the San Diego International Airport (SDIA) Airport Land Use Compatibility Plan (ALUCP), adopted and amended in 2014; and

WHEREAS, the plans submitted to the ALUC for the proposed project indicate that it would involve the construction of 36 attached residential units with leasable commercial space; and

WHEREAS, the proposed project would be located within the 60-65 decibel (dB) Community Noise Equivalent Level (CNEL) noise contour, and the ALUCP identifies residential and commercial uses located within the 60-65 dB CNEL noise contour as compatible with airport uses, provided that the residences are sound attenuated to 45 dB CNEL interior noise level; and

WHEREAS, the proposed project is located outside the SDIA Threshold Siting Surface (TSS) height restrictions and is in compliance with the ALUCP airspace protection surfaces, provided that the structure is marked and lighted in accordance with a determination of no hazard to air navigation issued by the Federal Aviation Administration (FAA) and an aviation easement for airspace is recorded with the County Recorder; and

WHEREAS, the proposed project is located within Safety Zone 3 Northeast - Uptown, and the ALUCP identifies and the ALUCP identifies policies for the allocation of uses for mixed-use development to be compatible with airport uses based upon applicable densities of units per acre and intensities of people per acre; and

WHEREAS, the proposed project is located within the overflight notification area, and the ALUCP requires a means of overflight notification to be provided for new residential land uses, which an aviation easement will satisfy; and

WHEREAS, the ALUC has considered the information provided by staff, including information in the staff report and other relevant material regarding the project; and

WHEREAS, the ALUC has provided an opportunity for the City of San Diego, and interested members of the public to present information regarding this matter.

NOW, THEREFORE, BE IT RESOLVED that the ALUC determines that the proposed project: Construction of 36 Attached Residential Units with Leasable Commercial Space at 2426 4th Avenue, City of San Diego, is not consistent with the SDIA ALUCP, which was adopted and amended in 2014, based upon the following facts and findings:

- (1) The proposed project involves the construction of 36 attached residential units with leasable commercial space; and
- (2) The proposed project is located within the 60-65 dB CNEL noise contour. The ALUCP identifies commercial uses located within the 60-65 dB CNEL noise contour as compatible with airport uses, and identifies residential uses located within the 60-65 dB CNEL noise contour as conditionally compatible with airport uses, provided that the residences are sound attenuated to 45 dB CNEL interior noise level; and
- (3) The proposed project is located outside the TSS. The proposed project would be compatible with the ALUCP airspace protection surfaces, provided that the structure is marked and lighted in accordance with a determination of no hazard to air navigation issued by the FAA and an aviation easement for airspace is recorded with the County Recorder; and
- (4) The proposed project is located within Safety Zone 3 Northeast (SZ 3NE) - Uptown. The ALUCP has a maximum intensity under the ALUCP of 278 people per acre. The ALUCP specifies that for mixed-use projects, half of the total people per acre compatibility threshold is applied to the nonresidential intensity and the remaining half to the residential density, as converted to an intensity figure based upon persons per household, as specified by the ALUCP within the corresponding zone. The property of 0.23 acres yields a total intensity of 64 people. Assigning half of those 64 people to the residential portion, at the ALUCP rate of 1.48 people per household for SZ

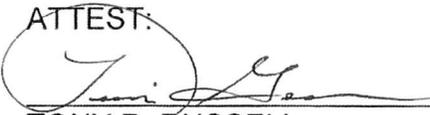
3NE, yields 22 residential units as a maximum compatible threshold. The other 32 people for the nonresidential half of the mixed-use project is multiplied by the ALUCP's most intense commercial occupancy factor of 170 people to yield a total maximum area of 5,440 square feet as compatible with the ALUCP. While the 1,085 square feet of commercial space is compatible, the project proposes 36 residential units, which exceeds the maximum compatible density of 22 units, and therefore does not comply with the SZ 3NE and mixed-use project policies of the ALUCP; and

- (5) The proposed project is located within the overflight notification area. The ALUCP requires a means of overflight notification to be provided for new residential land uses, for which an avigation easement will satisfy; and
- (6) Therefore, the proposed project is not consistent with the SDIA ALUCP; and

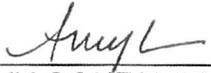
BE IT FURTHER RESOLVED that the ALUC finds this determination is not a "project" as defined by the California Environmental Quality Act (CEQA), Cal. Pub. Res. Code §21065, and is not a "development" as defined by the California Coastal Act, Cal. Pub. Res. Code §30106.

PASSED, ADOPTED AND APPROVED by the ALUC for San Diego County at a regular meeting this 4th day of October, 2018, by the following vote:

- AYES: Board Members: Boling, Cox, Desmond, Lloyd, Schumacher West
- NOES: Board Members: Robinson
- ABSENT: Board Members: Schiavoni
- ABSTAIN: Board Members: Kersey

ATTEST:

 for: TONY R. RUSSELL
 DIRECTOR, BOARD SERVICES /
 AUTHORITY CLERK

APPROVED AS TO FORM:



 AMY GONZALEZ
 GENERAL COUNSEL

RESOLUTION NUMBER R- 312874

DATE OF FINAL PASSAGE MAR 09 2020

ITEM# 202B
3/9/20

A RESOLUTION OF THE COUNCIL OF THE CITY OF SAN DIEGO DECLARING ITS PROPOSAL TO OVERRULE THE SAN DIEGO AIRPORT LAND USE COMMISSION'S DETERMINATION THAT THE PROPOSED 4th AVENUE APARTMENTS PROJECT IS INCONSISTENT WITH THE AIRPORT LAND USE COMPATIBILITY PLAN FOR THE SAN DIEGO INTERNATIONAL AIRPORT.

WHEREAS, Fourth and Laurel, a California limited partnership, Owner, and Richard Simis, Permittee, are proposing the construction of a 36-dwelling unit mixed-use development known as the 4th Avenue Apartments project (Project) on a vacant 0.23-acre property located at 2426 4th Avenue, between Laurel Street and Kalmia Street, within the Uptown Community Plan area and legally described as Lot "H" and "I" in Block 280 of Horton's Addition, in the City of San Diego, County of San Diego, State of California, According to Map thereof by LL.

Locking, Filed in the Office of the County Recorded of San Diego County; and

WHEREAS, the site is in the CC-3-9 Zone, Very High Fire Hazard Severity Overlay Zone, Community Plan Implementation Overlay Zone (CPIOZ-A), Residential Tandem Parking Overlay Zone, Transit Priority Area, Airport Influence Area for San Diego International Airport (SDIA) – Review Area 1, SDIA 60-65 Community Noise Equivalent Level (CNEL), SDIA Safety Zone 3 Northeast (3NE), Federal Aviation Administration (FAA) Part 77 Notification Area for SDIA and North Island Naval Air Station (NAS), the Airport Environs Overlay Zone (AEOZ), and the Airport Approach Overlay Zone (AAOZ) for SDIA; and

WHEREAS, the Project must be submitted to the San Diego County Regional Airport Authority (SDCRAA), serving as the Airport Land Use Commission (ALUC), for a consistency

determination with SDIA Airport Land Use Compatibility Plan (ALUCP) because of the Project's proposed density and location within SDIA-Review Area 1; and

WHEREAS, at its meeting on October 4, 2018, the SDCRAA, acting in its capacity as the ALUC, reviewed the Project and determined it inconsistent with the ALUCP because it exceeds the ALUCP's allowable intensity factor for mixed-use development with respect to residential density for the area in which it is located, Safety Zone 3 Northeast (3NE); and

WHEREAS, California Public Utilities Code (CPUC) section 21676.5(a) grants the City Council the authority to overrule a determination of inconsistency from the ALUC if the governing body undertakes a two-part process, with both parts requiring a two-thirds vote as follows: (1) makes proposed findings regarding purpose and intent of Public Utilities Code section 21670, and (2) approved the overrule at a noticed public hearing; and

WHEREAS, San Diego Municipal Code (SDMC) section 132.1555 requires that for the City Council to overrule a determination of inconsistency, it must adopt not only the proposed findings regarding purpose and intent set forth in Public Utilities Code section 21670, but also findings that the development is not detrimental to the public health, safety, and welfare, and that the development will minimize the public's exposure to excessive noise and safety hazards to the extent feasible; and

WHEREAS, the matter was set for public hearing on March 9, 2020, testimony having been heard, evidence having been submitted, and the City Council having fully considered the matter and being fully advised concerning the same; and

WHEREAS, under Charter section 280(a)(2), this resolution is not subject to veto by the Mayor because this matter requires the City Council to act as quasi-judicial body and where a public hearing was required by law implicating due process rights of individuals affected by the

decision and where the City Council was required by law to consider evidence at the hearing and to make legal findings based on the evidence present; and

WHEREAS, pursuant to CPUC section 21676.5(a) and SDMC section 132.1555, a two-thirds vote of the City Council is required for passage of this resolution; NOW, THEREFORE,

BE IT RESOLVED that the City Council declares its proposal to overrule the ALUC, finding that the Project is consistent with the purposes of CPUC section 21670, and adopts these findings as set forth in SDMC section 132.1555:

1. The proposed development will not be detrimental to the public health, safety, and welfare.

The Project proposes a residential and commercial mixed-use development consisting of 36 dwelling units, including four very-low income dwelling units, and an approximately 1,174 square-foot retail suite, parking and associated site improvements, with five development incentives in accordance with the City's Affordable Housing Regulations. The Project is a permitted use that is consistent with the Uptown Community Plan's Community Commercial land use designation, which allows up to 109 dwelling units per acre as part of a mixed-use development, the underlying CC-3-9 Zone, and the City's Affordable Housing Regulations. The Project site is located within the Uptown Community Plan's Neighborhood Village, which promotes very-high residential densities along major commercial transit corridors and proposes a development intensity which is consistent with the average intensity of the existing office and commercial mixed-use developments in the immediate area.

The Project site is located approximately 0.9 miles from the San Diego International Airport (SDIA) in a highly urban area. Project is an infill development subject to the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for SDIA. In 2014, the SDCRAA as the ALUC, adopted a comprehensive update to the ALUCP for SDIA. The ALUCP is intended to provide for the orderly development of the SDIA and the area surrounding the SDIA and protect public health, safety and welfare in areas around the SDIA.

For airspace protection, the ALUCP uses Threshold Sitting Surface (TSS) as the planning tool to limit building heights. The project site is outside of the TSS area and is therefore consistent with the ALUCP for airspace protection. The Project is also subject to the Federal Aviation Regulations (FAR), Part 77 which requires that projects submit application to the Federal Aviation Administration (FAA) to determine if the proposed buildings would be a hazard to air navigation. The City and the ALUCP also require that development projects obtain a "no hazard" to air navigation determination from the FAA. The Project received a determination of no hazard to air navigation from the FAA on July 18, 2017 and an extension of the determination on February 7, 2019. The proposed Project is consistent with the ALUCP for building height and has received a determination of no hazard from the FAA. Furthermore, the

Project has been reviewed pursuant to the California Environmental Quality Act, and the environmental analysis did not find any significant impacts to the public health and safety. Therefore, the proposed development will not be detrimental to the public health, safety, and welfare.

2. The proposed development will minimize the public's exposure to excessive noise and safety hazards to the extent feasible.

The Project will minimize the public's exposure to excessive noise and safety hazards to the extent feasible. The Project is located within the 60-65 decibel Community Noise Equivalent Level (dB CNEL) noise contour. The ALUCP for SDIA identifies commercial uses located within the 60-65 dB CNEL noise contour as compatible with airport uses, and identifies residential uses located within the 60-65 dB CNEL noise contour as conditionally compatible with airport uses, provided that the residences are sound attenuated to 45 dB CNEL interior noise level. The ALUCP for SDIA contains policies to ensure that noise attenuation is required to ensure an interior noise level of 45 dB CNEL for developments exposed to exterior aircraft noise levels 60 dB CNEL or greater. An acoustical report prepared for the Project evaluated traffic levels on Fourth Avenue and aircraft noise associated with the San Diego International Airport in relation to the proposed Project. The acoustical report indicates that the building face of the proposed Project would be exposed to a traffic noise level of 65.0 dB CNEL and that ultimate noise level from aircraft are estimated at 62 dB CNEL. The result is a combined noise level of 67 dB CNEL.

According to Table NE-3 of the General Plan, proposed projects having a noise level of 65-70 dB CNEL are "conditionally compatible" and must be capable of attenuating exterior noise to an indoor level of 45 dB CNEL. Based on calculations performed as part of the noise study, a minimum noise reduction of 22 dB would be achieved through provision of mechanical ventilation and acoustical treatments. Therefore, the proposed Project would meet the dB 45 CNEL interior noise level. The proposed Project will also be providing an avigation easement regarding the overflight of aircraft to the San Diego County Regional Airport Authority as a condition of approval as required by the Noise Element of the General Plan, and the ALUCP for SDIA.

3. The proposed development will meet the purpose and intent of the California Public Utilities Code Section 21670.

The purpose and intent of the California Public Utilities Code section 21670 is to "provide for the orderly development of each public use airport in the state and the area surrounding these airports so as to promote the overall goals and objectives of the California airport noise standards adopted pursuant to Section 21669 and to prevent the creation of new noise and safety problems." In addition, the purpose and intent of the section is also "to protect public health, safety, and welfare by ensuring the orderly expansion of airports and the adoption of land use measures that minimize the public's exposure to excessive noise and safety hazards within areas located around public airports to the extent that these areas are not already devoted to incompatible uses."

The proposed Project received a “no hazard” to air navigation determination from the FAA and is outside of Threshold Siting Surface used by the ALUCP for SDIA to limit building heights and protect airspace within the runway approach area; and therefore, the proposed development would not impact existing or an expansion of future airport operations.

An acoustical report prepared for the Project evaluated traffic levels on Fourth Avenue and aircraft noise associated with the San Diego International Airport in relation to the proposed Project. The acoustical report indicates that the building face of the proposed Project would be exposed to a traffic noise level of 65.0 dB CNEL and that ultimate noise level from aircraft are estimated at 62 dB CNEL. The result is a combined noise level of 67 dB CNEL. Based on calculations performed as part of the noise study, a minimum noise reduction of 22 dB would be achieved through provision of mechanical ventilation and acoustical treatments. The Project would have an interior noise level that would not exceed 45 dB CNEL for habitable areas consistent with the ALUCP for SDIA; and therefore, the proposed development would not expose the people living or working in the building to excessive noise hazards.

The proposed Project site is in a neighborhood with existing higher density multifamily and commercial uses. The proposed residential commercial mixed-use project is consistent with the adopted community plan land use. The ALUCP for SDIA identifies residential and commercial uses as compatible uses for the site of the proposed Project; and therefore, the Project uses are consistent with the ALUCP for SDIA.

The ALUCP for SDIA limits density and intensity for proposed uses by safety zone and neighborhood based on the average existing development intensity and density. Residential uses are measured by dwelling units per acre and commercial uses are measured by people per acre. For mixed-use development, the ALUCP allows for residential density to be converted to intensity measured by people per acre, based on a people per household factor in the ALUCP, which is then added to the commercial intensity for the total intensity. A maximum of 50 percent of the maximum site intensity may be applied for residential uses, and the Project exceeds that threshold.

The Project is located in the Uptown Safety Zone 3NE. The overall ALUCP people per acre intensity for that zone location is 278 people per acre, which equates to 64 people for the 0.23-acre site. The residential portion of this Project cannot exceed 50 percent, which is 32 people ($64/2$). The maximum compatible number of units would be 22, using the ALUCP factor of 1.48 people per household ($32/1.48$).

The other 32 people that comprise the nonresidential half of the mixed-use project is multiplied by the occupancy factor in the ALUCP to determine the maximum compatible commercial use. Retail sales has an occupancy factor of 170 square feet per person, which yields a total maximum area of 5,440 square feet as compatible with the ALUCP ($32 \times 170 = 5,440$). The nonresidential component of the Project, consisting of an approximately 1,174 square-foot retail suite, is significantly below the compatibility threshold. The Project would have a total intensity of 60 people (53 residents and 7 retail space occupants) which does not exceed the 64 people ALUCP limitation for the site.

California Public Utilities Code section 21670 directs Caltrans to prepare an Airport Land Use Planning Handbook (Handbook) to provide guidance to local ALUCs when preparing an ALUCP. Within high density areas with high-rise and mid-rise buildings, the Handbook allows for infill development at the average density and intensity of existing uses. The Project's proposed land use intensity is consistent with the existing average intensity for the high density residential and commercial mixed-use developments in the immediate area as shown in the following table. The existing office and mixed-use higher intensity developments shown in the table have an average of 351 people per acre. The Project would have an intensity of 261 people per acre; and therefore, the Project is consistent with the Handbook and CPUC section 21670.

Comparison between Existing Development and the Project							
Name	Address	Lot Size	Stories	Use	Non-Res Floor Area	Units	People per Acre
5 th & Laurel Financial Centre	2550 5 th Ave	1.38 Acres	18	Office	160,000	0	539
5 th & Laurel Corporate Center	2445 5 th Ave	0.36 Acres	6	Office	40,000	0	517
Imperial Towner Apartments	2399 5 th Ave	0.46 Acres	14	Mixed-Use	13,000	89	420
4 th Avenue Apartments	2455 4 th Ave	0.23	6	Mixed-Use	1,174	36	261
2400 6 th Apartments	2400 6 th Ave	0.35 Acres	11	Multi-Family	0	60	254
Laurel Bay	2400 5 th Ave	1.38 Acres	5	Mixed-Use	18,000	150	238
Park Laurel	2515 5 th Ave	1.34 Acres	14	Mixed-Use	10,000	94	139

The restriction of residential development based on a people per household factor and the 50 percent maximum site intensity that is applied to residential use for a mixed-use development per the ALUCP results in under-utilization of land and the inability of the Project to maximize density at a location along a major commercial transit corridor intended for high residential density. Furthermore, the Project's proposed land use intensity is consistent with existing high intensity office, residential, and commercial mixed-use developments in the immediate area. Based on the above, and considering the consistency and compatibility of the Project with the land use designation per the Uptown Community Plan, and the City's Affordable Housing regulations, the Project's intensity below the 64 people ALUCP limitation for the site, and below the average existing intensity of the high intensity development in the immediate area, and the Project's location in a highly urban area already devoted to similar land uses meets, the Project meets the intent of CPUC section 21670.

BE IT FURTHER RESOLVED, that based on the findings hereinbefore adopted by the City Council, the City Council proposes to overrule the determination of inconsistency by San Diego County Regional Airport Authority, acting as the Airport Land Use Commission for the San Diego County.

BE IT FURTHER RESOLVED, that staff is directed to send the Notice of Proposed Final Decision to Overrule to the ALUC, Caltrans Division of Aeronautics, and SDCRAA as the Airport Operator.

APPROVED: MARA W. ELLIOTT, City Attorney

By 

Corrine L. Neuffer
Deputy City Attorney

CLN:als
02/19/2020
Or.Dept:DSD
Doc. No.: 2332205

ATTACHMENT 9

Passed by the Council of The City of San Diego on MAR 09 2020, by the following vote:

Councilmembers	Yeas	Nays	Not Present	Recused
Barbara Bry	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Jennifer Campbell	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Chris Ward	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Monica Montgomery	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Mark Kersey	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Chris Cate	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Scott Sherman	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Vivian Moreno	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Georgette Gómez	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Date of final passage MAR 09 2020

(Please note: When a resolution is approved by the Mayor, the date of final passage is the date the approved resolution was returned to the Office of the City Clerk.)

AUTHENTICATED BY:

KEVIN L. FAULCONER
Mayor of The City of San Diego, California.

(Seal)

ELIZABETH S. MALAND
City Clerk of The City of San Diego, California.

By , Deputy

<p>Office of the City Clerk, San Diego, California</p> <p>Resolution Number R- <u>312874</u></p>
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Comparison between Existing Development and the 4th Avenue Apartments Project

Name	Address	Lot Size	Stories	Use	Non-Res Floor Area	Units	People per Acre
5th & Laurel Financial Centre	2550 5th Ave	1.38 Acres	18	Office	160,000	0	539
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4th Avenue Apartments	2455 4th Ave	0.23 Acres	6	Mixed-Use	1,174	36	261
2400 6th Apartments	2400 6th Ave	0.35 Acres	11	Multi-Family	0	60	254
Laurel Bay	2400 5th Ave	1.38 Acres	5	Mixed-Use	18,000	150	238
Park Laurel	2515 5th Ave	1.34 Acres	14	Mixed-Use	10,000	94	139

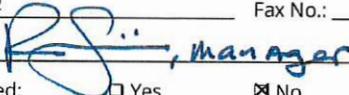
	City of San Diego Development Services 1222 First Ave., MS 302 San Diego, CA 92101 (619) 446-5000	<h1 style="margin: 0;">Ownership Disclosure Statement</h1>	FORM DS-318 October 2017
---	---	--	--

Approval Type: Check appropriate box for type of approval(s) requested: Neighborhood Use Permit Coastal Development Permit
 Neighborhood Development Permit Site Development Permit Planned Development Permit Conditional Use Permit Variance
 Tentative Map Vesting Tentative Map Map Waiver Land Use Plan Amendment Other _____

Project Title: Fourth Avenue Apartments **Project No. For City Use Only:** 588751
Project Address: 2426 4th Avenue
San Diego, CA 92101

Specify Form of Ownership/Legal Status (please check):
 Corporation Limited Liability -or- General - What State? CA Corporate Identification No. 82-3614461
 Partnership Individual

By signing the Ownership Disclosure Statement, the owner(s) acknowledge that an application for a permit, map or other matter will be filed with the City of San Diego on the subject property with the intent to record an encumbrance against the property. Please list below the owner(s), applicant(s), and other financially interested persons of the above referenced property. A financially interested party includes any individual, firm, co-partnership, joint venture, association, social club, fraternal organization, corporation, estate, trust, receiver or syndicate with a financial interest in the application. If the applicant includes a corporation or partnership, include the names, titles, addresses of all individuals owning more than 10% of the shares. If a publicly-owned corporation, include the names, titles, and addresses of the corporate officers. (A separate page may be attached if necessary.) If any person is a nonprofit organization or a trust, list the names and addresses of **ANY** person serving as an officer or director of the nonprofit organization or as trustee or beneficiary of the nonprofit organization. A signature is required of at least one of the property owners. Attach additional pages if needed. Note: The applicant is responsible for notifying the Project Manager of any changes in ownership during the time the application is being processed or considered. Changes in ownership are to be given to the Project Manager at least thirty days prior to any public hearing on the subject property. Failure to provide accurate and current ownership information could result in a delay in the hearing process.

Property Owner
 Name of Individual: Fourth and Laurel LP Owner Tenant/Lessee Successor Agency
 Street Address: 2820 Shelter Island Dr
 City: San Diego State: CA Zip: 92106
 Phone No.: 858-518-7372 Fax No.: _____ Email: Richard@nextspacedev.com
 Signature:  Date: 10/31/18
 Additional pages Attached: Yes No

Applicant
 Name of Individual: Richard Simis Owner Tenant/Lessee Successor Agency
 Street Address: 2820 Shelter Island Dr
 City: San Diego State: CA Zip: 92106
 Phone No.: 858-518-7372 Fax No.: _____ Email: Richard@nextspacedev.com
 Signature:  Date: 10/31/18
 Additional pages Attached: Yes No

Other Financially Interested Persons
 Name of Individual: _____ Owner Tenant/Lessee Successor Agency
 Street Address: _____
 City: _____ State: _____ Zip: _____
 Phone No.: _____ Fax No.: _____ Email: _____
 Signature: _____ Date: _____
 Additional pages Attached: Yes No

Fourth and Laurel LP

2820 Shelter Island Drive
San Diego, CA 92106

ATTACHMENT 11

December 4, 2018

Manager

Richard Simis-PBS Real Estate Investments LLC

Investor Capital

Andrew French

Bill Foster

Brian Keel

Christopher Ross

CJ Stotts

Darren Magildt

Dave & Cindy Osborne

Deborah N. Klein

Dennis Alviso

Frank Vretenar

Fred Register

Gary Tanner & Denise Chamblee

Harry R Bigham

Howard Kurshenbaum

Israel N Furmansky

Jay W Richen

Joel Roos

John Falconer

Joseph R John

Joseph Siemienowski

Lance Degrazler

Linda Belzberg

Lisa Gordon Arbittier

Louis E Vener

Louis Knierim

Louis M Galper

Maria Pum

Mark McKinnon

Melissa Reinard

Paul Braun

Robert F. Bernstein

Robert Yohanan

Russell Cahoon

Saul Klein

Scott A Paul

Scott Carstens

Steven Kitay

Thomas A Krause

Thomas Duran

Timothy R Penkala

Victor Laruccia

Wenkwei Lou

William W Winternitz

4TH AVENUE APARTMENTS

2426 4TH AVENUE

SAN DIEGO, CA 92101



PROJECT INFORMATION

PROJECT DIRECTORY:

OWNER:
NEXT SPACE DEVELOPMENT
2820 SHELTER ISLAND DRIVE
SAN DIEGO, CA 92106
ATTN: RUDY MEDINA
PH: 619.887.4047
EMAIL: rudy.medina@narcourtsusa.com

LANDSCAPE ARCHITECT:
MCULLOUGH LANDSCAPE ARCHITECTS
703 16TH STREET, STE. 100
SAN DIEGO, CA 92101
ATTN: DAVID MCULLOUGH
PH: 619.296.3150
FAX: 619.501.7725
EMAIL: david@mlaad.com

ARCHITECT:
AWBREY COOK ROGERS MCGILL
1945 14TH STREET, STE. 100
SAN DIEGO, CA 92101
ATTN: TOM AWBREY
PH: 619.398.3480
FAX: 619.398.3488
EMAIL: tom@acroma.com

CIVIL ENGINEER:
CIVIL LANDWORKS
115 COPPERWOOD WAY
OCEANSIDE, CA 92158
ATTN: DAVID CARON
PH: 760.968.8745
E-MAIL: dcaron@civilandworks.com

DEVELOPMENT DATA:

REQUIRED PERMITS:

- FAA PART 77 NOTIFICATION AREA
- AIRPORT OVERLAY ZONE

LEGAL DESCRIPTION:
LOTS H & I, BLOCK 280, HORTON'S ADDITION

OVERLAYS:

- AIRPORT APPROACH ZONE
- FIRE HAZARD SEVERITY ZONE
- TRANSIT AREA

PROPOSED BUILDING USE:
5 STORY RESIDENTIAL APARTMENTS OVER GROUND LEVEL COMMERCIAL & PARKING

GEOLOGICAL HAZARD CATEGORY:
S2

APN: 533-106-13-00

ZONING DESIGNATION: CC-3-9
IN ZONE CC-3-9 THERE IS NO ZONING STRUCTURE MAXIMUM HEIGHT LIMIT

EASEMENTS: NONE

BUILDING DATA:

F.A.R.

FAR ALLOWED: SECTION 131.0546 #2
3 x 10,000 = 30,000 SF
+10,060 SF (10,131 SF ACTUAL; BELOW GRADE PARKING BONUS)
TOTAL: 40,240 SF

FAR PROPOSED: 39,468 SF

OCCUPANCY TYPES:

TYPE R-2: RESIDENTIAL
TYPE M: MERCHANTILE
TYPE S-2: PARKING GARAGE

ALLOWABLE DWELLING UNITS:

10,060 / 400 = 25.15 WHICH CAN BE ROUNDED UP TO 26 UNITS.
4 UNITS AT THE "VERY LOW INCOME" LEVEL ARE PROVIDED WHICH IS 15% OF THE TOTAL UNITS BEING OFFERED AT AN AFFORDABLE LEVEL. PER TABLE 143-07A THIS GRANTS A DENSITY BONUS OF 50% AND 5 INCENTIVES.
26 BASE UNITS TIMES 1.5 IS 39 UNITS ALLOWED WITH THE DENSITY BONUS.

PARKING:

TOTAL PARKING REQUIRED: 24 SPACES
ACTUAL SPACES PROVIDED: 27 SPACES
SEE PARKING ANALYSIS ON SHEET A1.0

PROPOSED ENCLOSED FLOOR AREA:
SEE MATRIX ON SHEET A1.1

PROPOSED DWELLING UNITS:
SEE MATRIX ON SHEET A1.1

MOTORCYCLE PARKING:

REQUIRED: 4 SPACES
PROVIDED: 4 SPACES

BICYCLE PARKING:

REQUIRED: 15
PROVIDED: 15

BUILDING AREA & HEIGHT:

SITE AREA: 10,060 SF

SETBACKS:
FRONT: 0' MIN. 10' MAX.
SIDES: 10' MIN. 0' OPTIONAL
BACK: 10' MIN. 0' OPTIONAL

FULLY SPRINKLERED:
YES

ALLOWABLE HEIGHT PER CBC TABLE 504.3: 75'
ACTUAL BUILDING HEIGHT: 70'

CONSTRUCTION TYPES:

PODIUM: BASEMENT: FLOOR 1 TYPE IA
APARTMENTS: FLOORS 2-6: TYPE III-B

ALLOWABLE BUILDING AREA PER CBC TABLE 509.2.4:

TYPE IA: UNLIMITED
TYPE III-B: 48,000 SF

BUILDING FOOTPRINT:
9,397 SF

ACTUAL BUILDING AREA:

PODIUM - TYPE IA: UNLIMITED
BASEMENT: 10,131 SF
LEVEL 1: 9,296 SF
SUB-TOTAL: 19,427 SF

APARTMENTS - TYPE III-B: 48,000 SF
LEVEL 2: 9,449 SF
LEVEL 3: 7,176 SF
LEVEL 4: 7,148 SF
LEVEL 5: 7,176 SF
LEVEL 6: 7,183 SF
SUB-TOTAL: 38,132 SF

TOTAL: 57,559 SF

PROPOSED INCENTIVES REQUESTED:

- STRUCTURE HEIGHT INCREASE OVER CPOZ 65 FOOT HEIGHT LIMIT. REQUESTING BUILDING HEIGHT OF 70.15' AG. CC-3-9 ZONE DOES NOT HAVE A MAXIMUM HEIGHT STRUCTURE PER TABLE 131.056.
- ALLOW RESIDENTIAL USE WITHIN FRONT 30 FEET OF LOT.
- ELIMINATE PRIVATE EXTERIOR SPACE FOR 4) UNITS.
- SIDE AND REAR YARD SETBACK TO 1.625 FEET (SIDE) AND 1.5 FEET (REAR) WHERE 10' MINIMUM AND 0' OPTION IS ALLOWED. BUILDING MUST BE AT THE PROPERTY LINE OR 10 FEET FROM PROPERTY LINE.
- EXCEED THE AIRPORT APPROACH OVERLAY ZONE BUFFER THRESHOLD BY 8 FEET.

GENERAL

T1 TITLE SHEET

CIVIL

SHEET 1 PRELIMINARY GRADING PLAN
SHEET 2 PRELIMINARY GRADING PLAN

LANDSCAPE

L01.00 LANDSCAPE PLAN - LEVEL 1
L01.01 LANDSCAPE PLAN - LEVEL 2
L01.02 LANDSCAPE PLAN - LEVEL 6
L02.00 PLANTING PLAN - LEVEL 1
L02.01 PLANTING PLAN - LEVEL 2
L02.02 PLANTING PLAN - LEVEL 6

ARCHITECTURE

AFO.1 FIRE ACCESS PLAN
A0.1 PROPOSED SITE AND ACCESSIBILITY PLAN
A0.2 DRAFT CONDITIONS & SDGE VAULT INFORMATION (FOR REFERENCE ONLY)
A1.0 P1 LEVEL SUBTERRANEAN PARKING FLOOR PLAN
A1.1 GROUND LEVEL FLOOR PLAN
A1.2 2ND LEVEL FLOOR PLAN
A1.3 3RD LEVEL FLOOR PLAN
A1.4 4TH LEVEL FLOOR PLAN
A1.5 5TH LEVEL FLOOR PLAN
A1.6 6TH LEVEL FLOOR PLAN
A1.7 ROOF PLAN
A4.0 EAST ELEVATION
A4.1 SOUTH ELEVATION
A4.2 WEST ELEVATION
A4.3 NORTH ELEVATION
A5.1 BUILDING SECTION
A5.2 BUILDING SECTION
A5.3 BUILDING SECTION

VICINITY MAP



PROJECT DESCRIPTION:

THIS PROJECT INVOLVES THE CONSTRUCTION OF A 6 STORY ABOVE GRADE MULTIPLE OCCUPANCY STRUCTURE ON A CURRENTLY VACANT PLOT OF LAND WITH FOLLOWING FEATURES:

- BASEMENT AND GRADE LEVEL PARKING
- GRADE LEVEL LOBBY AND RETAIL TENANT
- 5 FLOORS OF APARTMENT TOTALING 36 UNITS

CONSTRUCTION WILL INVOLVE A CONCRETE PODIUM OF THE BASEMENT AND GRADE LEVELS WITH THE 5 STORY WOOD FRAMED APARTMENT BUILDING ABOVE.

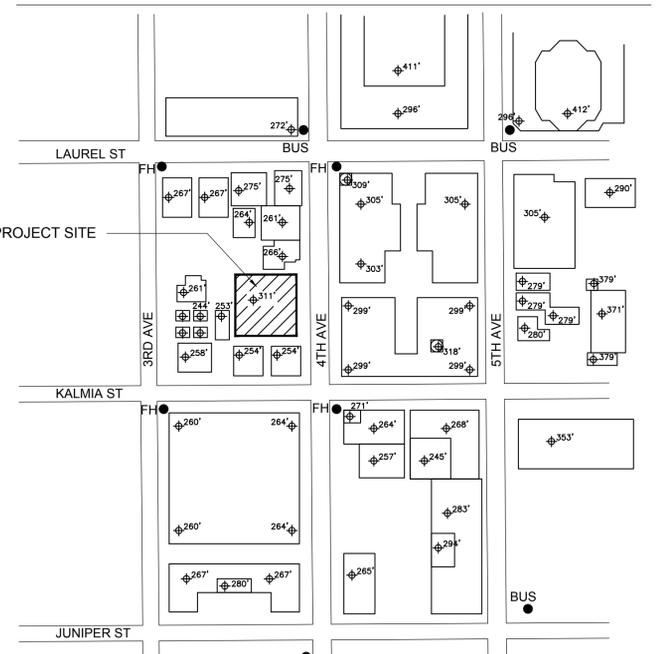
PROJECT NOTES:

- ADEQUATE NOISE ATTENUATION WILL BE PROVIDED FOR ALL RESIDENTIAL UNITS TO ENSURE AN INTERIOR NOISE LEVEL OF 45dB CNEL FOR ALL RESIDENTIAL HABITABLE ROOMS.

FIRE DEPARTMENT NOTES:

- FIRE APPARATUS ACCESS ROADS & WATER SUPPLIES FOR FIRE PROTECTION SHALL BE INSTALLED & MADE SERVICEABLE PRIOR TO & DURING CONSTRUCTION.
- RADIUS FOR FIRE HYDRANTS TO ALL PORTIONS OF THE EXTERIOR OF THE BUILDING IS BASED ON C105.1.
- A 3' MINIMUM CLEAR SPACE SHALL BE MAINTAINED AROUND THE CIRCUMFERENCE OF THE FIRE HYDRANTS UNLESS NOTED OTHERWISE.
- EVERY BUILDING 4 STORIES OR MORE IN HEIGHT SHALL BE PROVIDED WITH NOT LESS THAN 1 STANDPIPE FOR USE DURING CONSTRUCTION INSTALLED IN ACCORDANCE WITH CFC 3313.1. STAN DPIPE SHALL BE INSTALLED WHEN THE PROGRESS OF CONSTRUCTION IS NOT MORE THAN 40' IN HEIGHT ABOVE THE LOWEST LEVEL OF FIRE DEPARTMENT ACCESS. CFC 3313.1
- DECORATIVE MATERIALS SHALL BE MAINTAINED IN FLAME RETARDANT CONDITION.
- ALL BUILDINGS & SITES UNDER GOING CONSTRUCTION, ALTERATION, OR DEMOLITION SHALL COMPLY WITH THE REQUIREMENTS OF CFC CHAPTER 33.
- STAIRWAYS EXITING DIRECTLY TO THE EXTERIOR OF A BUILDING FOUR OR MORE STORIES IN HEIGHT SHALL BE PROVIDED WITH A MEANS FOR EMERGENCY ENTRY FOR FIRE DEPARTMENT ACCESS.

PROJECT SITE



COMMON EXTERIOR SPACE

FLOOR	AREA (SQ. FT.)
LEVEL 2	539 SF
LEVEL 6	589 SF
REQUIRED AREA	25SF x 36 = 900 SF
TOTAL COMMON AREA PROVIDED	1,128 SF

PRIVATE EXTERIOR SPACE

FLOOR	NO. OF UNITS	UNITS W/O BALCONIES*	SIZE (W.X.L.)	AREA (SQ. FT.)
LEVEL 2	8	4	4'-0.75"x13'-0"	52.8 SF
LEVEL 3	7	1**	4'-0.75"x13'-0" 5'-1.75"x12'-6" 5'-2.75"x13'-2"	52.8 SF 63.9 SF 68.4 SF
LEVEL 4	7	0	4'-0.75"x13'-0" 5'-1.75"x12'-6" 5'-2.75"x13'-2"	52.8 SF 63.9 SF 68.4 SF
LEVEL 5	8	0	4'-0.75"x13'-0" 5'-1.75"x12'-6" 5'-2.75"x13'-2"	52.8 SF 63.9 SF 68.4 SF
LEVEL 6	6	0	4'-0.75"x13'-0" 5'-1.75"x12'-6"	52.8 SF 63.9 SF

*INCENTIVE REQUESTED TO ADDRESS SDMC 131.0455(d) FOR THOSE UNITS NOT COVERED BY SDGE CLEARANCE REQUIREMENT FOR ELECTRICAL VAULT
**UNIT IMPACTED BY SDGE VAULT CLEARANCE REQUIREMENT; SEE SHEET A0.2 FOR MORE INFORMATION
• SEE FLOOR PLANS FOR DIMENSIONS

4TH AVENUE APARTMENTS

2426 4th AVENUE
SAN DIEGO, CALIFORNIA
92101

REVISIONS

NO	DESCRIPTION	DATE
	COMPLETENESS REVIEW	12.01.2017
	PLANNING PRELIMINARY SUBMITTAL	01.11.2018
	PLANNING SUBMITTAL	05.09.2018
	SDGE SUBMITTAL	07.09.2018
	PLANNING RESUBMITTAL	10.31.2018
	PLANNING RESUBMITTAL	12.19.2018
	PLANNING RESUBMITTAL	01.22.2018

SHEET TITLE:

TITLE SHEET

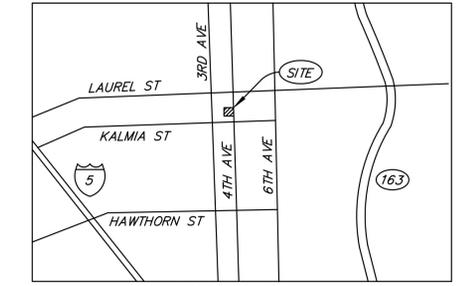


SHEET

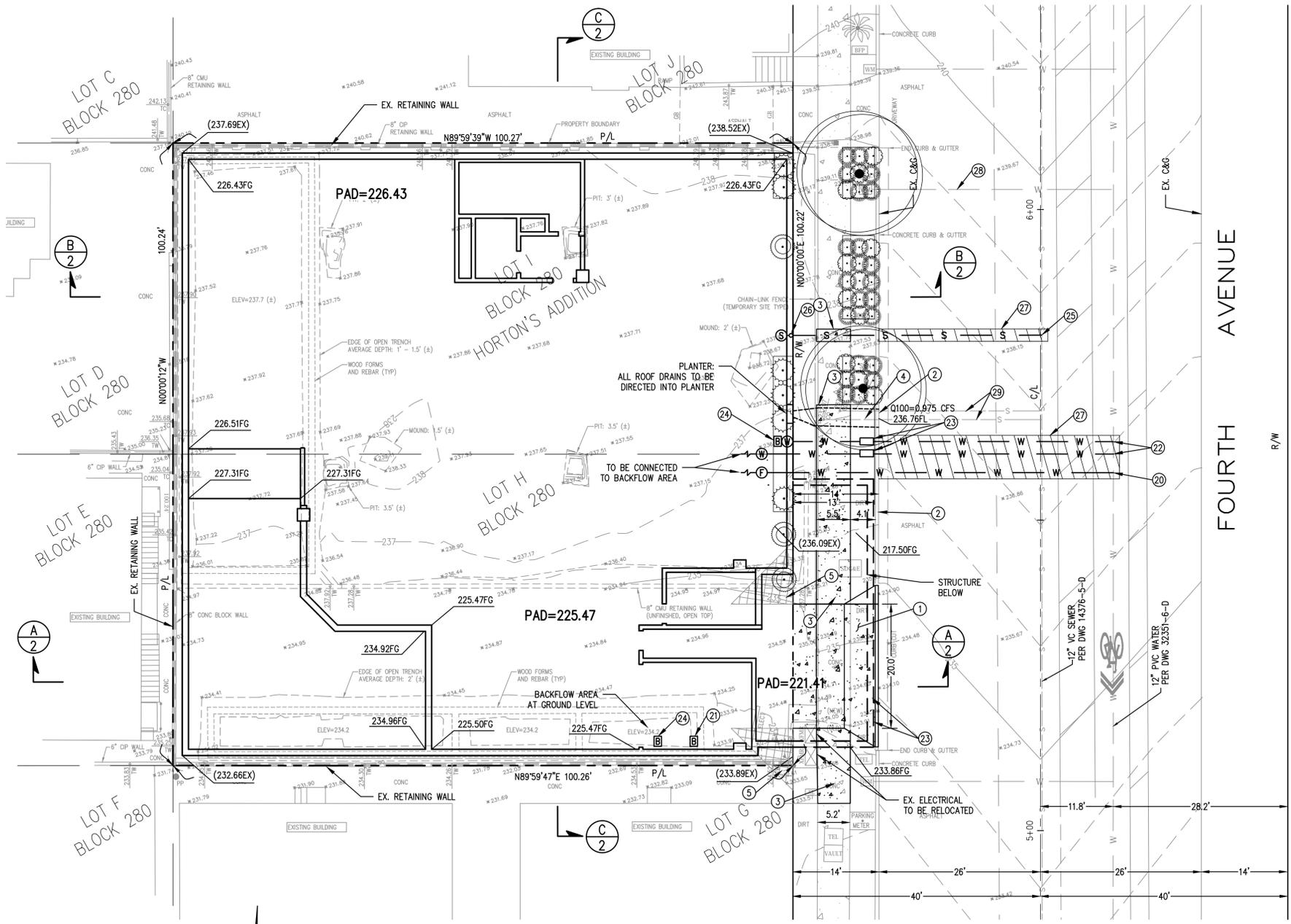
T1

PRELIMINARY GRADING PLAN FOR 2426 FOURTH AVENUE

NOT FOR CONSTRUCTION



VICINITY
NOT TO SCALE



OWNER

4TH AVENUE APARTMENTS, LLC
2820 SHELTER ISLAND DRIVE
SAN DIEGO, CA 92106

PROPERTY LEGAL DESCRIPTION

LOTS H AND I, BLOCK 280, HORTON'S ADDITION

ASSESSORS PARCEL NUMBER

2426 FOURTH AVENUE
533-106-13

TOPOGRAPHY & BASIS OF ELEVATIONS

TOPOGRAPHIC SURVEY IS BASED UPON A FIELD SURVEY CONDUCTED BY METROPOLITAN MAPPING, 3712 30TH STREET, SAN DIEGO, CA. SURVEY CONDUCTED ON 3-30-17.
CITY OF SAN DIEGO VERTICAL CONTROL BENCHMARK
MONUMENT DESCRIPTION: BRASS PLUG/TOP OF CURB
LOCATION: SOUTHEAST CORNER OF LAUREL STREET AND THIRD AVENUE
DATUM: MSL (NGVD 29)
ELEVATION: 242.54 FEET

EARTHWORK DATA

CUT = 4.715 C.Y. FILL = 5 C.Y.
EXPORT = 4.710 C.Y.

TOTAL LOT AREA = 10,060 S.F.
TOTAL DISTURBED AREA = 9,890 S.F.
DEPTH OF CUT = 15.0 FEET
DEPTH OF FILL = 1.5 FEET
*DEPTH OF CUT AND FILL DO NOT INCLUDE GEOTECHNICAL REMEDIATION RECOMMENDATIONS

THESE QUANTITIES DO NOT INCLUDE ANY LOSSES DUE TO SHRINKAGE, SUBSIDENCE, OVEREXCAVATION, OR ANY SPECIAL REQUIREMENTS THAT MAY BE SPECIFIED IN THE PRELIMINARY SOILS REPORT. THESE QUANTITIES ARE FOR PERMIT PURPOSES ONLY. ALL CONTRACTORS BIDDING ON THIS PROJECT SHOULD MAKE THEIR OWN DETERMINATION OF EARTHWORK QUANTITIES PRIOR TO SUBMITTING A BID.

ENGINEER

CIVIL LANDWORKS CORP.
110 COPPERWOOD WAY, SUITE P
OCEANSIDE, CA 92058
760-908-8745

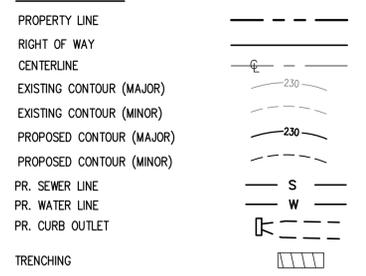


DAVID V. CARON 12-5-18

NOTES

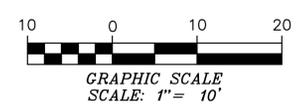
NO SOIL DISTURBANCE, NO SHORING, AND NO ENCROACHMENTS ARE PROPOSED INTO ADJACENT PROPERTIES

LEGEND:

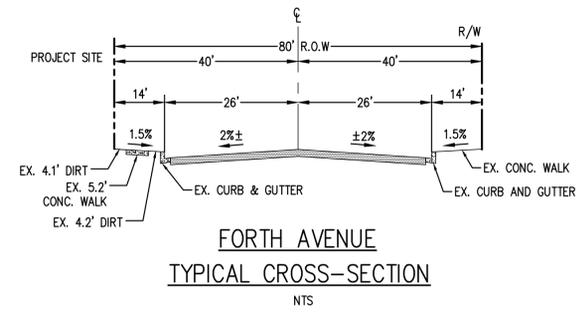


AREA TABULATIONS

EXISTING	PROPOSED
DRIVEWAY IMPERVIOUS= 0 SF	BUILDING IMPERVIOUS= 9,481 SF
WALKWAY IMPERVIOUS= 0 SF	BUILDING IMPERVIOUS (ENCROACH)= 414 SF
BUILDING IMPERVIOUS= 0 SF	PERVIOUS= 568 SF
PERVIOUS= 10,049 SF	
TOTAL= 10,049 SF	TOTAL= 10,463 SF
	TOTAL PROPOSED IMPERVIOUS AREA = 9,895 SF
	TOTAL PROPOSED PERVIOUS AREA = 568 SF



110 COPPERWOOD WAY, SUITE P, OCEANSIDE, CA 92058
PH: 760-908-8745 • info@civillandworks.com



CONSTRUCTION NOTES:

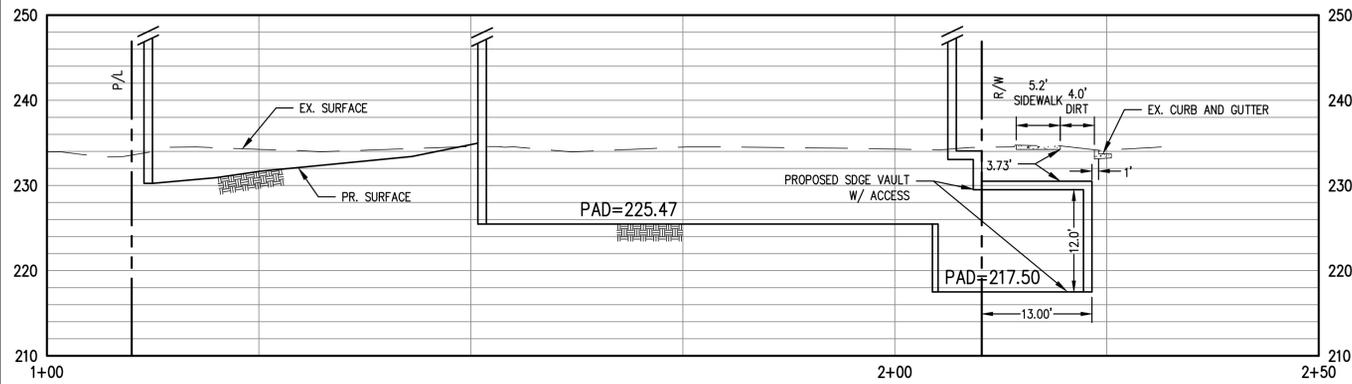
- CONSTRUCT CONCRETE DRIVEWAY PER CITY STANDARDS
- CURB AND GUTTER PER CURRENT CITY STANDARDS SDG-151
- REPLACE SIDEWALK PER CURRENT CITY STANDARDS SDG-155
- CURB OUTLET - TYPE A PER CURRENT CITY STANDARDS SDSRD D-25
- SITE VISIBILITY TRIANGLE AREA

NO OBSTRUCTION INCLUDING SOLID WALL IN THE VISIBILITY AREA SHALL EXCEED 3 FEET IN HEIGHT. PLANT MATERIAL, OTHER THAN TREES, WITHIN THE PUBLIC RIGHT OF WAY THAT IS LOCATED WITHIN VISIBILITY AREAS SHALL NOT EXCEED 24 INCHES IN HEIGHT, MEASURED FROM THE TOP OF THE ADJACENT CURB.

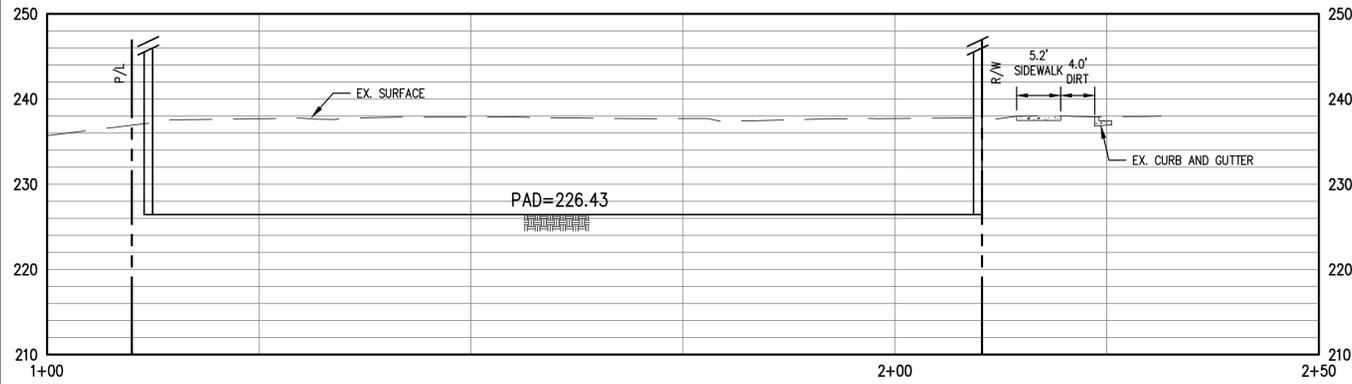
UTILITY NOTES:

- CONSTRUCT 8" FIRE SERVICE CONNECTION PER CURRENT CITY STANDARDS SDW-118
- CONSTRUCT 8" BACKFLOW PREVENTER. APOLLO RPDALF 4A
- CONSTRUCT 1" WATER SERVICE PER CURRENT CITY STANDARDS SDW-150
- CONSTRUCT 1" METER BOX PER CURRENT CITY STANDARDS SDW-134
- CONSTRUCT 1" BACKFLOW PREVENTER. APOLLO RPLF4A
- CONSTRUCT 6" SEWER PER CURRENT CITY STANDARDS
- CONSTRUCT SEWER CLEANOUT PER CURRENT CITY STANDARDS
- TRENCH PER CURRENT CITY STANDARDS SDW-107
- EX. WATER LATERAL TO BE ABANDONED
- EX. SEWER LATERAL TO BE ABANDONED

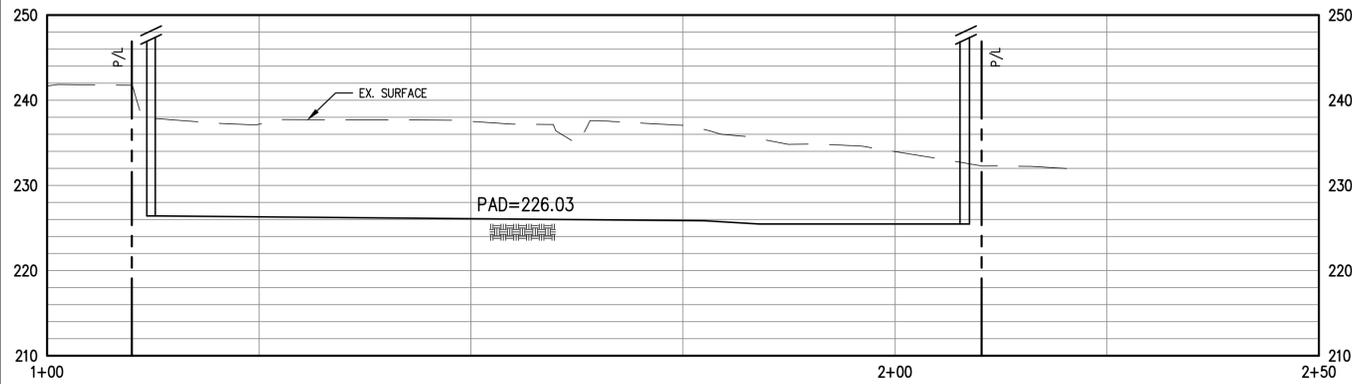
PRELIMINARY GRADING PLAN FOR 2426 FOURTH AVENUE NOT FOR CONSTRUCTION



PROFILE: Alignment - A-A
HOR.: 1"=10'
VERT: 1"=10'



PROFILE: Alignment - B-B
HOR.: 1"=10'
VERT: 1"=10'



PROFILE: Alignment - C-C
HOR.: 1"=10'
VERT: 1"=10'

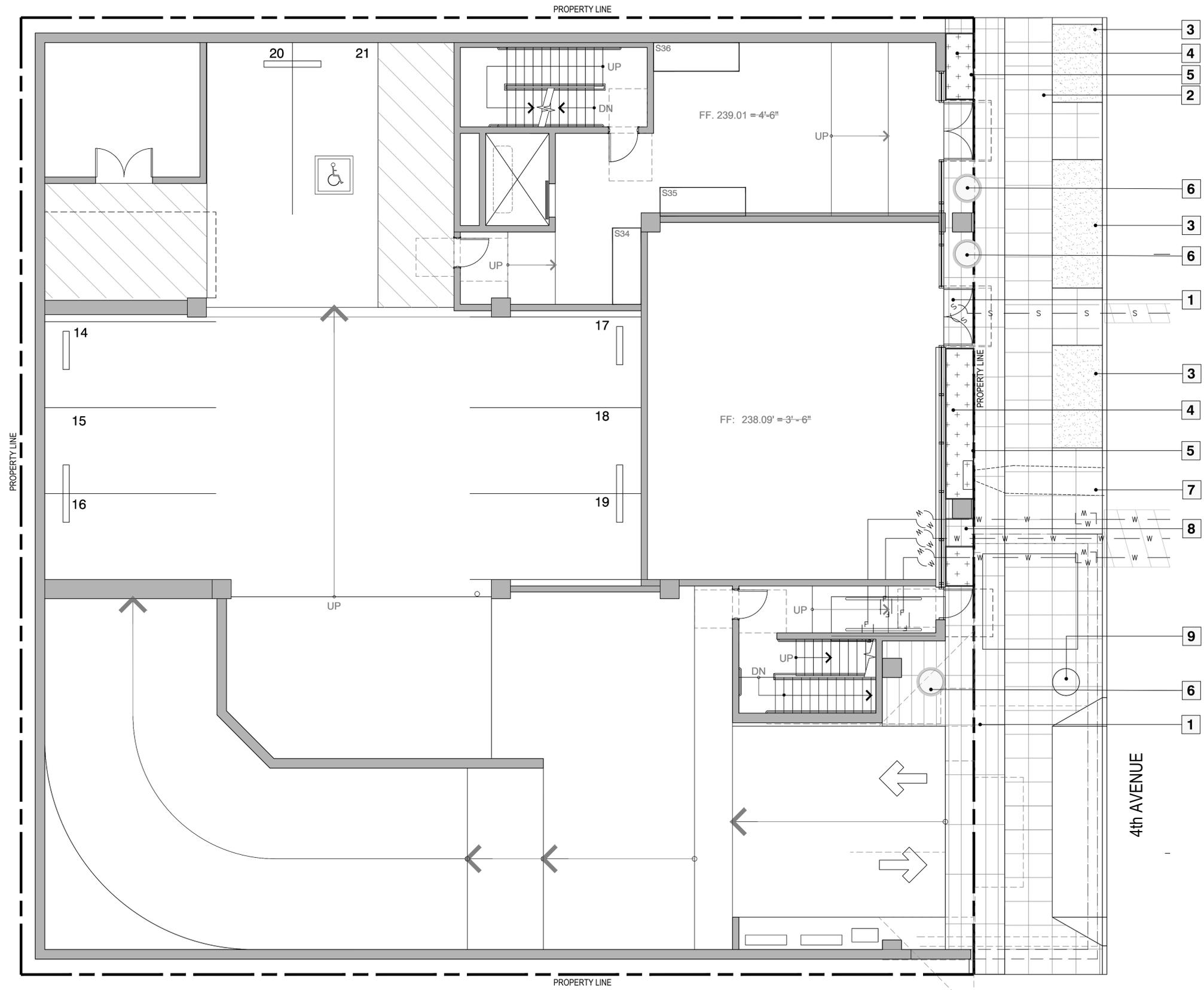
Source Control BMP Checklist for Standard Projects		Form I-4A
All development projects must implement source control BMPs. Refer to Chapter 4 and Appendix E of the BMP Design Manual for information to implement BMPs shown in this checklist. Note: All selected BMPs must be shown on the construction plans.		
Source Control Requirement	Applied ⁽¹⁾ ?	
4.2.1 Prevention of Illicit Discharges into the MS4	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No <input type="checkbox"/> N/A
4.2.2 Storm Drain Stenciling or Signage	<input type="checkbox"/> Yes	<input type="checkbox"/> No <input checked="" type="checkbox"/> N/A
4.2.3 Protect Outdoor Materials Storage Areas from Rainfall, Run-On, Runoff, and Wind Dispersal	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No <input type="checkbox"/> N/A
4.2.4 Protect Materials Stored in Outdoor Work Areas from Rainfall, Run-On, Runoff, and Wind Dispersal	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No <input type="checkbox"/> N/A
4.2.5 Protect Trash Storage Areas from Rainfall, Run-On, Runoff, and Wind Dispersal	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No <input type="checkbox"/> N/A
4.2.6 BMPs based on Potential Sources of Runoff Pollutants		
On-site storm drain inlets	<input type="checkbox"/> Yes	<input type="checkbox"/> No <input checked="" type="checkbox"/> N/A
Interior floor drains and elevator shaft sump pumps	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No <input type="checkbox"/> N/A
Interior parking garages	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No <input type="checkbox"/> N/A
Need for future indoor & structural pest control	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No <input type="checkbox"/> N/A
Landscape/Outdoor Pesticide Use	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No <input type="checkbox"/> N/A
Pools, spas, ponds, decorative fountains, and other water features	<input type="checkbox"/> Yes	<input type="checkbox"/> No <input checked="" type="checkbox"/> N/A
Food service	<input type="checkbox"/> Yes	<input type="checkbox"/> No <input checked="" type="checkbox"/> N/A
Refuse areas	<input type="checkbox"/> Yes	<input type="checkbox"/> No <input checked="" type="checkbox"/> N/A
Industrial processes	<input type="checkbox"/> Yes	<input type="checkbox"/> No <input checked="" type="checkbox"/> N/A
Outdoor storage of equipment or materials	<input type="checkbox"/> Yes	<input type="checkbox"/> No <input checked="" type="checkbox"/> N/A
Vehicle/Equipment Repair and Maintenance	<input type="checkbox"/> Yes	<input type="checkbox"/> No <input checked="" type="checkbox"/> N/A
Fuel Dispensing Areas	<input type="checkbox"/> Yes	<input type="checkbox"/> No <input checked="" type="checkbox"/> N/A
Loading Docks	<input type="checkbox"/> Yes	<input type="checkbox"/> No <input checked="" type="checkbox"/> N/A
Fire Sprinkler Test Water	<input type="checkbox"/> Yes	<input type="checkbox"/> No <input checked="" type="checkbox"/> N/A
Miscellaneous Drain or Wash Water	<input type="checkbox"/> Yes	<input type="checkbox"/> No <input checked="" type="checkbox"/> N/A
Plazas, sidewalks, and parking lots	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No <input type="checkbox"/> N/A
SC-6A: Large Trash Generating Facilities	<input type="checkbox"/> Yes	<input type="checkbox"/> No <input checked="" type="checkbox"/> N/A
SC-6B: Animal Facilities	<input type="checkbox"/> Yes	<input type="checkbox"/> No <input checked="" type="checkbox"/> N/A
SC-6C: Plant Nurseries and Garden Centers	<input type="checkbox"/> Yes	<input type="checkbox"/> No <input checked="" type="checkbox"/> N/A
SC-6D: Automotive Facilities	<input type="checkbox"/> Yes	<input type="checkbox"/> No <input checked="" type="checkbox"/> N/A
Discussion / justification for all "No" answers shown above:		

Site Design BMP Checklist for Standard Projects		Form I-5A
All development projects must implement site design BMPs. Refer to Chapter 4 and Appendix E of the BMP Design Manual for information to implement BMPs shown in this checklist. Note: All selected BMPs must be shown on the construction plans.		
Site Design Requirement	Applied ⁽¹⁾ ?	
4.3.1 Maintain Natural Drainage Pathways and Hydrologic Features	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No <input type="checkbox"/> N/A
4.3.2 Conserve Natural Areas, Soils, and Vegetation	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No <input type="checkbox"/> N/A
4.3.3 Minimize Impervious Area	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No <input type="checkbox"/> N/A
4.3.4 Minimize Soil Compaction	<input type="checkbox"/> Yes	<input type="checkbox"/> No <input checked="" type="checkbox"/> N/A
4.3.5 Impervious Area Dispersion	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No <input type="checkbox"/> N/A
4.3.6 Runoff Collection	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No <input type="checkbox"/> N/A
4.3.7 Landscaping with Native or Drought Tolerant Species	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No <input type="checkbox"/> N/A
4.3.8 Harvest and Use Precipitation	<input type="checkbox"/> Yes	<input type="checkbox"/> No <input checked="" type="checkbox"/> N/A
Discussion / justification for all "No" answers shown above:		
The existing drainage flows southerly and the proposed building will discharge roof drains into a planter and be discharged into a curb outlet along 4th Avenue.		
Proposed building is the entire property limits, Minimize impervious area is not possible.		

⁽¹⁾ Answer for each source control and site design category shall be pursuant to the following:

- "Yes" means the project will implement the BMP as described in Chapter 4 and/or Appendix E of the BMP Design Manual. Discussion / justification is not required.
- "No" means the BMP is applicable to the project but it is not feasible to implement. Discussion / justification must be provided.
- "N/A" means the BMP is not applicable at the project site because the project does not include the feature that is addressed by the BMP (e.g., the project has no outdoor materials storage areas). Discussion / justification may be provided.





LANDSCAPE CALCULATION

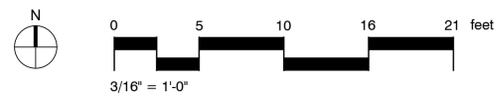
STREET TREES IN RIGHT OF WAY	QUANTITY
LENGTH OF 4th AVE. STREET FRONTAGE	100 L.F.
-STREET TREES REQUIRED	2
-STREET TREES PROVIDED	2

NOTES:
 THE BUILDING DOES NOT HAVE ANY SET BACK REQUIREMENTS, HENCE THERE IS NO STREET YARD OR REMAINING YARD.
 SEE PLANTING PLAN FOR TREE TYPES.
 SEE SHEET L02.00 FOR STREET TREES

OVERALL SITE PLAN LEGEND

TAG	DESCRIPTION
1	PAVING TYPE 1 - TO BE DETERMINED
2	30"X 30" SAN DIEGO STANDARD CONCRETE PAVING, NATURAL GREY
3	WOOD MULCH, MINIMUM DEPTH 3 INCH
4	LOW HEIGHT SHRUBS WITH CARBON SLATE GRAVEL AS DECORATIVE MULCH (3")
5	3' HIGH CORTEN PLANTER
6	PLANTS IN URNS
7	CURB DRAINAGE OUTLET
8	BACKFLOW PREVENTER LOCATION
9	MANHOLE COVER

PLAN - LEVEL 1



4TH AVENUE APARTMENTS
 2426 4th AVENUE
 SAN DIEGO, CALIFORNIA 92101

REVISIONS		
NO	DESCRIPTION	DATE
	COMPLETENESS REVIEW	12.01.2017
	PLANNING SUBMITTAL	01.11.2018
	PLANNING RESUBMITTAL	05.09.2018

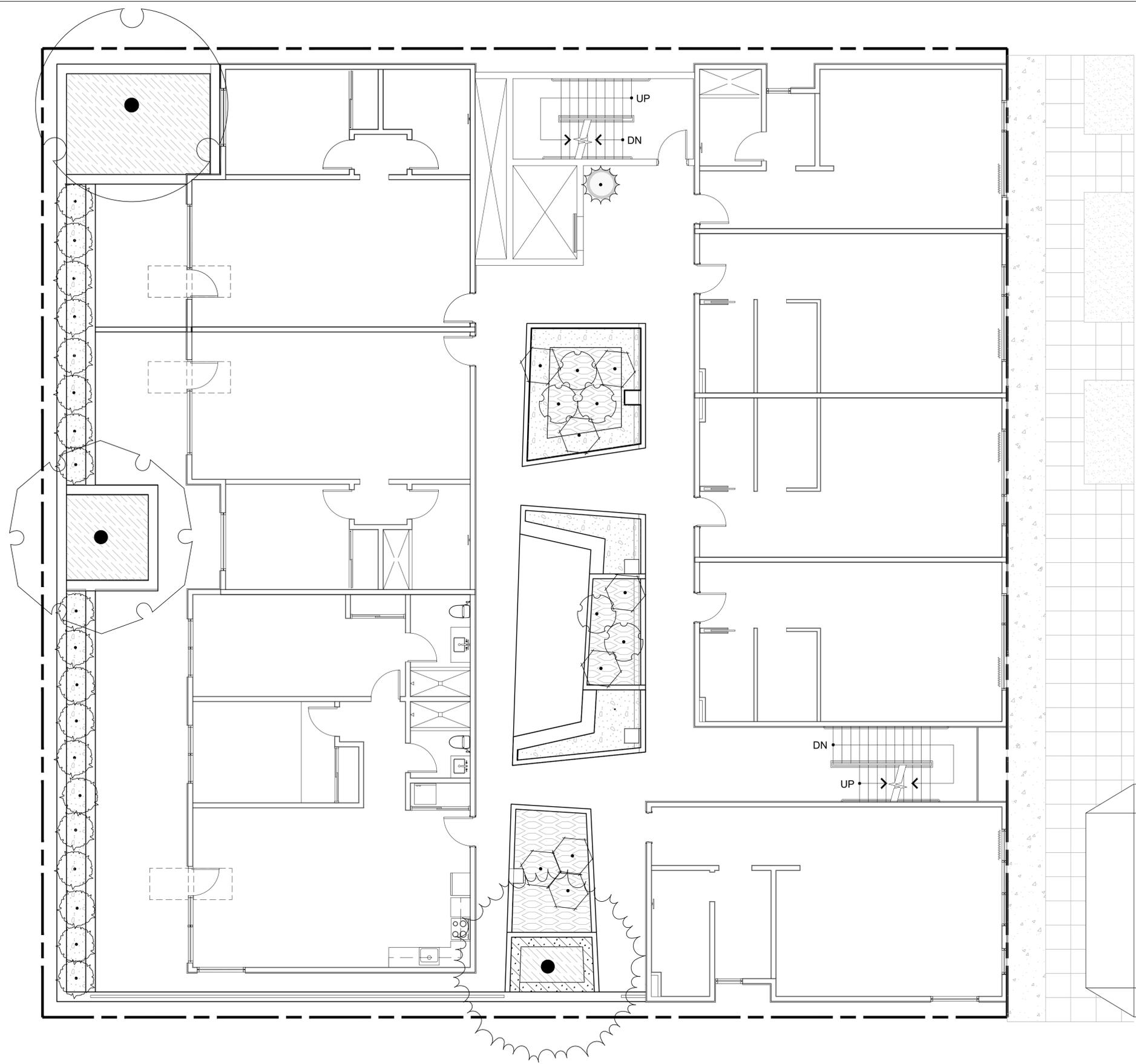
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LANDSCAPE
 PLAN- LEVEL 1

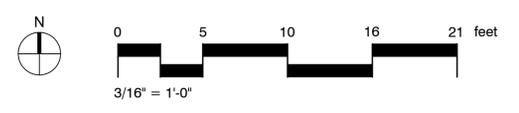
ACRM
 AWBREY COOK ROGERS MCGILL
 ARCHITECTS + INTERIORS

mLaSD
 McCallough Landscape Architecture, Inc.
 703 16th Street, Suite 100
 San Diego, California 92101
 Tel: (619) 236-3100 Fax: (619) 501-7725
 www.mlasd.com

SHEET
L01.00

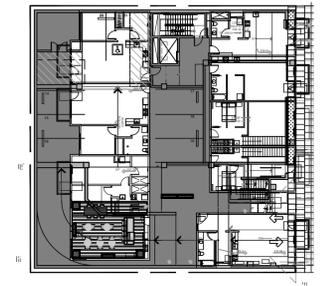


PLAN - LEVEL 2



PLANT SCHEDULE

TREES	BOTANICAL NAME / COMMON NAME	CONT	MATURE H X S	FUNCTION	QTY	
	ARBUS UNEDO / STRAWBERRY TREE	24"BOX	15'-25' X 15'-25'	ACCENT	1	
	ARBUS X 'MARINA' / ARBUS MULTI-TRUNK	24"BOX	15'-25' X 15'-25'	ACCENT	1	
	CERCIDIUM X 'DESERT MUSEUM' / THORNLESS PALO VERDE	24"BOX	25' X 25'	SPECIMEN	1	
SHRUBS	BOTANICAL NAME / COMMON NAME	CONT	HXS	FUNCTION	QTY	
	AGAVE ATTENUATA 'ARBOLEDA BLUE' / BLUE FOXTAIL AGAVE	5 GAL	4' X 5'		8	
	FICUS LYRATA / FIDDLE LEAF FIG	15 GAL			1	
	PODOCARPUS GRACILIOR / FERN PINE	15 GAL	MAINTAIN HEIGHT AND WIDTH - 25' X 10'		19	
	RUSSELLIA EQUISETIFORMIS / FIRECRACKER PLANT	5 GAL	3'X5'		5	
SHRUB AREAS	BOTANICAL NAME / COMMON NAME	CONT	HXS	FUNCTION	SPACING	QTY
	ASPARAGUS MEYERI / FOXTAIL FERN	5 GAL	2'X3'		24" o.c.	80
	HEUCHERA SANGUINEA / CORAL BELLS	5 GAL	1'-2' X 0.75' X 1'		12" o.c.	266
	ROSMARINUS OFFICINALIS / ROSEMARY	5 GAL	3'-6' X 3' X 4'		24" o.c.	59
GROUND COVERS	BOTANICAL NAME / COMMON NAME	CONT	HXS	FUNCTION	SPACING	QTY
	SENECIO RADICANS / STRING OF BANANAS	5 GAL	3'-5' X 12"		24" o.c.	7



LANDSCAPE CALCULATION

2ND FLOOR COMMON AREA	
- COMMON AREA OVERALL	3153 SFT (SEE HATCH AREA IN KEY PLAN)
- PLANTING AREA	798 SFT
- PERCENTAGE OF PLANTING AREA	25 %
- PLANT POINTS	1096 (60 POINTS ACHIEVED WITH TREES)

KEY PLAN

4TH AVENUE APARTMENTS
 2426 4th AVENUE
 SAN DIEGO, CALIFORNIA 92101

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	PLANNING RESUBMITTAL	05.09.2018

SHEET TITLE:

PLANTING
 PLAN-LEVEL 2

SHEET
L02.01

LEGEND - FIRE ACCESS PLAN

PATTERN / SYMBOL	DESCRIPTION
	CONCRETE PAVING (SEE ALSO CIVIL DRAWINGS)
	LANDSCAPE (SEE LANDSCAPE DRAWINGS)
	EXISTING BUILDING
	EXISTING RED PAINTED CURB
	FIRE HYDRANT; "E" = EXISTING
	STREET POLE W/ OVERHEAD LIGHT AND/OR TRAFFIC SIGNAL
	FIRE HOSE (MAX. 150' TOTAL LENGTH)

GENERAL NOTES - FIRE ACCESS PLAN

- FIRE APPARATUS ACCESS ROADS & WATER SUPPLIES FOR FIRE PROTECTION SHALL BE INSTALLED & MADE SERVICEABLE PRIOR TO & DURING CONSTRUCTION.
- RADIUS FOR FIRE HYDRANTS TO ALL PORTIONS OF THE EXTERIOR OF THE BUILDING IS BASED ON C105.1.
- A 3' MINIMUM CLEAR SPACE SHALL BE MAINTAINED AROUND THE CIRCUMFERENCE OF THE FIRE HYDRANTS UNLESS NOTED OTHERWISE.
- EVERY BUILDING 4 STORIES OR MORE IN HEIGHT SHALL BE PROVIDED WITH NOT LESS THAN 1 STANDPIPE FOR USE DURING CONSTRUCTION INSTALLED IN ACCORDANCE WITH CFC 3313.1. STANDPIPE SHALL BE INSTALLED WHEN THE PROGRESS OF CONSTRUCTION IS NOT MORE THAN 40' IN HEIGHT ABOVE THE LOWEST LEVEL OF FIRE DEPARTMENT ACCESS. CFC 3313.1 DECORATIVE MATERIALS SHALL BE MAINTAINED IN FLAME RETARDANT CONDITION.
- ALL BUILDINGS & SITES UNDER GOING CONSTRUCTION, ALTERATION, OR DEMOLITION SHALL COMPLY WITH THE REQUIREMENTS OF CFC CHAPTER 33.
- STAIRWAYS EXITING DIRECTLY TO THE EXTERIOR OF A BUILDING FOUR OR MORE STORIES IN HEIGHT SHALL BE PROVIDED WITH A MEANS FOR EMERGENCY ENTRY FOR FIRE DEPARTMENT ACCESS. POST INDICATOR VALVES, FIRE DEPARTMENT CONNECTIONS, AND ALARM BELL ARE TO BE LOCATED ON THE ADDRESS/ACCESS SIDE OF THE STRUCTURE.
- PROVIDE BUILDING ADDRESS NUMBERS, VISIBLE AND LEGIBLE FROM THE STREET OR ROAD FRONTING THE PROPERTY.
- VEGETATION SHALL BE SELECTED AND MAINTAINED IN SUCH A MANNER AS TO ALLOW IMMEDIATE ACCESS TO ALL HYDRANTS, VALVES, FIRE DEPARTMENT CONNECTIONS, PULL STATIONS, EXTINGUISHERS, SPRINKLER RISERS, ALARM CONTROL PANELS, RESCUE WINDOWS, AND OTHER DEVICES OR AREAS USED FOR FIREFIGHTING PURPOSES. VEGETATION OR BUILDING FEATURES SHALL NOT OBSTRUCT ADDRESS NUMBERS OR INHIBIT THE FUNCTIONING OF ALARM BELLS, HORNS OR STROBES.
- DECORATIVE MATERIALS SHALL BE MAINTAINED IN A FLAME-RETARDANT CONDITION (CFC SEC. 804)
- CONSTRUCTION DOCUMENTS APPROVED BY THE FIRE CODE OFFICIAL ARE APPROVED WITH THE INTENT THAT SUCH CONSTRUCTION DOCUMENTS COMPLY IN ALL RESPECTS WITH THE CFC. REVIEW AND APPROVAL BY THE FIRE CODE OFFICIAL SHALL NOT RELIEVE THE APPLICANT OF THE RESPONSIBILITY OF COMPLIANCE WITH THIS CODE (CFC SEC 105.4.4)
- GENERAL STRETCHER REQUIREMENTS - ALL BUILDINGS AND STRUCTURES WITH ONE OR MORE PASSENGER SERVICE ELEVATORS SHALL BE PROVIDED WITH NOT LESS THAN ONE MEDICAL EMERGENCY SERVICE ELEVATOR TO ALL LANDINGS MEETING THE PROVISIONS OF CBC SEC. 3002.4a
- CLEAR SPACE AROUND HYDRANTS - A 3 FOOT CLEAR SPACE SHALL BE MAINTAINED AROUND THE CIRCUMFERENCE OF FIRE HYDRANTS, EXCEPT AS OTHERWISE REQUIRED OR APPROVED (CFC 507.5.5)
- NEW BUILDINGS FOUR OR MORE STORIES ABOVE GRADE PLANE, EXCEPT THOSE WITH A ROOF SLOPE GREATER THAN FOUR UNITS VERTICAL IN 12 UNITS HORIZONTAL SHALL BE PROVIDED WITH A STAIRWAY TO THE ROOF (CFC SEC. 504.3)

4TH AVENUE APARTMENTS

2426 4th AVENUE
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	PLANNING RESUBMITTAL	01.22.2018

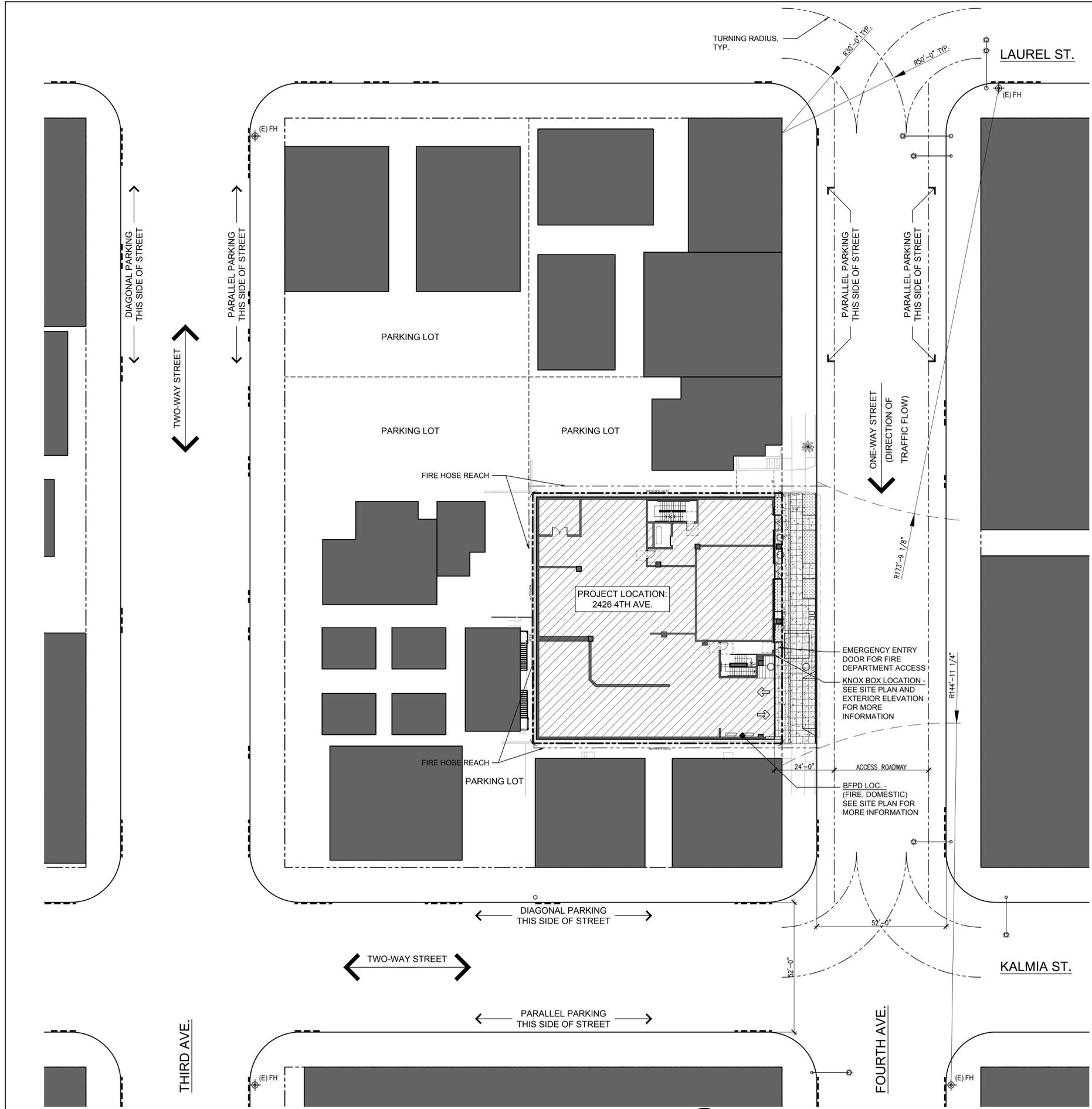
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FIRE ACCESS PLAN



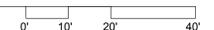
SHEET

AF0.1



1 PROPOSED FIRE ACCESS PLAN

SCALE 1" = 20'-0"



GENERAL NOTES - SITE PLAN

- POST INDICATOR VALVES, FIRE DEPARTMENT CONNECTIONS, AND ALARM BELL ARE TO BE LOCATED ON THE ADDRESS/ACCESS SIDE OF THE STRUCTURE.
- PROVIDE BUILDING ADDRESS NUMBERS, VISIBLE AND LEGIBLE FROM THE STREET OR ROAD FRONTING THE PROPERTY.

LEGEND - SITE PLAN

PATTERN	DESCRIPTION
	CONCRETE PAVING (SEE ALSO CIVIL DRAWINGS)
	LANDSCAPE (SEE LANDSCAPE DRAWINGS)
	CHAINLINK FENCE

KEYNOTES - SITE PLAN

101 NEW SIDEWALK - PER CIVIL DWGS	113 SETBACK; INCENTIVE USED TO ADDRESS SDMC 131.0543(b)
102 NEW CURB CUT @ GARAGE ENTRY	114 (E) CABLE UTILITY BOX TO BE RELOCATED - PER CIVIL DWGS.
103 NEW FIRE SERVICE LINE - PER CIVIL DWGS.	115 (E) WATER METER BOX TO BE RELOCATED - PER CIVIL DWGS.
104 NEW DOMESTIC WATER LINE - PER CIVIL DWGS.	116 (E) SEWER LATERALS TO BE REMOVED - PER CIVIL DWGS.
105 LINE OF NEW ELECTRIC VAULT BELOW. SEE SHEET A0.2 FOR MORE INFORMATION.	117 (E) CHAINLINK FENCE - TO BE RELOCATED TO ACCOMMODATE NEW WORK
106 EXIST TRANSFORMERS TO BE RELOCATED TO VAULT BELOW	118 CONCRETE VAULT MANHOLE
107 PROPERTY LINE	119 LANDSCAPE - PER LANDSCAPE DWGS.
108 BACKFLOW PREVENTERS - PER CIVIL DWGS.	120 KNOX BOX @ 5'-0" MIN. TO 7'-0" MAX. AFF
109 ADA COMPLIANT LANDING @ ENTRY DOORS	121 IRRIGATION BACKFLOW PREVENTER LOCATION
110 CORTEN PLANTER	122 WATER METERS - PER CIVIL DWGS.
111 3 HR RATED CONCRETE VAULT COVER	
112 HATCHED AREA INDICATES VISIBILITY AREA: NO FENCE OR VEGETATION EXCEEDING 2' IN HEIGHT	

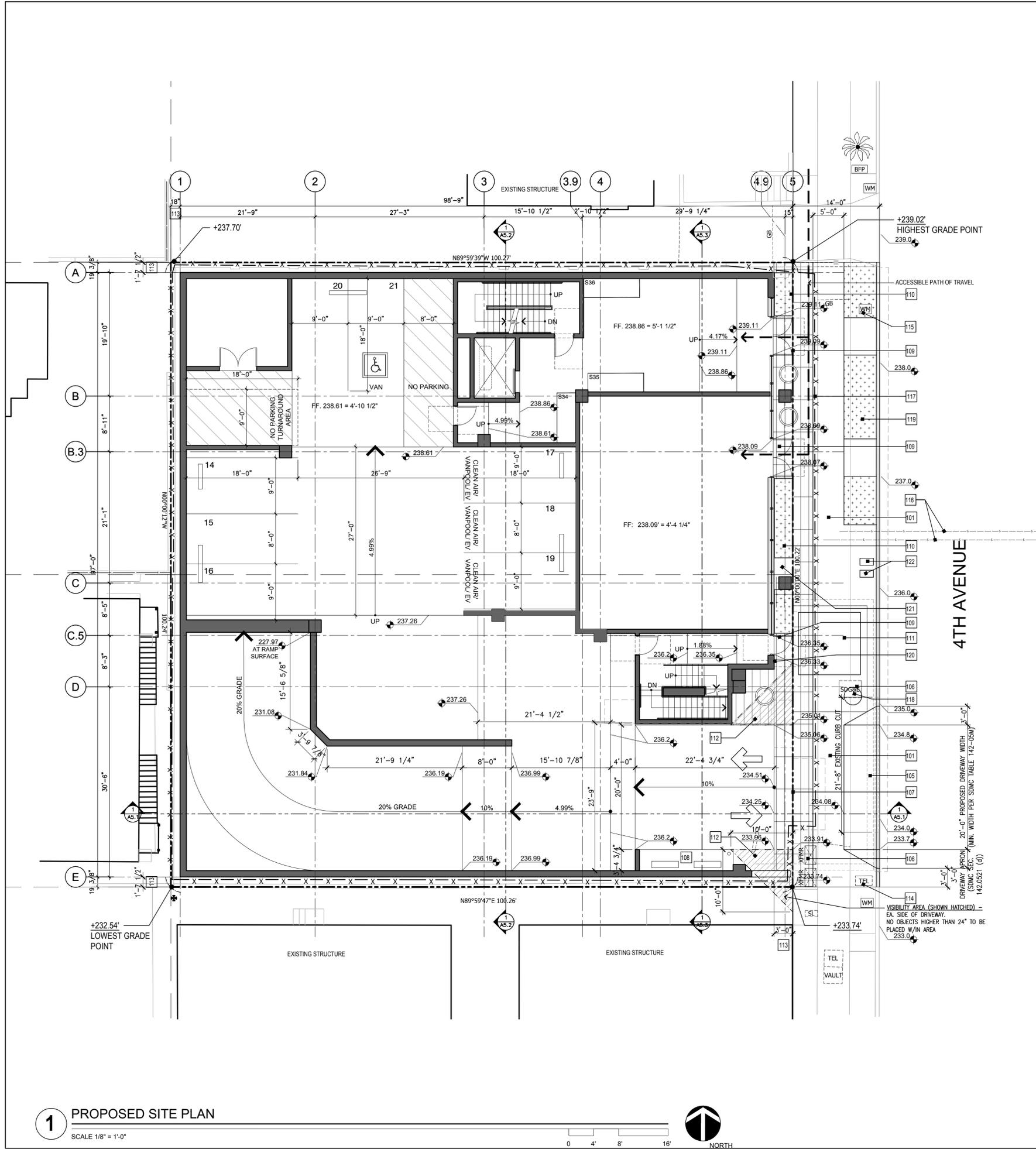
4TH AVENUE APARTMENTS
2426 4th AVENUE
SAN DIEGO, CALIFORNIA
92101

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	PLANNING RESUBMITTAL	12.19.2018
	PLANNING RESUBMITTAL	01.22.2018

SHEET TITLE:
PROPOSED SITE PLAN



SHEET
A0.1



1 PROPOSED SITE PLAN
SCALE 1/8" = 1'-0"



0 4' 8' 16'

26. VAULT EQUIPMENT COVER and PERSONNEL ACCESS OPENING (Below Grade)

- 26.1. Inside dimension of equipment opening shall measure 10ft x 10ft. Customer shall provide removable three-hour fire rated three-part concrete equipment opening covers. No coverings or facades are permitted over these covers unless an integral part of the covers themselves. Maximum weight of each section is not to exceed 4,800 lbs.
- 26.2. Customer to provide 4 lifting inserts with removable plugs (to keep out debris) near each corner of each equipment opening cover/section. Lifting inserts shall be 1in threaded coil inserts with 4,500 lbs safe working load tension – see section 3 & 4.
- 26.3. Personnel access opening (manhole) shall measure 30 inches in diameter with 34 inch diameter cast iron cover and recessed stainless steel penta-head bolts (4 total). See SDG&E Underground Standards page 3332. Personnel access opening to be at final grade. No coverings are permitted over this opening.
- 26.4. Customer to submit plans approved by a civil engineer registered in the State of California verifying the structural integrity of equipment opening covers and personnel access opening cover to withstand (AASHTO) HS-20 loading. Equipment opening cover shall be designed in accordance with SDG&E Underground Construction Standards. Plans to be submitted to SDG&E prior to construction/manufacture of vault roof and sectioned equipment opening.
- 26.5. Café fences, railings, partitions, enclosures, screens, barriers, walls, tables, chairs, umbrellas, podiums, or any other miscellaneous structures or furniture are not permitted above equipment opening cover or personnel access opening cover, temporarily or otherwise. Twenty four hour access for emergency and/or routine removal of covers is to be maintained at all times.
- 26.6. Equipment opening covers and personnel access open cover to sit flush with and at same grade as surrounding sidewalk.
- 26.7. Customer shall install felt insulation between concrete equipment opening covers and metal side rails/struts - to prevent surface vacuum buildup between covers and side rails. Install felt insulation such that top horizontal surface of insulation is 1/2 inch – 1inch below top surface of covers. Customer to seal perimeter of equipment opening covers with removable ultraviolet/water/oil resistant caulking.

24. VAULT INTERNAL DIMENSIONS: BELOW GRADE & STREET LEVEL VAULTS

Customer Type	Number of Transformers (26.1)		Clear and unobstructed space (26.1, 2.7.3.1.5)	Equipment opening & personnel access opening
	10	30		
Residential Only	1	1	18' X 16' (at grade)	(not required at grade)
	1	1	18' X 16' (below grade)	17' X 12' (min)
Commercial or Combination w/ Residential		1	20' X 44' (26.4)	17' X 12' (min) (not required at grade)
		2	36 1/2' X 29' or 20' x 57'	17' X 12' (min) (not required at grade)
		3	20' x 70'	17' X 12' (min) (not required at grade)

Table 4 – Vault dimensions based on type of service and equipment needs.

- 24.1. Customer must contact SDG&E Project Management department to determine dimensions for transformer combinations not listed. Contact SDG&E prior to submitting architectural building plans for Municipal approval.
- 24.2. Customer desiring deviation from standard vault dimensions must make formal written request to SDG&E Project Management department for consideration. Requests must be submitted to SDG&E prior to submitting architectural building plans for Municipal approval.
- 24.3. If customer is not able to provide clear & unobstructed space (no columns) in the dimensions specified above, please contact SDG&E Project Management department to determine acceptable alternative to prescribed dimensions above. Customer must coordinate with SDG&E prior to submitting architectural building plans for Municipal approval.
- 24.4. Customer may use 36 1/2' x 29ft dimensions if convenient.
- 24.5. Customer may provide larger vault than minimum dimensions listed above.

PROJECT MUNICIPAL CONDITIONS

LANDSCAPING:
PRIOR TO ISSUANCE OF ANY GRADING PERMIT, THE OWNER/PERMITEE SHALL SUBMIT COMPLETE CONSTRUCTION DOCUMENTS FOR THE REVEGETATION AND HYDRO-SEEDING OF ALL DISTURBED LAND IN ACCORDANCE WITH THE CITY OF SAN DIEGO LANDSCAPE STANDARDS, STORMWATER DESIGN MANUAL, AND TO THE SATISFACTION OF THE DEVELOPMENT SERVICES DEPARTMENT. ALL PLANS SHALL BE IN SUBSTANTIAL CONFORMANCE TO THIS PERMIT (INCLUDING ENVIRONMENTAL CONDITIONS) AND EXHIBIT 'A' ON FILE IN THE DEVELOPMENT SERVICES DEPARTMENT.

GEOLGY:
PRIOR TO THE ISSUANCE OF ANY CONSTRUCTION PERMITS (EITHER GRADING OR BUILDING PERMIT), THE OWNER/PERMITEE SHALL SUBMIT A GEOTECHNICAL INVESTIGATION REPORT PREPARED IN ACCORDANCE WITH THE CITY'S "GUIDELINES FOR GEOTECHNICAL REPORTS" THAT SPECIFICALLY ADDRESSED THE PROPOSED CONSTRUCTION PLANS. THE GEOTECHNICAL INVESTIGATION REPORT SHALL BE REVIEWED FOR ADEQUACY BY THE GEOLOGY SECTION OF DEVELOPMENT SERVICES PRIOR TO THE ISSUANCE OF ANY CONSTRUCTION PERMIT.

HOUSING COMMISSION:
PRIOR TO ISSUANCE OF ANY BUILDING PERMIT ASSOCIATED WITH THIS PROJECT THE OWNER/PERMITEE SHALL DEMONSTRATE COMPLIANCE WITH THE PROVISIONS OF THE AFFORDABLE HOUSING DENSITY BONUS REGULATIONS OF CHAPTER 14, ARTICLE 3, DIVISION 7 OF THE SAN DIEGO MUNICIPAL CODE. THE OWNER/PERMITEE SHALL ENTER INTO A WRITTEN AGREEMENT WITH THE SAN DIEGO HOUSING COMMISSION WHICH SHALL BE DRAFTED AND APPROVED BY THE SAN DIEGO HOUSING COMMISSION, EXECUTED BY THE OWNER/PERMITEE, AND SECURED BY A DEED OF TRUST WHICH INCORPORATES APPLICABLE AFFORDABILITY CONDITIONS CONSISTENT WITH THE SAN DIEGO MUNICIPAL CODE.

ENGINEERING:
PRIOR TO ISSUANCE OF ANY BUILDING PERMITS, THE OWNER/PERMITEE SHALL OBTAIN A BONDED GRADING PERMIT FOR THE GRADING PROPOSED FOR THIS PROJECT. ALL GRADING SHALL CONFORM TO REQUIREMENTS IN ACCORDANCE WITH THE CITY OF SAN DIEGO MUNICIPAL CODE IN A MANNER SATISFACTORY TO THE CITY ENGINEER.

PUD, WATER & SEWER:
PRIOR TO THE ISSUANCE OF ANY BUILDING PERMITS, THE OWNER/PERMITEE SHALL APPLY FOR A PLUMBING PERMIT FOR THE INSTALLATION OF APPROPRIATE ABOVE GROUND PRIVATE BACK FLOW PREVENTION DEVICES (ON EACH WATER SERVICE (DOMESTIC, FIRE AND IRRIGATION), IN A MANNER SATISFACTORY TO THE PUBLIC UTILITIES DIRECTOR AND THE CITY ENGINEER. BFPDS ARE TYPICALLY LOCATED ON PRIVATE PROPERTY, IN LINE WITH THE SERVICE AND IMMEDIATELY ADJACENT TO THE RIGHT-OF-WAY. THE PUBLIC UTILITIES DEPARTMENT WILL NOT PERMIT THE REQUIRED BFPDS TO BE LOCATED BELOW GRADE OR WITHIN THE STRUCTURE.

TRAFFIC:
ALL AUTOMOBILE, MOTORCYCLE AND BICYCLE PARKING SPACES MUST BE CONSTRUCTED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SDMC. ALL ON-SITE PARKING STALLS AND AISLE WIDTHS SHALL BE IN COMPLIANCE WITH REQUIREMENTS OF THE CITY'S LAND DEVELOPMENT CODE AND SHALL NOT BE CONVERTED AND/OR UTILIZED FOR ANY OTHER PURPOSE, UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE APPROPRIATE CITY DECISION MAKER IN ACCORDANCE WITH THE SDMC.

- 26.8. Equipment Opening & Personnel Access Opening shall be no more than 2 ft from edge of curb*.

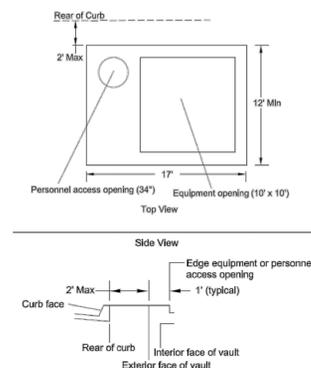


Figure 9 - Proximity of Below Grade Vault equipment and personnel access opening to street.

*Note: Customer is not permitted to install tables, chairs, partitions, posts, signs, screens, walls, fences, railings or barriers of any nature above or in front of SDG&E equipment opening or personnel access opening.

- 26.9. The following depiction shows typical arrangements of the "Clear & Unobstructed Space" (vault space for equipment) to the "Equipment and Personnel Access Opening".

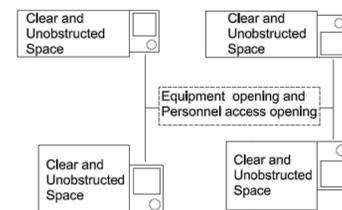


Figure 10 - Typical arrangement of vault room in relation to equipment and personnel access opening.

25. LADDER INSTALLATION FOR VAULTS GREATER THAN 12ft (Below Grade)

- 25.1. Minimum Vault Depth 12ft, Maximum Vault Depth 20ft.
- 25.2. Sump hole required, centered directly below personnel access opening
- 25.3. Installation of permanent ladder not required in personnel access opening. SDG&E will supply ladder when needed.

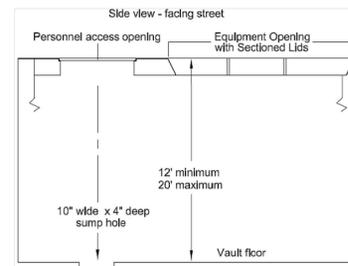


Figure 8 - Ladder installation details.

- 23.2. Below Grade Vault (Alternate):

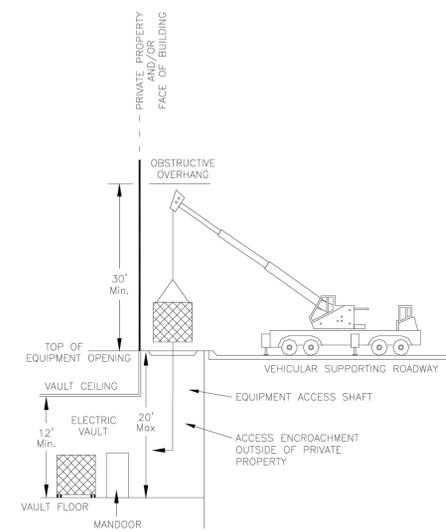


Figure 7 - Access to equipment for Below Grade vaults.

4TH AVENUE APARTMENTS
2426 4th AVENUE
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92101

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	PLANNING RESUBMITTAL	01.22.2018

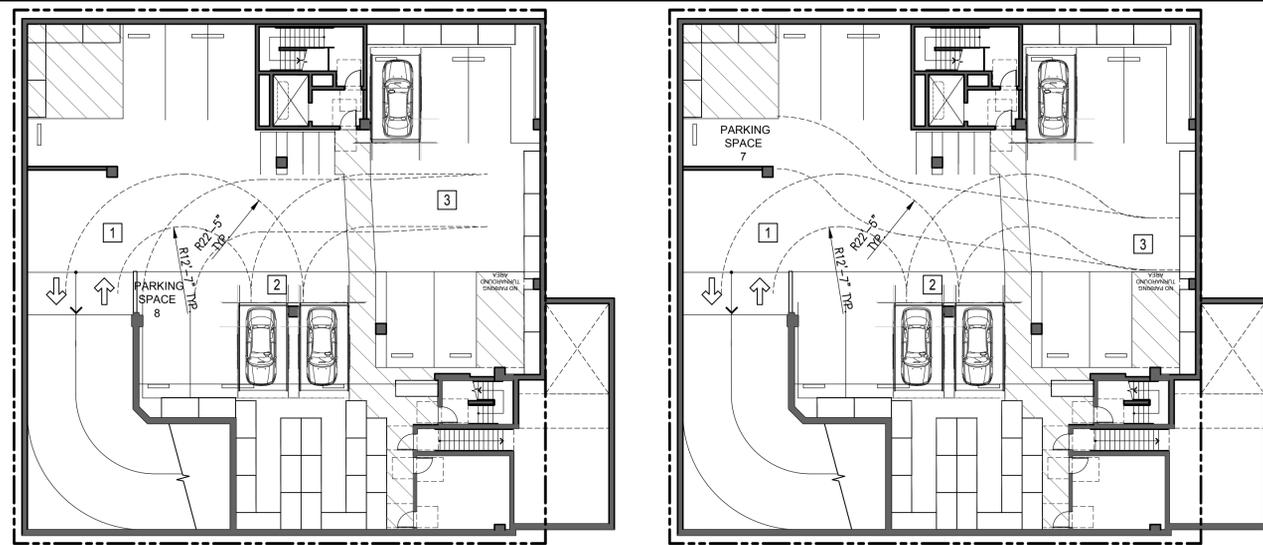
SHEET TITLE:

DRAFT CONDITIONS &
SDGE VAULT INFORMATION
(FOR REFERENCE ONLY)



SHEET

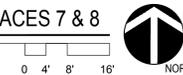
A0.2



- KEYNOTES - TURNING TEMPLATE**
1. TURN IN TO PARKING LEVEL
 2. REVERSE
 3. PULL FORWARD IN TO SPACE

P1 PARKING FLOOR PLAN - TURNING TEMPLATES, SPACES 7 & 8

SCALE: 1/16" = 1'-0"



FLOOR AREA

BASEMENT PARKING*	10,131 SF
LEVEL 1 (PARKING*)	6,744 SF (9,296 SF-2,552 SF) (LEVEL 1 FOOTPRINT-NON PARKING AREA)
SUB-TOTAL (PODIUM)	16,875 SF
LEVEL 1 (LOBBY, RETAIL, STAIRWELLS, TRASH AREA)	2,552 SF
LEVEL 2	9,208 SF
LEVEL 3	6,932 SF
LEVEL 4	6,904 SF
LEVEL 5	6,933 SF
LEVEL 6	6,939 SF
SUB-TOTAL (RESIDENTIAL, COMMERCIAL)	39,468 SF

* PARKING LEVEL EXCLUDED FROM F.A.R. CALCULATION - SUBTERRANEAN FLOOR, WRAPPED DESIGN, SCREENED FROM R.O.W. (113.0234(d)(3)(B)(i-iii))

PARKING RATIOS

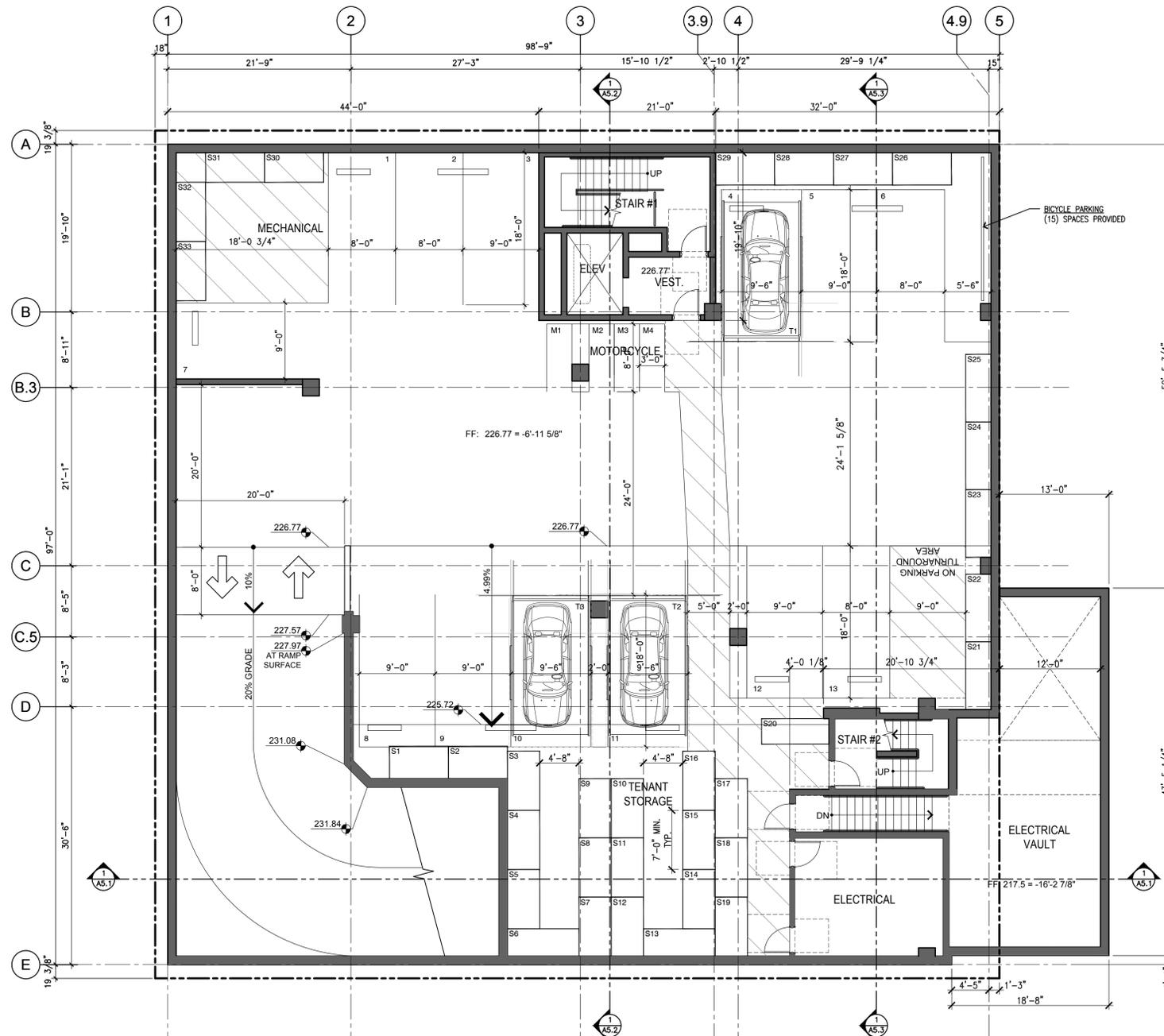
AUTOMOBILE		MOTORCYCLE	BICYCLE	EV
RESIDENTIAL (0.5 SPACES / DU OR BDRM) TABLE 143-07D	RETAIL (2.5 SPACES / 1,000SF) TABLE 142-05C	(0.1 SPACES / UNIT) TABLE 142-05C	(0.4 SPACES / UNIT @ 400SF OR 1-BDRM 0.5 SPACES / UNIT) TABLE 142-05C	(3 SPACES / 26-50 PARKING SPACES) SDMC SEC. 142.0530(d)(1)(B)(iii)
18 STUDIOS: 18X0.5 = 9	10 1-BDRM: 10X0.5 = 5	18 STUDIOS: 18X0.1 = 1.8	18 STUDIOS: 18X0.4 = 7.2	
6 2-BDRM: 12X0.5 = 6	2 LOFT: 2X0.5 = 1	10 1-BDRM: 10X0.1 = 1	10 1-BDRM: 10X0.4 = 4	
	1,174X2.5/1,000 = 2.5 = 3	6 2-BDRM: 6X0.1 = .6	6 2-BDRM: 6X0.5 = 3	
		2 LOFT: 2X0.1 = .2	2 LOFT: 2X0.4 = .8	
			3.6 = 4	
			15	3

AUTOMOBILE		MOTORCYCLE	BICYCLE	EV	
REQUIRED	TOTAL:	24	4	15	3
PROVIDED	STANDARD: TANDEM* (MECHANICAL LIFTS, T# 3)	13	7		
	SDMC 142.0555(a) AND 142.0556: ACCESSIBLE**	0	1		(3 SPACES / 26-50 PARKING SPACES) 33 PARKING SPACES = 3 EV SPACES
	TOTAL:	16	8		
	TOTAL:	24	4	15	3

**ACCESSIBLE SPACES (CBC SEC. 1109A.3) 2% X 36 UNITS = .72 = 1); VAN (CBC SEC. 1109A.8.6) 1

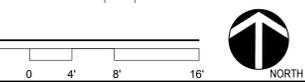
TENANT STORAGE UNITS

STORAGE UNITS (S#)	SIZE (W.X.H.X.L.)	VOLUME (CU. FT.)
S1-S5, S7-S12, S14-S19, S26-S29	7'-0"x9'-0"x3'-10"	241 CF
S6, S13	8'-6"x8'-10"x3'-3"	244 CF
S20-S25, S34	8'-0"x10'-0"x3'-0"	240 CF
S30-S33	7'-0"x10'-0"x3'-6"	245 CF
S35, S36	9'-0"x9'-0"x3'-0"	243 CF



1 P1 PARKING FLOOR PLAN

SCALE: 1/8" = 1'-0"



4TH AVENUE APARTMENTS
2426 4th AVENUE
SAN DIEGO, CALIFORNIA
92101

REVISIONS		
NO	DESCRIPTION	DATE
	COMPLETENESS REVIEW	12.01.2017
	PLANNING PRELIMINARY SUBMITTAL	01.11.2018
	PLANNING SUBMITTAL	05.09.2018
	SDGE SUBMITTAL	07.09.2018
	PLANNING RESUBMITTAL	10.31.2018
	PLANNING RESUBMITTAL	12.19.2018
	PLANNING RESUBMITTAL	01.22.2018

P1 LEVEL SUBTERRANEAN PARKING FLOOR PLAN



SHEET

A1.0

APARTMENT MATRIX

UNIT TYPE	STUDIO A	STUDIO B	STUDIO C	1 BED ROOM	2 BED ROOM	2 STORY LOFT	TOTAL
	764 SF*	552 SF*	549 SF*	776 SF*	1,178 SF*	1,124 SF	
LEVEL 2	1	3	1	2	1	0	8
LEVEL 3	1	1	1	2	2	0	7
LEVEL 4	1	1	1	2	2	0	7
LEVEL 5	1	1	1	2	1	2	8
LEVEL 6	1	2	1	2	0	-	6
TOTAL	5	8	5	10	6	2	36

* - GROSS SF

FLOOR AREA

BASEMENT PARKING*	10,131 SF
LEVEL 1 (PARKING*)	6,744 SF (9,296 SF-2,552 SF) (LEVEL 1 FOOTPRINT-NON PARKING AREA)
SUB-TOTAL (PODIUM)	16,875 SF
LEVEL 1 (LOBBY, RETAIL, STAIRWELLS, TRASH AREA)	2,552 SF
LEVEL 2	9,208 SF
LEVEL 3	6,932 SF
LEVEL 4	6,904 SF
LEVEL 5	6,933 SF
LEVEL 6	6,939 SF
SUB-TOTAL (RESIDENTIAL, COMMERCIAL)	39,468 SF

* PARKING LEVEL EXCLUDED FROM F.A.R. CALCULATION - SUBTERRANEAN FLOOR, WRAPPED DESIGN, SCREENED FROM R.O.W. (113.0234(d)(3)(B)(i-iii))

GENERAL NOTES - FLOOR PLAN

- GENERAL STRETCHER REQUIREMENTS - ALL BUILDINGS AND STRUCTURES WITH ONE OR MORE PASSENGER SERVICE ELEVATORS SHALL BE PROVIDED WITH NOT LESS THAN ONE MEDICAL EMERGENCY SERVICE ELEVATOR TO ALL LANDINGS MEETING THE PROVISIONS OF CBC SEC. 3002.4a
- EVERY BUILDING 4 STORIES OR MORE IN HEIGHT SHALL BE PROVIDED WITH NOT LESS THAN 1 STANDPIPE FOR USE DURING CONSTRUCTION INSTALLED IN ACCORDANCE WITH CFC 3313.1. STANDPIPE SHALL BE INSTALLED WHEN THE PROGRESS OF CONSTRUCTION IS NOT MORE THAN 40' IN HEIGHT ABOVE THE LOWEST LEVEL OF FIRE DEPARTMENT ACCESS. CFC 3313.1

TENANT STORAGE UNITS

STORAGE UNITS ('S#)	SIZE (W.X.H.X.L.)	VOLUME (CU. FT.)
S1-S5,S7-S12,S14-S19,S26-S29	7'-0"x9'-0"x3'-10"	241 CF
S6,S13	8'-6"x8'-10"x3'-3"	244 CF
S20-S25,S34	8'-0"x10'-0"x3'-0"	240 CF
S30-S33	7'-0"x10'-0"x3'-6"	245 CF
S35,S36	9'-0"x9'-0"x3'-0"	243 CF

UNIT FLOOR AREAS

STUDIO A	5 @ 764 SF	3,820 SF
STUDIO B	8 @ 552 SF	4,416 SF
STUDIO C	5 @ 549 SF	2,745 SF
1 BEDROOM	10 @ 776 SF	7,760 SF
2 BEDROOM	6 @ 1,178 SF	7,068 SF
LOFT	2 @ 1,124 SF	2,248 SF
TOTAL	36 UNITS	28,057 SF

KITCHEN AND BATHROOM COUNTS

	KITCHEN	BATHROOM
LEVEL 2	8	9
LEVEL 3	7	9
LEVEL 4	7	9
LEVEL 5	8	9
LEVEL 6	6	6
TOTAL	36	42

KEYNOTES - FLOOR PLAN

- 101 ELECTRICAL VAULT BELOW
- 102 CONCRETE VAULT COVER
- 103 CONCRETE VAULT MANHOLE
- 104 PAVING PER LANDSCAPE
- 105 PLANTING PER LANDSCAPE
- 106 CORTEN STEEL PLANTER
- 107 BACKFLOW PREVENTERS
- 108 KNOX BOX @ 5'-0" MIN. TO 7'-0" MAX. AFF
- 109 LIMIT LINE OF RESIDENTIAL USE 30' FROM LOT FRONT. INCENTIVE TO ALLOW RESIDENTIAL USE WITHIN FROM 30 FEET OF LOT, SDMC 131.0540(c)(1).
- 110 24"x84" GURNEY WITHIN ELEVATOR CAB
- 111 WATER METER VAULTS - SEE CIVIL DWGS.

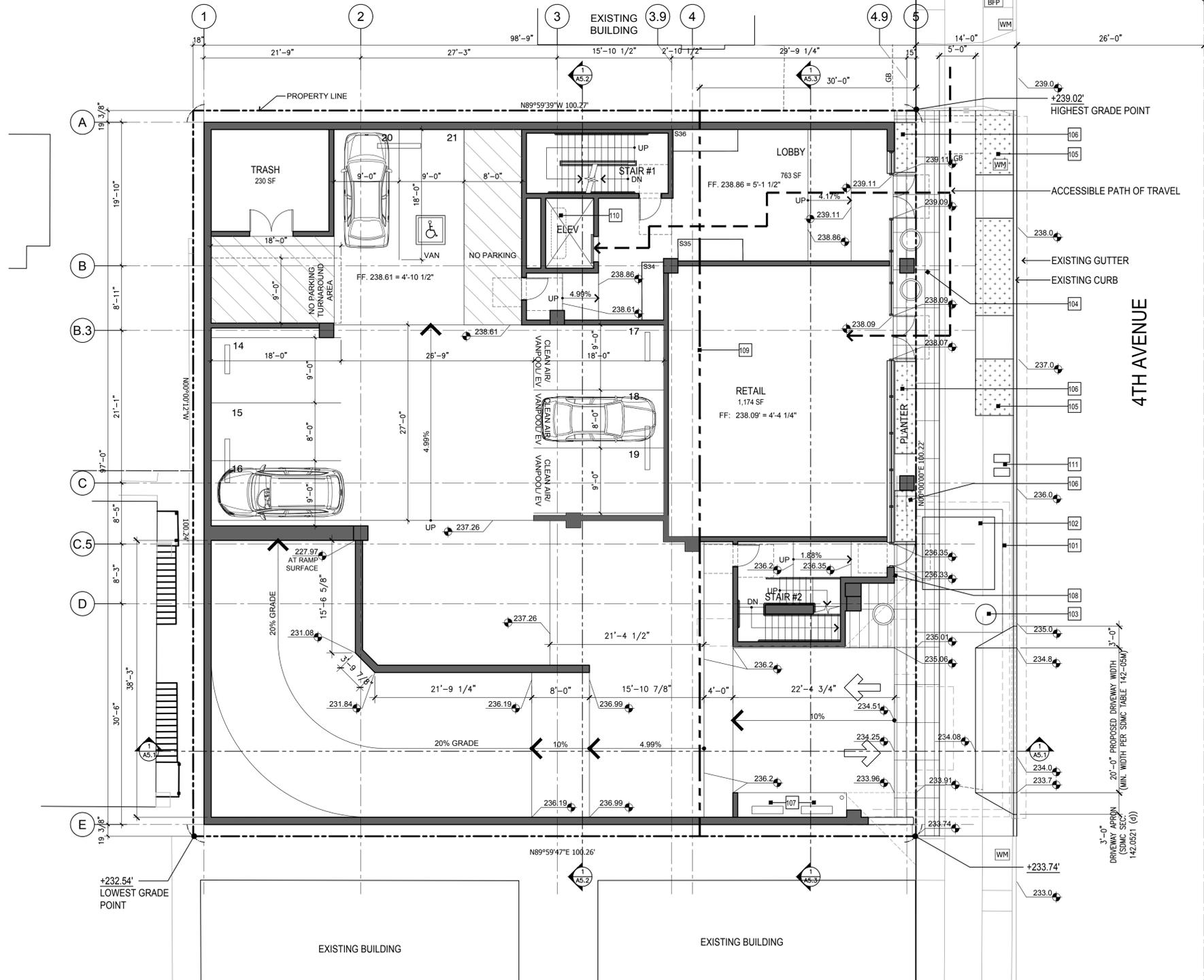
4TH AVENUE APARTMENTS
2426 4th AVENUE
SAN DIEGO, CALIFORNIA
92101

REVISIONS		
NO	DESCRIPTION	DATE
	COMPLETENESS REVIEW	12.01.2017
	PLANNING PRELIMINARY SUBMITTAL	01.11.2018
	PLANNING SUBMITTAL	05.09.2018
	SDGE SUBMITTAL	07.09.2018
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	PLANNING RESUBMITTAL	12.19.2018
	PLANNING RESUBMITTAL	01.22.2018

GROUND LEVEL FLOOR PLAN

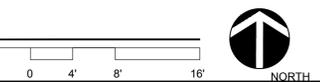


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A1.1



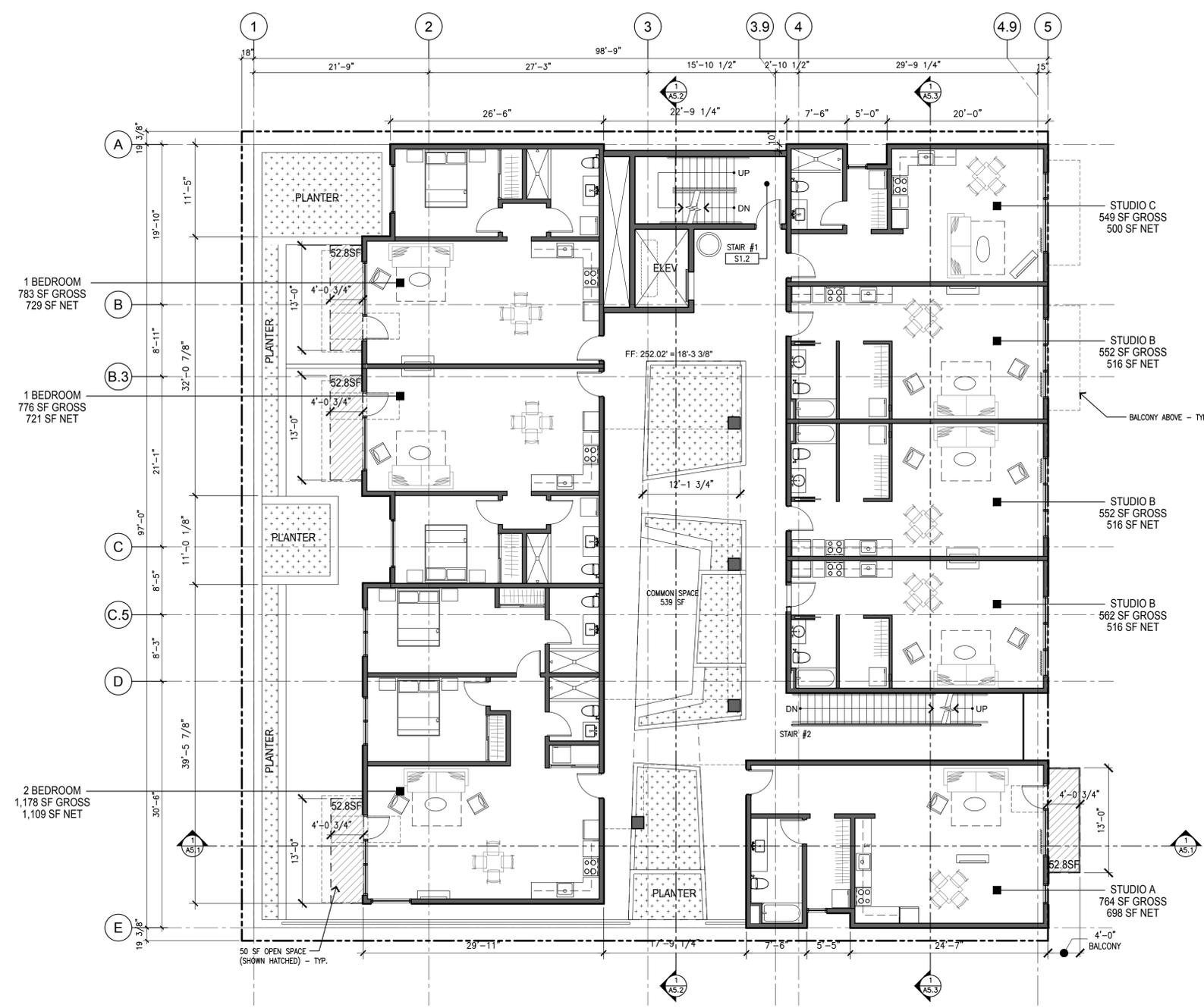
1 GROUND LEVEL FLOOR PLAN

SCALE 1/8" = 1'-0"



FLOOR AREA	
BASEMENT PARKING*	10,131 SF
LEVEL 1 (PARKING*)	6,744 SF (9,296 SF-2,552 SF) (LEVEL 1 FOOTPRINT-NON PARKING AREA)
SUB-TOTAL (PODIUM)	16,875 SF
LEVEL 1 (LOBBY, RETAIL, STAIRWELLS, TRASH AREA)	2,552 SF
LEVEL 2	9,208 SF
LEVEL 3	6,932 SF
LEVEL 4	6,904 SF
LEVEL 5	6,933 SF
LEVEL 6	6,939 SF
SUB-TOTAL (RESIDENTIAL, COMMERCIAL)	39,468 SF

* PARKING LEVEL EXCLUDED FROM F.A.R. CALCULATION - SUBTERRANEAN FLOOR, WRAPPED DESIGN, SCREENED FROM R.O.W. (113.0234(d)(3)(B)(i-iii))



4TH AVENUE APARTMENTS
2426 4th AVENUE
SAN DIEGO, CALIFORNIA
92101

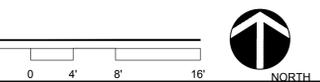
REVISIONS		
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6	PLANNING RESUBMITTAL	12.19.2018
7	PLANNING RESUBMITTAL	01.22.2018

SHEET TITLE:
2ND LEVEL FLOOR PLAN



SHEET
A1.2

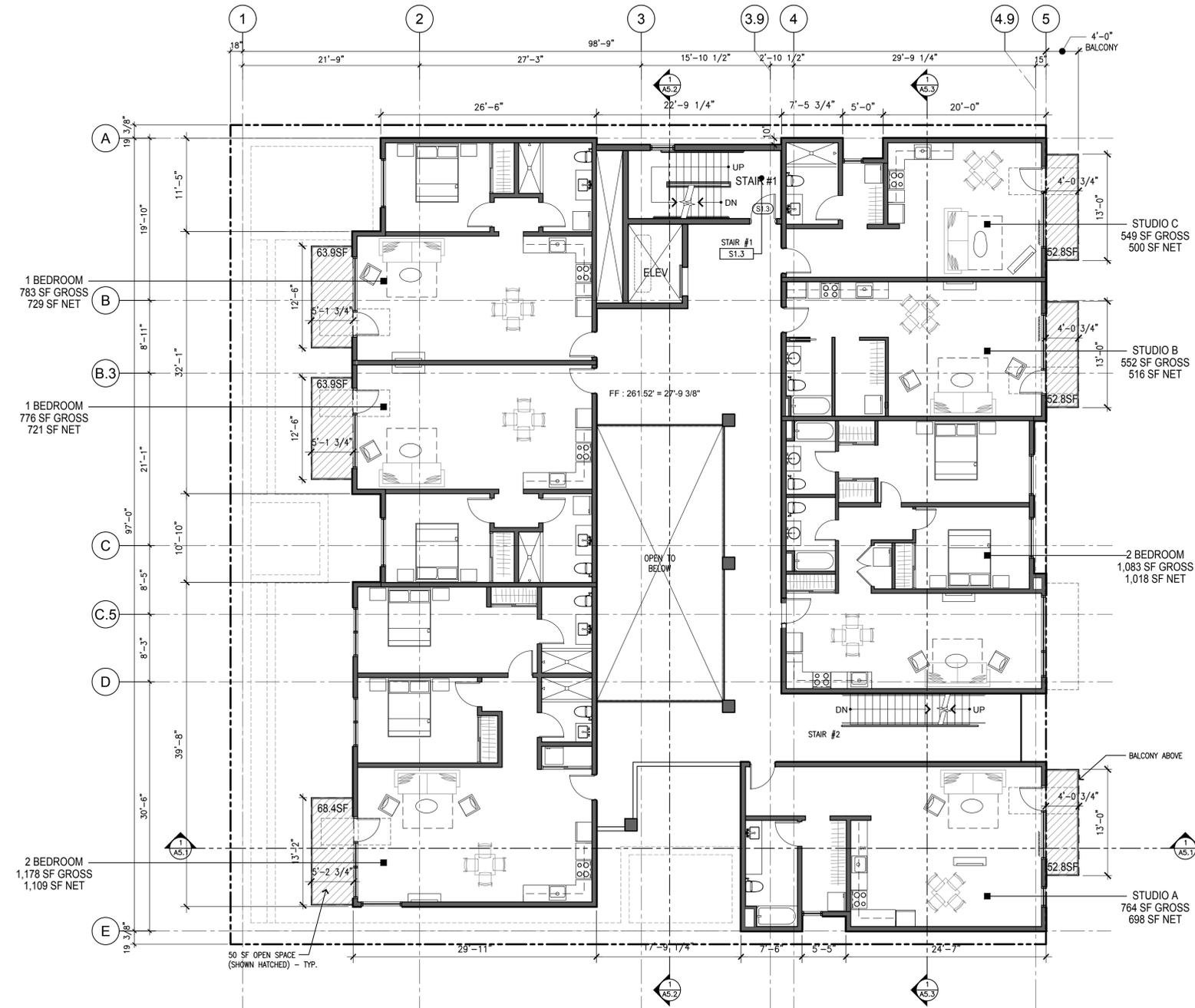
1 2ND LEVEL FLOOR PLAN
SCALE 1/8" = 1'-0"



FLOOR AREA

BASEMENT PARKING*	10,131 SF
LEVEL 1 (PARKING*)	6,744 SF (9,296 SF-2,552 SF) (LEVEL 1 FOOTPRINT-NON PARKING AREA)
SUB-TOTAL (PODIUM)	16,875 SF
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* PARKING LEVEL EXCLUDED FROM F.A.R. CALCULATION -
SUBTERRANEAN FLOOR, WRAPPED DESIGN, SCREENED
FROM R.O.W. (113.0234(d)(3)(B)(i-iii))



1 3RD LEVEL FLOOR PLAN
SCALE 1/8" = 1'-0"



4TH AVENUE APARTMENTS
2426 4th AVENUE
SAN DIEGO, CALIFORNIA
92101

REVISIONS		
NO	DESCRIPTION	DATE
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6	PLANNING RESUBMITTAL	12.19.2018
7	PLANNING RESUBMITTAL	01.22.2018

SHEET TITLE:
3RD LEVEL FLOOR PLAN

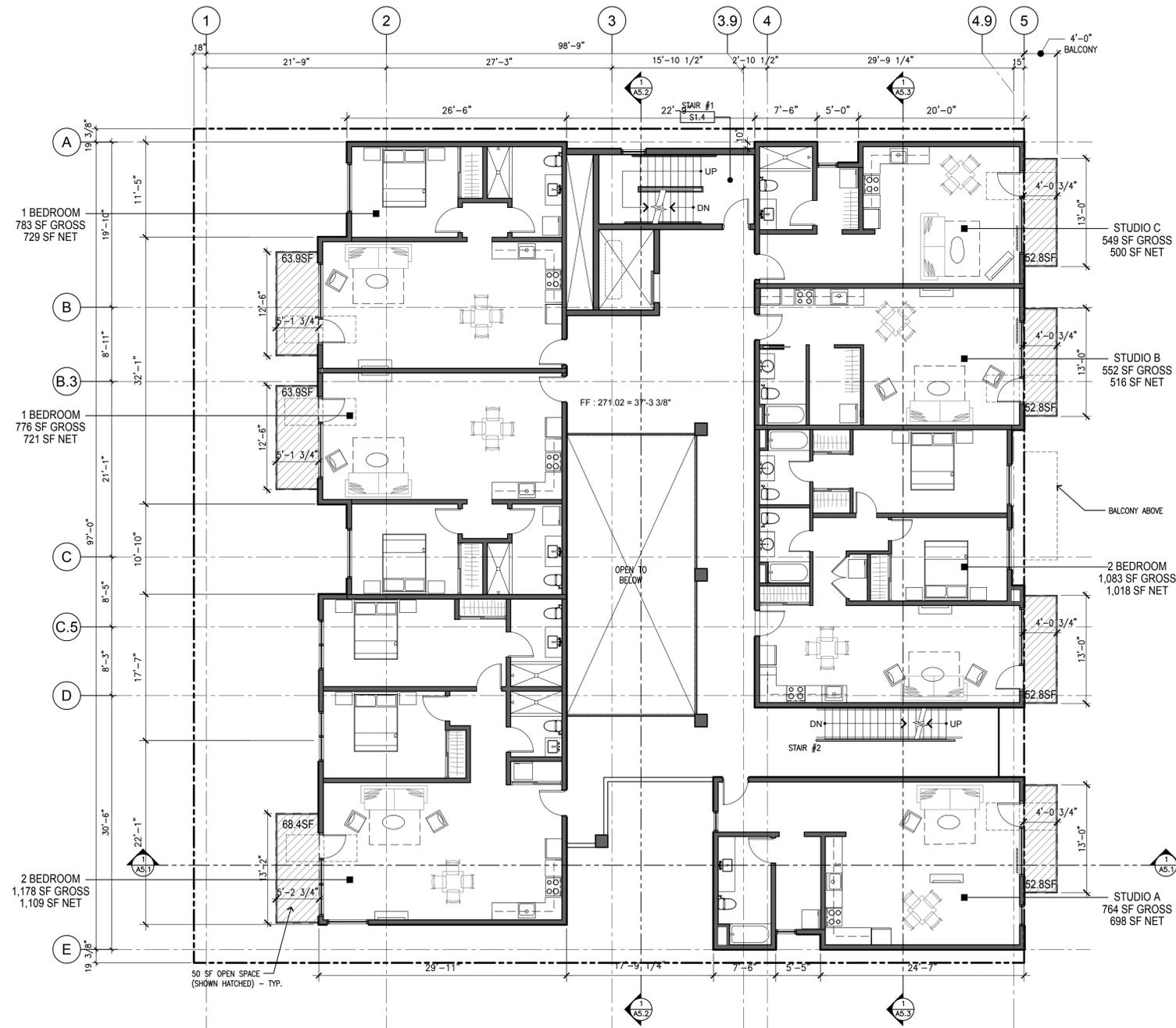


SHEET
A1.3

FLOOR AREA

BASEMENT PARKING*	10,131 SF
LEVEL 1 (PARKING*)	6,744 SF (9,296 SF-2,552 SF) (LEVEL 1 FOOTPRINT-NON PARKING AREA)
SUB-TOTAL (PODIUM)	16,875 SF
LEVEL 1 (LOBBY, RETAIL, STAIRWELLS, TRASH AREA)	2,552 SF
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1 4TH LEVEL FLOOR PLAN

SCALE 1/8" = 1'-0"



4TH AVENUE APARTMENTS
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92101

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6	PLANNING RESUBMITTAL	12.19.2018
7	PLANNING RESUBMITTAL	01.22.2018

SHEET TITLE:

4TH LEVEL FLOOR PLAN

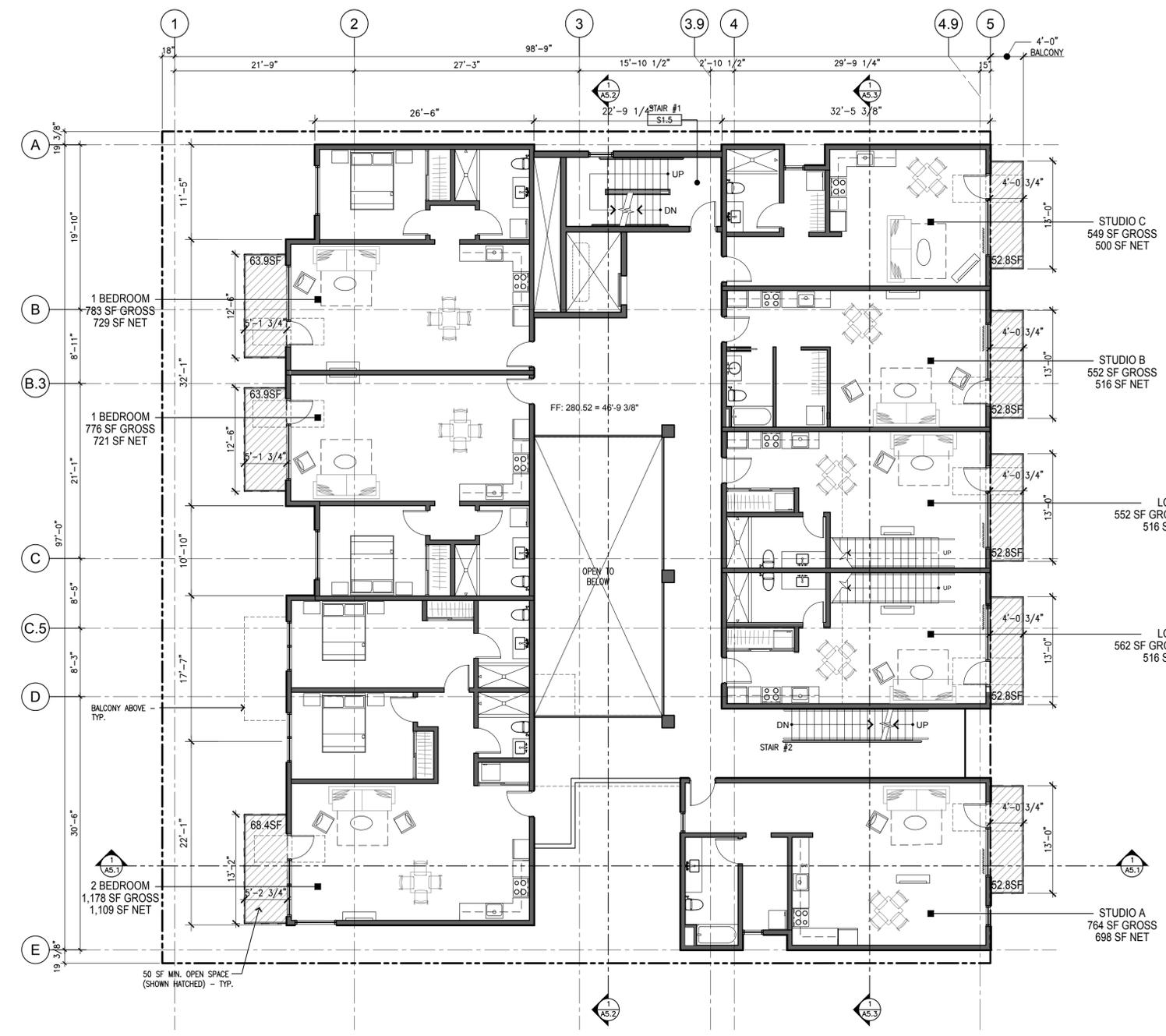


SHEET

A1.4

FLOOR AREA	
BASEMENT PARKING*	10,131 SF
LEVEL 1 (PARKING*)	6,744 SF (9,296 SF-2,552 SF) (LEVEL 1 FOOTPRINT-NON PARKING AREA)
SUB-TOTAL (PODIUM)	16,875 SF
LEVEL 1 (LOBBY, RETAIL, STAIRWELLS, TRASH AREA)	2,552 SF
LEVEL 2	9,208 SF
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LEVEL 5	6,933 SF
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4TH AVENUE APARTMENTS
2426 4th AVENUE
SAN DIEGO, CALIFORNIA
92101

REVISIONS		
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6	PLANNING RESUBMITTAL	12.19.2018
7	PLANNING RESUBMITTAL	01.22.2018

SHEET TITLE:
5TH LEVEL FLOOR PLAN



SHEET
A1.5

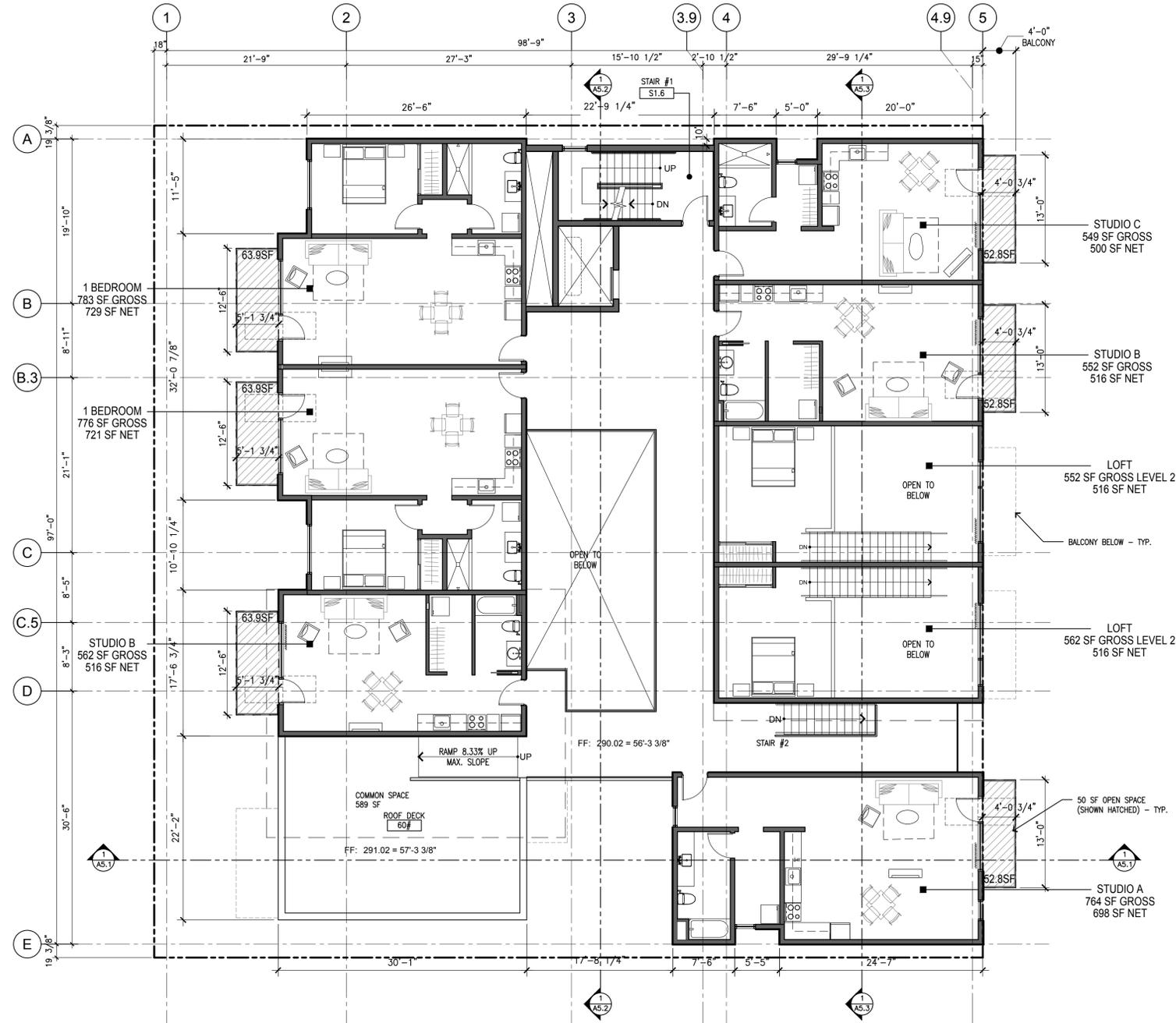
1 5TH LEVEL FLOOR PLAN
SCALE 1/8" = 1'-0"



FLOOR AREA

BASEMENT PARKING*	10,131 SF
LEVEL 1 (PARKING*)	6,744 SF (9,296 SF-2,552 SF) (LEVEL 1 FOOTPRINT-NON PARKING AREA)
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LEVEL 1 (LOBBY, RETAIL, STAIRWELLS, TRASH AREA)	2,552 SF
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SUBTERRANEAN FLOOR, WRAPPED DESIGN, SCREENED
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4TH AVENUE APARTMENTS
2426 4th AVENUE
SAN DIEGO, CALIFORNIA
92101

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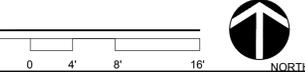
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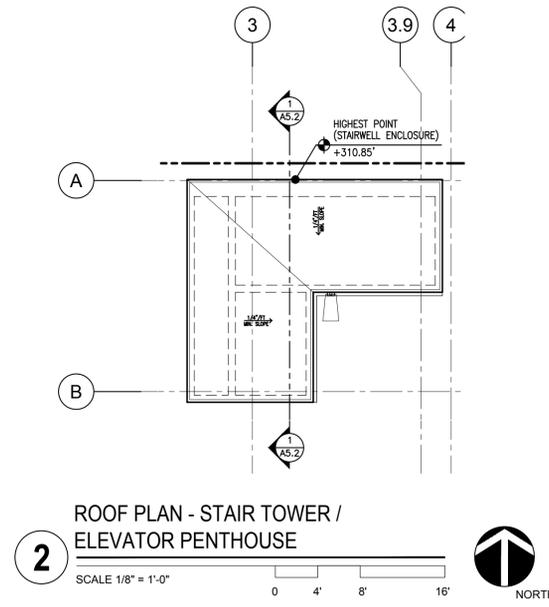
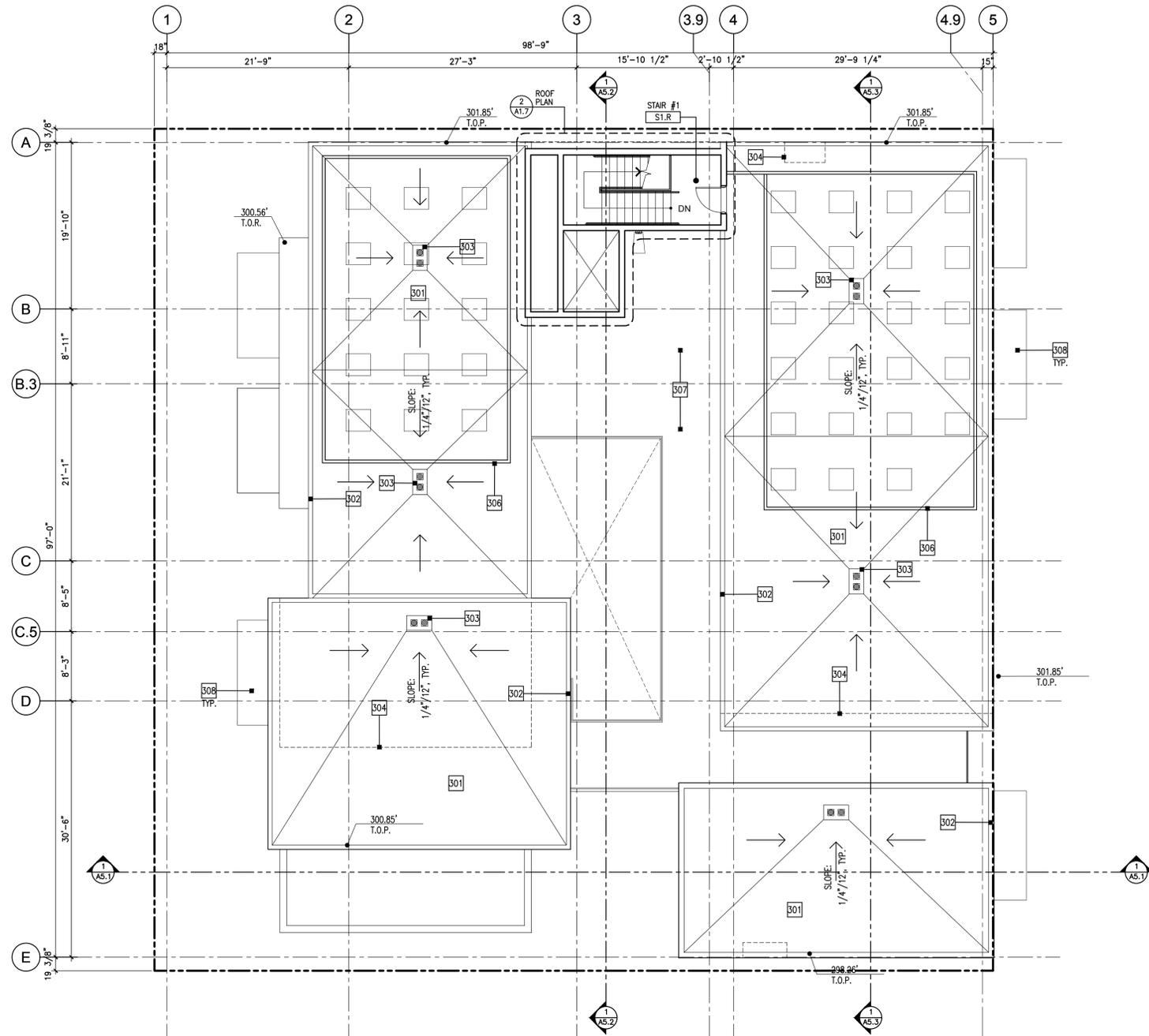
6TH LEVEL FLOOR PLAN



SHEET

A1.6





1 ROOF PLAN
SCALE 1/8" = 1'-0"

2 ROOF PLAN - STAIR TOWER / ELEVATOR PENTHOUSE
SCALE 1/8" = 1'-0"

KEYNOTES - ROOF PLAN

- B01 ROOF ASSEMBLY: TPO ROOF MEMBRANE O/
R-38 RIGID INSUL. O/ PLYWD SHEATHING O/
WOOD TRUSSES PER STRUCT
- B02 SHEET METAL COPING PAINTED TO MATCH
WALL
- B03 ROOF & OVERFLOW DRAINS
- B04 LINE OF WALL BELOW
- B05 ROOF HATCH
- B06 METAL PANEL ROOF SCREEN
- B07 LEVEL 6 FLOOR BELOW
- B08 BALCONY BELOW

4TH AVENUE APARTMENTS
2426 4th AVENUE
SAN DIEGO, CALIFORNIA
92101

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	PLANNING RESUBMITTAL	12.19.2018
	PLANNING RESUBMITTAL	01.22.2018

SHEET TITLE:
ROOF PLAN



SHEET
A1.7

GENERAL NOTES - EXTERIOR ELEVATIONS

- POST INDICATOR VALVES, FIRE DEPARTMENT CONNECTIONS, AND ALARM BELL ARE TO BE LOCATED ON THE ADDRESS/ACCESS SIDE OF THE STRUCTURE.
- PROVIDE BUILDING ADDRESS NUMBERS, VISIBLE AND LEGIBLE FROM THE STREET OR ROAD FRONTING THE PROPERTY.
- VEGETATION SHALL BE SELECTED AND MAINTAINED IN SUCH A MANNER AS TO ALLOW IMMEDIATE ACCESS TO ALL HYDRANTS, VALVES, FIRE DEPARTMENT CONNECTIONS, PULL STATIONS, EXTINGUISHERS, SPRINKLER RISERS, ALARM CONTROL PANELS, RESCUE WINDOWS, AND OTHER DEVICES OR AREAS USED FOR FIREFIGHTING PURPOSES. VEGETATION OR BUILDING FEATURES SHALL NOT OBSTRUCT ADDRESS NUMBERS OR INHIBIT THE FUNCTIONING OF ALARM BELLS, HORNS OR STROBES.

KEYNOTES - EXTERIOR ELEVATIONS

#401 EXPOSED CONCRETE	#413 PAINTED METAL PANEL FASCIA
#402 ALUMINUM STOREFRONT SYSTEM	#414 ROLLING GRILL
#403 WOOD & STEEL CABLE HANDRAIL	#415 C.I.P. CONCRETE WALL
#404 CORTEN STEEL PLANTER	#416 CONCRETE COLUMN
#405 EXTERIOR PLASTER SYSTEM	#417 LINE OF ELECTRICAL VAULT BELOW. SEE SHEET A0.2 FOR MORE INFORMATION
#406 OPERABLE HOPPER WINDOW	#418 PLUMB HEIGHT LINE OF 75'. INCENTIVE REQUEST FOR HEIGHT INCREASE FROM 65' MAX. PER CPIOZ.
#407 HVAC SCREEN PANELS	#419 EXTERIOR RATED TELEVISION IN WALL RECESS
#408 PLASTER CONTROL JOINT	#420 12" HIGH PIN MOUNTED ADDRESS NUMBER
#409 SM COPING PAINTED TO MATCH PLASTER	#421 FIRE DEPARTMENT CONNECTION
#410 PAINTED STEEL RAILING W/ CONC PANEL INFILL	#422 'KNOX BOX, MOUNT BETWEEN 5' MIN. TO 7' MAX.
#411 METAL MESH PANELS	#423 OPERABLE CASEMENT WINDOW
#412 CORRUGATED METAL PANELS	

NOTES:
 1. LEVEL 1 HEIGHT REQUIREMENT: MIN. 13' TO LEVEL 2
 HIGH END 18'-3 3/8", LOW END 13'-1"
 AVERAGE HEIGHT: 15'-7 1/2" > 13'-0"

LEGEND - EXTERIOR FINISH

(A) CORRUGATED METAL PANELS TYPE A: TBD
(B) CORRUGATED METAL PANELS TYPE B: TBD
(C) PLASTER FINISH: COLOR TBD
(D) PLASTER FINISH: COLOR TBD
(E) PLASTER FINISH: COLOR TBD

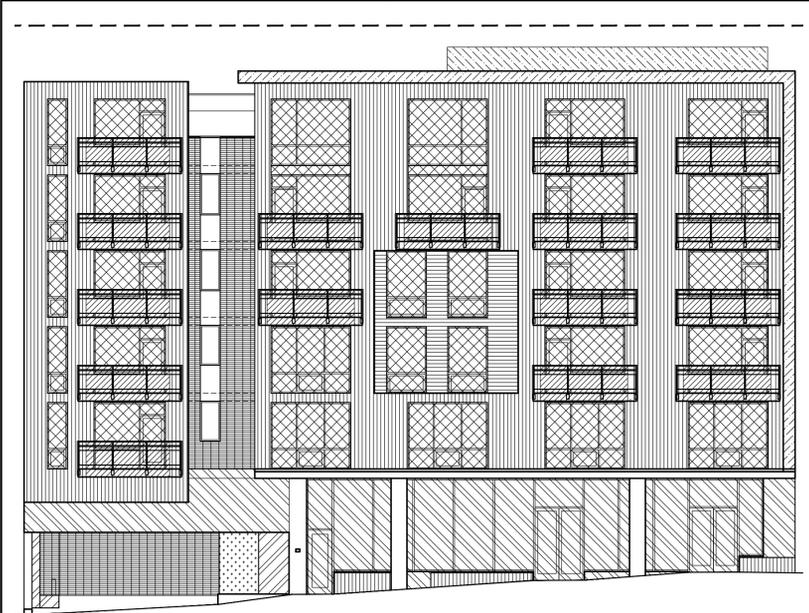
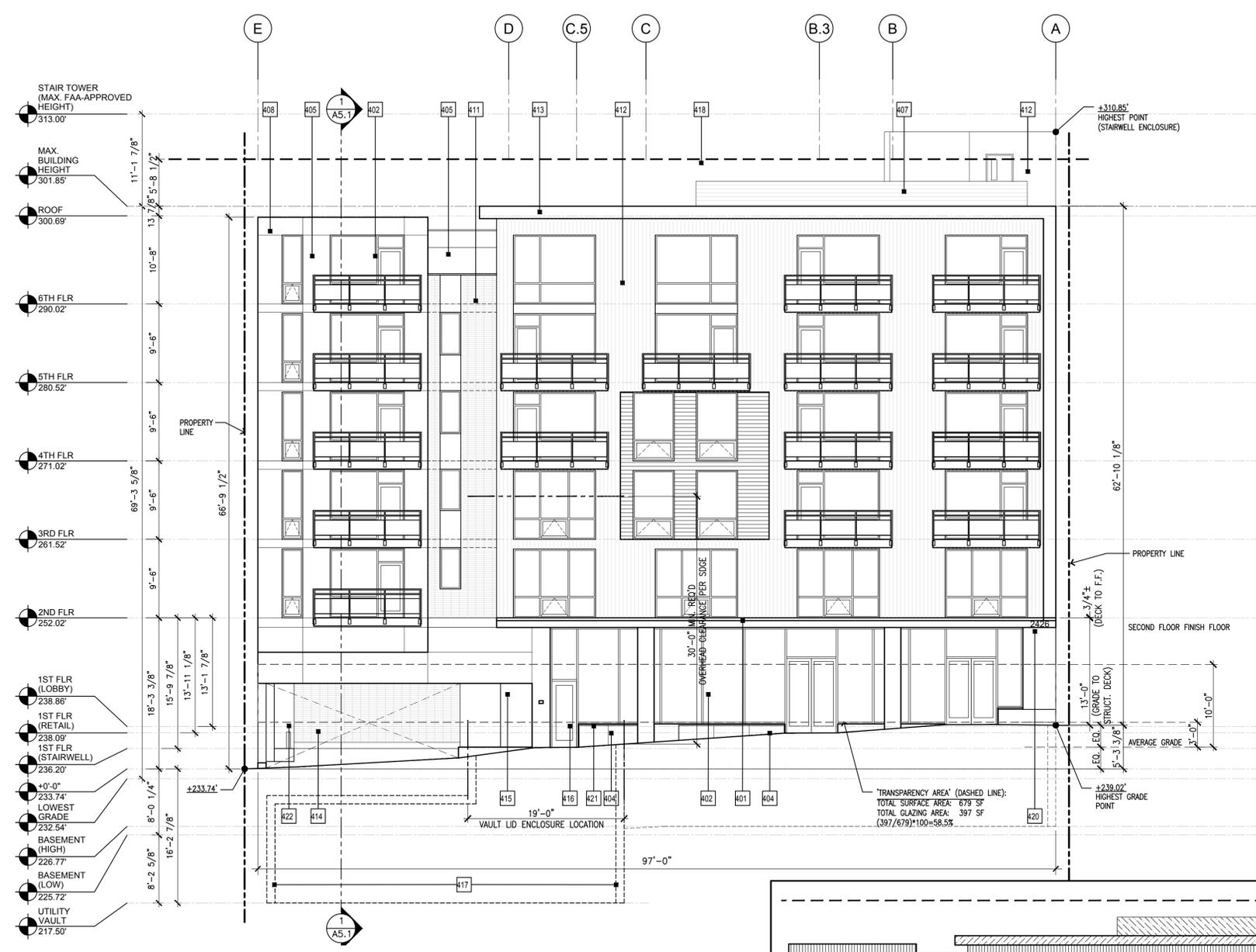
4TH AVENUE APARTMENTS
 2426 4th AVENUE
 SAN DIEGO, CALIFORNIA
 92101

REVISIONS		
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	PLANNING RESUBMITTAL	01.22.2018

SHEET TITLE:
EAST ELEVATION



SHEET
A4.0

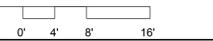
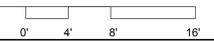


LEGEND - OFFSETTING PLANES

PATTERN	PLACEMENT RELATIVE TO REFERENCE PLANE (ℙ)
[Pattern]	0'-0"
[Pattern]	-0'-4"
[Pattern]	-1'-5"
[Pattern]	-1'-9"
[Pattern]	-2'-0"
[Pattern]	-3'-3.5"
[Pattern]	-7'-7"
[Pattern]	-9'-9"
[Pattern]	-24'-8"
[Pattern]	+1'-1"
[Pattern]	+5'-2.5"

1 EAST ELEVATION
 SCALE 1/8" = 1'-0"

2 OFFSET PLANES EXHIBIT
 SCALE 3/32" = 1'-0"



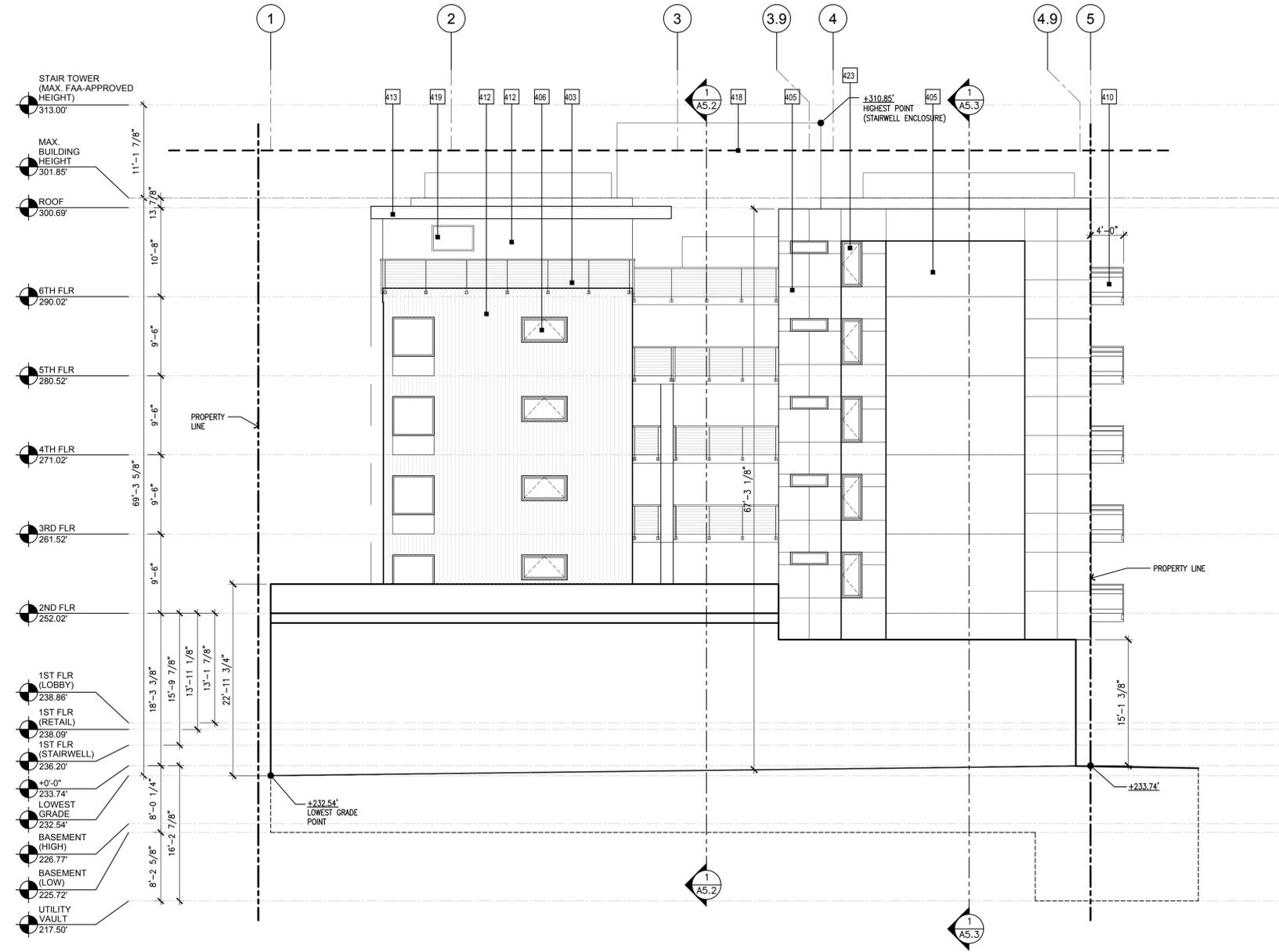
KEYNOTES - EXTERIOR ELEVATIONS

401 EXPOSED CONCRETE	413 PAINTED METAL PANEL FASCIA
402 ALUMINUM STOREFRONT SYSTEM	414 ROLLING GRILL
403 WOOD & STEEL CABLE HANDRAIL	415 C.I.P. CONCRETE WALL
404 CORTEN STEEL PLANTER	416 CONCRETE COLUMN
405 EXTERIOR PLASTER SYSTEM	417 LINE OF ELECTRICAL VAULT BELOW. SEE SHEET A0.2 FOR MORE INFORMATION
406 OPERABLE HOPPER WINDOW	418 PLUMB HEIGHT LINE OF 75'. INCENTIVE REQUEST FOR HEIGHT INCREASE FROM 65' MAX. PER CPIOZ.
407 HVAC SCREEN PANELS	419 EXTERIOR RATED TELEVISION IN WALL RECESS
408 PLASTER CONTROL JOINT	420 12" HIGH PIN MOUNTED ADDRESS NUMBER
409 SM COPING PAINTED TO MATCH PLASTER	421 FIRE DEPARTMENT CONNECTION
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411 METAL MESH PANELS	423 OPERABLE CASEMENT WINDOW
412 CORRUGATED METAL PANELS	

NOTES:
 1. LEVEL 1 HEIGHT REQUIREMENT: MIN. 13' TO LEVEL 2
 HIGH END 18'-3 3/8", LOW END 13'-1"
 AVERAGE HEIGHT: 15'-7 1/2" > 13'-0"

LEGEND - EXTERIOR FINISH

- (A) CORRUGATED METAL PANELS TYPE A: TBD
- (B) CORRUGATED METAL PANELS TYPE B: TBD
- (C) PLASTER FINISH: COLOR TBD
- (D) PLASTER FINISH: COLOR TBD
- (E) PLASTER FINISH: COLOR TBD



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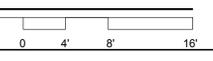
SHEET TITLE:
SOUTH ELEVATION



SHEET

A4.1

1 SOUTH ELEVATION
 SCALE 1/8" = 1'-0"



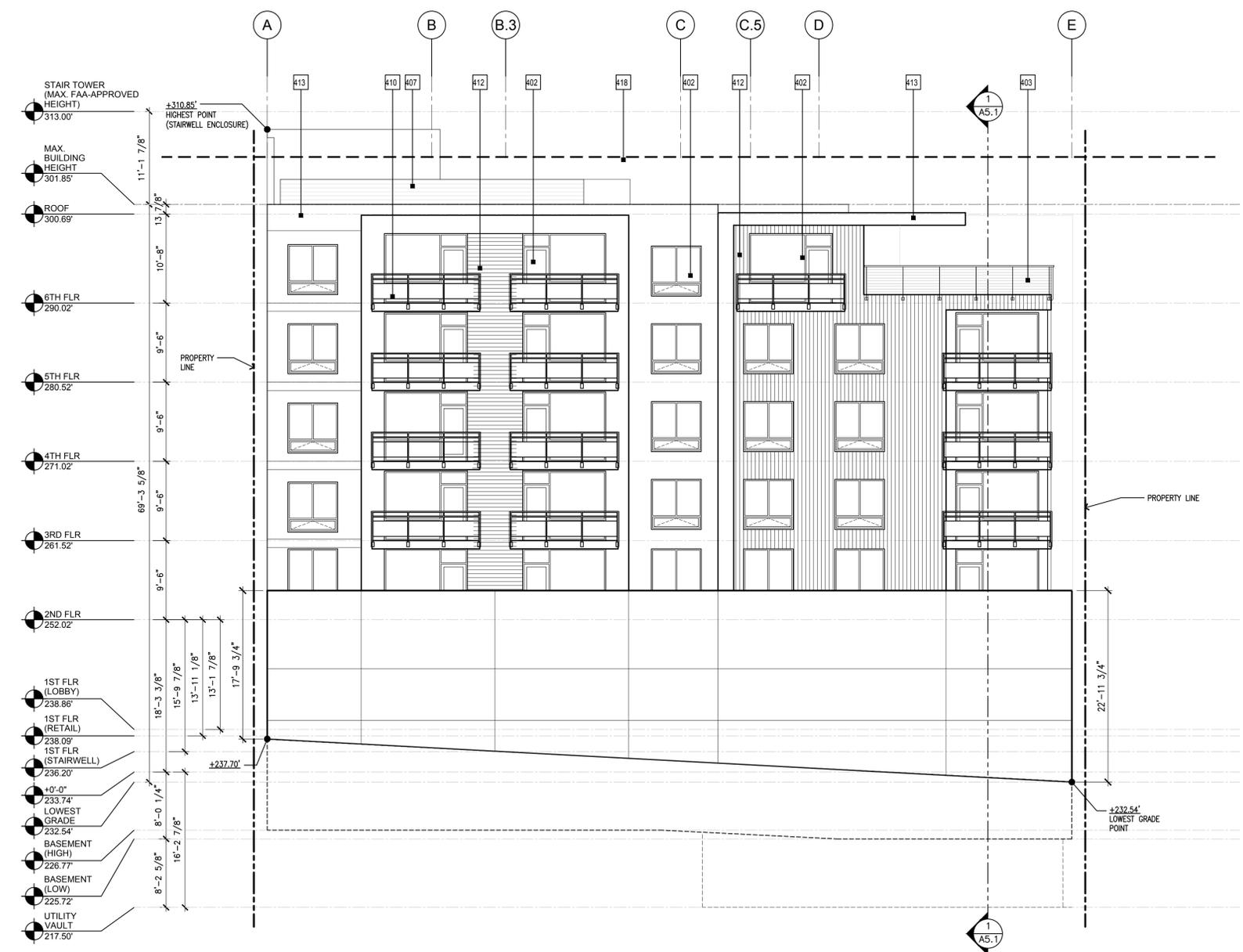
KEYNOTES - EXTERIOR ELEVATIONS

#01 EXPOSED CONCRETE	#13 PAINTED METAL PANEL FASCIA
#02 ALUMINUM STOREFRONT SYSTEM	#14 ROLLING GRILL
#03 WOOD & STEEL CABLE HANDRAIL	#15 C.I.P. CONCRETE WALL
#04 CORTEN STEEL PLANTER	#16 CONCRETE COLUMN
#05 EXTERIOR PLASTER SYSTEM	#17 LINE OF ELECTRICAL VAULT BELOW. SEE SHEET A0.2 FOR MORE INFORMATION
#06 OPERABLE HOPPER WINDOW	#18 PLUMB HEIGHT LINE OF 75'. INCENTIVE REQUEST FOR HEIGHT INCREASE FROM 65' MAX. PER CPIOZ.
#07 HVAC SCREEN PANELS	#19 EXTERIOR RATED TELEVISION IN WALL RECESS
#08 PLASTER CONTROL JOINT	#20 12" HIGH PIN MOUNTED ADDRESS NUMBER
#09 SM COPING PAINTED TO MATCH PLASTER	#21 FIRE DEPARTMENT CONNECTION
#10 PAINTED STEEL RAILING W/ CONC PANEL INFILL	#22 'KNOX BOX, MOUNT BETWEEN 5' MIN. TO 7' MAX.
#11 METAL MESH PANELS	#23 OPERABLE CASEMENT WINDOW
#12 CORRUGATED METAL PANELS	

NOTES:
 1. LEVEL 1 HEIGHT REQUIREMENT: MIN. 13' TO LEVEL 2
 HIGH END 18'-3 3/8", LOW END 13'-1"
 AVERAGE HEIGHT: 15'-7 1/2" > 13'-0"

LEGEND - EXTERIOR FINISH

- (A) CORRUGATED METAL PANELS TYPE A: TBD
- (B) CORRUGATED METAL PANELS TYPE B: TBD
- (C) PLASTER FINISH: COLOR TBD
- (D) PLASTER FINISH: COLOR TBD
- (E) PLASTER FINISH: COLOR TBD



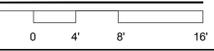
4TH AVENUE APARTMENTS
 2426 4th AVENUE
 SAN DIEGO, CALIFORNIA
 92101

REVISIONS		
NO	DESCRIPTION	DATE
	COMPLETENESS REVIEW	12.01.2017
	PLANNING PRELIMINARY SUBMITTAL	01.11.2018
	PLANNING SUBMITTAL	05.09.2018
	SDGE SUBMITTAL	07.09.2018
	PLANNING RESUBMITTAL	10.31.2018
	PLANNING RESUBMITTAL	12.19.2018
	PLANNING RESUBMITTAL	01.22.2018

SHEET TITLE:
WEST ELEVATION



SHEET
A4.2



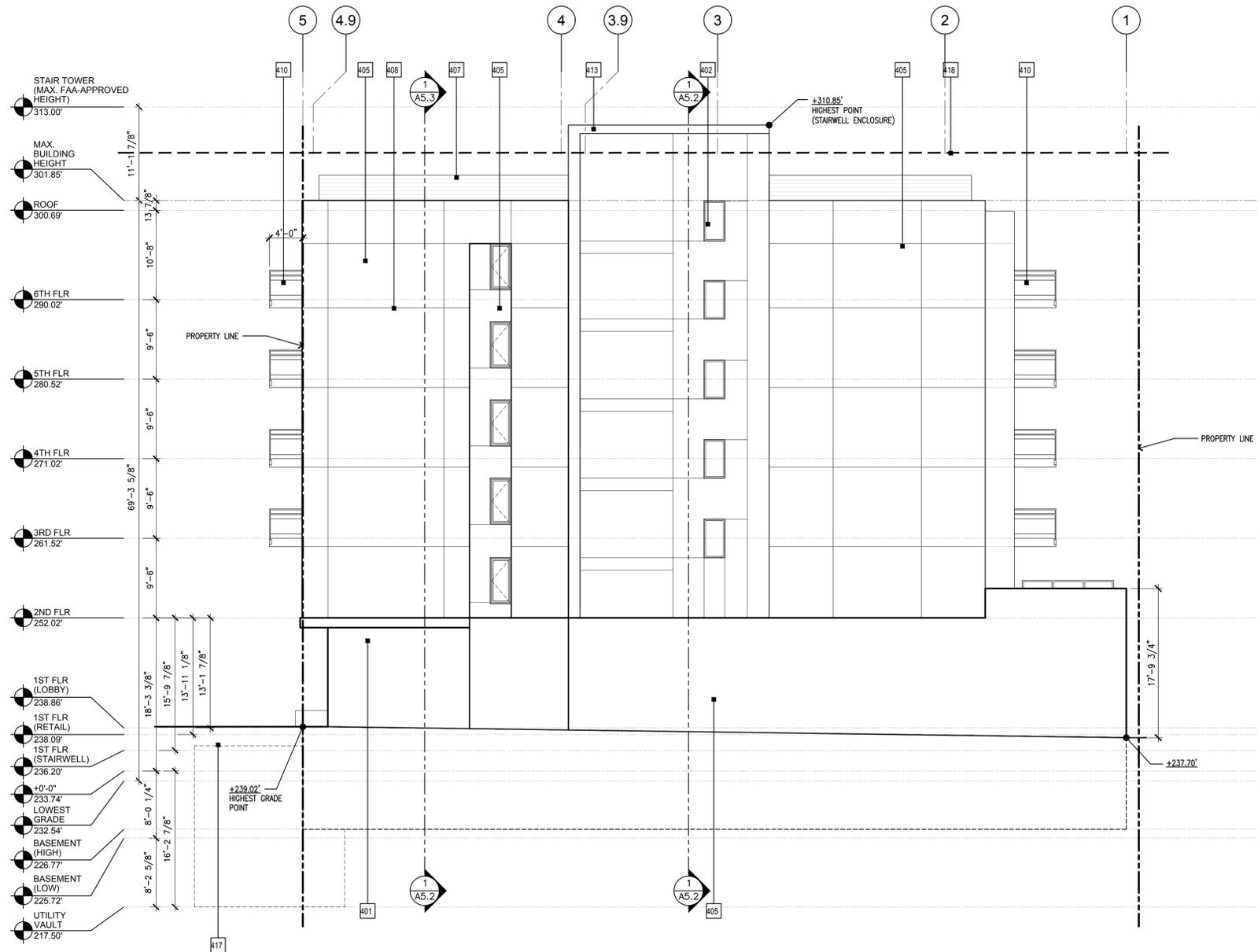
KEYNOTES - EXTERIOR ELEVATIONS

#01 EXPOSED CONCRETE	#13 PAINTED METAL PANEL FASCIA
#02 ALUMINUM STOREFRONT SYSTEM	#14 ROLLING GRILL
#03 WOOD & STEEL CABLE HANDRAIL	#15 C.I.P. CONCRETE WALL
#04 CORTEN STEEL PLANTER	#16 CONCRETE COLUMN
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NOTES:
 1. LEVEL 1 HEIGHT REQUIREMENT: MIN. 13' TO LEVEL 2
 HIGH END 18'-3 3/8", LOW END 13'-1"
 AVERAGE HEIGHT: 15'-7 1/2" > 13'-0"

LEGEND - EXTERIOR FINISH

(A)	CORRUGATED METAL PANELS TYPE A: TBD
(B)	CORRUGATED METAL PANELS TYPE B: TBD
(C)	PLASTER FINISH: COLOR TBD
(D)	PLASTER FINISH: COLOR TBD
(E)	PLASTER FINISH: COLOR TBD



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	PLANNING RESUBMITTAL	01.22.2018

SHEET TITLE:
NORTH ELEVATION



SHEET
A4.3

1 NORTH ELEVATION
 SCALE 1/8" = 1'-0"

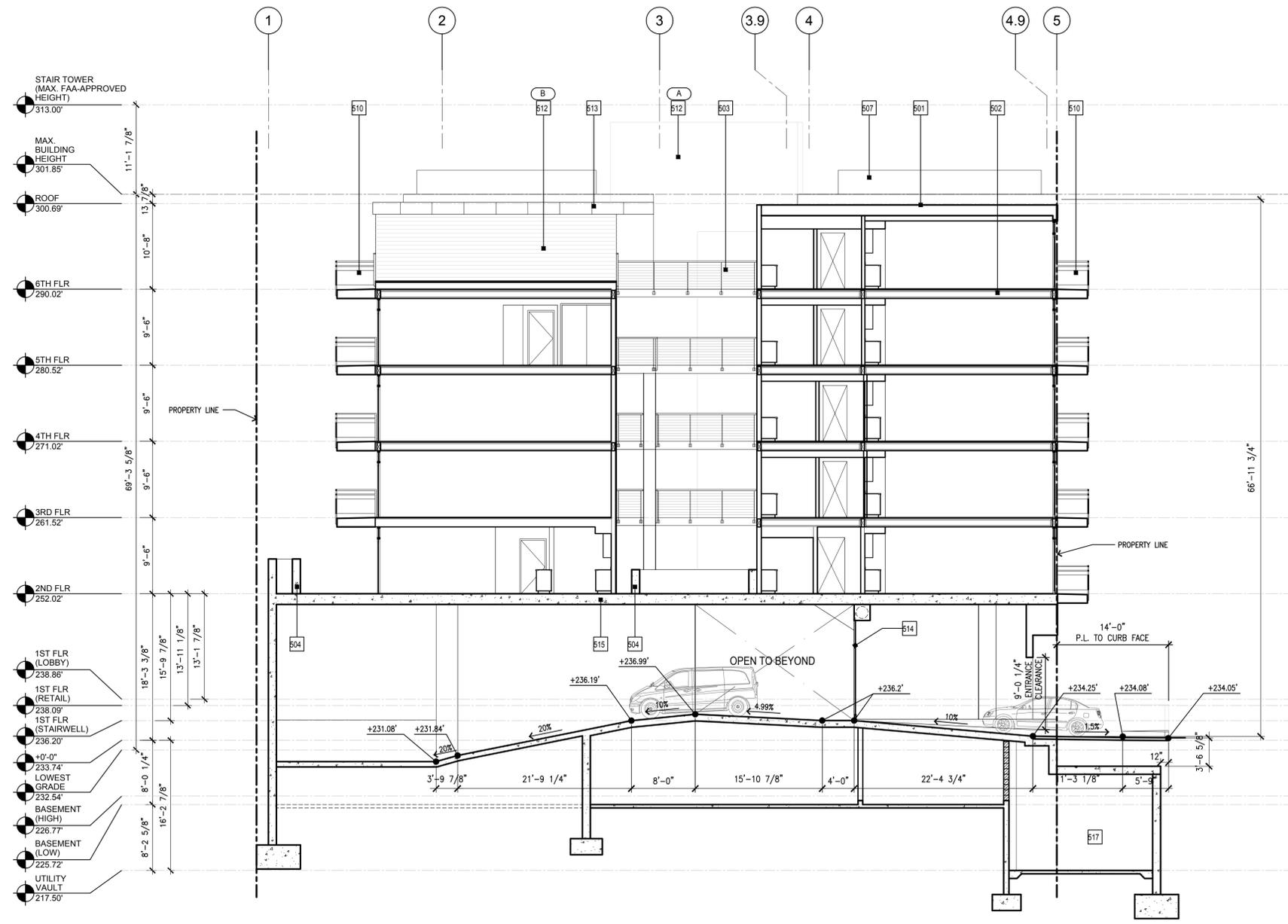


KEYNOTES - BUILDING SECTION

- 501 ROOF ASSEMBLY: TPO ROOF MEMBRANE O/
R-38 RIGID INSUL. O/ PLYWD SHEATHING
O/ WOOD TRUSSES PER STRUCT
- 502 TYPICAL FLOOR ASSEMBLY:
1 1/2" LWLT. CONC O/ PLYWD SHEATHING O/
WOOD TRUSSES PER STRUCT
- 503 WOOD & STEEL CABLE HANDRAIL
- 504 CONCRETE PLANTER
- 505 EXTERIOR PLASTER SYSTEM
- 506 ALUM STOREFRONT PER SCHEDULE
- 507 HVAC SCREEN PANELS
- 508 PLASTER CONTROL JOINT
- 509 SM COPING PAINTED TO MATCH PLASTER
- 510 PAINTED STEEL RAILING W/ CONC PANEL
INFILL
- 511 METAL MESH PANELS PER SPECS
- 512 CORRUGATED METAL PANELS
- 513 PAINTED METAL PANEL FASCIA
- 514 GARAGE GATE
- 515 CONCRETE PODIUM
- 516 GRADE PER CIVL
- 517 ELECTRICAL VAULT. SEE SHEET A0.2 AND
A1.0 FOR FURTHER INFORMATION.

LEGEND - EXTERIOR FINISH

- (A) CORRUGATED METAL PANELS TYPE A: TBD
- (B) CORRUGATED METAL PANELS TYPE B: TBD
- (C) PLASTER FINISH: COLOR TBD
- (D) PLASTER FINISH: COLOR TBD
- (E) PLASTER FINISH: COLOR TBD



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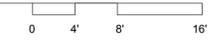
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NO	DESCRIPTION	DATE
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SHEET TITLE:
BUILDING SECTION



SHEET
A5.1

1 BUILDING SECTION
SCALE 1/8" = 1'-0"



KEYNOTES - BUILDING SECTION

- 501 ROOF ASSEMBLY: TPO ROOF MEMBRANE O/
R-38 RIGID INSUL. O/ PLYWD SHEATHING
O/ WOOD TRUSSES PER STRUCT
- 502 TYPICAL FLOOR ASSEMBLY:
1 1/2" LWTD. CONC O/ PLYWD SHEATHING O/
WOOD TRUSSES PER STRUCT
- 503 WOOD & STEEL CABLE HANDRAIL
- 504 CONCRETE PLANTER
- 505 EXTERIOR PLASTER SYSTEM
- 506 ALUM STOREFRONT PER SCHEDULE
- 507 HVAC SCREEN PANELS
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- 513 PAINTED METAL PANEL FASCIA
- 514 GARAGE GATE
- 515 CONCRETE PODIUM
- 516 GRADE PER CIVIL
- 517 ELECTRICAL VAULT. SEE SHEET A0.2 AND
A1.0 FOR FURTHER INFORMATION.

LEGEND - EXTERIOR FINISH

- A CORRUGATED METAL PANELS TYPE A: TBD
- B CORRUGATED METAL PANELS TYPE B: TBD
- C PLASTER FINISH: COLOR TBD
- D PLASTER FINISH: COLOR TBD
- E PLASTER FINISH: COLOR TBD

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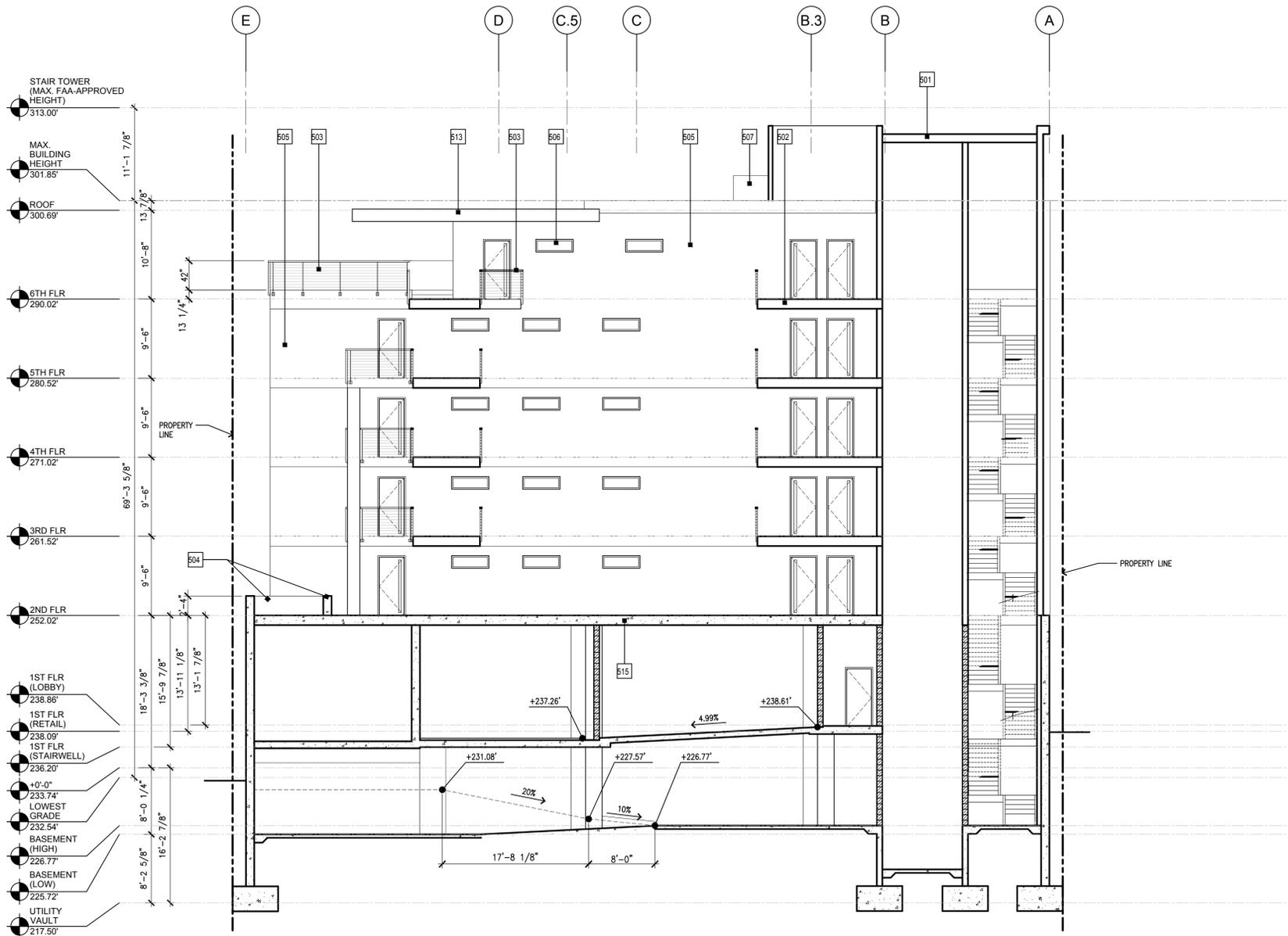
SHEET TITLE:

BUILDING SECTION



SHEET

A5.2



1 BUILDING SECTION
SCALE 1/8" = 1'-0"

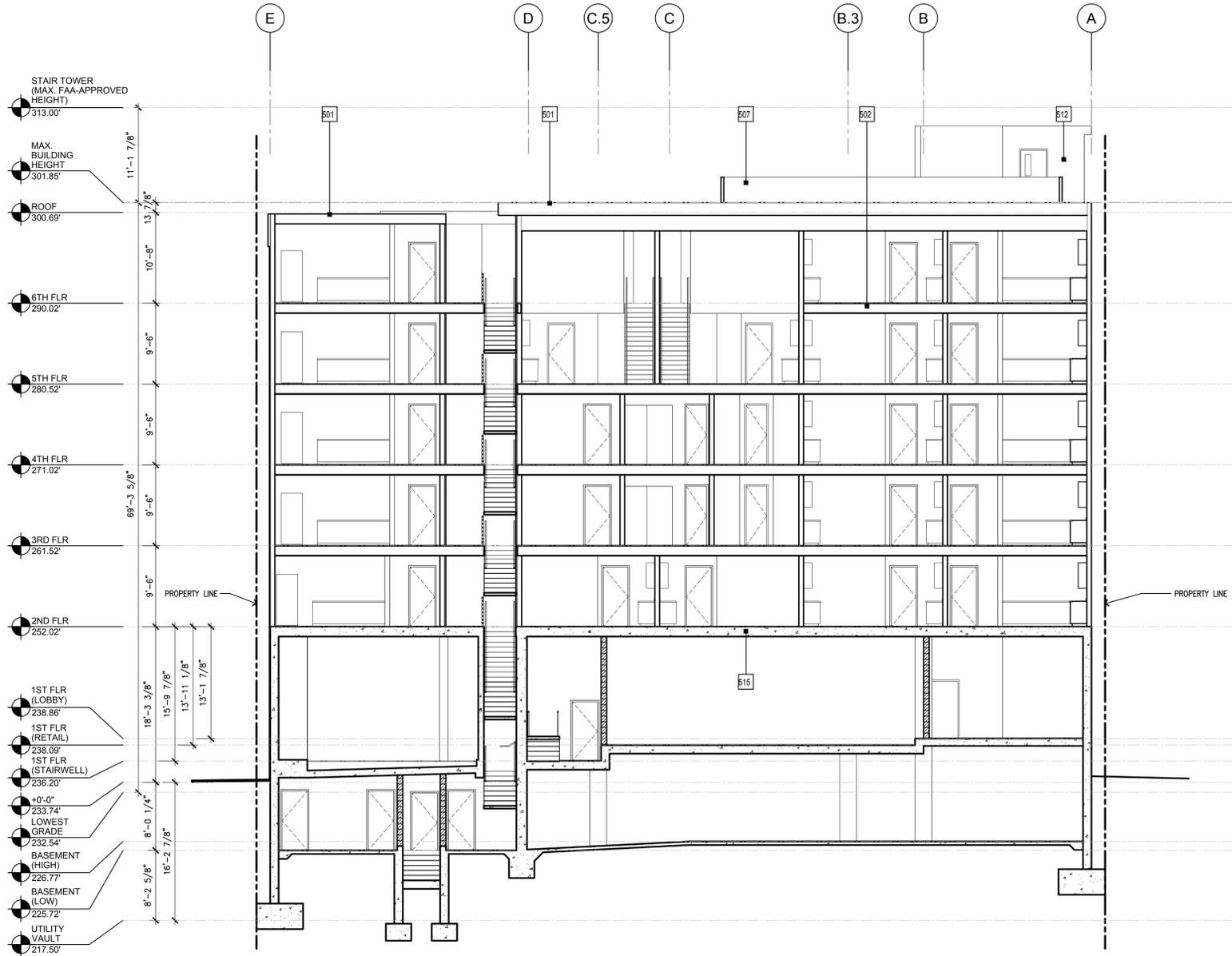


KEYNOTES - BUILDING SECTION

- 501 ROOF ASSEMBLY: TPO ROOF MEMBRANE O/
R-38 RIGID INSUL. O/ PLYWD SHEATHING
O/ WOOD TRUSSES PER STRUCT
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- 517 ELECTRICAL VAULT, SEE SHEET A0.2 AND
A1.0 FOR FURTHER INFORMATION.

LEGEND - EXTERIOR FINISH

- A CORRUGATED METAL PANELS TYPE A: TBD
- B CORRUGATED METAL PANELS TYPE B: TBD
- C PLASTER FINISH: COLOR TBD
- D PLASTER FINISH: COLOR TBD
- E PLASTER FINISH: COLOR TBD



STAIR TOWER
(MAX. FAA-APPROVED
HEIGHT)
313.00'

MAX.
BUILDING
HEIGHT
301.85'

ROOF
300.69'

6TH FLR
290.02'

5TH FLR
280.52'

4TH FLR
271.02'

3RD FLR
261.52'

2ND FLR
252.02'

1ST FLR
(LOBBY)
238.86'

1ST FLR
(RETAIL)
238.09'

1ST FLR
(STAIRWELL)
236.20'

+0'-0"
233.74'

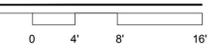
LOWEST
GRADE
232.54'

BASEMENT
(HIGH)
226.77'

BASEMENT
(LOW)
225.72'

UTILITY
VAULT
217.50'

1 BUILDING SECTION
SCALE 1/8" = 1'-0"



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SHEET TITLE:
BUILDING SECTION



SHEET
A5.3