



THE CITY OF SAN DIEGO

Report to the Planning Commission

DATE ISSUED: August 13, 2020 REPORT NO. PC-20-053

HEARING DATE: August 20, 2020

SUBJECT: 9G TOWER SDP/NDP/CUP/TM, Process Four Decision

PROJECT NUMBER: [649856](#)

OWNER/APPLICANT: 4 Lee Tenth LLC, Owner / Cisterra Partners LLC, Applicant

SUMMARY

Issue: Should the Planning Commission approve a request for a Site Development Permit (SDP), Neighborhood Development Permit (NDP), Conditional Use Permit (CUP), and Tentative Map (TM) for the construction of a 22-story, 253-foot tall mixed-use development comprised of 241 dwelling units (DU), 35,668 square feet (SF) of commercial space, and 243 parking spaces ("Project"), including substantial alterations to a historical resource, five deviations from the development regulations, off-site alcoholic beverage sales, and the creation of seven commercial condominiums units, located at 659 Ninth Avenue on the south side of G Street between Ninth and Tenth avenues in the Downtown Community Plan (DCP) area ("Downtown")?

Staff Recommendation: Approve SDP/NDP/CUP/TM No. 649856 for the Project.

Community Planning Group Recommendation: On January 15, 2020, the Downtown Community Planning Council (DCPC) voted 10-7 to recommend approval of the Project, including extending hours of off-site alcoholic beverage sales from 8:00 a.m. to 12:00 a.m. and the recommending the addition of more personal storage units in the Project.

Historical Resources Board Recommendation: On July 23, 2020, the City of San Diego ("City") Historical Resources Board (HRB) voted 8-0 to recommend that the Planning Commission adopt the findings and mitigation measures associated with the Project's SDP for the substantial alteration of the designated historical resource, HRB Site No. 1355 – the Pacific Telephone and Telegraph Company Garage.

Environmental Review: Development within the Downtown Community Planning area is covered under the following documents, all referred to as the "Downtown FEIR": Final Environmental Impact Report (FEIR) for the San Diego Downtown Community Plan, Centre City Planned District Ordinance, and 10th Amendment to the Centre City Redevelopment

Plan, certified by the former Redevelopment Agency ("Former Agency") and the City Council on March 14, 2006 (Resolutions R-04001 and R-301265, respectively); subsequent addenda to the FEIR certified by the Former Agency on August 3, 2007 (Former Agency Resolution R-04193), April 21, 2010 (Former Agency Resolution R-04510), and August 3, 2010 (Former Agency Resolution R-04544), and certified by the City Council on February 12, 2014 (City Council Resolution R-308724) and July 14, 2014 (City Council Resolution R-309115); and, the Final Supplemental Environmental Impact Report for the Downtown San Diego Mobility Plan certified by the City Council on June 21, 2016 (Resolution R-310561). Development within the Downtown Community Planning area is also covered under the following documents, all referred to as the "CAP FEIR": FEIR for the City of San Diego Climate Action Plan (CAP), certified by the City Council on December 15, 2015 (City Council Resolution R-310176), and the Addendum to the CAP, adopted by the City Council on July 12, 2016 (City Council Resolution R-310595). The Downtown FEIR and CAP FEIR are both "Program EIRs" prepared in compliance with California Environmental Quality Act (CEQA) Guidelines Section 15168. The information contained in the Downtown FEIR and the CAP FEIR reflects the independent judgement of the City of San Diego as the Lead Agency. The Downtown FEIR and CAP FEIR are located on the [City website](#). Consistent with best practices suggested by CEQA Guidelines Section 15168, a Downtown 15168 Consistency Evaluation ("Evaluation") has been completed for the Project. The Evaluation concluded that the environmental impacts of the Project were adequately addressed in the Downtown FEIR and CAP FEIR; that the Project is within the scope of the development program described in the Downtown FEIR and CAP FEIR and is adequately described within both documents for the purposes of CEQA; and, that none of the conditions listed in CEQA Guidelines Section 15162 exist. Therefore, no further environmental documentation is required under CEQA. The Evaluation is attached to this staff report (Attachment 11) for informational purposes only; no action regarding the Evaluation is required by the decision maker.

Fiscal Impact Statement: None with this action. All costs associated with the processing of the Project are paid from the deposit account maintained by the Applicant.

Code Enforcement Impact: None.

Housing Impact Statement: The Project proposes the construction of a net increase of 241 market-rate DU on a 25,061 SF site currently occupied by a commercial warehouse. The Project will comply with the Inclusionary Housing Ordinance by paying the in-lieu fee.

BACKGROUND

The Project site is located at 659 Ninth Avenue on the south side of G Street between Ninth and Tenth avenues (Attachment 1) in the East Village neighborhood of the DCP area and the Employment/Residential Mixed-Use land use district of the Centre City Planned District Ordinance (CCPDO). The site is by a mix of mid-to-high density residential and commercial land uses. Many Downtown amenities and attractions are located within walking distance of the site (Attachment 15, Sheet 4), including the Park & Market Trolley Station two blocks to the southeast, the future East Village Green Park three blocks to the east, and the Gaslamp Quarter four blocks to the west.

The Project site is currently developed with a commercial warehouse building, originally constructed in 1932. The building was designated as HRB Site No. 1355 on November 21, 2019 under Criterion A with a period of significance of 1932-1956 as a special element of the City's economic development, revealing the increasing importance of the automobile as America's favored method of private transportation from 1900-1956 and under Criterion C with a period of significance of 1932 as a good example of the Art Deco style. The designation includes the 1946 addition constructed within the period of significance.

The Project requires a Process 4 SDP for substantial alterations to a historical resource per San Diego Municipal Code (SDMC) Section 126.0505(d)(1). Substantial alteration of a designated historical resource means demolition, destruction, relocation, new construction, or alteration activities that would impair the significance of a historical resource. The SDP is needed because the redevelopment of the site cannot be determined to be consistent with the Secretary of the Interior's Standards for Historical Properties due to the loss of original materials on the historic structure, including the removal of the 1946 addition, and the massing, size, scale and proportion of the proposed 21-story tower above the historical resource.

The Project also requires a NDP for five deviations from the development regulations of the SDMC per Section 126.0404(p), a CUP for the sale off-site alcoholic beverage sales per Section 156.0315(b)(1), and a TM for the creation of seven commercial condominium units per Section 125.0410. Pursuant to Section 112.0103 of the SDMC, when an Applicant applies for more than one permit for a single development, the applications shall be consolidated for processing and shall be reviewed by a single decision maker. The decision maker shall act on the consolidated application at the highest level of authority for that development, and the findings required for approval of each permit shall be considered individually. The decision maker for this Project is the Planning Commission under a Process Four review. The decision is appealable to the City Council.

DISCUSSION

Project Description:

The Project consists of a 22-story, 253-foot tall mixed-use development comprised of 241 DU and 35,668 SF of commercial space on the ground and basement levels. The commercial space is contained within a historical resource, known as the Pacific Telephone and Telegraph Company Garage (HRB Site No. 1355), which will retain its historic façade, while the interior is proposed to be demolished and reconstructed to house a large-scale general merchandise retailer, containing a grocery component that includes the sale of alcoholic beverages for off-site consumption. Directly above the historical resource, the Project proposes five above-grade parking garage levels consisting of 243 parking spaces and 50 bicycle parking spaces. The upper 16 stories contain the 241 DU, which range in size from a 623 SF studio to a 1,951 SF three-bedroom unit. The Project provides residential amenities for residents on Level 7 and the rooftop, including 6,134 SF of outdoor common open space and 5,744 SF of interior common space. Personal storage units are proposed for 38% of the residential DU and balconies are provided for 56% of the residential DU. See the Project Data Sheet (Attachment 2) and Development Plans (Attachment 15) for full Project details. Additionally, the

Project requests a Tentative Map (TM) to create seven commercial condominium units on the single parcel. The TM is included as Attachment 14.



The design of the Project exhibits a modern architectural aesthetic that incorporates a designated historical resource into the street-level experience. The ground floor of the Project is characterized by the façade of the historical resource, which is being retained and restored. Modifications to the historical resource's exterior include the removal of the 1946 addition and a portion of the façade on Tenth Avenue for a driveway. In addition, doors will be added on two facades and storefront infill will be added at existing garage openings (Attachment 15, Sheets 26-28). A key design issue is the relationship between the historical resource and the new construction above it, including the transition to the five-level parking garage. To create visual interest on the expansive five-level façade of the parking garage, an arrangement of angled, flat, metal panels are proposed on a majority of the frontages, with a spandrel window wall system at the northwest corner (Attachment 15, Sheet 21). The upper 16 levels containing the residential DU exhibits a rectangular massing comprised of primarily glazing. Concrete frames that run the height of the tower and balconies modulate the tower's massing, while an offset rooftop level with a steel canopy creates an interesting transition to the sky (Attachment 15, Sheet 22).

As part of the Project, five deviations from the development regulations of the SDMC are being requested, as described below. These deviations are primarily a result of the combined constraints of the relatively small lot size and the retention of the historical resource on-site. The staff's draft findings for the NDP can be found in full in Attachment 4.

1. Sec. 131.0454 – Personal Storage: Reduce the number of required personal storage areas from 100% of DU (241 DU) to 38% of DU (91 DU).
2. Sec. 142.0560(j) – Driveway & Access Regulations: Reduce the minimum width required for the one-way driveway on Ninth Avenue width from 14 feet to 12 feet.
3. Sec. 156.0310(d)(3)(A) – Tower Lot Coverage: Increase the allowable maximum tower lot coverage from 50% to 66%.
4. Sec. 156.0310(d)(3)(B) – Tower Floor Plate: Increase the allowable maximum east-west tower floor plate dimension from 130 feet to 150 feet.
5. Sec. 156.0311(d)(1) – Transparency: Reduce the minimum required ground level transparency of the Tenth avenue building façade from 60% to 40%.

Community Plan Analysis:

The Downtown Community Plan (DCP) envisions Downtown as a multi-use regional center with strong employment and residential components, targeting a residential buildout population of approximately 90,000 people with a market for a broad array of supporting stores and services with opportunities to live close to jobs and transit. The DCP implements the City of Villages strategies of

the City's General Plan by directing growth in limited areas served by transit as an efficient use of urban land that reduces the need to develop outlying areas while creating opportunities for realistic alternatives to automobile travel.

The Project is located within the East Village neighborhood of the DCP area, which was historically a mix of commercial, warehouse, light industrial, educational, and residential uses and has seen a number of mid- and high-rise developments in the last twenty years. Ultimately, East Village is projected to grow to 46,000 residents. Various portions of East Village will have substantially different character, contributing to the eclecticism and interest of this neighborhood. The Northwest sub-district of this neighborhood, within which this Project is located, is envisioned as Downtown's residential core that will yield Downtown's highest-intensity residential-emphasis district. The Northwest's many opportunity sites, location at the heart of Downtown, accessibility to transit, and distance from the airport overflight zone make it ideal for high-intensity building.

While this part of the East Village neighborhood is envisioned to contain very high-intensity residential development to accommodate Downtown's growth and population goals, the DCP also strongly encourages the preservation, rehabilitation, restoration, reconstruction, and retention of designated historical resources and their incorporation into new development projects, whether in whole or in part.

The following are some key DCP Goals and Policies regarding both population and housing and historic preservation that are applicable to the Project:

- 3.2-G-5: Maintain a range of development intensities to provide diversity, while maintaining high overall intensities to use land efficiently and permit population and employment targets to be met.
- 3.5-G-2: Foster a rich mix of uses in all neighborhoods, while allowing differences in emphasis on uses to distinguish between them.
- 6.5-G-6: Develop Northwest as the most intensive residential area in concert with its central location, transit access, and available redevelopment sites.
- 6.5-G-8: Reinforce Northwest's proximity to Downtown destinations as an essential component of its character.
- 9.1-G-1: Protect historical resources to communicate Downtown's heritage.
- 9.1-G-2: Encourage the rehabilitation and reuse of historical resources.
- 9.2-G-1: Integrate historical resources into the Downtown fabric while achieving policies for significant development and population intensification.
- 9.2-P-1: Incorporate elements of historical buildings in new projects to impart heritage.
- 9.2-P-3: Promote the adaptive reuse of intact buildings (designated or not) and/or significant elements, as a cultural and suitability goal.

Project-Related Issues:

Conflicting DCP Goals: As described in the Community Plan Analysis, there are many goals of the DCP that apply to the Project in different ways. On one hand, the DCP has established a residential buildout population of approximately 90,000 people, including a target of 46,000 in the East Village

neighborhood alone. In pursuit of this goal, the Northwest sub-district of the East Village neighborhood is envisioned as the most intensive residential area, due to its central location nearby transit options, and many opportunities for redevelopment. This Project is one such opportunity. On a relatively small site (25,061 SF), the Project proposes a residential tower containing 241 DU, which equates to approximately 422 DU per acre. The Project is optimizing the use of the site by obtaining FAR Bonuses and requesting deviations from the development regulations of the CCPDO to maximize the buildable area, while maintaining a financially feasible project.

One the other hand, the DCP strongly encourages protecting historical resources throughout Downtown in order to communicate the story of Downtown and preserve its unique character that it has developed over the last 150 years. To do so, it encourages the rehabilitation and reuse of historical resources and recommends incorporating historical resources into new projects. This



Project also presents an opportunity to implement these historic preservation goals, with the presence of the historic Pacific Telephone and Telegraph Company Garage, original constructed in 1932 and designated as a historical resource by the City's HRB in 2019. With the retention and restoration of the original façade of the historical resource into the Project, it is advancing the historic preservation goals of the DCP.

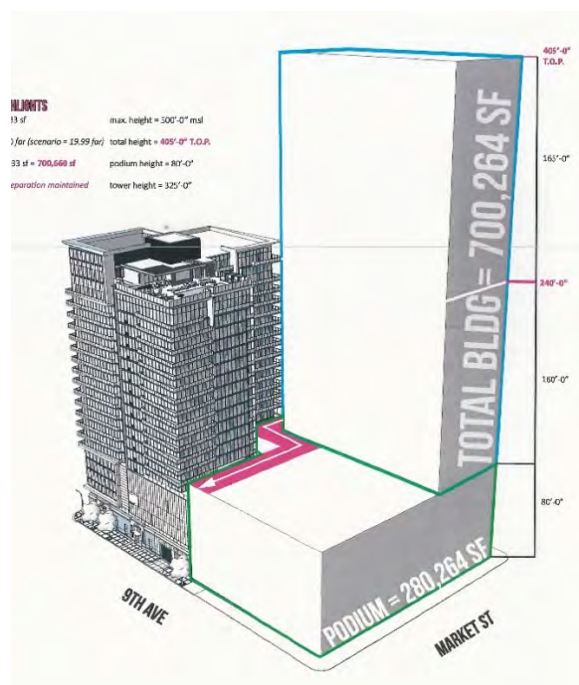
While the DCP sets high population targets and goals for high-density neighborhoods, it also encourages projects to protect and integrate existing historic structures, which can be a major constraint for any new construction. Reconciling these conflicting DCP goals is a balance, which the Project has attempted to strike. It adds a high-density residential development consistent with the DCP, yet it substantially alters the historic integrity of the historical resource through the addition of a large tower above the historical façade with no setbacks even while maintaining the character-defining architecture and street-level experience of the historical resource. In order to provide the higher density, it requires deviations from the development regulations of the code that increase the overall massing of the building. Staff has closely analyzed this balance between DCP goals and believes that the Project reasonably advances these key applicable goals of the DCP and has prepared the findings to support the SDP (Attachment 4).

Proposed Deviations: Two of the five requested deviations propose to increase the mass of the tower in order to maximize buildable area and the number of residential DUs in the Project. The Project proposes to increase the maximum tower lot coverage from 50% to 66% and increase the maximum east-west tower floor plate dimension from 130 feet to 150 feet (it should be noted that the CCPDO allows towers to exhibit a 200-foot dimension in the north/south direction, similar to the newly constructed Merian project two blocks to the east). As described in the DCP, the intent of these development regulations is to, "maximize open views of the sky and sun exposure for streets and public spaces" (DCP, 5.3-P-5). Additionally, appropriately scaled towers should have a good

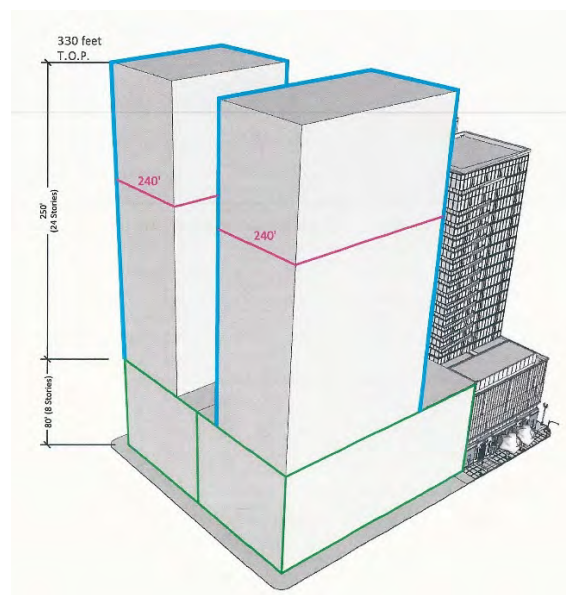
relationship with surrounding development, create an inviting street-level experience for pedestrians, and improve the character of the neighborhood.

According to the Applicant, the requested deviations are a result of cost efficiency measures designed to ensure the Project is financially feasible and able to secure the necessary financing. To achieve that, the deviations increase the amount of buildable area in order to maximize the number of DU. Another factor that drives the request for these deviations is the desire of the Applicant to keep the Project under 240 feet in height. As cited by the Applicant, buildings that exceed 240-feet necessitate a dual-frame structural system. With this different construction type, along with the other associated costs, the Applicant estimates that exceeding 240-feet in height would increase the construction costs of the Project by approximately \$8 million. While Staff acknowledges that developments exceeding 240 feet in height have increased construction costs, there are a number of high-rise buildings in Downtown that surpassed this height, including seven projects completed in the last two years, three currently under construction, and three currently in plan check. When the Project was presented to DCPC, although they ultimately voted to support the Project by a 10-7 vote, significant concern was raised by members about these two deviations and the Applicant's desire to keep the Project below 240 feet in height.

In response to staff's suggestion to address concerns and analyze the impact of the increased massing of the Project on the neighborhood if adjacent properties also developed with high-rise towers, the Applicant prepared a massing study (Attachment 7, Exhibit A). In it, two scenarios were modeled for the potential development of the remainder of the block with 1) a one-tower project on the remainder of the block and 2) two one-tower projects on each half of the remainder of the block. The Applicant demonstrated that in both scenarios, the potential projects can design buildings that maximize their buildable area while complying with the development regulations of the CCPDO.



Scenario 1 (one site with one-tower project)



Scenario 2 (two sites with one-tower projects)

The Project makes efforts to address the increased mass of the building by adhering to the DDG direction on building tower design. The tower is designed with a majority of the facades composed of glazing (DDG, 4.5.4.F) and incorporates balconies and architectural projections to provide desirable elevation and composition variety (DDG, 4.5.4.E). Overall, the tower expresses variety, avoids monotony, and distinguishes different building volumes (DDG, 4.4.1.C) by employing multiple glazing colors, different materials, and concrete framing components that span the height of the tower. Considering the minimal impact of the massing on neighboring properties, the enhanced design, and the advancement of the population goals of the DCP, staff believes that these deviations are justified, as described further in the draft findings (Attachment 4).

Off-Site Alcoholic Beverage Sales: The ground and basement levels of the Project are proposed to contain a large-scale general merchandise retail tenant, containing a grocery component. As part of this retail space, the Applicant is requesting the sale of alcoholic beverages for off-site consumption. Pursuant to SDMC Section 156.0315, any establishment within the Centre City Planned District engaged in the sale of alcoholic beverages for off-site consumption is required to obtain approval of a CUP and is subject to the following regulations:

- a) No wine or distilled spirits shall be sold in containers of less than 750 milliliters.
- b) No malt beverage products shall be sold in quantities of less than a six-pack of 12-ounce bottles or other containers totaling a minimum of 64 ounces.
- c) No alcoholic beverages shall be sold except between the hours of 10:00 a.m. and 10:00 p.m.

Per Section 156.0315(b)(1)(D) of the CCPDO, the Applicant may request an exception to the standard hours in (C) above, which may be granted by the decision-maker at the public hearing. As part of the request, the Applicant is seeking an exception from the standard hours to allow off-site alcohol beverage sales to occur from 8:00 a.m. to 12:00 a.m. seven days a week (Attachment 6). Historically, staff has not recommended approval of exceptions to the hours of off-site alcohol sales due to concerns that allowing alcohol sales to occur beyond the prescribed limits in the CCPDO will contribute to alcohol-related problems in the surrounding neighborhoods. However, exceptions have been granted for the following large-scale grocery stores:

- Alberton's; 655 14th Street; CUP 2006-56; 8:00 a.m. to 12:00 a.m., seven days a week
- Jimbo's; 92 Horton Plaza; CUP 2012-49; 8:00 a.m. to 10:00 p.m., seven days a week

NOTE: Ralph's at 101 G Street also has extended off-site alcoholic beverage sales hours; however, they are a previously conforming use that has operated with off-site alcoholic beverage sales prior to the requirement for a CUP.

In the past, Civic San Diego supported the extended hours for these two establishments because the alcohol sales make-up a very small portion of the overall inventory of the store and a small display footprint in relation to the store as a whole. That, combined with the typical conditions of approval for off-site alcoholic beverage sales, including conditions concerning security and loitering, contributed to the limited support for the extended hours for certain establishments in the past. When this Project was presented to the DCPC in January, 2020, their recommendation included

support for extended hours of off-site alcoholic beverage sales from 8:00 a.m. to 12:00 a.m., based on the characterization of the store that is consistent with the stores receiving past approvals for extended hours, Alberton's and Jimbo's.

The San Diego Police Department (SDPD) also reviewed the Project and provided a letter of recommendation to approve the Project with conditions; however, the SDPD does not support extended hours of off-site alcoholic beverage sales beyond the standard 10:00 a.m. to 10:00 p.m. or the sales of distilled spirits, as is permitted with a CUP in Section 156.0315(b)(1)(a) of the CCPDO subject to restrictions on container size. SDPD's recommendation cites an overconcentration of off-sale alcohol licenses in Downtown that they say contributes to a high crime and alcohol rate in the area. Their recommended conditions are reflected in the Draft Permit (Attachment 5) and can be read in full in Attachment 9. Despite the SDPD recommended conditions, the Applicant requests consideration by the Planning Commission of extended hours of alcoholic beverage sales from 8:00 a.m. to 12:00 a.m. consistent with other large-scale retailers, and the ability to sell distilled spirits, as detailed in their letter included as Attachment 6.

Conclusion:

City staff has reviewed the proposed Project and all issues identified through the review process have been resolved in conformance with adopted policies and regulations of the SDMC. Staff has provided draft findings to support approval of the Project (Attachment 4) and draft conditions of approval (Attachment 5). Staff recommends that the Planning Commission approve SDP/NDP/CUP/TM No. 649856 for the Project.

ALTERNATIVES

1. Approve SDP/NDP/CUP/TM No. 649856, with modifications.
2. Approve SDP/NDP/CUP/TM No. 649856, with modifications, including allowing the sale of distilled spirits and extending the standard hours of off-site alcoholic beverage sales to between 8:00 A.M. and 12:00 A.M. Pursuant to Section 156.0315(b)(1)(D) of the SDMC, if the Planning Commission wishes to grant the request for extended off-site alcoholic beverage hours, the following two findings must be made:
 - a. The request for an exception was provided in the public notice for the hearing; and
 - b. The proposed use and operations are compatible with existing and planned surrounding land uses.
3. Deny SDP/NDP/CUP/TM No. 649856, if the findings required to approve the Project cannot be affirmed.

Respectfully submitted,



James Alexander
Program Manager
Urban Division

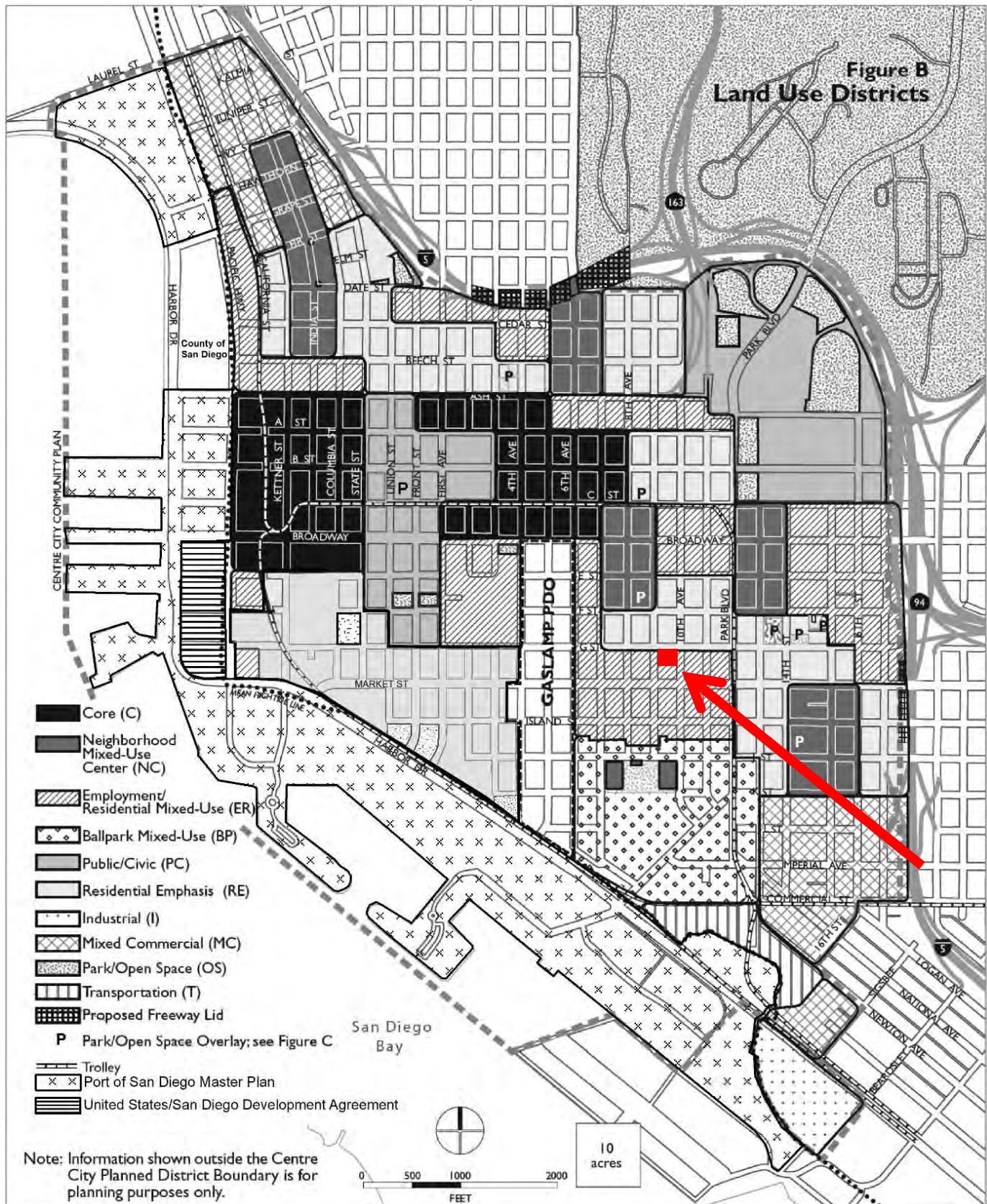


Brad Richter
Deputy Director
Urban Division

Attachments:

1. Project Location Map
2. Project Data Sheet
3. Ownership Disclosure Statement
4. Draft Permit Resolution with Findings
5. Draft Permit with Conditions
6. Applicant-submitted Request Letter
7. Applicant-submitted Draft Findings
8. Applicant-submitted Economic Analysis
9. Historic Treatment & Monitoring Plan
10. SDPD Recommendation
11. Downtown FEIR Consistency Evaluation & MMRP
12. Draft TM Resolution with Findings
13. Draft TM Conditions
14. Tentative Map
15. Development Plans dated July 20, 2020

PROJECT LOCATION MAP
9G – PROJ NO. 649856



PROJECT DATA SHEET
9G – PROJECT NO. 649856

Site Area	25,061 SF		
Base Min. FAR	3.5		
Base Max. FAR	6.0		
Max. FAR w/Amenity Bonuses	20.0		
Max. FAR w/Affordable Housing Bonus	23.0		
No Deviations to Height or Setbacks	23.6		
Proposed FAR	11.14		
FAR Bonuses Proposed	2.14 – FAR Payment Program 2.0 – Three Bedroom Bonus 1.0 – Green Building Bonus		
Total Above-Grade Gross Floor Area	279,232 SF		
Stories/Height	22 stories / 253 feet		
Number of Dwelling Units	241		
Amount of Commercial Lease Space	35,668 SF		
Housing Units Summary	#	Size Range	Average Size
Total	241		
Studios	60	623 – 663 SF	639 SF
1 Bedroom	119	766 – 1,037 SF	828 SF
2 Bedroom	30	1,203 – 1,269 SF	1,208 SF
3 Bedroom	32	1,322 – 1,951 SF	1,430 SF
Number of Buildings over 45 Years Old	1 - Pacific Telephone and Telegraph Company Garage, HRB Site No. 1355 (constructed in 1932)		
Inclusionary Affordable Housing Compliance	Payment of Inclusionary Housing Fee (\$12.73/SF) Estimated Payment: \$2,786,915 (\$11,564/DU)		
Parking Spaces	(Required/Proposed)		
Automobile Parking	36 / 243 (0 spaces/DU; 1 space/1,000 SF commercial)		
Motorcycle Parking	23 / 28 (1 space/10 residential spaces provided; 1 space/20 commercial spaces required)		
Bicycle Parking	52 / 53 (1 space/5 DU;1 short-term and 1 long-term space/20 commercial spaces)		
Common Indoor Space	Required: 500 SF Proposed: 5,744 SF		
Common Outdoor Open Space	Required: 3,759 SF Proposed: 6,134 SF		
Private Open Space (Balconies & Decks)	Required: 121 DU (50% of DU) Proposed: 136 DU (56% of DU)		
Pet Open Space	Required: 200 SF Proposed: 440 SF		
Residential Storage Areas	Required: 240 cubic feet for 100% of DU (241 DU) Proposed: 240 cubic feet for 38% of DU (91 DU)		
Assessor's Parcel Nos.	535-136-01-00		
Sustainability	LEED Silver certification		

	City of San Diego Development Services 1222 First Ave., MS 302 San Diego, CA 92101 (619) 446-5000	<h1 style="margin: 0;">Ownership Disclosure Statement</h1>	FORM DS-318 October 2017
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Approval Type: Check appropriate box for type of approval(s) requested: ☐ Neighborhood Use Permit ☐ Coastal Development Permit
☐ Neighborhood Development Permit ☒ Site Development Permit ☐ Planned Development Permit ☐ Conditional Use Permit ☐ Variance
☐ Tentative Map ☐ Vesting Tentative Map ☐ Map Waiver ☐ Land Use Plan Amendment • ☐ Other _____

Project Title: 9G Tower **Project No. For City Use Only:** _____

Project Address: 659 9th Ave.
 San Diego, CA 92101

Specify Form of Ownership/Legal Status (please check):

☐ Corporation ☒ Limited Liability -or- ☐ General - What State? CA Corporate Identification No. 200928510123
☐ Partnership ☐ Individual

By signing the Ownership Disclosure Statement, the owner(s) acknowledge that an application for a permit, map or other matter will be filed with the City of San Diego on the subject property with the intent to record an encumbrance against the property. Please list below the owner(s), applicant(s), and other financially interested persons of the above referenced property. A financially interested party includes any individual, firm, co-partnership, joint venture, association, social club, fraternal organization, corporation, estate, trust, receiver or syndicate with a financial interest in the application. If the applicant includes a corporation or partnership, include the names, titles, addresses of all individuals owning more than 10% of the shares. If a publicly-owned corporation, include the names, titles, and addresses of the corporate officers. (A separate page may be attached if necessary.) If any person is a nonprofit organization or a trust, list the names and addresses of ANY person serving as an officer or director of the nonprofit organization or as trustee or beneficiary of the nonprofit organization. A signature is required of at least one of the property owners. Attach additional pages if needed. Note: The applicant is responsible for notifying the Project Manager of any changes in ownership during the time the application is being processed or considered. Changes in ownership are to be given to the Project Manager at least thirty days prior to any public hearing on the subject property. Failure to provide accurate and current ownership information could result in a delay in the hearing process.

Property Owner

Name of Individual: Elizabeth Lee, Manager of 4 Lee Tenth, LLC ☒ Owner ☐ Tenant/Lessee ☐ Successor Agency
 Street Address: 660 Tenth Ave.
 City: San Diego State: CA Zip: 92101
 Phone No.: 619-869-1634 Fax No.: N/A Email: korliza@gmail.com
 Signature: Elizabeth Lee Date: 10/8/19
 Additional pages Attached: ☐ Yes ☒ No

Applicant

Name of Individual: Jason R. Wood, Principal of Cisterra Partners, LLC ☐ Owner ☒ Tenant/Lessee ☐ Successor Agency
 Street Address: 3580 Carmel Mountain Rd., Ste 460
 City: San Diego State: CA Zip: 92130
 Phone No.: 619-615-0200 Fax No.: N/A Email: jwood@cistarra.com
 Signature: [Signature] Date: 10.4.19
 Additional pages Attached: ☐ Yes ☒ No

Other Financially Interested Persons

Name of Individual: _____ ☐ Owner ☐ Tenant/Lessee ☐ Successor Agency
 Street Address: _____
 City: _____ State: _____ Zip: _____
 Phone No.: _____ Fax No.: _____ Email: _____
 Signature: _____ Date: _____
 Additional pages Attached: ☐ Yes ☐ No

PLANNING COMMISSION
RESOLUTION NO. _____

SITE DEVELOPMENT PERMIT/NEIGHBORHOOD DEVELOPMENT PERMIT/CONDITIONAL USE PERMIT
9G - PROJECT NO. 649856

WHEREAS, 4 LEE TENTH, LLC, Owner, and CISTERRA PARTNERS, LLC, Permittee, filed an application with the City of San Diego for a permit to construct a 22-story, 253-foot tall mixed-use development comprised of 241 dwelling units (DU), 35,668 square feet (SF) of retail space, and 243 parking spaces for a project known as 9G ("Project"), (as described in and by reference to the approved plans on file at the City of San Diego and corresponding conditions of approval for the associated Permit No. 649856), on a 25,061 square-foot site;

WHEREAS, the Project site is located at 659 Ninth Avenue in the Employment/Residential Mixed-Use (ER) land use district of the Centre City Planned District Ordinance (CCPDO) and the East Village neighborhood of the Downtown Community Plan (DCP) area;

WHEREAS, the project site is legally described as Lots A, B, C, K, and L in Block 83 OF Horton's Addition in the City of San Diego, County of San Diego, State of California, according to map thereof on file in the Office of the County Recorder of San Diego County;

WHEREAS, development within the Downtown Community Planning area is covered under the following documents, all referred to as the "Downtown FEIR": Final Environmental Impact Report (FEIR) for the San Diego Downtown Community Plan, Centre City Planned District Ordinance, and 10th Amendment to the Centre City Redevelopment Plan, certified by the former Redevelopment Agency ("Former Agency") and the City Council on March 14, 2006 (Resolutions R-04001 and R-301265, respectively); subsequent addenda to the FEIR certified by the Former Agency on August 3, 2007 (Former Agency Resolution R-04193), April 21, 2010 (Former Agency Resolution R-04510), and August 3, 2010 (Former Agency Resolution R-04544), and certified by the City Council on February 12, 2014 (City Council Resolution R-308724) and July 14, 2014 (City Council Resolution R-309115);

and, the Final Supplemental Environmental Impact Report for the Downtown San Diego Mobility Plan certified by the City Council on June 21, 2016 (Resolution R-310561). Development within the Downtown Community Planning area is also covered under the following documents, all referred to as the "CAP FEIR": FEIR for the City of San Diego Climate Action Plan (CAP), certified by the City Council on December 15, 2015 (City Council Resolution R-310176), and the Addendum to the CAP, adopted by the City Council on July 12, 2016 (City Council Resolution R-310595). The Downtown FEIR and CAP FEIR are both "Program EIRs" prepared in compliance with California Environmental Quality Act (CEQA) Guidelines Section 15168. The information contained in the Downtown FEIR and the CAP FEIR reflects the independent judgement of the City of San Diego as the Lead Agency and has been reviewed and considered by the decision maker before approving the project. Consistent with best practices suggested by CEQA Guidelines Section 15168, a Downtown 15168 Consistency Evaluation ("Evaluation") has been completed for the project. The Evaluation concluded that the environmental impacts of the project were adequately addressed in the Downtown FEIR and CAP FEIR; that the project is within the scope of the development program described in the Downtown FEIR and CAP FEIR and is adequately described within both documents for the purposes of CEQA; and, that none of the conditions listed in CEQA Guidelines Section 15162 exist. Therefore, no further environmental review is required under CEQA;

WHEREAS, on August 20, 2020, the Planning Commission of the City of San Diego considered SDP/NDP/CUP No. 649856 pursuant to the Land Development Code (LDC) of the City of San Diego; and

BE IT RESOLVED by the Planning Commission of the City of San Diego, that it adopts the following findings with respect to SDP/NDP/CUP No. 649856:

A. SITE DEVELOPMENT PERMIT [San Diego Municipal Code (SDMC) Section 126.0505]

1. Findings for all Site Development Permits:

a. The proposed development will not adversely affect the applicable land use plan.

The applicable land use plans for this site are the DCP and the CCPDO, which is located in the Northwest sub-district of the East Village neighborhood of the DCP and the Employment/Residential Mixed-Use (ER) district of the CCPDO. The Project proposes 241 residential DU and 35,668 SF of commercial/retail space. Both multi-family residential and retail land uses are permitted in the ER District, which provides a transition between the Core District and residential neighborhoods and allows a variety of uses that includes offices, hotels, educational, and medical facilities. The Project proposes a Floor Area Ratio (FAR) of 11.14, where the Base Maximum identified in the CCPDO is 6.0 and can be increased up to 20.0 with the CCPDO FAR Bonus Programs, which the Project is utilizing by providing three-bedroom DUs for an additional 2.0 FAR, achieving LEED Silver certification for 1.0 FAR, and purchasing 1.14 FAR. With a FAR of 11.14, the Project complies with the allowable FAR established in the CCPDO. The development regulations of the CCPDO regulate the urban form of the Project. Five deviations from the development regulations are proposed by the Project, including reducing the storage requirement, increasing the tower lot coverage, increasing the tower floor plate dimension, reducing the 10th Avenue transparency requirement, and reducing a Ninth Avenue driveway width. Deviations from the SDMC are permitted with approval of a NDP pursuant to Section 126.0404 and the applicable findings therein. The NDP findings for the deviations can be found in Section B below. With approval of the NDP for the deviations, the Project is consistent with the CCPDO.

The DCP states that the Northwest sub-district should develop as the most intensive residential area in concert with its central location, transit access, and available redevelopment sites (DCP, 6.5-G-6) to contribute to an ultimate East Village population target of 46,000. The Project proposes a 241-DU residential tower with a total FAR of 11.14 on a 25,061 SF site. This is a dense project as the residential DU totals roughly 422 DU per acre on this approximately half-acre site, where over 35,000 SF of commercial space and 243 parking spaces are also provided. The mix of uses is also consistent with the DCP's goal of fostering a rich mix of uses in all neighborhoods (DCP, 3.5-G-2). The placement of the approximately 35,000 SF commercial space, anticipated to contain a large-scale, general merchandise retailer, which will become an attraction itself, is appropriate for its location due to its close proximity to Downtown attractions like the Gaslamp Quarter and Petco Park, as well as transit access at the Park & Market Trolley Station and Market Street bus stops, which is consistent with the DCP goal for the Northwest sub-district to reinforce its close proximity to Downtown destinations (DCP, 6.5-G-8).

With 241 DUs on a 25,061 SF site, the Project is providing a high-intensity land use that is consistent with a dense, urban, Downtown environment and the goals of the DCP for high overall intensities that use land efficiently in order to meet employment and population targets (DCP, 3.2-G-5), while utilizing an existing on-site historical resource. Historic Preservation is addressed in Chapter 9 of the DCP and states that locally designated resources are to be retained on-site whenever possible and that

"Partial retention, relocation or demolition of a resource shall only be permitted through applicable City procedures," that are outlined in SDMC Section 143.02 "Historical Resources Regulations." Substantial alteration of a designated historical resource means demolition, destruction, relocation, new construction, or alteration activities that would impair the significance of a historical resource. The Applicant proposes to demolish the interior of the historical resource, retain the existing historic façade, and construct a 21-story, 253-foot tall mixed-use development above it, comprised of 241 dwelling units. The interior of the historic structure on the ground floor, as well as one new basement level, will contain a combined approximately 35,668 SF of commercial retail space. Directly above the historical resource is proposed to be five parking garage levels. The redevelopment of the site cannot be determined to be consistent with the Secretary of the Interior's Standards for Historical Properties due to the loss of original materials on the historic structure, including the removal of the 1946 addition, and the massing, size, scale and proportion of the proposed tower. Therefore, the proposed redevelopment and reuse of the Resource is, by definition, a substantial alteration requiring an SDP, consistent with SDMC Section 143.0251. The Planning Commission must make all the findings in SDMC Sections 126.0505(a) and 126.0505(i) before construction can occur. Therefore, the processing of this SDP complies with and will not adversely affect the applicable land use plan.

The goals and policies of the DCP generally stipulate that historical resources should be retained on-site and integrated into the Downtown fabric in a way that contributes to the achievement of the goals for significant development and population intensification (DCP, 9.2-G-1). The proposed development will serve to add 241 DU to a currently underutilized site, providing the desired density, while at the same time, retaining and restoring a historical resource on-site, consistent with the DCP goals of protecting historical resources to communicate Downtown's heritage (DCP, 9.1-G-1). Slight modifications are proposed to the historical resource itself to accommodate the proposed development, including demolition of the 1946 addition and a portion of the façade on Tenth Avenue for a one-way driveway, the addition of doors on two frontages, and storefront infill on existing garage openings; however, overall these modifications are very limited in order to maintain as much of the historic integrity of the resource as possible. The DCP encourages the rehabilitation and reuse of historical resources (DCP, 9.1-G-2) and the incorporation of elements of historical buildings into new projects (DCP, 9.2-P-1), as is proposed with the integration and restoration of the historical façade into the Project, which will contain the 35,000 SF commercial space. With compliance with the CCPDO and advancement of the goals of the DCP, the proposed development will not adversely affect the applicable land use plan.

b. The proposed development will not be detrimental to the public health, safety, and welfare.

The granting of the SDP and approval of the Project will not have a detrimental impact to the public health, safety, and general welfare. The Project is consistent with the plans for the East Village neighborhood as envisioned in the DCP by guiding the

Northwest quadrant's evolution as an intense residential area by utilizing its central location and making an efficient use of a site with a historical resource. The Project will adaptively reuse the currently underutilized historical resource with an active commercial land use that, along with the addition of new residents, will contribute to the neighborhood's vitality. Therefore, because the Project will improve the existing condition of the site and neighborhood by advancing the vision of the DCP, the proposed development will not be detrimental to the public health, safety, and welfare.

c. The proposed development will comply with the regulations of the Land Development Code including any allowable deviations pursuant to the Land Development Code.

The requested historical resource deviation will allow for the substantial alteration of a designated historical resource, the Pacific Telephone and Telegraph Company Garage (HRB Site No. 1355) by constructing 21 levels above it. The DCP encourages both the retention and reuse of historical resources on-site, as well as the maximization of intensities in the Downtown area. Per the CCPDO, the Base Maximum FAR of the site is 6.0, which the Project is exceeding through the use of CCPDO FAR Bonus programs, while at the same time retaining and restoring the historical resource. With approval of the SDP to allow the new construction above the historical resource and other modifications, including demolition of the 1946 addition, including the SDP Supplemental Findings included in Section 2 below, as well as a NDP for deviations from the development regulations of the CCPDO (the findings for which are included in Section B of this resolution), the proposed development will comply with all regulations of the LDC.

2. Supplemental Findings – Historical Resources deviation for Substantial Alteration of a Designated Historical Resource or Within a Historical District:

a. There are no feasible measures, including a less environmentally damaging alternative, that can further minimize the potential adverse effects on the designated historical resource or historical district.

The historical resource was designated because it was deemed to be a special element of the City's economic development as an example of America's emerging automobile industry in the first half of the 20th century. The historical resource was also designated for its retention of character defining features of the Art Deco style, including its smooth wall surfaces, flat roof, vertical projections above the roofline, fluted pilasters, geometric detailing, and metal windows.

The proposed Project (Base Project) retains the existing façade of the historical resource, adds a basement level below and 21 levels above the historical resource, including five levels of parking, with no setback from the existing facade. Modifications to the existing façade that cannot be determined to be consistent with the Secretary of the Interior's Standards for Treatment of Historic Properties include removal of the existing roof, the slab on grade, the 1946 addition, and a portion of the façade on Tenth Avenue for a driveway. In addition, doors will be added on two

facades and storefront infill will be added at existing garage openings. The construction of the 21 levels above the historical resource also cannot be determined to be consistent with the Secretary of Interior's Standards due to the proposed massing, size, scale and proportion of the tower.

The Applicant retained the London Moeder Advisors (LMA) to conduct an economic analysis to evaluate five alternatives for potential feasible measures to lessen the impact of the Project on the historical resource. The most important variables studied in the alternatives analysis were 1) the amount of setback of the tower from the façade of the historical resource and 2) height because these components comprise the primary adverse impacts to the historical resource. All alternatives retained either the whole historical resource or its original facades. The following five alternatives were evaluated for their respective Internal Rate of Return (IRR) and Yield of Cost (YOC) versus that of the Base Project, which is summarized in the table below:

Alternative	Historic Structure	Additions	Setbacks from Existing Facade	Dwelling Units	Retail Square Footage	Parking Spaces
Base Project	Retain existing facade	1 basement level, 5 parking levels, 16 residential levels	None	241	34,000	276
1	Rehabilitate existing structure	None	N/A	0	22,000	0
2	Retain existing facade	1 basement level, 1 parking level (open air)	N/A	0	34,000	34
3	Retain existing facade	1 basement level, 5 parking levels, 10 residential levels	15' on each street	155	34,000	189
3B	Retain existing facade	1 basement level, 5 parking levels, 16 residential levels	15' on each street	227	34,000	189
4	Retain existing facade	5 subterranean parking levels, 1 extra retail level, 11 residential levels	5' on level above existing façade, 15' for tower	168	34,000	202

The LMA analysis concluded that the three alternatives that exhibit tower setbacks above the historical resource, and thus had less impact on the historical resource, are not economically feasible due to their failure to meet the minimum IRR and YOC needed to achieve project financing (see Finding c below). The LMA analysis concluded that the Base Project, which provides no setbacks for the 21-story tower above the historical resource, was the option that best balanced economic feasibility with preservation of the historical resource. Therefore, there are no feasible measures, including a less environmentally damaging alternative, other than the Base Project that can further minimize the potential adverse effects on the designated historical resource or historical district.

b. The deviation is the minimum necessary to afford relief and accommodate the development and all feasible measures to mitigate for the loss of any portion of the historical resource have been provided by the applicant.

While the addition of 21 stories of new construction containing 241 DU and 243 parking spaces immediately above the historical resource will result in substantial alterations to the historical resource, the proposed Project will take steps to mitigate this impact. Historical resource mitigation measures have been developed and adopted within the Downtown Final Environmental Impact Report (Downtown FEIR), with which the Project has been evaluated against and deemed consistent. The Mitigation Monitoring and Reporting Program (MMRP) for the Downtown FEIR requires the implementation of a documentation program submitted to City Historic Resources Division staff for review and approval, a pre-construction meeting, implementation of the Treatment Plan and monitoring to ensure appropriate execution of the plan. Additionally, the historical resource was documented through the measured drawings and photography consistent with the Historical American Building Survey (HABS). A copy of this documentation will be archived with the City and other depositories as outlined in the MMRP.

The proposed Project rehabilitates the existing façade and minimizes the need for modifications to the historic façade, utilizing existing openings and maintaining the character-defining architectural features called out in the designation, with the exception of the removal of the 1946 addition along Ninth Avenue and the alteration to the 1932 façade on 10th Avenue for the driveway. These modifications are a result of the need for vehicular access for the site and the constraints of the presence of the original 1932 structure and lot size, limiting the available locations for driveways. Each driveway is one-way and the ingress driveway on Tenth Avenue is the minimum width allowed per City standards. The Project proposes a deviation for the on-way egress driveway on Ninth Avenue of 12 feet from the City standard of 14 feet, as described in subsequent sections below.

The construction of the 21 levels above the historical resource contributes to the loss of historic context; however, the economic analysis concluded that the Base Project was the only economically feasible alternative. In the Base Project, while no setbacks are provided between the existing façade and the new construction above, the

design of the new building incorporates a horizontal distinction between the historic façade and new construction through the use of materials and color. The first 4'-10" to 7'-11" (depending on height of parapet) of new wall above the historical resource is behind the face of the existing façade (one foot) and is comprised of dark gray metal panels intended to create negative space to help the historical resource appear as an independent component that is separate from the new construction above comprised of angled metal panels and spandrel glazing. The lack of setbacks allows for the maximization of buildable floor area on the lot, which allows for the number of DU and parking spaces to make the Project economically feasible project, as demonstrated in the LMA analysis, while at the same time retaining the historical resource on-site consistent with the DCP Goals and Policies. Therefore, the Project is designed with the minimum necessary deviation to afford relief to and accommodate the project and reasonable, feasible measures to mitigate the impact to the historical resource are proposed.

- c. The denial of the proposed development would result in economic hardship to the owner. For purposes of this finding, "economic hardship" means there is no reasonable beneficial use of the property and it is not feasible to derive a reasonable economic return from the property.**

The LMA analysis used the IRR and YOC as measures to determine the economic feasibility of each alternative. As stated in the report, the typical IRR for rental housing project ranges from 13-15%; anything less would struggle to attract investors and achieve project financing. The YOC spread over existing cap rates is required to be 1.5% for redevelopment; therefore, if cap rates are approximately 4% for residential projects and 6% for retail projects, the targeted YOC is 5.5% and 7.5%, respectively, for a project to be economically feasible and to qualify for project financing. The table below summarizes the conclusions of the LMA analysis for each alternative:

Alternative	IRR Min: 13-15%	YOC Max: 5.5-7.5%
Base	16.5%	5.6%
1	9.2%	3.9%
2	-4.2%	3.4%
3	7.2%	4.7%
3B	10.2%	4.7%
4	-6%	3.9%

The LMA Analysis concluded that, in order to retain the historical resource, only the Base Project is economically feasible, as the five alternatives fail to meet the minimum IRRs and YOCs to successfully attract investors and achieve project financing. Therefore, denial of the Project would result in economic hardship to the owner because there is no reasonable beneficial use of the property that does not require complete redevelopment in order to derive a reasonable economic return from the property.

B. NEIGHBORHOOD DEVELOPMENT PERMIT [SDMC Section 126.0404]**1. Findings for all Neighborhood Development Permits:****a. The proposed development will not adversely affect the applicable land use plan.**

The proposed Project is consistent with the objectives of the DCP, CCPDO, and the Downtown Design Guidelines (DDG). The Project advances the goals and policies of the DCP by contributing to the orderly growth of the East Village neighborhood with the construction of a mixed-use development with both residential and commercial uses, which adds to the balance of uses in Downtown and advances the goal of the making the Northwest sub-district of the East Village neighborhood the most intense residential area (DCP, 6.5-G-6). Specifically, the Project is adding 241 residential DU and approximately 35,668 SF of commercial space, providing a neighborhood amenity for surrounding residents and taking advantage of the site's central location near transit and other Downtown attractions, per the DCP (DCP, 6.5-G-6 and 6.5-G-8). With the 25,061 SF lot size and the high-intensity of the site with 241 DU, 35,000 SF of commercial space, 243 parking spaces, and a historical resource, the Project provides the desired diversity and mix of uses, while also contributing to high overall intensities in Downtown and using land efficiently to advance the population and employment targets of the DCP (DCP, 3.2-G-5).

The Project is designed at a scale that is similar to other existing buildings in the nearby vicinity, such as the 23-story Strata and 33-story The Mark to the south, and the newly constructed 34-story The Merian one block to the east. In the CCPDO, residential and commercial/retail uses are permitted land uses in the ER land use district. Per the DDG, the Project utilizes architectural articulations, provides street level activation, and uses high-quality materials. The building's massing is modulated through the use of glazing, concrete framing elements, rooftop off-set planes, and varying materials around the Project, which provide visual interest and architectural variation. At the street-level, the existing historic Pacific Telephone and Telegraph Company Garage (HRB Site No. 1355) is being retained, restored, and reused on the ground floor with a commercial/retail use that will serve as a neighborhood amenity and an active use that engages pedestrians. Standing in contrast to the historic façade at the ground floor, the tower of the Project makes use of a variety of high-quality materials to help break up the massing.

Five deviations from the development regulations are proposed by the Project, including reducing the storage requirement, increasing the tower lot coverage, increasing the tower floor plate dimension, reducing the 10th Avenue transparency requirement, and reducing a Ninth Avenue driveway width. As an in-fill/sustainable project in a mixed-use zone (See Finding B.2 below), deviations from the SDMC are permitted with approval a NDP pursuant to Section 126.0402(p). With approval of the NDP, the Project is consistent with the CCPDO. As a consolidated Project per Section 112.0103, the Planning Commission must make all the Findings in SDMC Sections 126.0404(a) and 126.0404(f) before construction can occur. Therefore, the processing of this NDP complies with and will not adversely affect the applicable land use plan.

Because the Project provides design solutions that are consistent with the DDGs and compatible with the neighborhood, advances the goals of the DCP, and provides a land use that is compatible with the CCPDO, the proposed development will not adversely affect any applicable land use plans.

b. The proposed development will not be detrimental to the public health, safety, and welfare.

The granting of the deviations and the approval of the Project will not have a detrimental impact to the public health, safety, and general welfare. The Project is consistent with the plans for the East Village neighborhood as envisioned in the DCP by advancing the development of the Northwest sub-district of the East Village neighborhood as the most intensive residential area and will contribute to its vitality by adding 241 residential units to the neighborhood and providing an attractive and engaging streetscape that includes ground level commercial lease space in a historical resource that adds character to the neighborhood. Because the components of this Project increase activity in the neighborhood, improve the aesthetics of the site, and advance the vision of the DCP, the proposed development will not be detrimental to the public health, safety and welfare.

c. The proposed development will comply with the applicable regulations of the Land Development Code including any allowable deviations pursuant to the Land Development Code.

With approval of the five requested deviations, the proposed development complies with all applicable regulations of the LDC, including the CCPDO and other applicable sections of the SDMC. The requested deviations serve to maximize the density and use of the site, which is constrained by the presence of the existing historical resource being retained and restored on-site and the lot size. For example, the reduction of the personal storage requirement and increase in tower lot coverage and tower floor plate serve to increase the buildable area and therefore allow for the highest and best use of the site by providing more housing units than would otherwise be feasible on the site. The maximization of the property with the provision of housing opportunities advances the population goals of the DCP. With the allowed deviations, the Project complies with the LDC and CCPDO.

2. Supplemental Findings – Affordable Housing, In-Fill Projects, or Sustainable Buildings Deviation

a. The development will materially assist in accomplishing the goal of providing affordable housing, in-fill projects, or sustainable building opportunities.

The Project is an in-fill project because it proposes the construction of a mixed-use development within a Transit Priority Area, per SDMC Section 143.0915(b). The Project is a high-intensity development, maximizing the use of the 25,061 SF lot to provide 241 residential DU and approximately 35,000 SF of commercial/retail space while at the same time retaining a historical resource on a 25,061 SF site.

Additionally, the Project proposes LEED Silver certification to obtain a 1.0 FAR Bonus. The CCPDO allows for a 1.0 FAR Bonus for either LEED Silver certification or California Green Building Standards Code (CALGreen) Tier 2 performance. SDMC Section 143.0915(c) identifies a project that incorporates the voluntary Tier 2 measures of CALGreen as a sustainable building. As an equivalent threshold to earn a 1.0 FAR Bonus, the Project is proposing LEED Silver certification and is therefore a sustainable building, consistent with SDMC Section 143.0915(c).

b. Any proposed deviations are appropriate for the proposed location.

The requested deviations to the CCPDO development regulations will allow for (1) a reduction of the required personal storage requirement from 100% of DU (241 DU) to 38% of DU (91 DU), (2) a reduction of the minimum width required for the on-way driveway on Ninth Avenue from 14 feet to 12 feet, (3) an increase of the allowable maximum tower lot coverage from 50% to 60% of the lot, (4) an increase of the allowable maximum east-west tower floor plate dimension from 130 feet to 150 feet, and (5) a reduction in the minimum required ground level transparency of the Tenth Avenue building façade from 60% to 40% to accommodate the existing historical resource. The site is constrained by the retention of the existing historical resource on-site and relatively small lot size in the context of achieving the desired density in a financially feasible residential development. With these constraints, buildable space is limited so three deviations are requested to allow for the highest use of the site to maximize the FAR and number of units in the Project to advance the population goals of the DCP by providing as many residential units as feasible in the Project. The remaining two deviations are requested to accommodate the retention of the existing historical resource on-site and minimize modifications to advance the historic preservation goals of the DCP by protecting, incorporating, and reusing historical resources in projects.

The deviations that serve to increase the number of DUs in the Project and maximize the use of the site not only advance the goals of the DCP by advancing population and housing targets and contributing to the evolution of the Northwest sub-district of the East Village neighborhood into the highest-intensity residential area, but are also deemed necessary by the Applicant to make the Project financially feasible and secure the appropriate project financing. Cost is also a limiting factor for the Applicant that prevents the Project height from exceeding 240 feet, so as to not trigger a more costly construction type. As is common in high-rise towers, a dual-frame structural system is required above 240 feet to strengthen the performance of the building. The Applicant estimates that exceeding 240 feet in the Project would add approximately \$4.5 million to the Project. So the deviations not only provide housing that advances the DCP goals, but also provide financial feasibility for the Project by reducing costs, while maximizing buildable area.

The reduction of the personal storage requirement has a significant impact. The CCPDO requires personal storage units that are at least 240 cubic feet in size for each DU, which in this case is 241 DU. If the footprint of each personal storage unit is approximately 38.6 SF (estimated 6.22-foot height, length, and width), 241 personal

storage units would equal over 9,000 SF of space, which, after accounting for circulation, core, access, etc., would likely use a full floor (typical tower square footage in the Project is 16,473 SF). A typical residential floor contains 16 DU, so it is reasonable to conclude the number of DU would be decreased if the full personal storage requirement was implemented.

The increase in the maximum tower lot coverage and tower floor plate dimensions maximizes the buildable area of the site in order to provide more DU. The tower lot coverage deviation increases the allowable coverage from 50% of the site to 66%, resulting in an extra 4,000 SF of buildable area per level. The tower floor plate dimension deviation increases the east-west tower dimension from 130 feet to 150 feet. The Applicant estimates that full compliance with the lot coverage and tower dimensions would require six additional stories to retain the same number of DU and add approximately \$8,000,000 to the Project costs, making the project financially infeasible. A massing study was produced that analyzed the impacts of these deviations on potential future development surrounding the site in the context of the development regulations of the CCPDO. The massing study provided two scenarios for the development of the remaining southern portion of the subject block: 1) one tower development on the entire southern portion of the block, and 2) the southern portion of the block divided into two with two independent tower developments. The increase in tower width is not substantial, given that the CCPDO allows 200-foot wide towers in a north/south direction similar to the recently constructed Merian project two blocks to the east. The massing studies demonstrate that adjacent properties could fully develop under the CCPDO development regulations, although the overall block coverage would exceed the desired maximum lot coverage of 50%.

The Project makes efforts to address the increased mass of the building caused by the increased tower lot coverage and floor plate dimension deviations by adhering to the DDG on building tower design. The tower is designed with a majority of the facades composed of glazing (DDG, 4.5.4.F) and incorporates balconies and architectural projections to provide desirable elevation and composition variety (DDG, 4.5.4.E). Overall, the tower expresses variety, avoids monotony, and distinguishes different building volumes (DDG, 4.4.1.C) by employing multiple glazing colors, different materials, and concrete framing components that span the height of the tower. Considering the minimal impact of the massing on neighboring properties, the enhanced design, and the advancement of the population and goals of the DCP, these deviations are appropriate for the proposed location.

The remaining two deviations regarding the driveway width and transparency requirement are related to the retention of the existing historical resource on-site. While the preceding deviations advance the population and intensity goals of the DCP, these deviations advance the historic preservation goals of the DCP to protect historical resources (9.1-G-1), rehabilitate and reuse them (9.1-G-2), and incorporate them into new projects (9.2-P-1), all while achieving policies for significant development and population intensification (9.2-G-1).

The Project is requesting a deviation from SDMC Section 142.0560(j) for a new one-way driveway for vehicles to egress from the site onto Ninth Avenue. The minimum standard driveway curb-cut width for a one-way non-residential curb cut and driveway is 14 feet wide and the Project is proposing a 12-foot driveway curb cut. The proposed driveway width is the result of the limited frontage available on Ninth Avenue without requiring modifications to the original 1932 façade of the historical resource. As proposed, both the loading bay driveway and adjacent exit-only driveway for parking garage on Ninth Avenue do not impact the façade of the historical structure. The Project parking circulation was designed with a driveway entrance on Tenth Avenue and a separate exit driveway on Ninth Avenue to limit modifications to the historic façade. In addition, the proposed 12-foot wide driveway curb cut design would maintain the driveway apron flare within the Project boundary with adequate separation between the curb cut opening and adjacent property line. The Project also provides visibility areas at the Project driveway per SDMC Diagram 113-02SS and proposes convex mirrors located within the site to enhance the visibility of pedestrians, cyclists, and other vehicles. A sight distance analysis was performed that confirmed adequate lines of sight for vehicles, bicycles, and pedestrians on Ninth Avenue. The reduced driveway width also serves to minimize breaks in the right-of-way for vehicles, which improves the pedestrian and streetscape environment in Downtown consistent with the Section 3.2.1 and 4.5.11 of the DDG and Section 5.4 of the DCP. Therefore, this deviation is appropriate for the proposed location.

The deviation to reduce the transparency requirement is a result of preserving as much of the original character and architecture of the existing historical structure. The transparency requirement of the CCPDO requires at least 60% of each building façade between three and 12 feet above the sidewalk to be transparent and allow views of the indoor spaces to create active storefronts and an engaging pedestrian experience. The historical resource, while a defining characteristic of the Project, is also a site constraint, limiting the design in many ways, including the placement of utility services. With the existing condition of the Tenth Avenue façade of the historical resource, originally constructed as a commercial warehouse and the goal of preserving as much of the historical resource façade as possible, after the placement of the driveways, utility services, and commercial space-serving stair and elevator, the resulting transparency is approximately 40% of the façade. Given the importance of retaining and restoring the historical resource on-site the efforts to design around it, the impact of the reduced façade transparency on one frontage on the overall Project is minimal and therefore appropriate for the proposed location.

C. CONDITIONAL USE PERMIT [SDMC Section 126.0305]

1. Findings for all Conditional Use Permits:

- a. The proposed development will not adversely affect the applicable land use plan.**

The Project is located in the ER land use district of the CCPDO. Per the CCPDO, establishments with off-site alcoholic beverages sales are permitted within the ER district with approval of a CUP. The proposed commercial/retail space is located on the ground and basement levels of the Project and will be a supplementary component of a large-scale general merchandise retailer. The proposed use will not adversely affect the applicable land use plan, as it adheres to the CCPDO, is in a land use district that accommodates a diverse array of uses, including commercial/retail businesses, and is consistent with the DCP goal of providing for an overall balance of uses and a full compendium of amenities and services (3.1-G-2). Therefore, the proposed development will not adversely affect the applicable land use plan.

b. The proposed development will not be detrimental to the public health, safety, and welfare.

The proposed off-site alcohol sales will not be detrimental to the public health, safety and welfare of the community as proposed and when operated with the recommended conditions of approval. The off-site sales of alcoholic beverages will be a relatively small component of a large-scale general merchandise retailer. The display area and footprint of the space will be minimal when compared to the overall store and therefore pose negligible detrimental effects. Additionally, conditions are proposed by the San Diego Police Department (SDPD) that aim to address potential adverse impacts of the use on the health, safety, and welfare of the neighborhood. These conditions prohibit loitering, litter, and graffiti, and requires video surveillance and employee training to prevent theft and sales of alcohol to minors. When in operating in compliance with the SDPD's recommended conditions of approval, the proposed development will not be detrimental to the public health, safety, and welfare.

c. The proposed development will comply with the regulations of the LDC including any allowable deviations pursuant to the LDC.

The Project will comply with the regulations of the SDMC for the off-site sale of alcoholic beverages in the proposed ground and basement level commercial/retail establishment. The proposed use will be required to comply with all applicable regulations of the CCPDO and conditions of the approved permit. Further, the proposed use will be required to obtain all necessary governmental approvals and comply with the applicable requirements of the State Department of Alcoholic Beverage Control and the SDPD.

d. The proposed use is appropriate at the proposed location.

The Project is located in the ER land use district and is within the East Village neighborhood of Downtown. A variety of uses are permitted in the ER land use district, including office, residential, hotel, research and development, educational, and medical facilities. The large-scale general merchandise retailer is appropriate at the proposed location along G Street, where it is in close proximity to several high-rise residential developments. The Northwest sub-district of the East Village

neighborhood is envisioned in the DCP to become the highest-intensity residential area, so a general merchandise store with a variety of products, including alcoholic beverages, will serve as a neighborhood amenity for residents, as well as an attraction to visitors in nearby Gaslamp Quarter and ballpark stadium. Therefore, the proposed use is appropriate at the proposed location.

The above findings are supported by the minutes, maps and exhibits, all of which are incorporated herein by this reference.

BE IT FURTHER RESOLVED that, based on the findings hereinbefore adopted by the Planning Commission, SDP/NDP/CUP No. 649856 is hereby GRANTED by the Planning Commission to the referenced Owner/Permittee, in the form, exhibits, terms and conditions as set forth in Permit No. 649856, a copy of which is attached hereto and made a part hereof.

James Alexander
Program Manager
Urban Division

Adopted on: August 20, 2020

IO#: 24008439

Attachments: SDP/NDP/CUP No. 649856

RECORDING REQUESTED BY
CITY OF SAN DIEGO
URBAN DIVISION
MAIL STATION 501

WHEN RECORDED MAIL TO
PROJECT MANAGEMENT
PERMIT CLERK
MAIL STATION 501

INTERNAL ORDER NUMBER: 24008439 SPACE ABOVE THIS LINE FOR RECORDER'S USE

SITE DEVELOPMENT PERMIT/NEIGHBORHOOD DEVELOPMENT PERMIT/CONDITIONAL USE PERMIT
9G - PROJECT NO. 649856
PLANNING COMMISSION

This Site Development Permit/Neighborhood Development Permit/Conditional Use Permit is granted by the Planning Commission of the City of San Diego ("City") to 4 Lee Tenth, LLC, Owner, and Cisterra Partners, LLC, Permittee, pursuant to San Diego Municipal Code (SDMC) Section 126.0505, 126.0404, 125.0440, and 126.0305, to allow 1) the substantial alteration of a historical resource, 2) five deviations from the development regulations of the SDMC, and 3) off-site alcoholic beverage sales for 9G ("Project"). The approximately 25,000 square-foot (SF) site is located at 659 Ninth Avenue (south side of G street between Ninth and Tenth avenues) in the East Village neighborhood of the Downtown Community Plan (DCP) area and within the Centre City Planned District. The project site is legally described as Lots A, B, C, K, and L in Block 83 of Horton's Addition in the City of San Diego, County of San Diego, State of California, according to map thereof on file in the Office of the County Recorder of San Diego County.

Subject to the terms and conditions set forth in this Permit, permission is granted to the Owner and Permittee to construct and operate a development and uses as described and identified by size, dimension, quantity, type, and location on the approved exhibits (Exhibit "A") dated June 9, 2020, on file in the Development Services Department (DSD).

The Project shall include:

- a. Construction of a 22-story, 253-foot tall mixed-use development, totaling approximately 432,175 SF and comprised of 241 dwelling units, approximately 35,668 SF of commercial space, and five levels of above-grade parking containing 243 parking spaces.
- b. **Site Development Permit (SDP):** Substantial alterations to designated Historical Resources Board (HRB) Site No. 1355, the Pacific Telephone and Telegraph Company Garage, including demolition of the interior and the 1946 addition and maintenance of the original 1932 structure with some modifications to accommodate vehicle driveways and the commercial storefront.
- c. **Neighborhood Development Permit (NDP):** Five deviations from the SDMC as followings:

1. Sec. 131.0454 – Personal Storage: Reduce the number of required personal storage areas from 100% of DU (241 DU) to 38% of DU (91 DU).
 2. Sec. 142.0560(j) – Driveway & Access Regulations: Reduce the minimum width required for the one-way driveway on Ninth Avenue width from 14 feet to 12 feet.
 3. Sec. 156.0310(d)(3)(A) – Tower Lot Coverage: Increase the allowable maximum tower lot coverage from 50% to 66%.
 4. Sec. 156.0310(d)(3)(B) – Tower Floor Plate: Increase the allowable maximum east-west tower floor plate dimension from 130 feet to 150 feet.
 5. Sec. 156.0311(d)(1) – Transparency: Reduce the minimum required ground level transparency of the Tenth avenue building façade from 60% to 40%.
- d. **Conditional Use Permit (CUP):** Off-site alcoholic beverage sales in the ground floor and basement level commercial spaces.
- e. Public and private accessory improvements determined by DSD to be consistent with the land use and development standards for this site in accordance with the adopted community plan, the California Environmental Quality Act (CEQA) and the CEQA Guidelines, the City Engineer's requirements, zoning regulations, conditions of this Permit, and any other applicable regulations of the SDMC.

STANDARD REQUIREMENTS:

1. This permit must be utilized within thirty-six (36) months after the date on which all rights of appeal have expired. If this permit is not utilized in accordance with Chapter 12, Article 6, Division 1 of the SDMC within the 36-month period, this permit shall be void unless an Extension of Time has been granted. Any such Extension of Time must meet all SDMC requirements and applicable guidelines in effect at the time the extension is considered by the appropriate decision maker. This permit must be utilized by September 3, 2023.
2. The utilization of this CUP is contingent upon the approval of a license to sell alcohol at this location by the California Department of Alcoholic Beverage Control (ABC). The issuance of this CUP does not guarantee that the ABC will grant an alcoholic beverage license for this location.
3. No permit for the construction, occupancy, or operation of any facility or improvement described herein shall be granted, nor shall any activity authorized by this Permit be conducted on the premises until:
 - a. The Owner/Permittee signs and returns the Permit to DSD; and
 - b. The Permit is recorded in the Office of the San Diego County Recorder.
4. While this Permit is in effect, the subject property shall be used only for the purposes and under the terms and conditions set forth in this Permit unless otherwise authorized by the appropriate City decision maker.

5. This Permit is a covenant running with the subject property and all of the requirements and conditions of this Permit and related documents shall be binding upon the Owner/Permittee and any successor(s) in interest.
6. The continued use of this Permit shall be subject to the regulations of this and any other applicable governmental agency.
7. Issuance of this Permit by the City does not authorize the Owner/Permittee for this Permit to violate any Federal, State or City laws, ordinances, regulations or policies including, but not limited to, the Endangered Species Act of 1973 [ESA] and any amendments thereto (16 U.S.C. § 1531 et seq.).
8. The Owner/Permittee shall secure all necessary building permits. The Owner/Permittee is informed that to secure these permits, substantial building modifications and site improvements may be required to comply with applicable building, fire, mechanical, and plumbing codes, and State and Federal disability access laws.
9. Construction plans shall be in substantial conformity to Exhibit "A." Changes, modifications, or alterations to the construction plans are prohibited unless appropriate application(s) or amendment(s) to this Permit have been granted.
10. All of the conditions contained in this Permit have been considered and were determined necessary to make the findings required for approval of this Permit. The Permit holder is required to comply with each and every condition in order to maintain the entitlements that are granted by this Permit.
11. If any condition of this Permit, on a legal challenge by the Owner/Permittee of this Permit, is found or held by a court of competent jurisdiction to be invalid, unenforceable, or unreasonable, this Permit shall be void. However, in such an event, the Owner/Permittee shall have the right, by paying applicable processing fees, to bring a request for a new permit without the "invalid" condition(s) back to the discretionary body which approved the Permit for a determination by that body as to whether all of the findings necessary for the issuance of the proposed permit can still be made in the absence of the "invalid" condition(s). Such hearing shall be a hearing de novo, and the discretionary body shall have the absolute right to approve, disapprove, or modify the proposed permit and the condition(s) contained therein.
12. The Owner/Permittee shall defend, indemnify, and hold harmless the City, its agents, officers, and employees from any and all claims, actions, proceedings, damages, judgments, or costs, including attorney's fees, against the City or its agents, officers, or employees, relating to the issuance of this permit including, but not limited to, any action to attack, set aside, void, challenge, or annul this development approval and any environmental document or decision. The City will promptly notify Owner/Permittee of any claim, action, or proceeding and, if the City should fail to cooperate fully in the defense, the Owner/Permittee shall not thereafter be responsible to defend, indemnify, and hold harmless the City or its agents, officers, and employees. The City may elect to conduct its own defense, participate in its own defense, or obtain independent legal counsel in defense of any claim related to this indemnification. In the event of such election, Owner/Permittee

shall pay all of the costs related thereto, including without limitation reasonable attorney's fees and costs. In the event of a disagreement between the City and Owner/Permittee regarding litigation issues, the City shall have the authority to control the litigation and make litigation related decisions, including, but not limited to, settlement or other disposition of the matter. However, the Owner/Permittee shall not be required to pay or perform any settlement unless such settlement is approved by Owner/Permittee.

13. Development Impact Fees: The development will be subject to Centre City Development Impact Fees. The fee shall be determined in accordance with the fee schedule in effect at the time of building permit issuance and with the SDMC. The Owner/Permittee shall provide to the City's Facilities Financing Department the following information at the time of application for building permit plan check: 1) total square footage for commercial lease spaces and all areas within the building dedicated to support those commercial spaces including, but not limited to: loading areas, service areas and corridors, utility rooms, and commercial parking areas; and 2) applicable floor plans showing those areas outlined for verification. In addition, it shall be responsibility of the Owner/Permittee to provide all necessary documentation for receiving any "credit" for existing buildings to be removed.

14. While this Permit is in effect, the subject property shall be used only for the purposes and under the terms and conditions set forth in this Permit unless otherwise authorized by the appropriate City decision maker.

15. The continued use of this Permit shall be subject to the regulations of this and any other applicable governmental agency.

16. Issuance of this Permit by City does not authorize the Owner/Permittee for this Permit to violate any Federal, State or City laws, ordinances, regulations or policies, including, but not limited to, the Endangered Species Act of 1973 [ESA] and any amendments thereto (16 U.S.C. §1531 et seq.).

17. This Permit is a covenant running with the subject property and all of the requirements and conditions of this Permit and related documents shall be binding upon the Owner/Permittee and any successor(s) in interest.

18. This development shall comply with the standards, policies, and requirements in effect at the time of approval of this development, including any successor(s) or new policies, financing mechanisms, phasing schedules, plans and ordinances adopted by the City.

19. No permit for construction, operation, or occupancy of any facility or improvement described herein shall be granted, nor shall any activity authorized by this Permit be conducted on the premises until this Permit is recorded in the Office of the San Diego County Recorder.

20. Owner/Permittee shall secure all necessary building permits. The Owner/Permittee is informed that to secure these permits, substantial building modifications and site improvements may be required to comply with applicable building, fire, mechanical, and plumbing codes, and State and Federal disability access laws.

21. Construction plans shall be in substantial conformity to the approved Basic Concept/Schematic Drawings on file at the City. Changes, modifications, or alterations to the construction plans are prohibited unless appropriate application(s)/amendment(s) to the Project have been granted.

ENVIRONMENTAL/MITIGATION REQUIREMENTS:

22. As required by SDMC Sec. 156.03049(h), the development shall comply with all applicable MMRP measures from the 2006 Downtown Final Environmental Impact Report (Downtown FEIR) for the DCP to the satisfaction of DSD and the City Engineer. Prior to issuance of any construction permit, all conditions of the MMRP shall be adhered to, to the satisfaction of the City Engineer. All mitigation measures described in the MMRP shall be implemented for the following issue areas:

AQ-B.1-1; HIST-A.1-1; HIST-A.1-2; HIST-B.1-1; PAL-A.1-1; NOI-B.1-1; NOI-C.1-1; TRF-A.1.1-2

CLIMATE ACTION PLAN REQUIREMENTS:

23. Owner/Permittee shall comply with the Climate Action Plan (CAP) Consistency Checklist stamped as Exhibit "A." Prior to issuance of any construction permit, all CAP strategies shall be noted within the first three (3) sheets of the construction plans under the heading "Climate Action Plan Requirements" and shall be enforced and implemented to the satisfaction of DSD, including:

- a. Cool/Green Roofs: Roofing materials with a minimum three-year aged solar reflection and thermal emittance or solar reflection index equal to or greater than the values specified in the voluntary measures under California Green Building Standards Code (CAL Green).
- b. Plumbing Fixtures & Fittings:
 - i. Residential:
 - 1. Kitchen faucets: Maximum flow rate not to exceed 1.5 gallons per minute at 60 PSI;
 - 2. Standard dishwashers: 4.25 gallons per cycle;
 - 3. Compact dishwashers: 3.5 gallons per cycle;
 - 4. Clothes washers: Water factor of six gallons per cubic feet of drum capacity.
 - ii. Nonresidential:
 - 1. Plumbing fixtures and fittings that do not exceed the maximum flow rate specified in Table A5.303.2.3.1 (voluntary measures) of CAL Green.
 - 2. Appliances and fixtures for commercial applications that meet the provisions of Section A5.303.3 (voluntary measures) of CAL Green.
- c. Electric Vehicle Charging: Of the total required listed cabinets, boxes, or enclosures, 50% have the necessary electric vehicle supply equipment installed to provide active electric vehicle charging stations ready for use.
- d. Bicycle Parking Spaces: Project provides more short- and long-term bicycle parking spaces than required in the SDMC.

- e. Shower Facilities: The Project includes changing/shower facilities in accordance with the voluntary measures under CAL Green for the nonresidential component.
- f. Designated Parking Spaces: The Project provides designated parking for a combination of low-emitting, fuel-efficient, and carpool/vanpool vehicles.

AFFORDABLE HOUSING REQUIREMENTS:

24. Prior to issuance of any residential building permit associated with this Project, the Owner/Permittee shall comply with the provisions of the Inclusionary Affordable Housing Regulations of SDMC, Chapter 14, Article 2, Division 13 and the Inclusionary Housing Procedures Manual. The Owner/Permittee shall either pay the Inclusionary In-Lieu Fee or provide affordable housing units at an offsite location in accordance with the Inclusionary Affordable Housing Regulations. If affordable units are provided at an offsite location, the Owner/Permittee shall enter into a written Agreement with the San Diego Housing Commission which shall be drafted and approved by the San Diego Housing Commission, executed by the Owner/Permittee, and secured by a deed of trust which incorporates applicable affordability conditions consistent with the SDMC.

AIRPORT REQUIREMENTS:

25. The Owner/Permittee shall comply with conditions established by the City Airport Approach Overlay Zone (and any successor or amendment thereto) which were approved at the Airport Land Use Commission (ALUC) meeting on March 5, 2020. The ALUC Board made the determination that the project is conditionally consistent with the San Diego International Airport Land Use Compatibility Plan (ALUCP). The Applicant shall comply with the following ALUC conditions:

- a. The structure and construction crane shall be marked and lighted in accordance with Federal Aviation Administration (FAA) procedures.
- b. An aviation easement for the building height shall be recorded with the County Recorder prior to building permit issuance.
- c. The ALUCP requires that a means of overflight notification be provided for new residential land uses. In instances when an aviation easement is required, the overflight notification requirement is satisfied.

ENGINEERING REQUIREMENTS:

26. This Permit shall comply with all Conditions of the Final Map for the Tentative Map No. 2365584.

27. The Tentative Map shall comply with the conditions of Permit No. 2358005.

GEOLOGY REQUIREMENTS:

28. The Owner/Permittee shall submit a geotechnical investigation report or update letter that specifically addresses the proposed construction plans. The geotechnical investigation report or update letter shall be reviewed for adequacy by the Geology Section of DSD prior to issuance of any construction permits.

29. The Owner/Permittee shall submit an as-graded geotechnical report prepared in accordance with the City's "Guidelines for Geotechnical Reports" following completion of the grading. The as-graded geotechnical report shall be reviewed for adequacy by the Geology section of DSD prior to exoneration of the bond and grading permit close-out.

HISTORICAL RESOURCES REQUIREMENTS:

30. Prior to the issuance of any construction permits, the Owner/Permittee shall submit drawings that incorporate the Treatment Plan as approved by HRB and City Historical Resources Staff on July 23, 2020.

31. Prior to issuance of any construction permits, the Historic American Building Survey (HABS) documentation as approved by HRB and City Historical Resources Staff on July 23, 2020 shall be submitted for archival storage with the City of San Diego HRB, South Coastal Information Center, the California Room of the City of San Diego Public Library, the San Diego Historical Society, and/or other historical society or group(s).

32. During construction of the Project, the Owner/Permittee shall implement the Monitoring Plan as approved by HRB and City Historical Resources staff on July 23, 2020. The Project's Principal Investigator shall send monitoring reports as described in the Monitoring Plan to the City's Mitigation Monitoring staff and Historical Resources staff. The Principal Investigator may submit a detailed letter to City staff prior to the start of work or during construction requesting a modification to the Monitoring Plan. This request shall be based on relevant information and site conditions.

LANDSCAPE REQUIREMENTS:

33. Prior to issuance of any grading permit, the Owner/Permittee shall submit complete construction documents for the revegetation and hydro-seeding of all disturbed land in accordance with the City Landscape Standards, Storm Water Design Manual, and to the satisfaction of DSD. All plans shall be in substantial conformance to this permit (including Environmental conditions) and Exhibit "A," on file in DSD.

34. Prior to issuance of any public improvement permit, the Owner/Permittee shall submit complete landscape construction documents for ROW improvements to DSD for approval. Improvement plans shall show, label, and dimension a 40-square-foot area around each tree which is unencumbered by utilities. Driveways, utilities, drains, water and sewer laterals shall be designed so as not to prohibit the placement of street trees.

35. Prior to issuance of any building permit (including shell), the Owner/Permittee shall submit complete landscape and irrigation construction documents, which are consistent with the Landscape Standards, to DSD for approval. The construction documents shall be in substantial

conformance with Exhibit "A," Landscape Development Plan, on file in DSD. Construction plans shall provide a 40-square-foot area around each tree that is unencumbered by hardscape and utilities unless otherwise approved per §142.0403(b)5.

36. The Owner/Permittee shall be responsible for the maintenance of all landscape improvements shown on the approved plans, including in the ROW, unless long-term maintenance of said landscaping will be the responsibility of another entity approved by DSD. All required landscape shall be maintained consistent with the Landscape Standards in a disease, weed, and litter free condition at all times. Severe pruning or "topping" of trees is not permitted.

37. If any required landscape (including existing or new plantings, hardscape, landscape features, etc.) indicated on the approved construction documents is damaged or removed, the Owner/Permittee shall repair and/or replace in kind and equivalent size per the approved documents to the satisfaction of DSD within 30 days of damage or Certificate of Occupancy.

PLANNING/DESIGN REQUIREMENTS:

38. Floor Area Ratio (FAR): The Project achieves a FAR of 11.14 through the following FAR bonuses to increase the Project FAR above the Base Maximum of 6.0 pursuant to SDMC: 1) three-bedroom units, 2) LEED silver certification, 3) FAR Payment Program.

39. Parking: 36 parking spaces shall be provided in the Project for the commercial space. No on-site parking is required for the residential DUs and the Project shall not provide more than 241 parking spaces for the residential DUs (one space per DU, excluding tandem spaces). The Project proposes 243 total parking spaces. The parking spaces shall be designed to City standards, except as permitted in SDMC Sec. 156.0313(k), and subject to the following provisions:

- a. The residential off-street parking spaces shall consist only of unbundled parking.
- b. The number of accessible off-street parking spaces shall be provided in accordance with Title 24 of the California Code of Regulations (California Building Standards Code).
- c. The number of off-street electric vehicle charging spaces shall be provided in accordance with the California Green Building Standards Code.
- d. One motorcycle parking space shall be provided for every 20 required vehicle spaces for the commercial space and for every ten parking spaces provided for the residential DU, or 23 spaces with the 243 parking spaces as proposed.

40. Bicycle Parking: Secured bicycle storage shall be provided to accommodate a minimum of 49 bicycles (one bicycle for every five DU and for every 20 required vehicle spaces for the commercial space). Bicycle storage areas shall be within a secured enclosure with access restricted to authorized persons and provide devices for the locking of individual bicycles. An additional two short-term and two long-term bicycle spaces shall be provided for the commercial component of the Project.

41. Urban Design Standards: The Project, including its architectural design concepts and off-site improvements, shall be consistent with the Centre City Planned District Ordinance (CCPDO) and Centre City Streetscape Manual (CCSM). These standards, together with the following specific conditions, will be used as a basis for evaluating the development through all stages of the development process.
42. Architectural Standards: The architecture of the development shall establish a high quality of design and complement the design and character of the East Village neighborhood as shown in the approved Basic Concept/Schematic Drawings on file with the City. The development shall utilize a coordinated color scheme consistent with the approved Basic Concept Drawings.
43. Form and Scale: The development shall consist of a 22-story mixed-use development (approximately 253 feet tall) measured to the top of the roofline, with roof equipment enclosures, elevator penthouses, and mechanical screening above this height permitted per the CCPDO and the FAA. All building elements shall be complementary in form, scale, and architectural style.
44. Building Materials: All building materials shall be of a high quality as shown in the Basic Concept/Schematic Drawings and approved materials board. All materials and installation shall exhibit high-quality design, detailing, and construction execution to create a durable and high-quality finish. The base of the buildings shall be clad in upgraded materials and carry down to within one inch of finish sidewalk grade, as illustrated in the approved Basic Concept/Schematic Drawings. Any graffiti coatings shall be extended the full height of the upgraded base materials or up to a natural design break such a cornice line. All downspouts, exhaust caps, and other additive elements shall be superior grade for urban locations, carefully composed to reinforce the architectural design. Reflectivity of the glass shall be the minimum reflectivity required by Title 24 of the California Code of Regulations (Title 24). All construction details shall be of the highest standard, as shown in the approved Basic Concept/Schematic Drawings on file with the City, and executed to minimize weathering, eliminate staining, and not cause deterioration of materials on adjacent properties or the ROW. No materials/colors substitutions shall be permitted without prior written City consent.
45. Street Level Design: Street level windows shall be clear glass and may be lightly tinted. Architectural features such as awnings and other design features which add human scale to the streetscape are encouraged where they are consistent with the design theme of the structure. Exit corridors including garage entrances shall provide a finished appearance to the street with street level exterior finishes wrapping into the openings a minimum of ten feet, or the garage door, whichever is deeper. All exhaust caps, lighting, sprinkler heads, and other elements on the undersides of all balconies and surfaces shall be logically composed and placed to minimize their visibility, while meeting code requirements. All soffit materials shall be high quality and consistent with adjacent elevation materials and incorporate drip edges and other details to minimize staining and ensure long-term durability.
46. Utilitarian Areas: Areas housing trash, storage, or other utility services shall be completely concealed from view of the ROW and adjoining developments, except for utilities required to be exposed by the City or utility company. The development shall provide trash and recyclable material storage areas per SDMC Sections 142.0810 and 142.0820. Such areas shall be provided within an enclosed building area and kept clean and orderly at all times.

47. Mail and Delivery Locations: It is the Owner/Permittee's responsibility to coordinate mail service and mailbox locations with the United States Postal Service and to minimize curb spaces devoted to postal and loading use. The Owner/Permittee shall locate all mailboxes and parcel lockers outside of the ROW either within the building or recessed into a building wall.
48. Circulation and Parking: Owner/Permittee shall prepare a plan which identifies the location of curbside parking control zones, parking meters, fire hydrants, valet services if any, trees, street lights to the satisfaction of the City, and consistent with the performance standards in the CCPDO and CCSM. Such plan shall be submitted in conjunction with Construction Permits. All parking shall meet the requirements of the Building Department, Fire Department and City Engineer. All parking shall be mechanically ventilated. The exhaust system for mechanically ventilated structures shall be located to mitigate noise and exhaust impacts on the public ROW. The garage doors shall be a minimum 80% opaque to prevent views into the garage areas.
49. Open Space and Development Amenities: A landscape plan that illustrates the relationship of the proposed on and off-site improvements and the location of water, and electrical hookups to the satisfaction of the City and consistent with the performance standards in the CCPDO, shall be submitted with construction drawings.
50. Roof Tops: A rooftop equipment and appurtenance location and screening plan and consistent with the performance standards in the CCPDO shall be prepared and submitted to the satisfaction of the City with construction drawings. Any roof-top mechanical equipment shall be grouped, enclosed, and screened from surrounding views.
51. Signage: All signs shall comply with the City Sign Regulations and the CCPDO.
52. Lighting: A lighting plan which highlights the architectural qualities of the proposed development and also enhances the lighting of the public ROW shall be submitted with construction drawings. All lighting shall be designed to avoid illumination of, or glare to, adjoining properties, including those across any street.
53. Noise Control: All mechanical equipment, including but not limited to, air conditioning, heating and exhaust systems, shall comply with the City Noise Ordinance and California Noise Insulation Standards as set forth in Title 24. The Owner/Permittee shall provide evidence of compliance with construction drawings.
54. Street Address: Building address numbers shall be provided that are visible and legible from the ROW.
55. On-Site Improvements: All on-site improvements shall be designed as part of an integral site development. An on-site improvement plan shall be submitted to the satisfaction of the City with construction drawings.
56. Off-Site Improvements: Public improvements shall be installed in accordance with the Centre City Streetscape Manual (CCSM). The Manual is currently being updated and the Owner/Permittee

shall install the appropriate improvements according to the latest requirements at the time of Building Permit issuance.

57. Street Trees: Street trees shall be Chinese Flame on G Street, Chinese Evergreen Elm on Ninth Avenue, and Jacaranda on Tenth Avenue, per the CCSM. All trees shall be planted at a minimum 36-inch box size with tree grates provided as specified in the CCSM and shall meet the requirements of Title 24. Tree spacing shall be accommodated after street lights have been sited, and generally spaced 20 to 25 feet on center. All landscaping shall be irrigated with private water service from the subject development. Associated tree grates shall be Gateway on G Street and Tenth Avenue and CCDC Standard on Ninth Avenue, per the CCSM.

58. Street Lights: Street lights shall be Gateway on G Street and Tenth Avenue and Standard on Ninth Avenue, per the CCSM. All existing lights shall be evaluated to determine if they meet current City requirements and shall be modified or replaced if necessary.

59. Sidewalk Paving: Paving shall be Gateway on G Street and Tenth Avenue and CCDC Standard on Ninth Avenue, per the CCSM. Any specialized paving materials shall be approved through the execution of an EMRA with the City.

60. Litter Containers: The development shall include trash receptacles, one at each intersection.

61. Landscaping: All required landscaping shall be maintained in a disease, weed, and litter free condition at all times. If any required landscaping (including existing or new plantings, hardscape, landscape features, etc.) indicated on the approved construction documents is damaged or removed during demolition or construction, it shall be repaired and/or replaced in kind and equivalent in size per the approved documents and to the satisfaction of the City within 30 days of damage or Certificate of Occupancy, whichever occurs first.

62. Planters: Planters shall be permitted to encroach into the ROW a maximum of two feet. The planter encroachment shall be measured from the property line to the face of the curb/wall surrounding the planter. A minimum five-foot clear path shall be maintained between the face of the planter and the edge of any tree grate or other obstruction in the ROW.

63. On-Street Parking: Owner/Permittee shall maximize the on-street parking, wherever feasible.

64. Franchise Public Utilities: The Owner/Permittee shall be responsible for the installation or relocation of franchise utility connections including, but not limited to, gas, electric, telephone and cable, to the development and all extensions of those utilities in public streets. Existing franchised utilities located above grade serving the property and in the sidewalk ROW shall be removed and incorporated into the adjoining development. All franchise utilities shall be installed as identified in the Basic Concept Drawings. Any above grade devices shall be screened from view from the ROW.

65. Construction Fence: Owner/Permittee shall install a construction fence pursuant to specifications of, and a permit from, the City Engineer. The fence shall be solid plywood with wood framing, painted a consistent color with the development's design, and shall contain a pedestrian

passageway, signs, and lighting as required by the City Engineer. The fencing shall be maintained in good condition and free of graffiti at all times.

66. Development Identification Signs: Prior to commencement of construction on the site, the Owner and/or Permittee shall prepare and install, at its cost and expense, one sign on the barricade around the site which identifies the development. The sign shall be at least four feet by six feet and be visible to passing pedestrian and vehicular traffic. The signs shall at a minimum include: 1) Color rendering of the development, 2) Development name, 3) Developer, 4) Completion Date, 5) For information call _____. Additional development signs may be provided around the perimeter of the site. All signs shall be limited to a maximum of 160 sq. ft. per street frontage. Graphics may also be painted on any barricades surrounding the site. All signs and graphics shall be submitted to the City for approval prior to installation.

POLICE DEPARTMENT REQUIREMENTS:

- 67. No off-sale distilled spirits.
- 68. No alcoholic beverages shall be sold except between the hours of 10:00 a.m. and 10:00 p.m.
- 69. Malt liquor, beer, or wine cooler products, regardless of container size, must be sold in manufacturer pre-packaged multi-unit quantities totaling a minimum of 64 ounces.
- 70. No wine shall be sold in containers of less than 750 milliliters.
- 71. Loitering is prohibited.
- 72. No public pay phones are permitted on the premises or adjacent and under the control of the Owner/Permittee.
- 73. No arcade games of any type will be allowed/maintained inside the premises.
- 74. Video surveillance shall be recorded and available to law enforcement upon request covering the interior and public access points of the premises. Upon request of law enforcement, video surveillance shall not be deleted, voided, or destroyed. Recordings shall be maintained for a minimum of 30 days absent a request of law enforcement.
- 75. Litter and any unauthorized graffiti shall be removed promptly.
- 76. All retail employees shall be trained in State of California Alcoholic Beverage Control (ABC) regulations (LEAD), theft deterrence, and policies against the sales of alcoholic beverages to those under 21 years of age, transient, and/or intoxicated.
- 77. There shall be no exterior alcoholic advertising or signage of any kind or type, including advertising directed to the exterior from within, promoting or indicating the availability of alcoholic beverages. Interior displays of alcoholic beverages or signs which are clearly visible to the exterior shall constitute a violation of this condition.

TRANSPORTATION REQUIREMENTS

78. All automobile, motorcycle and bicycle parking spaces must be constructed in accordance with the requirements of the SDMC. All on-site parking stalls and aisle widths shall be in compliance with requirements of the City's Land Development Code and shall not be converted and/or utilized for any other purpose, unless otherwise authorized in writing by the appropriate City decision maker in accordance with the SDMC.

79. Prior to the issuance of any building permits, the Owner/Permittee shall assure by permit and bond, the construction of the northerly driveway to a 16-foot driveway per SDG-163 and the southerly driveway to a 12-foot one-way exit only driveway along 9th Avenue consistent with City standards, satisfactory to the City Engineer. Along with the construction of the driveways, the Owner/Permittee shall install red curb at each - at the southerly driveway, 29 feet of red curb to the south, and at the northern driveway, 21 feet of red curb to the north. All improvements shall be completed and accepted by the City Engineer prior to first occupancy.

PUBLIC UTILITIES DEPARTMENT REQUIREMENTS:

80. Prior to the issuance of any building permits, the Owner/Permittee shall finalize water and sewer capacity charges. Capacity charges, as well as service and meter size, are determined by water meter data card which is completed during the building plan review process.

81. Prior to the issuance of any building permits, the Owner/Permittee shall assure, by permit and bond, the design and construction of new water and sewer service(s) outside of any driveway or drive aisle and the abandonment of any existing unused water and sewer services within the ROW adjacent to the Project site, in a manner satisfactory to the PUD Director and City Engineer.

82. The Owner/Permittee shall apply for a plumbing permit for the installation of appropriate private backflow prevention device(s) (BFPD) on each water service (domestic, fire, and irrigation) in a manner satisfactory to the PUD Director and the City Engineer. BFPDs shall be located above ground on private property, in line with the service and immediately adjacent to the ROW.

83. No trees or shrubs exceeding three feet in height at maturity shall be installed within ten feet of any sewer facilities and five feet of any water facilities.

APPROVED by the Planning Commission of the City of San Diego on August 20, 2020 and Resolution No. _____.

Permit Type/PTS Approval No.: SDP/NDP/CUP No. 649856

Date of Approval: August 20, 2020

AUTHENTICATED BY THE CITY OF SAN DIEGO URBAN DIVISION

James Alexander
Program Manager
Urban Division

**NOTE: Notary acknowledgment
must be attached per Civil Code
section 1189 et seq.**

The undersigned Owner/Permittee, by execution hereof, agrees to each and every condition of this Permit and promises to perform each and every obligation of Owner/Permittee hereunder.

4 Lee Tenth, LLC
Owner

By _____
Elizabeth Lee
Manager

Cisterra Partners, LLC
Permittee

By _____
Jason R. Wood
Principal

**NOTE: Notary acknowledgments
must be attached per Civil Code
section 1189 et seq.**



July 30, 2020

Mr. James Alexander
Program Manager, Urban Division
City of San Diego
1222 First Avenue, Third Floor
San Diego, CA 92101

RE: 9G – Project # 649856

Dear James:

We understand that there are three items in the draft permit document being submitted to Planning Commission for approval on August 20, 2020 that have to be written as drafted but from Cisterra's standpoint need to be approved by Planning Commission with an adjustment to the drafted language. The following are the three items, our proposed substitute language, and an explanation on why for each:

Curb Utilization – we request that references to 29 feet and 21 feet of red curbs at the driveways be removed and replaced with language that simply says a complete project curb utilization plan will be submitted and subject to the approval of the City Engineer prior to issuance of a building permit. We understand that the use of red curbs in these general areas will be necessary but we believe the extent of the length of these red curbs to accommodate truck turns needs further study so we can maximize the usage of green, white, or non-colored curb for parking. Should the City Engineer conclude after our submittal of additional studies that the 29 and 21 feet are required then we will adhere to that requirement.

Distilled Spirits – we request that the restriction on the off-site sale of distilled spirits be adjusted to allow these sales provided they are not in containers of less than 750 milliliters. This would have the condition be the same as the condition permitting other comparable retailers like Albertsons and Grocery Outlet to sell these products. Target, our long-term tenant for this space, has been a responsible retailer in other San Diego locations, and nationally, and should be permitted the right to sell these products.

Alcohol Sales Hours – we request that the restriction on the hours of alcohol sales from 10:00 a.m. to 10:00 p.m. be expanded to 8:00 a.m. to 12:00 a.m. These are the same hours Albertsons is permitted to sell alcohol, Target is a responsible retailer, and the Downtown Community Planning Council provided their recommended approval of our permit application after our presentation of the project that included discussion on this topic with these hours being part of the recommended approval.

The requested modifications for Distilled Spirits sales and Alcohol Sales Hours are essential in finalizing Target's due diligence for this space, which is essential to the viability of the project as a whole.

Sincerely,

Jason Wood
Project Principal

**DRAFT
SITE DEVELOPMENT PERMIT FINDINGS
FOR**

660 10th Avenue and 659 9th Avenue

San Diego Historical Resource #1355

Centre City San Diego

Prepared by:

Marie Burke Lia, Attorney at Law, on behalf of the Project Applicants

April 10, 2020

Revised - June 8, 2020

FINDINGS

Site Development Permit – Section 126.0505

(a) Findings for all Site Development Permits

1. The proposed development will not adversely affect the applicable land use plan.

The proposed project is the substantial alteration of The Pacific Telephone and Telegraph Company Garage, which has been designated as San Diego Historical Resource #1355 and is located at 660 10th Avenue and 659 9th Avenue in the East Village neighborhood of Centre City San Diego.

The property is currently owned by 4 Lee LLC and is used as a commercial fixtures retail store and warehouse. The Property is defined in the Legal Description as Assessor's Parcel Number 535-136-01, Lots A, B, C, K and L of Block 83 of Horton's Addition in the City of San Diego.

The Property was built and used as the Southern California/Pacific Telephone and Telegraph garage. From 1937 to 1984, 660 10th Avenue it was listed in the City Directories as the Southern California Telephone Co. (Garage)/Pacific Telephone and Telegraph Co. (Garage) and from 1990 to 1994, this portion of the building served as an airport and livery service. From 1997 to 2016, it served as the Farkas Store and We Buy Used Inc. 659 9th Avenue was listed in the City Directories as Pacific Telephone Telegraph Co. (Truck Repair) from 1971 to 1984. From 1988 to 2002, various tenants occupied this space, after which Chung Lee was listed. The building is currently occupied and has retained the design characteristics of the Art Deco style of architecture.

The Downtown Community Plan defines the vision for development of the entire Downtown region with land use and housing issues addressed in Chapter 3. Chapter 6 addresses the Community Plan Vision for the East Village with specific attention to the Northwest East Village Sub-District that the project resides in; and Chapter 9 addresses the Historic Preservation aspects of the project. The project is designed to strike a balance between the Community Plan Vision for the district to be a "high-intensity residential zone" where "intensities will parallel those of the Civic/Core" and Chapter 9's goals to preserve historic resources. This is a particularly difficult balance given the limited size of the site and the resulting constraints placed on any additional development. Compound that with the challenges of maintaining the historic façade,

which is 12' thick poured in place concrete, while excavating below grade and integrating new development density becomes quite a challenge.

According to Figure 3-4 describing Land Use, this property is located in the Employment/Residential Mixed-Use district. According to page 3-7, this classification provides synergies between educational institutions and residential neighborhoods or transition between the Core and residential neighborhoods. This classification permits a variety of uses, including office, residential, hotel, research and development and educational and medical facilities.

Chapter 6 calls for the Northwest sub-district to “yield downtown’s highest-intensity residential-emphasis district. Residential towers will share the area with offices, as well as ground-level commercial uses, and residents will enjoy creative pursuits in re-used civic landmark buildings.” It further states that “Establishing peak residential intensities in Northwest will help to maximize use of the area’s transit access.”

The desired development intensity for the area is described on page 3-17 where the Plan establishes intensity standards for various parts of downtown. Intensity is measured as Floor Area Ratio (FAR), which is obtained by dividing gross floor area by lot area. Figure 3-9 of the Plan shows the allowable minimum and maximum FARs for various sites. The minimum FAR for the subject property is 3.5 and the maximum is 6.0. “Proposed base development intensities in the Community Plan range from 2.0 to 10.0, modulated to provide diversity of scale, as well as high intensities in selected locations.” Policy 3.2-P-3 allows “intensity bonuses for development projects in specific locations established by this Plan that provide public amenities/benefits beyond those required for normal development approvals. Per figure 3-11 the subject property has a Total Bonus FAR available of 14 and per figure 3-12 the Maximum FAR with bonuses can be 20.

In light of the Community Plan’s Vision to maximize density on sites in the subject district, the project makes use of several bonus density provisions to achieve a total FAR density of 11.14. The bonus density provisions utilized include providing 10% three-Bedroom units; Construction the project to a LEED Silver standard; and purchasing FAR bonus density. Several factors limit the Project’s ability to achieve higher densities including:

- Maintaining the historic façade
- Height limit of building podiums to 80’
- Allowable Lot coverage
- Maximum Tower floor plate dimensions

- Financial feasibility of building higher than 240’

Several deviations to the CCPDO are being requested to achieve the proposed density. Those include increasing the Lot Coverage from 50% to 66% and increasing the allowable tower floor plate dimension in the east-west direction from 130’ to 150’.

The proposed project complies with Chapters 3 and 6 of the Downtown Community Plan as a mixed-use (residential and non-residential) project that will contain active retail and commercial ground floor uses. The design standards utilized will establish a highly pedestrian oriented development on the site.

Historic Preservation is addressed in Chapter 9 of the Downtown Community Plan. The existing structure on the project site is a designated local historical resource, #1355, The Pacific Telephone and Telegraph Company Garage. As indicated in Table 9-1 of the Plan, locally designated resources are to be retained on-site whenever possible.

The proposed development evaluated multiple options for how to maximize density on the site while maintaining as much of the historic structure as possible. Five concepts were studied in detail to define alternatives and evaluate the potential impacts on project configuration, historic preservation and economic impact. Those alternatives are addressed in detail in the Supplemental Findings herein, but the conclusion of those studies was that the optimal solution that was economically feasible, while best preserving the historic nature of the existing building, is the proposed development scheme.

“Partial retention, relocation or demolition of a resource shall only be permitted through applicable City procedures.” The applicable City procedures are established in San Diego Municipal Code Chapter 14, Article 3, Division 2, entitled “Historical Resources Regulations.” §143.0210 (2) (C) requires a Site Development Permit in accordance with Process Four for any development that proposes to deviate from the development regulations for historical resources described in this division. Substantial alteration of a designated resource by partial retention or other means is a deviation from the historical resource regulations and therefore a Site Development Permit, as authorized by Chapter 12, Article 6, Division 5, entitled “Site Development Permit Procedures” is required. The decision maker, in this instance the Planning Commission, must make all of the Findings in §126.0505(a) and §126.0505(i) before the partial retention of a locally designated historical resource can occur. Therefore, the processing of this Site Development Permit application is in compliance with and will not adversely affect this aspect of the applicable land use plan. The proposed project will be in compliance with the Downtown Community Plan.

2. The proposed development will not be detrimental to the public health, safety and welfare.

The proposed project would retain the existing façade of the designated structure on the site, add one subterranean level of retail space, construct five levels of parking above the existing structure and sixteen levels of residential units above the levels of parking.

The proposed development will be consistent with the Downtown Community Plan.

The project site is 24,829 (25,000) square feet bounded 9th Avenue, G Street and 10th Avenue. The Property is defined as Assessor's Parcel Number 535-136-01, Lots A,B,C, K and L of Block 83 of Horton's Addition in the City of San Diego. The construction will be Type 1, fire rated and sprinklered, meeting occupancy classification R1 as required by the California Building Code CBC 2010.

The proposed development complies with the Development Regulations of the Centre City Planned District Ordinance (§156.0310), including the Residential Development Regulations (§156.0310 (g) as they apply to developments containing more than 50 units in terms of Common Outdoor Open Space, Common Indoor Space, Private Open Space and Pet Open Space.

The proposed development complies with the Urban Design Regulations of the Planned District Ordinance (§156.0311), the Performance Standards of the Planned District Ordinance (§156.0312), the Residential Off-Street Parking Space Requirements of the Planned District Ordinance (§156.0313),

The proposed development complies with all San Diego Municipal Code and Uniform Building Code provisions intended ensure that the public health, safety and welfare are protected and enhanced by this construction.

3. The proposed development will comply with the applicable regulations of the Land Development Code.

The proposed project will construct a high density, 22 story high rise tower of mixed-use residential development with two levels of retail, five levels of parking and sixteen levels of residential use.

The proposed development will comply with the applicable provisions of the Centre City Planned District Ordinance in the following manner. It is located within the Employment/Residential Mixed-Use District that specifically calls for this type of

property use. The development will comply with the PDO's FAR regulations that call for a maximum base floor area ratio of 6.0 at this site with a maximum FAR with Bonuses of 20. It will comply with the PDO's Development Regulations pertaining to lot size, minimum building setbacks, building heights, building bulk, building base, ground floor heights, commercial space depth and residential development regulations.

The proposed development will request deviations from the PDO requirements for Personal Storage, Parking Standards, Maximum Lot Coverage, and Floor Plate Dimensions in the Tower. These deviations all address the proposed new construction on the site. One more deviation will be requested to protect the historic façade on the ground level as follows:

The preservation of the existing historic façade has created constraints for utility services needed to develop the site. The existing façade has limited penetrations which requires utilities to be placed in certain specific locations. The utilities along 10th Avenue include standard Back Flow Prevention Devices, that, per City standards, need to be spaced with proscribed separation distances to allow for servicing and separate points of connection. The transparency for the 10th Avenue façade will be reduced from 60% to 40%, but the transparency throughout all of the street facades combined will meet the Municipal Code requirements.

The building will comply with the PDO's Urban Design Regulations pertaining to building orientation, façade articulation, street level design, pedestrian entrances, blank walls, tower design, glass and glazing, exterior projecting balconies, rooftops, encroachments into public rights-of-way, building identification, regulations pertaining to historical resources requiring a Site Development Permit, additional standards for residential developments, additional standards for main streets, and urban open space design guidelines. It will comply with the PDO's Off Street Parking and Loading Standards.

The relevant Land Development Code's Planning and Development Regulations for topics not addressed in the Centre City Planned District Ordinance are contained in that Code's Chapter 14 and include: Grading Regulations, Draining Regulations, Landscape Regulations, Parking Regulations, Refuse and Recyclable Materials Storage, Mechanical and Utility Equipment Storage Regulations, Loading Regulations, Building Regulations, Electrical Regulations and Plumbing Regulations. The proposed development will comply with all of these regulations, since a building permit would not be issued without such compliance. Therefore, the proposed development will comply with all applicable regulations of the Land Development Code.

(i) Supplemental Findings – Historical Resources Deviation for Substantial Alteration of a Designated Historical Resource

Supplemental Finding (1) There are no feasible measures, including a less environmentally damaging alternative, that can further minimize the potential adverse effects to the designated historical resource.

The proposed project would retain the existing façade of the designated 1932 structure on the site, add one level of subterranean retail space, (19,500 sf), construct five levels of parking above the existing structure and sixteen levels of residential units above the levels of parking, resulting in 242 residential units. The 1946 addition would not be retained.

Various development alternatives were prepared to evaluate different programs for the site and evaluate their respective impacts to:

- the existing historic structure;
- the financial feasibility;
- the conformance with Community Plan goals;
- the ability to meet code & zoning regulations

The architectural plans for the Base Project were developed by the Project Architects, Carrier Johnson, and included site plans, floor plans, elevations and sections. That firm was also responsible for the architectural graphics illustrating the Base Project and the four project alternatives discussed below. These architectural graphics are attached to these Findings as *Exhibit A*.

The HABS documentation for the existing buildings was prepared by Union Architecture and included site plans, floor plans, elevations and sections. This documentation is attached as *Exhibit B*.

The construction cost estimates for the base project and each alternative were prepared by California licensed construction firms with historical and new property experience. The Economic Feasibility Analysis for the Base Project and each alternative was prepared by The London Moeder Group and was based on information provided by the above listed parties. Such information included feedback on the scope, schedule and budget for purposes of that analysis which is attached to these Findings as *Exhibit C*.

Base Project:

This is the proposed development for the site which includes, maintaining the existing facade of the 1932 historic structure for ground level retail use and adding one

subterranean level of retail for a total of 34,000 net square feet, five levels of parking above ground consisting of 276 spaces, and 16 levels of residential consisting of 242 units. Separation of the new development from the historic façade will be achieved by creating a horizontal “lens” at the first level of parking above. This lens is of dark colored metal panel and provides a negative space that separates the historic from new architecture. Changes to the historic structure include the removal of the existing roof and slab on grade and the 1946 addition, adding exiting doors on two facades, storefront infill within the original garage openings, and removal of a small section of the façade along 9th St.

Alternative 1:

Rehabilitate the existing 25,000 square foot historic structures and use 22,000 square feet as net retail space. Alternative 1 keeps the 1932 building and the 1946 addition as is with no alterations to the façade and/or roof trusses.

Alternative 2:

Maintain the existing facade of the 1932 historic structure, use the ground level for retail use, add one subterranean level of retail for a total of 34,000 net retail square feet, construct a loading dock, and construct a parking ramp to roof parking consisting of 34 spaces. This alternative was investigated to determine the minimum renovations needed for the retail space to meet market standards and incorporate on-site parking. Changes to the historic structure include the removal of the 1946 addition, the existing roof and slab on grade, and storefront infill within the original garage openings.

Alternative 3:

Maintain the existing facade of the 1932 historic structure, use the ground level for retail use, add one subterranean level of retail for a total of 34,000 net square feet, construct five levels of parking above consisting of 189 spaces, and construct ten levels of residential above consisting of 155 units. This alternative investigates the impacts of the requested 15’ foot setback on all streets from face of historic façade to proposed tower and above grade parking. Other changes to the historic structure would be the same as the Base Project.

The purpose of this study was to determine the impacts of providing a 15’ setback from the historic façade for all above grade development.

The physical yields for the development of Alternative 3 include:

- Reduced parking count (approximately 189 stalls) due to less floor plate,
- Street-wall height limits, which limit the levels of the garage (or tower base) above grade, preventing additional floors of garage to be added without

significantly reducing the parking count.

- Fewer residential units due to lower parking count.

Alternative 3B:

At request of City Planning and Historical, this Alternative is a variation of #3, providing the same setbacks behind the façade of the historic building but with a taller tower totaling 23 stories.

The physical yields for the development of Alternative 3 include:

- Reduced parking count (approximately 189 stalls) due to less floor plate,
- Street-wall height limits, which limit the levels of the garage (or tower base) above grade, preventing additional floors of garage to be added without significantly reducing the parking count.
- Reduced rates for the units due to lower parking count.

Alternative 4:

Maintain the existing facade of the 1932 historic structure, add five subterranean levels of parking consisting of 202 spaces, rehab ground floor retail and add one second story level of retail for a total of 34,000 net square feet, and construct eleven levels of residential consisting of 168 units. This proposal provides a 5' setback from the historic façade at the second floor and a 15 foot setback at the 3rd floor and above. Changes to the historic structure would be the same as the Base Project.

The purpose of this Alternative 4 study was to determine the impacts of providing all parking below grade.

The physical yields for the development of Alternative 4 include:

- Reduced parking count (approximately 202 stalls),
- 5' setback from historic façade (instead of 15') for 2nd floor retail to achieve minimum required retail area.

Fewer residential units due to lower parking count.

Analysis of the Base Project and the Four Alternatives

Alternative 1 was prepared to evaluate the impact of restoring the 1932 building and the 1946 addition and using them as a retail venue. While this maintains the designated historical resource, including the façade, roof and structure, it is not an optimal space for retail use, has no on-site parking for the use and is not economically viable.

Alternative 2 was prepared to evaluate the minimum renovation needed to accommodate a viable retail use in the 1932 building with on-site parking. While this alternative is feasible to construct it, requires nearly the same level of modifications to the existing building as the Base Project and is not economically viable.

Alternative 3 was prepared to evaluate the impact of providing a 15' setback from the historic façade of the 1932 building to the development above. This illustrates the significant impact to parking that happens when the façade is set back. Due to the proportions of the site, providing a setback causes the parking deck levels to be extremely inefficient, producing far less parking and resulting in far less residential units. It is not feasible to build additional residential units, even though the City does not require parking for the residences, because market studies show that a minimum amount of parking is required to make the project feasible to rent. Building the parking levels higher to achieve more parking and thus more residential is not feasible because the parking is so inefficient and, therefore, so expensive it becomes cost prohibitive. Either variation of this alternative is not economically viable.

Alternative 3B was prepared at request of the city to evaluate the unit count for a tower of equal height to the Base Project. Although the unit count is increased, there is still a reduced floor plate due to the 15 foot setback provided along 'G' Street and 9th Avenue. This reduced floor plate totals 227 units and 155 residential parking stalls, providing a parking ratio of .68 per unit. The average parking ratio for market rate apartments in Downtown San Diego is higher than 1 per Unit. The City does not require parking for the residences, however a minimum amount of parking is required to make the project feasible to rent at a competitive rate and to obtain financing for the project.

Alternative 4 was prepared to evaluate the impact of providing all the parking below grade. To achieve the equivalent retail area needed to make the retail viable, this alternative allows only a 5' setback from the historic façade to the 2nd level retail space. Not shown on graphic. Above that the residential structure is set back the full 15'. This alternative also yields less parking and less residential units. The parking is the maximum that can be built below grade as this alternative assumes 5 stories below grade. Another problem with this alternative is that the existing façade would need to be held up in place above 5 stories of shoring. This would be cost prohibitive if not totally infeasible. The alternative is not economically viable.

Other variations of these alternatives were evaluated and discarded as they did not improve the economics or lessen the impact to the existing historic structure. Any setback to the structure above from the face of the historic façade causes significant loss

of parking efficiency due to the dimensions of the site. The narrow width leaves no room for flexibility in the configuration of the above grade parking. Building parking higher is restricted by the building podium height defined in the CCPDO. The Base Program was, therefore, considered the optimal solution that strikes a balance between historic preservation and development density.

Supplemental Finding (2) The deviation is the minimum necessary to afford relief and accommodate the development and all feasible measures to mitigate for the loss of any portion of the historical resource have been provided by the applicant.

The initial building on this site was constructed in 1932 as a single story, approximately 22,000 square foot, garage for the Southern and later Pacific Telephone and Telegraph Company with Art Deco styling that was popular during that period. In 1946, an approximately 2,000 square foot, addition was constructed immediately to the south. Over the years, this building complex was converted from garage to warehouse use, but its exterior appearance was retained. The retained art deco style elements include large raised panels with vertical detailing. The three-sided framing of these panels extends above the roofline on the three street elevations. Raised flat horizontal bands are present around the edge of the roofline and connect the vertical panels. Some of the vertical panels contain metal foldup style garage doors, others contain metal-framed, fixed pane multi-lite window sections. Both buildings are in good condition and no significant exterior alterations have been noted. Both buildings were included in the designation of this property as San Diego Historical Resource #1355 in November of 2019.

The Mitigation Monitoring and Reporting Program of the Downtown FEIR/SEIR provides that resources listed on San Diego Register of Historical Resources shall, whenever possible, be retained on-site. Partial retention, relocation or demolition of a resource shall only be permitted according to Chapter 14, Article 3, Division 2 of the Historical Resources Regulations of the Land Development Code. Said Article 3 requires a Site Development Permit in accordance with Process Four be obtained for Multiple dwelling unit residential development on any size lot when a historical resource is present. (SDMC §143.0210(e)(2)(B)) A Site Development Permit decided in accordance with Process Four is also required for any development that deviates from the historical resources regulations as described in Section 143.0210 per (SDMC §126.0502(d)(20)(E)). The instant Site Development Permit proceedings are being undertaken for this purpose.

The City of San Diego applies the Secretary of the Interior's Standards for Rehabilitation provide the guidelines to be applied to rehabilitation projects affecting designated local resources.

Secretary of the Interior's Standards for Rehabilitation

1. *A property will be used as it was historically or will be given a new use that requires minimal change to its distinctive materials, features, spaces and spatial relationships.* The subject property will be given a new use that will require substantial change to its spaces and spatial relationships.
2. *The historic character of a property will be retained and preserved.* The removal of distinctive materials or alterations of features, spaces and spatial relationships that characterize a property will be avoided. But the historical character of the property will not be retained.
3. *Each property will be recognized as a physical record of its time, place and use.* Changes that create a false sense of historical development, such as adding conjectural features or elements from other historic properties, will not be undertaken. As the result of this project, the property will not be recognized as a physical record of its time, place and use.
4. *Changes to a property that have acquired historic significance in their own right will be retained and preserved.* No changes to the property that have acquired historic significance in their own right exist.
5. *Distinctive materials, features, finishes and construction techniques or examples of craftsmanship that characterize a property will be preserved.* The distinctive materials, features, finishes and construction techniques at the street level will be preserved but will be overshadowed by the new development.
6. *Deteriorated historic features will be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature will match the old in design, color, texture and, where possible, materials.* Replacement of missing features if any exist will be substantiated by documentary and physical evidence. Deteriorated historic features will be repaired rather than replaced.
7. *Chemical or physical treatments, if appropriate, will be undertaken using the gentlest means possible. Treatments that cause damage to historic materials will not be used.* This Standard can be met.
8. *Archaeological resources will be protected and preserved in place. If*

such resources must be disturbed, mitigation measures will be undertaken. No such resources are anticipated.

9. *New additions, exterior alterations or related new construction will not destroy historic materials, features or spatial relationships that characterize the property. The new work shall be differentiated from the old and will be compatible with the historic materials, features, size, scale and proportion, and massing to protect the integrity of the property and its environment. It is the project's intent to meet this Standard.*
10. *New additions and adjacent or related new construction will be undertaken in such a manner that, if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.*

Conclusion: It is indisputable that this project will not meet the Secretary of the Interior's Standards for Rehabilitation. If it did, the project would not need a Site Development Permit.

The 1932 historical resource will be retained and rehabilitated on its original site and it will be incorporated into a new residential and retail complex that will rise 21 stories above consistent with the current zoning for this site. The complex will be designed in a manner that laterally separates the historical resource from the new construction above in order to preserve the historical appearance of the designated resource.

The 1946 addition will be removed to allow for vehicular egress and loading dock. Other alternatives were studied for vehicular ingress and egress as well as a loading dock but they all required significant alteration to the 1932 façade and significant demolition of the 1946 façade. The proposed design minimizes any modification to the 1932 façade, utilizing existing openings for vehicular and occupant entry and with minimal modifications for required emergency exits and utility access. The full scope of the modifications to the façade is defined in the Treatment Plan, which is submitted as part of the Mitigation, Monitoring and Reporting Program presented in *Exhibit B* to this Report.

The proposed lateral separation at the second floor above the historic façade with a horizontal "lens" element is the only solution that worked for the site given the dimensional constraints of the property. This solution allows the historic façade to maintain its character and read as an independent element. Great care was taken in

exploring the dimensions, materiality and color of this lens to be respectful of the historic façade and create a “negative space” between the two elements of the project.

The Mitigation Monitoring and Reporting Program (MMRP) for the Downtown Community Plan requires the implementation of Mitigation Measure *HIST- A.1-3* if a (locally) designated historical resource would be only partially retained. That Mitigation Measure requires the submission of a Documentation Program that must include Photo Documentation and Measured Drawings of the resource to the Historical Resources Board Staff for review and approval. This Mitigation Measure will be implemented. Copies of the Mitigation Monitoring and Reporting Program Components are attached to these Findings as *Exhibit B*.

The subject property was evaluated in a *Historical Resources Technical Report* prepared by Marie Burke Lia and Dolores Mellon in July of 2019 and revised in September of 2019. That Report concluded that the subject property is eligible for the Local Historical Register under Criterion A and C, but not eligible for the California or National Registers. In November of 2019, the property was listed as Historical Resources #1355 under Criterion A as a special element of the City’s economic development, revealing the increasing importance of the automobile as America’s favored method of private transportation from 1900 – 1956, and under Criterion C through the retention of character-defining features of the Art Deco style, retaining a good level of architectural from its 1932 period of significance.

The Downtown Community Plan (DCP) and Centre City Planned District Ordinance (CCPDO) are subject to the Downtown FEIR, a “Program EIR” prepared in compliance with the California Environmental Quality Act (CEQA). A Downtown FEIR Consistency Evaluation will be prepared for the Project in compliance with CEQA and Local Guidelines. Under this process, an Evaluation is prepared for each subsequent proposed action to determine whether the potential impacts of a project were anticipated in the Downtown FEIR.

The incorporation of designated historical resources into new construction has been an established practice in the City of San Diego since 1990. There have been eight such projects that have used the Site Development Permit process under §126.0505 to achieve this solution that preserves the appearance and location of the historical resource and permits the redevelopment of the site in accordance with current zoning. These incorporation examples include the Howe House into the First and Robinson Apartment complex, the Carnation Building into the Icon Condominiums, the California Stamp Company into the Doma Condominiums, the Egyptian Theater into the Egyptian Condominiums, Station B into the Electra Condominiums, Exclusive Cleaners into the

Deca Condominiums, Griswold Pharmacy into the Paseo de Mission Hills and the John O'Day Commercial Building into the Kettner Lofts, aka AV8. A graphic exhibit documenting these incorporations is attached as *Exhibit D* to these Findings.

In each of these instances, the required Site Development Permit Findings were made that the deviation to incorporate the resource into the new development was found to be the minimum necessary to afford relief and accommodate the development and all feasible measures to mitigate the loss of any portion of the historical resource had been provided. The same Finding can be made for this project. The documentation herein demonstrates that the minimum necessary revisions to the existing historic structure will be undertaken and that a comprehensive mitigation and monitoring program will be put in place.

Supplemental Finding (3) The denial of the proposed development would result in economic hardship to the owner. For purposes of this Finding, “economic hardship: means there is no reasonable beneficial use of a property and it is not feasible to derive a reasonable economic return for the property.

In order to determine whether there are feasible alternative development plans for this property, that would minimize the potential adverse effects to the designated historical resource, the attached Economic Feasibility Analysis, conducted by the firm of London Moeder Advisors, was prepared based on the Base Project and 4 Alternatives (that were prepared by Carrier Johnson and are defined more fully above). That Analysis, which is attached as Exhibit C, reached the following conclusions.

Base Project

The Base Project would retain the existing façade of the historic structure, add one level of subterranean retail space, (19,500 sf), construct five levels of parking above the existing structure and sixteen levels of residential units above the levels of parking, resulting in 242 residential units. The total project costs would be \$142,807,588 and the total gross profit generated from this investment is forecasted to be \$45,955,665.

The Net Operating Income (NOC) at stabilization is estimated at \$7,954,860, which when compared to the total costs of the project, will result in a Yield on Costs (YOC) of 5.6% which satisfies the minimum 5.5% for project feasibility.

The Internal Rate of Return (IRR) on this investment is forecasted to be 16.5%, which also demonstrates that the project is economically feasible. The typical IRR for rental housing projects ranges from 13% to 15%. Any IRR below this range would struggle to attract investors and achieve project financing.

Four less environmentally damaging alternatives, that would minimize the potential adverse effects, have been evaluated.

Alternative 1

Alternative 1 would retain and rehabilitate the existing warehouse retail structure (21,944 square feet) and return the space to retail use. The Economic Feasibility Analysis determined that the almost 22,000 square foot space would rent for \$2.75 a square foot.

The total forecasted rehabilitation costs for this Alternative are \$14,627,435, which would exceed the forecasted sale price of \$14,537,297.

This alternative would generate approximately \$43,197.909 less total profit than the Base Project. But more importantly, the forecasted YOC (yield on costs) would be 3.9% whereas the minimum YOC necessary to finance retail properties is 7.5%.

Alternative 2

Alternative 2 would retain and rehabilitate the existing warehouse retail structure, add one subterranean retail level and replace the roof structure with rooftop parking. The 34,000 square foot retail space is estimated to rent for \$3.00 per square foot.

The total project costs are \$27,989,507 and the forecasted sale price is \$24,509,162. This results in a financial loss for the project forecast to be \$2,538,708, which is not economically feasible.

With a forecasted profit at disposition of negative \$2,538,708, this Alternative would generate \$48,494,373 less total profit than the Base Project.

Alternative 3

Alternative 3 studies the effect of providing a 15' setback from the historic façade and would retain and rehabilitate the existing warehouse retail façade, add one level of subterranean retail space, add five levels of parking above and ten levels of residential units above the parking, resulting in 155 residential units.

The total project costs are forecasted to be \$105,590,612 and the forecasted sales price is \$116,078,147.

With a forecasted profit at disposition of \$15,145,802, this Alternative would generate \$30,809,863 less total profit than the Base Project.

This results in an economically infeasible project because the Yield on Costs is 4.7% where the minimum requirement is 5.5%. It is also the case that the Internal Rate of Return (IRR) would be 7%, which is below 13%-15% required to attract investors and qualify for financing.

Alternative 3B

Alternative 3B assumes retaining the existing façade of the historic structure, the addition of one subterranean level of retail space, construction of five levels of parking above the existing structure, and 16 levels of residential units above the levels of parking. The 227 residential units are to begin leasing immediately after construction is complete with two months of pre-leasing. The project is assumed to be sold after a five-year period.

The 227 market rate units average 900 square feet in size. There will be a total of 189 parking spaces (155 assigned to residential) included in the parking levels of the building. In addition, there will be 19,500 square feet of below grade retail and 14,500 square feet of retail on the first level of the building.

When the 227 market rate units are leased after construction is completed, the forecasted average rent is estimated to be \$3.73 per Square foot for the units with parking and \$3.50 per square foot for units without parking. The 34,000 square feet of total retail space is estimated to at \$3.37 per square foot.

The forecasted sale price for the entire project is \$159,276,163. It is important to note that the assumed exit cap rate is 4.75% (25 basis point above market) to reflect the 72 units that do not have parking provided. Projects that do not provide parking represent increased marketability risk to the developer and lender. Total project costs are forecasted at \$136,477,630 resulting in a IRR of only 10.2%, well below the minimum returns needed to obtain financing commitments.

Alternative 4

Alternative 4 studies the effect of putting all parking below grade and providing a smaller setback from the historic façade. This Alternate would retain and rehabilitate the existing warehouse retail façade, add one subterranean and one second story level of retail, five subterranean levels of parking and eleven levels of residential above the

second level of retail, resulting in 168 residential units.

The forecasted project costs are \$126,493,773 and the forecasted sales price for the entire project is \$110,331,239, a financial loss of \$14,556,301. This Alternative would generate \$60,511,966 less profit than the base project.

Also, the Internal Rate of Return (IRR) would be 6.5%, below the 13% - 15% required to attract investors and qualify for financing.

Five Alternatives to the proposed project were identified and evaluated by the London Moeder Advisors as discussed above. Each of those Alternatives fail to meet the minimum levels of financial performance that would make the project feasible. Each generates levels of Internal Rate of Return (IRR) and Yield on Costs (YOC) that is far below the minimum acceptable levels for financing. The required Finding of economic hardship can be made, there is no reasonable alternative beneficial use of the property and it is not feasible to derive a reasonable economic return from this property other than the proposed Base Project.

Therefore, the Supplemental Findings for the Historical Resources Deviation for Substantial Alteration of a Designated Historical Resource can be made.

DRAFT
Neighborhood Development Permit Findings
For

660 10th Avenue and 659 9th Avenue

Centre City San Diego

Prepared by:

**Duane Hagewood/Michael Stonehouse, Carrier Johnson + CULTURE, on
behalf of the Project Applicants**

April 15, 2020

Revised May 29, 2020

Revised June 9, 2020

Revised July 1, 2020

Revised July 20, 2020

FINDINGS

Neighborhood Development Permit – Section 126.0404

(143) Findings for Neighborhood Development Permit

143. The proposed development will not adversely affect the applicable land use plan.

The Downtown Community Plan defines the vision for development of the entire Downtown region with land use and housing issues addressed in Chapter 3. Chapter 6 addresses the Community Plan Vision for the East Village with specific attention to the Northwest East Village Sub-District that the project resides in. The project is designed to strike a balance between the Community Plan Vision for the district to be a “high-intensity residential zone” where “intensities will parallel those of the Civic/Core” and Chapter 9’s goals to preserve an historic resource.

According to Figure 3-4 describing Land Use, this property is located in the Employment/Residential Mixed-Use district. According to page 3-7, this classification permits a variety of uses, including office, residential, hotel, research and development and educational and medical facilities.

The minimum FAR for the subject property is 3.5 and the maximum is 6.0. “Proposed base development intensities in the Community Plan range from 2.0 to 10.0, modulated to provide diversity of scale, as well as high intensities in selected locations.” Policy 3.2-P-3 allows “intensity bonuses for development projects in specific locations established by this Plan that provide public amenities/benefits beyond those required for normal development approvals. Per figure 3-11 the subject property has a Total Bonus FAR available of 14 and per figure 3-12 the Maximum FAR with bonuses can be 20.

The proposed project makes use of several density bonuses to achieve a total FAR of 11.14. The bonuses include providing 10% three-Bedroom units; Construction the project to a LEED Silver standard; and purchasing FAR bonus density. Several factors limit the Project’s ability to achieve higher densities including:

- Maintaining the historic façade
- Height limit of building podiums to 80'
- Allowable Lot coverage
- Maximum Tower floor plate dimensions
- Financial feasibility of building higher than 240'

Several deviations to the CCPDO are being requested to specifically achieve the proposed density. Those include increasing the Lot Coverage and increasing the allowable tower floor plate dimension in the east-west direction.

The proposed project complies with Chapters 3 and 6 of the Downtown Community Plan as a mixed-use (residential and non-residential) project that will contain active retail and commercial ground floor uses. The design standards utilized will establish a highly pedestrian oriented development on the site.

2. The proposed development will not be detrimental to the public health, safety and welfare.

The proposed development will be consistent with the Downtown Community Plan.

The site is 24,829 square feet bounded 9th Avenue, G Street and 10th Avenue. The Property is defined as Assessor's Parcel Number 535-136-01, Lots A,B,C, K and L of Block 83 of Horton's Addition in the City of San Diego. The construction will be Type 1, fire rated and sprinklered, meeting occupancy classification R1 as required by the California Building Code CBC 2019.

The proposed development complies with the Development Regulations of the Centre City Planned District Ordinance (§156.0310), including the Residential Development Regulations (§156.0310 (g) as they apply to developments containing more than 50 units in terms of Common Outdoor Open Space, Common Indoor Space, Private Open Space and Pet Open Space.

The proposed development complies with the Urban Design Regulations of the Planned District Ordinance (§156.0311), the Performance Standards of the Planned District Ordinance (§156.0312), the Residential Off-Street Parking Space Requirements of the Planned District Ordinance (§156.0313),

The proposed development complies with all San Diego Municipal Code and Uniform Building Code provisions intended ensure that the public health, safety and welfare

are protected and enhanced by this construction.

3. The proposed development will comply with the applicable regulations of the Land Development Code including any allowable deviations pursuant to the Land Development Code.

The proposed development will comply with the applicable provisions of the Centre City Planned District Ordinance. It is located within the Employment/Residential Mixed-Use District that specifically calls for this type of property use. The development will comply with the PDO's FAR regulations that call for a maximum base floor area ratio of 6.0 at this site with a maximum FAR with Bonuses of 20. It will comply with the PDO's Development Regulations pertaining to lot size, minimum building setbacks, building heights, building bulk, building base, ground floor heights, commercial space depth and residential development regulations.

The building will comply with the PDO's Urban Design Regulations pertaining to building orientation, façade articulation, street level design, pedestrian entrances, blank walls, tower design, glass and glazing, exterior projecting balconies, rooftops, encroachments into public rights-of-way, building identification, regulations pertaining to historical resources requiring a Site Development Permit, additional standards for residential developments, additional standards for main streets, and urban open space design guidelines. It will comply with the PDO's Off Street Parking and Loading Standards.

The proposed development will request deviations from the PDO requirements including:

Personal Storage

The municipal code Sec. 131.0454 states that "each dwelling unit shall have a fully enclosed, personal storage area outside the unit that is at least 240 cubic feet with a minimum 7-foot horizontal dimension along one plane." The applicant would like to request a deviation to reduce the storage requirement to 91 storage areas total. This includes recently added 40 storage areas to be provided at tandem parking stalls. Per code the personal storage requirements for the proposed 241 dwelling units, equals to a net total of 57,840 cu ft of space, not including circulation requirements. The floor area required for this amount of storage and circulation is approximately 10,000 to 12,000 square feet of floor area. The proposed building envelope has been determined through height restraints per the California Building Code, zoning regulations for limited tower floor plates, and setbacks. The proposed design maximizes the building envelope and provides all usable occupied space above grade.

Utilities, parking, and storage are forced to be within 5 levels above grade. The alternative is providing additional storage that would reduce residential and/or retail use or parking capacity.

The project has been designed to provide sufficient room to meet the minimum standards for both parking and storage. The applicant anticipates that there will be a higher market demand for parking than the minimum code standard and a lower market demand for personal storage per the PDO. The applicants plan is to design level 6 for parking use, but if parking demand decreases, this space can be dedicated for personal storage and still meet the parking requirements per the PDO. In the future, property management can adjust the amount of parking and storage provided to meet the market demand of each.

In addition, it should be noted that each residential unit has been designed with one to two walk in closets, rather than a normal 4' to 6' wide single depth closet. This will facilitate storage for the residents within their units.

Driveways

The proposed project is requesting a deviation from San Diego Municipal Code (SDMC) [Section 142.0560\(j\)](#) for a new proposed one-way driveway for vehicles to egress from the site onto Ninth Avenue. The minimum standard driveway curb-cut width for a one-way non-residential curb cut and driveway is 14 feet (ft) wide, and the project is proposing a 12 ft driveway curb-cut. Widening the one-way driveway was explored but deemed not feasible due to the preservation requirements of a historic façade and the location of the adjacent loading zone driveway. Because of the preservation of the historic façade and limitations to site access due to the widths of existing façade openings, there is limited frontage to provide all vehicle access, including on-site loading.

The project provides Visibility Areas at the project driveway per [SDMC Diagram 113-02SS](#) and also proposes the installation of convex mirrors located within the project site at the proposed 12 ft wide driveway to enhance the visibility of pedestrians to motorists. A sight distance analysis was provided that confirmed adequate line of sight for traffic egressing could be provided identifying adequate visibility of vehicles, bicycles, and pedestrians. In addition, the proposed 12 ft wide driveway curb cut design would maintain the driveway apron flare within the project boundary with adequate separation between the curb cut opening and adjacent property line.

Maximum Lot Coverage Tower

The municipal code Sec. 156.0310 (d)(3)(A) states that “The maximum lot coverage

of the tower of the building shall be 50 percent of the lot area,” The applicant would like to request a deviation to increase the lot coverage to 66 percent of the lot area. Limiting the lot coverage to 50% for the tower will result in the project not being feasible because it will reduce the number of residential dwelling units and FAR density that can be achieved. Strictly complying with the 50% maximum lot coverage for the tower while maintaining the same square footage of residential units would require displacing 4,000 s.f. of net residential area from each floor to new floors in the tower (60,000 s.f. total of useable space) and result in 6 additional floors being added to the tower. These floors would need elevator, stair & corridor cores of 2,167 sf. for each floor resulting in an additional gross area of 13,000 s.f. to achieve the same rentable residential square feet. This change would result in an increase of \$3,150,000 to the project construction costs (13,000 sf x \$242/sf) for the same net useable residential footage. There would also be an additional \$20/sf to purchase the additional FAR and \$10/sf in soft costs totaling \$400,000. Together that puts the additional area at a cost of \$3,550,000.

In addition, these extra floors will cause the tower to exceed the 240’ height limit imposed by the California Building Code, which requires a redundant structural support and other code restrictions for high rises over 240 ft. Buildings above that height are approximately \$10 per square foot more expensive to construct (when applied to the entire building area). We have enclosed a letter from Swinerton Builders, the project General Contractor, that documents the additional cost at approximately \$4,500,000 for this project.

The additional square footage plus the higher cost of construction to comply with the 50% coverage restrictions would result in a total financial impact to the project of \$8,050,000 and cause the project to be financially infeasible. The proposed deviation for maximum lot coverage provides the smallest possible encroachment into the required setbacks. In addition, the deviation allows the project to maximize residential dwelling units and achieve the greatest floor to area ratio acceptable to the leasing and financing markets given the historic building and parking demands, all in continuance of the General Plan and Downtown Community Plan policies and goals supporting construction of residential dwelling units and high floor area ratios.

The city has requested a full block massing study to be provided to demonstrate how the remainder of the block can be developed and in compliance with the policies in the Downtown Community Plan (5.3-G.1 and 5.3-G.3), CCPDO (bulk standards for towers), and Downtown Design Guidelines (4.4.1.E). Exhibit ‘A’ of these findings shows two scenarios demonstrating potential development for the remaining lots.

Scenario 1 includes all remaining lots of the southern portion of the block totaling 35,000 s.f. for one development containing a tower that maximizes the tower lot coverage at 50%. The future tower maintains a 40 ft separation from the tower located on proposed development

Scenario 2 includes all remaining lots of the southern portion divided into two separate developments on a 15,000 s.f site and a 20,000 s.f. site. The future towers maintains a 40 ft separation from the tower located on proposed development, and can provide a tower floorplate ranging from 7,500 to 10,000 s.f. Because these sites are oriented North-South, it meets all design guidelines and PDO regulations.

In addition to the massing study, further investigation of other developments under similar conditions have been done to demonstrate how deviations were requested in order to meet density goals. Most applicants who are proposing a tower design on sites between 10,000 to 25,000 square feet have requested a deviation for the limited Tower width and/or Lot Coverage. These sites listed below show a demand for the deviations needed for similar projects.

Project	Current Status	Site Area	Site Orientation	FAR	Tower Width	Lot Coverage	Deviations/ Bonus Incentives
3 rd and A	Construction	25,000	East/West	15.9	133 ft	54-77%	Lot Coverage
Luma	Built	20,000	North/South	13.8	75 ft	66.0%	Lot Coverage
India & Beech	Review	10,000	Square	16.7	85 ft	63.8%	Lot Coverage Tower Setback View Corridor Setback
1122 4 th Ave. (California Theater)	Review	25,000	East/West	23	151 ft	48.7%	Tower Width
13 th & Broadway	Construction	20,000	East/West	8.8	159.3 ft	60.0%	Tower Width Lot Coverage

3rd and A and Luma were both granted Lot Coverage deviations and did not provide on-site affordable, choosing instead to pay the in-lieu fee. While other applicants provided on-site affordable and obtained affordable housing bonus incentives to automatically obtain the additional lot coverage, it clearly demonstrates that the lot coverage restriction in the development standards for a tower design do not accommodate smaller sites, making it difficult to provide a workable floor plate and nearly impossible to make the financials work.

Floor Plate Dimensions Tower

The municipal code Sec. 156.0310 (d)(3)(B) and Table 156-0310-A, states that the maximum East-West tower floor plate dimensions shall be 130 feet. The applicant would like to request a deviation to increase the East-West dimension to 150 feet. The required maximum East-West dimension for the tower will reduce the number of residential dwelling units and FAR density that can be achieved. As demonstrated in the survey, a majority of the proposed sites where tower orientation is dictated by the site in the East-West, a tower width increase has been requested. Due to our specific site constraints this conflicts with the intentions of the PDO which encourages tower development to be in a North-South orientation. To achieve the proposed number of residential dwelling units and strictly comply with the dimension for the tower floorplate would require either additional encroachments into the setback requirements or a reduced floor plate and additional floors to be added to the tower resulting in a taller building that would significantly increase the cost of construction and adversely impact the financial feasibility of the project. In addition, the deviation allows the project to provide additional residential dwelling units and to achieve a floor to area ratio.

Transparency of Façade

The municipal code Sec. 156.0311(d)(1) states that “A minimum of 60 percent of the street-facing building façade containing non-residential uses between 3 and 12 feet above the sidewalk shall be comprised of clear, non-reflective windows that allow views of indoor space.” The applicant would like to request a deviation to reduce the transparency of the 10th Avenue façade length to 40 percent.

The preservation of the existing historic façade has created constraints for utility services needed to develop the site. The existing façade has limited penetrations which requires utilities to be placed in certain specific locations. The utilities along 10th Avenue include standard Back Flow Prevention Devices, and per city standards need to be spaced with proscribed separation distances to allow for servicing and separate points of connections.

Further investigations have been done including the consulting from Save Our Heritage Organization and Historic Review Board, which determined that the only way to increase the transparency would be to alter existing or add new openings to the historic façade which would harm the preservation. These proposed changes would not be supported by HRB or SOHO.

The relevant Land Development Code’s Planning and Development Regulations for topics not addressed in the Centre City Planned District Ordinance are contained in that Code’s Chapter 14 and include: Grading Regulations, Draining Regulations,

Landscape Regulations, Parking Regulations, Refuse and Recyclable Materials Storage, Mechanical and Utility Equipment Storage Regulations, Loading Regulations, Building Regulations, Electrical Regulations and Plumbing Regulations. The proposed development will comply with all of these regulations, since a building permit would not be issued without such compliance. Therefore, the proposed development will comply with all applicable regulations of the Land Development Code.

In conclusion, we believe it is justifiable for the City to grant Cisterra the requested PDO deviations of a larger tower lot coverage, wider building, and no tower setbacks from the refurbished historic building because of the burden of maintaining the historic building within the project. The added cost of shoring up to and under the building façade to maintain that façade during excavation and construction of the subterranean basement level is approximately \$2 Million more than if we were permitted to simply tear down the building and excavate without it there. To accommodate that added cost and provide the benefit to the City of maintaining a historic resource we need to avoid the added costs of additional FAR and of exceeding the 240' height threshold which adds approximately \$8,000,000 to the project costs.

(f) Supplemental Findings – Affordable Housing, In-Fill Projects, or Sustainable Buildings Deviation.

Supplemental Finding (1) The development will materially assist in accomplishing the goal of providing affordable housing, in-fill projects, or sustainable buildings opportunities

The proposed development will comply with the applicable provisions of the Centre City Planned District Ordinance and the San Diego Downtown Community Plan for the purpose of an “In-fill” project and “Sustainable Building”.

Chapter 5 of the SDDCP encourages development “to fill existing gaps and provide new centers for all of downtown’s neighborhoods, to ensure that virtually all residents will be within less than a ten-minute walk from everyday amenities”. Section 143.0915(b)(2) qualifies an in-fill project as “Residential or mixed-use development, where all or a portion of the premises is located within a Transit Priority Area”. The proposed project complies with these provisions.

Section 143.0915 defines Sustainable Buildings as “Residential and non-residential development projects that incorporate the Voluntary Tier 2 Measures of Title 24, Part 11 California Green Building Standards Code in effect at the time the building permit

application is deemed complete”. In addition to incorporating Tier 2 Measure of Title 24, the proposed project will be applying for LEED Silver.

Supplemental Finding (2) Any proposed deviations are appropriate for the proposed location.

The proposed development will comply with the applicable provisions of the Centre City Planned District Ordinance with approval of the deviations, which are appropriate to the site and specific conditions and preservation the existing historic façade.

The proposed deviations requested as a result of the site constraints will make it possible for the applicant to construct a high density and quality residential project on a currently underutilized site, and consistent with the Downtown Community Plan. The project will result in a distinct design compatible with the surrounding neighborhood while furthering the ability to meet employment and residential targets as anticipated in the DCP.

The deviations listed in section 3 of the findings comply with the Supplemental Development Regulations Section 143.0920, and complies with the requirements of the Historical Resources Regulations section 143.0220.



Cisterra Development
3580 Camel Mountain Road
San Diego, CA 92130

May 28, 2020

Paul,

Swinerton Builders has been assisting Cisterra Development in developing a cost-effective program to build market rate apartments on the Farkas Building site. We have advised them that it is more cost effective if you can stay below a 240' height.

When you exceed that height the prescriptive code requires that you design your building with moment frame and a shear wall frame, creating a dual frame structural system. An alternative solution is to use performance-based design. Performance Based Design utilizes a peer review panel of engineers to validate structural calculations for a shear wall frame. This solution requires heavier reinforcing and higher concrete strengths among other things to allow the building to perform properly against lateral forces. This is the least expensive way to build above 240'.

Other factors that have cost impacts to building over this height include exterior skin in the form of higher balcony rails, window washing, higher elevator travel speeds and wet fire sprinkler costs. We are currently in preconstruction on 3 market rate apartment towers in downtown San Diego that exceed the 240' height. Our history shows the premium for buildings over the 240' height is roughly \$10 SF over the entire gross building area including garage space, which translates to approximately \$4,500,000 for this project.

The groups we have worked with have spread the premium costs over a larger number of floors and units to help offset the impact to the project and generate additional revenue. The height of the buildings we have priced that exceed the 240' height have typically been over 400'.

We have benchmarked these taller towers against several recently completed projects in downtown San Diego under the 240' limit such as Luma, Shift, and Alexan and advised Cisterra of the cost premiums accordingly.

Swinerton Builders

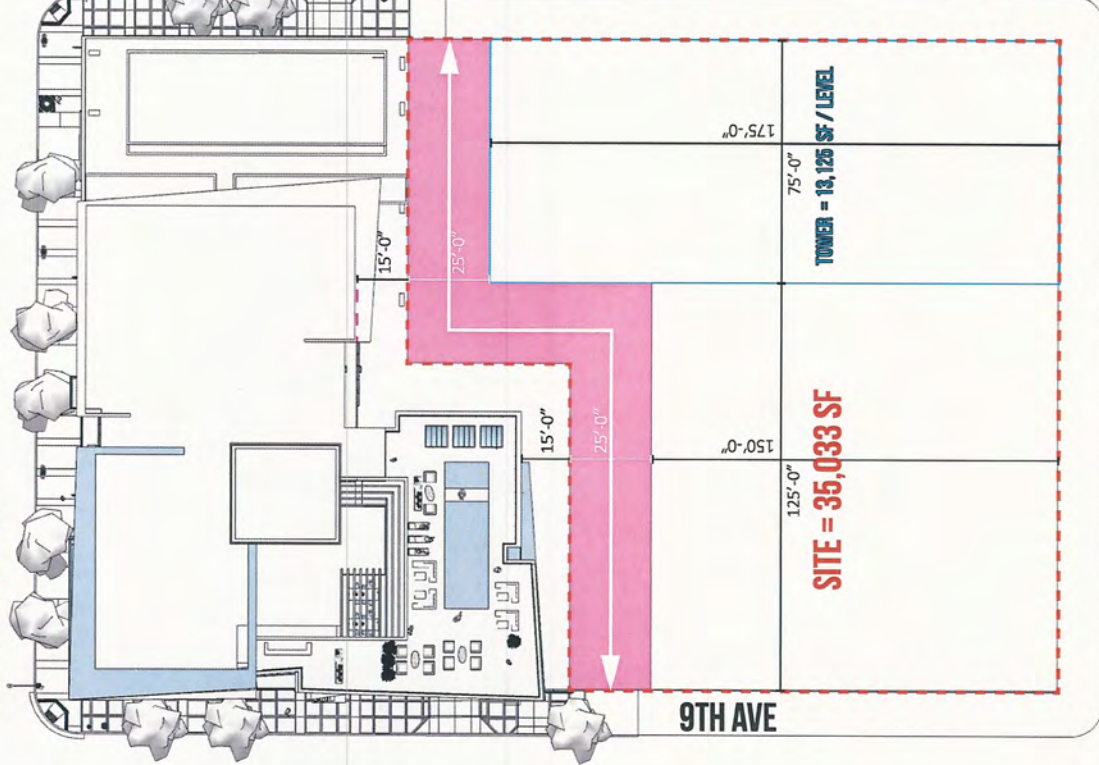
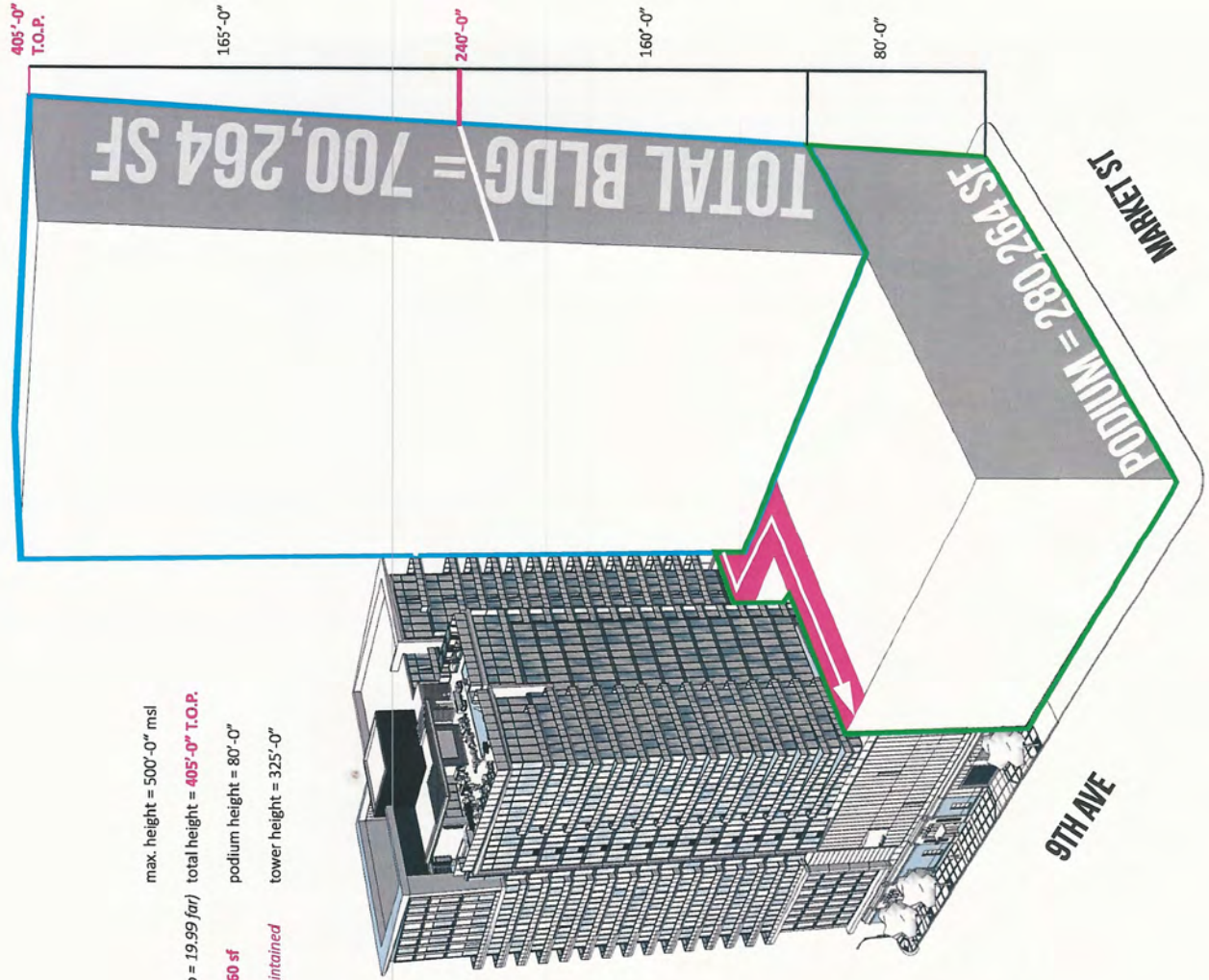
Randy Vandewater
Director of Preconstruction

Exhibit 'A'
Massing Study

SCENARIO 1: ALL REMAINING PARCELS IN ONE DEVELOPMENT

ZONING HIGHLIGHTS

OT size = 35,033 sf
 max. height = 500'-0" msl
 max. far = 20.0 far (scenario = 19.99 far)
 total height = 405'-0" T.O.P.
 podium height = 80'-0"
 tower height = 325'-0"
 40'-0" tower separation maintained

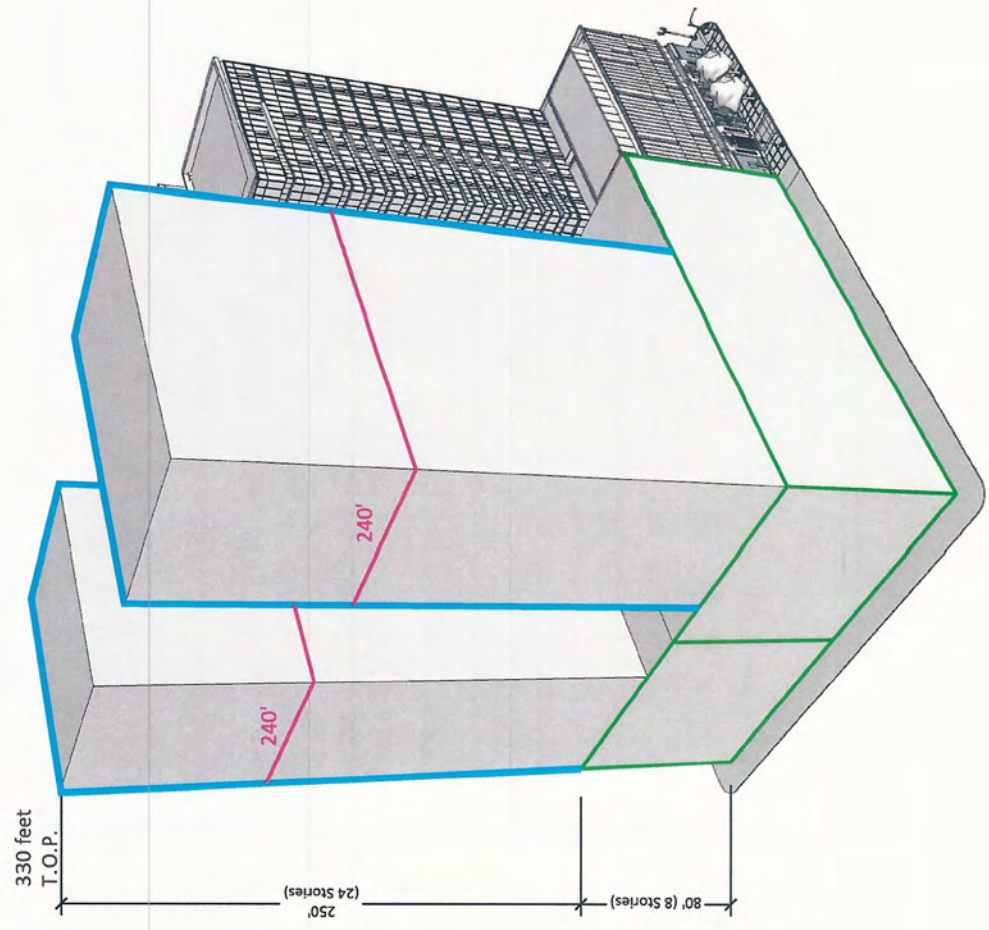
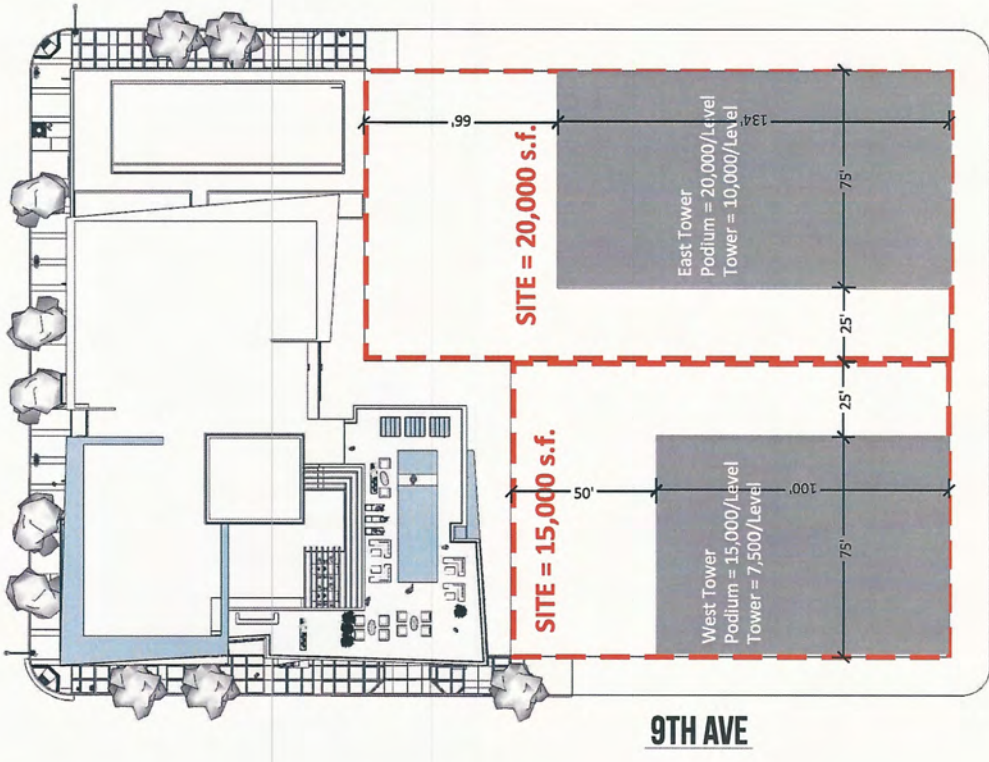


MARKET ST
 9G - CISTERRA carrierjohnson + CULTURE

SCENARIO 2: ALL REMAINING PARCELS IN TWO SEPARATE DEVELOPMENTS

West Lot
 Site = 15,000 s.f.
 FAR = 20
 Allowable Gross = 300,000 s.f.

East Lot
 Site = 20,000 s.f.
 FAR = 20
 Allowable Gross = 400,000 s.f.



MARKET ST

9TH AVE





london moeder
advisors

Cisterra — 9th & G
Economic Alternative Analysis

May 27, 2020

825 10th Avenue
San Diego, CA 92101
619. 269.4010

2792 Gateway Road #104
Carlsbad, CA 92024
619.269.4012

londonmoeder.com



May 27, 2020

Mr. Jason Wood
Cisterra Partners, LLC
3580 Carmel Mountain Road
Suite 460
San Diego, CA 92130

Via email: jwood@cisterra.com

RE: 9th Avenue & G Street – Economic Alternative Analysis

Cisterra Development is currently in the entitlement phase of redeveloping the parcel that houses the Farkas Fixtures Store, considered of historical significance by the City of San Diego. The site includes full frontage on G Street between 9th and 10th avenues and represents 25,000 square feet of the 60,000 square-foot block.

London Moeder Advisors has completed an economic analysis of various development alternatives for the property. The purpose of this analysis is to analyze the proposed Base Project and the financial impacts and economic feasibility of the development alternatives.

We have analyzed five development alternatives for the property, in addition to the Base Project, which include:

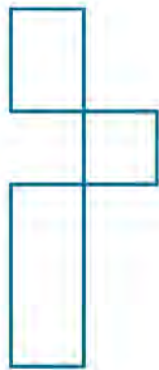
- ➔ **Base Project**: Maintain the existing façade of the historic structure, add one subterranean level with 19,500 net square feet of retail, construct five levels of parking above ground consisting of 276 spaces, and construct 16 levels of residential consisting of 242 units.
- ➔ **Alternative 1**: rehabilitate the existing 25,000 square foot historic structure and use as 22,000 square feet of net retail space.
- ➔ **Alternative 2**: Maintain the existing façade of the historic structure, add one subterranean level of retail for a total of 34,000 net square feet, construct a loading dock, and construct a parking ramp to roof parking consisting of 34 spaces.
- ➔ **Alternative 3**: Maintain the existing façade of the historic structure, add one subterranean level of retail for a total of 34,000 net square feet, construct five levels of parking above ground consisting of 189 spaces, and construct ten levels of residential consisting of 155 units.
- ➔ **Alternative 3B**: Maintain the existing façade of the historic structure, add one subterranean level of retail for a total of 34,000 net square feet, construct five levels of parking above ground consisting of 189 spaces, and construct 16 levels of residential consisting of 227 units, of which 72 units will not have designated parking within the building.
- ➔ **Alternative 4**: Maintain the existing façade of the historic structure, add five subterranean levels of parking consisting of 202 spaces, rehab ground floor retail and add one second

story level of retail for a total of 34,000 net square feet, and construct 11 levels of residential consisting of 168 units.

Conclusions of Economic Alternatives

We analyzed the project performance of the Base Project that is proposed for the property. The Base Project includes construction of one subterranean level of retail space, five levels of above ground parking, and 16 levels of residential units. The average size of the residential units is 905 square feet.

We have assumed a 21-month construction period with the lease up of residential units commencing immediately after completion, including two months of pre-leasing. The project will be sold after a five-year holding period. The following table summarizes the impacts to the Base Project under each of the five alternatives:



Cisterra - 9th & G
Summary of Scenarios

Base Project
22 Story Tower

<u>Rentable Space:</u>	
Residential	242 units
Parking	276 spaces
Retail	34,000 SF
Total Net Rentable	253,000 SF
Total Gross	426,650 SF

Total Profit	\$45,955,665
Stabilized Yield On Cost	5.6%
IRR	16%

Alternative 1
Refurbish Existing Building

<u>Rentable Space:</u>	
Residential	0 units
Parking	0 spaces
Retail	22,000 SF
Total Net Rentable	22,000 SF
Total Gross	25,000 SF

Total Profit	\$2,757,756
Difference from Base (\$)	-\$43,197,909
Difference from Base (%)	-94.0%
Stabilized Yield On Cost	3.9%
IRR	9%

Alternative 2
Retain Façade + Rooftop Parking

<u>Rentable Space:</u>	
Residential	0 units
Parking	34 spaces
Retail	34,000 SF
Total Net Rentable	34,000 SF
Total Gross	42,000 SF

Total Profit	(\$2,538,708)
Difference from Base (\$)	-\$48,494,373
Difference from Base (%)	-105.5%
Stabilized Yield On Cost	3.4%
IRR	-4%

Alternative 3
16 Story Tower

<u>Rentable Space:</u>	
Residential	155 units
Parking	189 spaces
Retail	34,000 SF
Total Net Rentable	173,400 SF
Total Gross	298,060 SF

Total Profit	\$15,145,802
Difference from Base (\$)	-\$30,809,863
Difference from Base (%)	-67.0%
Stabilized Yield On Cost	4.7%
IRR	7%

Alternative 3B
22 Story Tower

<u>Rentable Space:</u>	
Residential	227 units
Parking	189 spaces
Retail	34,000 SF
Total Net Rentable	238,325 SF
Total Gross	373,069 SF

Total Profit	\$30,600,518
Difference from Base (\$)	-\$15,355,147
Difference from Base (%)	-33.4%
Stabilized Yield On Cost	4.7%
IRR	10%

Alternative 4
13 Story Tower

<u>Rentable Space:</u>	
Residential	168 units
Parking	202 spaces
Retail	34,000 SF
Total Net Rentable	185,200 SF
Total Gross	347,400 SF

Total Profit	(\$12,279,294)
Difference from Base (\$)	-\$58,234,959
Difference from Base (%)	-126.7%
Stabilized Yield On Cost	3.9%
IRR	-6%

Source: London Moeder Advisors

We have determined that only the Base Project is economically feasible. This project is forecasted to generate a total profit of approximately \$46 million. In addition, this project is estimated to generate a Net Operating Income ("NOI") at stabilization of \$7,954,860, which when compared to the total costs of the project represents a Yield On Cost ("YOC") of 5.6%.

Based on performing feasibility analyses and consulting services on hundreds of real estate projects, it is our experience that a redevelopment project requires the Yield On Costs spread over existing cap rates to be 1.5% to be economically feasible and to qualify for project financing. Meaning if cap rates are approximately 4% for residential projects and approximately 6% for retail projects the targeted YOC is 5.5% and 7.5% respectively.

The internal rate of return ("IRR") of the Base Project is forecasted to be 16.5%. This also demonstrates that the project is economically feasible. The typical minimum IRR for rental housing projects ranges from 13% to 15%. Any IRR below this range would struggle to attract investors and achieve project financing.

Alternative 1 is not economically feasible. The refurbishing of the existing retail building results in an NOI at stabilization of \$566,620. When compared to the total costs of the project the YOC is 3.9%, which is below the 7.5% YOC threshold required. In addition, the total profit is reduced by \$43.2 million (-94.0%) compared to the Base Project.

Alternative 2 is not economically feasible. Due to the high cost of subterranean construction without the addition of sufficient revenue producing density, the estimated NOI at stabilization is \$952,796. When compared to the total costs of the project the YOC is 3.4%, which is below the 7.5% YOC threshold required. In addition, the total profit is reduced by \$48.5 million (-105.5%) compared to the Base Project. This alternative also results in a financial loss for the developer of \$2,538,708.

Alternative 3 is not economically feasible. Due to the expensive costs of high-rise construction without the addition of sufficient revenue producing density, the estimated NOI at stabilization is \$4,973,574. When compared to the total costs of the project the YOC is 4.7%, which is below the 5.5% YOC threshold required. The total profit in this alternative is also reduced by \$30.8 million (-67.0%) compared to the Base Project. In addition, the IRR for this alternative is only 7.2%, which is below the minimum targeted IRR of 13% to 15%.

Alternative 3B is not economically feasible. Due to the expensive costs of high-rise construction without the addition of sufficient revenue producing density, the estimated NOI at stabilization is \$6,482,227. When compared to the total costs of the project the YOC is 4.7%, which is below the 5.5% YOC threshold required. The total profit in this alternative is also reduced by \$15.4 million (-33.4%) compared to the Base Project. In addition, the IRR for this alternative is only 10.2%, which is below the minimum targeted IRR of 13% to 15%. A critical factor in this alternative is that 72 units will not have parking spaces provided. This represents a financial challenge because no projects have been financed without parking. It is likely that financing terms could be less favorable than the assumptions in this report for projects without parking due to increased marketability risk.

Alternative 4 is not economically feasible. Due to the expensive costs of high-rise and subterranean construction without the addition of sufficient revenue producing density and increased rents, the estimated NOI at stabilization is \$4,907,520. When compared to the total costs of the project the YOC is 3.9%, which is below the 5.5% YOC threshold required. The total profit in this alternative is also reduced by \$58.2 million (-126.7%) compared to the Base Project. In addition, the IRR for this alternative is negative 6% and represents a financial loss for the developer of an estimated \$12.3 million.

Approach to Analysis

To determine the impact to the project, we prepared financial proformas for the five alternatives and compared the performances to the Base Project proforma. In each proforma, we assumed the following:

- Construction period of 21 months, 6 months, 15 months, 21 months, 19.25 months, and 23 months for the Base Case and Alternative 1 to Alternative 4, respectively.
- Rental residential units begin leasing immediately after construction is completed with two months of pre-leasing.
- Construction costs are provided by the developer and London Moeder Advisors based on similar projects and construction types.
- Rental rates and revenues were established by our survey of market rents for competitive projects in the area.
- Residential rental units that do not offer parking are discounted at an average of \$200 per month to reflect the cost of finding replacement parking within a four-block radius. The typical cost of monthly parking in a four-block radius ranges from \$165 to \$220 per space.
- Residential rental units that offer parking is estimated to stabilize at a 4.5% vacancy rate.
- Residential rental units that do not offer parking are more difficult to lease than those with parking; therefore, we have assumed these units will stabilize at a 10% vacancy rate.
- Rental residential units achieve a \$50 per floor rent premium per month. Top floor units achieve a \$100 per floor rent premium per month.
- The following summarizes the financial proformas we have prepared for analyzing the project, which are included in the [Appendix](#).

Base Project

The Base Project includes retaining the existing façade of the historic structure, the addition of one subterranean level of retail space, construction of five levels of parking above the existing structure, and 16 levels of residential units above the levels of parking. The 242 residential units are to begin leasing after construction is complete with two months of pre-leasing. The project is to be sold after a five-year holding period.

The 242 market rate units include an average of 905 square feet. There will be a total of 276 parking spaces included in the parking levels of the building. In addition, there will be 19,500 square feet of below grade retail and 14,500 square feet of retail on the first level of the building.

When the 242 market rate units are leased after construction is completed, the forecasted average rent is estimated to be \$3.72 per square foot. The 34,000 square feet of total retail space is estimated to rent at \$3.37 per square foot.

The total gross profit generated from this investment is forecasted to be \$45,955,665. In addition, this project is estimated to generate an NOI at stabilization of \$7,954,860, which when compared

to the total costs of the project represents a YOC of 5.6%, which satisfies the minimum requirement of 5.5% for project feasibility.

The IRR of the investment is forecasted to be 16.5%. This also demonstrates that the project is economically feasible. The typical minimum IRR for rental housing projects ranges from 13% to 15%. Any IRR below this range would struggle to attract investors and achieve project financing.

Alternative 1

Alternative 1 assumes rehabilitation of the existing warehouse retail structure (25,000 square feet) and returning the space to retail operations. The retail space is assumed to be leased immediately after construction is completed and the project sold after a five-year holding period.

The 22,000 square feet of retail space is estimated to rent at \$2.75 per square foot (today's dollars).

The forecasted sale price for the entire project is \$14,537,297. Total project costs are forecasted at \$14,627,435.

With a total forecasted profit at disposition of \$2,757,756, Alternative 1 would generate approximately \$43,197,909 less total profit than the Base Project (94% reduction). But more importantly the project is not economically feasible because the forecasted YOC of 3.9% is below the minimum requirement of 7.5% for retail properties.

Alternative 2

Alternative 2 assumes retaining the existing façade of the historic structure, the addition of one subterranean level of retail space, and the replacement of the existing roof structure with rooftop parking. The retail space is assumed to be leased immediately after construction is completed and the project sold after a five-year holding period.

The 34,000 square feet of retail space is estimated to rent at \$3.00 per square foot (today's dollars).

The forecasted sale price for the entire project is \$24,509,162. Total project costs are forecasted at \$27,989,507. This results in a financial loss for the project, which is forecasted to be \$2,538,708 and therefore not economically feasible.

With a total forecasted profit at disposition of negative \$2,538,708, Alternative 2 would generate approximately \$48,494,373 less total profit than the Base Project (105% reduction).

Alternative 3

Alternative 3 assumes retaining the existing façade of the historic structure, the addition of one subterranean level of retail space, construction of five levels of parking above the existing structure, and 10 levels of residential units above the levels of parking. The 155 residential units are to begin

leasing immediately after construction is complete with two months of pre-leasing. The project is assumed to be sold after a five-year holding period.

The 155 market rate units average 899 square feet in size. There will be a total of 189 parking spaces included in the parking levels of the building. In addition, there will be 19,500 square feet of below grade retail and 14,500 square feet of retail on the first level of the building.

When the 155 market rate units are leased after construction is completed, the forecasted average rent is estimated to be \$3.56 per square foot. The 34,000 square feet of total retail space is estimated to rent at \$3.37 per square foot.

The forecasted sale price for the entire project is \$116,078,147. Total project costs are forecasted at \$105,590,612.

With a total forecasted profit at disposition of \$15,145,802, Alternative 3 would generate approximately \$30,809,863 less total profit than the Base Project (67% reduction). But more importantly the project is not economically feasible because the forecasted YOC (4.7%) does not meet the minimum required of 5.5% to be feasible.

To further illustrate the infeasibility of Alternative 3, the IRR of this project is forecasted to be 7%. This also demonstrates that the project is infeasible because an IRR below 13% to 15% will struggle to attract investors and qualify for project financing.

Alternative 3B

Alternative 3B assumes retaining the existing façade of the historic structure, the addition of one subterranean level of retail space, construction of five levels of parking above the existing structure, and 16 levels of residential units above the levels of parking. The 227 residential units are to begin leasing immediately after construction is complete with two months of pre-leasing. The project is assumed to be sold after a five-year holding period.

The 227 market rate units average 900 square feet in size. There will be a total of 189 parking spaces included in the parking levels of the building. In addition, there will be 19,500 square feet of below grade retail and 14,500 square feet of retail on the first level of the building.

When the 227 market rate units are leased after construction is completed, the forecasted average rent is estimated to be \$3.73 per square foot for units with parking and \$3.50 per square foot for units without parking. The 34,000 square feet of total retail space is estimated to rent at \$3.37 per square foot.

The forecasted sale price for the entire project is \$159,276,163. It is important to note that the assumed exit cap rate is 4.75% (25 basis point above market) to reflect the 72 units that do not have parking provided. Projects that do not provide parking represent increased marketability risk to the developer and lender. Total project costs are forecasted at \$136,477,630.

With a total forecasted profit at disposition of \$30,600,518, Alternative 3B would generate approximately \$15,355,147 less total profit than the Base Project (33.4% reduction). But more importantly the project is not economically feasible because the forecasted YOC (4.7%) does not meet the minimum required of 5.5% to be feasible.

To further illustrate the infeasibility of Alternative 3B, the IRR of this project is forecasted to be 10.2%. This also demonstrates that the project is infeasible because an IRR below 13% to 15% will struggle to attract investors and qualify for project financing.

A critical factor in this alternative is that 72 units will not have parking spaces provided. This represents a financial challenge because no projects have been financed without parking. It is likely that financing terms could be less favorable than the assumptions in this report for projects without parking due to increased marketability risk.

Alternative 4

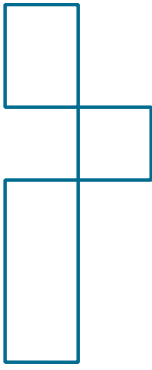
Alternative 4 assumes retaining the existing façade of the historic structure, the addition of five subterranean levels of parking, rehabilitation of ground floor retail and the addition of one second story level of retail for a total of 34,000 net square feet, and construction of 11 levels of residential units above. The 168 residential units are to begin leasing immediately after construction is complete with two months of pre-leasing. The project is assumed to be sold after a five-year holding period.

The 168 market rate units average 900 square feet in size. There will be a total of 202 parking spaces included in the parking levels of the building. In addition, there will be 14,000 square feet of retail on the first level, and 20,000 square feet of retail on the second level of the building.

When the 168 market rate units are leased after construction is completed, the forecasted average rent is estimated to be \$3.30 per square foot. The 34,000 square feet of total retail space is estimated to rent at \$3.37 per square foot.

The forecasted sale price for the entire project is \$110,749,569. Total project costs are forecasted at \$124,727,164. This results in a financial loss for the project, which is forecasted to be \$12,279,294 and therefore not economically feasible.

With a total forecasted loss at disposition of \$12,279,294, Alternative 4 would generate approximately \$58,234,959 less total profit than the Base Project (127% reduction). In addition, the IRR is forecasted to be negative 6%, and will therefore struggle to attract investors and qualify for project financing.



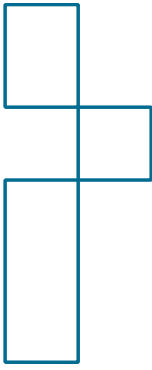
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Should you have any questions regarding this analysis, please feel free to contact our firm.

Sincerely,

Gary H. London

Nathan Moeder



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Appendix

Cisterra - 9th & G
Base Project - 22 Story Tower
Assumptions & Results

HOLDING & DISPOSITION

Holding Period:	5
Cap Rate On Sale (Residential):	4.50%
Cap Rate On Sale (Retail):	5.50%
Commissions & Closing Costs:	1.00%
Value at Time of Sale (Year 5)	\$0
Asset Value PSF	\$0

BUILDING ASSUMPTIONS

Project FAR	17.07
Units Per Acre	422
Land S.F.	25,000
Building Gross SF	426,650
Building FAR/NSF Efficiency	59.30%
Net Rentable Area	253,000
Parking Spaces	276
Total # of Units	242

FINANCING

Construction Financing:	
Loan Amount	\$92,824,932
Loan to Cost	65%
Interest Rate	5.75%
Term (Months)	24
Refinance:	YES
Refinance at End of Year:	2
Permanent Loan Amount	\$112,165,274
Less: Construction Loan	(\$92,824,932)
Less: Loan Fees	0.50% (\$560,826.37)
Net Proceeds From Refinance	\$18,779,516
Permanent Loan Info:	
Loan Amount	\$112,165,274
Amortization	30
Interest Rate	4.5%
Annual Debt Service	\$6,819,900
Next Year NOI @ Refi	\$7,210,625
Value at Refi	\$160,236,106
Loan To Value	70%
Debt Coverage Ratio	1.17
Debt Yield	6.43%

RESIDUAL LAND VALUE

Land S.F.	25,000
Land Value	\$10,000,000
\$/S.F. of Land	\$400
\$/Unit	\$41,322

PROJECT SUMMARY

Residential Market Rate	# of Units	% of Total Mix	Avg. Unit Size	Total Net Rentable	Avg. Monthly Rent	Total Annual Rent	\$/S.F. Rent
Studio	60	24.8%	700	42,000	\$2,500	\$1,800,000	\$3.57
1 Bed	52	21.5%	800	41,600	\$3,000	\$1,872,000	\$3.75
1 Bed + D	65	26.9%	900	58,500	\$3,300	\$2,574,000	\$3.67
2 Bed	40	16.5%	1,110	44,400	\$3,600	\$1,728,000	\$3.24
3 Bed	25	10.3%	1,300	32,500	\$4,100	\$1,230,000	\$3.15
Subtotal	242	100.0%	905	219,000	\$3,169	\$9,204,000	\$3.50
Affordable Units ¹							
Studio	0	0.0%	0	0	\$0	\$0	
Small 1 Bed	0	0.0%	0	0	\$0	\$0	
Large 1 Bed	0	0.0%	0	0	\$0	\$0	
2 Bed	0	0.0%	0	0	\$0	\$0	
Subtotal	0	0.0%	0	0	\$0	\$0	
Total	242	100.0%	905	219,000		\$9,204,000	

Parking	Space	Avg.	Total
	276 spaces		
Retail	Monthly Rent	Monthly Rent	Annual Rent
	\$4,000 SF	\$0	\$0
Other Income		\$3.18 PSF	\$1,297,440
		\$0	

CONSTRUCTION COSTS

	Total Cost	Cost Per Unit	Cost Per Gross S.F.
Land Costs	\$10,000,000	\$41,322	\$23.44
Hard Costs			
Below Grade Retail	\$3,987,750	\$16,478	\$204.50
Above Grade Parking	\$11,637,500	\$48,089	\$95.00
Retail	\$2,240,250	\$9,257	\$154.50
Amenity	\$1,150,000	\$4,752	\$230.00
Outdoor Amenity	\$1,740,000	\$7,190	\$217.50
Type I Residential	\$64,298,875	\$265,698	\$242.50
Site	\$1,187,500	\$4,907	\$47.50
GC/GR Mark-Ups	\$14,130,735	\$58,391	\$33.12
Hard Costs Subtotal	\$100,372,610	\$414,763	\$235.26
Soft Costs	20.0%	\$20,074,522	\$82,953
			\$47.05
Contingency	5.0%	\$6,022,357	\$24,886
Financing		\$6,338,100	\$26,190
			\$14.86
Total Project Costs	\$142,807,588	\$590,114	\$334.72
Less: Loan Amount	\$92,824,932	\$383,574	\$217.57
Initial Investment:	\$49,982,656	\$206,540	\$117.15

INVESTMENT PERFORMANCE

Stabilized NOI	Year 4	\$7,954,860
Total Project Costs		\$142,807,588
Stabilized Yield On Cost		5.57%
Return on Equity		Cash Flow
Initial		(\$49,982,656)
Year 1	0.0%	\$0
Year 2	37.6%	\$18,779,516
Year 3	0.6%	\$318,619
Year 4	2.1%	\$1,055,412
Year 5	151.6%	\$75,784,774
Total Profit		\$45,955,665
Before Tax IRR		16.5%

¹ @ 100% AMI
Source: London Moeder Advisors

	Initial	Year 1 2020	Year 2 2021	Year 3 2022	Year 4 2023	Year 5 2024	Year 6 2025
	0	1	2	3	4	5	6
Total Market Rate Units		Construction	Construction	242	242	242	242
Units Leased (Market Rate)				217	231	231	231
Units Leased (Affordable)				0	0	0	0
Units Vacant (Market Rate)				26	11	11	11
Occupancy Rate (Market Rate)				89.5%	95.5%	95.5%	95.5%
Vacancy Rate (Market Rate)				10.5%	4.5%	4.5%	4.5%
Monthly Rent (Market Rate)		\$3,169	\$3,265	\$3,362	\$3,463	\$3,567	\$3,674
Monthly Rent Per S.F. (Market Rate)		\$3.50	\$3.61	\$3.72	\$3.83	\$3.94	\$4.06
Monthly Rent Per space (Parking)		\$0	\$0	\$0	\$0	\$0	\$0
Monthly Rent Per S.F. (Retail)		\$3.18	\$3.28	\$3.37	\$3.47	\$3.58	\$3.69
Annual Increase In Rent			3.0%	3.0%	3.0%	3.0%	3.0%
Gross Rental Income (Market Rate Units)			\$9,764,524	\$10,057,459	\$10,359,183		\$10,669,959
Gross Rental Income (Affordable Units)							\$0
Gross Parking Income			\$0	\$0	\$0	\$0	\$0
Gross Retail Income (NNN)			\$1,376,454	\$1,417,748	\$1,460,280		\$1,504,089
Other Income			\$0	\$0	\$0	\$0	\$0
Less: Vacancy & Credit Loss (Parking)							\$0
Less: Vacancy & Credit Loss (Retail)				5.0%			
Less: Vacancy & Credit Loss (Residential)				5.0%			
Subtotal: Net Rental Income			\$10,043,249	\$10,947,162	\$11,276,577		\$11,613,845
Total: Net Rental Income			\$10,043,249	\$10,947,162	\$11,275,577		\$11,613,845
Less: Operating Expenses ¹	Per Unit	% Increase					
Less: Property Taxes ²	(\$6,000)	2.5%	\$0	\$0	(\$1,529,885)	(\$1,568,132)	(\$1,607,335)
Operating Expenses - Residential	(\$5,583)	2.0%	\$0	\$0	(\$1,462,418)	(\$1,491,666)	(\$1,521,499)
Net Operating Income	(\$11,583)		\$0	\$0	(\$2,992,302)	(\$3,059,798)	(\$3,128,834)
Less: Capital Expenditures			\$0	\$0	\$7,954,860	\$8,215,779	\$8,485,010
Less: I/O (interim) financing			\$0	\$0	(\$79,549)	(\$82,158)	(\$84,850)
Less: Permanent Debt Service			\$0	\$0	\$0	\$0	\$0
Subtotal			\$0	\$0	(\$6,819,900)	(\$6,819,900)	\$0
Net Proceeds from Refinance:			\$0	\$0	(\$6,892,006)	(\$6,902,057)	(\$84,850)
Cash Flow From Operations			\$0	\$18,779,516	\$0	\$0	\$0
			\$0	\$18,779,516	\$1,055,412	\$1,313,722	\$8,400,160

Disposition	Residential (incl. parking)
Cap Rate	4.50%
Next Year NOI	\$7,056,126
Asset Value	\$156,802,800
Asset Value Per Net SF	\$620
Asset Value Per Unit	\$647,945
Retail	
Cap Rate	5.50%
Next Year NOI	\$1,428,884
Asset Value	\$25,979,711
Asset Value Per Space	\$94,129
Sale Price	\$182,782,511
Less: Commissions & Closing Costs	(\$1,827,825)
Less: Principal Balance of Loan O/S	(\$106,483,634)
Net Proceeds from Disposition	\$74,471,052
Total Cash Flow Before Taxes	(\$49,982,656)
	\$0
	\$18,779,516
	\$318,619
	\$1,055,412
	\$75,784,774

IRR	16%
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Notes:

¹ \$600 per unit per month

² 1.1% of 90% of construction costs

Cisterra - 9th & G
Alternative 1 - Refurbish Existing Building
Assumptions & Results

HOLDING & DISPOSITION

Holding Period:	5
Cap Rate On Sale (Residential):	4.50%
Cap Rate On Sale (Retail):	5.50%
Commissions & Closing Costs:	1.00%
Value at Time of Sale (Year 5)	\$0
Asset Value PSF	\$0

BUILDING ASSUMPTIONS

Project FAR	1.00
Land S.F.	25,000
Building Gross SF	25,000
Building FAR/NSF Efficiency	88.00%
Net Rentable Area	22,000
Parking Spaces	0

FINANCING

Construction Financing:	
Loan Amount	\$9,507,833
Loan to Cost	65%
Interest Rate	5.75%
Term (Months)	24
Refinance:	YES
Refinance at End of Year:	1
Permanent Loan Amount	\$7,554,935
Less: Construction Loan	(\$9,507,833)
Less: Loan Fees	0.50% (\$37,774.68)
Net Proceeds From Refinance	(\$1,990,672)
Permanent Loan Info:	
Loan Amount	\$7,554,935
Amortization	30
Interest Rate	4.5%
Annual Debt Service	\$459,357
Next Year NOI @ Refi	\$566,620
Value at Refi	\$12,591,559
Loan To Value	60%
Debt Coverage Ratio	1.23
Debt Yield	7.50%

RESIDUAL LAND VALUE

Land S.F.	25,000
Land Value	\$10,000,000
\$/S.F. of Land	\$400

PROJECT SUMMARY

	Space	Avg. Monthly Rent	Total Monthly Rent	Total Annual Rent
Parking	0 spaces	\$0	\$0	\$0
Retail	22,000 SF	\$2.75 PSF	\$60,500	\$726,000
Other Income	\$0			

CONSTRUCTION COSTS

	Total Cost	Per Gross S.F.	Cost
Land Costs	\$10,000,000	\$400.00	
Hard Costs			
Below Grade Retail	\$0	\$0.00	\$0.00
Above Grade Parking	\$0	\$0.00	\$0.00
Retail	\$2,137,500	\$85.50	\$85.50
Amenity	\$0	\$0.00	\$0.00
Outdoor Amenity	\$0	\$0.00	\$0.00
Type I Residential	\$0	\$0.00	\$0.00
Site	\$562,500	\$22.50	\$22.50
GC/GR Mark-Ups	\$441,580	\$17.50	\$17.50
Hard Costs Subtotal	\$3,141,580	\$125.66	\$125.66
Soft Costs	\$628,316	\$25.13	\$25.13
Contingency	\$188,495	\$7.54	\$7.54
Financing	\$669,045	\$26.76	\$26.76
Total Project Costs	\$14,627,435	\$585.10	\$585.10
Less: Loan Amount	\$9,507,833	\$380.31	\$380.31
Initial Investment:	\$5,119,602	\$204.78	\$204.78

INVESTMENT PERFORMANCE

Stabilized NOI	\$566,620	Year 2
Total Project Costs	\$14,627,435	
Stabilized Yield On Cost	3.87%	
Initial	Return on Equity	Cash Flow
Year 1	0.0%	(\$5,119,602)
Year 2	2.0%	\$0
Year 3	2.3%	\$101,597
Year 4	2.7%	\$119,849
Year 5	146.8%	\$138,677
Total Profit	\$2,757,756	\$7,517,235
Before Tax IRR	9.2%	

Cisterra - 9th & G
Alternative 1 - Refurbish Existing Building
Cash Flow Forecast

	Initial	Year 1 2020	Year 2 2021	Year 3 2022	Year 4 2023	Year 5 2024	Year 6 2025
	0	1	2	3	4	5	6
	Construction						
Monthly Rent Per space (Parking)		\$0	\$0	\$0	\$0	\$0	\$0
Monthly Rent Per S.F. (Retail)		\$2.75	\$2.83	\$2.92	\$3.00	\$3.10	\$3.19
Annual Increase In Rent			3.0%	3.0%	3.0%	3.0%	3.0%
Gross Parking Income			\$0	\$0	\$0	\$0	\$0
Gross Retail Income (NNN)			\$747,780	\$770,213	\$793,320	\$817,119	\$841,633
Other Income			\$0	\$0	\$0	\$0	\$0
Less: Vacancy & Credit Loss (Parking)							
Less: Vacancy & Credit Loss (Retail)							
			5.0%				
Subtotal: Net Rental Income			5.0%				
			(\$37,389)	(\$38,511)	(\$39,666)	(\$40,856)	(\$42,082)
			\$710,391	\$731,703	\$753,654	\$776,263	\$799,551
Total: Net Rental Income			\$710,391	\$731,703	\$753,654	\$776,263	\$799,551
Less: Property Taxes ¹							
			(\$138,188.06)	(\$146,646)	(\$149,579)	(\$152,571)	(\$155,622)
Operating Expenses			(\$138,188)	(\$146,646)	(\$149,579)	(\$152,571)	(\$155,622)
Net Operating Income			\$0	\$585,056	\$604,075	\$623,693	\$643,929
Less: Capital Expenditures			\$0				
Less: I/O (interim) financing			\$0	(\$5,851)	(\$6,041)	(\$6,237)	(\$6,439)
Less: Permanent Debt Service			\$0	\$0	\$0	\$0	\$0
			\$0	(\$459,357)	(\$459,357)	(\$459,357)	\$0
Subtotal			\$0	(\$465,208)	(\$465,398)	(\$465,594)	(\$6,439)
Net Proceeds from Refinance:			(\$1,990,672)	\$0	\$0	\$0	\$0
Cash Flow From Operations			(\$1,990,672)	\$101,597	\$138,677	\$158,099	\$637,490

Disposition	
Retail	
Cap Rate	5.50%
Next Year NOI	\$799,551
Asset Value	\$14,537,297
Asset Value Per SF	\$661
Sale Price	\$14,537,297
Less: Commissions & Closing Costs	(\$145,373)
Less: Principal Balance of Loan O/S	(\$7,032,787)
Net Proceeds from Disposition	\$7,359,137
Total Cash Flow Before Taxes	(\$5,119,602)
	\$0
	\$101,597
	\$138,677
	\$7,517,235

IRR	9%
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Notes:

¹ 1.1% of 90% of construction costs

Cisterra - 9th & G

Alternative 2 - Existing Retail + Rooftop Parking

Assumptions & Results

HOLDING & DISPOSITION

Holding Period:	5
Cap Rate On Sale (Residential):	4.50%
Cap Rate On Sale (Retail):	5.50%
Commissions & Closing Costs:	1.00%
Value at Time of Sale (Year 5)	\$0
Asset Value PSF	\$0

BUILDING ASSUMPTIONS

Project FAR	1.68
Land S.F.	25,000
Building Gross SF	42,000
Building FAR/NSF Efficiency	80.95%
Net Rentable Area	34,000
Parking Spaces	34

FINANCING

Construction Financing:	
Loan Amount	\$18,193,179
Loan to Cost	65%
Interest Rate	5.75%
Term (Months)	24
Refinance:	YES
Refinance at End of Year:	2
Permanent Loan Amount	\$12,703,941
Less: Construction Loan	(\$18,193,179)
Less: Loan Fees	0.50% (\$63,519,701)
Net Proceeds From Refinance	(\$5,552,759)
Permanent Loan Info:	
Loan Amount	\$12,703,941
Amortization	30
Interest Rate	4.5%
Annual Debt Service	\$772,428
Next Year NOI @ Refi	\$952,796
Value at Refi	\$21,173,234
Loan To Value	60%
Debt Coverage Ratio	1.23
Debt Yield	7.50%

RESIDUAL LAND VALUE

Land S.F.	25,000
Land Value	\$10,000,000
\$/S.F. of Land	\$400

PROJECT SUMMARY

	Space	Avg. Monthly Rent	Total Monthly Rent	Total Annual Rent
Parking	34 spaces	\$0	\$0	\$0
Retail	34,000 SF	\$3.00 PSF	\$102,000	\$1,224,000
Other Income	\$0			

CONSTRUCTION COSTS

	Total Cost	Per Gross S.F.	Cost
Land Costs	\$10,000,000		\$238.10
Hard Costs			
Below Grade Retail	\$3,501,225		\$204.75
Above Grade Parking	\$1,935,375		\$99.25
Retail	\$2,670,525		\$107.25
Amenity	\$0		\$0.00
Outdoor Amenity	\$0		\$0.00
Type I Residential	\$0		\$0.00
Site	\$937,500		\$37.50
GC/GR Mark-Ups	\$4,232,752		\$101.00
Hard Costs Subtotal	\$13,277,377		\$316.13
Soft Costs	\$2,655,475	20.0%	\$63.23
Contingency	\$796,643	5.0%	\$18.97
Financing	\$1,260,012		\$30.00
Total Project Costs	\$27,989,507		\$666.42
Less: Loan Amount	\$18,193,179		\$433.17
Initial Investment:	\$9,796,327		\$233.25

INVESTMENT PERFORMANCE

Stabilized NOI	Year 3	\$952,796
Total Project Costs		\$27,989,507
Stabilized Yield On Cost		3.40%
Initial	Return on Equity	Cash Flow
Year 1		(\$9,796,327)
Year 2	0.0%	\$0
Year 3	-56.7%	(\$5,552,759)
Year 4	1.7%	\$170,840
Year 5	2.1%	\$201,918
	127.0%	\$12,437,621
Total Profit		(\$2,538,708)
Before Tax IRR		-4.2%

Cisterra - 9th & G
Alternative 2 - Existing Retail + Rooftop Parking
Cash Flow Forecast

	Initial	Year 1 2020	Year 2 2021	Year 3 2022	Year 4 2023	Year 5 2024	Year 6 2025
	0	1	2	3	4	5	6
	Construction	Construction					
Monthly Rent Per space (Parking)	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Monthly Rent Per S.F. (Retail)	\$3.00	\$3.09	\$3.18	\$3.28	\$3.38	\$3.48	\$3.48
Annual Increase In Rent		3.0%	3.0%	3.0%	3.0%	3.0%	3.0%
Gross Parking Income			\$0	\$0	\$0	\$0	\$0
Gross Retail Income (NNN)			\$1,298,542	\$1,337,498	\$1,377,623	\$1,418,951	\$1,418,951
Other Income			\$0	\$0	\$0	\$0	\$0
Less: Vacancy & Credit Loss (Parking)							
Less: Vacancy & Credit Loss (Retail)							
	5.0%						
Subtotal: Net Rental Income			(\$64,927)	(\$66,875)	(\$68,881)	(\$70,948)	(\$70,948)
	5.0%		\$1,233,615	\$1,270,623	\$1,308,742	\$1,348,004	\$1,348,004
Total: Net Rental Income			\$1,233,615	\$1,270,623	\$1,308,742	\$1,348,004	\$1,348,004
Less: Property Taxes ¹		\$0	\$0	(\$280,819)	(\$286,435)	(\$292,164)	(\$298,007)
Operating Expenses		\$0	\$0	(\$280,819)	(\$286,435)	(\$292,164)	(\$298,007)
Net Operating Income		\$0	\$0	\$952,796	\$984,188	\$1,016,578	\$1,049,997
Less: Capital Expenditures		\$0	\$0	(\$9,528)	(\$9,842)	(\$10,166)	(\$10,500)
Less: I/O (interim) financing		\$0	\$0	\$0	\$0	\$0	\$0
Less: Permanent Debt Service		\$0	\$0	(\$772,428)	(\$772,428)	(\$772,428)	\$0
Subtotal		\$0	\$0	(\$781,956)	(\$782,270)	(\$782,594)	(\$10,500)
Net Proceeds from Refinance:		\$0	(\$5,552,759)	\$0	\$0	\$0	\$0
Cash Flow From Operations		\$0	(\$5,552,759)	\$170,840	\$201,918	\$233,984	\$1,039,497

Disposition	
Retail	5.50%
Cap Rate	\$1,348,004
Next Year NOI	\$24,509,162
Asset Value	\$720,858
Asset Value Per SF	\$24,509,162
Sale Price	(\$245,092)
Less: Commissions & Closing Costs	(\$12,060,433)
Less: Principal Balance of Loan O/S	\$12,203,637
Net Proceeds from Disposition	
Total Cash Flow Before Taxes	(\$9,796,327)
	\$0
	(\$5,552,759)
	\$170,840
	\$201,918
	\$12,437,621

IRR	-4%
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Notes:

¹ 1.1% of 90% of construction costs

Cisterra - 9th & G
Alternative 3 - 16 Story Tower
Assumptions & Results

HOLDING & DISPOSITION

Holding Period:	5
Cap Rate On Sale (Residential):	4.50%
Cap Rate On Sale (Retail):	5.50%
Commissions & Closing Costs:	1.00%
Value at Time of Sale (Year 5)	\$0
Asset Value PSF	\$0

BUILDING ASSUMPTIONS

Project FAR	11.92
Units Per Acre	270
Land S.F.	25,000
Building Gross SF	298,060
Building FAR/NSF Efficiency	58.18%
Net Rentable Area	173,400
Parking Spaces	189
Total # of Units	155

FINANCING

Construction Financing:	
Loan Amount	\$68,633,898
Loan to Cost	65%
Interest Rate	5.75%
Term (Months)	24
Refinance:	YES
Refinance at End of Year:	2

Permanent Loan Amount	\$69,077,415
Less: Construction Loan	(\$68,633,898)
Less: Loan Fees	0.50% (\$345,387.07)
Net Proceeds From Refinance	\$98,150

Permanent Loan Info:	
Loan Amount	\$69,077,415
Amortization	30
Interest Rate	4.5%
Annual Debt Service	\$4,200,061
Next Year NOI @ Refi	\$4,973,574
Value at Refi	\$110,523,864
Loan To Value	63%
Debt Coverage Ratio	1.18
Debt Yield	7.20%

RESIDUAL LAND VALUE

Land S.F.	25,000
Land Value	\$10,000,000
\$/S.F. of Land	\$400
\$/Unit	\$64,516

PROJECT SUMMARY

Residential Market Rate	# of Units	% of Total Mix	Avg. Unit Size	Total Net Rentable	Avg. Monthly Rent	Total Annual Rent	\$/S.F. Rent
Studio	39	25.2%	700	27,300	\$2,350	\$1,099,800	\$3.36
1 Bed	33	21.3%	800	26,400	\$2,850	\$1,128,600	\$3.56
1 Bed + D	42	27.1%	900	37,800	\$3,150	\$1,587,600	\$3.50
2 Bed	25	16.1%	1,100	27,500	\$3,450	\$1,035,000	\$3.14
3 Bed	16	10.3%	1,275	20,400	\$3,950	\$758,400	\$3.10
Subtotal	155	100.0%	899	139,400	\$3,016	\$5,609,400	\$3.35
Affordable Units ¹							
Studio	0	0.0%	0	0	\$0	\$0	
Small 1 Bed	0	0.0%	0	0	\$0	\$0	
Large 1 Bed	0	0.0%	0	0	\$0	\$0	
2 Bed	0	0.0%	0	0	\$0	\$0	
Subtotal	0	0.0%	0	0	\$0	\$0	
Total	155	100.0%	899	139,400		\$5,609,400	

Parking	Space	Avg. Monthly Rent	Total Annual Rent
Retail	189 spaces	\$0	\$0
Other Income	34,000 SF	\$3.18 PSF	\$1,297,440
	\$0		

CONSTRUCTION COSTS

	Total Cost	Cost Per Unit	Cost Per Gross S.F.
Land Costs	\$10,000,000	\$64,516	\$33.55
Hard Costs			
Below Grade Retail	\$4,709,250	\$30,382	\$204.75
Above Grade Parking	\$7,650,000	\$49,355	\$90.00
Retail	\$2,243,875	\$14,477	\$154.75
Amenity	\$1,150,000	\$7,419	\$230.00
Outdoor Amenity	\$1,260,000	\$8,129	\$157.50
Type I Residential	\$41,360,800	\$266,844	\$242.50
Site	\$1,187,500	\$7,661	\$47.50
GC/GR Mark-Ups	\$12,580,255	\$81,163	\$42.50
Hard Costs Subtotal	\$72,141,680	\$465,430	\$242.04
Soft Costs	\$14,428,336	\$93,086	\$48.41
Contingency	\$4,328,501	\$27,926	\$14.52
Financing	\$4,692,096	\$30,272	\$15.74
Total Project Costs	\$105,590,612	\$681,230	\$354.26
Less: Loan Amount	\$68,633,898	\$442,799	\$230.27
Initial Investment:	\$36,956,714	\$238,430	\$123.99

INVESTMENT PERFORMANCE

Stabilized NOI	Year 3	\$4,973,574
Total Project Costs		\$105,590,612
Stabilized Yield On Cost		4.7%
Return on Equity		Cash Flow
Initial		(\$36,956,714)
Year 1	0.0%	\$0
Year 2	0.3%	\$98,130
Year 3	2.0%	\$723,777
Year 4	2.4%	\$886,720
Year 5	136.4%	\$50,393,890
Total Profit Before Tax IRR		\$15,145,802
		7.2%

¹ @ 100% AMI

Cisterra - 9th & G
Alternative 3 - 16 Story Tower
Cash Flow Forecast

	Initial	Year 1 2020	Year 2 2021	Year 3 2022	Year 4 2023	Year 5 2024	Year 6 2025
	0	1	2	3	4	5	6
Total Market Rate Units				155	155	155	155
Units Leased (Market Rate)				148	148	148	148
Units Leased (Affordable)			Construction	0	0	0	0
Units Vacant (Market Rate)	Construction			7	7	7	7
Occupancy Rate (Market Rate)			95.5%	95.5%	95.5%	95.5%	95.5%
Vacancy Rate (Market Rate)			4.5%	4.5%	4.5%	4.5%	4.5%
Monthly Rent (Market Rate)	\$3,016	\$3,106	\$3,199	\$3,295	\$3,394	\$3,496	\$3,496
Monthly Rent Per S.F. (Market Rate)	\$3.35	\$3.45	\$3.56	\$3.66	\$3.77	\$3.89	\$3.89
Monthly Rent Per space (Parking)	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Monthly Rent Per S.F. (Retail)	\$3.18	\$3.28	\$3.37	\$3.47	\$3.58	\$3.69	\$3.69
Annual Increase In Rent		3.0%	3.0%	3.0%	3.0%	3.0%	3.0%
Gross Rental Income (Market Rate Units)			\$5,951,012	\$6,129,543	\$6,313,429		\$6,502,832
Gross Rental Income (Affordable Units)							
Gross Parking Income			\$0	\$0	\$0	\$0	\$0
Gross Retail Income (NNN)			\$1,376,454	\$1,417,748	\$1,460,280	\$1,504,089	\$1,504,089
Other Income			\$0	\$0	\$0	\$0	\$0
Less: Vacancy & Credit Loss (Parking)							
Less: Vacancy & Credit Loss (Retail)			5.0%	5.0%	5.0%	5.0%	5.0%
Less: Vacancy & Credit Loss (Residential)			5.0%	5.0%	5.0%	5.0%	5.0%
Subtotal: Net Rental Income			(\$68,823)	(\$70,887)	(\$73,014)	(\$75,204)	(\$75,204)
			(\$268,755)	(\$276,818)	(\$285,123)	(\$293,676)	(\$293,676)
			\$6,989,888	\$7,199,585	\$7,415,673	\$7,638,040	\$7,638,040
Total: Net Rental Income			\$6,989,888	\$7,199,585	\$7,415,573	\$7,638,040	\$7,638,040
Less: Operating Expenses ¹		\$0	\$0	(\$956,279)	(\$980,186)	(\$1,004,690)	(\$1,029,808)
Less: Property Taxes ²		\$0	\$0	(\$1,060,036)	(\$1,081,236)	(\$1,102,861)	(\$1,124,918)
Operating Expenses - Residential		\$0	\$0	(\$2,016,315)	(\$2,061,422)	(\$2,107,552)	(\$2,154,726)
Net Operating Income		\$0	\$0	\$4,973,574	\$5,138,163	\$5,308,021	\$5,483,314
Less: Capital Expenditures		\$0	\$0	(\$49,736)	(\$51,382)	(\$53,080)	(\$54,833)
Less: I/O (interim) financing		\$0	\$0	\$0	\$0	\$0	\$0
Less: Permanent Debt Service		\$0	\$0	(\$4,200,061)	(\$4,200,061)	(\$4,200,061)	\$0
Subtotal		\$0	\$0	(\$4,249,797)	(\$4,251,443)	(\$4,253,142)	(\$54,833)
Net Proceeds from Refinance:		\$0	\$98,130	\$0	\$0	\$0	\$0
Cash Flow From Operations		\$0	\$98,130	\$723,777	\$886,720	\$1,054,879	\$5,428,481

Disposition	
Residential (incl. parking)	
Cap Rate	4.50%
Next Year NOI	\$4,054,430
Asset Value	\$90,098,435
Asset Value Per Net SF	\$520
Asset Value Per Unit	\$581,280
Retail	
Cap Rate	5.50%
Next Year NOI	\$1,428,884
Asset Value	\$25,979,711
Asset Value Per Space	\$137,459
Sale Price	\$116,078,147
Less: Commissions & Closing Costs	(\$1,160,781)
Less: Principal Balance of Loan O/S	(\$65,578,355)
Net Proceeds from Disposition	\$49,339,011
Total Cash Flow Before Taxes	\$50,393,890
IRR	-57%

Notes:

¹ \$500 per unit per month

² 1.1% of 90% of construction costs

Cisterra - 9th & G
Alternative 3B - 22 Story Tower
Assumptions & Results

HOLDING & DISPOSITION

Holding Period:	\$
Cap Rate On Sale (Residential): ⁴	4.75%
Cap Rate On Sale (Retail):	5.50%
Commissions & Closing Costs:	1.00%
Value at Time of Sale (Year 5)	\$159,276,163
Asset Value PSF	\$427

BUILDING ASSUMPTIONS

Project FAR	14.92
Units Per Acre	396
Land S.F.	25,000
Building Gross SF	373,069
Building FAR/NSF Efficiency	63.88%
Net Rentable Area	238,325
Parking Spaces	189
Total # of Units	227

FINANCING

Construction Financing:	
Loan Amount	\$88,710,460
Loan to Cost	65%
Interest Rate	5.75%
Term (Months)	24
Refinance:	YES
Refinance at End of Year:	2

Permanent Loan Amount	\$85,292,460
Less: Construction Loan	(\$88,710,460)
Less: Loan Fees	0.50% (\$426,462.30)
Net Proceeds From Refinance	(\$3,844,462)

Permanent Loan Info:

Loan Amount	\$85,292,460
Amortization	30
Interest Rate	4.5%
Annual Debt Service	\$5,185,972
Next Year NOI @ Refi	\$6,482,227
Value at Refi	\$136,467,936
Loan To Value	63%
Debt Coverage Ratio	1.25
Debt Yield	7.60%

RESIDUAL LAND VALUE

Land S.F.	25,000
Land Value	\$10,000,000
S/S.F. of Land	\$400
S/Unit	\$44,053

PROJECT SUMMARY

Residential	# of Units	% of Total Mix	Avg. Unit Size	Total Net Rentable	Avg. Monthly Rent	Total Annual Rent	S/S.F. Rent
Market Rate - Parked	39	17.2%	700	27,300	\$2,500	\$1170,000	\$3.57
Studio	33	14.5%	800	26,400	\$3,000	\$1,188,000	\$3.75
1 Bed	42	18.5%	900	37,800	\$3,300	\$1,663,200	\$3.67
1 Bed + D	25	11.0%	1,100	27,500	\$3,600	\$1,080,000	\$3.27
2 Bed	16	7.0%	1,275	20,400	\$4,100	\$787,200	\$3.22
3 Bed	155	68.3%	899	139,400	\$3,166	\$5,888,400	\$3.52
Subtotal							
Market Rate - No Parking ^{1,2}	18	7.9%	700	12,600	\$2,300	\$496,800	\$3.29
Studio	15	6.6%	800	12,000	\$2,800	\$504,000	\$3.50
1 Bed	19	8.4%	900	17,100	\$3,100	\$706,800	\$3.44
1 Bed + D	13	5.7%	1,100	14,300	\$3,400	\$550,400	\$3.09
2 Bed	7	3.1%	1,275	8,925	\$3,900	\$327,600	\$3.06
3 Bed	72	31.7%	902	64,925	\$2,969	\$2,565,600	\$3.29
Subtotal							
Affordable Units ³	0	0.0%	0	0	\$0	\$0	\$0
Studio	0	0.0%	0	0	\$0	\$0	\$0
Small 1 Bed	0	0.0%	0	0	\$0	\$0	\$0
Large 1 Bed	0	0.0%	0	0	\$0	\$0	\$0
2 Bed	0	0.0%	0	0	\$0	\$0	\$0
Subtotal	0	0.0%	0	0	\$0	\$0	\$0
Total	227	100.0%	900	204,325	\$3,104	\$8,454,000	\$3.45

Parking	Space	Avg. Monthly Rent	Total Annual Rent
Retail	189 spaces	\$0	\$0
Other Income	34,000 SF	\$3.18 PSF	\$1,297,440
	\$0		

CONSTRUCTION COSTS

	Total Cost	Cost Per Unit	Cost Per Gross S.F.
Land Costs	\$10,000,000	\$44,053	\$26.80
Hard Costs			
Below Grade Retail	\$4,766,750	\$20,999	\$207.25
Above Grade Parking	\$8,075,000	\$35,573	\$95.00
Retail	\$2,280,125	\$10,045	\$157.25
Amenity	\$1,175,000	\$5,176	\$235.00
Outdoor Amenity	\$1,320,000	\$5,815	\$165.00
Type I Residential	\$61,392,250	\$270,450	\$250.00
Site	\$1,250,000	\$5,507	\$50.00
GC/GR Mark-Ups	\$15,311,896	\$67,453	\$41.04
Hard Costs Subtotal	\$95,571,021	\$421,018	\$256.18
Soft Costs	\$19,114,204	\$84,204	\$51.24
Contingency	\$5,734,261	\$25,261	\$15.37
Financing	\$6,058,143	\$26,688	\$16.24
Total Project Costs	\$136,477,630	\$601,223	\$365.82
Less: Loan Amount	\$88,710,460	\$390,795	\$237.79
Initial Investment:	\$47,767,171	\$210,428	\$128.04

INVESTMENT PERFORMANCE

Stabilized NOI	Year 3	\$6,482,227
Total Project Costs		\$136,477,630
Stabilized Yield On Cost		4.7%
Initial	Return on Equity	Cash Flow
Year 1	0.0%	(\$47,767,171)
Year 2	-8.0%	\$0
Year 3	2.6%	(\$3,844,462)
Year 4	4.2%	\$1,231,432
Year 5	165.3%	\$2,016,453
Total Profit		\$78,964,265
Before Tax IRR		\$30,600,518
		10.2%

¹ Rents discounted to reflect alternative Downtown parking within 4 blocks @ \$160 - \$225 /month.

² 10% stabilized vacancy reflective of local leasing agents.

³ @ 100% AMI

⁴ Assumed 25 basis points added to cap rate to reflect the 72 units that do not have parking provided. This represents increased market risk to both the developer and lender.

Source: London Moeder Advisors, Swinerton (construction estimates)

	Initial	Year 1 2020	Year 2 2021	Year 3 2022	Year 4 2023	Year 5 2024	Year 6 2025
	0	1	2	3	4	5	6
Total Market Rate Units				227	227	227	227
Units Leased (Market Rate - Parked)				136	148	148	148
Units Leased (Market Rate - No Parking)				61	65	65	65
Units Leased (Affordable)			Construction	0	0	0	0
Units Vacant (Market Rate - Parked)				20	7	7	7
Units Vacant (Market Rate - No Parking)				11	7	7	7
Occupancy Rate (Market Rate)				86.4%	93.8%	93.8%	93.8%
Vacancy Rate (Market Rate)				13.6%	6.2%	6.2%	6.2%
Monthly Rent (Market Rate - Parked)		\$3,166	\$3,261	\$3,359	\$3,459	\$3,563	\$3,670
Monthly Rent Per S.F. (Market Rate - Parked)		\$3.52	\$3.63	\$3.73	\$3.85	\$3.96	\$4.08
Monthly Rent (Market Rate - No Parking)		\$2,969	\$3,059	\$3,150	\$3,245	\$3,342	\$3,442
Monthly Rent Per S.F. (Market Rate - No Parking)		\$3.29	\$3.40	\$3.50	\$3.61	\$3.72	\$3.83
Monthly Rent Per Space (Parking)		\$0	\$0	\$0	\$0	\$0	\$0
Monthly Rent Per S.F. (Retail)		\$3.18	\$3.28	\$3.37	\$3.47	\$3.58	\$3.69
Annual Increase In Rent			3.0%	3.0%	3.0%	3.0%	3.0%
Gross Rental Income (Market Rate - Parked)				\$6,247,004	\$6,434,414	\$6,627,446	\$6,826,269
Gross Rental Income (Market Rate - No Parking)				\$2,721,845	\$2,803,500	\$2,887,605	\$2,974,234
Gross Rental Income (Affordable)							
Gross Parking Income				\$0	\$0	\$0	\$0
Gross Retail Income (NNN)				\$1,376,454	\$1,417,748	\$1,460,280	\$1,504,089
Other Income				\$0	\$0	\$0	\$0
Less: Vacancy & Credit Loss (Market Rate - Parked)				(\$785,913)	(\$290,586)	(\$299,304)	(\$308,283)
Less: Vacancy & Credit Loss (Market Rate - No Parking)				(\$428,439)	(\$272,563)	(\$280,739)	(\$289,162)
Less: Vacancy & Credit Loss (Affordable)							
Less: Vacancy & Credit Loss (Parking)							
Less: Vacancy & Credit Loss (Retail)				(\$68,823)	(\$70,887)	(\$73,014)	(\$75,204)
Subtotal: Net Rental Income				\$9,062,128	\$10,021,625	\$10,322,274	\$10,631,942
Total: Net Rental Income				\$9,062,128	\$10,021,625	\$10,322,274	\$10,631,942
Less: Operating Expenses ¹	Per Unit			\$0			
Less: Property Taxes ²	% Increase			\$0			
Operating Expenses - Residential				(\$1,688)			
Net Operating Income				\$0			
Less: Capital Expenditures				\$0			
Less: I/O (Interim) financing				\$0			
Less: Permanent Debt Service				\$0			
Subtotal				\$0			
Net Proceeds from Refinance:				\$0			
Cash Flow From Operations				\$0			

Disposition	
Residential (incl. parking)	
Cap Rate	4.75%
Next Year NOI	\$6,331,581
Asset Value	\$133,296,451
Asset Value Per Net SF	\$559
Asset Value Per Unit	\$587,209
Retail	
Cap Rate	5.50%
Next Year NOI	\$1,428,884
Asset Value	\$25,979,711
Asset Value Per Space	\$137,459
Sale Price	\$159,276,163
Less: Commissions & Closing Costs	(\$1,592,762)
Less: Principal Balance of Loan O/S	(\$80,972,040)
Net Proceeds from Disposition	\$76,711,362
Total Cash Flow Before Taxes	\$78,964,265

IRR	10%
-----	-----

Notes:

¹\$500 per unit per month
²1.1% of 90% of construction costs

Cisterra - 9th & G
Alternative 4 - 13 Story Tower
Assumptions & Results

HOLDING & DISPOSITION

Holding Period:	5
Cap Rate On Sale (Residential):	4.50%
Cap Rate On Sale (Retail):	5.50%
Commissions & Closing Costs:	1.00%
Value at Time of Sale (Year 5)	\$0
Asset Value PSF	\$0

BUILDING ASSUMPTIONS

Project FAR	13.90
Units Per Acre	293
Land S.F.	25,000
Building Gross SF	347,400
Building FAR/NSF Efficiency	53.31%
Net Rentable Area	185,200
Parking Spaces	202
Total # of Units	168

FINANCING

Construction Financing:	
Loan Amount	\$81,072,657
Loan to Cost	65%
Interest Rate	5.75%
Term (Months)	24
Refinance:	YES
Refinance at End of Year:	3
Permanent Loan Amount	\$68,160,004
Less: Construction Loan	(\$81,072,657)
Less: Loan Fees	0.50% (\$340,800)
Net Proceeds From Refinance	(\$13,253,452)
Permanent Loan Info:	
Loan Amount	\$68,160,004
Amortization	30
Interest Rate	4.5%
Annual Debt Service	\$4,144,281
Next Year NOI @ Refi	\$4,907,520
Value at Refi	\$109,056,007
Loan To Value	63%
Debt Coverage Ratio	1.18
Debt Yield	7.20%

RESIDUAL LAND VALUE

Land S.F.	25,000
Land Value	\$10,000,000
\$/S.F. of Land	\$400
\$/Unit	\$59,524

PROJECT SUMMARY

Residential Market Rate	# of Units	% of Total Mix	Avg. Unit Size	Total Net Rentable	Avg. Monthly Rent	Total Annual Rent	\$/S.F. Rent
Studio	43	25.6%	700	30,100	\$2,130	\$1,099,080	\$3.04
1 Bed	35	20.8%	800	28,000	\$2,630	\$1,104,600	\$3.29
1 Bed + D	45	26.8%	900	40,500	\$2,930	\$1,582,200	\$3.26
2 Bed	27	16.1%	1,100	29,700	\$3,230	\$1,046,520	\$2.94
3 Bed	18	10.7%	1,272	22,900	\$3,730	\$805,680	\$2.93
Subtotal	168	100.0%	900	151,200	\$2,797	\$5,638,080	\$3.11
Affordable Units ¹							
Studio	0	0.0%	0	0	\$0	\$0	
Small 1 Bed	0	0.0%	0	0	\$0	\$0	
Large 1 Bed	0	0.0%	0	0	\$0	\$0	
2 Bed	0	0.0%	0	0	\$0	\$0	
Subtotal	0	0.0%	0	0	\$0	\$0	
Total	168	100.0%	900	151,200		\$5,638,080	

Parking	Space	Avg.	Total
	202 spaces		
Retail	34,000 SF	\$0	
Other Income		\$3.18 PSF	\$1,297,440

CONSTRUCTION COSTS

	Total Cost	Cost Per Unit	Cost Per Gross S.F.
Land Costs	\$10,000,000	\$59,524	\$28.79
Hard Costs			
Below Grade Retail	\$0	\$0	\$0.00
Below Grade Parking	\$15,275,000	\$90,923	\$130.00
Retail	\$6,570,500	\$39,110	\$193.25
Amenity	\$1,150,000	\$6,845	\$230.00
Outdoor Amenity	\$1,260,000	\$7,500	\$157.50
Type I Residential	\$46,293,250	\$275,555	\$242.50
Site	\$1,187,500	\$7,068	\$47.50
GC/GR Mark-Ups	\$14,921,458	\$88,818	\$42.95
Hard Costs Subtotal	\$86,657,708	\$515,820	\$249.45
Soft Costs	\$17,331,542	\$103,164	\$49.89
Contingency	\$5,199,462	\$30,949	\$14.97
Financing	\$5,538,453	\$32,967	\$15.94
Total Project Costs	\$124,727,164	\$742,424	\$359.03
Less: Loan Amount	\$81,072,657	\$482,575	\$233.37
Initial Investment:	\$43,654,507	\$259,848	\$125.66

INVESTMENT PERFORMANCE

Stabilized NOI	Year 4	\$4,907,520
Total Project Costs		\$124,727,164
Stabilized Yield On Cost		3.9%
Return on Equity		
Initial		Cash Flow
Year 1	0.0%	(\$43,654,507)
Year 2	0.0%	\$0
Year 3	-32.0%	(\$13,948,525)
Year 4	1.6%	\$714,164
Year 5	102.2%	\$44,609,574
Total Profit		(\$12,279,294)
Before Tax IRR		-5.5%

¹ @ 100% AMI
Source: London Moeder Advisors

Cisterra - 9th & G
Alternative 4 - 13 Story Tower
Cash Flow Forecast

	Initial	Year 1 2020	Year 2 2021	Year 3 2022	Year 4 2023	Year 5 2024	Year 6 2025
	0	1	2	3	4	5	6
Total Market Rate Units				168	168	168	168
Units Leased (Market Rate)				136	161	161	161
Units Leased (Affordable)				0	0	0	0
Units Vacant (Market Rate)				32	7	7	7
Occupancy Rate (Market Rate)				80.7%	95.8%	95.8%	95.8%
Vacancy Rate (Market Rate)				19.3%	4.2%	4.2%	4.2%
Monthly Rent (Market Rate)		\$2,797	\$2,881	\$2,967	\$3,056	\$3,148	\$3,242
Monthly Rent Per S.F. (Market Rate)		\$3.11	\$3.20	\$3.30	\$3.40	\$3.50	\$3.60
Monthly Rent Per space (Parking)		\$0	\$0	\$0	\$0	\$0	\$0
Monthly Rent Per S.F. (Retail)		\$3.18	\$3.28	\$3.37	\$3.47	\$3.58	\$3.69
Annual Increase In Rent			3.0%	3.0%	3.0%	3.0%	3.0%
Gross Rental Income (Market Rate Units)			\$5,981,439	\$6,160,882	\$6,345,709		\$6,536,080
Gross Rental Income (Affordable Units)				\$0	\$0	\$0	\$0
Gross Parking Income			\$1,376,454	\$1,417,748	\$1,460,280	\$1,504,089	\$1,504,089
Gross Retail Income (NNN)				\$0	\$0	\$0	\$0
Other Income							
Less: Vacancy & Credit Loss (Parking)	5.0%						
Less: Vacancy & Credit Loss (Retail)	5.0%			(\$68,823)	(\$70,887)	(\$73,014)	(\$75,204)
Less: Vacancy & Credit Loss (Residential)			(\$1,154,157)	(\$256,703)	(\$264,405)	(\$272,337)	(\$272,337)
Subtotal: Net Rental Income			\$6,134,914	\$7,251,039	\$7,468,570		\$7,692,627
Total: Net Rental Income			\$6,134,914	\$7,251,039	\$7,468,570		\$7,692,627
Less: Operating Expenses ¹	Per Unit		\$0	\$0	(\$1,066,283)	(\$1,092,940)	(\$1,120,264)
Less: Property Taxes ²	% Increase		\$0	\$0	(\$1,252,192)	(\$1,277,236)	(\$1,302,780)
Operating Expenses - Residential			\$0	\$0	(\$2,343,519)	(\$2,395,721)	(\$2,449,100)
Net Operating Income			\$0	\$0	\$4,907,520	\$5,072,850	\$5,243,528
Less: Capital Expenditures			\$0	\$0	(\$40,067)	(\$49,075)	(\$50,728)
Less: I/O (interim) financing			\$0	\$0	(\$4,661,678)	\$0	\$0
Less: Permanent Debt Service			\$0	\$0	(\$4,144,281)	(\$4,144,281)	\$0
Subtotal			\$0	\$0	(\$4,701,744)	(\$4,193,356)	(\$52,435)
Net Proceeds from Refinance:			\$0	\$0	\$0	\$0	\$0
Cash Flow From Operations			\$0	\$0	\$13,948,525	\$714,164	\$5,191,092
Disposition							
Residential (incl. parking)							
Cap Rate						4.50%	
Next Year NOI						\$3,814,644	
Asset Value						\$84,769,857	
Asset Value Per Net SF						\$458	
Asset Value Per Unit						\$504,582	
Retail							
Cap Rate						5.50%	
Next Year NOI						\$1,428,884	
Asset Value						\$25,979,711	
Asset Value Per Space						\$128,612	
Sale Price						\$110,749,569	
Less: Commissions & Closing Costs						(\$1,107,496)	
Less: Principal Balance of Loan O/S						(\$65,910,339)	
Net Proceeds from Disposition						\$43,731,733	
Total Cash Flow Before Taxes			\$0	\$0	\$13,948,525	\$714,164	\$44,609,574
IRR						-6%	

Notes:

¹ \$500 per unit per month

² 1.1% of 90% of construction costs

Corporate Profile

London Moeder Advisors

REPRESENTATIVE SERVICES

Market and Feasibility Studies	Development Services	Litigation Consulting
Financial Structuring	Fiscal Impact	Workout Projects
Asset Disposition	Strategic Planning	MAI Valuation
Government Processing	Capital Access	Economic Analysis

London Moeder Advisors (formerly The London Group) was formed in 1991 to provide real estate advisory services to a broad range of clientele. The firm principals, Gary London and Nathan Moeder, combine for over 60 years of experience. We have analyzed, packaged and achieved capital for a wide variety of real estate projects. Clients who are actively pursuing, developing and investing in projects have regularly sought our advice and financial analysis capabilities. Our experience ranges from large scale, master planned communities to urban redevelopment projects, spanning all land uses and development issues of all sizes and types. These engagements have been undertaken principally throughout North America and Mexico.

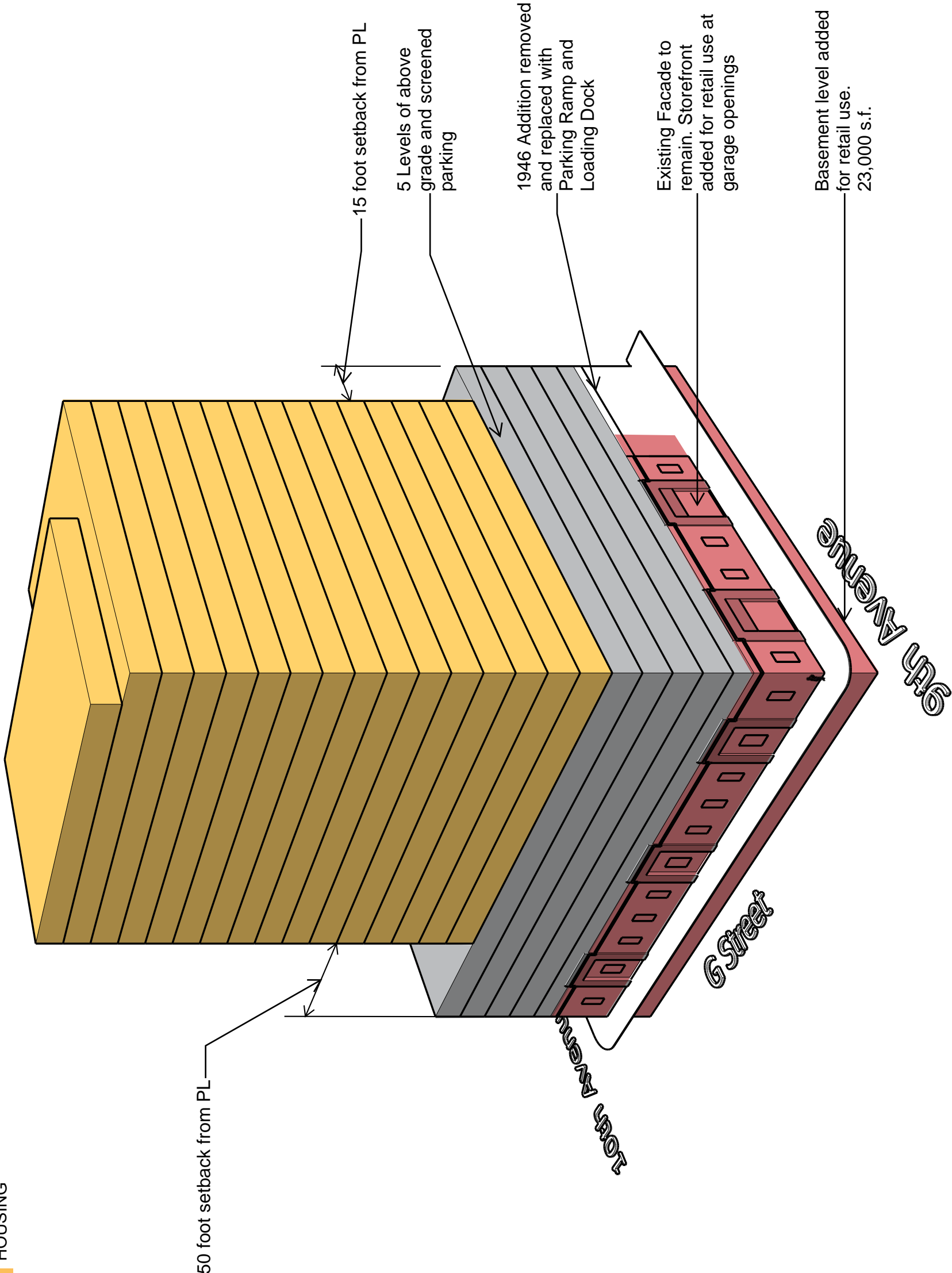
A snapshot of a few of the services we render for both the residential and commercial sectors:

- **Market Analysis** for mixed use, urban and suburban properties. Studies concentrate on market depth for specific products, detailed recommendations for product type, absorption and future competition. It also includes economic overviews and forecasts of the relevant communities.
- **Financial Feasibility Studies** for new projects of multiple types, including condominium, apartment, office, and master-planned communities. Studies incorporate debt and equity needs, sensitivity analyses, rates of return and land valuations.
- **Litigation support/expert witness services** for real estate and financial related issues, including economic damages/losses, valuations, historic market conditions and due diligence. We have extensive deposition, trial, mediation and arbitration experience.
- **Investment studies for firms acquiring or disposing of real estate.** Studies include valuation, repositioning projects and portfolios, economic/real estate forecasts and valuation of partnerships. Often, the commercial studies include the valuation of businesses.
- **Estate Planning services** including valuation of portfolios, development of strategies for disposition or repositioning portfolios, succession planning and advisory services for high net worth individuals. We have also been involved in numerous marriage dissolution assignments where real estate is involved.
- **Fiscal Impact, Job Generation and Economic Multiplier Effect Reports**, traditionally prepared for larger commercial projects and in support of Environmental Impact Reports. We have been retained by both developers and municipalities for these reports. The studies typically relate to the tax revenues and employment impacts of new projects.

The London Group also draws upon the experience of professional relationships in the development, legal services, financial placement fields as well as its own staff. Clients who are actively investigating and investing in apartment projects, retail centers, commercial projects, mixed use developments and large master plans have regularly sought our advice and financial analysis capabilities.

San Diego: 825 10th Ave | San Diego, CA 92101 | (619) 269-4010
Carlsbad: 2792 Gateway Road #104 | Carlsbad, CA 92009 | (619) 269-4012

- PARKING
- RETAIL
- HOUSING



Base Case

Level B1			
Retail		19,500 sf	
BOH/Util		3,500 sf	
	<i>Subtotal</i>	<i>23,000 sf</i>	
Level 1			
Retail		14,500 sf	
Lobby/Leasing		3,400 sf	
BOH/Util		6,850 sf	
	<i>Subtotal</i>	<i>24,750 sf</i>	
Levels 2-6 Parking			
Retail Parking		24,500 sf	34
Residential Parking		98,000 sf	242
	<i>Subtotal</i>	<i>122,500 sf</i>	<i>276</i>
Levels 7-22			
Residential Units		219,000 sf	242
Residential Amenities		5,000 sf	
Core/Corridor		32,400 sf	
	<i>Subtotal</i>	<i>256,400 sf</i>	
Total Gross		426,650 sf	
Retail N.R.		34,000 sf	
Residential N.R.		219,000 sf	

Units Mix Type	Average Unit Size	# of Units	% of Units	Total SF
Studio	700 sf	60	24.8%	42,000 sf
1 Bedroom	800 sf	52	21.5%	41,600 sf
1 Bed + Den	900 sf	65	26.9%	58,500 sf
2 Bedroom	1,110 sf	40	16.5%	44,400 sf
3 Bedroom	1,300 sf	25	10.3%	32,500 sf
<i>Total / Ave.</i>		<i>242</i>	<i>100.0%</i>	<i>219,000 sf</i>

- PARKING
- RETAIL
- HOUSING

Base Case

Level B1	
Retail	19,500 sf
BOH/Util	3,500 sf
<i>Subtotal</i>	
<i>23,000 sf</i>	

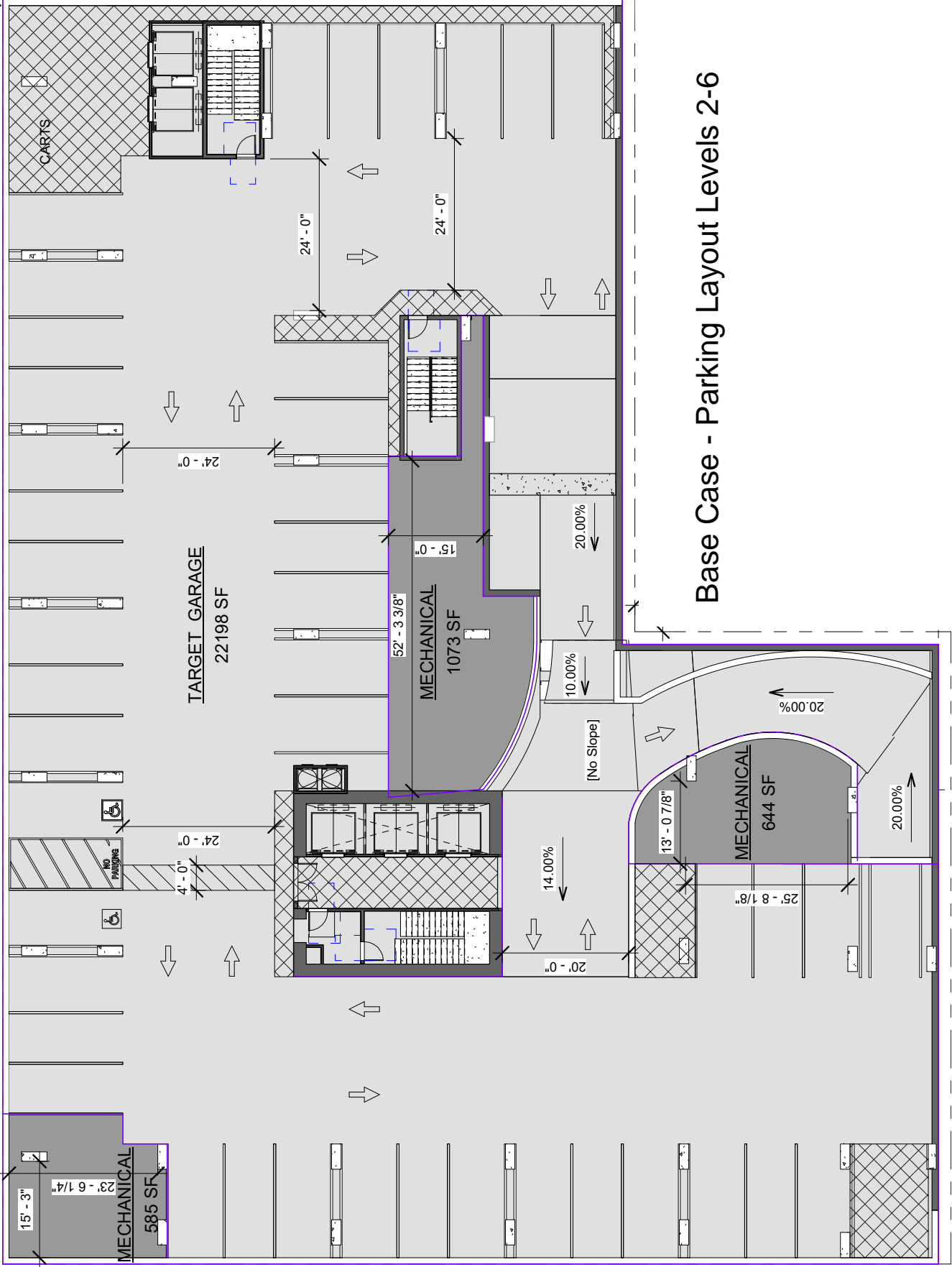
Level 1	
Retail	14,500 sf
Lobby/Leasing	3,400 sf
BOH/Util	6,850 sf
<i>Subtotal</i>	
<i>24,750 sf</i>	

Levels 2-6 Parking	
Retail Parking	24,500 sf
Residential Parking	98,000 sf
<i>Subtotal</i>	
<i>122,500 sf</i>	
	34
	242
	276

Levels 7-22	
Residential Units	219,000 sf
Residential Amenities	5,000 sf
Core/Corridor	32,400 sf
<i>Subtotal</i>	
<i>256,400 sf</i>	
	242

Total Gross	426,650 sf
Retail N.R.	34,000 sf
Residential N.R.	219,000 sf

Units Mix Type	Average Unit Size	# of		Total SF
		Unit Size	Units	
Studio	700 sf	60	24.8%	42,000 sf
1 Bedroom	800 sf	52	21.5%	41,600 sf
1 Bed + Den	900 sf	65	26.9%	58,500 sf
2 Bedroom	1,110 sf	40	16.5%	44,400 sf
3 Bedroom	1,300 sf	25	10.3%	32,500 sf
<i>Total / Ave.</i>		<i>905 sf</i>	<i>242</i>	<i>100.0%</i>
				<i>219,000 sf</i>

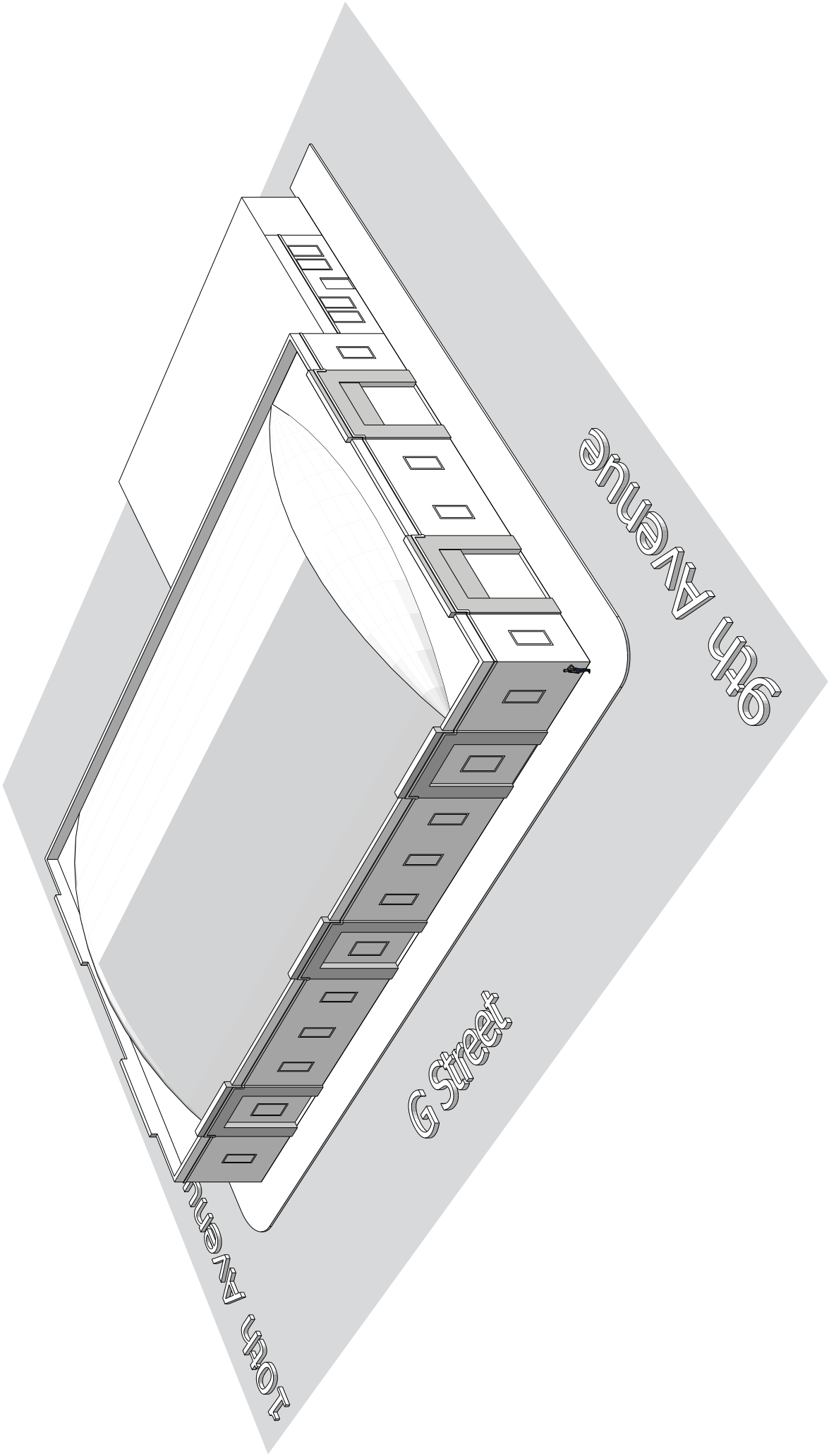


Base Case - Parking Layout Levels 2-6

Alternative 1

Level 1	
Retail	22,000 sf
BOH/Util	3,000 sf
No Parking	

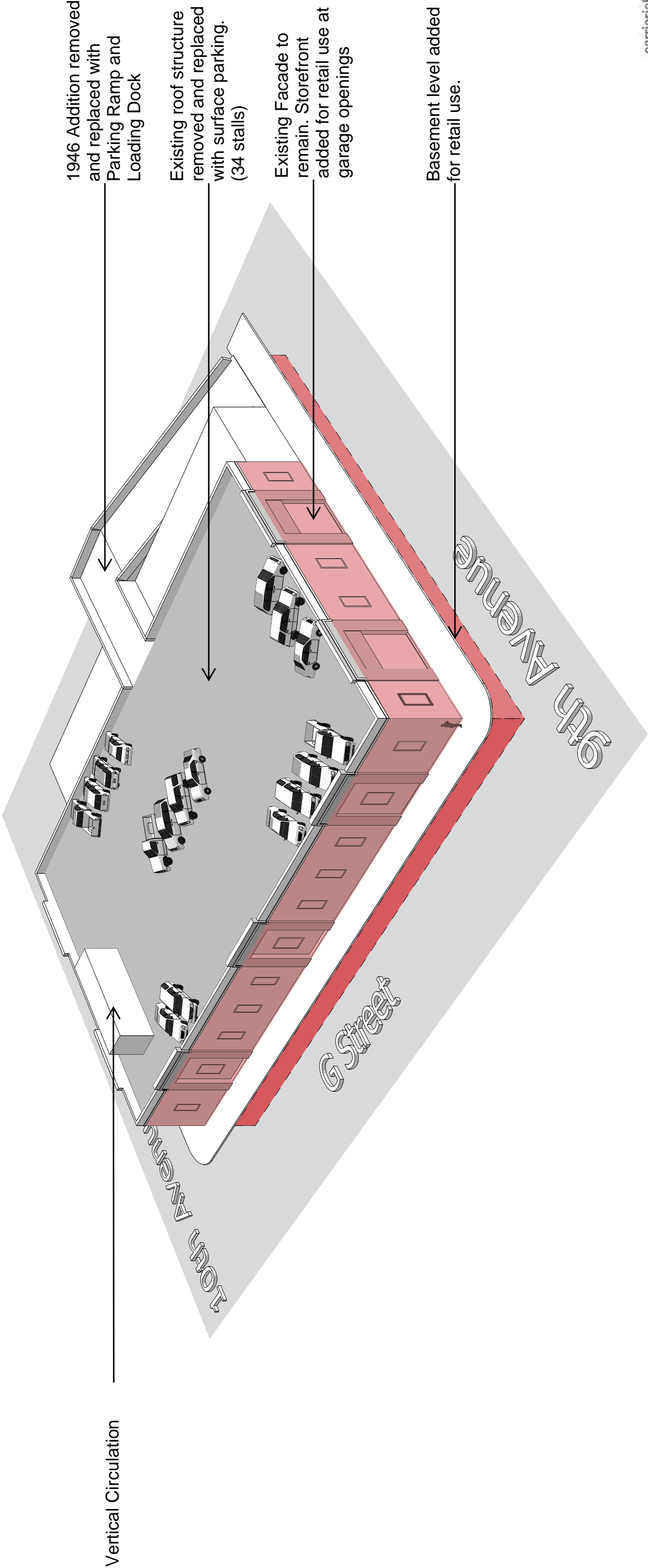
Total Gross	25,000 sf
Total Net Retail	22,000 sf



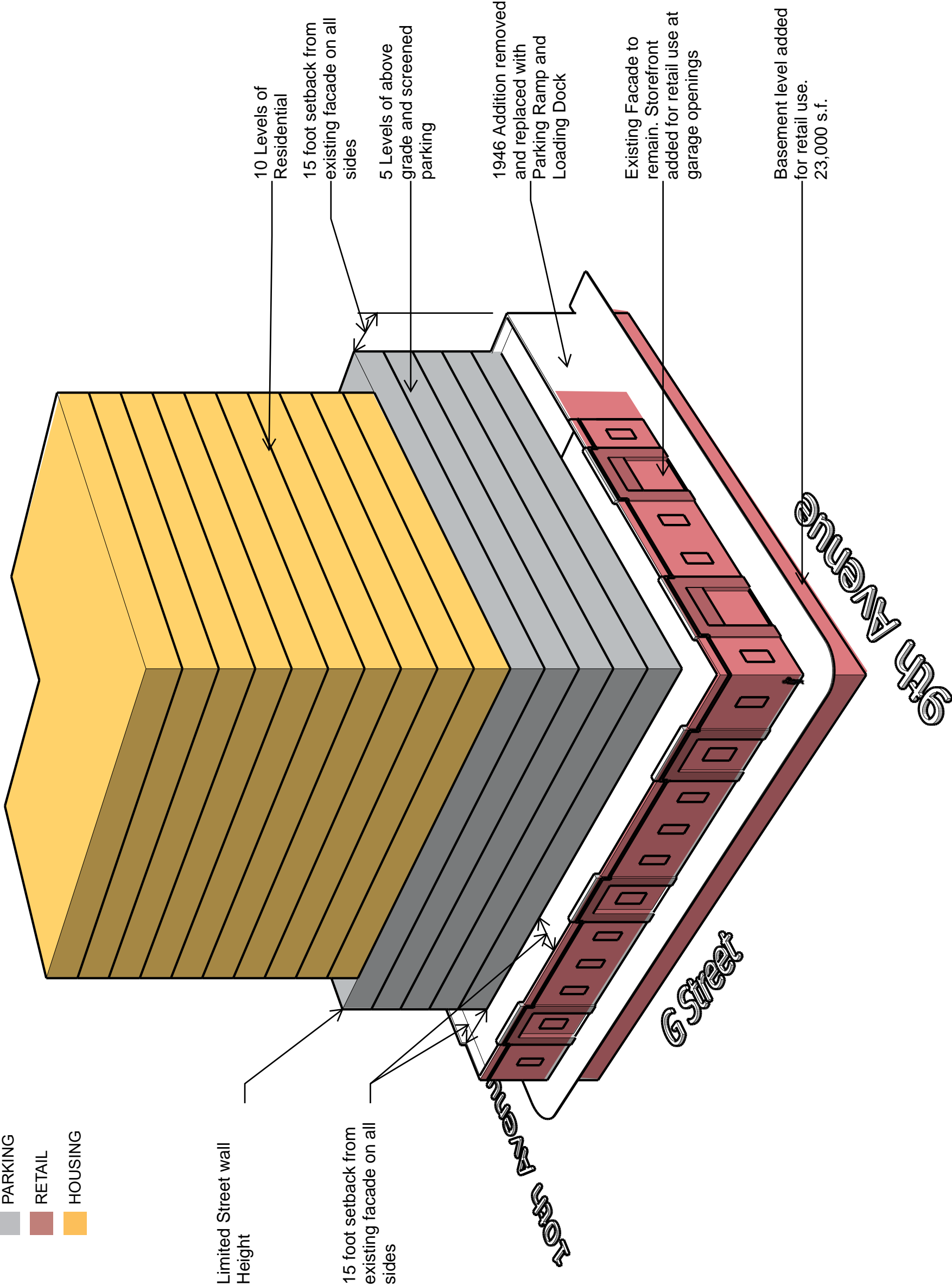
- PARKING
- RETAIL
- HOUSING

Alternative 2

Level B1		
Retail	14,500 sf	
BOH/Util	2,600 sf	
	17,100 sf	
Level 1		
Retail	19,500 sf	
BOH/Util	5,400 sf	
	24,900 sf	
	Subtotal	
Parking (open)	19,500 sf	
Total Gross		42,000 sf
Total Net Retail		34,000 sf



- PARKING
- RETAIL
- HOUSING



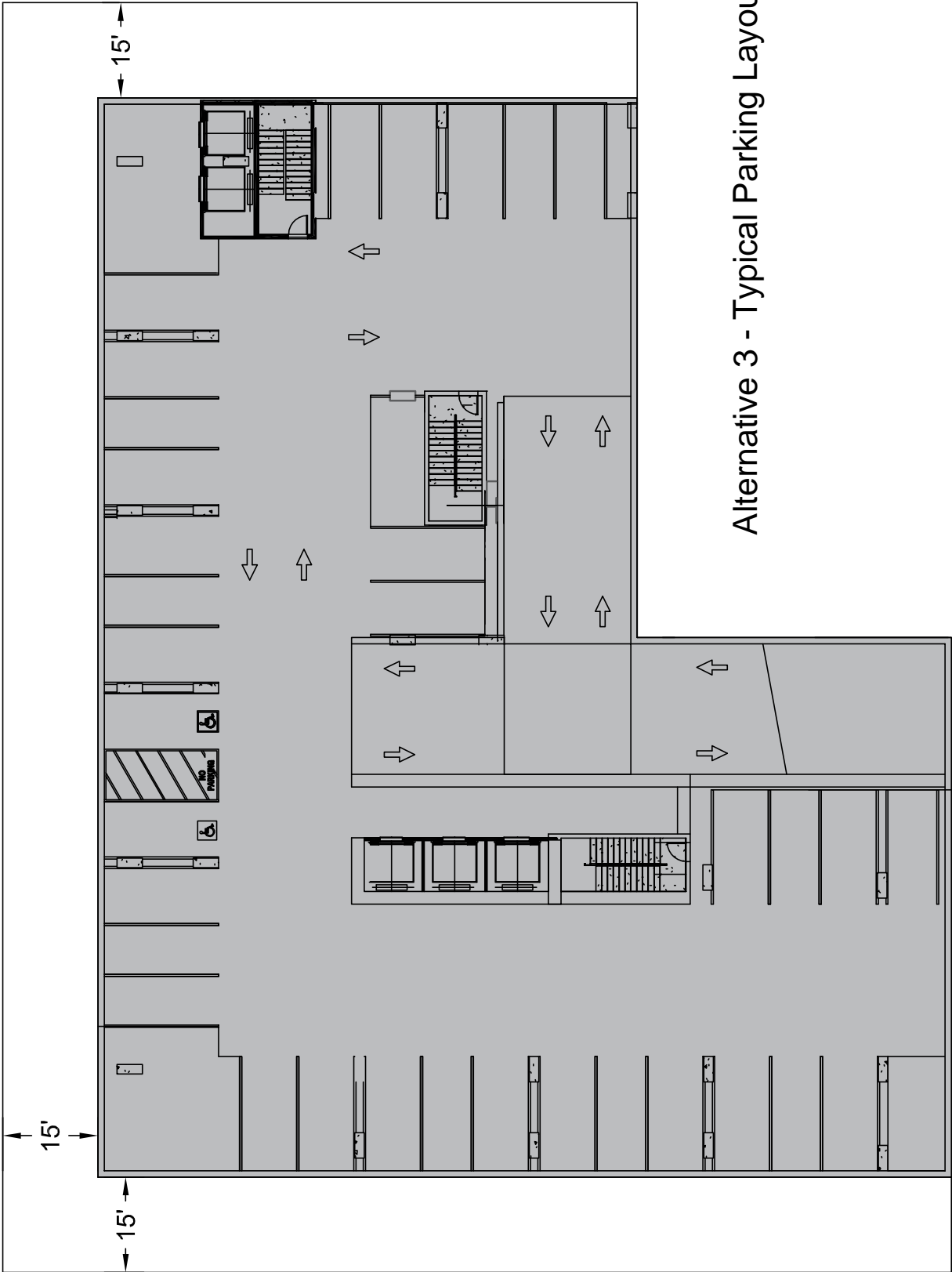
Alternative 3

Level B1			
Retail		19,500 sf	
BOH/Util		3,500 sf	
	<i>Subtotal</i>	<i>23,000 sf</i>	
Level 1			
Retail		14,500 sf	
Lobby/Leasing		3,400 sf	
BOH/Util		6,850 sf	
	<i>Subtotal</i>	<i>24,750 sf</i>	
Levels 2-6 Parking			
Retail Parking		15,300 sf	34
Residential Parking		69,700 sf	155
	<i>Subtotal</i>	<i>85,000 sf</i>	<i>189</i>
Levels 7-16			
Residential Units		139,400 sf	155
Residential Amenities		5,000 sf	
Core/Corridor		20,910 sf	
	<i>Subtotal</i>	<i>165,310 sf</i>	
Total Gross		298,060 sf	
Retail N.R.		34,000 sf	
Residential N.R.		139,400 sf	
Units Mix			
Type	Average Unit Size	# of Units	% of Units Total SF
Studio	700 sf	39	25.2% 27,300 sf
1 Bedroom	800 sf	33	21.3% 26,400 sf
1 Bed + Den	900 sf	42	27.1% 37,800 sf
2 Bedroom	1,100 sf	25	16.1% 27,500 sf
3 Bedroom	1,275 sf	16	10.3% 20,400 sf
<i>Total / Ave.</i>		<i>899 sf</i>	<i>155</i> <i>100.0%</i> <i>139,400 sf</i>

- PARKING
- RETAIL
- HOUSING

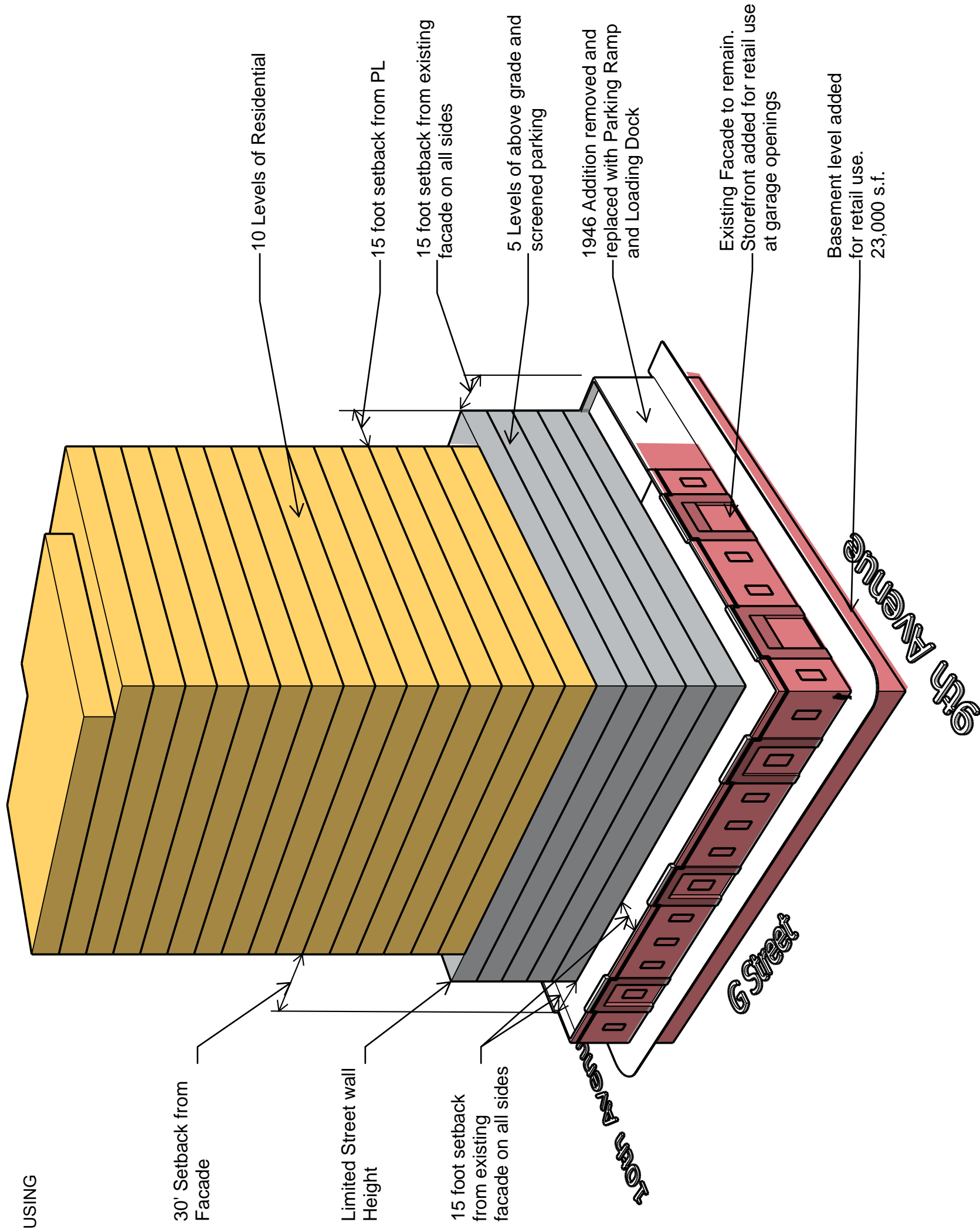
Alternative 3

Level B1			
Retail			19,500 sf
BOH/Util			3,500 sf
		Subtotal	23,000 sf
Level 1			
Retail			14,500 sf
Lobby/Leasing			3,400 sf
BOH/Util			6,850 sf
		Subtotal	24,750 sf
Levels 2-6 Parking			
Retail Parking			34
Residential Parking			155
		Subtotal	189
Levels 7-16			
Residential Units			139,400 sf
Residential Amenities			5,000 sf
Core/Corridor			20,910 sf
		Subtotal	165,310 sf
Total Gross			
			298,060 sf
Retail N.R.			
			34,000 sf
Residential N.R.			
			139,400 sf
Units Mix			
Type	Average Unit Size	# of Units	% of Units Total SF
Studio	700 sf	39	25.2%
1 Bedroom	800 sf	33	21.3%
1 Bed + Den	900 sf	42	27.1%
2 Bedroom	1,100 sf	25	16.1%
3 Bedroom	1,275 sf	16	10.3%
Total / Ave.			
	899 sf	155	100.0%
			139,400 sf



Alternative 3 - Typical Parking Layout

9th and 'G' - Feasibility Study



Alternative 3B

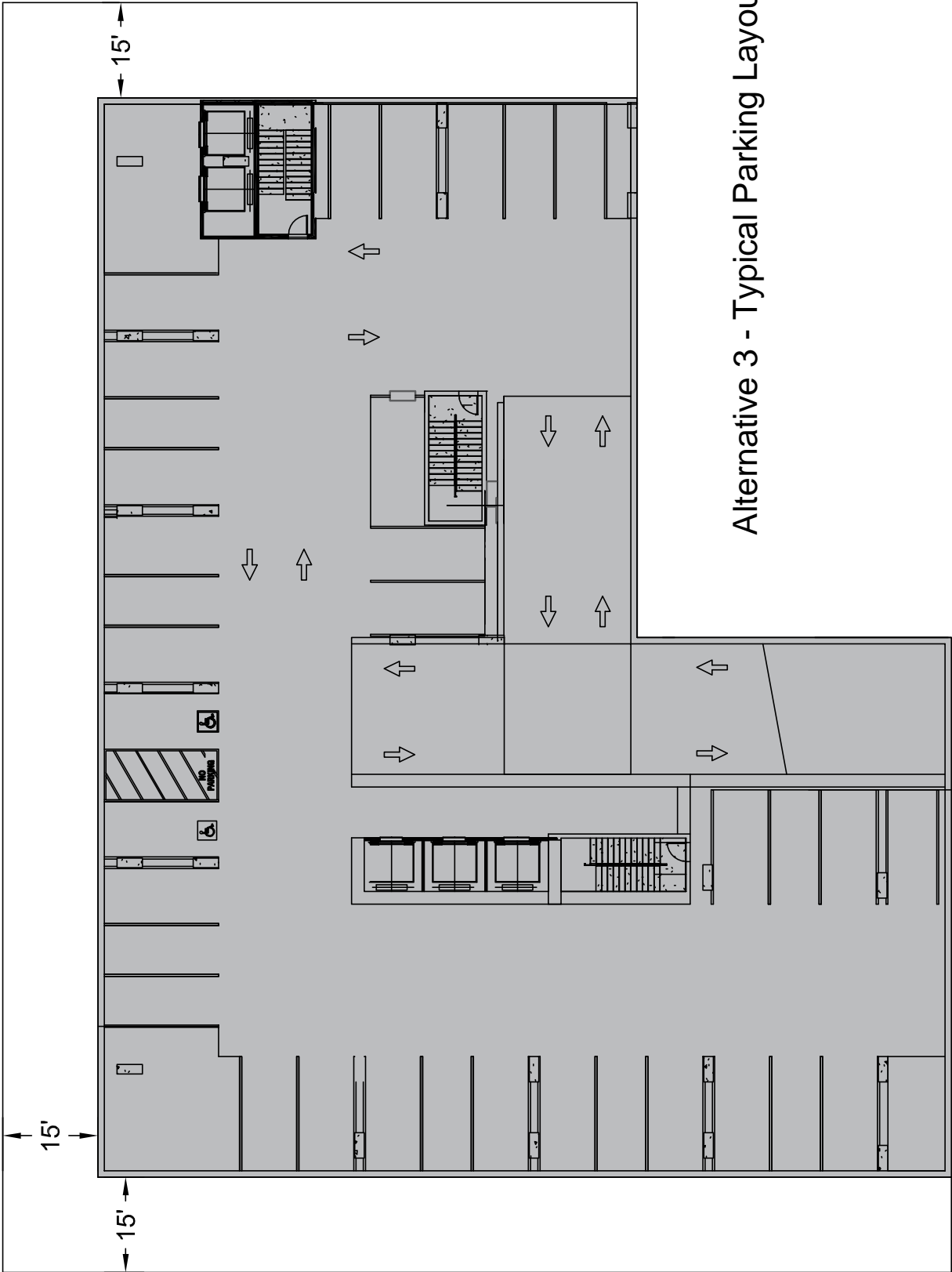
Parking rate at .68/Unit

Level B1		
Retail		19,500 sf
BOH/Util		3,500 sf
	<i>Subtotal</i>	23,000 sf
Level 1		
Retail		14,500 sf
Lobby/Leasing		3,400 sf
BOH/Util		6,850 sf
	<i>Subtotal</i>	24,750 sf
Levels 2-6 Parking		
Retail Parking		15,300 sf
Residential Parking		69,700 sf
	<i>Subtotal</i>	85,000 sf
Levels 7-22		
Residential Units		204,625 sf
Residential Amenities		5,000 sf
Core/Corridor		30,694 sf
	<i>Subtotal</i>	240,319 sf
		227

Total Gross	373,069 sf
Retail N.R.	34,000 sf
Residential N.R.	204,625 sf

Units Mix	Average	# of	% of	Total SF
Type	Unit Size	Units	Units	
Studio	700 sf	57	25.0%	39,900 sf
1 Bedroom	800 sf	47	21.0%	37,600 sf
1 Bed + Den	900 sf	61	27.0%	54,900 sf
2 Bedroom	1,100 sf	39	17.0%	42,900 sf
3 Bedroom	1,275 sf	23	10.0%	29,325 sf
Total / Ave.	901 sf	227	100.0%	204,625 sf

- PARKING
- RETAIL
- HOUSING

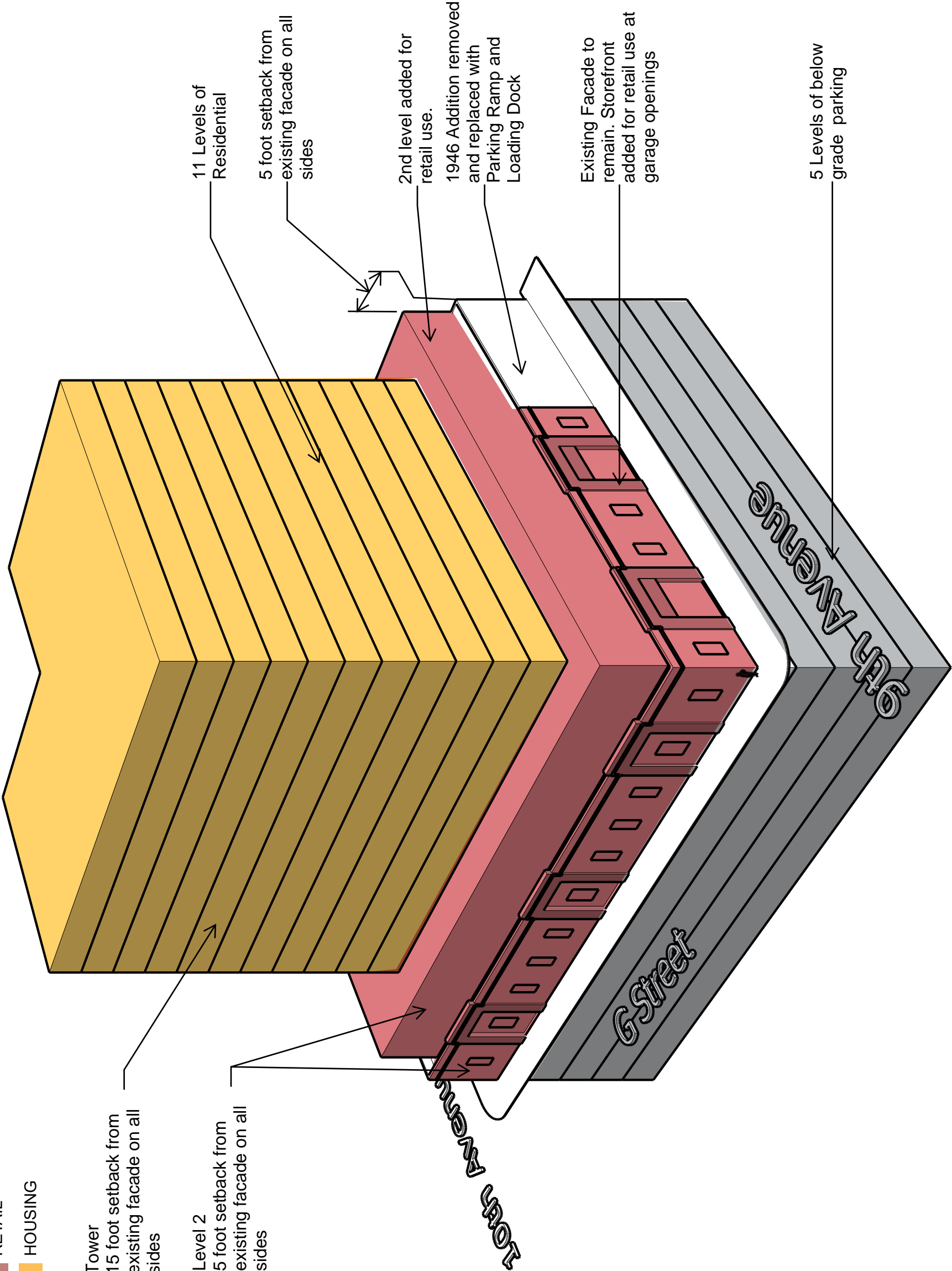


Alternative 3 - Typical Parking Layout

Alternative 3

Level B1			
Retail		19,500 sf	
BOH/Util		3,500 sf	
	Subtotal	23,000 sf	
Level 1			
Retail		14,500 sf	
Lobby/Leasing		3,400 sf	
BOH/Util		6,850 sf	
	Subtotal	24,750 sf	
Levels 2-6 Parking			
Retail Parking		15,300 sf	34
Residential Parking		69,700 sf	155
	Subtotal	85,000 sf	189
Levels 7-16			
Residential Units		139,400 sf	155
Residential Amenities		5,000 sf	
Core/Corridor		20,910 sf	
	Subtotal	165,310 sf	
Total Gross		298,060 sf	
Retail N.R.		34,000 sf	
Residential N.R.		139,400 sf	
Units Mix			
Type	Average Unit Size	# of Units	% of Units
Studio	700 sf	39	25.2%
1 Bedroom	800 sf	33	21.3%
1 Bed + Den	900 sf	42	27.1%
2 Bedroom	1,100 sf	25	16.1%
3 Bedroom	1,275 sf	16	10.3%
Total / Ave.		899 sf	155
		100.0%	139,400 sf

- PARKING
- RETAIL
- HOUSING



Alternative 4

Levels B1-B5		
Retail Parking	15,300 sf	34
Residential Parking	98,700 sf	168
BOH/Util	3,500 sf	
<i>Subtotal</i>	<i>117,500 sf</i>	<i>202</i>

Level 1		
Retail	14,000 sf	
Lobby/Leasing	3,400 sf	
BOH/Util	7,500 sf	
<i>Subtotal</i>	<i>24,900 sf</i>	

Level 2		
Retail	20,000 sf	

Levels 3-13		
Residential Units	151,200 sf	168
Residential Amenities	5,000 sf	
Core/Corridor	28,800 sf	
<i>Subtotal</i>	<i>185,000 sf</i>	

Total Gross	347,400 sf	
Retail N.R.	34,000 sf	
Residential N.R.	151,200 sf	

Units Mix Type	Average Unit Size	# of Units	% of Units	Total SF
Studio	700 sf	43	25.0%	30,100 sf
1 Bedroom	800 sf	35	21.0%	28,000 sf
1 Bed + Den	900 sf	45	27.0%	40,500 sf
2 Bedroom	1,100 sf	27	16.0%	29,700 sf
3 Bedroom	1,272 sf	18	11.0%	22,900 sf

<i>Total / Ave.</i>	<i>900 sf</i>	<i>168</i>	<i>100.0%</i>	<i>151,200 sf</i>
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UNION ARCHITECTURE INC.344 22nd STREET. SAN DIEGO, CA. 92102 619-269-4941**TREATMENT PLAN**

DATE: March 20, 2020

PROJECT: The Pacific Telephone and Telegraph Co. Garage
 Located at:
 660 10th Ave. and 559 9th Ave.
 San Diego, CA 92101
 APN: # 535-136-01-00
 HRB Site #1355
 Year built: 1932 and 1946
 Period of Significance: 1932-1956

SUBJECT: Treatment Plan for rehabilitation.

PROJECT TEAM:

Developer: Paul Thometz, Cisterra Partners, LLC
 Project Architect: Frank A. Landry, Carrier Johnson + Culture.
 Historic Architect & Monitor: John H. Eisenhart, Union Architecture Inc.
 Principal Investigator: Marie Burke Lia, Attorney at Law
 General Contractor: T.B.D.
 Structural Engineer: T.B.D.

PROPERTY DESCRIPTION:

The Pacific Telephone and Telegraph Co. Garage building, a one story commercial building, is located at 660 10th Ave and 559 9th Ave. It was initially constructed in 1932 in the Art Deco Style, an addition to the south, similar in style was built in 1946 (period of significance of resource is 1932 to 1956). The parking garage, later warehouse building, is historically designated under City of San Diego Historic guidelines with the HRB #1355.

The City of San Diego Historical Resources Board based its designation of the resource on the following findings:

Criterion A: The property is historically significant as a special element of the City's economic development, revealing the increasing importance of the automobile as America's favored method of private transportation from 1900-1956, the building retains integrity to its 1932 – 1956 period of significance, specifically, the resource which was constructed in 1932 for use as a garage, continues to convey the significant character defining features of the automobile-related development as identified in the east Village

Historical Context Statement, including a single story, warehouse-like massing; limited ornamentation, raised vertical projections and numerous garage doors.

Criterion C: The property is historically significant through retention of character defining features of the Art Deco Style and retains a good level of architectural integrity from its 1932 period of significance. Specifically, the resource exhibits smooth wall surfaces, a flat roof, vertical projections above the roofline, fluted pilasters, geometric detailing and metal windows.

The footprint of the 1932 building is rectangular, it measures approximately 100' x 200'. The later addition to the south measures 36' x 60'.

Exterior walls are mostly constructed of board formed concrete, with the western portion of the southern elevation of the 1932 building consisting of concrete post and beam construction with brick in-fill.

The 3 main elevations (the street elevations) are laid out symmetrical. They are structured horizontally into a base zone, main wall field and a continuous decorative cornice / parapet banding at the top. All decorative detailing is cast in place concrete as part of the wall.

All door openings have a decorative motif at the top. Windows are commercial steel sash type with wire mesh glazing. They are covered with protective metal woven mesh on the exterior. The roof and ceiling are comprised of a wooden "bow" truss system. There are six skylights on the roof of the 1932 structure. All roofing is built-up roofing.

The building will be rehabilitated as part of a new development on the site. The 1932 building will be integrated with a new multi-story mixed-use building, the 1946 addition will be demolished. Modifications to the 1932 building include demolition of the ceiling / roof and demolition of the south elevation. Existing historic openings (doors and windows) on the east, north and west elevations will be modified in select locations to allow for the new use. This Treatment Plan is being prepared to rehabilitate the historic structure at the existing location.

INTRODUCTION:

The implementation of the Treatment Plan for rehabilitation is under the supervision of the Project Architect and Historic Architect / Monitor in a manner consistent with the mitigation, monitoring, and reporting program for this project. This Treatment Plan is accompanied by a copy of HABS drawings of the property prepared by the Historic Architect / Monitor, drawings that outline the proposed stabilization and preparation of the structure for rehabilitation. This Treatment Plan and its related drawings will be included in all subsequent plans for the discretionary permit processing, construction documents, building permit, and observational process.

PREPARATION OF STRUCTURE:

1. Preparation of the structure prior to any rehabilitation / demolition work:

The entire structure at 660 10th Ave. is to be stabilized, braced, and secured. Specific procedures to be determined by qualified structural engineers and general contractor.

The implementation of these procedures will occur only after review from Historic Architect / Monitor. Exterior concrete trim pieces, windows and doors affected by this shall be protected from damage.

Windows and doors to remain should be protected in place, optionally, they may be removed, labeled and stored to be reinstalled during rehabilitation and fenestration openings to be covered with 3/4" plywood nailed to wood bracing. This option may be acceptable per monitor's approval.

Any existing exterior plumbing pipes, wiring etc. shall be removed along with site utilities to be disconnected. Monitor to be notified prior to modification of structure required for rehabilitation. Consistent with Standards #6, 7, 9 and 10.

2. Removal of interior of structure / select demolition at exterior:

Prior to the start of any demolition / removal process the Contractor and Monitor will meet on site to review the scope of demolition / removal work.

The entire 1946 structure, located at 559 9th Ave. will be demolished. The 1932 structure located at 660 10th Ave. will only be partially demolished as described in the Treatment Plan drawings and as follows: The wood stud walls, plaster, windows and doors on the first and second floor will be demolished. The roof / ceiling will be demolished. The concrete floor will be demolished. The southern concrete / brick-in-fill exterior wall will be demolished. Select existing openings will be enlarged on the street facades.

Contractor shall inform Monitor of discovery of items during demolition and removal. The Monitor will evaluate relevance of such materials on site. Consistent with Standards #6, 7, and 9.

3. Post-demo stabilization and protection of structure:

The entire remaining structure is to be stabilized, braced, and secured during the rehabilitation process. Specific procedures to be determined by qualified structural engineers and general contractor.

The implementation of these procedures will occur only after review from Historic Architect / Monitor. Exterior concrete trim pieces, windows and doors shall be protected from potential damage during construction.

EXISTING FOUNDATION:

The Pacific Telephone and Telegraph Co. Garage building has concrete foundation walls and footings. The depth of the foundation and footing cannot be determined prior to excavation. Consistent with Standards #9 and 10.

NEW FOUNDATION:

The east, north and west elevations are to remain in place. They will be stabilized following the structural engineer / general contractor's recommendations. This may involve new shotcrete on the interior side of existing concrete exterior walls, new foundation walls, steel reinforcement on the interior side (new openings) and new footings.

Consistent with Standards #9 and 10

EXISTING WALLS / EXISTING FRAMING:

The existing walls consist of board formed concrete, painted at exterior and interior. The south elevation consists of brick-in fill and boardformed concrete, this exterior wall will be demolished entirely.

Consistent with Standards #2, 9 and 10.

NEW WALLS / NEW FRAMING:

The existing board formed concrete walls will remain in place at the east, north and west elevations. These elevations will be affected by modifications to allow for the new use. Select existing window and door openings will be modified.

The existing building will be incorporated with a new development. New construction will be differentiated from existing historic.

Consistent with Standards #2, 9 and 10.

ROOF:

The roof and ceiling will be demolished. Any new roofing, gutters and downspouts shall be located as not to detract / conflict with the existing facades. Consistent with Standards #6, 9 and 10.

EXTERIOR FINISHES:

The existing board formed concrete walls will remain in place in the east, north and west elevations. The concrete finish is painted. Any cracks and exposed steel reinforcement shall be repaired during the rehabilitation process to protect the existing concrete structure from progressing damage due to water intrusion. Repairs shall be done in a manner chemically compatible with the composition of the existing historic concrete mixture (follow Preservation Brief #15 of the National Park Service of the US

Department of the Interior, <https://www.nps.gov/tps/how-to-preserve/briefs/15-concrete.htm#preservation>)
Consistent with Standards #2, 6, 7, 9, and 10.

EXTERIOR DOORS AND WINDOWS:

The steel doors and windows are generally in good shape. The contractor should repair as needed for smooth operation. If a feature of the sash or frame is missing or deteriorated beyond repair (monitor to determine condition) a replicated profile of the missing element shall be used. If the sash and fixed units are a new unit, they should match the profile of the existing original rail and style (ie. Hope's steel windows, Torrance Co. or equal, shop drawings to be submitted for approval). Follow Preservation Brief #13, of the National Park Service of the US Department of the Interior, <https://www.nps.gov/tps/how-to-preserve/briefs/13-steel-windows.htm#repair>
All of these options would be consistent with the standards, repair is generally the preferred option. The single leaf entry door on the east elevation is non-original.
Consistent with Standards #2, 6, 7, 9 and 10.

ELECTRICAL & LIGHTING:

Existing electrical and lighting system will be upgraded to conform to current code. All conduit and non-historic electrical and lighting on the exterior shall be removed. Electrical meters shall be located discretely away from public view. Exterior lighting fixtures to be surface mounted and to be sympathetic to Art Deco style influence.
Consistent with Standards #9 and 10.

PLUMBING:

All exterior plumbing and vent pipes to be dismantled. New interior plumbing and vents to be installed as required. The plumbing system will be upgraded to conform to current code. Consistent with Standards #9 and 10.

HEATING:

New HVAC to conform with current code, any equipment is to be screened from public view. Consistent with Standards #9 and 10.

PAINTING:

If lead paint is detected, follow current laws for careful removal or apply removal per EPA lead paint regulations.

New paint scheme on the exterior of the building shall be in Art Deco style colors.
Historic monitor and City staff to approve final painting scheme.
The colors can be existing scheme or a painting scheme from Dunn Edwards Company: Trade Winds. Det647 - Field

So Chic. Det614 - Trim, cornice, fluting.
Red Clay. Det447 - Windows
Consistent with Standards #6.

LANDSCAPING:

The new site will be landscaped and hardscaped in accordance with all relevant regulations of the Land Development Code for the relocation, rehabilitation, and reuse of historic resources. Consistent with Standards #9 and 10.

REPAIR WORK:

Repair work is preferred over use of replicated historic new materials. Retention of as much historic fabric as possible is paramount during rehabilitation. If a historic element is damaged but not in jeopardy of creating a performance issue with regard to the envelope of the building or building system, the feature should remain. Consistent with Standards #2, 5, 6 and 7.

RECONSTRUCTION / REHABILITATION:

The cleaning of all historic material / fabric shall occur by using the gentlest means possible. An appropriate means of control and disposal of lead or other chemicals shall be provided. Historic fabric shall be retained as much as possible. Do not sandblast or water power wash materials.

The character defining massing / form of the structure is a one story commercial building, 100 ft. x 200 ft. with wood "bow" trusses spanning across at 15'-0" o.c.. The character defining material elements are: Board formed concrete walls with concrete detailing at base, cornice and parapet, large steel windows with wire-glass panes and overhead roll-up doors.

Should damage occur to the resource, it shall be repaired in conformance with the Secretary of the Interior's Standards for Rehabilitation or Reconstruction. Attachment of materials shall be similar to the original, historic method. Consistent with Standards #2, 6, 9, and 10.

ATTACHMENTS:

Treatment Plan Drawings.

The Secretary of the Interior's Standards for Rehabilitation

The Standards (Department of Interior regulations, 36 CFR 67) pertain to historic buildings of all materials, construction types, sizes, and occupancy and encompass the exterior and the interior, related landscape features and the building's site and environment as well as attached, adjacent, or related new construction. The Standards

are to be applied to specific rehabilitation projects in a reasonable manner, taking into consideration economic and technical feasibility.

1. A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment.
2. The historic character of a property shall be retained and preserved. The removal of historic materials or alteration of features and spaces that characterize a property shall be avoided.
3. Each property shall be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or architectural elements from other buildings, shall not be undertaken.
4. Most properties change over time; those changes that have acquired historic significance in their own right shall be retained and preserved.
5. Distinctive features, finishes, and construction techniques or examples of craftsmanship that characterize a property shall be preserved.
6. Deteriorated historic features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature shall match the old in design, color, texture, and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical, or pictorial evidence.
7. Chemical or physical treatments, such as sandblasting, that cause damage to historic materials shall not be used. The surface cleaning of structures, if appropriate, shall be undertaken using the gentlest means possible.
8. Significant archeological resources affected by a project shall be protected and preserved. If such resources must be disturbed, mitigation measures shall be undertaken.
9. New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment.
10. New additions and adjacent or related new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

UNION ARCHITECTURE INC.

344 22nd STREET, SAN DIEGO, CA. 92102 T:619-788-2862

MONITORING PLAN

DATE: March 20, 2020

PROJECT: The Pacific Telephone and Telegraph Co. Garage
Located at:
660 10th Ave. and 559 9th Ave.
San Diego, CA 92101
APN: # 535-136-01-00
HRB Site #1355
Year built: 1932 and 1946
Period of Significance: 1932-1956

SUBJECT: Monitoring Plan for rehabilitation.

PROJECT TEAM:

Developer (D):	Paul Thometz, Cisterra Partners.
Project Architect (PA):	Frank A. Landry, Carrier Johnson + Culture.
Historic Architect & Monitor (HAM):	John Eisenhart, Union Architecture Inc.
Principal Investigator (PI):	Marie Burke Lia, Attorney at Law
General Contractor (GC):	T.B.D.
Structural Engineer (SE):	T.B.D.
Construction Manager (CM):	T.B.D.
Building Inspector (BI):	T.B.D.
City of San Diego, Environmental (EM):	Jennifer Wakem
City of San Diego, Planning Historical (HM) :	Suzanne Segur

LOCATIONS: Monitoring will take place on the site at 660 10th Avenue.

PROPERTY DESCRIPTION:

The Pacific Telephone and Telegraph Co. Garage building, a one story commercial building, is located at 660 10th Ave and 559 9th Ave. It was initially constructed in 1932 in the Art Deco Style, an addition to the south, similar in style was built in 1946. The parking garage, later warehouse building, is historically designated and significant under City of San Diego Historic guidelines (HRB #1355) per Criteria "A" (special element of the City's Development) and "C" (Architecture). The typology of the resource as an auto garage is a significant character defining feature of the automobile-related development of the City and

its Architecture is a good example of the Art Deco Style. The period of significance is 1932-1956.

This Monitoring Plan will follow the Treatment Plan and supporting architectural documents prepared to rehabilitate this historic structure at its current location at 660 10th Avenue.

Treatment Plan documents shall be used by Monitor as guidelines.

Monitoring actions are as follows:

1. Pre-construction meeting (D, PA, HAM, GC, PI, CM, City of SD: BI, EM & HM)

Issue:

- a. Overview of Treatment Plan and Monitoring Plan as related to historic resource rehabilitation.

2. Preparation of structure for demolition (HAM, CM, PA, SE).

Issue:

- a. Monitor to be present **prior** to removal / demolition of any interior walls, any windows, doors, wooden roof, skylights and south elevation.
- b. Discuss pre-demo stabilization and protection of historic structure.
- c. Review work involved by CM to protect-in-place / remove / take-off typical template samples for rehabilitation. Windows, doors, any items that may be removed and taken from the structure to be rehabilitated / reinstalled shall be labeled, photographed, removed and protected for transport and storage.

Any labels should include the following information:

- *Property name and address item was taken from.*
- *Photo of item (applicable for typical windows, typ. concrete profile etc.).*
- *Information where item was taken (location for window)*
- *Contact information of dismantling contractor.*
- *Property Owner contact.*
- *Project Architect contact / Historic Architect contact.*
- *Historic Monitor contact.*
- *City Historic Planner contact.*
- *Dismantling date.*

- d. Monitor to approve structure is ready for select demolition.

3. Select demolition of structure (HAM, PA, CM).

Issue:

- a. Demolition of interior walls, windows, doors, channels at overhead doors, wooden roof, skylights and south elevation are completed. Monitor to review completed work and any items that have been removed and labeled for storage
- b. Discuss post-demo stabilization and protection of historic structure.
- c. Overview of Treatment Plan, Architectural, Landscaping and Engineering documents as related to rehabilitation on site.

4. Review of stabilization and protection of historic structure during rehabilitation on site (new footings, foundation, shotcrete, utilities, site preparation) (D, HAM, PA, CM, SE).

Issue:

- a. Review of preparation work for rehabilitation on site.

5. Continued monitoring of rehabilitation of structure. Monthly or as required by construction activity (HAM, PA, CM).

Issue:

- a. Review rehabilitation of resource in accordance with Treatment Plan and Architectural, Landscaping and Engineering Documents.
- b. Review of rehabilitation of windows, doors, concrete repair, concrete details.

6. Final Monitoring (HAM, PA, CM, D).

Issue:

- a. Final punch list of items to complete rehabilitation work according to Treatment Plan and Architectural, Landscaping and Engineering Documents.

7. Draft Report (HAM, PI).

Issue:

- a. Draft report of monitoring process to be submitted to PI for review.

8. Final Report (HAM, PI, D).

Issue:

- a. Final report of monitoring process, review updating of HABS and Treatment Plan documents to be submitted to PI for distribution to Developmental Services and San Diego History Center for archives.

End of Monitoring

SAN DIEGO POLICE DEPARTMENT CONDITIONAL USE PERMIT RECOMMENDATION

PREMISE ADDRESS: 659 9th Av

TYPE OF BUSINESS: large retail space

FEDERAL CENSUS TRACT: 52.00

NUMBER OF ALCOHOL LICENSES ALLOWED: 4

NUMBER OF ALCOHOL LICENSES EXISTING: 5 (over-concentrated)

CRIME RATE IN THIS CENSUS TRACT: 747.2% - High Crime
(Note: Considered High Crime If Exceeds 120% of City-wide Average)

THREE OR MORE REPORTED CRIMES AT THIS PREMISE WITHIN PAST YEAR ☒ YES ☐ NO

IS THE PREMISE WITHIN 600 FEET OF INCOMPATIBLE FACILITY ☐ YES ☒ NO

IS THE PREMISE WITHIN 100 FEET OF RESIDENTIALLY ZONED PROPERTY ☒ YES ☐ NO

ABC LICENSE REVOKED AT THIS PREMISE WITHIN PAST YEAR ☐ YES ☒ NO

HAS APPLICANT BEEN CONVICTED OF ANY FELONY ☐ YES ☒ NO

WILL THIS BUSINESS BE DETRIMENTAL TO THE PUBLIC HEALTH, SAFETY,
AND WELFARE OF THE COMMUNITY AND CITY ☐ YES ☒ NO

COMMENTS/OTHER FACTORS CONSIDERED:

The premises is located within census tract 52.00. The reported crime rate for 2019 within Census Tract 52.00 was 747.2 and the alcohol crime rate was 502.4%.

SUGGESTED CONDITIONS:

Due to the extremely high crime rate and alcohol rate coupled with the overconcentration of Off-Sale alcohol licenses the San Diego Police Department agrees with the issuance of a Conditional Use Permit so long as the following conditions are incorporated:

1. No Off-Sale of distilled spirits.
2. No alcoholic beverages shall be sold except between the hours of 10:00 a.m. and 10:00 p.m.
3. Malt liquor, beer or wine cooler products, regardless of container size, must be sold in manufacturer pre-packaged multi-unit quantities totaling a minimum of 64 ounces.
4. No wine shall be sold in containers of less than 750 milliliters.
5. Loitering will be prohibited.
6. No public pay phones will be permitted on the premise or adjacent and under the control of the applicant.
7. No arcade games of any type will be allowed/maintained inside the premise.
8. Video surveillance shall be recording and available to law enforcement upon request covering the interior and public access points of the premises. Upon request of law enforcement video surveillance shall not be deleted, voided or destroyed. Recordings shall be maintained for a minimum of 30 days absent a request of law enforcement.
9. Litter and any unauthorized graffiti will be removed promptly.

10. All retail employees are trained in ABC regulations (LEAD), theft deterrence and policies against the sales of alcoholic beverages to those under 21 years of age, and/or intoxicated.

11. There shall be no exterior alcoholic advertising or signage of any kind or type, including advertising directed to the exterior from within, promoting or indicating the availability of alcoholic beverages. Interior displays of alcoholic beverages or signs which are clearly visible to the exterior shall constitute a violation of these conditions.

SAN DIEGO POLICE DEPARTMENT RECOMMENDATION:

APPROVE ☒

DENY ☐

Benjamin McCurry
Name of SDPD Vice Sergeant (Print)

(619) 531-2973
Telephone Number

[Signature]
Signature of SDPD Vice Sergeant

7-20-20
Date of Review



ABC Report

Required Parameters

Reporting Period: 01/2019 to 12/2019

Agency: SAN DIEGO

Optional Parameters

Geographical Area:

Group by: Census Tract

Prior Report Number: B98S328R

Total Part 1 Crime and Part 2 Arrest for Agency: 61,677

Total Census Tract: 302

Average Total per Census Tract: 204.2

(120% or above is High Crime Area indicated by *)

	Census Tract	Totals	% Average by Census Tract
88	004200	109	53.4%
89	004300	115	56.3%
90	004400	144	70.5%
91	004501	128	62.7%
92	004600	153	74.9%
93	004700	438	214.5%*
94	004800	284	139.1%*
95	004900	222	108.7%
96	005000	335	164.0%*
97	005100	3,438	1,683.4%*
98	005200	1,526	747.2%*
99	005300	2,341	1,146.3%*
100	005400	1,466	717.8%*
101	005500	19	9.3%
102	005600	1,151	563.6%*
103	005700	252	123.4%*
104	005800	428	209.6%*
105	005900	204	99.9%
106	006000	264	129.3%*
107	006100	141	69.0%
108	006200	56	27.4%
109	006300	21	10.3%
110	006500	2,249	1,101.2%*
111	006600	174	85.2%
112	006801	158	77.4%
113	006802	463	226.7%*
114	006900	149	73.0%
115	007002	47	23.0%
116	007100	128	62.7%

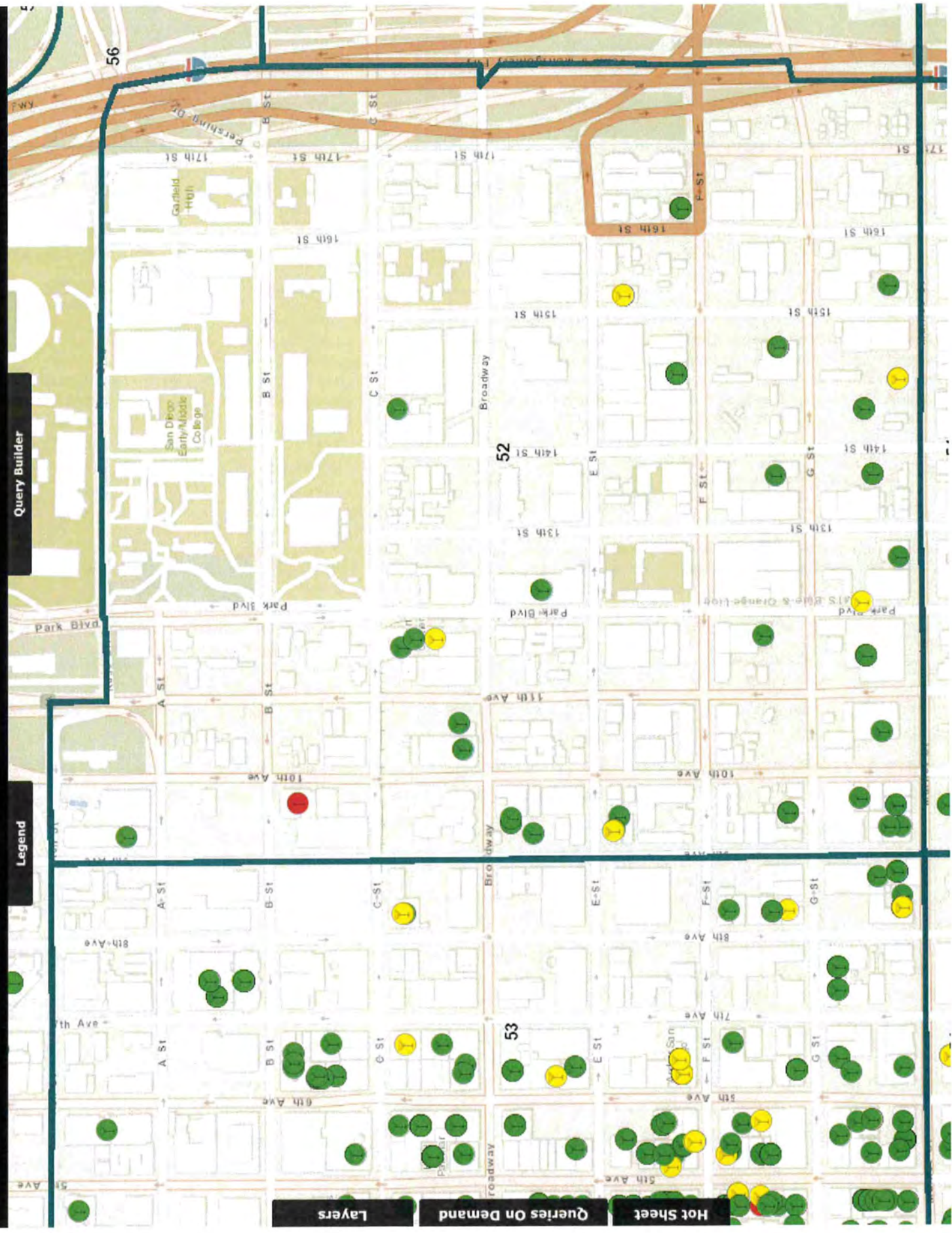
Legend

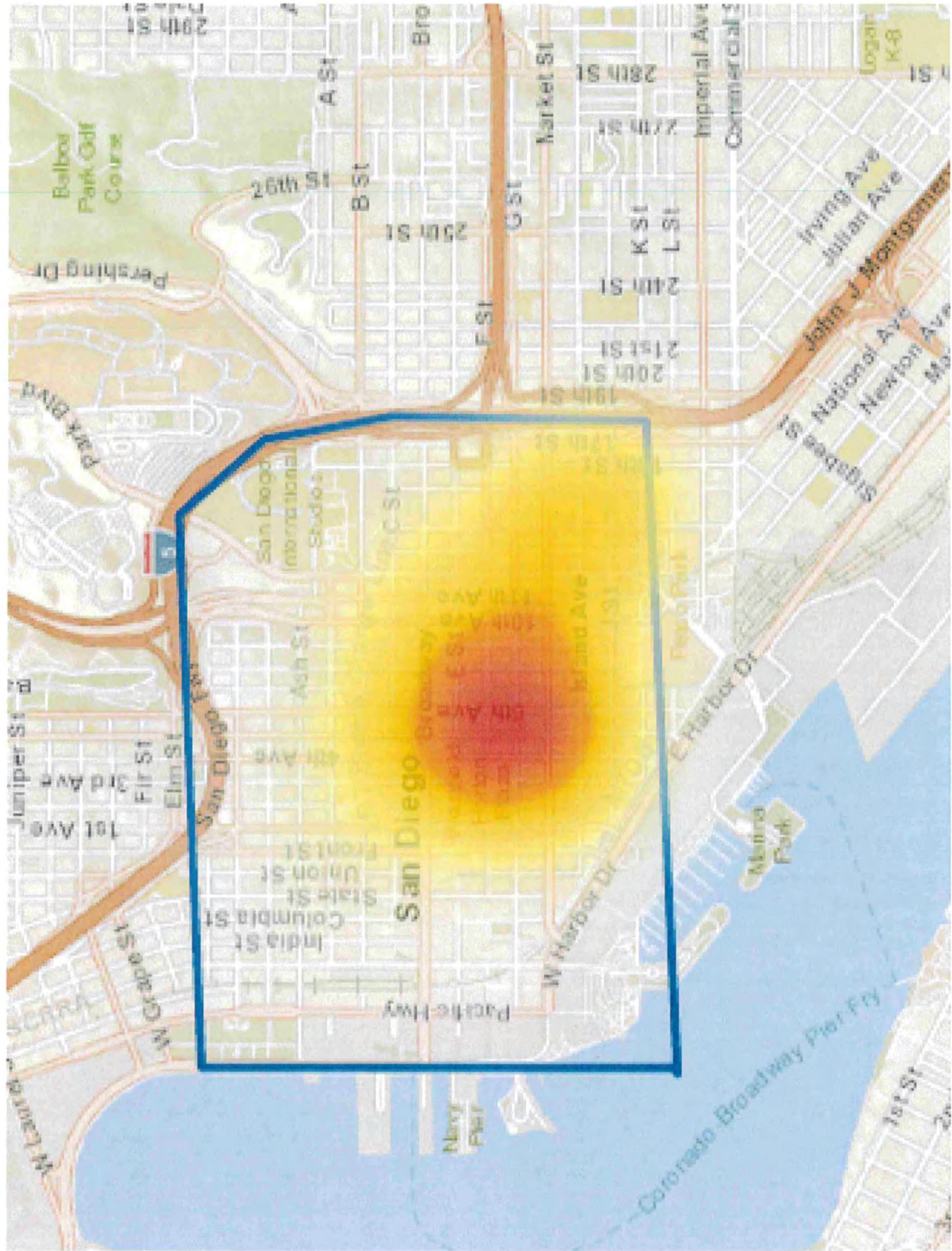
Query Builder

Layers

Queries On Demand

Hot Sheet





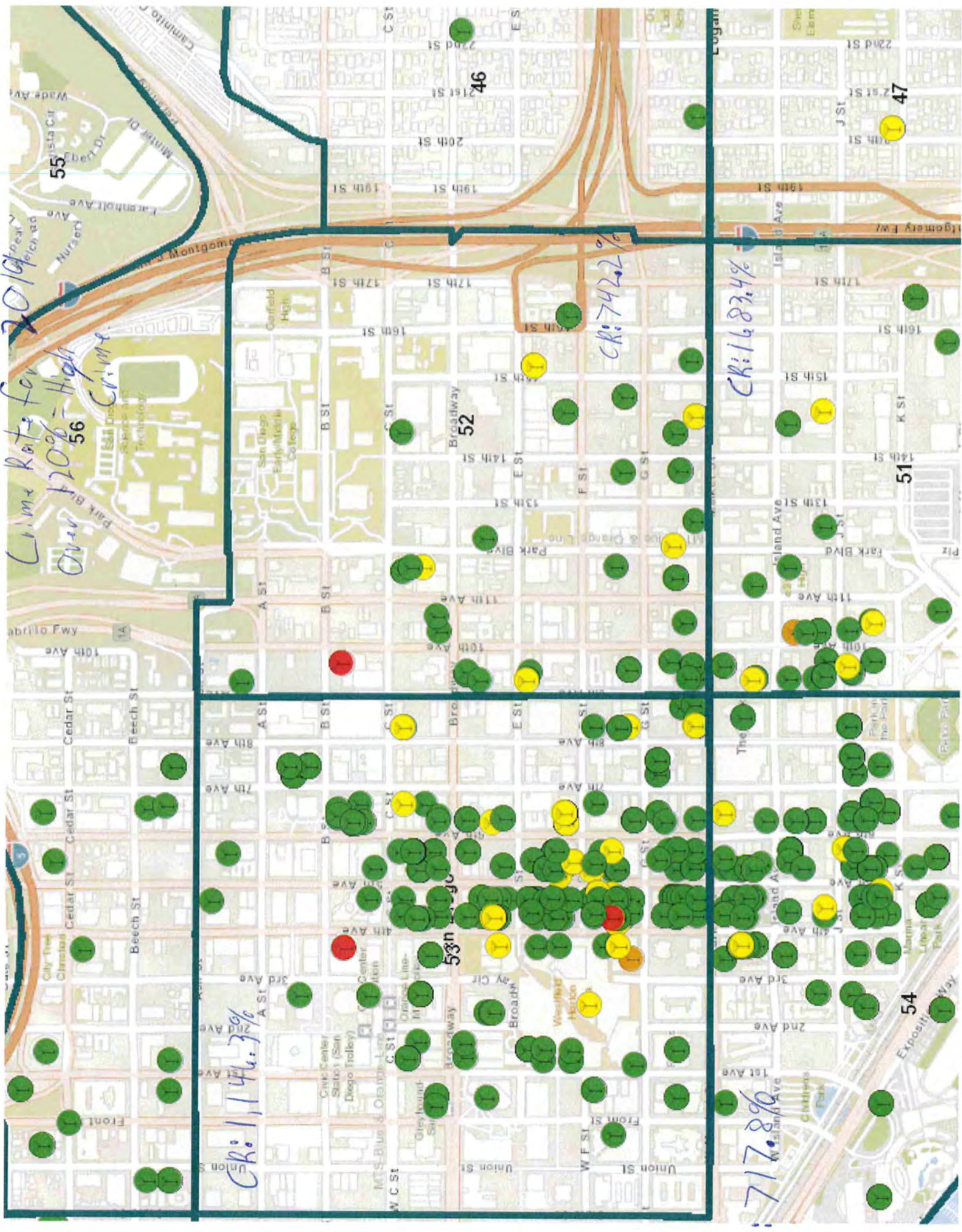
Crime Rate for 2019
Over 120% - High
56 Crime

CR: 114.7%

CR: 742.2%

CR: 1683.4%

CR: 717.8%



**DOWNTOWN
FINAL ENVIRONMENTAL IMPACT REPORT (Downtown FEIR)
CONSISTENCY EVALUATION
FOR THE
9G TOWER PROJECT
PTS# 649856**

JULY 2020

Prepared by: City of San Diego
Development Services Department
1222 First Avenue
San Diego, CA 92101

DOWNTOWN FEIR CONSISTENCY EVALUATION

1. PROJECT TITLE: 9G Tower ("Project")

2. DEVELOPER: Cisterra

3. PROJECT LOCATION AND SETTING: The Downtown Community Planning (DCP) area includes approximately 1,500 acres within the metropolitan core of the City of San Diego, bounded by Laurel Street and Interstate 5 on the north; Interstate 5, Commercial Street, 16th Street, Sigsbee Street, Newton Avenue, Harbor Drive, and the extension of Beardsley Street on the east and southeast; and San Diego Bay on the south and west and southwest. The major north-south access routes to downtown are Interstate 5, State Route 163, and Pacific Highway. The major east-west access route to downtown is State Route 94. Surrounding areas include the community of Uptown and Balboa Park to the north, Golden Hill and Sherman Heights to the east, Barrio Logan and Logan Heights to the South and the City of Coronado to the west across San Diego Bay.

The proposed redevelopment is located at a 0.57 acre site at 659 9th Avenue in the Employment/Residential Mixed-Use land use district of the Centre City Planned District Ordinance (CCPDO) and the East Village neighborhood of the DCP area.

4. PROJECT DESCRIPTION: Process Four Site Development Permit, Neighborhood Development Permit, Tentative Map, and Conditional Use Permit for the renovation of an existing historic building and construction of a new 432,175 square foot mixed-use development. The project is a 22-story high-rise consisting of 241 dwelling units, commercial retail at the basement level and ground floor, and 5 levels of above ground parking for 273 vehicles. The project would also include the installation of connections to existing utility lines and streetscape improvements to the surrounding property.

5. CEQA COMPLIANCE: The DCP, CCPDO, Redevelopment Plan for the Centre City Redevelopment Project and related activities have been addressed by the following environmental documents, which were prepared prior to this Consistency Evaluation and are hereby incorporated by reference:

FEIR for the DCP, CCPDO, and 10th Amendment to the Redevelopment Plan for the Centre City Project (State Clearinghouse Number 2003041001, certified by the Redevelopment Agency (Resolution No. R-04001) and the San Diego City Council (City Council) (Resolution No. R-301265), with date of final passage on March 14, 2006.

Addendum to the Downtown FEIR for the 11th Amendment to the Redevelopment Plan for the Centre City Redevelopment Project, Amendments to the DCP, CCPDO, Marina Planned District Ordinance, and Mitigation, Monitoring and Reporting Program of the Downtown FEIR for the DCP, CCPDO, and the Redevelopment Plan for the Centre City Redevelopment Project certified by the Redevelopment Agency (Resolution No. R-04193) and by the City Council (Resolution No. R-302932), with date of final passage on July 31, 2007.

Second Addendum to the Downtown FEIR for the proposed amendments to the DCP, CCPDO, Marina Planned District Ordinance, and Mitigation, Monitoring and Reporting

Program (MMRP) certified by the Redevelopment Agency (Resolution No. R-04508), with date of final passage on April 21, 2010.

Third Addendum to the Downtown FEIR for the RE District Amendments to the CCPDO certified by the Redevelopment Agency (Resolution No. R-04510), with date of final passage on April 21, 2010.

Fourth Addendum to the Downtown FEIR for the San Diego Civic Center Complex Project certified by the Redevelopment Agency (Resolution No. R-04544) with date of final passage on August 3, 2010.

Fifth Addendum to the Downtown FEIR for the Industrial Buffer Overlay Zone Amendments to the CCPDO certified by the City Council (Resolution No. R-308724) with a date of final passage on February 12, 2014.

Sixth Addendum to the Downtown FEIR for the India and Date Project certified by the City Council (Resolution No. R-309115) with a date of final passage on July 14, 2014.

The Final Supplemental Environmental Impact Report for the Downtown San Diego Mobility Plan certified by the City Council on June 21, 2016 (Resolution R-310561).

The City of San Diego FEIR for the Climate Action Plan (“CAP FEIR”) certified by the City Council on December 15, 2015, (City Council Resolution R-310175) which includes the Addendum to the CAP FEIR certified by the City Council on July 12, 2016.

The Downtown FEIR and the CAP FEIR are “Program EIRs” prepared in compliance with California Environmental Quality Act (CEQA) Guidelines Section 15168. The aforementioned environmental documents are the most recent and comprehensive environmental documents pertaining to the proposed Project. The Downtown FEIR and subsequent addenda are available for review at the City of San Diego, Development Services Department located at 1222 First Avenue, San Diego, CA 92101. The CAP FEIR is available at the offices of the City of San Diego Planning Department located at 1010 Second Avenue, Suite 1200, San Diego, CA 92101.

This Downtown FEIR Consistency Evaluation (“Evaluation”) has been prepared for the Project in compliance with State CEQA and Local Guidelines. Under these Guidelines, environmental review for subsequent proposed actions is accomplished using the Evaluation process, as allowed by Sections 15168 and 15180 of the State CEQA Guidelines. The Evaluation includes the evaluation criteria as defined in Section 15063 of the State CEQA Guidelines.

Under this process, an Evaluation is prepared for each subsequent proposed action to determine whether the potential impacts were anticipated in the Downtown FEIR and the CAP FEIR. No additional documentation is required for subsequent proposed actions if the Evaluation determines that the potential impacts have been adequately addressed in the CAP FEIR and the Downtown FEIR and subsequent proposed actions implement appropriate mitigation measures identified in the MMRP that accompanies the FEIR.

If the Evaluation identifies new impacts or a substantial change in circumstances, additional environmental documentation is required. The form of this documentation depends upon the nature of the impacts of the subsequent proposed action being proposed. Should a proposed

action result in: a) new or substantially more severe significant impacts that are not adequately addressed in the Downtown FEIR or CAP FEIR, or b) there is a substantial change in circumstances that would require major revision to the Downtown FEIR or the CAP FEIR, or c) that any mitigation measures or alternatives previously found not to be feasible or not previously considered would substantially reduce or lessen any significant effects of the Project on the environment, a Subsequent or Supplemental Environmental Impact Report (EIR) would be prepared in accordance with Sections 15162 or 15163 of the State CEQA Guidelines (CEQA Statutes Section 21166).

If the lead agency under CEQA finds that pursuant to Sections 15162 and 15163, no new significant impacts will occur or no new mitigation will be required, the lead agency can approve the subsequent proposed action to be within the scope of the Project covered by the Downtown FEIR and CAP FEIR, and no new environmental document is required.

6. PROJECT-SPECIFIC ENVIRONMENTAL ANALYSIS: See attached Environmental Checklist and Section 10 *Evaluation of Environmental Impacts*.

7. MITIGATION, MONITORING AND REPORTING PROGRAM: As described in the Environmental Checklist and summarized in **Attachment A**, the following mitigation measures included in the MMRP, found in Volume 1.B.2 of the Downtown FEIR, will be implemented by the proposed Project:

AQ-B.1-1; HIST-A.1-1; HIST-A.1-2; HIST-B.1-1; PAL-A.1-1; NOI-B.1-1; NOI-C.1-1; TRF-A.1.1-2

8. DETERMINATION: In accordance with Sections 15168 and 15180 of the CEQA Guidelines, the potential impacts associated with future development within the DCP area are addressed in the Downtown FEIR prepared for the DCP, CCPDO, and the six subsequent addenda to the Downtown FEIR listed in Section 6 above, as well as the Final Supplemental EIR for the Downtown San Diego Mobility Plan and the CAP FEIR. These documents address the potential environmental effects of future development within the Centre City Redevelopment Project based on build out forecasts projected from the land use designations, density bonus, and other policies and regulations governing development intensity and density. Based on this analysis, the Downtown FEIR and its subsequent addenda and the CAP FEIR, as listed in Section 6 above, concluded that development would result in significant impacts related to the following issues (mitigation and type of impact shown in parentheses):

Significant but Mitigated Impacts

- Air Quality: Construction Emissions (AQ-B.1) (D)
- Paleontology: Impacts to Significant Paleontological Resources (PAL-A.1) (D/C)
- Noise: Interior Traffic Level Increase on Grid Streets (NOI-B.1) (D)

Significant and Not Mitigated Impacts

- Air Quality: Mobile Source Emissions (AQ-A.1) (C)
- Historical Resources: Archeological and Built Environment (HIST-B.1) (D/C) and HIST-A.1-1 and HIST-A.1-2
- Water Quality: Urban Runoff (WQ-A.1) (C)

- Land Use: Physical Changes Related to Transient Activity (LU-B.6) (C)
- Noise: Exterior Traffic Level Increase on Grid Streets (NOI-A.1) (C)
- Traffic: Impact on Surrounding Streets (TRF-A.1) (C)
- Traffic: Impact on Freeway Ramps and Segments (TRF-A.2) (C)

In certifying the Downtown FEIR and approving the DCP, CCPDO, and 10th Amendment to the Redevelopment Plan, the City Council and Redevelopment Agency adopted a Statement of Overriding Considerations which determined that the unmitigated impacts were acceptable in light of economic, legal, social, technological or other factors including the following.

Overriding Considerations

1. Develop downtown as the primary urban center for the region
2. Maximize employment opportunities within the downtown area
3. Develop full-service, walkable neighborhoods linked to the assets downtown offers
4. Increase and improve parks and public spaces
5. Relieve growth pressure on outlying communities
6. Maximize the advantages of downtown's climate and waterfront setting
7. Implement a coordinated, efficient system of vehicular, transit, bicycle, and pedestrian traffic
8. Integrate historical resources into the new downtown plan
9. Facilitate and improve the development of business and economic opportunities located in the downtown area
10. Integrate health and human services into neighborhoods within downtown
11. Encourage a regular process of review to ensure that the Plan and related activities are best meeting the vision and goals of the Plan.

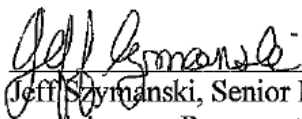
The proposed activity detailed and analyzed in this Evaluation are adequately addressed in the environmental documents noted above and there is no change in circumstance, substantial additional information, or substantial Project changes to warrant additional environmental review. Because the prior environmental documents adequately covered this activity as part of the previously approved Project, this activity is not a separate Project for purposes of review under CEQA pursuant to CEQA Guidelines Sections 15060(c)(3), 15180, and 15378(c).

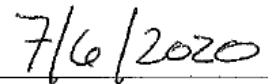
SUMMARY OF FINDINGS: In accordance with Public Resources Code Sections 21166, 21083.3, and CEQA Guidelines Sections 15168 and 15183, the following findings are derived from the environmental review documented by this Evaluation and the Downtown FEIR and CAP FEIR as amended:

1. No substantial changes are proposed in the Centre City Redevelopment Project, or with respect to the circumstances under which the Centre City Redevelopment Project is to be undertaken as a result of the development of the proposed Project, which will require important or major revisions in the Downtown FEIR and the six subsequent addenda to the FEIR or with the CAP FEIR;
2. No new information of substantial importance to the Centre City Redevelopment Project has become available that shows the Project will have any significant effects not discussed previously in the Downtown FEIR or subsequent addenda to the Downtown FEIR or CAP FEIR; or that any significant effects previously examined will be substantially more severe than shown in the CAP FEIR and the Downtown

FEIR or subsequent addenda to the FEIR; or that any mitigation measures or alternatives previously found not to be feasible or not previously considered would substantially reduce or lessen any significant effects of the Project on the environment;

3. No Negative Declaration, Subsequent EIR, or Supplement or Addendum to the CAP EIR and the Downtown FEIR, as amended, is necessary or required;
4. The proposed actions will have no significant effect on the environment, except as identified and considered in the CAP FEIR and the Downtown FEIR and subsequent addenda to the Downtown FEIR for the Centre City Redevelopment Project. No new or additional project-specific mitigation measures are required for this Project; and
5. The proposed actions would not have any new effects that were not adequately covered in the CAP FEIR and Downtown FEIR or addenda to the Downtown FEIR, and therefore, the proposed Project is within the scope of the program approved under the CAP FEIR and Downtown FEIR and subsequent addenda listed in Section 6 above.


Jeff Szymanski, Senior Planner, City of San Diego
Lead Agency Representative/Preparer


Date

ENVIRONMENTAL CHECKLIST

10. EVALUATION OF ENVIRONMENTAL IMPACTS

This environmental checklist evaluates the potential environmental effects of the proposed Project consistent with the significance thresholds and analysis methods contained in the CAP FEIR and the Downtown FEIR for the DCP, CCPDO, and Redevelopment Plan for the Centre City Project Area. Based on the assumption that the proposed activity is adequately addressed in the Downtown FEIR and CAP FEIR, the following table indicates how the impacts of the proposed activity relate to the conclusions of the Downtown FEIR and CAP FEIR. As a result, the impacts are classified into one of the following categories:

- Significant and Not Mitigated (SNM)
- Significant but Mitigated (SM)
- Not Significant (NS)

The checklist identifies each potential environmental effect and provides information supporting the conclusion drawn as to the degree of impact associated with the proposed Project. As applicable, mitigation measures from the Downtown FEIR and CAP FEIR are identified and are summarized in **Attachment A** to this Evaluation. Some of the mitigation measures are plan-wide and not within the control of the proposed Project. Other measures, however, are to be specifically implemented by the proposed Project. Consistent with the Downtown FEIR and CAP FEIR analysis, the following issue areas have been identified as Significant and Not Mitigated even with inclusion of the proposed mitigation measures, where feasible:

- Air Quality: Mobile Source Emissions (AQ-A.1) (C)
- Historical Resources: Archeological (HIST-B.1) (D/C)
- Water Quality: Urban Runoff (WQ-A.1) (C)
- Land Use: Physical Changes Related to Transient Activity (LU-B.6) (C)
- Noise: Exterior Traffic Level Increase on Grid Streets (NOI-A.1) (C)
- Traffic: Impact on Surrounding Streets (TRF-A.1) (C)
- Traffic: Impact on Freeway Ramps and Segments (TRF-A.2) (C).

The following Overriding Considerations apply directly to the proposed Project:

1. Develop downtown as the primary urban center for the region
2. Maximize employment opportunities within the downtown area
3. Develop full-service, walkable neighborhoods linked to the assets downtown offers
4. Relieve growth pressure on outlying communities
5. Implement a coordinated, efficient system of vehicular, transit, bicycle, and pedestrian traffic
6. Facilitate and improve the development of business and economic opportunities located in the downtown area

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
1. AESTHETICS/VISUAL QUALITY:						
<p>(a) <i>Substantially disturb a scenic resource, vista or view from a public viewing area, including a State scenic highway or view corridor designated by the DCP?</i></p> <p>Views of scenic resources including San Diego Bay, San Diego-Coronado Bay Bridge, Point Loma, Coronado, Petco Park, and the downtown skyline are afforded by the public viewing areas within and around the downtown and along view corridor streets within the planning area.</p> <p>The CCPDO includes several requirements that reduce a project's impact on scenic vistas. These include view corridor setbacks on specific streets to maintain views and controls building bulk by setting limits on minimum tower spacing, street wall design, maximum lot coverage, and building dimensions.</p> <p>The site does not possess any significant scenic resources that could be impacted by the proposed Project therefore impacts to on-site scenic resources are not significant. Impacts associated with scenic vistas would be similar to the DCP FEIR and would not be significant.</p>					X	X
<p>(b) <i>Substantially incompatible with the bulk, scale, color and/or design of surrounding development?</i></p> <p>The bulk, scale, and design of the Project would be compatible with existing development in the area. Development of the site would improve the area by providing a new, modern building on a currently underutilized site. The Project would utilize high quality materials and contemporary design sensitive to the character of the surrounding neighborhood. Additionally, a variety of existing and planning low, mid, and high-rise buildings are located within the</p>					X	X

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
<p>vicinity of the Project site and the scale of the proposed Project would be consistent with that of surrounding buildings. The project would also, incorporate the historically designated Pacific Telephone and Telegraph Co. Garage building into the design of the project.</p> <p>Therefore, project-level and cumulative impacts associated with this issue would not occur.</p>						
<p><i>(c) Substantially affect daytime or nighttime views in the area due to lighting?</i></p> <p>The proposed project would not involve a substantial amount of exterior lighting or include materials that would generate substantial glare. Furthermore, outdoor lighting that would be incorporated into the proposed project would be shielded or directed away so that direct light or glare does not adversely impact adjacent land uses. The City's Light Pollution Law (San Diego Municipal Code (SDMC) Section 101.1300 et seq.) also protects nighttime views (e.g., astronomical activities) and light-sensitive land uses from excessive light generated by development in the downtown area. The proposed project's conformance with these requirements would ensure that direct and cumulative impacts associated with this issue are not significant.</p>					X	X
2. AGRICULTURAL RESOURCES:						
<p><i>(a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland) to non-agricultural use?</i></p> <p>The DCP Area is an urban downtown environment that does not contain land designated as prime agricultural soil by the Soils Conservation Service. In</p>					X	X

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
addition, it does not contain prime farmland designated by the California Department of Conservation. Therefore, no impact to agricultural resources would occur.						
<p><i>(b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?</i></p> <p>The DCP Area does not contain, nor is it near, land zoned for agricultural use or land subject to a Williamson Act Contract pursuant to Section 512101 of the California Government Code. Therefore, impacts resulting from conflicts with existing zoning for agricultural use or a Williamson Act Contract would not occur.</p>					X	X

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
3. AIR QUALITY:						
<p>(a) <i>Conflict with or obstruct implementation of an applicable air quality plan, including the County's Regional Air Quality Strategies (RFS) or the State Implementation Plan?</i></p> <p>The proposed Project site is located within the San Diego Air Basin, which is under the jurisdiction of the San Diego Air Pollution Control District (SDAPCD). The San Diego Air Basin is designated by state and federal air quality standards as nonattainment for ozone and particulate matter (PM) less than 10 microns (PM10) and less than 2.5 microns (PM 2.5) in equivalent diameter. The SDAPCD has developed a Regional Air Quality Strategy (RAQS) to attain the state air quality standards for ozone. The proposed Project is consistent with the land use and transit-supportive policies and regulations of the DCP and CCPDO; which are in accordance with those of the RAQS. Therefore, the proposed Project would not conflict with, but would help implement, the RAQS with its compact, high intensity land use and transit-supportive design. Therefore, no impact to the applicable air quality plan would occur.</p>					X	X
<p>(b) <i>Expose sensitive receptors to substantial air contaminants including, but not limited to, criteria pollutants, smoke, soot, grime, toxic fumes and substances, particulate matter, or any other emissions that may endanger human health?</i></p> <p>The Project could involve the exposure of sensitive receptors to substantial air contaminants during short-term construction activities and over the long-term operation of the Project. Construction activities associated with the Project could result in potentially significant impacts related to the exposure of sensitive receptors to substantial emissions of particulate matter. The potential for impacts to sensitive receptors</p>			X			X

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
<p>during construction activities would be mitigated to below a level of significance through compliance with the City’s mandatory standard dust control measures and the dust control and construction equipment emission reduction measures required by FEIR Mitigation Measure AQ-B.1-1 (Attachment A).</p> <p>The Project could also involve the exposure of sensitive receptors to air contaminants over the long-term operation of the Project, such as carbon monoxide exposure (commonly referred to as CO “hot spots”) due to traffic congestion near the Project site. However, the FEIR concludes that development within the DCP Area would not expose sensitive receptors to significant levels of any of the substantial air contaminants. Since the land use designation of the proposed development does not differ from the land use designation assumed in the FEIR analysis, the Project would not expose sensitive receptors to substantial air contaminants beyond the levels assumed in the FEIR. Additionally, the Project is not located close enough to any industrial activities to be impacted by any emissions potentially associated with such activities. Therefore, impacts associated with this issue would not be significant. Project impacts associated with the generation of substantial air contaminants are discussed below in Section 3.c.</p>						
<p>(c) <i>Generate substantial air contaminants including, but not limited to, criteria pollutants, smoke, soot, grime, toxic fumes and substances, particulate matter, or any other emissions that may endanger human health?</i></p> <p>Implementation of the Project could result in potentially adverse air quality impacts related to the following air emission generators: construction and mobile-sources. Site preparation activities and construction of the Project would involve short-term, potentially adverse impacts associated with the creation of dust and the generation of</p>		X	X			

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
<p>construction equipment emissions. The clearing, grading, excavation, and other construction activities associated with the Project would result in dust and equipment emissions that, when considered together, could endanger human health. Implementation of Downtown FEIR Mitigation Measure AQ-B.1-1 (see Attachment A) would reduce dust and construction equipment emissions generated during construction of the Project to a level below significance.</p> <p>The air emissions generated by automobile trips associated with the Project would not exceed air quality significance standards established by the San Diego Air Pollution Control District. However, the Project's mobile source emissions, in combination with dust generated during the construction of the Project, would contribute to the significant and unmitigated cumulative impact to air quality identified in the Downtown FEIR. No uses are proposed that would significantly increase stationary-source emissions in Downtown; therefore, impacts from stationary sources would be not significant.</p>						
4. BIOLOGICAL RESOURCES:						
<p>(a) <i>Substantially effect, either directly or through habitat modifications, any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by local, state or federal agencies?</i></p> <p>Due to the highly urbanized nature of the DCP Area, there are no sensitive plants or animal species, habitats, or wildlife migration corridors. In addition, the ornamental trees and landscaping included in the Project are considered of no significant value to the native wildlife in their proposed location. Therefore, no impact associated with this issue could occur.</p>					X	X

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
<p><i>(b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, and regulations by local, state or federal agencies?</i></p> <p>As identified in the FEIR, the DCP Area is not within a sub-region of the San Diego County Multiple Species Conservation Program (MSCP). Therefore, impacts associated with substantial adverse effects on riparian habitat or other sensitive natural communities identified in local or regional plans, policies, and regulations by local, state or federal agencies would not occur.</p>					X	X
5. GEOLOGY AND SOILS:						
<p><i>(a) Substantial health and safety risk associated with seismic or geologic hazards?</i></p> <p>The site is not located within a currently established State of California Earthquake Fault Zone but is within the Downtown Special Fault Zone.</p> <p>Nova Services Inc. prepared a Geotechnical Investigation (September 2019) and a Fault Investigation (April 2020) for the Project. It states that based on the results of the investigation, Nova Services finds that the Project site is not transected by active faults and the risk of surface rupture is considered to be very low and that the site can be developed as proposed provided the recommendations of the investigation are followed and implemented during design and construction.</p> <p>Although the potential for geologic hazards (landslides, liquefaction, slope failure, and seismically-induced settlement) is considered low due to the site's moderate to non-expansive geologic structure, such hazards could nevertheless occur. Conformance with, and implementation of, all</p>					X	X

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
seismic-safety development requirements, including all applicable requirements of the Alquist-Priolo Zone Act, the seismic design requirements of the International Building Code (IBC), the City of San Diego Notification of Geologic Hazard procedures, and all other applicable requirements would ensure that the potential impacts associated with seismic and geologic hazards are not significant.						
6. GREENHOUSE GAS EMISSIONS:						
<p>(a) <i>Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?</i></p> <p>The DCP provides for the growth and buildout of Downtown. The City's CAP FEIR analyzed greenhouse gas ("GHG") emissions on a citywide basis – inclusive of the anticipated assumptions for the growth and buildout of Downtown. The City's CAP outlines measures that would support substantial progress towards the City's 2035 GHG emissions reduction targets, which are intended to keep the City in-line to achieve its share of 2050 GHG reductions.</p> <p>The CAP Consistency Checklist was adopted to uniformly implement the CAP for project-specific analyses of GHG emission impacts. The Project has been analyzed against the CAP Consistency Checklist and based on this analysis, it has been determined that the Project would be consistent with the CAP and would not contribute to cumulative GHG emissions that would be inconsistent with the CAP. As such, the Project would be consistent with the anticipated growth and buildout assumptions of both the DCP and the CAP. Therefore, this impact is considered not significant.</p>					X	X

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
<p><i>(b) Conflict with any applicable plan, policy, or regulation of an agency adopted for the purpose of reducing the emissions of greenhouse gas?</i></p> <p>As stated above in Section 6.a., construction and operation of the proposed Project would not result in a significant impact related to GHG emissions on the environment. The Project is consistent with the City's CAP and growth assumptions under the DCP as stated in Section 6.a. Additionally, the Project would be consistent with the recommendations within Policy CE-A.2 of the City of San Diego's General Plan Conservation Element. Therefore, the Project does not conflict with any applicable plan, policy, or regulation adopted for the purpose of reducing the emissions of greenhouse gases. This impact is considered not significant.</p>					X	X
7. HAZARDS AND HAZARDOUS MATERIALS:						
<p><i>(a) Substantial health and safety risk related to onsite hazardous materials?</i></p> <p>The Downtown FEIR states that contact with, or exposure to, hazardous building materials, soil and ground water contaminated with hazardous materials, or other hazardous materials could adversely affect human health and safety during short-term construction or long term operation of a development. The Project is subject to federal, state, and local agency regulations for the handling of hazardous building materials and waste. Compliance with all applicable requirements of the County of San Diego Department of Environmental Health and federal, state, and local regulations for the handling of hazardous building materials and waste would ensure that potential health and safety impacts caused by exposure to on-site hazardous materials are not significant during short term, construction activities. In addition, herbicides and fertilizers associated with</p>					X	X

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
the landscaping of the Project could pose a significant health risk over the long term operation of the Project. However, the Project's adherence to existing mandatory federal, state, and local regulations controlling these materials would ensure that long-term health and safety impacts associated with on-site hazardous materials over the long term operation of the Project are not significant.						
<p><i>(b) Be located on or within 2,000 feet of a site that is included on a list of hazardous materials sites compiled pursuant to Government Code §65962.5 and, as a result, would it create a significant hazard to the public or the environment?</i></p> <p>The Project is not located on or within 2,000 feet of a site on the State of California Hazardous Waste and Substances Sites List; however, there are sites within 2,000 feet of the Project site that are listed on the County of San Diego's Site Assessment Mitigation (SAM) Case Listing. The Downtown FEIR states that significant impacts to human health and the environment regarding hazardous waste sites would be avoided through compliance with mandatory federal, state, and local regulations as described in Section 7.a above. Therefore, the Downtown FEIR states that no mitigation measures would be required.</p>					X	X
<p><i>(c) Substantial safety risk to operations at San Diego International Airport?</i></p> <p>According to the Airport Land Use Compatibility Plan (ALUCP) for San Diego International Airport (SDIA), the entire DCP area is located within the SDIA Airport Influence Area. The Downtown FEIR identifies policies that regulate development within areas affected by Lindbergh Field including building heights, use and intensity limitations, and noise sensitive uses. The Project does not exceed the intensity of development</p>					X	X

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
assumed under the DCP, nor does it include components that would in any way violate or impede adherence to the policies of the ALUCP nor involve impacts related to the creation of substantial safety risks. The Federal Aviation Administration (FAA) issued a determination that the Project does not exceed obstruction standards and would not be a hazard to air navigation on 9/12/19. As a condition of this determination, the structure is to be marked/lighted in accordance with FAA standards. On 2/19/20 the Airport Land Use Commission determined that the Project is conditionally consistency with the ALUCP, provided compliance with the aforementioned conditions of the FAA. Therefore, there are no potential direct or cumulative impacts related to this issue.						
<p><i>(d) Substantially impair implementation of an adopted emergency response plan or emergency evacuation plan?</i></p> <p>The Project does not propose any features that would affect an emergency response or evacuation plan. Therefore, no impact associated with this issue is anticipated.</p>					X	X
8. HISTORICAL RESOURCES:						
<p><i>(a) Substantially impact a significant historical resource, as defined in § 15064.5?</i></p> <p>The project site located at 660 10th Avenue is a designated historic resource and is listed as HRB Site #1355, The Pacific Telephone and Telegraph Co. Garage. The garage is a one story commercial building that was constructed in the Art Deco Style. Because of the proposed alterations to the historic structure a Treatment Plan (Union Architects, December 2019) was submitted to City of San Diego Plan Historic staff for review. After some revisions to the plan it was approved. The Project proposes to retain portions of</p>			X	X		

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
<p>the historic resource on-site and incorporate them into the design of the new construction, resulting in a substantial alteration of the historic resource.</p> <p>The restoration is consistent with DCP Goals 9.1-G-1, “Protect historical resources to communicate Downtown’s heritage,” 9.1-G-2, “Encourage the rehabilitation and reuse of historical resources,” and 9.2-G-1, “Integrate historical resources into the Downtown fabric while achieving policies for significant development and population intensification,” and with DCP Policies 9.2-P-1, “Incorporate elements of historical buildings in new projects to impart heritage,” and 9.2-P-3, “Promote the adaptive reuse of intact buildings (designated or not) and/or significant elements, as a cultural and suitability goal.”</p> <p>DCP FEIR Mitigation Measure HIST-A.1-1 requires compliance with Chapter 14, Article 3, Division 2 of the SDMC, which regulates historic resources. Mitigation Measure HIST-A.1-2 requires the applicant to submit a Treatment Plan for retained historic resources for review and approval. Implementation of SDMC §143.0201 et seq., as required by Mitigation Measures HIST-A.1-1 and HIST-A.1-2 (Attachment A), will further ensure that the inclusion of the Pacific Telephone and Telegraph Co. Garage will not significantly impact the historic resource.</p> <p>The Project would include substantial alterations to the Pacific Telephone and Telegraph Co. Garage that may be approved through the SDP review and approved by the City in conformance with the City’s Historical Resources Regulations as provided in Mitigation Measure HIST-A.1-1 and HIST-A.1-2.</p>						

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
<p><i>(b) Substantially impact a significant archaeological resource pursuant to § 15064.5, including the disturbance of human remains interred outside of formal cemeteries?</i></p> <p>According to the Downtown FEIR, the likelihood of encountering archaeological resources is greatest for projects that include grading and/or excavation of areas on which past grading and/or excavation activities have been minimal (e.g., surface parking lots). Since archaeological resources have been found within inches of the ground surface in Downtown, even minimal grading activities can impact these resources. In addition, the likelihood of encountering subsurface human remains during construction and excavation activities, although considered low, is possible. Thus, the excavation, demolition, and surface clearance activities associated with development of the Project and the level of subterranean parking could have potentially adverse impacts to archaeological resources, including buried human remains. Implementation of Downtown FEIR Mitigation Measure HIST-B.1-1, (see Attachment A) would minimize, but not fully mitigate, these potential impacts. Since the potential for archaeological resources and human remains on the Project site cannot be confirmed until grading is conducted, the exact nature and extent of impacts associated with the proposed Project cannot be predicted. Consequently, the required mitigation may or may not be sufficient to reduce these direct project-level impacts to below a level of significance. Therefore, project-level impacts associated with this issue remain potentially significant and not fully mitigated, consistent with the analysis of the Downtown FEIR. Furthermore, project-level significant impacts to important archaeological resources would contribute to the potentially significant and unmitigated cumulative impacts</p>	X	X				

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
identified in the Downtown FEIR.						
<p><i>(c) Substantially impact a unique paleontological resource or site or unique geologic feature?</i></p> <p>The Project site is underlain by the San Diego Formation and Bay Point Formation, which has high paleontological resource potential. The Downtown FEIR concludes that development would have potentially adverse impacts to paleontological resources if grading and/or excavation activities are conducted beyond a depth of 1-3 feet. The Project's proposal for a basement level would involve excavation beyond the FEIR standard, resulting in potentially significant impacts to paleontological resources. Implementation of Downtown FEIR Mitigation Measure PAL-A.1-1 (see Attachment A) would ensure that the Project's potentially direct impacts to paleontological resources are not significant. Furthermore, the Project would not impact any resources outside of the Project site. The mitigation measures for direct impacts fully mitigate for paleontological impacts, therefore, the Project's contribution to cumulative impacts to paleontological resources would be significant but mitigated because the same measures that mitigate direct impacts would also mitigate for any cumulative impacts.</p>			X	X		
9. HYDROLOGY AND WATER QUALITY:						
<p><i>(a) Substantially degrade groundwater or surface water quality?</i></p> <p>The Project's construction and grading activities may involve soil excavation at a depth that could surpass known groundwater levels, which would indicate that groundwater dewatering might be required. Compliance with the requirements of either (1) the San Diego Regional Water Quality Control Board under a National Pollution Discharge Elimination</p>		X			X	

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
<p>system general permit for construction dewatering (if dewatering is discharged to surface waters), or (2) the City of San Diego Metropolitan Wastewater Department (if dewatering is discharged into the City's sanitary sewer system under the Industrial Waste Pretreatment Program), and (3) the mandatory requirements controlling the treatment and disposal of contaminated dewatered groundwater would ensure that potential impacts associated with construction dewatering and the handling of contaminated groundwater are not significant. In addition, Best Management Practices (BMPs) required as part of the local Storm Water Pollution Prevention Plan (SWPPP) would ensure that short-term water quality impacts during construction are not significant. The proposed Project would result in hard structure areas and other impervious surfaces that would generate urban runoff with the potential to degrade groundwater or surface water quality. However, implementation of BMPs required by the local Standard Urban Storm water Mitigation Program (SUSMP) and Storm water Standards would reduce the Project's long-term impacts. Thus, adherence to the state and local water quality controls would ensure that direct impacts to groundwater and surface water quality would not be significant.</p> <p>Despite not resulting in direct impacts to water quality, the Downtown FEIR found that the urban runoff generated by the cumulative development in the downtown would contribute to the existing significant cumulative impact to the water quality of San Diego Bay. No mitigation other than adherence to existing regulations has been identified in the Downtown FEIR to feasibly reduce this cumulative impact to below a level of significance. Consistent with the Downtown FEIR, the Project's contribution to the cumulative water quality impact would remain significant and</p>						

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
unmitigated.						
<p><i>(b) Substantially increase impervious surfaces and associated runoff flow rates or volumes?</i></p> <p>The project site is currently developed and covered with impervious surfaces. Implementation of the Project would not substantially increase the runoff volume entering the storm drain system. The Downtown FEIR found that implementation of the DCP would not result in a substantial increase in impervious surfaces within Downtown because the area is a highly urbanized area paved with pervious surfaces and very little vacant land (approximately 3 percent of the planning area). Redevelopment of downtown is therefore anticipated to replace impervious surfaces that already exist and development of the small number of undeveloped sites would not result in a substantial increase in impermeable surface area or a significant impact on the existing storm drain system. The Project is also required to comply with the City of San Diego BMPs required as part of the local SWPPP. The Project incorporates a variety of pervious surfaces (such as landscape areas and open spaces), as well as features designed to utilize storm water. Implementation of these features is encouraged by the DCP as they capture rain water and reduce surface volume entering the storm drain system. Therefore, impacts associated within this issue are not significant. (Impacts associated with the quality of urban runoff are analyzed in Section 9a.)</p>					X	X
<p><i>(c) Substantially impede or redirect flows within a 100-year flood hazard area?</i></p> <p>The Project site is not located within a 100-year floodplain. Similarly, the Project would not affect off-site flood hazard areas, as no 100-year floodplains are</p>					X	X

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
located downstream. Therefore, impacts associated with these issues are not significant.						
<p><i>(d) Substantially increase erosion and sedimentation?</i></p> <p>The potential for erosion and sedimentation could increase during the short-term during site preparation and other construction activities. As discussed in the Downtown FEIR, the proposed Project's compliance with regulations mandating the preparation and implementation of a SWPPP would ensure that impacts associated with erosion and sedimentation are not significant.</p>					X	X
10. LAND USE AND PLANNING:						
<p><i>(a) Physically divide an established community?</i></p> <p>The Project does not propose any features or structures that would physically divide an established community. Impacts associated with this issue would not occur.</p>					X	X
<p><i>(b) Substantially conflict with the City's General Plan and Progress Guide, Downtown Community Plan or other applicable land use plan, policy, or regulation?</i></p> <p>The Land Use District for the site is Employment/Residential Mixed-Use (ER) which provides synergies between educational institutions and residential neighborhoods or transition between the Core District and residential neighborhoods. A variety of uses are permitted in this district, including office, residential, hotel, research and development, educational, and medical facilities.</p> <p>The Project would not conflict with other applicable land use plans, policies, or regulations. The Project complies with the goals and policies of the DCP and would not create adverse environmental impacts under</p>					X	X

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
CEQA. With the approval of the requested Neighborhood Development Permit for the proposed deviations, the Project will meet all applicable development standards of the CCPDO and SDMC. Therefore, no significant direct or cumulative impacts associated with an adopted land use plan would occur.						
<p>(c) <i>Substantial incompatibility with surrounding land uses?</i></p> <p>Sources of land use incompatibility include, amongst others, lighting, industrial activities, shading, and noise. The Project would not result in or be subject to, adverse impacts due to substantially incompatible land uses. Compliance with the City's Light Pollution Ordinance would ensure that land use incompatibility impacts related to the Project's emission of, and exposure to, lighting are not significant. In addition, the Downtown FEIR concludes that existing mandatory regulations addressing land use compatibility with industrial activities would ensure that residents of, and visitors to, the Project are not subject to potential land use incompatibilities (potential land use incompatibilities resulting from hazardous materials and air emissions are evaluated elsewhere in this evaluation).</p> <p>Potentially significant impacts associated with the Project's incompatibility with traffic noise on adjacent grid streets are discussed in Sections 12.b and 12.c. No impacts associated with incompatibility with surrounding land use would occur.</p>					X	X

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
<p>(d) <i>Substantially impact surrounding communities due to sanitation and litter problems generated by transients displaced by Downtown development?</i></p> <p>Although not expected to be a substantial direct impact of the Project because substantial numbers of transients are not known to congregate on-site, the Project, in tandem with other Downtown development activities, would have a significant cumulative impact on surrounding communities resulting from sanitation problems and litter generation by transients who are displaced from Downtown into surrounding canyons and vacant land as discussed in the Downtown FEIR. Continued support of Homeless Outreach Teams (HOTs) and similar transient outreach efforts would reduce, but not fully mitigate, the adverse impacts to surrounding neighborhoods caused by the transient relocation. Therefore, the Project would result in cumulatively significant and not fully mitigated impacts to surrounding neighborhoods.</p>		X			X	
11. MINERAL RESOURCES:						
<p>(a) <i>Substantially reduce the availability of important mineral resources?</i></p> <p>The Downtown FEIR states that the viable extraction of mineral resources is limited in the DCP Area due to its urban nature and the fact that the area is not recognized for having high mineral resource potential. Therefore, no impact associated with this issue would occur.</p>					X	X
12. NOISE:						
<p>(a) <i>Substantial noise generation?</i></p> <p>A noise analysis report (Dbf Associates, February 2020) was prepared for the project and determined that no substantial noise generation impacts would occur from any stationary sources over the long-term. Short-</p>		X			X	

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
term construction noise impacts would be avoided by adherence to construction noise limitations imposed by the City's Noise Abatement and Control Ordinance. The Downtown FEIR defines a significant long-term traffic noise increase as an increase of at least 3.0 dB (A) CNEL for street. The Downtown FEIR identified nine street segments in Downtown that would be significantly impacted as a result of traffic generation; however, none of these identified segments are in the direct vicinity of the Project site. Nevertheless, automobile trips generated by the project, would, in combination with other development in Downtown significantly increase noise on several street segments resulting in cumulatively significant noise impacts. The Downtown FEIR concludes that there are no feasible mitigation measures available to reduce the significant noise increase in noise on affected roadways and this impact remains significant and unavoidable.						
<p><i>(b) Substantial exposure of required outdoor residential open spaces or public parks and plazas to noise levels (e.g. exposure to levels exceeding 65 dBA CNEL)?</i></p> <p>The Project contains a residential component and under the CCPDO, developments of this size are required to contain common outdoor open space areas. No public parks and/or plazas are proposed in the Project.</p> <p>The noise analysis prepared by Dbf Associates identified that future exterior traffic noise levels at the proposed project site would range from less than 60 dBA CNEL at the south building façade to approximately 71 dBA CNEL at the northeast building façade corner. Future exterior traffic noise levels at the Level 22 exterior amenity area would be 65 dBA CNEL or below, and would be considered “less than significant” by the City. Future exterior roadway</p>					X	X

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
<p>traffic noise levels at private balconies facing G Street, 9th Avenue, or 10th Avenue would exceed 65 dBA CNEL without abatement. Private balconies included in the project's required open space calculations must include a 3.5-foot-high noise barrier facing the roadway .</p> <p>According to NOI-B.1-2, an acoustical analysis must be performed to determine if any required outdoor open space areas would be exposed to noise levels in excess of 65 dBA CNEL and, provided noise attenuation would not interfere with the primary purpose or design intent of the exterior use, measures must be included in the building plan to reduce noise exposure, to the extent feasible. Full attenuation of noise may be contrary to the goal of creating outdoor open spaces for residences. If full enclosure of the open space would be required to fully attenuate noise, it would defeat the basic goal of providing "outdoor" open space. The project does not exceed the intensity of development assumed under the Downtown FEIR. Impacts associated with DCP buildout would remain significant and unavoidable, but the Project would not increase the severity of impacts related to exterior traffic noise in residential development.</p>						
<p><i>(c) Substantial interior noise within habitable rooms (e.g. levels in excess of 45 dBA CNEL)?</i></p> <p>According to the noise analysis interior noise levels within habitable rooms would not exceed 45 dBA CNEL (the standard set forth in the DCP FEIR) with a typical exterior wall, windows, and glass doors with a sound rating of STC 28 and mechanical ventilation in units to maintain a consistent level of acoustical quality around the Project sufficient to achieve compliant interior noise levels in all habitable units. Pursuant to Downtown FEIR Mitigation Measure NOI-B.1-1, the project would be required to implement the design features included in the</p>			X			X

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
Acoustical Analysis (2020). Therefore, project impacts related to the exposure of persons to or generation of noise levels in excess of noise standards would be less than significant with mitigation included in the Downtown FEIR.						
13. POPULATION AND HOUSING:						
<p><i>(a) Substantially induce population growth in an area?</i></p> <p>The Downtown FEIR concludes that build-out of Downtown would not induce substantial population growth that results in adverse physical changes. The Project is consistent with the DCP and CCPDO and does not exceed those analyzed throughout the Downtown FEIR. Therefore, project-level and cumulative impacts associated with this issue are not significant.</p>					X	X
<p><i>(b) Substantial displacement of existing housing units or people?</i></p> <p>The Project site, currently developed with a commercial warehouse building and does not contain any dwelling units. No existing housing units would be affected by the development or operation of the proposed project. Overall displacement of existing housing units or persons would not occur as a result of the proposed project, and the construction of replacement housing would not be required. Therefore, no direct or cumulative impacts associated with this issue would occur.</p>					X	X
14. PUBLIC SERVICES AND UTILITIES:						
<p><i>(a) Substantial adverse physical impacts associated with the provision of new schools?</i></p> <p>The population of school-aged children attending public schools is dependent on current and future residential development. In and of itself, the Project would not</p>					X	X

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
generate a sufficient number of students to warrant construction of a new school facility. However, the FEIR concludes that the additional student population anticipated at build out of Downtown would require the construction of at least one additional school, and that additional capacity could potentially be accommodated in existing facilities. The specific future location of new facilities is unknown at the present time. Pursuant to Section 15145 of CEQA, analysis of the physical changes in Downtown, which may occur from future construction of these public facilities, would be speculative and no further analysis of their impacts is required. Construction of any additional schools would be subject to CEQA. Environmental documentation prepared pursuant to CEQA would identify potentially significant impacts and appropriate mitigation measures. Therefore, implementation of the Project would not result in direct or cumulative impacts associated with this issue.						
<p><i>(b) Substantial adverse physical impacts associated with the provision of new libraries?</i></p> <p>The FEIR concludes that, cumulatively, development in Downtown would generate the need for a new Central Library which was completed in 2013 and possibly several smaller libraries in Downtown. In and of itself, the proposed Project would not generate additional demand necessitating the construction of new library facilities. However, according to the analysis in the Downtown FEIR, future development projects are considered to contribute to the cumulative need for new library facilities Downtown identified in the Downtown FEIR. Nevertheless, the specific future location of these facilities is unknown at present. Pursuant to Section 15145 of CEQA, analysis of the physical changes in Downtown, which may occur from future construction of these public facilities, would be speculative and no further analysis of their impacts is</p>					X	X

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
required. Construction of any additional library facilities would be subject to CEQA. Environmental documentation prepared pursuant to CEQA would identify potentially significant impacts and appropriate mitigation measures. Therefore, approval of the Project would not result in direct or cumulative impacts associated with this issue.						
<p><i>(c) Substantial adverse physical impacts associated with the provision of new fire protection/ emergency facilities?</i></p> <p>The Project would not generate a level of demand for fire protection/emergency facilities beyond the level assumed by the Downtown FEIR. Pursuant to Section 15145 of the California Environmental Quality Act (CEQA), analysis of the physical changes in Downtown that may occur from future construction of fire station facilities would be speculative and no further analysis of the impact is required. Environmental documentation prepared pursuant to CEQA would identify significant impacts and appropriate mitigation measures for any future fire station facilities.</p>					X	X
<p><i>(d) Substantial adverse physical impacts associated with the provision of new law enforcement facilities?</i></p> <p>The Downtown FEIR analyzes impacts to law enforcement service resulting from the cumulative development of Downtown and concludes the construction of new law enforcement facilities would not be required. Since the land use designation of the proposed development is consistent with the land use designation assumed in the Downtown FEIR analysis, the Project would not generate a level of demand for law enforcement facilities beyond the level assumed by the Downtown FEIR. However, the need for a new facility could be identified in the future. Pursuant to</p>					X	X

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
Section 15145 of the California Environmental Quality Act (CEQA), analysis of the physical changes in the downtown planning area that may occur from the future construction of law enforcement facilities would be speculative and no future analysis of their impacts would be required. However, construction of new law enforcement facilities would be subject to CEQA. Environmental documentation prepared pursuant to CEQA would identify potentially significant impacts and appropriate mitigation measures.						
<p><i>(e) Substantial adverse physical impacts associated with the provision of new water transmission or treatment facilities?</i></p> <p>The Public Utilities Department provides water service to Downtown and delivers more than 200,000 million acre-feet annually to over 1.3 million residents. During an average year the Department's water supply is made up of 10 to 20 percent of local rainfall, with the remaining amount imported from regional water suppliers including the San Diego County Water Authority (SDCWA) and the Metropolitan Water District (MWD). Potable water pipelines are located underneath the majority of Downtown's streets mimicking the above-ground street grid pattern.</p> <p>According to the Downtown FEIR, in the short term, planned water supplies and transmission or treatment facilities are adequate for development of Downtown. Water transmission infrastructure necessary to transport water supply to Downtown is already in place. Build out of the 2006 DCP, was considered in the updated 2015 SDCWA Urban Water Management Plan (UWMP). The Metropolitan Water District (MWD) and the SDCWA have developed water supply plans to improve reliability and reduce dependence on existing imported supplies. MWD's Regional Urban Water Management Plan (RUWMP)</p>					X	X

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
<p>and Integrated Water Management Plan (IWMP), and the SDCWA 2015 UWMP and annual water supply report include water infrastructure projects that meet long-term supply needs through securing water from the State Water Project, Colorado River, local water supply development, and recycled water. The SDCWA 2015 UWMP demonstrates that there will be sufficient water supplies available to meet demands for existing and planned future developments that are projected to occur by 2040.</p> <p>California Water Code Section 10910 requires projects analyzed under CEQA to assess water demand and compare that finding to the jurisdiction's projected water supply.</p> <p>Senate Bill 610 (SB 610) requires the preparation of a Water Supply Assessment (WSA) when a development project exceeds a threshold of 500 equivalent dwelling units. The Project does not exceed this threshold. Thus, the proposed project does not trigger the requirements of SB 610 and is consistent with the DCP, direct and cumulative impacts related to water supply would be considered not significant.</p>						
<p><i>(f) Substantial adverse physical impacts associated with the provision of new storm water facilities?</i></p> <p>The Downtown FEIR concludes that the cumulative development of the downtown would not impact the existing downtown storm drain system. Since implementation of the Project would not result in a significant increase of impervious surfaces, the amount of runoff volume entering the storm drain system would not create demand for new storm water facilities. Direct and cumulative impacts associated with this issue are considered not significant. Direct and cumulative impacts associated with this issue are considered not significant.</p>					X	X

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
<p>(g) <i>Substantial adverse physical impacts associated with the provision of new wastewater transmission or treatment facilities?</i></p> <p>The Downtown FEIR concludes that new wastewater treatment facilities would not be required to address the cumulative development of the Downtown. In addition, sewer improvements that may be needed to serve the Project are categorically exempt from environmental review under CEQA as stated in the Downtown FEIR. Therefore, impacts associated with this issue would not be significant.</p>					X	X
<p>(h) <i>Substantial adverse physical impacts associated with the provision of new landfill facilities?</i></p> <p>The Downtown FEIR concludes that cumulative development within the Downtown would increase the amount of solid waste to the Miramar Landfill and contribute to the eventual need for an alternative landfill. Although the proposed Project would generate a higher level of solid waste than the existing use of the site, implementation of a mandatory Waste Management Plan and compliance with the applicable provisions of the SDMC would ensure that both short-term and long-term project-level impacts are not significant. However, the Project would contribute, in combination with other development activities in Downtown, to the cumulative increase in the generation of solid waste sent to Miramar Landfill and the eventual need for a new landfill as identified in the Downtown FEIR. The location and size of a new landfill is unknown at this time. Pursuant to Section 15145 of CEQA, analysis from the physical changes that may occur from future construction of landfills would be speculative and no further analysis of their impacts is required. However, construction or expansion of a landfill would be subject to CEQA. Environmental documentation prepared pursuant to</p>					X	X

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
CEQA would identify potentially significant impacts of the proposed Project and appropriate mitigation measures. Therefore, cumulative impacts of the proposed Project are also considered not significant.						
15. PARKS AND RECREATIONAL FACILITIES:						
<p>(a) <i>Substantial increase in the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?</i></p> <p>The Downtown FEIR discusses impacts to parks and other recreational facilities and the maintenance thereof and concludes that build out of Downtown would not result in significant impacts associated with this issue. Since the land use designation of the proposed development does not differ from the land use designation assumed in the Downtown FEIR analysis, the Project would not generate a level of demand for parks and recreational facilities beyond the level assumed by the Downtown FEIR. Therefore, substantial deterioration of existing neighborhood or regional parks would not occur or be substantially accelerated as a result of the Project. No significant impacts with this issue would occur.</p>					X	X
16. TRANSPORTATION/TRAFFIC:						
<p>(a) <i>Cause the LOS on a roadway segment or intersection to drop below LOS E?</i></p> <p>The FEIR states that projects generating greater than 2,400 ADT would result in potentially significant impacts to the level of service (LOS) of roadway segment or intersection and requires implementation of mitigation measures at the Project level to mitigate the impact. The expected trip generation for this mixed-used project is approximately 1,967 average daily trips. This is based on approximately 964 average daily trips for the</p>		X			X	

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
<p>241 residential units at a rate of 4 trips/d.u. and 1,003 average daily trips at a rate of 28 trips/1,000 square feet. for the 35,813 square feet of commercial space. The rates are taken from the Centre City Cumulative Trip Rates of City of San Diego Trip Generation Manual. The Project's direct impacts on downtown roadway segments or intersections would not be significant.</p> <p>Traffic generated by the Project in combination with traffic generated by other downtown development would contribute to the significant cumulative impacts projected in the DCP FEIR to occur on a number of downtown roadway segments and intersections, and street within neighborhoods surrounding the DCP area at buildout of the downtown.</p> <p>The DCP FEIR includes mitigation measures to address impacts associated with buildout of the DCP, but the DCP FEIR acknowledges that the identified measures may or may not be able to fully mitigate these cumulative impacts due to constraints imposed by bicycle and pedestrian activities and the land uses adjacent to affected roadways. Pursuant to Downtown FEIR Mitigation Measure TRF-A.1.1-2, the applicant will also be required to pay development impact fees to fund a fair share fee towards transportation improvements for the DCP Area. As required by Downtown FEIR Mitigation Measure TRF-A.1.1-3, the City adopted the Downtown Community Public Facilities Financing Plan 2015 that established a transportation fee. The transportation fee is intended to fund street, transit, bicycle, pedestrian improvements, promenades, and below grade parking structures, as further set forth in the Downtown Community PFFP.</p>						
<p><i>(b) Cause the LOS on a freeway segment to drop below LOS E or cause a ramp delay in excess of 15 minutes?</i></p> <p>The Downtown FEIR concludes that development within Downtown will result in significant cumulative</p>		X			X	

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
impacts to freeway segments and ramps serving the Downtown area. Since the land use designation of the Project is consistent with the land use designation assumed in the Downtown FEIR analysis, the Project would contribute on a cumulative-level to the substandard LOS F identified in the Downtown FEIR on all freeway segments in the Downtown area and several ramps serving Downtown. Downtown FEIR Mitigation Measure TRF-A.1.1-1 would reduce these impacts to the extent feasible, but not to below the level of significance. This mitigation measure is not the responsibility of the Project, and therefore is not included in Attachment A. The Downtown FEIR concludes that the uncertainty associated with implementing freeway improvements and limitations in increasing ramp capacity limits the feasibility of fully mitigating impacts to these facilities. Thus, the Project's cumulative-level impacts to freeways would remain significant and unavoidable, consistent with the analysis of the Downtown FEIR. The Project would not have a direct impact on freeway segments and ramps.						
<p>(c) <i>Substantially discourage the use of alternative modes of transportation or cause transit service capacity to be exceeded?</i></p> <p>The proposed Project in and of itself does not include any features that would discourage the use of alternative modes of transportation. The Project's proximity to several other community serving uses, including nearby shopping and recreational activities also encourage walking. Additionally, visitors of the proposed Project would be encouraged to use alternative transportation means as there are several bus lines and the MTS facility/trolley station within a five-minute walk. Therefore, the Project will cause no significant impacts related to alternative modes of transportation or cause</p>					X	X

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
transit service capacity to be exceeded.						
17. MANDATORY FINDINGS OF SIGNIFICANCE:						
<p>(a) <i>Does the Project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?</i></p> <p>As indicated in the Downtown FEIR, due to the highly urbanized nature of the Downtown area, no sensitive plant or animal species, habitats, or wildlife migration corridors are located in the Downtown area. Additionally, the Project does not have the potential to eliminate important examples of major periods of California history or pre-history at the Project level. No other aspects of the Project would substantially degrade the environment. Cumulative impacts are described in Section 17.b below.</p>					X	X
<p>(b) <i>Does the Project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a Project are considerable when viewed in connection with the effects of past Projects, the effects of other current Projects, and the effects of probable future Projects)?</i></p> <p>As acknowledged in the Downtown FEIR, the buildout of Downtown would result in cumulative impacts associated with: air quality, historical resources, paleontological resources, physical changes associated with transient activities, noise, parking, traffic, and water quality. This Project would contribute to those impacts. Implementation of the</p>		X				

	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
Issues and Supporting Information						
mitigation measures identified in the Downtown FEIR would reduce some significant impacts; however, the impacts would remain significant and immitigable as identified in the Downtown FEIR and the Statement of Overriding Considerations adopted by the City. This Project's contribution would not be greater than anticipated by the Downtown FEIR and therefore no further analysis is required.						
<p><i>(c) Does the Project have environmental effects that would cause substantial adverse effects on human beings, either directly or indirectly?</i></p> <p>As acknowledged in the FEIR, the build-out of Downtown would result in cumulative impacts associated with: air quality, historical resources, paleontological resources, physical changes associated with transient activities, noise, traffic, and water quality. This Project would contribute to those impacts. However, the impacts associated with this Project would be no greater than those assumed in the Downtown FEIR and therefore no further environmental review is required under CEQA.</p>	X	X				

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Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
AIR QUALITY (AQ)				
Impact AQ-B.1	<p>Dust and construction equipment engine emissions generated during grading and demolition would impact local and regional air quality. (Direct and Cumulative)</p> <p>Mitigation Measure AQ-B.1-1: Prior to approval of a Grading or Demolition Permit, the City shall confirm that the following conditions have been applied, as appropriate:</p> <ol style="list-style-type: none"> Exposed soil areas shall be watered twice per day. On windy days or when fugitive dust can be observed leaving the development site, additional applications of water shall be applied as necessary to prevent visible dust plumes from leaving the development site. When wind velocities are forecast to exceed 25 mph, all ground disturbing activities shall be halted until winds that are forecast to abate below this threshold. Dust suppression techniques shall be implemented including, but not limited to, the following: <ol style="list-style-type: none"> Portions of the construction site to remain inactive longer than a period of three months shall be seeded and watered until grass cover is grown or otherwise stabilized in a manner acceptable to Civic San Diego. On-site access points shall be paved as soon as feasible or watered periodically or otherwise stabilized. Material transported off-site shall be either sufficiently watered or securely covered to prevent excessive amounts of dust. The area disturbed by clearing, grading, earthmoving, or excavation operations shall be minimized at all times. Vehicles on the construction site shall travel at speeds less than 15 mph. Material stockpiles subject to wind erosion during construction activities, which will not be utilized within three days, shall be covered with plastic, an alternative cover deemed equivalent to plastic, or sprayed with a nontoxic chemical stabilizer. Where vehicles leave the construction site and enter adjacent public streets, the streets shall be swept daily or washed down at the end of the work day to remove soil tracked onto 	Prior to Demolition or Grading Permit (Design)	Developer	City

9 G Tower Project (PTS# 649856) Downtown FEIR/SEIR Mitigation Monitoring and Reporting Program			
Significant Impact(s)	Mitigation Measure(s)	Implementation	
		Time Frame	Verification Responsibility
	<p>the paved surface. Any visible track-out extending for more than fifty (50) feet from the access point shall be swept or washed within thirty (30) minutes of deposition.</p> <p>6. All diesel-powered vehicles and equipment shall be properly operated and maintained.</p> <p>7. All diesel-powered vehicles and gasoline-powered equipment shall be turned off when not in use for more than five minutes, as required by state law.</p> <p>8. The construction contractor shall utilize electric or natural gas-powered equipment in lieu of gasoline or diesel-powered engines, where feasible.</p> <p>9. As much as possible, the construction contractor shall time the construction activities so as not to interfere with peak hour traffic. In order to minimize obstruction of through traffic lanes adjacent to the site, a flag-person shall be retained to maintain safety adjacent to existing roadways, if necessary.</p> <p>10. The construction contractor shall support and encourage ridesharing and transit incentives for the construction crew.</p> <p>11. Low VOC coatings shall be used as required by SDAPCD Rule 67. Spray equipment with high transfer efficiency, such as the high volume-low pressure spray method, or manual coatings application such as paint brush hand roller, trowel, spatula, dauber, rag, or sponge, shall be used to reduce VOC emissions, where feasible.</p> <p>12. If construction equipment powered by alternative fuel sources (liquefied natural gas/compressed natural gas) is available at comparable cost, the developer shall specify that such equipment be used during all construction activities on the development site.</p> <p>13. The developer shall require the use of particulate filters on diesel construction equipment if use of such filters is demonstrated to be cost-competitive for use on this development.</p> <p>14. During demolition activities, safety measures as required by City/County/State for removal of toxic or hazardous materials shall be utilized.</p> <p>15. Rubble piles shall be maintained in a damp state to minimize dust generation.</p> <p>16. During finish work, low-VOC paints and efficient transfer systems shall be utilized, to the extent possible.</p>		

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Significant Impact(s)	Mitigation Measure(s)	Implementation		
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	17. If alternative-fueled and/or particulate filter-equipped construction equipment is not feasible, construction equipment shall use the newest, least-polluting equipment, whenever possible. During finish work, low-VOC paints and efficient transfer systems shall be utilized, to the extent possible.			
HISTORICAL RESOURCES (HIST)				
Impact HIST-A.1	Future development in Downtown could impact significant architectural structures. (Direct and Cumulative)			
	<p>Mitigation Measure HIST-A.1-1: For construction or development permits that may impact potentially historical resources which are 45 years of age or older and which have not been evaluated for local, state and federal historic significance, a site specific survey shall be required in accordance with the Historical Resources Regulations in the LDC. Based on the survey and the best information available, City Staff to the Historical Resources Board (HRB) shall determine whether historical resources exist, whether potential historical resource(s) is/are eligible for designation as designated historical resource(s) by the HRB, and the precise location of the resource(s). The identified historical resource(s) may be nominated for HRB designation as a result of the survey pursuant to Chapter 12, Article 3, Division 2, Designation of Historical Resource procedures, of the LDC.</p> <p>All applications for construction and development permits where historical resources are present on the site shall be evaluated by City Staff to the HRB pursuant to Chapter 14, Article 3, Division 2, Historical Resources Regulations of the LDC.</p> <p>1. National Register-Listed/Eligible, California Register-Listed/Eligible Resources: Resources listed in or formally determined eligible for the National Register or California Register and resources identified as contributing within a National or California Register District, shall be retained onsite and any improvements, renovation, rehabilitation and/or adaptive reuse of the property shall ensure its preservation and be consistent with the Secretary of the Interior's Standards for the Treatment of Historic Properties (1995) and the associated Guidelines.</p> <p>2. San Diego Register-Listed Resources: Resources listed in the San Diego Register of Historical Resources, or determined to be a contributor to a San Diego Register District, shall, whenever possible, be retained on-site. Partial retention, relocation, or demolition of</p>	<p>Prior to Development Permit (Design)</p> <p>Prior to Demolition, Grading, and/or Building Permit (Design)</p> <p>Prior to Certificate of Occupancy (Implementation)</p>	Developer	Civic San Diego /City

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Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	a resource shall only be permitted according to Chapter 14, Article 3, Division 2, Historical Resources Regulations of the LDC.			

	<p>Mitigation Measure HIST-A.1-2: If the potential exists for direct and/or indirect impacts to retained or relocated designated and/or potential historical resources (“historical resources”), the following measures shall be implemented in coordination with a Development Services Department designee and/or City Staff to the HRB (“City Staff”) in accordance with Chapter 14, Article 3, Division 2, Historical Resources Regulations of the LDC.</p> <p>I. Prior to Permit Issuance</p> <p>A. Construction Plan Check</p> <ol style="list-style-type: none"> 1. Prior to Notice to Proceed (NTP) for any construction permits, including but not limited to, the first Grading Permit Building Permits, but prior to the first Preconstruction (Precon) Meeting, whichever is applicable, City Staff shall verify that the requirements for historical monitoring during demolition and/or stabilization have been noted on the appropriate construction documents. <ol style="list-style-type: none"> (a) Stabilization work cannot begin until a Precon Meeting has been held at least one week prior to issuance of appropriate permits. (b) Physical description, including the year and type of historical resource, and extent of stabilization shall be noted on the plans. <p>B. Submittal of Treatment Plan for Retained Historical Resources</p> <ol style="list-style-type: none"> 1. Prior to NTP for any construction permits, including but not limited to, the first Grading Permit and Building Permits, but prior to the first Precon Meeting, whichever is applicable, the Applicant shall submit a Treatment Plan to City Staff for review and approval in accordance with the Secretary of the Interior’s Standards for the Treatment of Historic Properties (1995) and the associated Guidelines. The Treatment Plan shall include measures for protecting any historical resources, as defined in the LDC, during construction related activities (e.g., removal of non-historic features, demolition of adjacent structures, subsurface structural support, etc.). The Treatment Plan shall be shown as notes on all construction documents (i.e., Grading and/or Building Plans). <p>C. Letters of Qualification have been submitted to City Staff</p> <ol style="list-style-type: none"> 1. The applicant shall submit a letter of verification to City Staff identifying the Principal Investigator (PI) for the project and the names of all persons involved in this MMRP (i.e., Architectural Historian, Historic Architect and/or Historian), as defined in the City of San Diego HRG. 2. City Staff will provide a letter to the applicant confirming that the qualifications of the PI and all persons involved in the historical monitoring of the project meet the qualification standards established by the HRG. 		
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	<p>3. Prior to the start of work, the applicant must obtain approval from City Staff for any personnel changes associated with the monitoring program.</p> <p>II. Prior to Start of Construction</p> <p>A. Documentation Program (DP)</p> <p>1. Prior to the first Precon Meeting and/or issuance of any construction permit, the DP shall be submitted to City Staff for review and approval and shall include the following:</p> <p>(a) Photo Documentation</p> <p>(1) Documentation shall include professional quality photo documentation of the historical resource(s) prior to any construction that may cause direct and/or indirect impacts to the resource(s) with 35mm black and white photographs, 4x6 standard format, taken of all four elevations and close-ups of select architectural elements, such as, but not limited to, roof/wall junctions, window treatments, and decorative hardware. Photographs shall be of archival quality and easily reproducible.</p> <p>(2) Xerox copies or CD of the photographs shall be submitted for archival storage with the City of San Diego HRB and the Civic San Diego Project file. One set of original photographs and negatives shall be submitted for archival storage with the California Room of the City of San Diego Public Library, the San Diego Historical Society and/or other relative historical society or group(s).</p> <p>(b) Required drawings</p> <p>(1) Measured drawings of the building's exterior elevations depicting existing conditions or other relevant features shall be produced from recorded, accurate measurements. If portions of the building are not accessible for measurement, or cannot be reproduced from historic sources, they should not be drawn, but clearly labeled as not accessible. Drawings produced in ink on translucent material or archivally stable material (blue-line drawings) are acceptable). Standard drawing sizes are 19 by 24 inches or 24 by 36 inches, standard scale is 1/4 inch = 1 foot.</p> <p>(2) One set of measured drawings shall be submitted for archival storage with the City of San Diego HRB, the Civic San Diego Project file, the South Coastal Information Center, the California Room of the City of San Diego Public Library, the San Diego Historical Society and/or other historical society or group(s).</p>		
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	<p>2. Prior to the first Precon Meeting, City Staff shall verify that the DP has been approved.</p> <p>B. PI Shall Attend Precon Meetings</p> <p>1. Prior to beginning any work that may impact any historical resource(s) which is/are subject to this MMRP, the Applicant shall arrange a Precon Meeting that shall include the PI, Construction Manager (CM) and/or Grading Contractor, Resident Engineer (RE), Historical Monitor(s), Building Inspector (BI), if appropriate, and City Staff. The qualified Historian and/or Architectural Historian shall attend any grading/excavation related Precon Meetings to make comments and/or suggestions concerning the Historical Monitoring program with the Construction Manager and/or Grading Contractor.</p> <p>(a) If the PI is unable to attend the Precon Meeting, the Applicant shall schedule a focused Precon Meeting with City Staff, the PI, RE, CM or BI, if appropriate, prior to the start of any work that requires monitoring.</p> <p>2. Historical Monitoring Plan</p> <p>(a) Prior to the start of any work that is subject to an Historical Monitoring Plan, the PI shall submit an Historical Monitoring Plan which describes how the monitoring would be accomplished for approval by City Staff. The Historical Monitoring Plan shall include an Historical Monitoring Exhibit (HME) based on the appropriate construction documents (reduced to 11x17 inches) to City Staff identifying the areas to be monitored including the delineation of grading/excavation limits.</p> <p>(b) Prior to the start of any work, the PI shall also submit a construction schedule to City Staff through the RE indicating when and where monitoring will occur.</p> <p>(c) The PI may submit a detailed letter to City Staff prior to the start of work or during construction requesting a modification to the monitoring program. This request shall be based on relevant information such as review of final construction documents which indicate site conditions such as underpinning, shoring and/or extensive excavation which could result in impacts to, and/or reduce impacts to the on-site or adjacent historical resource.</p> <p>C. Implementation of Approved Treatment Plan for Historical Resources</p> <p>1. Implementation of the approved Treatment Plan for the protection of historical resources within the project site may not begin prior to the completion of the Documentation Program as defined above.</p>		
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	<p>2. The qualified Historical Monitor(s) shall attend weekly jobsite meetings and be on-site daily during the stabilization phase for any retained or adjacent historical resource to photo document the Treatment Plan process.</p> <p>3. The qualified Historical Monitor(s) shall document activity via the Consultant Site Visit Record (CSVr). The CSVr's shall be faxed by the CM to the RE the first day and last day (Notification of Monitoring Completion) of the Treatment Plan process and in the case of ANY unanticipated incidents. The RE shall forward copies to City Staff.</p> <p>4. Prior to the start of any construction related activities, the applicant shall provide verification to City Staff that all historical resources on-site have been adequately stabilized in accordance with the approved Treatment Plan. This may include a site visit with City Staff, the CM, RE or BI, but may also be accomplished through submittal of the draft Treatment Plan photo documentation report.</p> <p>5. City Staff will provide written verification to the RE or BI after the site visit or upon approval of draft Treatment Plan report indicating that construction related activities can proceed.</p> <p>III. During Construction</p> <p>A. Qualified Historical Monitor(s) Shall be Present During Grading/Excavation/Trenching</p> <ol style="list-style-type: none"> 1. The Qualified Historical Monitor(s) shall be present full-time during grading/excavation/trenching activities which could result in impacts to historical resources as identified on the HME. The Construction Manager is responsible for notifying the RE, PI, and City Staff of changes to any construction activities. 2. The Qualified Historical Monitor(s) shall document field activity via the CSVr. The CSVr's shall be faxed by the CM to the RE the first day of monitoring, the last day of monitoring, monthly (Notification of Monitoring Completion), and in the case of ANY incidents involving the historical resource. The RE shall forward copies to City Staff. 3. The PI may submit a detailed letter to City Staff during construction requesting a modification to the monitoring program when a field condition arises which could effect the historical resource being retained on-site or adjacent to the construction site. <p>B. Notification Process</p> <ol style="list-style-type: none"> 1. In the event of damage to a historical resource retained on-site or adjacent to the project site, the Qualified Historical Monitor(s) shall direct the contractor to temporarily divert construction activities in the area of historical resource and 		
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	<p>immediately notify the RE or BI, as appropriate, and the PI (unless Monitor is the PI).</p> <p>2. The PI shall immediately notify City Staff by phone of the incident, and shall also submit written documentation to City Staff within 24 hours by fax or email with photos of the resource in context, if possible.</p> <p>C. Determination/Evaluation of Impacts to a Historical Resource</p> <p>1. The PI shall evaluate the incident relative to the historical resource.</p> <p>(a) The PI shall immediately notify City Staff by phone to discuss the incident and shall also submit a letter to City Staff indicating whether additional mitigation is required.</p> <p>(b) If impacts to the historical resource are significant, the PI shall submit a proposal for City Staff review and written approval in accordance with Chapter 14, Article 3, Division 2, Historical Resources Regulations of the LDC and the Secretary of the Interior's Standards for the Treatment of Historic Properties (1995) and the associated Guidelines. Direct and/or indirect impacts to historical resources from construction activities must be mitigated before work will be allowed to resume.</p> <p>(c) If impacts to the historical resource are not considered significant, the PI shall submit a letter to City Staff indicating that the incident will be documented in the Final Monitoring Report. The letter shall also indicate that that no further work is required.</p> <p>IV. Night Work</p> <p>A. If night and/or weekend work is included in the contract</p> <p>1. When night and/or weekend work is included in the contract package, the extent and timing shall be presented and discussed at the Precon Meeting.</p> <p>2. The following procedures shall be followed.</p> <p>(a) No Impacts/Incidents</p> <p>In the event that no historical resources were impacted during night and/or weekend work, the PI shall record the information on the CSVr and submit to City Staff via fax by 8 a.m. of the next business day.</p> <p>(b) Potentially Significant Impacts</p>		
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	<p>If the PI determines that a potentially significant impact has occurred to a historical resource, the procedures detailed under Section III - During Construction shall be followed.</p> <p>(c) The PI shall immediately contact City Staff, or by 8 a.m. of the next business day to report and discuss the findings as indicated in Section III-B, unless other specific arrangements have been made.</p> <p>B. If night and/or weekend work becomes necessary during the course of construction:</p> <ol style="list-style-type: none"> 1. The Construction Manager shall notify the RE, or BI, as appropriate, a minimum of 24 hours before the work is to begin. 2. The RE, or BI, as appropriate, shall notify City Staff immediately. <p>C. All other procedures described above shall apply, as appropriate.</p> <p>V. Post Construction</p> <p>A. Submittal of Draft Monitoring Report</p> <ol style="list-style-type: none"> 1. The PI shall submit two copies of the Draft Monitoring Report (even if negative), prepared in accordance with the Historical Resources Guidelines (HRG) and Appendices which describes the results, analysis, and conclusions of all phases of the Historical Monitoring Plan (with appropriate graphics) to City Staff for review and approval within 90 days following the completion of monitoring. <ol style="list-style-type: none"> (a) The preconstruction Treatment Plan and Documentation Plan (photos and measured drawings) and Historical Commemorative Program, if applicable, shall be included and/or incorporated into the Draft Monitoring Report. (b) The PI shall be responsible for updating (on the appropriate State of California Department of Park and Recreation forms-DPR 523 A/B) any existing site forms to document the partial and/or complete demolition of the resource. Updated forms shall be submitted to the South Coastal Information Center with the Final Monitoring Report. 2. City Staff shall return the Draft Monitoring Report to the PI for revision or, for preparation of the Final Report. 3. The PI shall submit revised Draft Monitoring Report to City Staff for approval. 4. City Staff shall provide written verification to the PI of the approved report. 5. City Staff shall notify the RE or BI, as appropriate, of receipt of all Draft Monitoring Report submittals and approvals. 		
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		Time Frame	Responsibility	Verification Responsibility
	B. Final Monitoring Report(s) 1. The PI shall submit one copy of the approved Final Monitoring Report to the RE or BI as appropriate, and one copy to City Staff (even if negative), within 90 days after notification from City Staff that the draft report has been approved. 2. The RE shall, in no case, issue the Notice of Completion until receiving a copy of the approved Final Monitoring Report from City Staff.			
Impact HIST-B.1	Development in Downtown could impact significant buried archaeological resources. (Direct and Cumulative) Mitigation Measure HIST-B.1-1: If the potential exists for direct and/or indirect impacts to significant buried archaeological resources, the following measures shall be implemented in coordination with a Development Services Department designee and/or City Staff to the HRB ("City Staff") in accordance with Chapter 14, Article 3, Division 2, Historical Resources Regulations of the LDC. Prior to issuance of any permit that could directly affect an archaeological resource, City Staff shall assure that all elements of the MMRP are performed in accordance with all applicable City regulations and guidelines by an Archaeologist meeting the qualifications specified in Appendix B of the San Diego LDC, Historical Resources Guidelines. City Staff shall also require that the following steps be taken to determine: (1) the presence of archaeological resources and (2) the appropriate mitigation for any significant resources which may be impacted by a development activity. Sites may include residential and commercial properties, privies, trash pits, building foundations, and industrial features representing the contributions of people from diverse socio-economic and ethnic backgrounds. Sites may also include resources associated with pre-historic Native American activities. Archeological resources which also meet the definition of historical resources or unique archaeological resources under CEQA or the SDMC shall be treated in accordance with the following evaluation procedures and applicable mitigation program: Step 1–Initial Evaluation An initial evaluation for the potential of significant subsurface archaeological resources shall be prepared to the satisfaction of City Staff as part of an Environmental Secondary Study for any activity which involves excavation or building demolition. The initial evaluation shall be guided	Prior to Demolition or Grading Permit (Design) Prior to Certificate of Occupancy (Implementation)	Developer	City Staff

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	<p>by an appropriate level research design in accordance with the City's LDC, Historical Resources Guidelines. The person completing the initial review shall meet the qualification requirements as set forth in the Historical Resources Guidelines and shall be approved by City Staff. The initial evaluation shall consist, at a minimum, of a review of the following historical sources: The 1876 Bird's Eye View of San Diego, all Sanborn Fire Insurance Company maps, appropriate City directories and maps that identify historical properties or archaeological sites, and a records search at the South Coastal Information Center for archaeological resources located within the property boundaries. Historical and existing land uses shall also be reviewed to assess the potential presence of significant prehistoric and historic archaeological resources. The person completing the initial review shall also consult with and consider input from local individuals and groups with expertise in the historical resources of the San Diego area. These experts may include the University of California, San Diego State University, San Diego Museum of Man, Save Our Heritage Organization, local historical and archaeological groups, the Native American Heritage Commission (NAHC), designated community planning groups, and other individuals or groups that may have specific knowledge of the area. Consultation with these or other individuals and groups shall occur as early as possible in the evaluation process.</p> <p>When the initial evaluation indicates that important archaeological sites may be present on a project site but their presence cannot be confirmed prior to construction or demolition due to obstructions or spatially limited testing and data recovery, the applicant shall prepare and implement an archaeological monitoring program as a condition of development approval to the satisfaction of City Staff. If the NAHC Sacred Lands File search is positive for Native American resources within the project site, then additional evaluation must include participation of a local Native American consultant in accordance with CEQA Sections 15064.5(d), 15126.4(b)(3) and Public Resources Code Section 21083.2.</p> <p>No further action is required if the initial evaluation demonstrates there is no potential for subsurface resources. The results of this research shall be summarized in the Secondary Study.</p> <p>Step 2-Testing</p> <p>A testing program is required if the initial evaluation demonstrates that there is a potential for subsurface resources. The testing program shall be conducted during the hazardous materials remediation or following the removal of any structure or surface covering which may be underlain by potential resources. The removal of these structures shall be conducted in a manner</p>			

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Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>which minimizes disturbance of underlying soil. This shall entail a separate phase of investigations from any mitigation monitoring during construction.</p> <p>The testing program shall be performed by a qualified Historical Archaeologist meeting the qualifications specified in Appendix B of the San Diego LDC, HRG. The Historical Archaeologist must be approved by City Staff prior to commencement. Before commencing the testing, a treatment plan shall be submitted for City Staff approval that reviews the initial evaluation results and includes a research design. The research design shall be prepared in accordance with the City's HRG and include a discussion of field methods, research questions against which discoveries shall be evaluated for significance, collection strategy, laboratory and analytical approaches, and curation arrangements. All tasks shall be in conformity with best practices in the field of historic urban archaeology.</p> <p>A recommended approach for historic urban sites is at a minimum fills and debris along interior lot lines or other areas indicated on Sanborn maps.</p> <p>Security measures such as a locked fence or surveillance shall be taken to prevent looting or vandalism of archaeological resources as soon as demolition is complete or paved surfaces are removed. These measures shall be maintained during archaeological field investigations. It is recommended that exposed features be covered with steel plates or fill dirt when not being investigated.</p> <p>The results of the testing phase shall be submitted in writing to City Staff and shall include the research design, testing results, significance evaluation, and recommendations for further treatment. Final determination of significance shall be made in consultation with City Staff, and with the Native American community, if the finds are prehistoric. If no significant resources are found and site conditions are such that there is no potential for further discoveries, then no further action is required. If no significant resources are found but results of the initial evaluation and testing phase indicates there is still a potential for resources to be present in portions of the property that could not be tested, then mitigation monitoring is required and shall be conducted in accordance with the provisions set forth in Step 4 - Monitoring. If significant resources are discovered during the testing program, then data recovery in accordance with Step 3 shall be undertaken prior to construction. If the existence or probable likelihood of Native American human remains or associated grave goods area discovered through the testing program, the Qualified Archaeologist shall stop work in the area, notify the City Building Inspector, City staff, and immediately implement the procedures set forth in CEQA</p>			

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Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>Guidelines Section 15064.5 and the California PRC Section 5097.98 for discovery of human remains. This procedure is further detailed in the Mitigation, Monitoring and Reporting Program (Step 4). City Staff must concur with evaluation results before the next steps can proceed.</p> <p>Step 3–Data Recovery</p> <p>For any site determined to be significant, a Research Design and Data Recovery Program shall be prepared in accordance with the City’s Historical Resources Guidelines, approved by City Staff, and carried out to mitigate impacts before any activity is conducted which could potentially disturb significant resources. The archaeologist shall notify City Staff of the date upon which data recovery will commence ten (10) working days in advance.</p> <p>All cultural materials collected shall be cleaned, catalogued and permanently curated with an appropriate institution. Native American burial resources shall be treated in the manner agreed to by the Native American representative or be reinterred on the site in an area not subject to further disturbance in accordance with CEQA section 15164.5 and the Public Resources Code section 5097.98. All artifacts shall be analyzed to identify function and chronology as they relate to the history of the area. Faunal material shall be identified as to species and specialty studies shall be completed, as appropriate. All newly discovered archaeological sites shall be recorded with the South Coastal Information Center at San Diego State University. Any human bones and associated grave goods of Native American origin encountered during Step 2-Testing, shall, upon consultation, be turned over to the appropriate Native American representative(s) for treatment in accordance with state regulations as further outlined under Step 4-Monitoring (Section IV. Discovery of Human Remains).</p> <p>A draft Data Recovery Report shall be submitted to City Staff within twelve months of the commencement of the data recovery. Data Recovery Reports shall describe the research design or questions, historic context of the finds, field results, analysis of artifacts, and conclusions. Appropriate figures, maps and tables shall accompany the text. The report shall also include a catalogue of all finds and a description of curation arrangements at an approved facility, and a general statement indicating the disposition of any human remains encountered during the data recovery effort (please note that the location of reinternment and/or repatriation is confidential and not subject to public disclosure in accordance with state law). Finalization of draft reports shall be subject to City Staff review.</p> <p>Step 4 – Monitoring</p>			

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		Time Frame	Responsibility	Verification Responsibility
	<p>If no significant resources are encountered, but results of the initial evaluation and testing phase indicates there is still a potential for resources to be present in portions of the property that could not be tested, then mitigation monitoring is required and shall be conducted in accordance with the following provisions and components:</p> <p>I. Prior to Permit Issuance</p> <p>A. Construction Plan Check</p> <ol style="list-style-type: none"> 1. Prior to NTP for any construction permits, including but not limited to, the first Grading Permit, Demolition Permits and Building Permits, but prior to the first Precon Meeting, whichever is applicable, City Staff shall verify that the requirements for Archaeological Monitoring and Native American monitoring, where the project may impact Native American resources, have been noted on the appropriate construction documents. <p>B. Letters of Qualification have been submitted to City Staff</p> <ol style="list-style-type: none"> 1. The applicant shall submit a letter of verification to City Staff identifying the PI for the project and the names of all persons involved in the archaeological monitoring program, as defined in the City of San Diego HRG. If applicable, individuals involved in the archaeological monitoring program must have completed the 40-hour Hazardous Waste Operations and Emergency Response training with certification documentation. 2. City Staff will provide a letter to the applicant confirming that the qualifications of the PI and all persons involved in the archaeological monitoring of the project meet the qualifications established in the HRG. 3. Prior to the start of work, the applicant must obtain written approval from City Staff for any personnel changes associated with the monitoring program. <p>II. Prior to Start of Construction</p> <p>A. Verification of Records Search</p> <ol style="list-style-type: none"> 1. The PI shall provide verification to City Staff that a site-specific records search (1/4 mile radius) has been completed. Verification includes, but is not limited to a copy 			

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	<p>of a confirmation letter from South Coastal Information Center, or, if the search was in-house, a letter of verification from the PI stating that the search was completed.</p> <ol style="list-style-type: none"> The letter shall introduce any pertinent information concerning expectations and probabilities of discovery during trenching and/or grading activities. The PI may submit a detailed letter to City Staff requesting a reduction to the 1/4 mile radius. <p>B. PI Shall Attend Precon Meetings</p> <ol style="list-style-type: none"> <p>Prior to beginning any work that requires monitoring, the Applicant shall arrange a Precon Meeting that shall include the PI, Native American consultant/monitor (where Native American resources may be impacted), CM and/or Grading Contractor, RE, the Native American representative(s) (where Native American resources may be impacted), BI, if appropriate, and City Staff. The qualified Archaeologist and the Native American consultant/monitor shall attend any grading/excavation related Precon Meetings to make comments and/or suggestions concerning the Archaeological Monitoring program with the Construction Manager and/or Grading Contractor.</p> <p>(a) If the PI is unable to attend the Precon Meeting, the Applicant shall schedule a focused Precon Meeting with City Staff, the PI, RE, CM or BI, if appropriate, prior to the start of any work that requires monitoring.</p> Archaeological Monitoring Plan (AMP) <p>(a) Prior to the start of any work that requires monitoring, the PI shall submit an Archaeological Monitoring Plan (with verification that the AMP has been reviewed and approved by the Native American consultant/monitor when Native American resources may be impacted) which describes how the monitoring would be accomplished for approval by City Staff and the Native American monitor. The AMP shall include an Archaeological Monitoring Exhibit (AME) based on the appropriate construction documents (reduced to 11 by 17 inches) to City Staff identifying the areas to be monitored including the delineation of grading/excavation limits.</p> 			

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	<p>(b) The AME shall be based on the results of a site-specific records search as well as information regarding existing known soil conditions (native or formation).</p> <p>(c) Prior to the start of any work, the PI shall also submit a construction schedule to City Staff through the RE indicating when and where monitoring will occur.</p> <p>(d) The PI may submit a detailed letter to City Staff prior to the start of work or during construction requesting a modification to the monitoring program. This request shall be based on relevant information such as review of final construction documents which indicate site conditions such as depth of excavation and/or site graded to bedrock, etc., which may reduce or increase the potential for resources to be present.</p> <p>III. During Construction</p> <p>A. Monitor(s) Shall be Present During Grading/Excavation/Trenching</p> <ol style="list-style-type: none"> 1. The Archaeological monitor shall be present full-time during all soil disturbing and grading/excavation /trenching activities which could result in impacts to archaeological resources as identified on the AME. The Construction Manager is responsible for notifying the RE, PI, and City Staff of changes to any construction activities. 2. The Native American consultant/monitor shall determine the extent of their presence during soil disturbing and grading/excavation/trenching activities based on the AME, and provide that information to the PI and City Staff. If prehistoric resources are encountered during the Native American consultant/monitor's absence, work shall stop and the Discovery Notification Processes detailed in Sections III.B-C, and IVA-D shall commence. 3. The archeological and Native American consultant/monitor shall document field activity via the CSV. The CSV's shall be faxed by the CM to the RE the first day of monitoring, the last day of monitoring, monthly (Notification of Monitoring Completion), and in the case of ANY discoveries. The RE shall forward copies to City Staff. 4. The PI may submit a detailed letter to City Staff during construction requesting a modification to the monitoring program when a field condition such as modern 			

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	<p>disturbance post-dating the previous grading/trenching activities, presence of fossil formations, or when native soils are encountered that may reduce or increase the potential for resources to be present.</p> <p>B. Discovery Notification Process</p> <ol style="list-style-type: none"> 1. In the event of a discovery, the Archaeological Monitor shall direct the contractor to temporarily divert all soil disturbing activities, including but not limited to, digging, trenching, excavating, or grading activities in the area of discovery and in the area reasonably suspected to overlay adjacent resources and immediately notify the RE or BI, as appropriate. 2. The Monitor shall immediately notify the PI (unless Monitor is the PI) of the discovery. 3. The PI shall immediately notify City Staff by phone of the discovery, and shall also submit written documentation to City Staff within 24 hours by fax or email with photos of the resource in context, if possible. 4. No soil shall be exported off-site until a determination can be made regarding the significance of the resource specifically if Native American resources are encountered. <p>C. Determination of Significance</p> <ol style="list-style-type: none"> 1. The PI and Native American consultant/monitor, where Native American resources are discovered, shall evaluate the significance of the resource. If Human Remains are involved, follow protocol in Section IV below. <ol style="list-style-type: none"> (a) The PI shall immediately notify City Staff by phone to discuss significance determination and shall also submit a letter to City Staff indicating whether additional mitigation is required. (b) If the resource is significant, the PI shall submit an Archaeological Data Recovery Program which has been reviewed by the Native American consultant/monitor when applicable, and obtain written approval from City Staff and the Native American representative(s), if applicable. Impacts to 			

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	<p>significant resources must be mitigated before ground disturbing activities in the area of discovery will be allowed to resume.</p> <p>(c) If the resource is not significant, the PI shall submit a letter to City Staff indicating that artifacts will be collected, curated, and documented in the Final Monitoring Report. The letter shall also indicate that that no further work is required.</p> <p>IV. Discovery of Human Remains</p> <p>If human remains are discovered, work shall halt in that area and no soil shall be exported off-site until a determination can be made regarding the provenance of the human remains; and the following procedures set forth in CEQA Section 15064.5(e), the California Public Resources Code (Sec. 5097.98) and State Health and Safety Code (Sec. 7050.5) shall be undertaken:</p> <p>A. Notification</p> <ol style="list-style-type: none"> 1. Archaeological Monitor shall notify the RE or BI as appropriate, City Staff, and the PI, if the Monitor is not qualified as a PI. City Staff will notify the appropriate Senior Planner in the Environmental Analysis Section of the Development Services Department to assist with the discovery process. 2. The PI shall notify the Medical Examiner after consultation with the RE, either in person or via telephone. <p>B. Isolate discovery site</p> <ol style="list-style-type: none"> 1. Work shall be directed away from the location of the discovery and any nearby area reasonably suspected to overlay adjacent human remains until a determination can be made by the Medical Examiner in consultation with the PI concerning the provenance of the remains. 2. The Medical Examiner, in consultation with the PI, will determine the need for a field examination to determine the provenance. 			

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	<p>3. If a field examination is not warranted, the Medical Examiner will determine with input from the PI, if the remains are or are most likely to be of Native American origin.</p> <p>C. If Human Remains are determined to be Native American</p> <ol style="list-style-type: none"> 1. The Medical Examiner will notify the NAHC within 24 hours. By law, ONLY the Medical Examiner can make this call. 2. NAHC will immediately identify the person or persons determined to be the Most Likely Descendent (MLD) and provide contact information. 3. The MLD will contact the PI within 24 hours or sooner after the Medical Examiner has completed coordination, to begin the consultation process in accordance with CEQA Section 15064.5(e) and the California Public Resources and Health & Safety Codes. 4. The MLD will have 48 hours to make recommendations to the property owner or representative, for the treatment or disposition with proper dignity, of the human remains and associated grave goods. 5. Disposition of Native American Human Remains will be determined between the MLD and the PI, and if: <ol style="list-style-type: none"> (a) The NAHC is unable to identify the MLD, OR the MLD failed to make a recommendation within 48 hours after being notified by the Commission; OR; (b) The landowner or authorized representative rejects the recommendation of the MLD and mediation in accordance with PRC 5097.94 (k) by the NAHC fails to provide measures acceptable to the landowner, THEN, (c) In order to protect these sites, the Landowner shall do one or more of the following: <ol style="list-style-type: none"> (1) Record the site with the NAHC; (2) Record an open space or conservation easement on the site; (3) Record a document with the County. 			

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	<p>6. Upon the discovery of multiple Native American human remains during a ground disturbing land development activity, the landowner may agree that additional conferral with descendant is necessary to consider culturally appropriate treatment of multiple Native American human remains. Culturally appropriate treatment of such a discovery may be ascertained from review of the site utilizing cultural and archaeological standards. Where the parties are unable to agree on the appropriate treatment measures the human remains and buried with Native American human remains shall be reinterred with appropriate dignity, pursuant to Section 5.c., above.</p> <p>D. If Human Remains are not Native American</p> <ol style="list-style-type: none"> 1. The PI shall contact the Medical Examiner and notify them of the historic era context of the burial. 2. The Medical Examiner will determine the appropriate course of action with the PI and City staff (PRC 5097.98). 3. If the remains are of historic origin, they shall be appropriately removed and conveyed to the San Diego Museum of Man for analysis. The decision for internment of the human remains shall be made in consultation with City Staff, the applicant/landowner and the San Diego Museum of Man. <p>V. Night and/or Weekend Work</p> <p>A. If night and/or work is included in the contract</p> <ol style="list-style-type: none"> 1. When night and/or weekend work is included in the contract package, the extent and timing shall be presented and discussed at the Precon Meeting. 2. The following procedures shall be followed. <ol style="list-style-type: none"> (a) No Discoveries In the event that no discoveries were encountered during night and/or weekend work, the PI shall record the information on the CSVR and submit to City Staff via fax by 8 am of the next business day. (b) Discoveries 			

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	<p>All discoveries shall be processed and documented using the existing procedures detailed in Sections III - During Construction, and IV – Discovery of Human Remains. Discovery of human remains shall always be treated as a significant discovery.</p> <p>(c) Potentially Significant Discoveries</p> <p>If the PI determines that a potentially significant discovery has been made, the procedures detailed under Section III - During Construction and IV-Discovery of Human Remains shall be followed.</p> <p>(d) The PI shall immediately contact City Staff, or by 8 am of the next business day to report and discuss the findings as indicated in Section III-B, unless other specific arrangements have been made.</p> <p>B. If night and/or weekend work becomes necessary during the course of construction</p> <ol style="list-style-type: none"> 1. The Construction Manager shall notify the RE, or BI, as appropriate, a minimum of 24 hours before the work is to begin. 2. The RE, or BI, as appropriate, shall notify City Staff immediately. <p>C. All other procedures described above shall apply, as appropriate.</p> <p>VI. Post Construction</p> <p>A. Submittal of Draft Monitoring Report</p> <ol style="list-style-type: none"> 1. The PI shall submit two copies of the Draft Monitoring Report (even if negative) prepared in accordance with the HRG and Appendices which describes the results, analysis, and conclusions of all phases of the Archaeological Monitoring Program (with appropriate graphics) to City Staff, for review and approval within 90 days following the completion of monitoring. (a) For significant archaeological resources encountered during monitoring, the Archaeological Data Recovery Program shall be included in the Draft Monitoring Report. 			

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	<p>(b) Recording sites with State of California Department of Parks and Recreation</p> <p>The PI shall be responsible for recording (on the appropriate State of California Department of Park and Recreation forms-DPR 523 A/B) any significant or potentially significant resources encountered during the Archaeological Monitoring Program in accordance with the City's Historical Resources Guidelines, and submittal of such forms to the South Coastal Information Center with the Final Monitoring Report.</p> <p>2. City Staff shall return the Draft Monitoring Report to the PI for revision or, for preparation of the Final Report.</p> <p>3. The PI shall submit revised Draft Monitoring Report to City Staff for approval.</p> <p>4. City Staff shall provide written verification to the PI of the approved report.</p> <p>5. City Staff shall notify the RE or BI, as appropriate, of receipt of all Draft Monitoring Report submittals and approvals.</p> <p>B. Handling of Artifacts and Submittal of Collections Management Plan, if applicable</p> <p>1. The PI shall be responsible for ensuring that all cultural remains collected are cleaned and catalogued.</p> <p>2. The PI shall be responsible for ensuring that all artifacts are analyzed to identify function and chronology as they relate to the history of the area; that faunal material is identified as to species; and that specialty studies are completed, as appropriate.</p> <p>3. The PI shall submit a Collections Management Plan to City Staff for review and approval for any project which results in a substantial collection of historical artifacts.</p> <p>C. Curation of artifacts: Accession Agreement and Acceptance Verification</p> <p>1. The PI shall be responsible for ensuring that all artifacts associated with the survey, testing and/or data recovery for this project are permanently curated with an appropriate institution. This shall be completed in consultation with City Staff and the Native American representative, as applicable.</p>			

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	<p>2. The PI shall include the Acceptance Verification from the curation institution in the Final Monitoring Report submitted to the RE or BI and City Staff.</p> <p>3. When applicable to the situation, the PI shall include written verification from the Native American consultant/monitor indicating that Native American resources were treated in accordance with state law and/or applicable agreements. If the resources were reentered, verification shall be provided to show what protective measures were taken to ensure no further disturbance in accordance with section IV – Discovery of Human Remains, subsection 5.(d).</p> <p>D. Final Monitoring Report(s)</p> <p>1. The PI shall submit one copy of the approved Final Monitoring Report to the RE or BI as appropriate, and one copy to City Staff (even if negative), within 90 days after notification from City Staff that the draft report has been approved.</p> <p>2. The RE shall, in no case, issue the Notice of Completion until receiving a copy of the approved Final Monitoring Report from City Staff which includes the Acceptance Verification from the curation institution.</p>			
LAND USE (LND)				
Impact LU-B.2	Noise generated by I-5 and highly traveled grid streets could cause noise levels in noise-sensitive uses not governed by Title 24 to exceed 45 dB(A). (Direct)			
	Mitigation Measures NOI-B.1-1 and NOI-C.1.1, as described below.	Prior to Building Permit (Design) Prior to Certificate of Occupancy (Implementation)	Developer	Civic San Diego/City

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Impact LU-B.3	Noise levels in Downtown areas within the 65 CNEL contour of SDIA could exceed 45 dB(A) for noise sensitive uses not covered by Title 24. (Direct)			
	Mitigation Measures NOI-B.1-1, as described below.	Prior to Building Permit (Design) Prior to Certificate of Occupancy (Implementation)	Developer	Civic San Diego/City
Impact LU-B.4	Noise generated by train horns, engines and wheels as well as bells at crossing gates would significantly disrupt sleep of residents along the railroad tracks. (Direct)			
	Mitigation Measure LU-B.4-1: Prior to approval of a Building Permit which would expose habitable rooms to disruptive railroad noise, an acoustical analysis shall be performed. The analysis shall determine the expected exterior and interior noise levels related to railroad activity. As feasible, noise attenuation measures shall be identified which would reduce noise levels to 45 dB(A) CNEL or less in habitable rooms. Recommended measures shall be incorporated into building plans before approval of a Building Permit.	Prior to Building Permit (Design) Prior to Certificate of Occupancy (Implementation)	Developer	City
NOISE (NOD)				
Impact NOI-B.1	Noise generated by I-5 and highly traveled grid streets could cause interior noise levels in noise-sensitive uses (exclusive of residential and hotel uses) to exceed 45 dB(A). (Direct)			
	Mitigation Measure NOI-B.1-1: Prior to approval of a Building Permit for any residential, hospital, or hotel within 475 feet of the centerline of Interstate 5 or adjacent to a roadway carrying more than 7,000 ADT, an acoustical analysis shall be performed to confirm that architectural or other design features are included which would assure that noise levels within habitable rooms would not exceed 45 dB(A) CNEL.	Prior to Building Permit (Design) Prior to Certificate of Occupancy (Implementation)	Developer	Civic San Diego/City

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PALEONTOLOGICAL RESOURCES (PAL)				
Impact PAL-A.1	Excavation in geologic formations with a moderate to high potential for paleontological resources could have an significant impact on these resources, if present. (Direct)			

	<p><i>Mitigation Measure PAL-A.1-1:</i> In the event the Secondary Study indicates the potential for significant paleontological resources, the following measures shall be implemented as determined appropriate by Civic San Diego.</p> <p>I. Prior to Permit Issuance</p> <p>A. Construction Plan Check</p> <ol style="list-style-type: none"> 1. Prior to NTP for any construction permits, including but not limited to, the first Grading Permit, Demolition Permits and Building Permits, but prior to the first preconstruction meeting, whichever is applicable, Centre City Development Corporation Civic San Diego shall verify that the requirements for paleontological monitoring have been noted on the appropriate construction documents. <p>B. Letters of Qualification have been submitted to Civic San Diego</p> <ol style="list-style-type: none"> 1. The applicant shall submit a letter of verification to Civic San Diego identifying the PI for the project and the names of all persons involved in the paleontological monitoring program, as defined in the City of San Diego Paleontology Guidelines. 2. Civic San Diego will provide a letter to the applicant confirming the qualifications of the PI and all persons involved in the paleontological monitoring of the project. 3. Prior to the start of work, the applicant shall obtain approval from Civic San Diego for any personnel changes associated with the monitoring program. <p>II. Prior to Start of Construction</p> <p>A. Verification of Records Search</p> <ol style="list-style-type: none"> 1. The PI shall provide verification to Civic San Diego that a site-specific records search has been completed. Verification includes, but is not limited to a copy of a confirmation letter from San Diego Natural History Museum, other institution or, if the search was in-house, a letter of verification from the PI stating that the search was completed. 2. The letter shall introduce any pertinent information concerning expectations and probabilities of discovery during trenching and/or grading activities. <p>B. PI Shall Attend Precon Meetings</p> <ol style="list-style-type: none"> 1. Prior to beginning any work that requires monitoring, the Applicant shall arrange a Precon Meeting that shall include the PI, CM and/or Grading Contractor, RE, BI, if appropriate, and Civic San Diego. The qualified paleontologist shall attend any grading/excavation related Precon Meetings to make comments and/or suggestions 		
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	<p>concerning the paleontological monitoring program with the Construction Manager and/or Grading Contractor.</p> <p>a. If the PI is unable to attend the Precon Meeting, the Applicant shall schedule a focused Precon Meeting with Civic San Diego, the PI, RE, CM or BI, if appropriate, prior to the start of any work that requires monitoring.</p> <p>2. Identify Areas to be Monitored</p> <p>a. Prior to the start of any work that requires monitoring, the PI shall submit a Paleontological Monitoring Exhibit (PME) based on the appropriate construction documents (reduced to 11 by 17 inches) to Civic San Diego identifying the areas to be monitored including the delineation of grading/excavation limits. The PME shall be based on the results of a site specific records search as well as information regarding existing known soil conditions (native or formation).</p> <p>3. When Monitoring Will Occur</p> <p>a. Prior to the start of any work, the PI shall also submit a construction schedule to Civic San Diego through the RE indicating when and where monitoring will occur.</p> <p>b. The PI may submit a detailed letter to Civic San Diego prior to the start of work or during construction requesting a modification to the monitoring program. This request shall be based on relevant information such as review of final construction documents which indicate conditions such as depth of excavation and/or site graded to bedrock, presence or absence of fossil resources, etc., which may reduce or increase the potential for resources to be present.</p> <p>III. During Construction</p> <p>A. Monitor Shall be Present During Grading/Excavation/Trenching</p> <p>1. The monitor shall be present full-time during grading/excavation/trenching activities as identified on the PME that could result in impacts to formations with high and moderate resource sensitivity. The Construction Manager is responsible for notifying the RE, PI, and Civic San Diego of changes to any construction activities.</p>			

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	<p>2. The monitor shall document field activity via the CSV. The CSV's shall be faxed by the CM to the RE the first day of monitoring, the last day of monitoring, monthly (Notification of Monitoring Completion), and in the case of any discoveries. The RE shall forward copies to Civic San Diego.</p> <p>3. The PI may submit a detailed letter to Civic San Diego during construction requesting a modification to the monitoring program when a field condition such as trenching activities that do not encounter formation soils as previously assumed, and/or when unique/unusual fossils are encountered, which may reduce or increase the potential for resources to be present.</p> <p>B. Discovery Notification Process</p> <p>1. In the event of a discovery, the Paleontological Monitor shall direct the contractor to temporarily divert trenching activities in the area of discovery and immediately notify the RE or BI, as appropriate.</p> <p>2. The Monitor shall immediately notify the PI (unless Monitor is the PI) of the discovery.</p> <p>3. The PI shall immediately notify Civic San Diego by phone of the discovery, and shall also submit written documentation to Civic San Diego within 24 hours by fax or email with photos of the resource in context, if possible.</p> <p>C. Determination of Significance</p> <p>1. The PI shall evaluate the significance of the resource.</p> <p>a. The PI shall immediately notify Civic San Diego by phone to discuss significance determination and shall also submit a letter to Civic San Diego indicating whether additional mitigation is required. The determination of significance for fossil discoveries shall be at the discretion of the PI.</p> <p>b. If the resource is significant, the PI shall submit a Paleontological Recovery Program and obtain written approval from Civic San Diego. Impacts to significant resources must be mitigated before ground disturbing activities in the area of discovery will be allowed to resume.</p>			

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	<p>c. If resource is not significant (e.g., small pieces of broken common shell fragments or other scattered common fossils) the PI shall notify the RE, or BI as appropriate, that a non-significant discovery has been made. The Paleontologist shall continue to monitor the area without notification to Civic San Diego unless a significant resource is encountered.</p> <p>d. The PI shall submit a letter to Civic San Diego indicating that fossil resources will be collected, curated, and documented in the Final Monitoring Report. The letter shall also indicate that no further work is required.</p> <p>IV. Night Work</p> <p>A. If night work is included in the contract</p> <ol style="list-style-type: none"> When night work is included in the contract package, the extent and timing shall be presented and discussed at the precon meeting. The following procedures shall be followed. <ol style="list-style-type: none"> No Discoveries <p>(1) In the event that no discoveries were encountered during night work, The PI shall record the information on the CSVr and submit to Civic San Diego via fax by 9 a.m. the following morning, if possible.</p> Discoveries <p>(1) All discoveries shall be processed and documented using the existing procedures detailed in Sections III - During Construction.</p> Potentially Significant Discoveries <p>(1) If the PI determines that a potentially significant discovery has been made, the procedures detailed under Section III - During Construction shall be followed.</p> The PI shall immediately contact Civic San Diego, or by 8 a.m. the following morning to report and discuss the findings as indicated in Section III-B, unless other specific arrangements have been made. 		

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	<p>B. If night work becomes necessary during the course of construction</p> <ol style="list-style-type: none"> 1. The Construction Manager shall notify the RE, or BI, as appropriate, a minimum of 24 hours before the work is to begin. 2. The RE, or BI, as appropriate, shall notify Civic San Diego immediately. C. All other procedures described above shall apply, as appropriate. <p>V. Post Construction</p> <ol style="list-style-type: none"> A. Submittal of Draft Monitoring Report <ol style="list-style-type: none"> 1. The PI shall submit two copies of the Draft Monitoring Report (even if negative) which describes the results, analysis, and conclusions of all phases of the Paleontological Monitoring Program (with appropriate graphics) to Civic San Diego for review and approval within 90 days following the completion of monitoring, <ol style="list-style-type: none"> a. For significant paleontological resources encountered during monitoring, the Paleontological Recovery Program shall be included in the Draft Monitoring Report. b. Recording Sites with the San Diego Natural History Museum <ol style="list-style-type: none"> (1) The PI shall be responsible for recording (on the appropriate forms) any significant or potentially significant fossil resources encountered during the Paleontological Monitoring Program in accordance with the City's Paleontological Guidelines, and submittal of such forms to the San Diego Natural History Museum with the Final Monitoring Report. 2. Civic San Diego shall return the Draft Monitoring Report to the PI for revision or, for preparation of the Final Report. 			

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		Time Frame	Responsibility	Verification Responsibility
	<p>3. The PI shall submit revised Draft Monitoring Report to Civic San Diego for approval.</p> <p>4. Civic San Diego shall provide written verification to the PI of the approved report.</p> <p>5. Civic San Diego shall notify the RE or BI, as appropriate, of receipt of all Draft Monitoring Report submittals and approvals.</p> <p>B. Handling of Fossil Remains</p> <p>1. The PI shall be responsible for ensuring that all fossil remains collected are cleaned and catalogued.</p> <p>2. The PI shall be responsible for ensuring that all fossil remains are analyzed to identify function and chronology as they relate to the geologic history of the area; that faunal material is identified as to species; and that specialty studies are completed, as appropriate</p> <p>C. Curation of fossil remains: Deed of Gift and Acceptance Verification</p> <p>1. The PI shall be responsible for ensuring that all fossil remains associated with the monitoring for this project are permanently curated with an appropriate institution.</p> <p>2. The PI shall include the Acceptance Verification from the curation institution in the Final Monitoring Report submitted to the RE or BI and Civic San Diego.</p> <p>D. Final Monitoring Report(s)</p> <p>1. The PI shall submit two copies of the Final Monitoring Report to Civic San Diego (even if negative), within 90 days after notification from Civic San Diego that the draft report has been approved.</p> <p>2. The RE shall, in no case, issue the Notice of Completion until receiving a copy of the approved Final Monitoring Report from Civic San Diego which includes the Acceptance Verification from the curation institution.</p>			
TRAFFIC AND CIRCULATION (TRF)				
Impact TRF-A.1.1	Increased traffic on grid streets from Downtown development would result in unacceptable levels of service on specific roadway intersections and/or segments within downtown. (Direct)			

9 G Tower Project (PTS# 649856) Downtown FEIR/SEIR Mitigation Monitoring and Reporting Program				
Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	Mitigation Measure TRF-A.1.1-2: Prior to approval of any development which would generate a sufficient number of trips to qualify as a large project under the Congestion Management Program (i.e. more than 2,400 daily trips, or 200 trips during a peak hour period), a traffic study shall be completed. The traffic study shall be prepared in accordance with City's Traffic Impact Study Manual. If the traffic study indicates that roadways substantially affected by the project would operate at LOS F with the addition of project traffic, the traffic study shall identify improvements to grid street segments and/or intersections consistent with the Downtown San Diego Mobility Plan which would be required within the next five years to achieve an acceptable LOS or reduce congestion, to the extent feasible. If the needed improvements are already included in the City of San Diego's CIP, or the equivalent, no further action shall be required. If any of the required improvements are not included in the CIP, or not expected within five years of project completion, the City of San Diego shall amend the CIP, within one year of project approval, to include the required improvements and assure that they will be implemented within five years of project completion. At Civic San Diego's discretion, the developer may be assessed a pro-rated share of the cost of improvements as a condition of project approval.	Prior to Development Permit (Design)	Developer	Civic San Diego/City
Impact TRF-A.2.1-1	Elimination of Cedar St. off-ramp would impact other freeway ramps by redirecting traffic to other off ramps serving downtown. (Direct)			
	Mitigation Measure TRF A.2.2-1: Prior to elimination of the Cedar Street off-ramp from I-5, a traffic study shall be done by Civic San Diego in consultation with the City of San Diego and Caltrans to determine the potential effects associated with elimination of the off-ramp and the conversion of Cedar Street from one- to two-way. The report shall also identify roadway modifications that would minimize potential impacts on local surface streets and I-5.	Prior to elimination of Cedar Street off-ramp (Design/Implementation)	Civic San Diego/City	Civic San Diego/City

PLANNING COMMISSION
RESOLUTION NO. R-_____

TENTATIVE MAP NO. 2365584
9G - PROJECT NO. 649856

WHEREAS, Cisterra Partners, LLC, Subdivider, and Fuscoe Engineering, Engineer, submitted an application to the City of San Diego for Tentative Map No. 2365584 for 9G, a 22-story, 253-foot mixed-use development ("Project") located at 659 Ninth Avenue on the south side of G Street between Ninth and Tenth avenues. The property is legally described as Lots A, B, C, K, and L in Block 83 OF Horton's Addition in the City of San Diego, County of San Diego, State of California, according to map thereof on file in the Office of the County Recorder of San Diego County; and

WHEREAS, the Map proposes the Subdivision of a 25,061 square foot (SF) site into seven commercial condominiums; and

WHEREAS, development within the Downtown Community Planning (DCP) area is covered under the following documents, all referred to as the "Downtown FEIR": Final Environmental Impact Report (FEIR) for the San Diego Downtown Community Plan, Centre City Planned District Ordinance, and 10th Amendment to the Centre City Redevelopment Plan, certified by the former Redevelopment Agency ("Former Agency") and the City Council on March 14, 2006 (Resolutions R-04001 and R-301265, respectively); subsequent addenda to the FEIR certified by the Former Agency on August 3, 2007 (Former Agency Resolution R-04193), April 21, 2010 (Former Agency Resolution R-04510), and August 3, 2010 (Former Agency Resolution R-04544), and certified by the City Council on February 12, 2014 (City Council Resolution R-308724) and July 14, 2014 (City Council Resolution R-309115); and, the Final Supplemental Environmental Impact Report for the Downtown San Diego Mobility Plan certified by the City Council on June 21, 2016 (Resolution R-310561). Development within the Downtown Community Planning area is also covered under the following documents, all referred to

as the "CAP FEIR": FEIR for the City of San Diego Climate Action Plan (CAP), adopted by the City Council on December 15, 2015 (City Council Resolution R-310176), and the Addendum to the CAP, certified by the City Council on July 12, 2016 (City Council Resolution R-310595). The Downtown FEIR and CAP FEIR are both "Program EIRs" prepared in compliance with California Environmental Quality Act (CEQA) Guidelines Section 15168. The information contained in the Downtown FEIR and the CAP FEIR reflects the independent judgement of the City of San Diego as the Lead Agency and has been reviewed and considered by the decision maker before approving the project. Consistent with best practices suggested by CEQA Guidelines Section 15168, a Downtown 15168 Consistency Evaluation ("Evaluation") has been completed for the project. The Evaluation concluded that the environmental impacts of the project were adequately addressed in the Downtown FEIR and CAP FEIR; that the project is within the scope of the development program described in the Downtown FEIR and CAP FEIR and is adequately described within both documents for the purposes of CEQA; and, that none of the conditions listed in CEQA Guidelines Section 15162 exist. Therefore, no further environmental documentation is required under CEQA; and

WHEREAS, the project complies with the requirements of a geotechnical report pursuant to Subdivision Map Act sections 66490 and 66491(b)-(f) and San Diego Municipal Code section 144.0220; and

WHEREAS, the subdivision is a condominium project as defined in California Civil Code section 4125 and filed pursuant to the Subdivision Map Act. The total number of condominium units is seven; and

WHEREAS, on August 20, 2020, the Planning Commission of the City of San Diego considered Tentative Map No. 2365584, and pursuant to San Diego Municipal Code section(s) 125.0440 and Subdivision Map Act section 66428, received for its consideration written and oral presentations,

evidence having been submitted, and testimony having been heard from all interested parties at the public hearing, and the Planning Commission having fully considered the matter and being fully advised concerning the same; NOW THEREFORE,

BE IT RESOLVED by the Planning Commission of the City of San Diego, that it adopts the following findings with respect to Tentative Map No. 2365584:

1. The proposed subdivision and its design or improvement are consistent with the policies, goals, and objectives of the applicable land use plan.

The proposed subdivision and Project design is consistent with the objectives of the DCP, Centre City Planned District Ordinance (CCPDO), and the Downtown Design Guidelines (DDG). The Project advances the goals and policies of the DCP by contributing to the orderly growth of the East Village neighborhood with the construction of a mixed-use development with both residential and commercial uses, which adds to the balance of uses in Downtown and advances the goal of the making the Northwest sub-district of the East Village neighborhood the most intense residential area. Specifically, the Project is adding 241 residential dwelling units (DU) and approximately 35,000 SF of commercial space, providing a neighborhood amenity for surrounding residents and taking advantage of the site's central location near transit and other Downtown attractions, per the DCP (6.5-G-6 and 6.5-G-8). In the CCPDO, residential and commercial/retail uses are permitted land uses in the Employment/Residential Mixed-Use (ER) land use district. Per the DDG, the Project utilizes architectural articulations, provides street level activation, and uses high-quality materials. The building's massing is modulated through the use of glazing, concrete framing elements, rooftop off-set planes, and varying materials around the Project, which provide visual interest and architectural variation. Therefore, the proposed subdivision and its design/improvement are consistent with the policies, goals, and objectives of the applicable land use plan.

2. The proposed subdivision complies with the applicable zoning and development regulations of the Land Development Code, including any allowable deviations pursuant to the Land Development Code.

The subdivision proposes the creation of seven commercial condominium units in a mixed-use development containing 241 residential DU and 35,668 SF of commercial space. Residential and commercial land uses are permitted in the ER district of the CCPDO.

The Project proposes five deviations from the development regulations of the San Diego Municipal Code (SDMC) to 1) reduce the number of required personal storage areas from 100% of DU (241 DU) to 38% of DU (91 DU), 2) reduce the minimum width required for the one-way driveway on Ninth Avenue width from 14 feet to 12 feet, 3) increase the allowable maximum tower lot coverage from 50% to 66%, 4) increase the allowable maximum east-west tower floor plate dimension from 130 feet to 150 feet, and 5) Reduce the minimum required ground level transparency of the Tenth avenue building façade from 60% to 40%. The requested deviations serve to maximize the density and use of the site, which is constrained by the presence of the existing historical resource being

retained and restored on-site and the lot size. The reduction of the personal storage requirement and increase in tower lot coverage and tower floor plate allow for more buildable area and therefore allow for the highest and best use of the site by providing more housing units than would otherwise be feasible on the site and therefore making the Project financially feasible. The maximization of the property with the provision of housing opportunities advances the population goals of the DCP. The reduction of the driveway width and transparency requirement are a result of the Project's effort to minimize modifications to the historical resource façade, which advances the historic preservation goals of the DCP.

Deviations from the SDMC are permitted with approval a NDP pursuant to Section 126.0402(p). Residential and commercial land uses are permitted in the ER land use district of the CCPDO, so with approval of the NDP and compliance with the conditions of approval of the permit, the proposed development is consistent with the SDMC. The Project proposes the creation of seven commercial condominium units and complies with all development regulations of the Land Development Code with approval of SDP/NDP/CUP No. 649856. Therefore, the proposed subdivision complies with the applicable zoning and development regulations of the Land Development Code including any allowable deviations pursuant to the Land Development Code.

3. The site is physically suitable for the type and density of development.

The Project proposes a mixed-use development with 241 DU and approximately 35,668 SF of commercial space on a 25,061 SF site. The site is located within the ER district of the CCPDO which allows a wide range of land uses, including residential and commercial. The Project contains 241 residential DU and 35,668 SF of commercial space, which are permitted in the ER district and is a type of development suitable for the site. The Base Maximum Floor Area Ratio (FAR) of the site is 6.0, which can be increased up to 20.0 with the utilization of the CCPDO FAR bonus programs. The Project's FAR is proposed to be 11.14, which they are achieving with the 6.0 Base FAR established in the CCPDO, and an additional 2.0 for the provision of three-bedroom DUs, 1.0 for LEED Silver certification, and 1.14 through the Payment Program. The proposed 11.14 FAR is within the allowable range of FAR permitted on the site by the CCPDO and DCP; therefore, the site is physically suitable for the type and density of development.

4. The design of the subdivision or the proposed improvements are not likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat.

The Project was determined to be consistent with the previously certified Downtown Final Environmental Impact Report (Downtown FEIR). Therefore, per CEQA Guidelines section 15162, no subsequent or supplemental EIR is required for this project. The Project site is located within an urbanized environment where there are no watercourses or environmentally sensitive lands harboring fish or wildlife on or adjacent to the site. Therefore, the design of the subdivision will not cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat.

5. The design of the subdivision or the type of improvement will not be detrimental to the public health, safety, and welfare.

The design of the subdivision will not have a detrimental impact upon the public's health, safety and general welfare. The Project is consistent with the plans for the East Village neighborhood as envisioned in the DCP by advancing the development of the Northwest sub-district of the East Village neighborhood as the most intensive residential area and will contribute to its vitality by creating new commercial units that serve as neighborhood amenities. Public improvements include new curb, gutter and sidewalks, reconstruction of existing curb ramps to meet City Standards, and streetscaping around the site. The completion of the specified improvement to the satisfaction of the City Engineer will be required prior to the approval of the Certificate of Compliance to ensure public health, safety and welfare. The Project complies with the DCP and is providing public improvements; therefore, the design of the subdivision or the type of improvement will not be detrimental to the public health, safety, and welfare.

6. The design of the subdivision or the type of improvements will not conflict with easements acquired by the public at large for access through or use of property within the proposed subdivision.

The subdivision proposes the creation of seven commercial condominium units and has frontage on G Street, Ninth Avenue, and Tenth Avenue. The site does not contain any easements acquired by the public at large for access through or use of property within the subdivision. Therefore, the design of the subdivision and proposed improvement would not conflict with easements acquired by the public at large for access through or use of property within the proposed subdivision.

7. The design of the proposed subdivision provides, to the extent feasible, for future passive or natural heating and cooling opportunities.

The subdivision proposes to create seven commercial condominium units for a proposed development within a single lot. Architectural features and techniques are incorporated into the proposed Project which including roofing materials with three-year aged solar reflection and thermal emittance values consistent with the voluntary measures under the California Green Building Standards Code to natural cool the roof and reduce heat absorption, solar-rated windows to reflect heat and open to allow for natural cooling opportunities. Passive cooling opportunities are provided with private exterior balconies in 91 DU (38%) and 6,134 SF of common, landscaped, outdoor open space. Therefore, the design of the proposed subdivision provides sufficient future passive or natural heating and cooling opportunities.

8. The decision maker has considered the effects of the proposed subdivision on the housing needs of the region and that those needs are balanced against the needs for public services and the available fiscal and environmental resources.

The subdivision proposes to create seven new commercial condominium units. The 0.57-acre site is located on the south side of G Street between Ninth and Tenth avenues in the East Village neighborhood of the DCP area. Development Impact Fees (DIF), school, water/sewer connection and other impact fees will be required at the building permit issuance. The Project will comply with the City's Inclusionary Housing Ordinance through payment of the in-lieu fee at the time of building permit issuance. The Project is located in an urban, developed neighborhood and served by existing public infrastructure, including improved right-of-way and water and sewer lines. The previously certified Downtown FEIR, with which this Project has been deemed consistent, evaluated the

environmental impacts of the Downtown area at full build-out of 90,000 residents including public service needs. The decision maker has reviewed the administrative record including the Project plans, environmental documentation, and public testimony to determine the effects of the proposed subdivision on the housing needs of the region. Those needs were balanced against the needs for public services and the available fiscal and environmental resources and it is found that the addition of seven commercial condominium units is consistent with the housing needs anticipated for Downtown.

BE IT FURTHER RESOLVED, that based on the Findings hereinbefore adopted by the Planning Commission, Tentative Map No. 2365584 is hereby granted to the referenced Subdivider/Engineer subject to the attached conditions which are made a part of this resolution by this reference.

By

James Alexander
Program Manager
Urban Division

ATTACHMENT: Tentative Map Conditions

Internal Order No. 24008439

PLANNING COMMISSION
CONDITIONS FOR TENTATIVE MAP NO. 22365584
9G TM - PROJECT NO. 649856
ADOPTED BY RESOLUTION NO. _____ ON AUGUST 20, 2020

GENERAL

1. This Tentative Map will expire on August 20, 2023.
2. Compliance with all of the following conditions shall be completed and/or assured, to the satisfaction of the City Engineer, prior to the recordation of the Final Map/Parcel Map, unless otherwise noted.
3. Prior to the recordation of the Final Map/Parcel Map, taxes must be paid on this property pursuant to Subdivision Map Act section 66492. To satisfy this condition, a tax certificate stating that there are no unpaid lien conditions against the subdivision must be recorded in the Office of the San Diego County Recorder.
4. The Tentative Map shall conform to the provisions of SDP/NDP/CUP No. 649856.
5. The Subdivider shall defend, indemnify, and hold the City (including its agents, officers, and employees [together, "Indemnified Parties"]) harmless from any claim, action, or proceeding, against the City and/or any Indemnified Parties to attack, set aside, void, or annul City's approval of this project, which action is brought within the time period provided for in Government Code section 66499.37. City shall promptly notify Subdivider of any claim, action, or proceeding and shall cooperate fully in the defense. If City fails to promptly notify Subdivider of any claim, action, or proceeding, or if City fails to cooperate fully in the defense, Subdivider shall not thereafter be responsible to defend, indemnify, or hold City and/or any Indemnified Parties harmless. City may participate in the defense of any claim, action, or proceeding if City both bears its own attorney's fees and costs, City defends the action in good faith, and Subdivider is not required to pay or perform any settlement unless such settlement is approved by the Subdivider.

ENGINEERING

6. The Subdivider shall assure, by permit and bond, the removal of existing driveways, and replace it with curb, gutter and sidewalk per City Standard, adjacent to the site on 9th Avenue, G Street, and 10th Avenue, satisfactory to the City Engineer.
7. The Subdivider shall assure, by permit and bond, the reconstruction of the existing curb with curb and gutter per current City Standards, adjacent to the site on 9th Avenue, G Street, and 10th Avenue, satisfactory to the City Engineer.
8. The Subdivider shall assure, by permit and bond, to reconstruct the sidewalk with current City Standard sidewalk, maintaining the existing sidewalk scoring pattern and preserving the contractor's stamp, adjacent to the site on 9th Avenue, G Street, and 10th Avenue, satisfactory to the City Engineer.

9. The Subdivider shall assure, by permit and bond, to reconstruct the two existing curb ramps at the corner of 9th Avenue/G Street and 10th Avenue/G Street, with current City Standard curb ramp Standard Drawing SDG-130 and SDG-132 with Detectable/Tactile Warning Tile, satisfactory to the City Engineer.
10. The Subdivider shall obtain an Encroachment Maintenance Removal Agreement, from the City Engineer, for the sidewalk underdrain/curb outlet and proposed private improvements of any kind, including enhanced pavers, street trees and landscaping to be installed within the in the public right-of-way (ROW).
11. The Subdivider shall obtain a Grading Permit for the shoring proposed for this project, satisfactory to the City Engineer.
12. The Subdivider shall incorporate any construction Best Management Practices necessary to comply with Chapter 14, Article 2, Division 1 (Grading Regulations) of the SDMC, into the construction plans or specifications.
13. The Subdivider shall submit a Water Pollution Control Plan (WPCP). The WPCP shall be prepared in accordance with the guidelines in Part 2 Construction BMP Standards Chapter 4 of the City's Storm Water Standards.
14. The drainage system proposed for this development, as shown on the site plan, is subject to approval by the City Engineer.
15. The Subdivider shall enter into a Maintenance Agreement for the ongoing permanent BMP maintenance, satisfactory to the City Engineer.
16. The Subdivider shall submit a Technical Report that will be subject to final review and approval by the City Engineer, based on the Storm Water Standards in effect at the time of the construction permit issuance.
17. Conformance with the "General Conditions for Tentative Subdivision Maps," filed in the Office of the City Clerk under Document No. 767688 on May 7, 1980, is required. Only those exceptions to the General Conditions which are shown on the Tentative Map and covered in these special conditions will be authorized. All public improvements and incidental facilities shall be designed in accordance with criteria established in the Street Design Manual, filed with the City Clerk as Document No. RR-297376.

MAPPING

18. Prior to the expiration of the tentative map (TM), if approved, a Parcel or Final map to consolidate and subdivide the properties into a maximum of seven commercial condominium units shall be recorded at the Office of the San Diego County Recorder.
19. Prior to the recordation of the map, taxes must be paid or bonded for the property pursuant to Section 66492 of the Subdivision map Act. A current original tax certificate, recorded in the Office of the San Diego County Recorder must be provided to satisfy this condition. If a tax

bond is required as indicated in the tax certificate, ensure that it is paid or posted at the County Clerk of the Board of Supervisors Office and supply proof prior to the recordation of the map.

20. The map shall be based on field survey and all lot corners must be marked with durable survey monuments pursuant to SDMC Section 144.0311(d) and Subdivision Map Action Section 66495. All survey monuments shall be set prior to the recordation of the map, unless the setting of monuments is deemed impractical due to the proposed improvements and/or grading associated with the Project, in which case, delayed monumentation may be applied on the map in accordance with SDMC Section 144.0130.
21. All subdivision maps in the City are required to be tied to the California Coordinate system of 1983 (CCS83), Zone 6 pursuant to Section 8801-8819 of the California Public Resources Code.
22. The map shall:
 - a. Use the California Coordinate system for its "Basis of Bearings" and express all measured and calculated bearing values in terms of said system. The angle of grid divergence from a true meridian (theta or mapping angle) and the north point of said map shall appear on each sheet thereof. Establishment of said Basis of Bearings may be by use of existing Horizontal Control stations or astronomic observations.
 - b. Show two measured ties from the boundary of the map to existing Horizontal Control stations having California Coordinate values of First Order accuracy. These tie lines to the existing control shall be shown in relation to the California Coordinate System (i.e. grid bearings and grid distances). All other distances shown on the map are to be shown as ground distances. A combined factor for the conversion of grid-to-ground shall be shown on the map.
23. "Basis of Bearings" means the source of uniform orientation of all measured bearings shown on the map. Unless otherwise approved, this source shall be the California Coordinate System, Zone 6, North American Datum of 1983 [NAD 83].
24. "California Coordinate System" means the coordinate system as defined in Section 8801 through 8819 of the California Public Resources Code. The specified zone for San Diego County is "Zone 6," and the official datum is the "North American Datum of 1983."

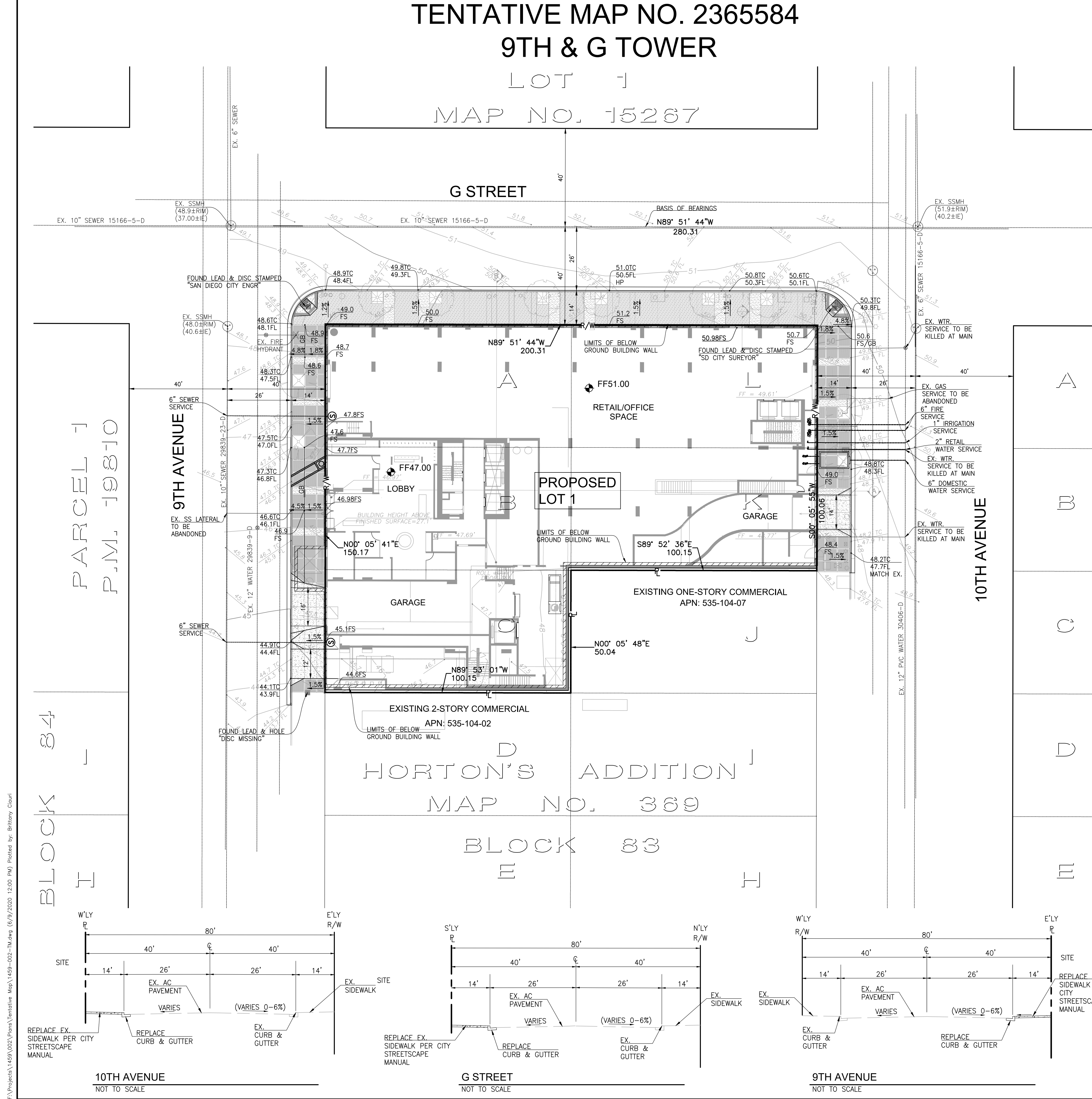
INFORMATION:

- The approval of this Tentative Map by the Planning Commission of the City of San Diego does not authorize the subdivider to violate any Federal, State, or City laws, ordinances, regulations, or policies including but not limited to, the Federal Endangered Species Act of 1973 and any amendments thereto (16 USC § 1531 et seq.).
- If the Subdivider makes any request for new water and sewer facilities (including services, fire hydrants, and laterals), the Subdivider shall design and construct such facilities in accordance with established criteria in the most current editions of the

City of San Diego water and sewer design guides and City regulations, standards and practices pertaining thereto. Off-site improvements may be required to provide adequate and acceptable levels of service and will be determined at final engineering.

- Subsequent applications related to this Tentative Map will be subject to fees and charges based on the rate and calculation method in effect at the time of payment.
- Any party on whom fees, dedications, reservations, or other exactions have been imposed as conditions of approval of the Tentative Map, may protest the imposition within ninety days of the approval of this Tentative Map by filing a written protest with the San Diego City Clerk pursuant to Government Code sections 66020 and/or 66021.
- Where in the course of development of private property, public facilities are damaged or removed, the Subdivider shall at no cost to the City, obtain the required permits for work in the public right-of-way, and repair or replace the public facility to the satisfaction of the City Engineer (San Diego Municipal Code § 142.0607).

Internal Order No. 24008439



TENTATIVE MAP NOTE

THIS IS A MAP OF A CONDOMINIUM PROJECT AS DEFINED IN SECTION 4125 OR 6542 OF THE CIVIL CODE OF THE STATE OF CALIFORNIA AND IS FILED PURSUANT TO THE SUBDIVISION MAP ACT. THE TOTAL NUMBER OF COMMERCIAL CONDOMINIUM UNITS IS 7.

NUMBER OF LOTS AND AREA

NO. OF EXISTING LOTS = FIVE (5)
NO. OF PROPOSED LOTS = ONE (1)
GROSS AREA = 0.575 AC (25,061 SF)

LEGAL DESCRIPTION

THE LAND REFERRED TO HEREIN BELOW IS SITUATED IN THE CITY OF SAN DIEGO, IN THE COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOW:

LOTS A, B, C, K AND L IN BLOCK 83 OF HORTON'S ADDITION, IN THE CITY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO MAP THEREOF ON FILE IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY.

APN: 535-136-01-00

SITE ADDRESS

659 9TH AVENUE, SAN DIEGO, CA 92101

ZONING / OVERLAYS

EXISTING ZONE: CCPD-ER AIRPORT INFLUENCE AREA (AIA) AREA #2
PROPOSED ZONE: CCPD-ER RESIDENTIAL TANDEM PARKING TRANSIT AREA PRIORITY

MINIMUM SETBACKS

FRONT: 0 MINIMUM
SIDE: 0 MINIMUM
STREET SIDE: 0 FEET

NUMBER OF UNITS & PARKING RATE

STUDIO 60
1 BEDROOM 119
2 BEDROOM 30
3 BEDROOM 27
PENTHOUSES 5
TOTAL RESIDENTIAL APARTMENT UNITS (RENTAL) = 241

PROVIDED RESIDENTIAL PARKING:
PARKING LEVEL 3-6: 230
TOTAL RESIDENTIAL PARKING PROVIDED= 230

COMMERCIAL PARKING RATE:
COMMERCIAL PARKING LEVEL 2: 43
COMMERCIAL PARKING REQUIRED/PROVIDED= 43

OWNERSHIP

A LEASEHOLD TITLE INTEREST IN THE SUBJECT PROPERTY WILL BE HELD BY: CISTERA PARTNERS, LLC., A CALIFORNIA LIMITED LIABILITY COMPANY. FEE TITLE INTEREST IS CURRENTLY HELD BY 4 LEE TENTH, LLC, A CALIFORNIA LIMITED LIABILITY COMPANY.

BY: *Elizabeth Lee*
NAME: *Elizabeth Lee*
TITLE: *Co-owner / Manager of 4 Lee Tenth LLC*

TITLE REPORT

PREPARED BY: CHICAGO TITLE COMPANY
2365 NORTHSIDE DRIVE
SUITE 600
SAN DIEGO, CA 92108
ORDER NO. 00109991-993-SD2-CFU
TRANSMITTAL DATED: MAY 23, 2019

EASEMENTS

INDICATES PROPERTY IS SUBJECT TO THE FACT THAT SAID LAND LIES WITHIN THE BOUNDARIES OF THE CENTRE CITY REDEVELOPMENT AREA AS DISCLOSED BY DOCUMENT RECORDED APRIL 30, 2007 AS INSTRUMENT NO. 2007-0292863 AND SEPTEMBER 4, 2009 AS INSTRUMENT NO. 2009-0499449, BOTH OF OFFICIAL RECORDS. AFFECTS SUBJECT PROPERTY BUT IS NOT PLOTTABLE

BASIS OF BEARING

THE BASIS OF BEARINGS OF THIS SURVEY IS THE CENTERLINE OF "G" STREET AS SHOWN ON MAP 15267. I.E. N 89°51'44"W

REFERENCE DRAWINGS

CITY OF SAN DIEGO DRAWING NUMBERS:
15166-D "G" STREET INTERCEPTOR SEWER
2346-AD FOURTH AVENUE, FIFTH AVENUE, SIXTH AVENUE,
29839-D PLANS FOR THE CONSTRUCTION OF WATER & SEWER GROUP 539

UTILITY NOTE

WATER: CITY OF SAN DIEGO
IRRIGATION: CITY OF SAN DIEGO
SEWER: CITY OF SAN DIEGO
GAS & SDG&E
ELECTRIC: AT&T
TELEPHONE: COX COMMUNICATIONS
CABLE: COX COMMUNICATIONS

ALL EXISTING UTILITIES ARE UNDERGROUND. ALL PROPOSED UTILITIES SERVING THE SUBDIVISION SHALL BE UNDERGROUNDED WITH THE APPROPRIATE PERMITS.

MAPPING NOTE

A PARCEL MAP SHALL BE FILED AT THE COUNTY RECORDER'S OFFICE PRIOR TO THE EXPIRATION OF THE TENTATIVE MAP. IF APPROVED, A DETAILED PROCEDURE OF SURVEY SHALL BE SHOWN ON THE FINAL OR PARCEL MAP AND ALL PROPERTY CORNERS SHALL BE MARKED WITH DURABLE SURVEY MONUMENTS.

LEGEND

ITEM	SYMBOL
PROPERTY BOUNDARY	---
RIGHT OF WAY	---
STREET CENTERLINE	---
PROPOSED IMPROVEMENTS	
ITEM	SYMBOL
BUILDING FOOTPRINT	---
6" CURB & GUTTER	---
WATER SERVICE	---
FIRE WATER SERVICE	---
SEWER SERVICE	---
IRRIGATION SERVICE	---
BACKFLOW PREVENTER	---
WATER METER VAULT	---
CURB OUTLET PER SDRSD D25-A	---
PROPRIETARY BIOFILTRATION UNIT	---
CURB RAMP	---

EXISTING IMPROVEMENTS

ITEM	SYMBOL
EXISTING FIRE HYDRANT	---
EXISTING STREET LIGHT	---
EXISTING TRAFFIC SIGNAL	---
EXISTING SEWER MAIN	---
EXISTING WATER MAIN	---
EXISTING GAS MAIN	---
EXISTING ELECTRICAL	---
EXISTING FIBER OPTICS	---

COORDINATE INDEX

CCS '83: 1838-6281
L.C.: 198-1721

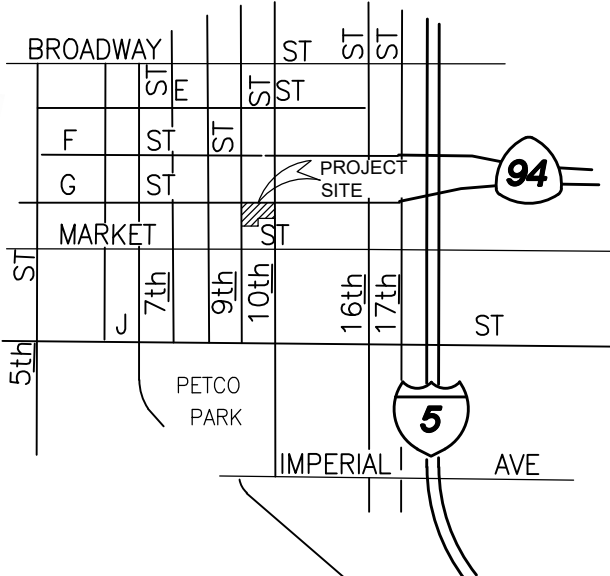
BENCHMARK

THE BENCHMARK USED FOR THIS SURVEY IS THE NORTHWEST BRASS PLUG LOCATED AT THE INTERSECTION OF NINTH AVENUE AND "F" STREET.

ELEV. = 55.931' MSL (NGVD 29)

MAP LEGEND

INDICATES FOUND LEAD AND DISC AS NOTED



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ATTACHMENT 15

9G
PLANNED DEVELOPMENT PERMIT
PACKAGE

3rd SUBMITTAL
7.20.20

ISSUES:

PROJECT NO:
5937.00
FILE NAME:
BM 300/5937 00-Cisterra
-ARCH - Cisterra05_2018.3.rvt
DRAWN BY: CHECKED BY:
Author Checker
PLOT DATE:
6/9/2020 2:02:26 PM
TITLE:

Cover

1

9G
PLANNED DEVELOPMENT
PERMIT PACKAGE

659 9th Avenue, California 92101
DATE 7.20.20
THIRD SUBMITTAL

Cisterra
DEVELOPMENT

carrierjohnson + CULTURE
architecture + environments + brand strategy + graphics

carrierjohnson + CULTURE
architecture + environments + brand strategy + graphics

Cisterra
DEVELOPMENT

185 W F St suite 500, San Diego, CA 92101
phone 619.239.2353

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PROJECT ADDRESS

659 9TH AVE SAN DIEGO, CA, 92101-6409
APN: 535-136-01-00

OWNER'S NAME AND ADDRESS

4 Lee Tenth, LLC, a California limited liability company.

PROJECT DESCRIPTION

1. NEW CONSTRUCTION, PARTIAL BLOCK MIXED-USE DEVELOPMENT.
2. 22 STORY RESIDENTIAL TOWER, WITH GROUND FLOOR AND (1) LEVEL SUBGRADE RETAIL/OFFICE SPACE.
3. 3 LEVELS OF ABOVE GROUND PARKING
3. RENOVATION OF EXISTING HISTORIC BUILDING; FARKAS STORE FIXTURE BUILDING, BUILT 1932.
4. SITE WORK TO STREETScape SURROUND PROPERTY.

LEGAL DESCRIPTION

THE LAND REFERRED TO HEREIN BELOW IS SITUATED IN THE CITY OF SAN DIEGO, IN THE COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOW:

LOTS A, B, C, K AND L IN BLOCK 83 OF HORTON'S ADDITION, IN THE CITY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO THE MAP THEREOF ON FILE IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY.

PROJECT TEAM

APPLICANT/DEVELOPER

Cisterra Development
3580 Carmel Mountain Road, Suite 450
San Diego, Ca 92130
T: 619.615.0200
E: JWood@cisterra.com
Contact: Jason Wood

ARCHITECT / LANDSCAPE ARCHITECT

Carrier Johnson + Culture
185 W F Street, suite 500
San Diego, Ca. 92101
T: 619.239.2353
E: dlh@carrierjohnson.com
Contact: Duane Hagewood

HISTORIC ARCHITECT

Union Architecture, Inc.
344 22ND STREET
SAN DIEGO, CA 92102
T: 619.269.4941

CIVIL ENGINEER

Fusco Engineering, Inc.
6390 Greenwich Drive, Suite 170
San Diego, Ca. 92122
T: 858.554.1500
E: bsmith@fuscoe.com
Contact: Bryan Smith

FLOOR AREA RATIO

BASE FAR 6.0

FAR BONUSES:
3 BEDROOM UNITS 2.0
LEED SILVER 1.0
PURCHASED FAR 2.14

ALLOWED FAR 11.14
PROPOSED FAR 11.14

CONSTRUCTION TYPE

TYPE IA FULLY SPRINKLERED

OCCUPANCY CLASSIFICATION

R-2 RESIDENTIAL
A-3 RESIDENTIAL AMENITIES
B OFFICE
M RETAIL
S-2 PARKING
S-1 STORAGE

APPLICABLE BUILDING CODE

CBC 2019
CITY OF SAN DIEGO MUNICIPAL CODE

PROPOSED BUILDING HEIGHT

22 STORY HIGHRISE
50.75 FEET SITE ELEVATION (SE)
253 FEET ABOVE GROUND LEVEL (AGL)
304 FEET ABOVE MEAN SEA LEVEL (AMSL)

DRAWING INDEX:

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| 2 | Program Sheet Index |
| 3 | Program Summary |
| 4 | Vicinity Map |
| 5 | Photographic Survey |
| 6 | Site Plan |
| 7 | Fire Access Plan |
| 8 | Site Accesability |
| 9 | Level B1 / Basement |
| 10 | Level 1 / Ground Floor |
| 11 | Level 2 / Parking |
| 12 | Level 3 / Parking |
| 13 | Level 4 / Parking |
| 13a | Level 5 / Parking |
| 14 | Level 6 / Parking |
| 15 | Level 7 / Residential |
| 16 | Level 8-19 / Residential |
| 17 | Level 20 / Residential |
| 18 | Level 21 / Residential |
| 19 | Level 22 / Ameneties |
| 20 | Roof Level |
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| 60 | Civil - Conceptual Grading & Utility Plan |
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| 60b | Civil - Conceptual Proposed Curb Usage Plan |
| 61 | FAR Bonus Incentives |
| 62 | FAR Bonus Incentives |
| 63 | FAR Bonus Incentives |

REQUESTED DEVIATIONS

Relevant CCPDO (9-2019) sections for which the project is seeking deviations

- | | |
|---------------------------------|--|
| 1. Personal Storage | 131.0454 |
| 2. Driveway Width | 142.0560 (j), & Table 142-05m |
| 3. Max. Lot Coverage Tower | 156.0310 (d)(3)(A) |
| 4. Floor Plate Dimensions Tower | 156.0310 (d)(3)(B), & Table 156-0310-A |
| 5. Transparency of Facade | 156.0311(d)(1) |

REQUIRED DISCRETIONARY PERMITS/APPROVALS

1. Site Development Permit
2. Tentative Map
3. Conditional Use Permit for sale of alcohol

9G
PLANNED DEVELOPMENT PERMIT
PACKAGE

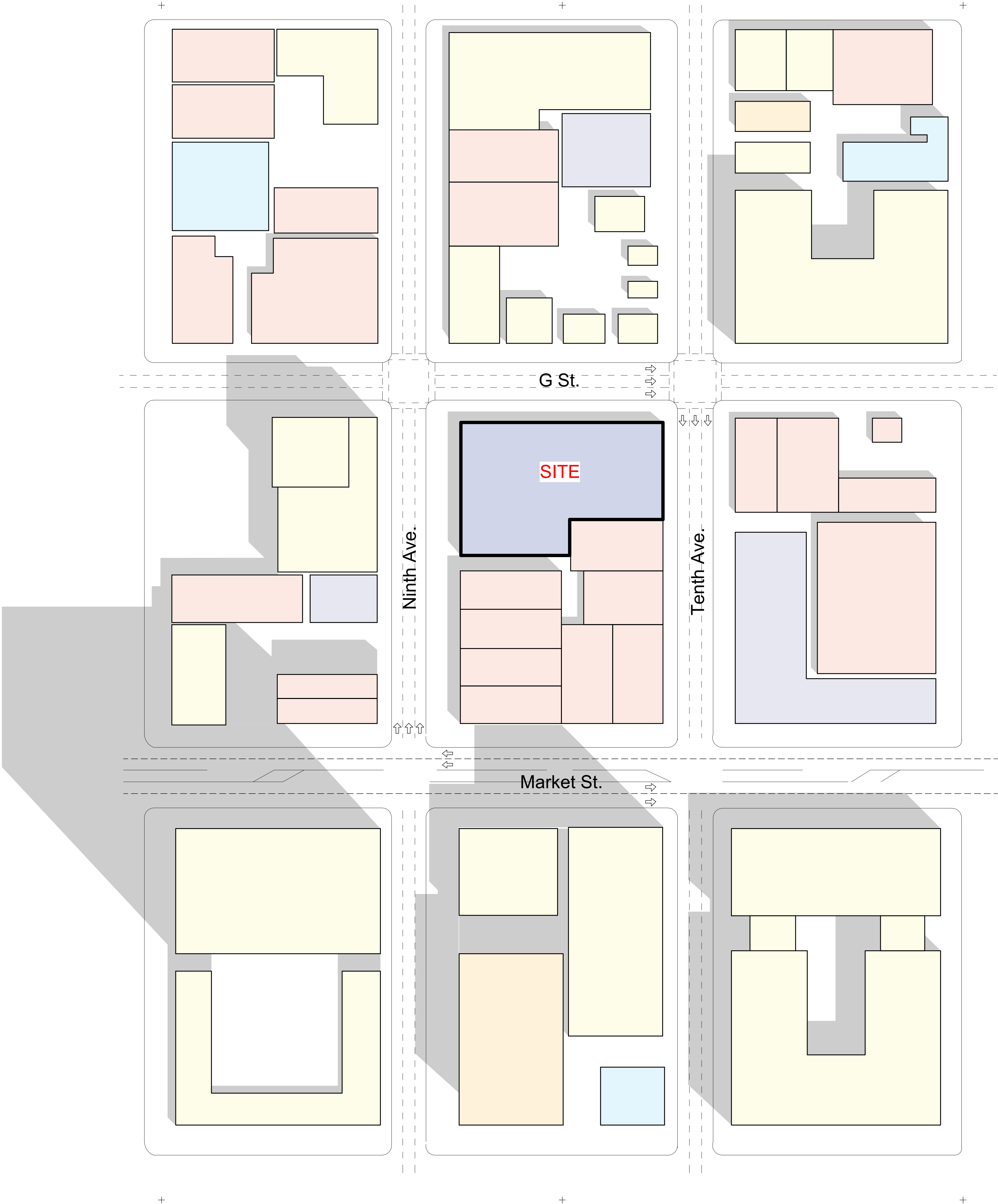
3rd SUBMITTAL
6.9.20

ISSUES:

PROJECT NO:
5937.00
FILE NAME:
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-2020\ARCH - Cisterra\05_2018.3.rvt
DRAWN BY: Author
CHECKED BY: Checker
PLOT DATE:
6/9/2020 2:02:27 PM
TITLE:

Program Sheet
Index

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- LEGEND:
- SITE
 - COMMERCIAL
 - RESIDENTIAL
 - PARKING
 - HOSPITALITY
 - OFFICE



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PHOTO KEY MAP



1. VIEW SOUTHWEST ALONG 9TH AVE. - BUSINESS PREMISES



2. VIEW WEST ALONG 9TH AVE. - RESIDENTIAL BUILDING



3. VIEW NORTHWEST ALONG 9TH & G ST. - CROSSFIT GYM



4. VIEW NORTHEAST ALONG 10TH & G ST. - RESIDENTIAL BUILDING



5. VIEW SOUTHEAST ALONG 10TH AVE. - PUBLIC PARKING



6. VIEW SOUTHEAST ALONG 9TH AVE. - FARKAS STORE BUILDING



7. VIEW EAST ALONG G ST. - RESIDENTIAL BUILDING AND FARKAS STORE



8. VIEW SOUTHWEST CORNER ALONG 9TH & G ST. - FARKAS STORE BUILDING



9. VIEW SOUTH ALONG 10TH ST. - BUSINESS PREMISES AND FARKAS STORE BUILDING

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185 W F St suite 500, San Diego, CA 92101
phone 619.239.2353

PLANNED DEVELOPMENT PERMIT PACKAGE

3rd SUBMITTAL
6.9.20

ISSUES:

PROJECT NO: 37.00

FILE NAME: 36015937.00-Cisterra ARCH_Cisterra9G_2018.3.rvt

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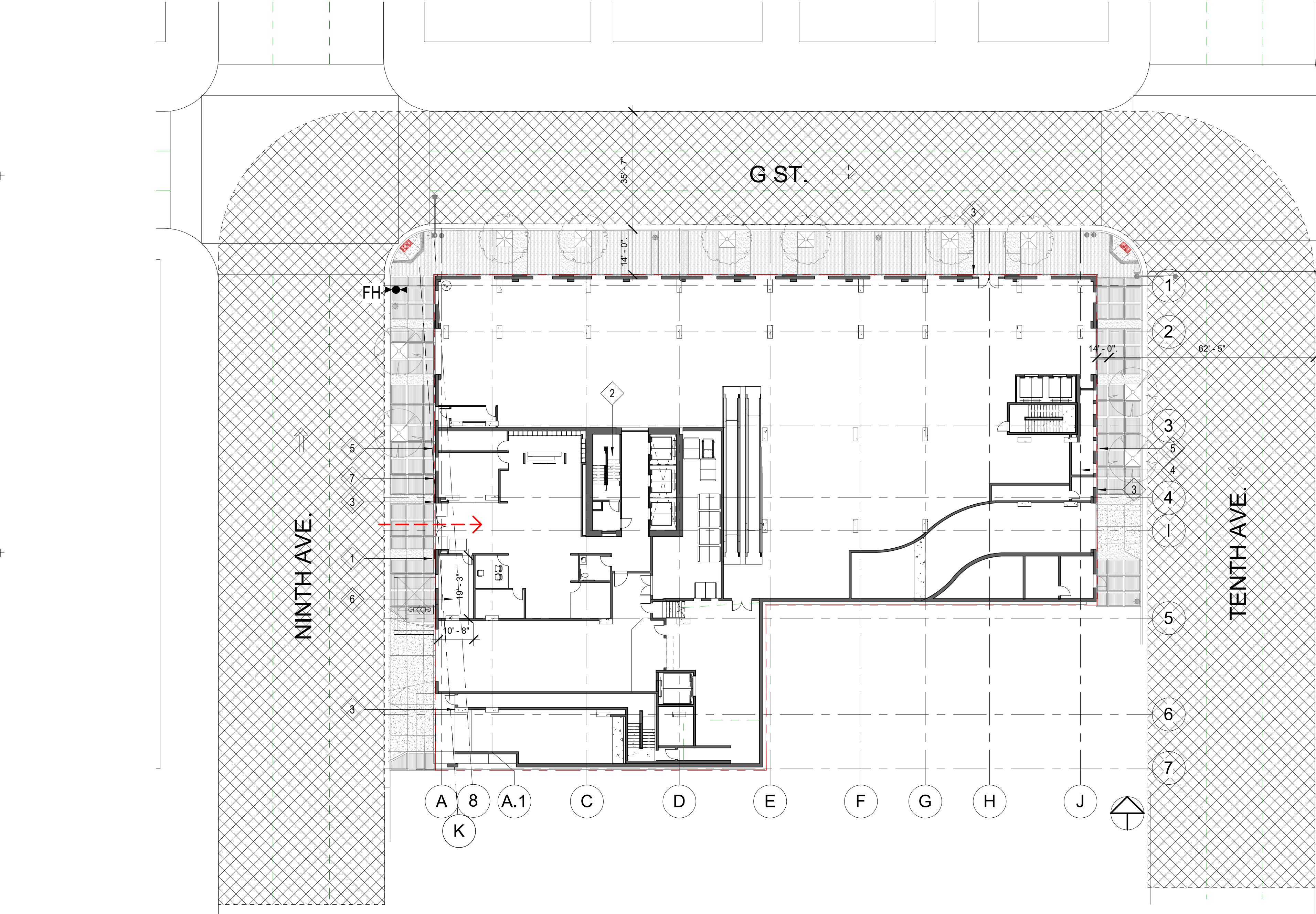
Site Plan

1" = 20'-0"

7.20.20

6

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1. PROVIDE BUILDING ADDRESS NUMBERS, VISIBLE AND LEGIBLE FROM THE STREET OR ROAD FRONTING THE PROPERTY PER SAN DIEGO MUNICIPAL CODE SECTION 95.0209.
2. POST INDICATOR VALVES, FIRE DEPARTMENT CONNECTIONS, AND ALARM BELL ARE TO BE LOCATED ON THE ADDRESS/ACCESS SIDE OF THE STRUCTURE.
3. CBC SEC. 3002.4A – GENERAL STRETCHER REQUIREMENTS – ALL BUILDINGS AND STRUCTURES WITH ONE OR MORE PASSENGER SERVICE ELEVATORS SHALL BE PROVIDED WITH NOT LESS THAN ONE MEDICAL EMERGENCY SERVICE ELEVATOR TO ALL LANDINGS MEETING THE PROVISIONS OF SECTION 3002.4A
4. THE REQUIRED WIDTH OF ACCESS ROADWAYS SHALL NOT BE OBSTRUCTED IN ANY MANNER, INCLUDING THE PARKING OF VEHICLES. WHERE NO SPACE IS PROVIDED FOR PARKING ALONG ACCESS ROADWAYS, THEY SHALL BE KEPT CLEAR BY THE POSTING OF SIGNS OR THE PAINTING OF CURBS PER POLICY A-14-1
5. STAIRWAYS EXITING DIRECTLY TO THE EXTERIOR OF A BUILDING FOUR OR MORE STORIES IN HEIGHT SHALL BE PROVIDED WITH A MEANS FOR EMERGENCY ENTRY FOR FIRE DEPARTMENT ACCESS. DOORS AT THESE LOCATIONS SHALL ALSO COMPLY WITH THIS REQUIREMENT.
6. CFC 504.3 – NEW BUILDINGS FOUR OR MORE STORIES ABOVE GRADE PLANE, EXCEPT THOSE WITH A ROOF SLOPE GREATER THAN FOUR UNITS VERTICAL IN 12 UNITS HORIZONTAL SHALL BE PROVIDED WITH A STAIRWAY TO THE ROOF.
7. EVERY BUILDING FOUR STORIES OR MORE IN HEIGHT SHALL BE PROVIDED WITH NOT LESS THAN ONE STANDPIPE FOR USE DURING CONSTRUCTION INSTALLED IN ACCORDANCE WITH CFC 3313.1. STANDPIPE SHALL BE INSTALLED WHEN PROGRESS OF CONSTRUCTION IS NOT MORE THAN 40 FEET IN HEIGHT ABOVE THE LOWEST LEVEL OF FIRE DEPARTMENT ACCESS. CFC 3313.1
8. VEGETATION SHALL BE SELECTED AND MAINTAINED IN SUCH MANNER AS TO ALLOW IMMEDIATE ACCESS TO ALL HYDRANTS, VALVES, FIRE DEPARTMENT CONNECTIONS, PULL STATIONS, EXTINGUISHERS, SPRINKLER RISERS, ALARM CONTROL PANELS, RESCUE WINDOWS, AND OTHER DEVICES OR AREAS USED FOR FIRE FIGHTING PURPOSES. VEGETATION OF BUILDING FEATURES SHALL NOT OBSTRUCT ADDRESS NUMBERS OR INHIBIT THE FUNCTIONING OF ALARM BELLS, HORNS OR STROBES.
9. DECORATIVE MATERIALS SHALL BE MAINTAINED IN A FLAME RETARDANT CONDITION. CFC SEC. 804
10. ALL BUILDINGS AND SITES UNDERGOING CONSTRUCTION, ALTERATION, OR DEMOLITION SHALL COMPLY WITH THE REQUIREMENTS OF CHAPTER 33 OF THE CFC
11. FIRE APPARATUS ACCESS ROADS AND WATER SUPPLIES FOR FIRE PROTECTION, SHALL BE INSTALLED AND MADE SERVICEABLE PRIOR TO AND DURING TIME OF CONSTRUCTION CFC 501.4
12. FIRE COMMAND CENTER SHALL COMPLY WITH NFPA 72 AND SHALL CONTAIN THE FEATURES LISTED IN CFC SECTION 508
13. POST INDICATOR VALVES, FIRE DEPARTMENT CONNECTIONS, AND ALARM BELL ARE TO BE LOCATED ON THE ADDRESS/ACCESS SIDE OF THE STRUCTURE. 912.2.1
14. SAN DIEGO MUNICIPAL CODE SECTION 55.507 ITEM (C) HYDRANT LOCATIONS SHALL BE IDENTIFIED BY THE INSTALLATION OF REFLECTIVE BLUE COLORED MARKERS. SUCH MARKERS SHALL BE AFFIXED TO THE ROADWAY SURFACE, APPROXIMATELY CENTERED BETWEEN CURBS, AND AT A RIGHT ANGLE TO THE HYDRANT.
15. EXTERIOR DOORS AND OPENINGS REQUIRED BY THIS CODE OR THE CBC SHALL BE MAINTAINED READILY ACCESSIBLE FOR EMERGENCY ACCESS BY THE FIRE DEPARTMENT. AN APPROVED ACCESS WALKWAY LEADING FROM FIRE APPARATUS ACCESS ROADS TO EXTERIOR OPENINGS SHALL BE PROVIDED WHEN REQUIRED BY THE FIRE CODE OFFICIAL.
16. EMERGENCY SYSTEMS CONFORMING WITH CFC SECTION 604 AND THE CALIFORNIA ELECTRICAL CODE SHALL BE PROVIDED.
17. CFC 105.4.4 CONSTRUCTION DOCUMENTS APPROVED BY THE FIRE CODE OFFICIAL ARE APPROVED WITH THE INTENT THAT SUCH CONSTRUCTION DOCUMENTS COMPLY IN ALL RESPECTS WITH THE CFC. REVIEW AND APPROVAL BY THE FIRE CODE OFFICIAL SHALL NOT RELIEVE THE APPLICANT OF THE RESPONSIBILITY OF COMPLIANCE WITH THIS CODE.
18. ROOMS CONTAINING CONTROLS FOR A/C SYSTEMS, SPRINKLER RISERS AND VALVES, OR OTHER FIRE DETECTION, SUPPRESSION OR CONTROL ELEMENTS SHALL BE IDENTIFIED FOR THE USE OF THE FIRE DEPARTMENT. APPROVED SIGNS REQUIRED TO IDENTIFY FIRE PROTECTION EQUIPMENT AND EQUIPMENT LOCATION SHALL BE CONSTRUCTED OF DURABLE MATERIALS, PERMANENTLY INSTALLED AND READILY VISIBLE.
19. FD POLICY 10-09 - HIGH RISE BUILDINGS FDC' SHALL HAVE FOUR 2 1/2 INCH INLETS. HIGH RISE BUILDINGS SHALL HAVE TWO REMOTELY LOCATED FDC'S FOR EACH ZONE. HIGH RISE AND OTHER BUILDINGS EQUIPPED WITH HOSE VALVES OF THE PRESSURE REGULATING TYPE (PRV'S) SHALL PROVIDE A SIGN INDICATING THE MINIMUM PRESSURE THE FIRE APPARATUS REQUIRED TO PUMP TO PUMP INTO THE FDC. (EXAMPLE: FD MINIMUM PUMP PRESSURE 225 PSI). A WEATHER-RESISTANT SIGN SECURED WITH A CORROSION RESISTANT CHAIN OR FASTENER SHALL INDICATE THE ADDRESS, PORTION OF THE BUILDING SERVED. SEE NFPA 14 - 6.4.6 FOR HEIGHT REQUIREMENTS.
20. A "DISCRETIONARY" PLAN REVIEW IS "CONCEPTUAL" BY DEFINITION, AND AS SUCH DOES NOT CONSTITUTE AN APPROVAL FOR FIRE ACCESS. IT SHALL THEREFORE BE INCUMBENT OF THE APPLICANT TO ENSURE THAT A FIRE PLAN REVIEW CYCLE IS PROVIDED DURING THE "MINISTERIAL" REVIEW. ALSO, AN "EXHIBIT A" PACKAGE WITH OR WITHOUT A "FIRE ACCESS" PLAN DOES NOT CONSTITUTE AN APPROVED FAP FOR ISSUANCE OF CONSTRUCTION/BUILDING PERMIT.
21. FIRE APPARATUS ACCESS ROADS SHALL BE DESIGNED AND MAINTAINED TO SUPPORT THE IMPOSED LOADS OF FIRE APPARATUS AND SHALL BE SURFACED SO AS TO PROVIDE ALL WEATHER DRIVING CAPABILITIES.

22. AERIAL FIRE ACCESS ROAD(S) ADJACENT TO BUILDINGS THAT ARE GREATER THAN 30 FEET IN HEIGHT FROM GRADE PLANE, SHALL HAVE A MINIMUM WIDTH OF 26 FEET. THE PROXIMAL EDGE OF AERIAL FIRE ACCESS SHALL BE A MINIMUM OF 15-30 FEET FROM THE BUILDING FACADE(S) AND/OR PLUMB LINE OF EAVE(S). AERIAL ACCESS SHALL BE PROVIDED ALONG ONE ENTIRE LONG SIDE(S) OF THE BUILDING(S).
23. ALL REQUIRED HOSE PULLS ARE SHOWN TO REACH ALL PORTIONS OF THE EXTERIOR OF THE BUILDING(S) PER POLICY A-14-1. HOSE PULL IS MEASURED FROM THE FIRE APPARATUS (ENGINE) WHEN THE FIRE ENGINE IS IN A FIRE ACCESS ROAD/LANE. HOSE PULL CAN BE MEASURED FROM MULTIPLE LOCATIONS WITHIN THE ACCESS ROAD/LANE. THE HOSE PULLS MUST CONNECT OR OVERLAP TO SHOW COMPLETE COVERAGE. FOR SPRINKLERED BUILDING(S); THE MAXIMUM HOSE PULL IS 200'. FOR NON-SPRINKLERED BUILDING(S); THE MAXIMUM HOSE PULL IS 150'. CHANGE IN VERTICAL ELEVATION MUST ALSO BE ACCOUNTED FOR.
24. ALL EXISTING AND/OR PROPOSED FIRE HYDRANTS WITHIN 600 FEET OF THE PROJECT SITE AND A 300 FEET RADIUS OVERLAY SHALL BE SHOWN TO ENCOMPASS ALL PORTIONS OF ALL STRUCTURES AS PART OF SUBMITTED PROJECT.
25. ALL RED CURB/NO PARKING SIGN AREAS HAVE BEEN SHOWN WITH A KEY INDICATOR. ALL REQUIRED ACCESS ROADWAYS SHALL NOT PROVIDE LESS THAN THE REQUIRED/APPROVED WIDTH AND/OR BE OBSTRUCTED IN ANY MANNER, INCLUDING THE PARKING OF VEHICLES. WHERE INADEQUATE WIDTH HAS NOT BEEN PROVIDED FOR PARKING ALONG ACCESS ROADWAYS, THEN SUCH ACCESS SHALL BE KEPT CLEAR BY THE POSTING OF SIGNS OR THE PAINTING OF CURBS PER POLICY A-14-1.
26. THE LOCATION(S) OF AN APPROVED "KNOX" KEY BOX ARE SHOWN ON THE FAP AND FOLLOW THE SAN DIEGO FIRE DEPARTMENT FPB POLICY K-15-2. (SANDIEGO.GOV/FIRE-SERVICES-&PROGRAMS, POLICIES, KNOX BOX REQUIREMENTS).
27. MINIMUM ROOM SIZE FOR FIRE COMMAND CENTER IS 200 SQUARE FEET IN AREA WITH A MINIMUM DIMENSION OF 10 FEET.
28. AT LEAST ONE FIRE EXTINGUISHER WITH A MINIMUM RATING OF 2-A-10-BC SHALL BE PROVIDED WITHIN 75 FEET MAXIMUM TRAVEL DISTANCE FOR EACH 6,000 SQUARE FEET OR PORTION THEREOF ON EACH FLOOR CFC SEC 906.
29. STRUCTURES UNDER CONSTRUCTION, ALTERATION OR DEMOLITION SHALL BE PROVIDED WITH NOT LESS THAN ONE APPROVED PORTABLE FIRE EXTINGUISHER IN ACCORDANCE WITH SECTION 906 AND SIZED FOR NOT LESS THAN ORDINARY HAZARD (2A10BC) AS FOLLOWS:
 1. AT EACH STAIRWAY ON ALL FLOOR LEVELS WHERE COMBUSTIBLE MATERIALS HAVE ACCUMULATED.
 2. IN EVERY STORAGE AND CONSTRUCTION SHED
 3. ADDITIONAL PORTABLE FIRE EXTINGUISHERS SHALL BE PROVIDED WHERE SPECIAL HAZARDS EXIST, INCLUDING, BUT NOT LIMITED TO, THE STORAGE AND USE OF FLAMMABLE AND COMBUSTIBLE LIQUIDS. CFC 3315.
30. PROVIDE STAIRWAY IDENTIFICATION SIGNS PER CFC 3315.
31. A CLASS I (OR I AND I OR II) STANDPIPE OUTLET CONNECTION IS REQUIRED IN OCC. OF 4 OR MORE STORIES AT EVERY FLOOR LEVEL CONNECTION OF EVERY REQUIRED STAIRWAY ABOVE OR BELOW GRADE. OUTLETS AT STAIRWAYS SHALL BE LOCATED WITHIN THE EXIT ENCLOSURE OR, IN THE CASE OF PRESSURIZED ENCLOSURES, WITHIN THE VESTIBULE OR EXTERIOR BALCONY, GIVING ACCESS TO THE STAIRWAY. THERE SHALL BE AT LEAST ONE OULET ABOVE THE ROOF LINE WHEN THE ROOF HAS A SLOPE OF LESS THAN 4/12 UNITS HORIZONTAL. IN BUILDINGS WHERE MORE THAN ONE STANDPIPE IS PROVIDED, THE STANDPIPES SHALL BE INTERCONNECTED CFC 905.
32. FIRE PROTECTION EQUIPMENT SHALL BE IDENTIFIED IN AN APPROVED MANNER. ROOMS CONTAINING CONTROLS FOR A/C SYSTEMS, SPRINKLER RISERS AND VALVES, OR OTHER FIRE DETECTION, SUPPRESSION OR CONTROL ELEMENTS SHALL BE IDENTIFIED FOR THE USE OF THE FIRE DEPARTMENT. APPROVED SIGNS REQUIRED TO IDENTIFY FIRE
33. FUEL TANKS (TO INCLUDE BELLY TANKS OF GENERATOR EQUIPMENT) REQUIRE PLAN CHECK AND APPROVAL FROM THE TECHNICAL SERVICES SECTION OF THE FIRE DEPARTMENT. TO OBTAIN A BUILDING PERMIT / TA NUMBER OR REQUEST A REVIEW FOR PERMIT, CALL (619) 533-4477 TO SCHEDULE AN APPOINTMENT. BUILDING FINAL WILL NOT BE APPROVED UNTIL THE TANK PERMIT HAS BEEN APPROVED.

3rd SUBMITTAL
6.9.20

ISSUES:

PROJECT NO:
5937.00
FILE NAME:
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-RESEARCH_Culture05_2018.3.rvt
DRAWN BY: Author
CHECKED BY: Checker
PLOT DATE:
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TITLE:

Fire Access Plan

7.20.20

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SITE ACCESSIBILITY PLAN LEGEND

ACCESSIBLE PATH OF TRAVEL PER CBC 11B-403, 405
PATH OF TRAVEL (POT) AS INDICATED IS A BARRIER
FREE ACCESS ROUTE WITHOUT ANY ABRUPT
VERTICAL CHANGES EXCEEDING 1/2" BEVELED AT
1:2 MAXIMUM SLOPE, EXCEPT THAT VERTICAL
CHANGES DO NOT EXCEED 1/4" VERTICAL AND IS
AT LEAST 48" WIDE. SURFACE IS SLIP RESISTANT
(MEDIUM BROOM FINISH WITH AMPLITUDE OF 1/16"
TO 1/32", STABLE, FIRM, AND SMOOTH. CROSS
SLOPE DOES NOT EXCEED 2% AND SLOPE IN THE
DIRECTION OF TRAVEL IS NO MORE THAN 5%
UNLESS OTHERWISE INDICATED, POT SHALL BE
MAINTAINED FREE OF OVERHANGING
OBSTRUCTIONS TO 80" MINIMUM AND PROTRUDING
OBJECTS GREATER THAN 4" PROJECTION FROM A
WALL AND ABOVE 27" AND LESS THAN 80",
CONTRACTOR TO VERIFY THAT THERE ARE NO
BARRIERS IN THE POT, AND THE POT COMPLIES
WITH CBC 11B-403, 405.

3rd SUBMITTAL
6.9.20

ISSUES:

PROJECT NO:
5937.00

FILE NAME:
BM 360-16207-00-Cisterra
-SGARCH -Cisterra9G_2018.3.rvt

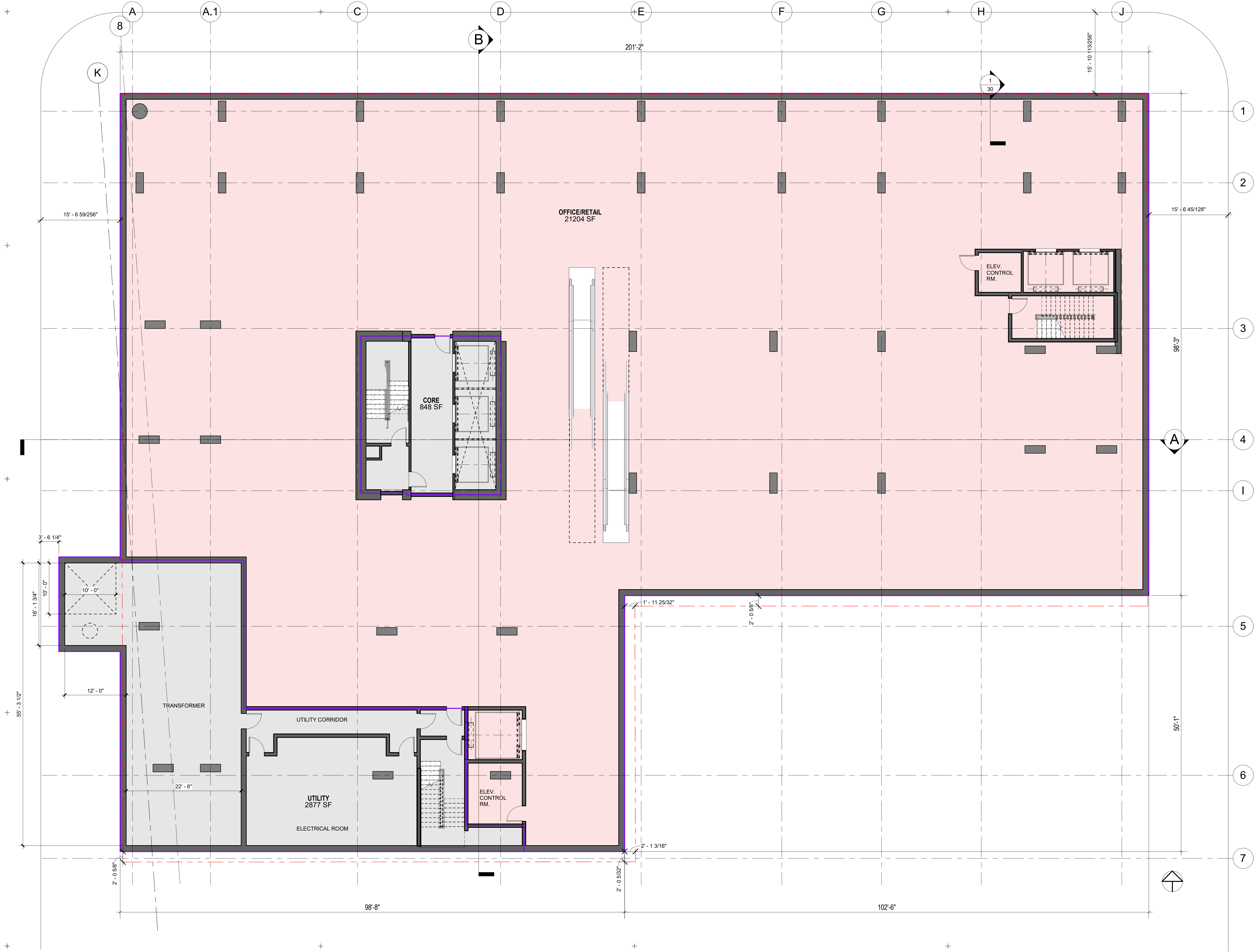
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CHECKED BY: Checker

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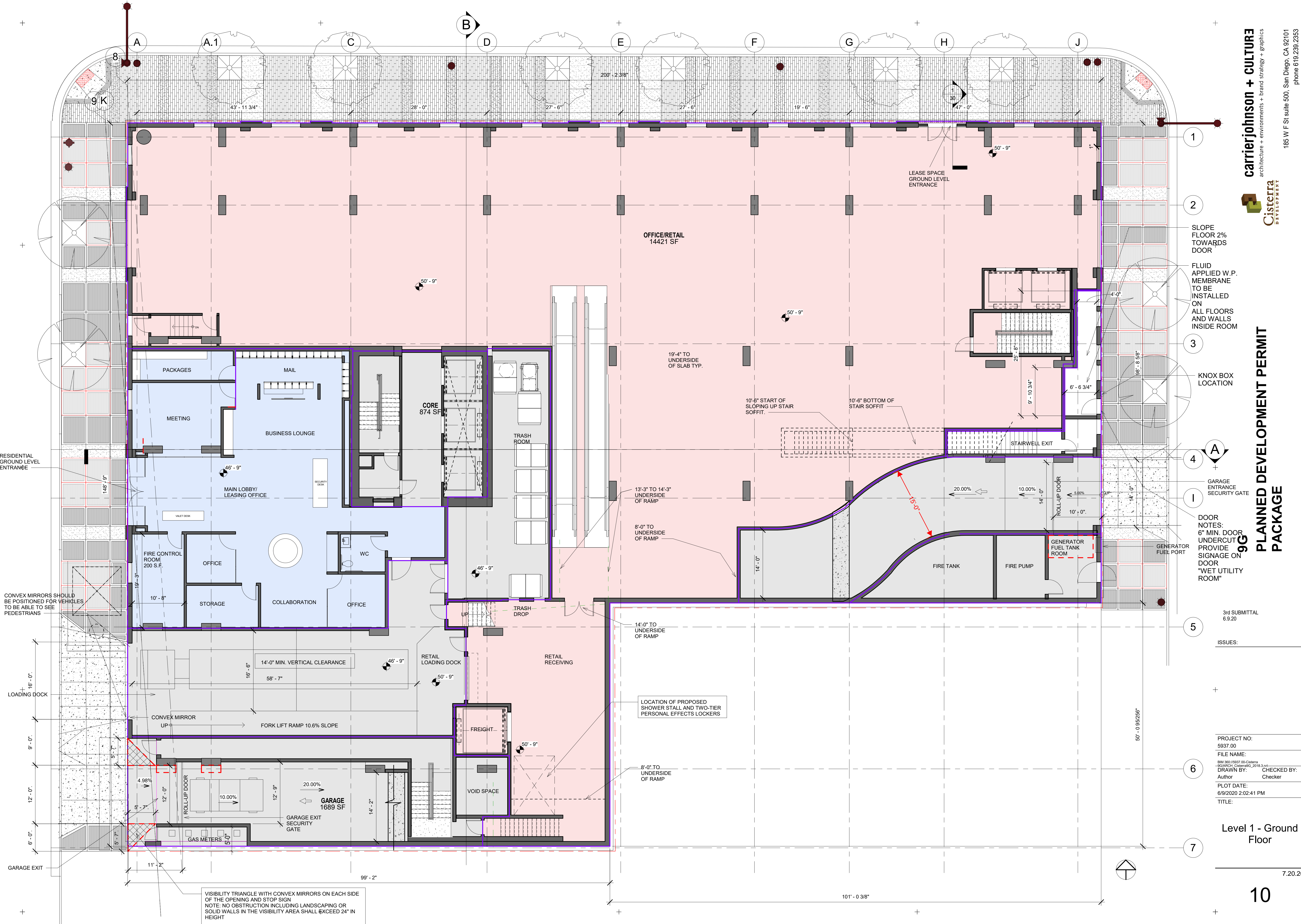
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Site Accessibility

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99G
DOOR NOTES:
6" MIN. DOOR
UNDERCUT
PROVIDE
SIGNAGE ON
DOOR
"WET UTILITY
ROOM"

3rd SUBMITTAL
6.9.20

ISSUES:

PROJECT NO:
5937.00
FILE NAME:
BIM 300/5537 00-Carter
-20200324_Carter-05_2018.3.rvt
DRAWN BY: Author
CHECKED BY: Checker
PLOT DATE:
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TITLE:

Level 1 - Ground
Floor

7.20.20

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FLOOR PLAN NOTES

- 1 DESIGNATED STALL FOR LOW-EMITTING, FUEL EFFICIENT, AND CARPOOL / VAN VEHICLES
- 2 ELECTRIC VEHICLE CHARGING SPACE:
TO HAVE THE NECESSARY VEHICLE SUPPLY EQUIPMENT INSTALLED UPON BUILD-OUT TO
PROVIDE ELECTRIC VEHICLE CHARGING STATION READY TO USE
- 3 FUTURE ELECTRIC VEHICLE CHARGING SPACE:
WITH A LISTED CABINET BOX, OR ENCLOSURE CONNECTED TO A CONDUIT LINKING THE
PARKING SPACES W/ ELECTRICAL SERVICE
- 4 ROOM FOR:
49 RESIDENTIAL BICYCLE PARKING SPACES
2 LONG TERM COMMERCIAL BICYCLE PARKING SPACES
- 5 2 SHORT TERM COMMERCIAL BICYCLE PARKING SPACES
- 6 TURN AROUND AREA - STRIPE "NO PARKING"
- 7 OVERHEAD STORAGE UNIT
- 8 STORAGE UNIT
- MC 3' - 8" MOTORCYCLE PARKING SPACE

TANDEM STALL NOTE:
BOTH TANDEM SPACES SHALL BE ASSIGNED TO THE SAME DWELLING UNIT

9G PLANNED DEVELOPMENT PERMIT PACKAGE

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7.1.20

ISSUES:

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5937.00
FILE NAME:
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TITLE:

Level 2 - Parking

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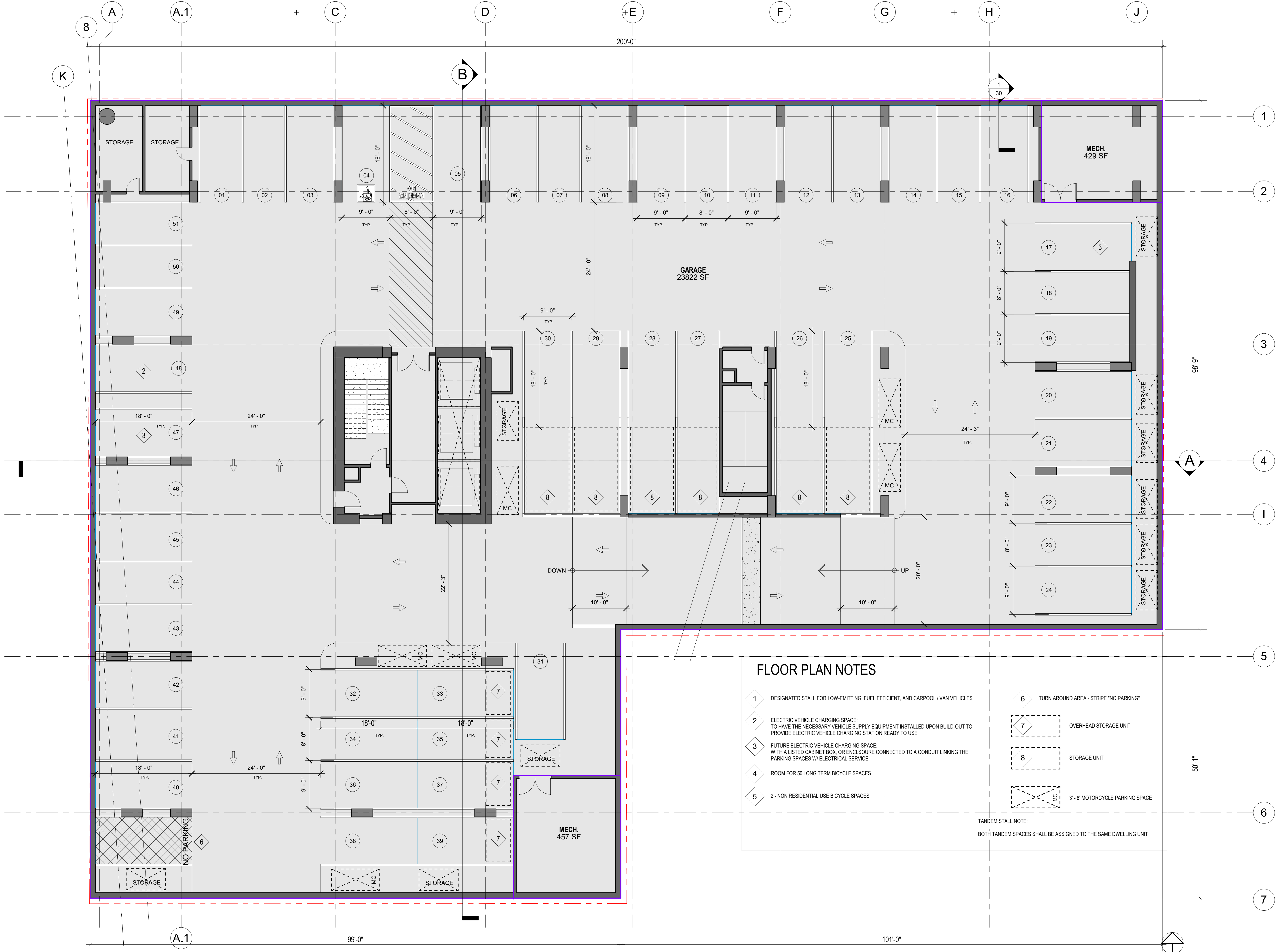
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Level 3 - Parking

12

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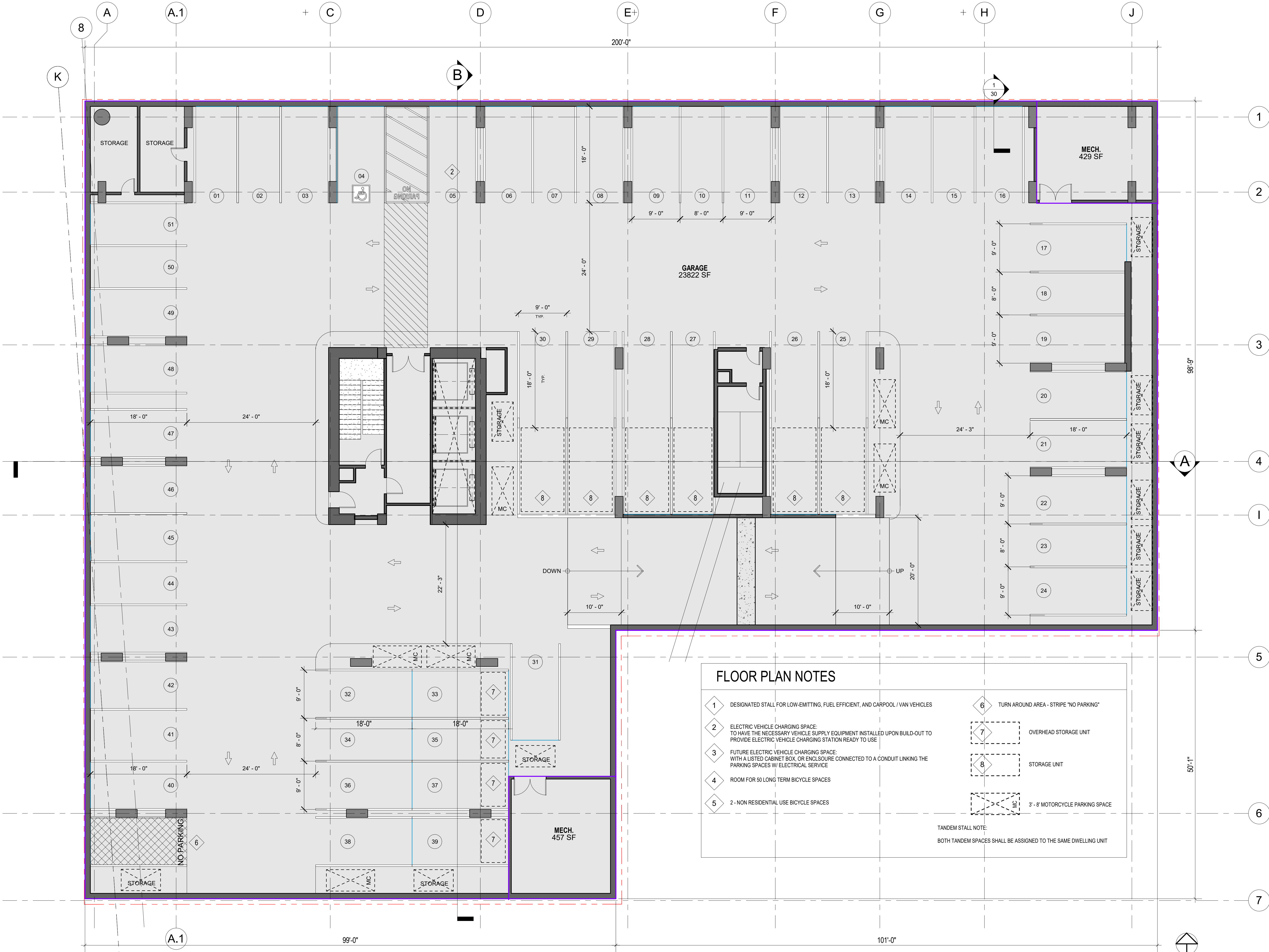
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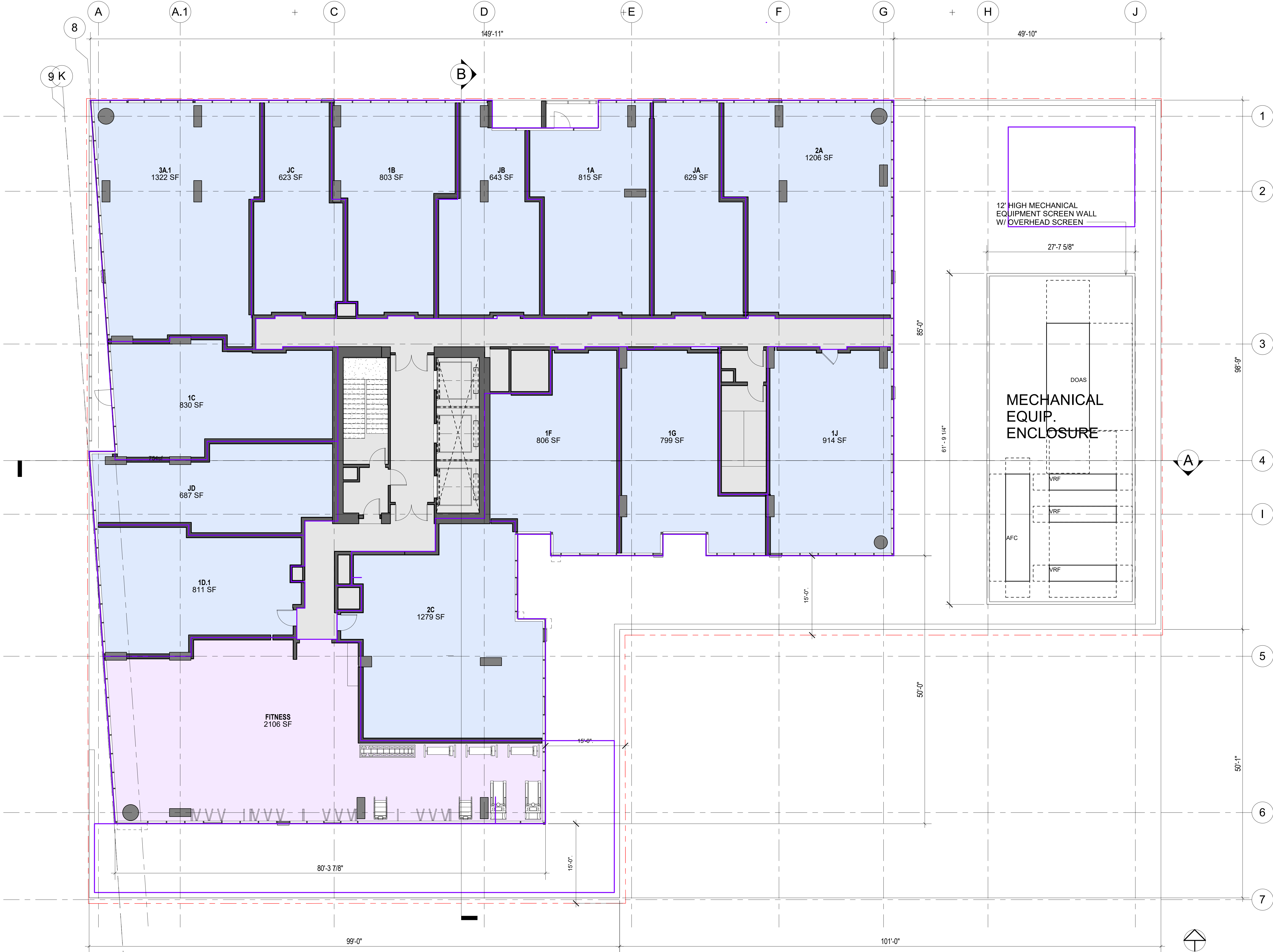
Level 4 - Parking

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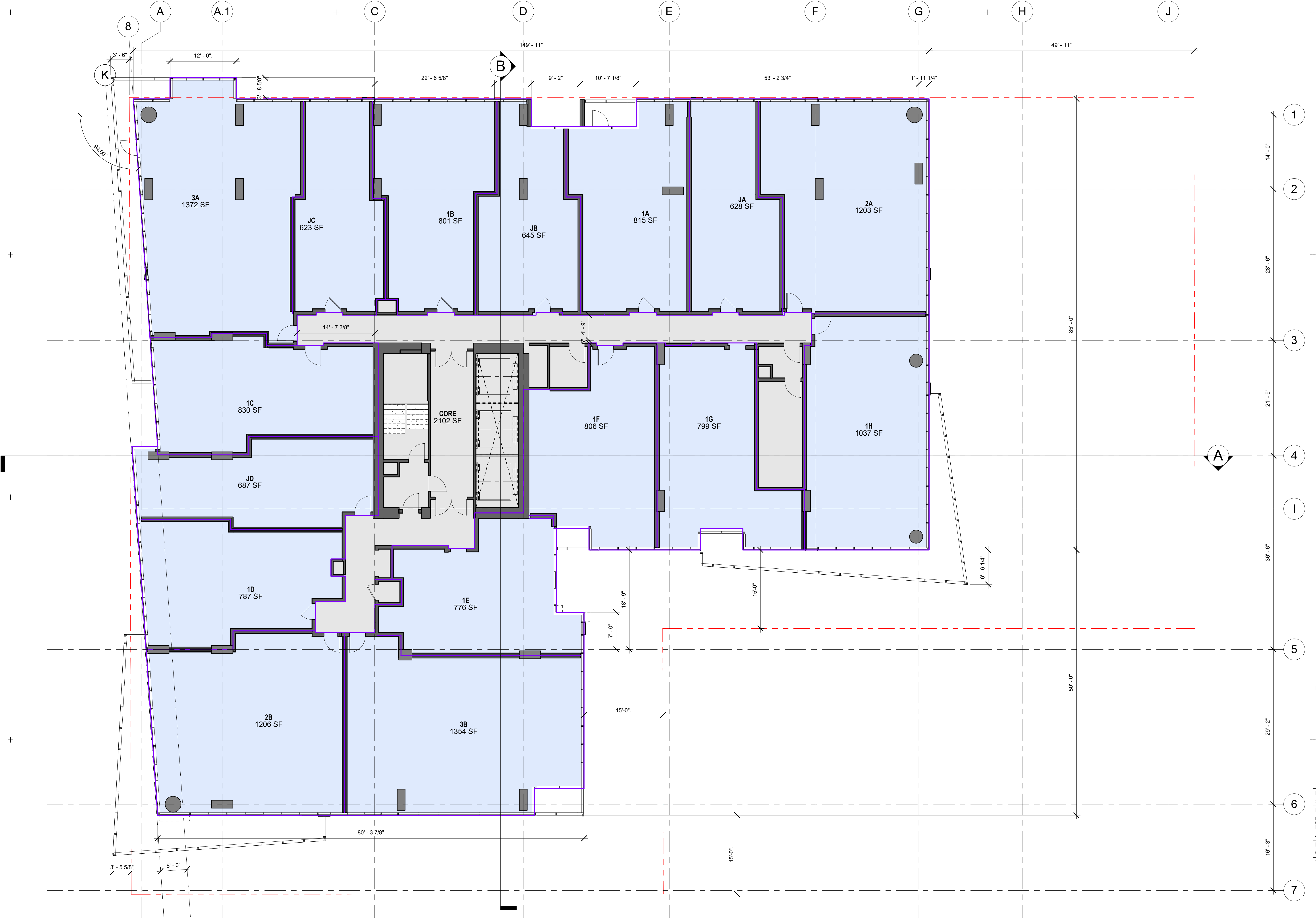


9G
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PACKAGE

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Level 7 -
Residential

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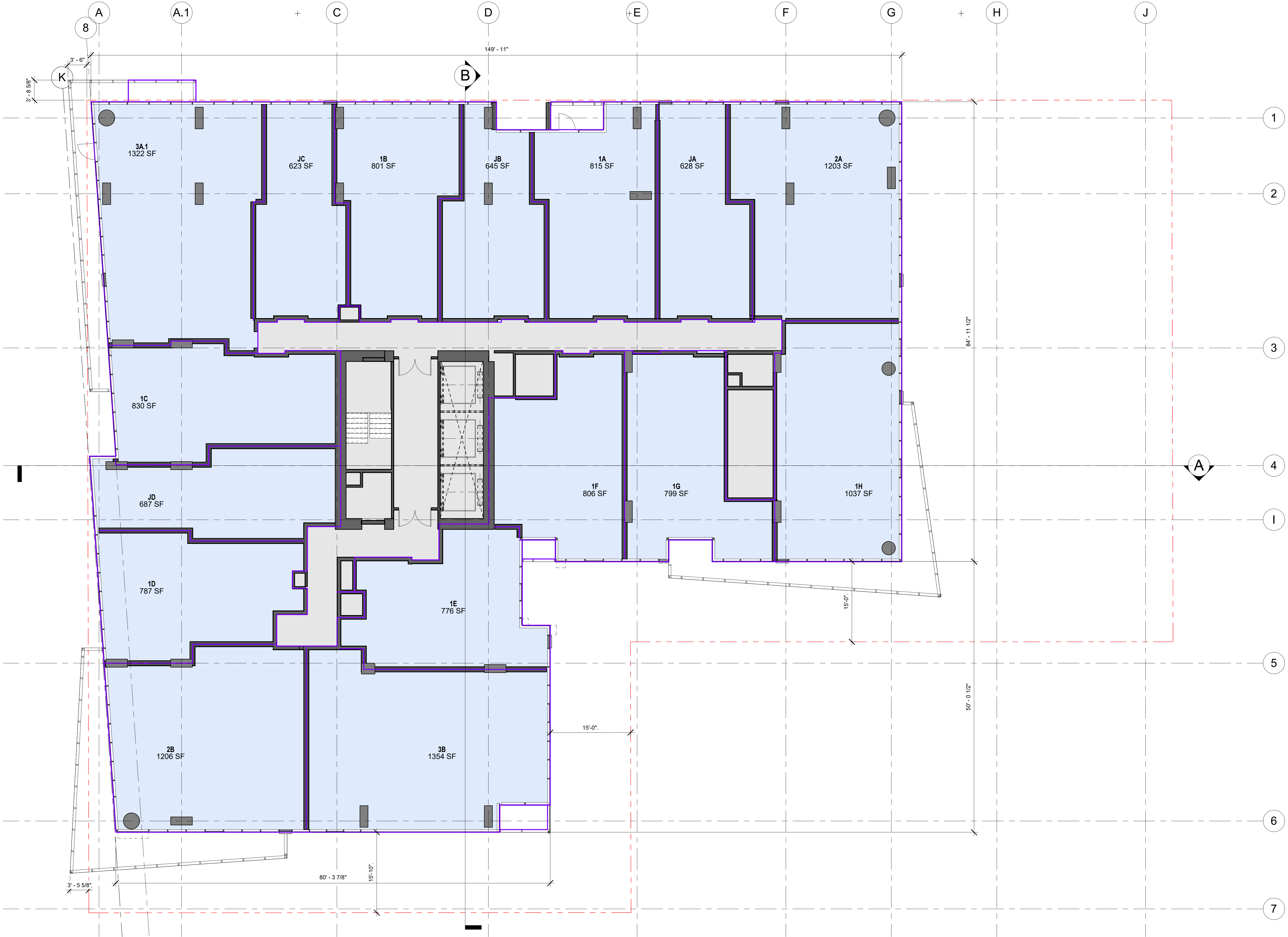


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Levels 8-19 -
Residential

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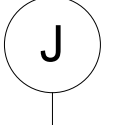
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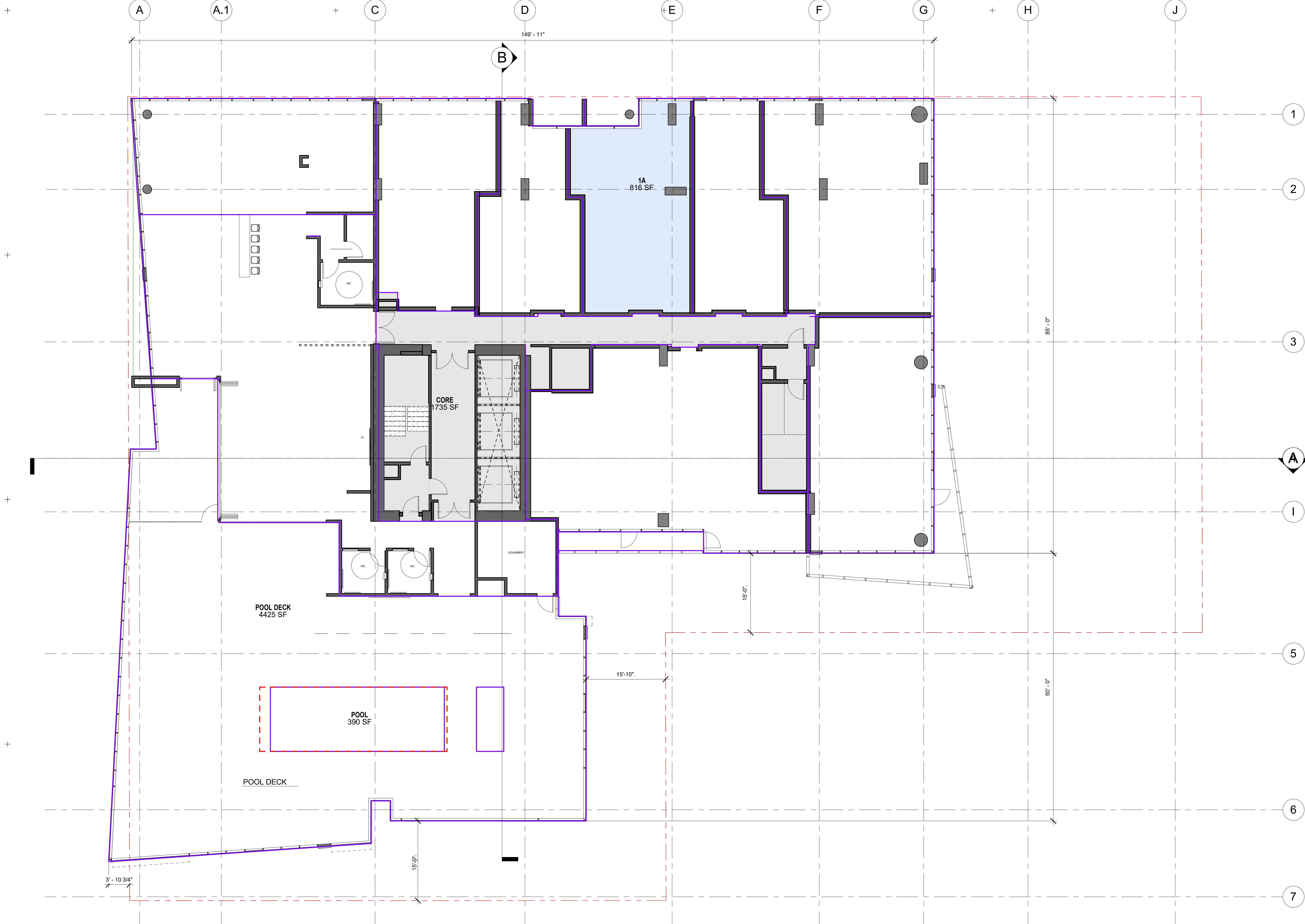
Level 20 -
Residential

+



18

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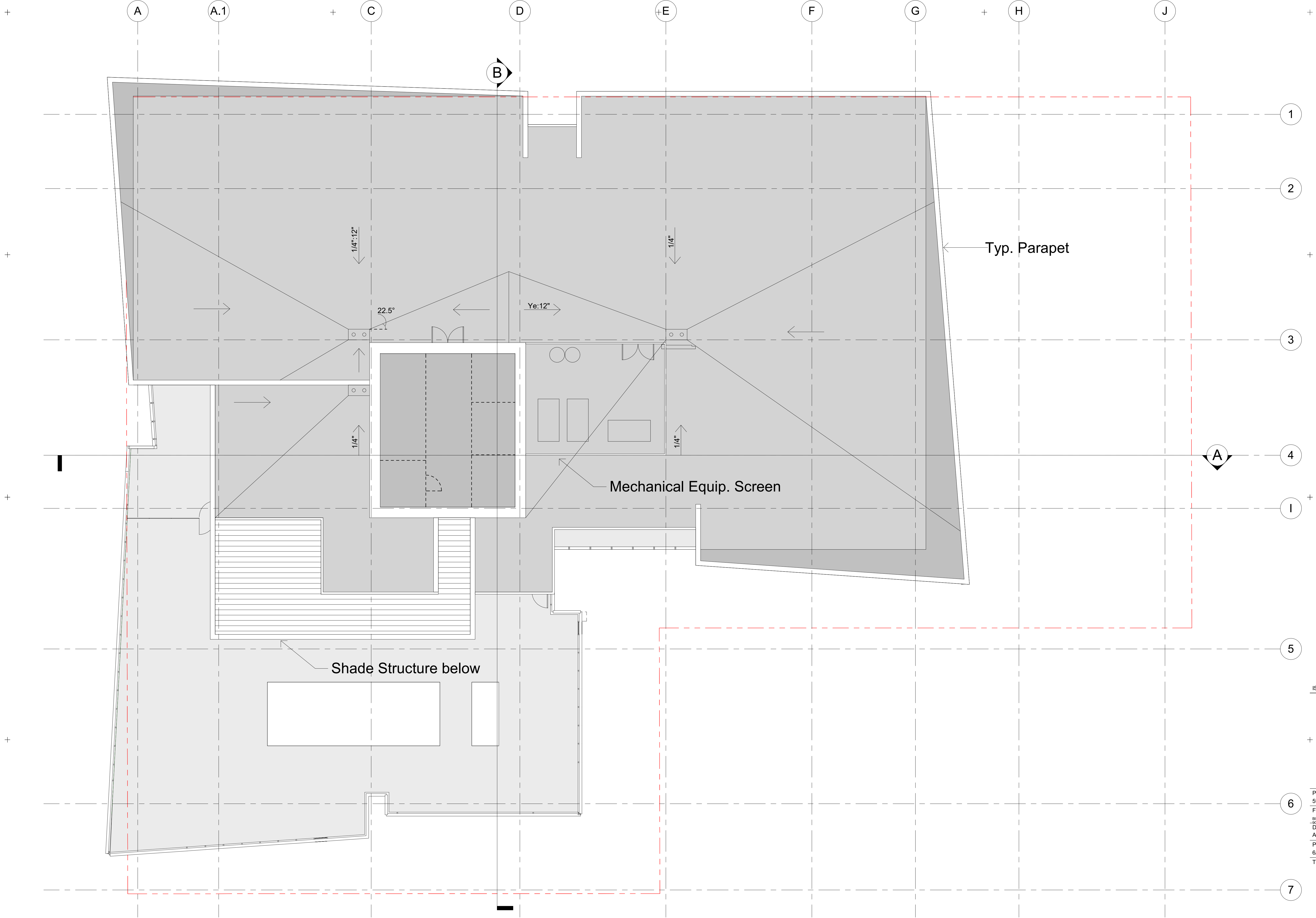
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TITLE:

Level 22 -
Amenities

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TITLE:

Roof Level

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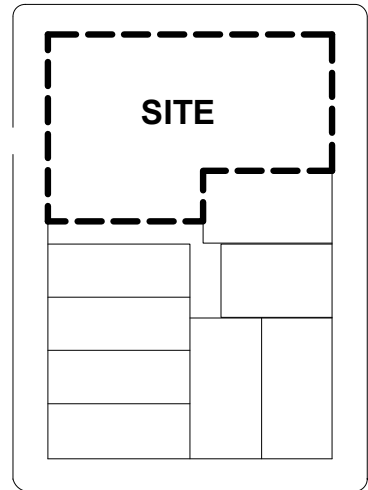
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TITLE:

Elevation - North

7.20.20

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MATERIAL SELECTION

- 1 Window Wall Glazing System
- 2 Concrete Panel
- 3 Glass Railing
- 4 Angled/Flat Metal Panel
- 5 Metal Louver mechanical
- 6 Steel Canopy
- 7 Spandrel Window Wall
- 8 Steel Canopy
- 9 Lighting Signage Element

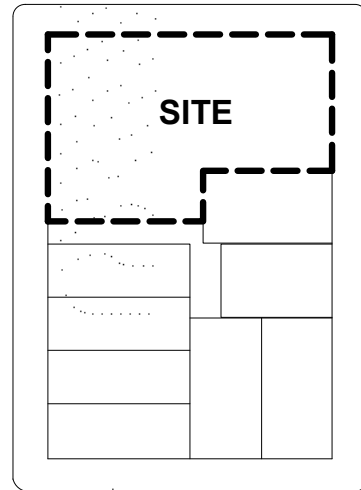
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TITLE:

Elevation - West

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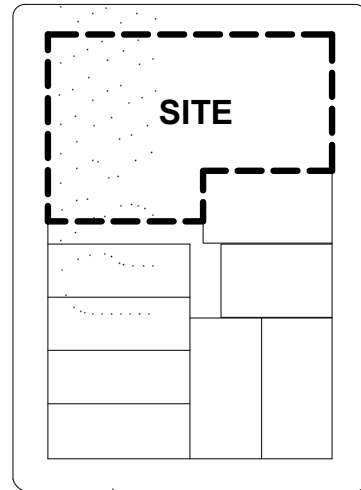
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TITLE:

Elevation - South

7.20.20

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MATERIAL SELECTION

- 1 Window Wall Glazing System
- 2 Concrete Panel
- 3 Glass Railing
- 4 Angled/flat Metal Panel
- 5 Metal Louver mechanical
- 6 Steel Canopy
- 7 Spandrel Window Wall
- 8 Steel Canopy
- 9 Lighting Signage Element

3rd SUBMITTAL
6.9.20

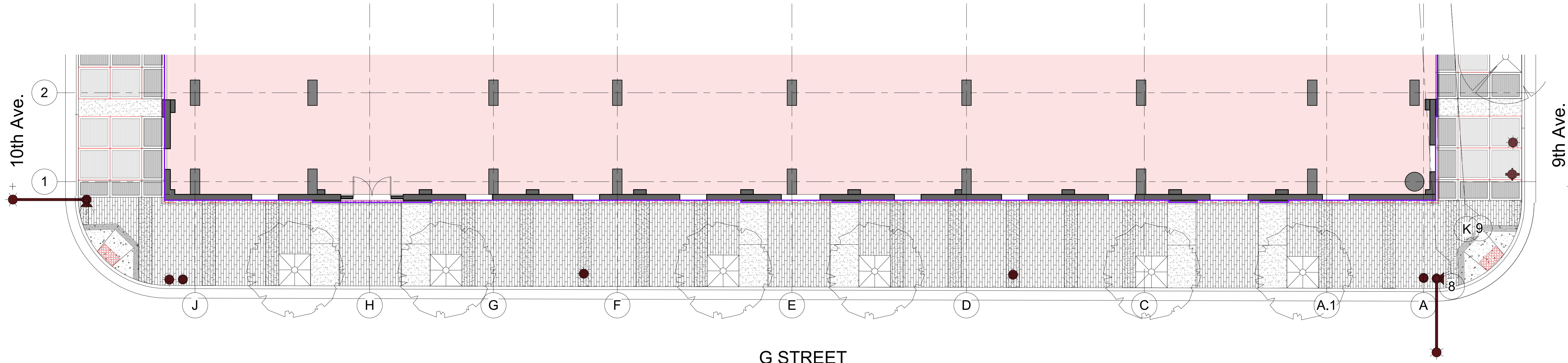
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TITLE:

Elevation - East

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MATERIAL SELECTION

- 1 Lighting Signage Element
- 2 Angled/Flat Metal Panel
- 3 Concrete Panel
- 4 Spandrel Window Wall
- 5 Precast Concrete

3rd SUBMITTAL
6.9.20

ISSUES:

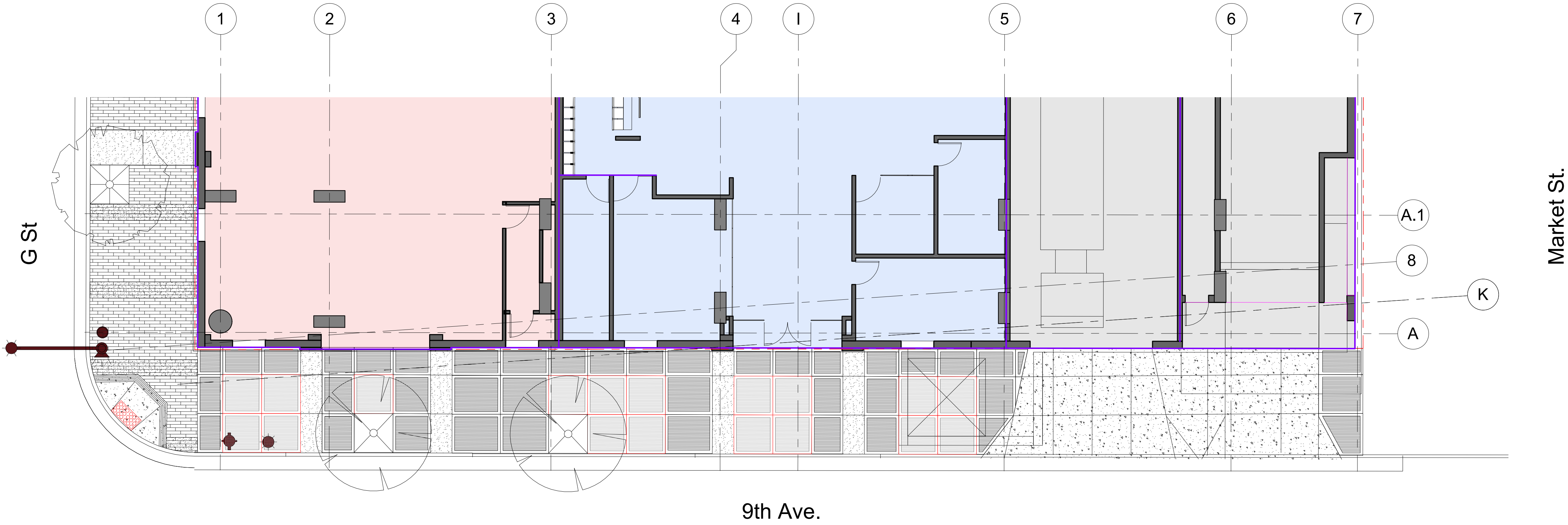
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Enlarged North
Elevation

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MATERIAL SELECTION

- 1 Lighting Signage Element
- 2 Angled/Flat Metal Panel
- 3 Concrete Panel
- 4 Spandrel Window Wall
- 5 Precast Concrete



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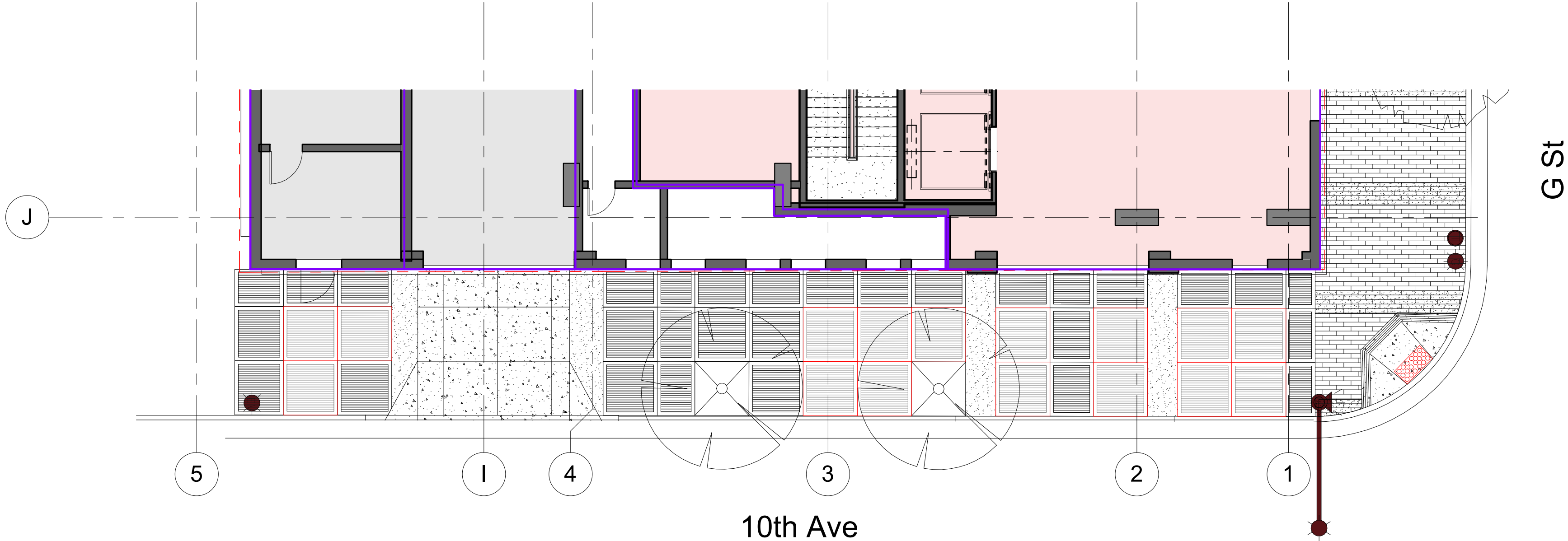
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MATERIAL SELECTION

- 1 Lighting Signage Element
- 2 Angled/Flat Metal Panel
- 3 Concrete Panel
- 4 Spandrel Window Wall
- 5 Precast Concrete



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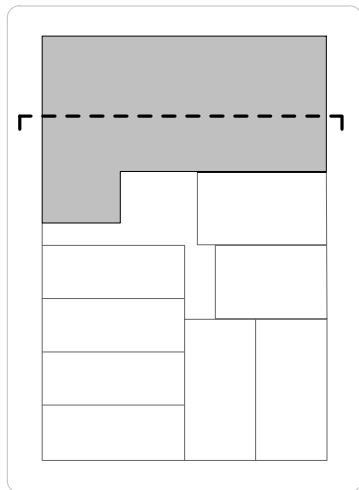
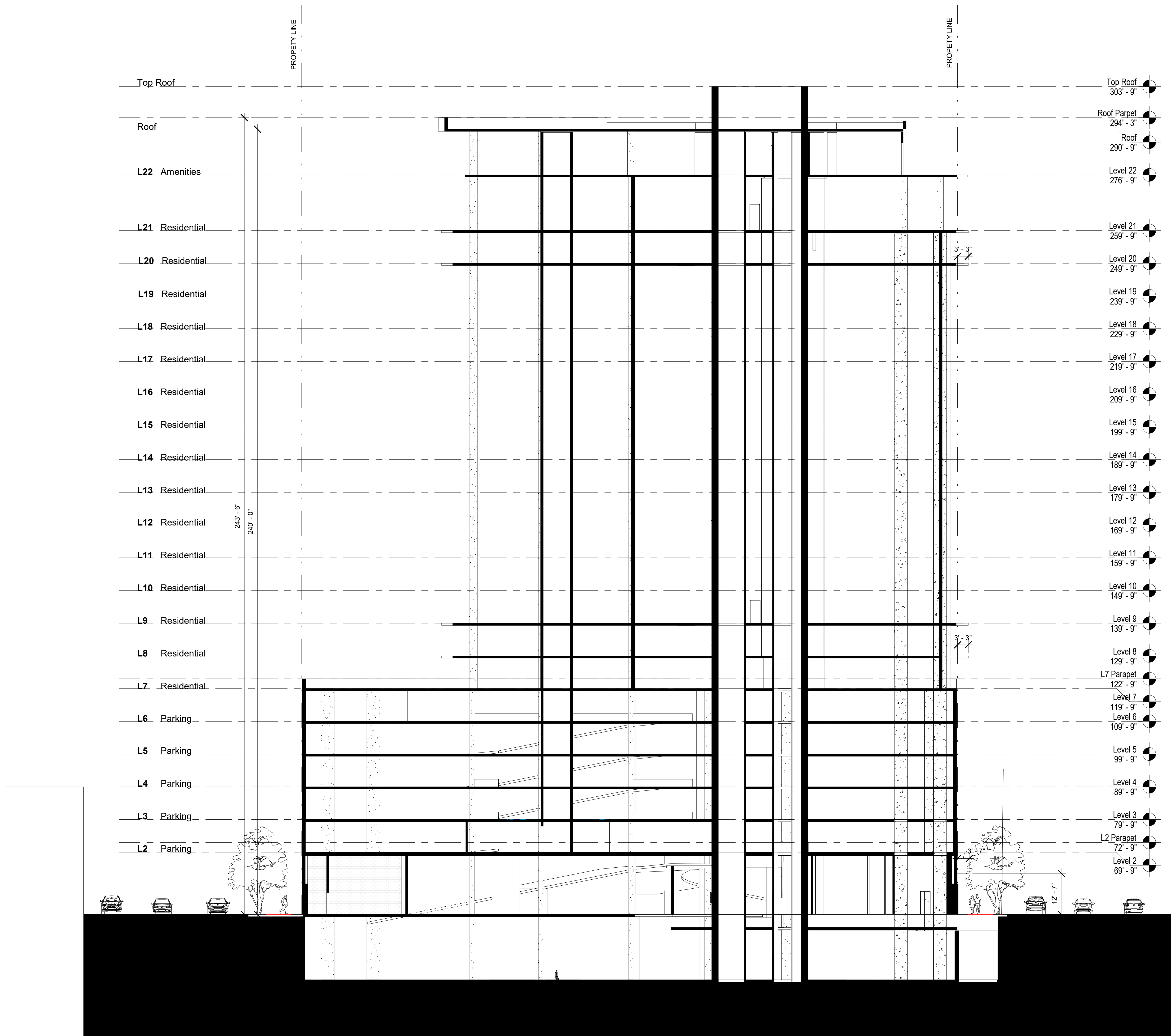
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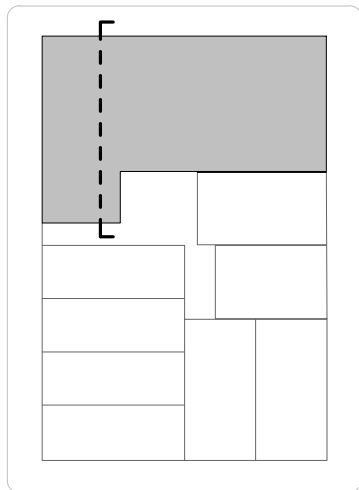
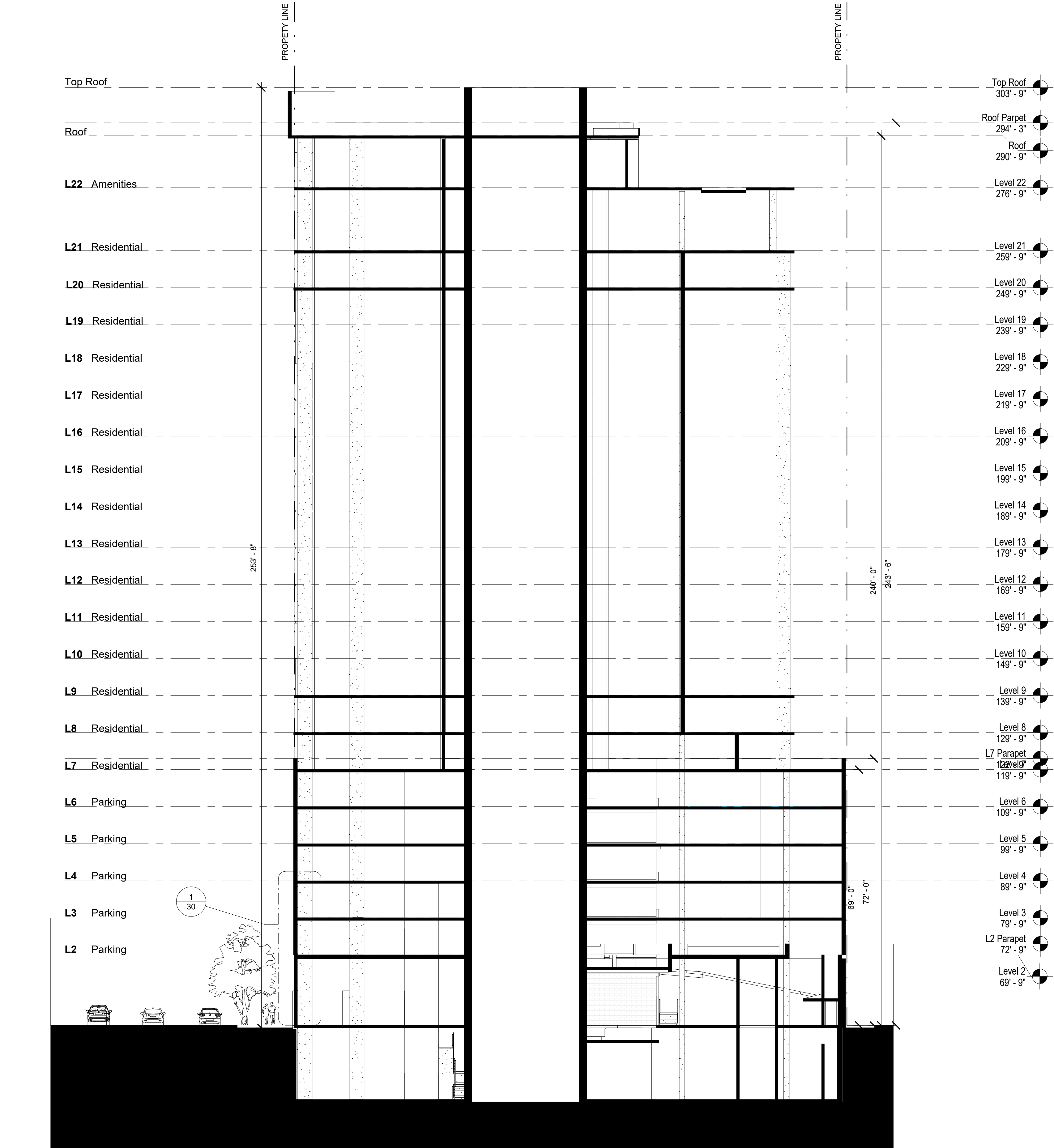
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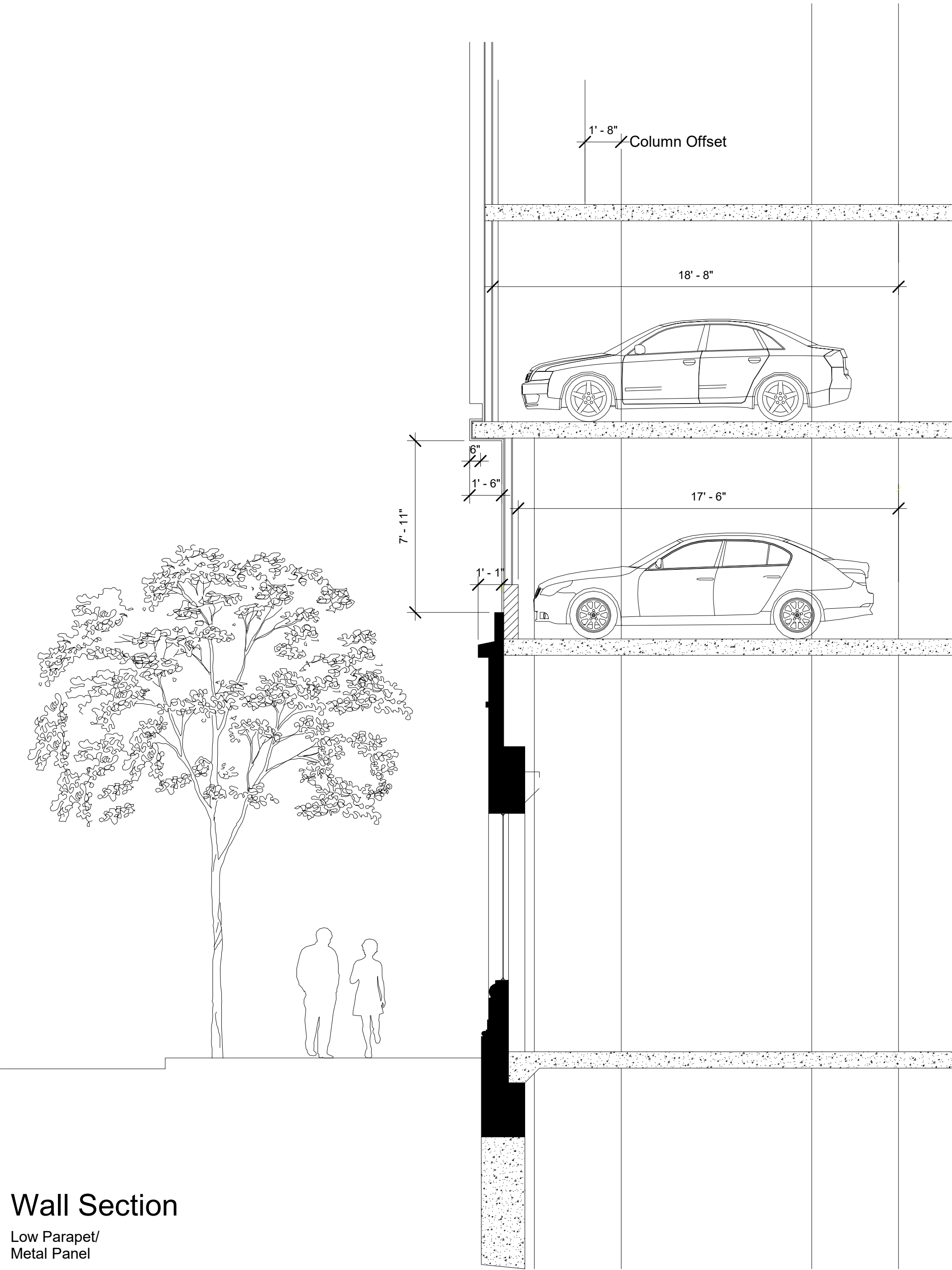
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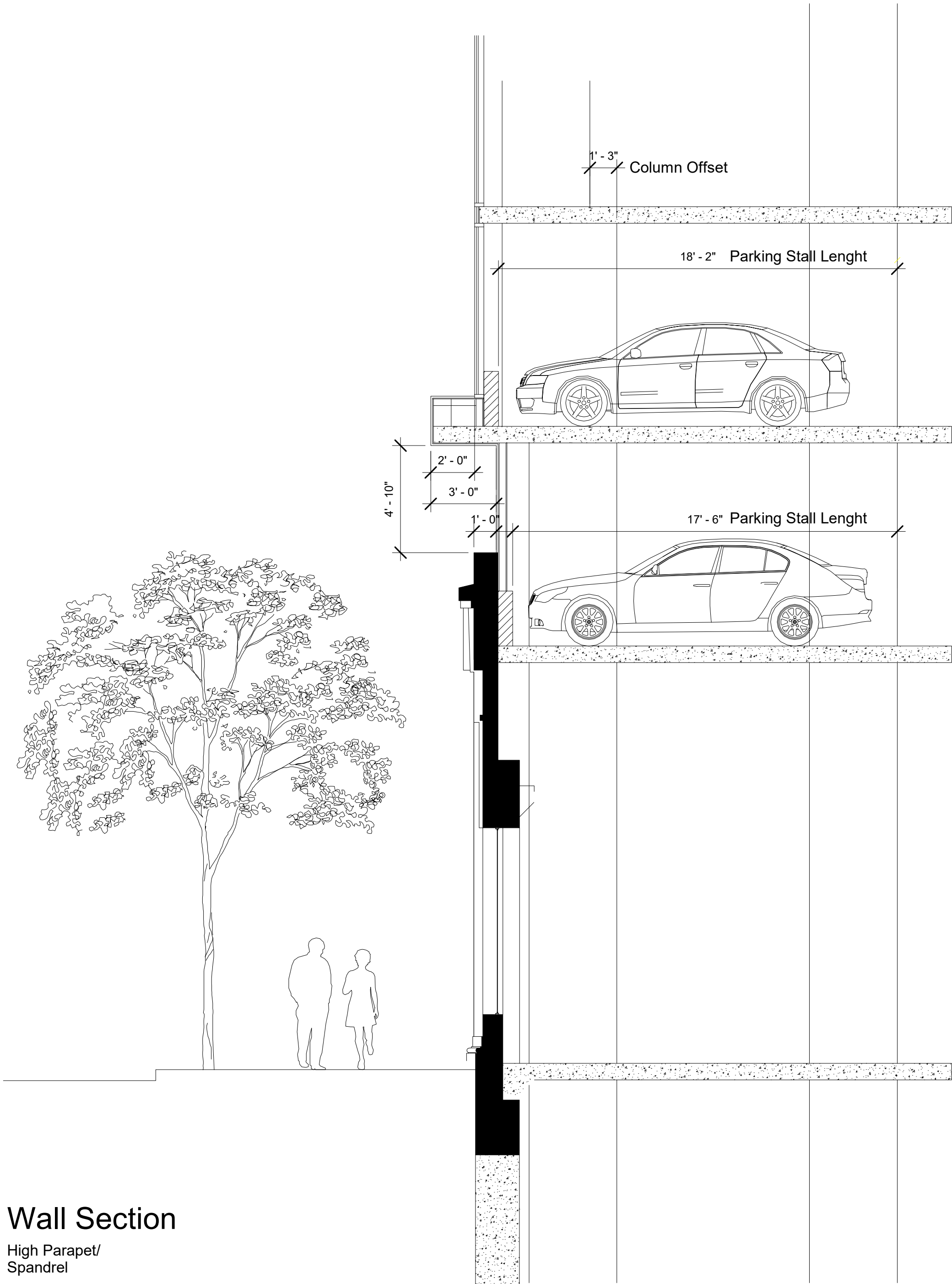
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Wall Section
Low Parapet/
Metal Panel



Wall Section
High Parapet/
Spandrel

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PACKAGE

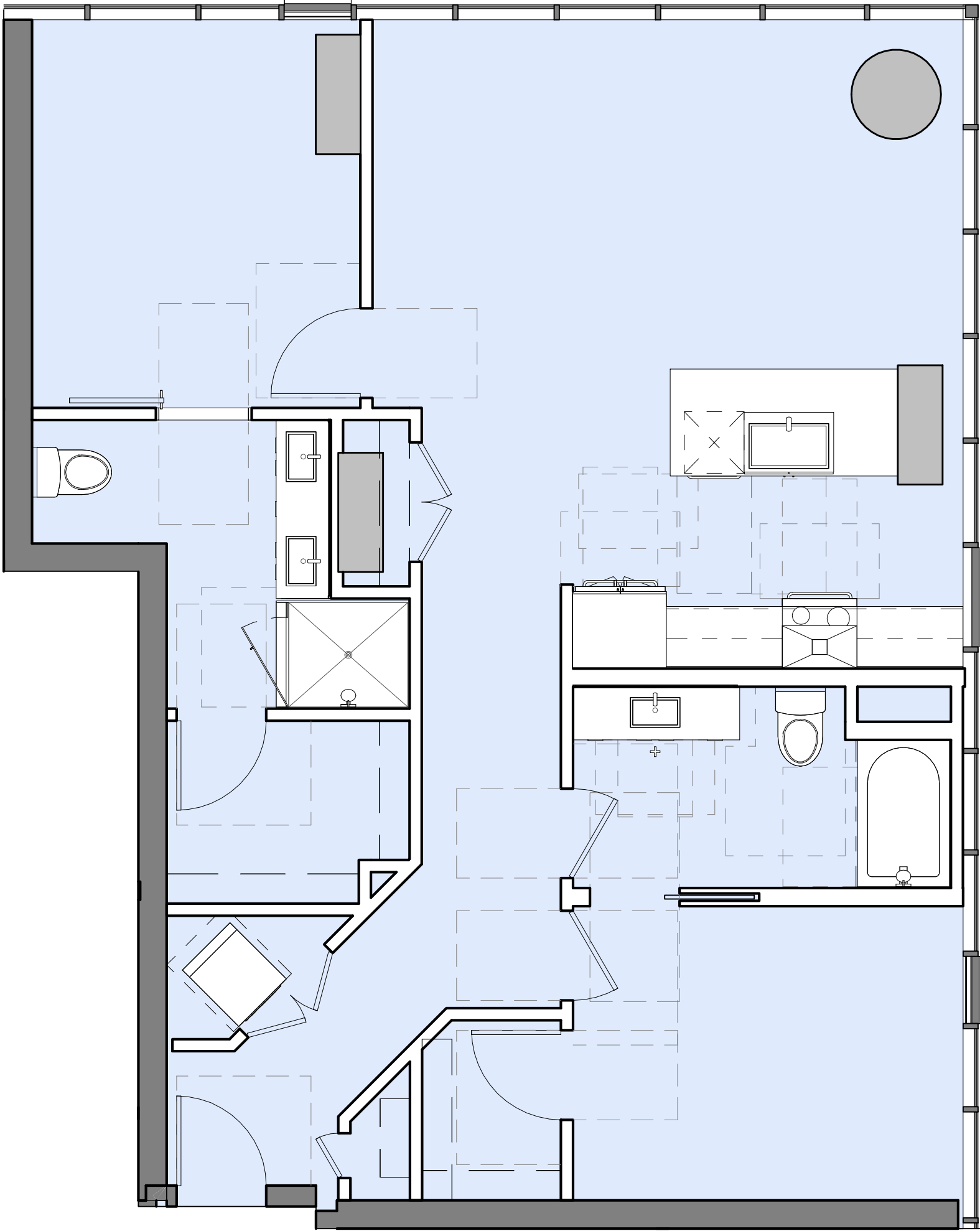
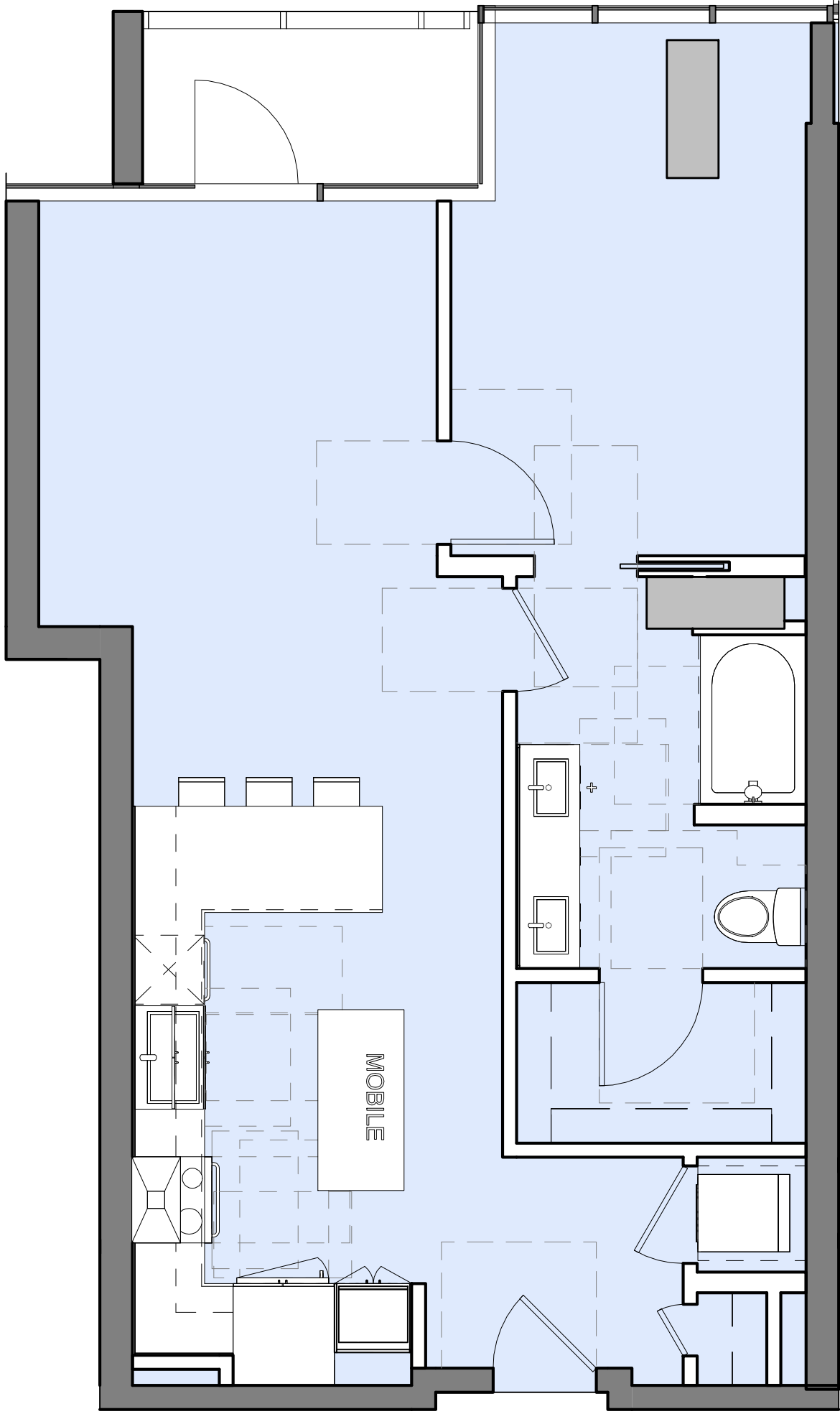
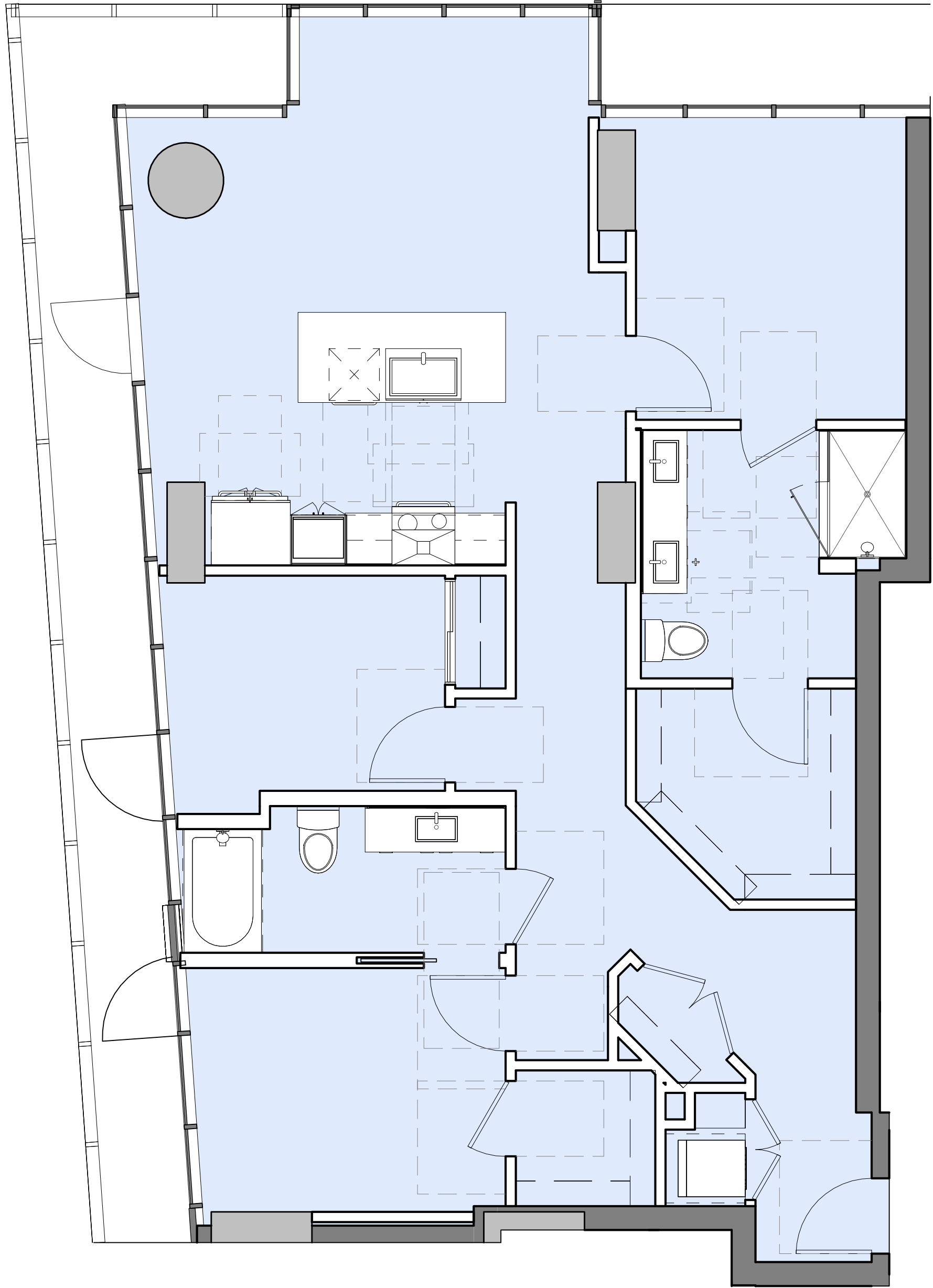
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6.9.20

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Author Checker
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TITLE:

Detail Section

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Enlarged Units



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185 W F St suite 500, San Diego, CA 92101
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Perspective

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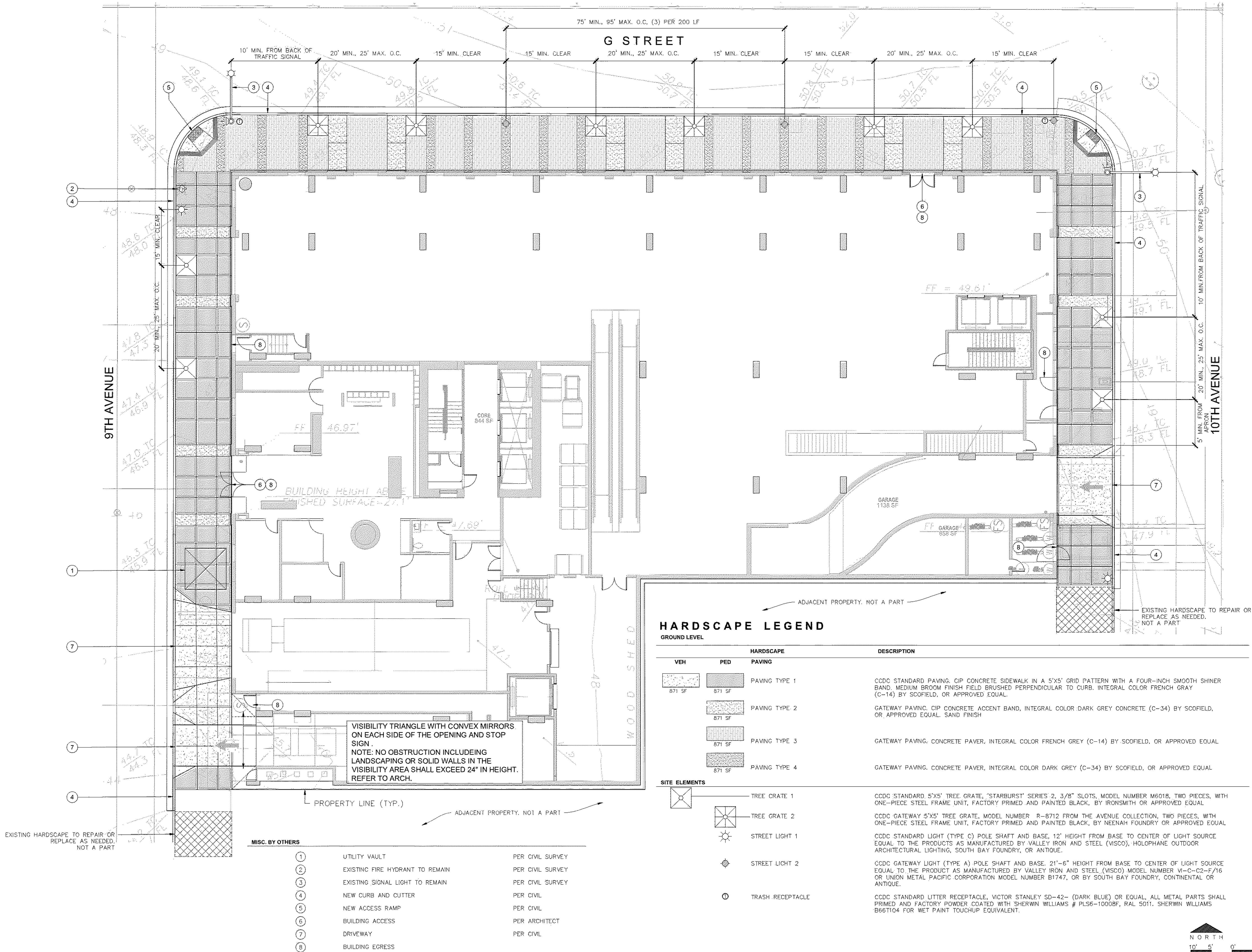
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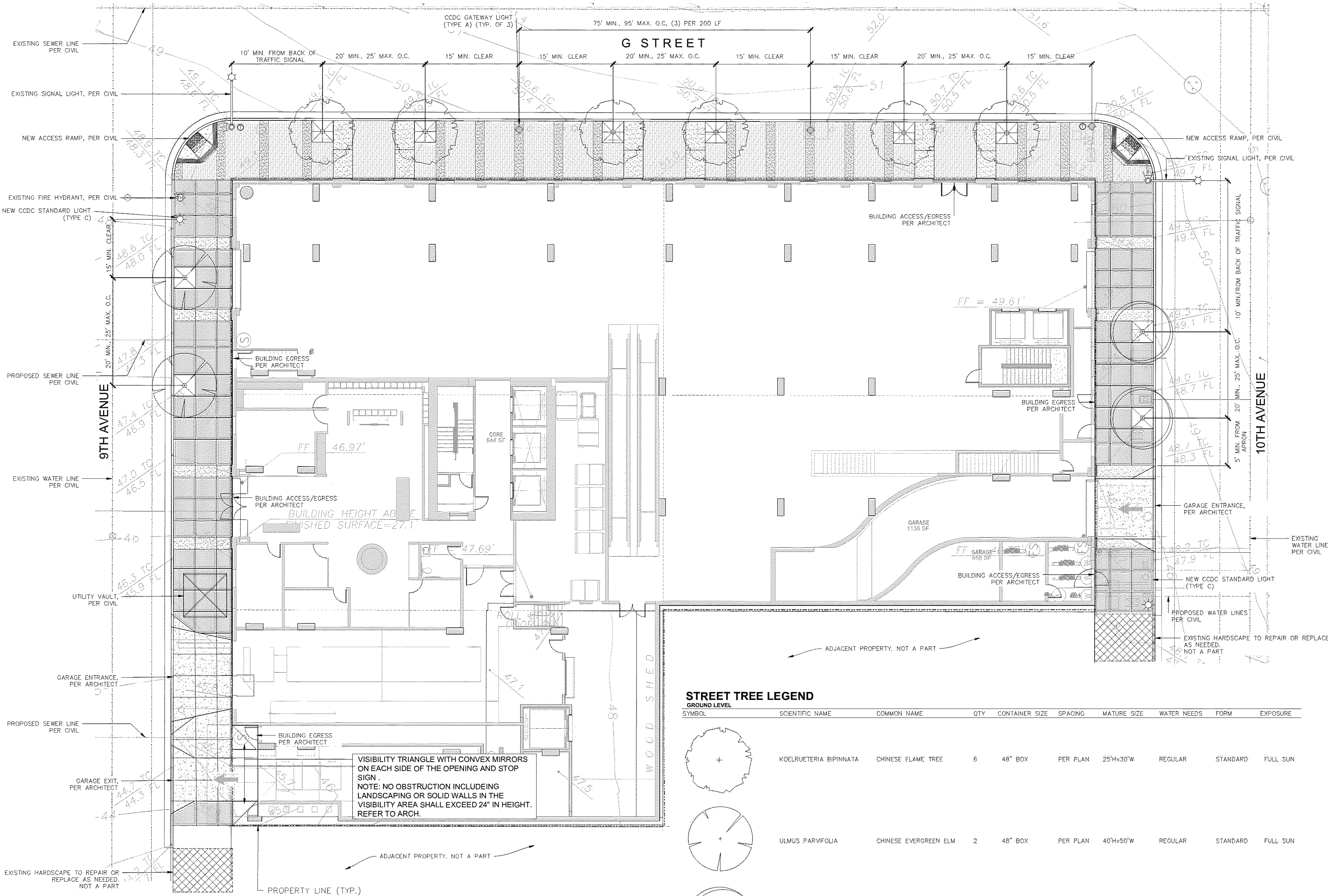
Perspective

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LONG-TERM MAINTENANCE / CONFORMANCE

ALL RIGHT-OF-WAY LANDSCAPE AS SHOWN ON THESE PLANS SHALL BE MAINTAINED BY OWNER. THE RIGHT-OF-WAY SHALL BE MAINTAINED FREE OF DEBRIS AND LITTER AND ALL PLANT MATERIAL SHALL BE MAINTAINED IN A HEALTHY GROWING CONDITION CONSISTENT WITH CENTRE CITY STREET DESIGN MANUAL AND THE CITY OF SAN DIEGO LANDSCAPE REGULATIONS AND STANDARDS.

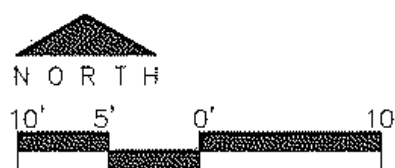
MINIMUM TREE SEPARATION DISTANCE

FIFTEEN (15) FEET MINIMUM FROM STREET LIGHTS.
FIVE (5) FEET MINIMUM FROM DRIVEWAY APRON FLARES.
TWENTY (20) FEET MINIMUM FROM THE FRONT OF ANY TRAFFIC SIGNAL OR STOP SIGN.
TEN (10) FEET MINIMUM FROM THE BACK OF ANY TRAFFIC SIGNAL OR STOP SIGN.
FIVE (5) FEET MINIMUM FROM UNDERGROUND UTILITIES.
TEN (10) FEET MINIMUM FROM SEWER LINES.

STREET TREE LEGEND

SYMBOL	SCIENTIFIC NAME	COMMON NAME	QTY	CONTAINER SIZE	SPACING	MATURE SIZE	WATER NEEDS	FORM	EXPOSURE
	KOELRUTERIA BIPINNATA	CHINESE FLAME TREE	6	48" BOX	PER PLAN	25'Hx30'W	REGULAR	STANDARD	FULL SUN
	ULMUS PARVIFOLIA	CHINESE EVERGREEN ELM	2	48" BOX	PER PLAN	40'Hx50'W	REGULAR	STANDARD	FULL SUN
	JACARANDA MIMOSIFOLIA	JACARANDA	2	48" BOX	PER PLAN	30'Hx25'W	REGULAR	STANDARD	FULL SUN

TREES PLANTED IN URBAN SPACE AREAS SHOULD HAVE A MINIMUM PLANTING AREA OF 100 SQUARE FEET, WITH A MINIMUM SOIL DEPTH OF 5 FEET



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Street Tree Plan

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Shrubs should have a minimum planting area of 24 square feet, with a minimum soil depth of 30-inches.



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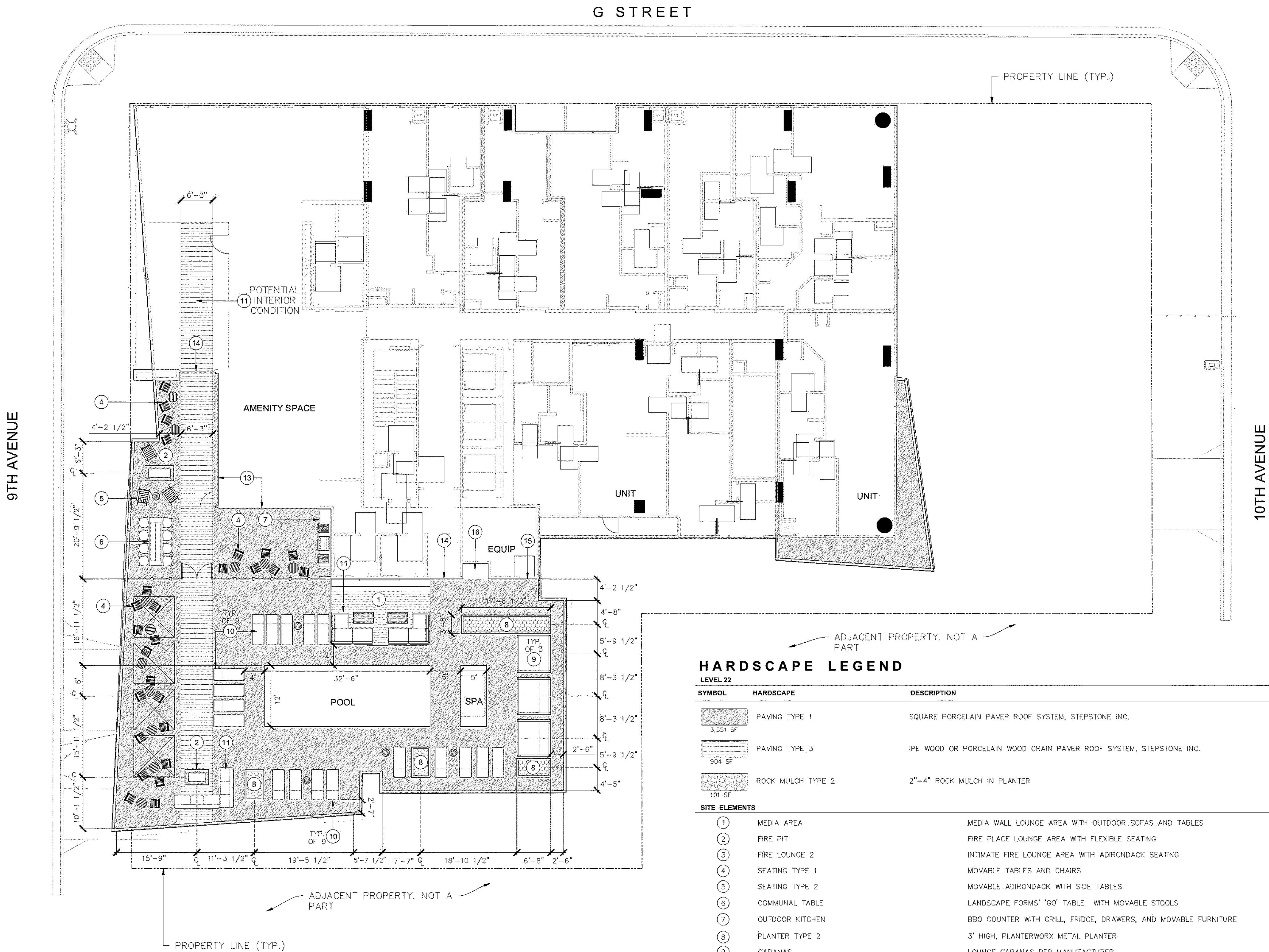
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Level 7
Illustrative

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HARDSCAPE LEGEND

LEVEL 22

SYMBOL	HARDSCAPE	DESCRIPTION
	PAVING TYPE 1	SQUARE PORCELAIN PAVER ROOF SYSTEM, STEPSTONE INC.
	PAVING TYPE 3	IPE WOOD OR PORCELAIN WOOD GRAIN PAVER ROOF SYSTEM, STEPSTONE INC.
	ROCK MULCH TYPE 2	2'-4" ROCK MULCH IN PLANTER

SITE ELEMENTS

1	MEDIA AREA	MEDIA WALL LOUNGE AREA WITH OUTDOOR SOFAS AND TABLES
2	FIRE PIT	FIRE PLACE LOUNGE AREA WITH FLEXIBLE SEATING
3	FIRE LOUNGE 2	INTIMATE FIRE LOUNGE AREA WITH ADIRONDACK SEATING
4	SEATING TYPE 1	MOVABLE TABLES AND CHAIRS
5	SEATING TYPE 2	MOVABLE ADIRONDACK WITH SIDE TABLES
6	COMMUNAL TABLE	LANDSCAPE FORMS' 'GO' TABLE WITH MOVABLE STOOLS
7	OUTDOOR KITCHEN	BBO COUNTER WITH GRILL, FRIDGE, DRAWERS, AND MOVABLE FURNITURE
8	PLANTER TYPE 2	3' HIGH, PLANTERWORX METAL PLANTER
9	CABANAS	LOUNGE CABANAS PER MANUFACTURER
10	CHAISE LOUNGES	OUTDOOR CHAISE LOUNGE CHAIRS AND SIDE TABLES
11	SEATING TYPE 3	SOFA SEATING WITH COFFEE TABLE
12	POOL FENCE AND GATE	5' HIGH GLASS POOL FENCE WITH GLASS SWING GATE AND PUSH HARDWARE
13	AMENITY SPACE	PER ARCHITECT
14	BUILDING ENTRY	PER ARCHITECT
15	MAINTENANCE ACCESS	PER ARCHITECT
16	POOL SHOWER AND WATER FOUNTAINS	PER ARCHITECT



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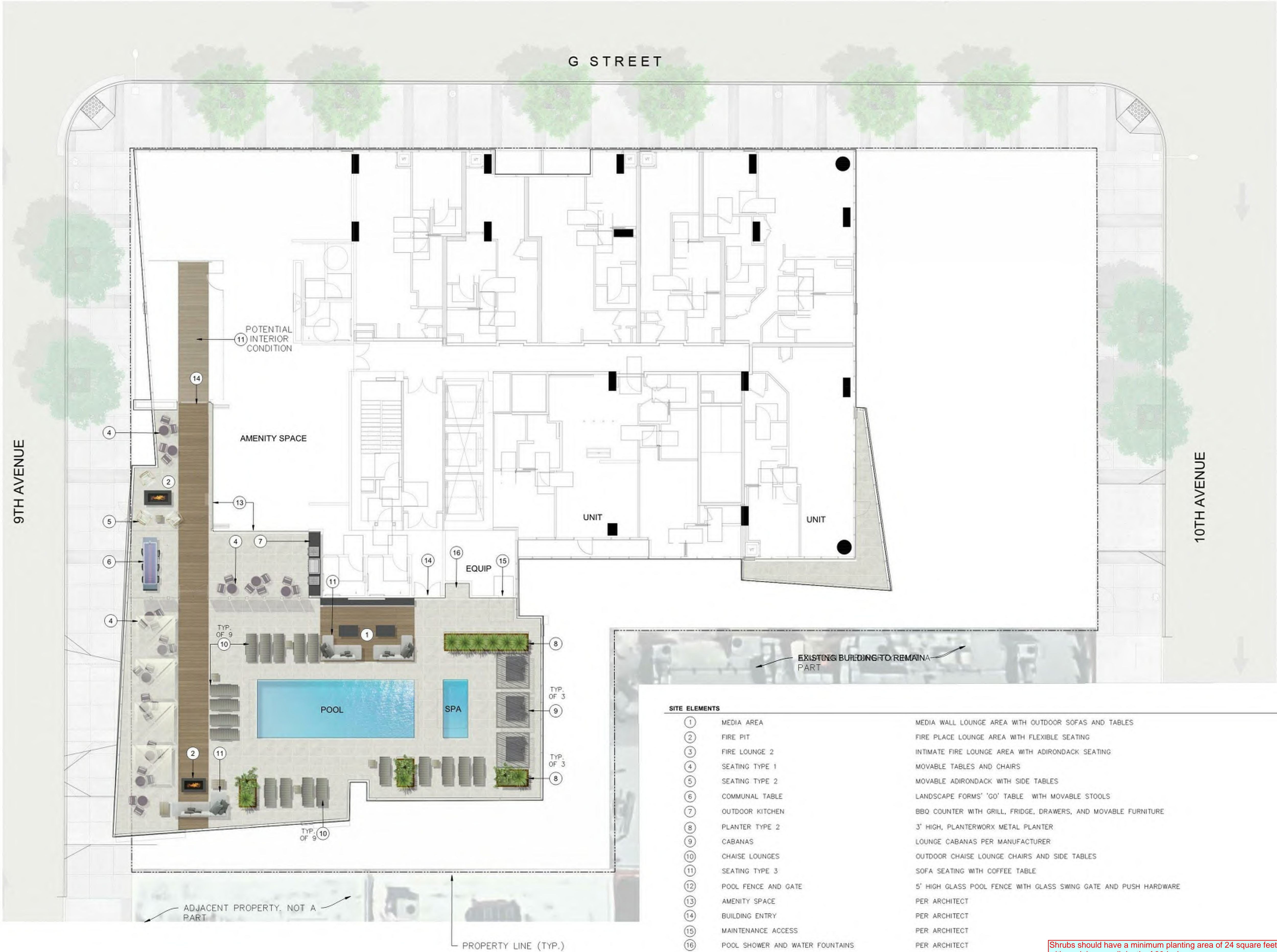
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Level 22
Hardscape Plan

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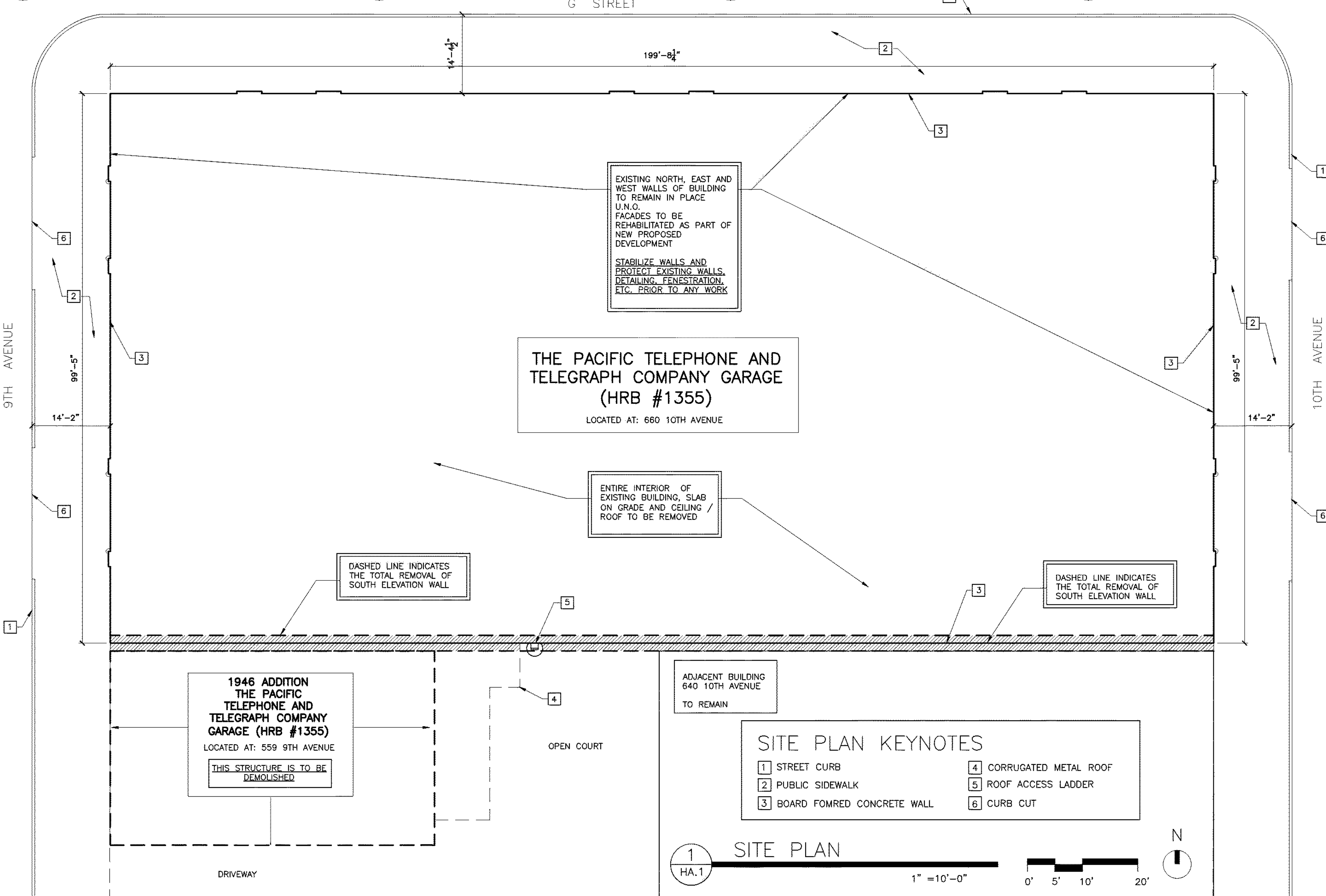
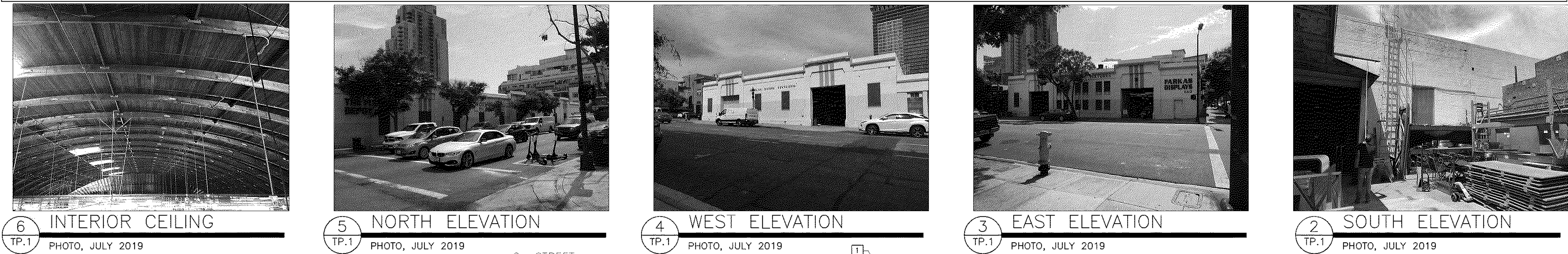
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Level 22
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TREATMENT PLAN: THE PACIFIC TELEPHONE AND TELEGRAPH COMPANY GARAGE (HRB #1355)
660 TENTH AVENUE AND 559 9TH AVENUE, SAN DIEGO, CA 92101
PERIOD OF SIGNIFICANCE 1932-1956



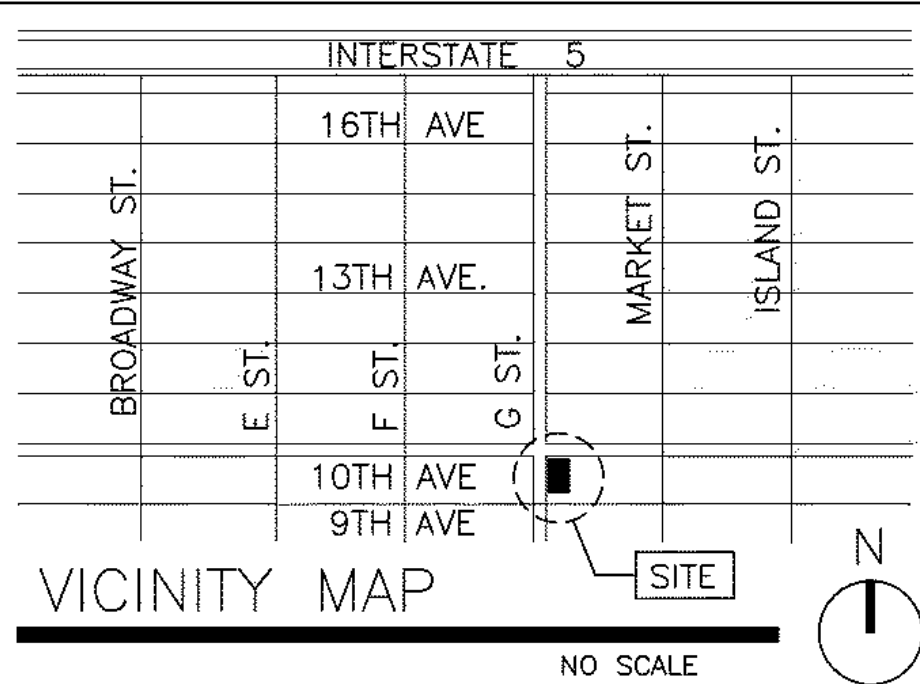
DESCRIPTION OF RESOURCE

THE DESIGNATED RESOURCE (HRB# 1355) WITH A PERIOD OF SIGNIFICANCE OF 1932 - 1956 IS LOCATED AT 660 10TH AVE. AND 659 9TH AVE., SAN DIEGO, CA 92101 (APN:535-136-00-00). THE RESOURCE CONSISTS OF A ONE STORY COMMERCIAL BUILDING WITH A RECTANGULAR FOOTPRINT OF APPROX. 100' x 200', CONSTRUCTED IN 1932 IN THE ART DECO STYLE AND A ONE STORY ADDITION, SIMILAR IN STYLE, BUILT IN 1946 DIRECTLY ADJACENT TO THE SOUTH. THE ADDITION MEASURES APPROX. 36' x 60'.

THE CITY OF SAN DIEGO HISTORICAL RESOURCES BOARD BASED ITS DESIGNATION OF THE RESOURCE ON THE FOLLOWING FINDINGS:

CRITERION A: THE PROPERTY IS HISTORICALLY SIGNIFICANT AS A SPECIAL ELEMENT OF THE CITY'S ECONOMIC DEVELOPMENT, REVEALING THE INCREASING IMPORTANCE OF THE AUTOMOBILE AS AMERICA'S FAVORED METHOD OF PRIVATE TRANSPORTATION FROM 1900-1956, THE BUILDING RETAINS INTEGRITY TO ITS 1932-1956 PERIOD OF SIGNIFICANCE. SPECIFICALLY, THE RESOURCE WHICH WAS CONSTRUCTED IN 1932 FOR USE AS A GARAGE, CONTINUES TO CONVEY THE SIGNIFICANT CHARACTER DEFINING FEATURES OF AUTOMOBILE-RELATED DEVELOPMENT AS IDENTIFIED IN THE EAST VILLAGE HISTORICAL CONTEXT STATEMENT, INCLUDING A SINGLE STORY, WAREHOUSE-LIKE MASSING; LIMITED ORNAMENTATION, RAISED VERTICAL PROJECTIONS AND NUMEROUS GARAGE DOORS.

CRITERION C: THE PROPERTY IS HISTORICALLY SIGNIFICANT THROUGH THE RETENTION OF CHARACTER DEFINING FEATURES OF THE ART DECO STYLE AND RETAINS A GOOD LEVEL OF ARCHITECTURAL INTEGRITY FROM ITS 1932 PERIOD OF SIGNIFICANCE. SPECIFICALLY, THE RESOURCE EXHIBITS SMOOTH WALL SURFACES, A FLAT ROOF, VERTICAL PROJECTIONS ABOVE THE ROOFLINE, FLUTED PILASTERS, GEOMETRIC DETAILING AND METAL WINDOWS.



INDEX OF SHEETS	
TP 1	SITE PLAN, PHOTOS, VICINITY MAP
TP 1.1	MONITORING PLAN AND TREATMENT PLAN
TP 1.2	TREATMENT PLAN CONTINUED
TP 2	FLOOR PLAN
TP 3	REFLECTED CEILING PLAN
TP 4	ROOF PLAN
TP 5	WEST AND EAST ELEVATION
TP 6	NORTH ELEVATION
TP 7	SOUTH ELEVATION
TP 8	LONGITUDINAL BLDG. SECTION A-A
TP 9	BLDG. CROSS SECTION B-B, WINDOW DTLS.
TP 10	WALL SECTIONS C THROUGH E

TREATMENT PLAN

PROJECT : THE PACIFIC TELEPHONE & TELEGRAPH CO. GARAGE
HRB #1355 TREATMENT PLAN
660 10TH AVE. AND 559 9TH AVE., SAN DIEGO, CA 92101
CLIENT : CISTERRA PARTNERS, LLC
3580 CARMEL MOUNTAIN ROAD, STE. # 460, SAN DIEGO, CA 92130

ARCHITECT: UNION ARCHITECTURE, INC.
JOHN H. EISENHART, ARCHITECT
EVA THORN, INTERIORS
344 22ND STREET, SAN DIEGO, CA 92102
TELEPHONE: 619.786.2882 / WWW.UNIONARCH.COM

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REV: 10.10.2019
REV: 03.20.2020
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SHEET 1 OF 12

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660 TENTH AVENUE AND 559 9TH AVENUE, SAN DIEGO, CA 92101
PERIOD OF SIGNIFICANCE 1932–1956

<p><u>3. Select demolition of structure (HAM, PA, CM).</u></p> <p>Issue:</p> <p>a. Demolition of interior walls, windows, doors, channels at overhead doors, wooden roof, skylights and south elevation are completed. Monitor to review completed work and any items that have been removed and labeled for storage</p> <p>b. Discuss post-demo stabilization and protection of historic structure.</p> <p>c. Overview of Treatment Plan, Architectural, Landscaping and Engineering documents as related to rehabilitation on site.</p> <p><u>4. Review of stabilization and protection of historic structure during rehabilitation on site (new footings, foundation, shotcrete, utilities, site preparation) (D, HAM, PA, CM, SE).</u></p> <p>Issue:</p> <p>a. Review of preparation work for rehabilitation on site.</p> <p><u>5. Continued monitoring of rehabilitation of structure. Monthly or as required by construction activity (HAM, PA, CM).</u></p> <p>Issue:</p> <p>a. Review rehabilitation of resource in accordance with Treatment Plan and Architectural, Landscaping and Engineering Documents.</p> <p>b. Review of rehabilitation of windows, doors, concrete repair, concrete details.</p> <p><u>6. Final Monitoring (HAM, PA, CM, D).</u></p> <p>Issue:</p> <p>a. Final punch list of items to complete rehabilitation work according to Treatment Plan and Architectural, Landscaping and Engineering Documents.</p> <p><u>7. Draft Report (HAM, PI).</u></p> <p>Issue:</p> <p>a. Draft report of monitoring process to be submitted to PI for review.</p> <p>Page 3 of 4</p>	<p>its Architecture is a good example of the Art Deco Style. The period of significance is 1932-1956.</p> <p>This Monitoring Plan will follow the Treatment Plan and supporting architectural documents prepared to rehabilitate this historic structure at its current location at 660 10th Avenue.</p> <p>Treatment Plan documents shall be used by Monitor as guidelines. <u>Monitoring actions are as follows:</u></p> <p><u>1. Pre-construction meeting (D, PA, HAM, GC, PI, CM, City of SD: BI, EM & HM)</u></p> <p>Issue:</p> <p>a. Overview of Treatment Plan and Monitoring Plan as related to historic resource rehabilitation.</p> <p><u>2. Preparation of structure for demolition (HAM, CM, PA, SE).</u></p> <p>Issue:</p> <p>a. Monitor to be present <u>prior</u> to removal / demolition of any interior walls, any windows, doors, wooden roof, skylights and south elevation.</p> <p>b. Discuss pre-demo stabilization and protection of historic structure.</p> <p>c. Review work involved by CM to protect-in-place / remove / take-off typical template samples for rehabilitation. Windows, doors, any items that may be removed and taken from the structure to be rehabilitated / reinstalled shall be labeled, photographed, removed and protected for transport and storage.</p> <p><i>Any labels should include the following information:</i></p> <ul style="list-style-type: none">• Property name and address item was taken from.• Photo of item (applicable for typical windows, typ. concrete profile etc.).• Information where item was taken (location for window)• Contact information of dismantling contractor.• Property Owner contact.• Project Architect contact / Historic Architect contact.• Historic Monitor contact.• City Historic Planner contact.• Dismantling date. <p>d. Monitor to approve structure is ready for select demolition.</p> <p>Page 2 of 4</p>	<p>UNION ARCHITECTURE INC.</p> <p>344 22ND STREET, SAN DIEGO, CA. 92102 T:619-788-2862</p> <p><u>MONITORING PLAN</u></p> <p>DATE: March 20, 2020</p> <p>PROJECT: The Pacific Telephone and Telegraph Co. Garage Located at: 660 10th Ave. and 559 9th Ave. San Diego, CA 92101 APN: # 535-136-01-00 HRB Site #1355 Year built: 1932 and 1946 Period of Significance: 1932-1956</p> <p>SUBJECT: Monitoring Plan for rehabilitation.</p> <p>PROJECT TEAM:</p> <p>Developer (D): Paul Thometz, Cisterra Partners. Project Architect (PA): Frank A. Landry, Carrier Johnson + Culture. Historic Architect & Monitor (HAM): John Eisenhart, Union Architecture Inc. Principal Investigator (PI): Marie Burke Lia, Attorney at Law General Contractor (GC): T.B.D. Structural Engineer (SE): T.B.D. Construction Manager (CM): T.B.D. Building Inspector (BI): T.B.D. City of San Diego, Environmental (EM): Jennifer Wakem City of San Diego, Planning Historical (HM): Suzanne Segur</p> <p>LOCATIONS: Monitoring will take place on the site at 660 10th Avenue.</p> <p>PROPERTY DESCRIPTION:</p> <p>The Pacific Telephone and Telegraph Co. Garage building, a one story commercial building, is located at 660 10th Ave and 559 9th Ave. It was initially constructed in 1932 in the Art Deco Style, an addition to the south, similar in style was built in 1946. The parking garage, later warehouse building, is historically designated and significant under City of San Diego Historic guidelines (HRB #1355) per Criteria "A" (special element of the City's Development) and "C" (Architecture). The typology of the resource as an auto garage is a significant character defining feature of the automobile-related development of the City and</p> <p>Page 1 of 4</p>
<p>Historical Context Statement, including a single story, warehouse-like massing; limited ornamentation, raised vertical projections and numerous garage doors. Criterion C: The property is historically significant through retention of character defining features of the Art Deco Style and retains a good level of architectural integrity from its 1932 period if significance. Specifically, the resource exhibits smooth wall surfaces, a flat roof, vertical projections above the roofline, fluted pilasters, geometric detailing and metal windows.</p> <p>The footprint of the 1932 building is rectangular, it measures approximately 100' x 200'. The later addition to the south measures 36' x 60'. Exterior walls are mostly constructed of board formed concrete, with the western portion of the southern elevation of the 1932 building consisting of concrete post and beam construction with brick in-fill.</p> <p>The 3 main elevations (the street elevations) are laid out symmetrical. They are structured horizontally into a base zone, main wall field and a continuous decorative cornice / parapet banding at the top. All decorative detailing is cast in place concrete as part of the wall.</p> <p>All door openings have a decorative motif at the top. Windows are commercial steel sash type with wire mesh glazing. They are covered with protective metal woven mesh on the exterior. The roof and ceiling are comprised of a wooden "bow" truss system. There are six skylights on the roof of the 1932 structure. All roofing is built-up roofing.</p> <p>The building will be rehabilitated as part of a new development on the site. The 1932 building will be integrated with a new multi-story mixed-use building, the 1946 addition will be demolished. Modifications to the 1932 building include demolition of the ceiling / roof and demolition of the south elevation. Existing historic openings (doors and windows) on the east, north and west elevations will be modified in select locations to allow for the new use. This Treatment Plan is being prepared to rehabilitate the historic structure at the existing location.</p> <p>INTRODUCTION:</p> <p>The implementation of the Treatment Plan for rehabilitation is under the supervision of the Project Architect and Historic Architect / Monitor in a manner consistent with the mitigation, monitoring, and reporting program for this project. This Treatment Plan is accompanied by a copy of HABS drawings of the property prepared by the Historic Architect / Monitor, drawings that outline the proposed stabilization and preparation of the structure for rehabilitation. This Treatment Plan and its related drawings will be included in all subsequent plans for the discretionary permit processing, construction documents, building permit, and observational process.</p> <p>TREATMENT PLAN CONTINUED ON SHEET TP 1.2</p> <p>Page 2 of 7</p>	<p>UNION ARCHITECTURE INC.</p> <p>344 22ND STREET, SAN DIEGO, CA. 92102 619-269-4941</p> <p><u>TREATMENT PLAN</u></p> <p>DATE: March 20, 2020</p> <p>PROJECT: The Pacific Telephone and Telegraph Co. Garage Located at: 660 10th Ave. and 559 9th Ave. San Diego, CA 92101 APN: # 535-136-01-00 HRB Site #1355 Year built: 1932 and 1946 Period of Significance: 1932-1956</p> <p>SUBJECT: Treatment Plan for rehabilitation.</p> <p>PROJECT TEAM:</p> <p>Developer: Paul Thometz, Cisterra Partners, LLC Project Architect: Frank A. Landry, Carrier Johnson + Culture. Historic Architect & Monitor: John H. Eisenhart, Union Architecture Inc. Principal Investigator: Marie Burke Lia, Attorney at Law General Contractor: T.B.D. Structural Engineer: T.B.D.</p> <p>PROPERTY DESCRIPTION:</p> <p>The Pacific Telephone and Telegraph Co. Garage building, a one story commercial building, is located at 660 10th Ave and 559 9th Ave. It was initially constructed in 1932 in the Art Deco Style, an addition to the south, similar in style was built in 1946 (period of significance of resource is 1932 to 1956). The parking garage, later warehouse building, is historically designated under City of San Diego Historic guidelines with the HRB #1355.</p> <p>The City of San Diego Historical Resources Board based its designation of the resource on the following findings:</p> <p>Criterion A: The property is historically significant as a special element of the City's economic development, revealing the increasing importance of the automobile as America's favored method of private transportation from 1900-1956, the building retains integrity to its 1932 – 1956 period of significance, specifically, the resource which was constructed in 1932 for use as a garage, continues to convey the significant character defining features of the automobile-related development as identified in the east Village</p> <p>Page 1 of 7</p>	<p><u>8. Final Report (HAM, PI, D).</u></p> <p>Issue:</p> <p>a. Final report of monitoring process, review updating of HABS and Treatment Plan documents to be submitted to PI for distribution to Developmental Services and San Diego History Center for archives.</p> <p><u>End of Monitoring</u></p> <p>Page 4 of 4</p>

TREATMENT PLAN

PROJECT : THE PACIFIC TELEPHONE & TELEGRAPH CO. GARAGE
HRB #1355 TREATMENT PLAN
660 10TH AVE. AND 559 9TH AVE., SAN DIEGO, CA 92101
CLIENT : CISTERRA PARTNERS, LLC
3580 CARMEL MOUNTAIN ROAD, STE. # 460, SAN DIEGO, CA 92130

ARCHITECT: UNION ARCHITECTURE, INC.
JOHN H. EISENHART, ARCHITECT
EVA THORN, INTERIORS
344 22ND STREET, SAN DIEGO, CA 92102
TELEPHONE: 619.788.2862 / WWW.UNIONARCH.COM

PROJECT NO: 5937.00
FILE NAME: BHM 5937.00-Cisterra-2020-03-20-TP1.1-1.2
DRAWN BY: CHECKED BY: Author Checker
PLOT DATE: 6/9/2020 2:03:46 PM
TITLE:

DATE: 09.13.2019
REV: 03.20.2020
TP. 1.1
SHEET 2 OF 12

9G PLANNED DEVELOPMENT PERMIT PACKAGE

3rd SUBMITTAL
6.9.20

ISSUES:

PROJECT NO: 5937.00
FILE NAME: BHM 5937.00-Cisterra-2020-03-20-TP1.1-1.2
DRAWN BY: CHECKED BY: Author Checker
PLOT DATE: 6/9/2020 2:03:46 PM
TITLE:

TP.1.1

7.20.20

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TREATMENT PLAN: THE PACIFIC TELEPHONE AND TELEGRAPH COMPANY GARAGE (HRB #1355)
660 TENTH AVENUE AND 559 9TH AVENUE, SAN DIEGO, CA 92101
PERIOD OF SIGNIFICANCE 1932–1956

Department of the Interior, <https://www.nps.gov/tps/how-to-preserve/briefs/15-concrete.htm#preservation>)
Consistent with Standards #2, 6, 7, 9, and 10.

EXTERIOR DOORS AND WINDOWS:

The steel doors and windows are generally in good shape. The contractor should repair as needed for smooth operation. If a feature of the sash or frame is missing or deteriorated beyond repair (monitor to determine condition) a replicated profile of the missing element shall be used. If the sash and fixed units are a new unit, they should match the profile of the existing original rail and style (ie. Hope's steel windows, Torrance Co. or equal, shop drawings to be submitted for approval). Follow Preservation Brief #13, of the National Park Service of the US Department of the Interior, <https://www.nps.gov/tps/how-to-preserve/briefs/13-steel-windows.htm#repair>
All of these options would be consistent with the standards, repair is generally the preferred option. The single leaf entry door on the east elevation is non-original. Consistent with Standards #2, 6, 7, 9 and 10.

ELECTRICAL & LIGHTING:

Existing electrical and lighting system will be upgraded to conform to current code. All conduit and non-historic electrical and lighting on the exterior shall be removed. Electrical meters shall be located discretely away from public view. Exterior lighting fixtures to be surface mounted and to be sympathetic to Art Deco style influence. Consistent with Standards #9 and 10.

PLUMBING:

All exterior plumbing and vent pipes to be dismantled. New interior plumbing and vents to be installed as required. The plumbing system will be upgraded to conform to current code. Consistent with Standards #9 and 10.

HEATING:

New HVAC to conform with current code, any equipment is to be screened from public view. Consistent with Standards #9 and 10.

PAINTING:

If lead paint is detected, follow current laws for careful removal or apply removal per EPA lead paint regulations.

New paint scheme on the exterior of the building shall be in Art Deco style colors. Historic monitor and City staff to approve final painting scheme.
The colors can be existing scheme or a painting scheme from Dunn Edwards Company: Trade Winds. Det647 - Field

Page 5 of 7

EXISTING FOUNDATION:

The Pacific Telephone and Telegraph Co. Garage building has concrete foundation walls and footings. The depth of the foundation and footing cannot be determined prior to excavation. Consistent with Standards #9 and 10.

NEW FOUNDATION:

The east, north and west elevations are to remain in place. They will be stabilized following the structural engineer / general contractor's recommendations. This may involve new shotcrete on the interior side of existing concrete exterior walls, new foundation walls, steel reinforcement on the interior side (new openings) and new footings. Consistent with Standards #9 and 10

EXISTING WALLS / EXISTING FRAMING:

The existing walls consist of board formed concrete, painted at exterior and interior. The south elevation consists of brick-in fill and boardformed concrete, this exterior wall will be demolished entirely. Consistent with Standards #2, 9 and 10.

NEW WALLS / NEW FRAMING:

The existing board formed concrete walls will remain in place at the east, north and west elevations. These elevations will be affected by modifications to allow for the new use. Select existing window and door openings will be modified.
The existing building will be incorporated with a new development. New construction will be differentiated from existing historic. Consistent with Standards #2, 9 and 10.

ROOF:

The roof and ceiling will be demolished. Any new roofing, gutters and downspouts shall be located as not to detract / conflict with the existing facades. Consistent with Standards #6, 9 and 10.

EXTERIOR FINISHES:

The existing board formed concrete walls will remain in place in the east, north and west elevations. The concrete finish is painted. Any cracks and exposed steel reinforcement shall be repaired during the rehabilitation process to protect the existing concrete structure from progressing damage due to water intrusion. Repairs shall be done in a manner chemically compatible with the composition of the existing historic concrete mixture (follow Preservation Brief #15 of the National Park Service of the US

Page 4 of 7

TREATMENT PLAN CONTINUED
FROM SHEET TP 1.1

PREPARATION OF STRUCTURE:

1. Preparation of the structure prior to any rehabilitation / demolition work:

The entire structure at 660 10th Ave. is to be stabilized, braced, and secured. Specific procedures to be determined by qualified structural engineers and general contractor.

The implementation of these procedures will occur only after review from Historic Architect / Monitor. Exterior concrete trim pieces, windows and doors affected by this shall be protected from damage.

Windows and doors to remain should be protected in place, optionally, they may be removed, labeled and stored to be reinstalled during rehabilitation and fenestration openings to be covered with ¾" plywood nailed to wood bracing. This option may be acceptable per monitor's approval.

Any existing exterior plumbing pipes, wiring etc. shall be removed along with site utilities to be disconnected. Monitor to be notified prior to modification of structure required for rehabilitation. Consistent with Standards #6, 7, 9 and 10.

2. Removal of interior of structure / select demolition at exterior:

Prior to the start of any demolition / removal process the Contractor and Monitor will meet on site to review the scope of demolition / removal work.
The entire 1946 structure, located at 559 9th Ave. will be demolished. The 1932 structure located at 660 10th Ave. will only be partially demolished as described in the Treatment Plan drawings and as follows: The wood stud walls, plaster, windows and doors on the first and second floor will be demolished. The roof / ceiling will be demolished. The concrete floor will be demolished. The southern concrete / brick-in-fill exterior wall will be demolished. Select existing openings will be enlarged on the street facades.

Contractor shall inform Monitor of discovery of items during demolition and removal. The Monitor will evaluate relevance of such materials on site. Consistent with Standards #6, 7, and 9.

3. Post-demo stabilization and protection of structure:

The entire remaining structure is to be stabilized, braced, and secured during the rehabilitation process. Specific procedures to be determined by qualified structural engineers and general contractor.

The implementation of these procedures will occur only after review from Historic Architect / Monitor. Exterior concrete trim pieces, windows and doors shall be protected from potential damage during construction.

Page 3 of 7

are to be applied to specific rehabilitation projects in a reasonable manner, taking into consideration economic and technical feasibility.

1. A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment.

2. The historic character of a property shall be retained and preserved. The removal of historic materials or alteration of features and spaces that characterize a property shall be avoided.

3. Each property shall be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or architectural elements from other buildings, shall not be undertaken.

4. Most properties change over time; those changes that have acquired historic significance in their own right shall be retained and preserved.

5. Distinctive features, finishes, and construction techniques or examples of craftsmanship that characterize a property shall be preserved.

6. Deteriorated historic features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature shall match the old in design, color, texture, and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical, or pictorial evidence.

7. Chemical or physical treatments, such as sandblasting, that cause damage to historic materials shall not be used. The surface cleaning of structures, if appropriate, shall be undertaken using the gentlest means possible.

8. Significant archeological resources affected by a project shall be protected and preserved. If such resources must be disturbed, mitigation measures shall be undertaken.

9. New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment.

10. New additions and adjacent or related new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

Page 6 of 7

So Chic. Det614 - Trim, cornice, fluting.
Red Clay. Det447 - Windows
Consistent with Standards #6.

LANDSCAPING:

The new site will be landscaped and hardscaped in accordance with all relevant regulations of the Land Development Code for the relocation, rehabilitation, and reuse of historic resources. Consistent with Standards #9 and 10.

REPAIR WORK:

Repair work is preferred over use of replicated historic new materials. Retention of as much historic fabric as possible is paramount during rehabilitation. If a historic element is damaged but not in jeopardy of creating a performance issue with regard to the envelope of the building or building system, the feature should remain. Consistent with Standards #2, 5, 6 and 7.

RECONSTRUCTION / REHABILITATION:

The cleaning of all historic material / fabric shall occur by using the gentlest means possible. An appropriate means of control and disposal of lead or other chemicals shall be provided. Historic fabric shall be retained as much as possible. Do not sandblast or water power wash materials.
The character defining massing / form of the structure is a one story commercial building, 100 ft. x 200 ft. with wood "bow" trusses spanning across at 15'-0" o.c.. The character defining material elements are: Board formed concrete walls with concrete detailing at base, cornice and parapet, large steel windows with wire-glass panes and overhead roll-up doors.
Should damage occur to the resource, it shall be repaired in conformance with the Secretary of the Interior's Standards for Rehabilitation or Reconstruction. Attachment of materials shall be similar to the original, historic method. Consistent with Standards #2, 6, 9, and 10.

ATTACHMENTS:

Treatment Plan Drawings.

The Secretary of the Interior's Standards for Rehabilitation

The Standards (Department of Interior regulations, 36 CFR 67) pertain to historic buildings of all materials, construction types, sizes, and occupancy and encompass the exterior and the interior, related landscape features and the building's site and environment as well as attached, adjacent, or related new construction. The Standards

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TREATMENT PLAN

PROJECT :
THE PACIFIC TELEPHONE & TELEGRAPH CO. GARAGE
HRB #1355 TREATMENT PLAN
660 10TH AVE. AND 559 9TH AVE., SAN DIEGO, CA 92101
CLIENT :
CISTERRA PARTNERS, LLC
3580 CARMEL MOUNTAIN ROAD, STE. # 460, SAN DIEGO, CA 92130

ARCHITECT:
UNION ARCHITECTURE, INC.
JOHN H. EISENHART, ARCHITECT
EVA THORN, INTERIORS
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DATE: 09.13.2019
REV: 03.20.2020

TP. 1.2

SHEET 3 OF 12

3rd SUBMITTAL
6.9.20

ISSUES:

PROJECT NO:
5937.00
FILE NAME:
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-SEARCH-Caterpillar-2018.3.rvt
DRAWN BY: CHECKED BY:
Author Checker
PLOT DATE:
6/9/2020 2:03:47 PM
TITLE:

TP.1.2

7.20.20

49

9G
PLANNED DEVELOPMENT PERMIT
PACKAGE



carrierjohnson + CULTURE
architecture + environments + brand strategy + graphics

185 W F St suite 500, San Diego, CA 92101
phone 619.239.2353

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NOTE:

A. THE 1932 BUILDING IS ORIGINAL EXCEPT FOR:
1. THE METAL OVERHEAD ROLL-UP DOORS.
2. THE WIRE MESH WINDOW GUARDS

B. CIP : ABBR. CAST-IN-PLACE CONCRETE

KEYNOTES

1 CONCRETE SIDEWALK / GRADE
2 KEYSTONE TYPE DECORATIVE CAST-IN PLACE CONCRETE MOULDING
3 CONCRETE FLOOR
4 BOARD FORMED (CAST-IN-PLACE) CONCRETE WALL, PAINTED
5 CONCRETE CAST-IN-PLACE (CIP) BASE/TORUS MOULDING DETAIL, SEE 5/TP.10

6 CONCRETE (CIP) MOULDED CORNICE BAND
7 PARAPET (CIP CONCRETE)
8 "FLUTING" DETAIL AT PIER (CIP CONCRETE)
9 STEEL SASH WINDOWS W/ AWNING FUNCTION (SEE DTLS. 1 + 2/TP.9)
10 BEAD MOULDING CONCRETE WINDOW SILL (CIP)
11 RECTANGULAR BLOCK CIP CONCRETE
12 CIP VERTICAL CONCRETE STRIP DECORATIVE RELIEF (PROTRUDING FROM WALL PLANE)

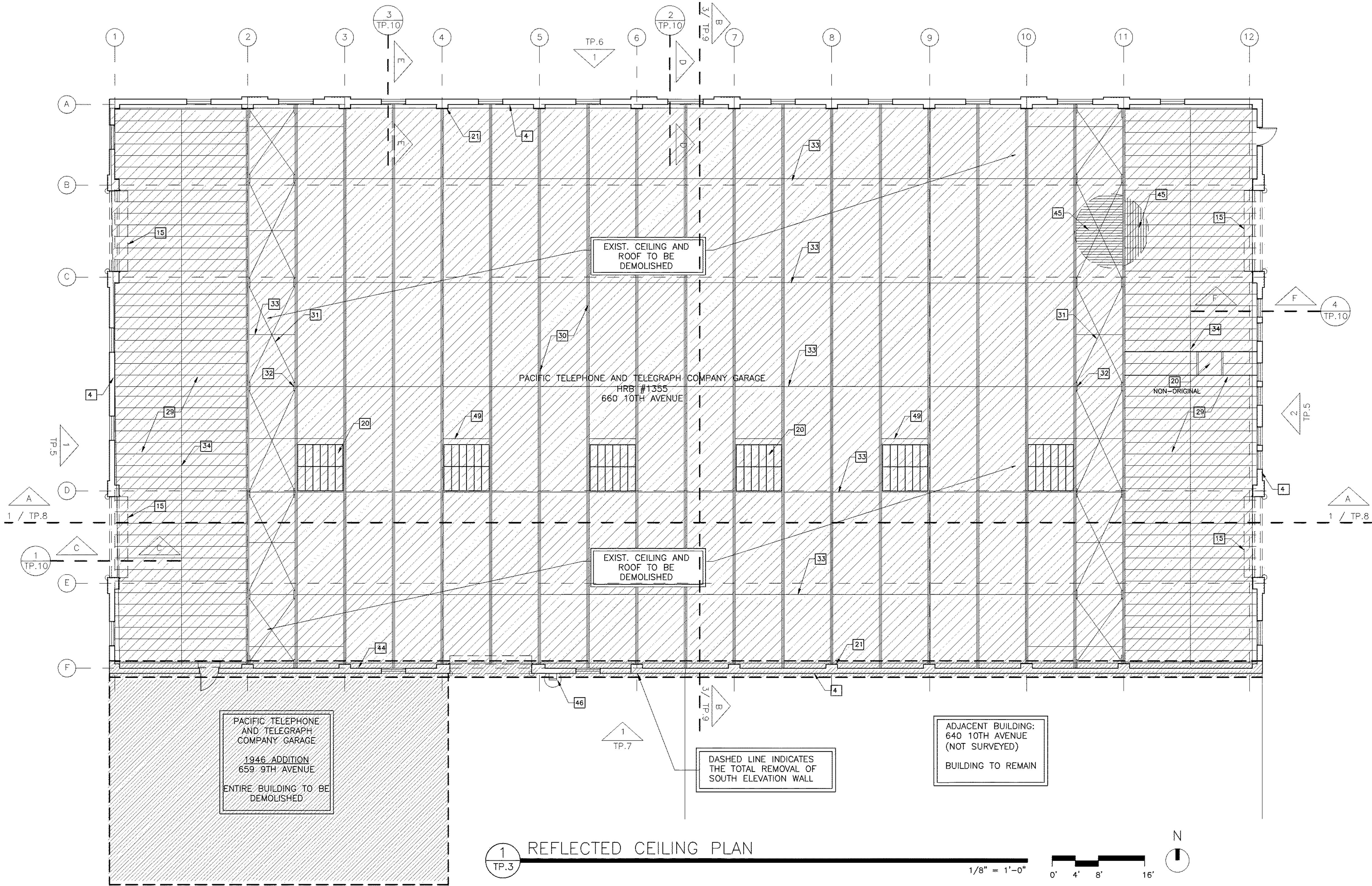
13 DECORATIVE METAL VENT
14 CONCRETE CIP DOOR TRIM (JAMB & HEAD)
15 OVERHEAD STEEL ROLL-UP DOOR W/ METAL WALL CHANNEL
16 "BULLET" BOLLARD
17 BUILT-UP ROOFING
18 ROOF OVERFLOW DRAIN
19 ROOF DRAIN / DOWNSPOUT
20 METAL SASH SKYLIGHT

21 CONCRETE CIP PIER
22 CONCRETE CIP BEAM / CORBEL
23 METAL HANDRAIL
24 2X4 STUD WALL @ 16" O.C.
25 2x CEILING JOIST @ 24" O.C.
26 2X10 FLOOR JOIST @ 16" O.C.
27 PLASTER FINISH WALL
28 7/8" WOOD SHEATHING

29 2x14 ROOF RAFTERS @ 2'-0" O.C.
30 (2) 3X14 WOODEN "BOW" TRUSSES
31 3/4" STEEL TIE RODS
32 STEEL ANGLE
33 3X8 ROOF RAFTER
34 2X6 WOOD CROSS BRIDGING
35 WOOD DOOR
36 2x4 ACOUSTIC LAY IN CEILING (WITH PLASTER CEILING ABOVE)

37 1X WOODEN TREAD AT STAIR
38 WROUGHT IRON RAILING
39 STEEL "C" CHANNEL
40 CORRUGATED METAL CANOPY (OVERHANG)
41 VINYL FLOORING
42 STEEL WINDOWS
43 METAL MESH WINDOW GUARDS
44 BRICK

45 1X6 DOUGLAS FIR SHEATHING
46 METAL ROOF ACCESS LADDER
47 TILE FLOORING
48 STEEL DOOR
49 2X8 ROOF RAFTER
50 CIP CONCRETE HORIZ. BANDING
51 RECESSED WALL CAVITY W/ UTILITY CONNECTION (APPROX. 7.5" DEEP)



1 TP.3 REFLECTED CEILING PLAN

TREATMENT PLAN

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THE PACIFIC TELEPHONE & TELEGRAPH CO. GARAGE
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TP. 3
SHEET 5 OF 12

9G
PLANNED DEVELOPMENT PERMIT
PACKAGE

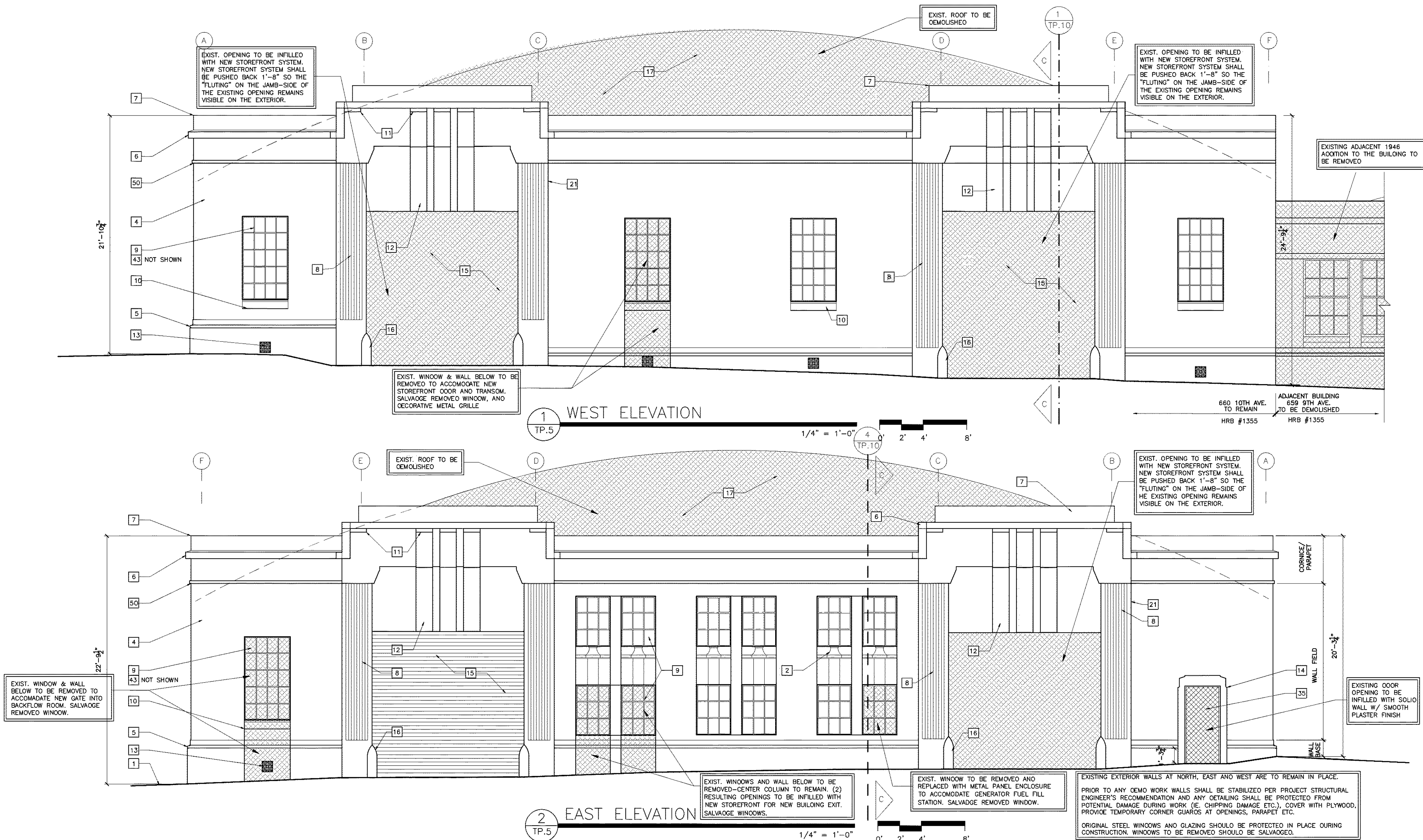
3rd SUBMITTAL
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NOTE:	KEYNOTES									
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	B. CIP : ABBR. CAST-IN PLACE CONCRETE									
	1 CONCRETE SIDEWALK / GRADE									
	2 KEYSTONE TYPE DECORATIVE CAST-IN PLACE CONCRETE MOULDING									
	3 CONCRETE FLOOR									
	4 BOARD FORMED (CAST-IN-PLACE) CONCRETE WALL, PAINTED									
	5 CONCRETE CAST-IN-PLACE (CIP) BASE/TORUS MOULDING DETAIL, SEE 5/TP.10									
	6 CONCRETE (CIP) MOULDED CORNICE BAND									
	7 PARAPET (CIP CONCRETE)									
8 "FLUTING" DETAIL AT PIER (CIP CONCRETE)										
9 STEEL SASH WINDOWS W/ AWNING FUNCTION (SEE DTLS. 1 + 2/TP.9)										
10 BEAD MOULDING CONCRETE WINDOW SILL (CIP)										
11 RECTANGULAR BLOCK CIP CONCRETE										
12 CIP VERTICAL CONCRETE STRIP DECORATIVE RELIEF (PROTRUDING FROM WALL PLANE)										
13 DECORATIVE METAL VENT										
14 CONCRETE CIP DOOR TRIM (JAMB & HEAD)										
15 OVERHEAD STEEL ROLL-UP DOOR W/ METAL WALL CHANNEL										
16 "BULLET" BOLLARD										
17 BUILT-UP ROOFING										
18 ROOF OVERFLOW DRAIN										
19 ROOF DRAIN / DOWNSPOUT										
20 METAL SASH SKYLIGHT										
21 CONCRETE CIP PIER										
22 CONCRETE CIP BEAM / CORBEL										
23 METAL HANDRAIL										
24 2X4 STUD WALL @ 16" O.C.										
25 2x CEILING JOIST @ 24" O.C.										
26 2X10 FLOOR JOIST @ 16" O.C.										
27 PLASTER FINISH WALL										
28 7/8" WOOD SHEATHING										
29 2x14 ROOF RAFTERS @ 2'-0" O.C.										
30 (2) 3X14 WOODEN "BOW" TRUSSES										
31 ¾" STEEL TIE RODS										
32 STEEL ANGLE										
33 3X8 ROOF RAFTER										
34 2X6 WOOD CROSS BRIDGING										
35 WOOD DOOR										
36 2x4 ACOUSTIC LAY IN CEILING (WITH PLASTER CEILING ABOVE)										
37 1X WOODEN TREAD AT STAIR										
38 WROUGHT IRON RAILING										
39 STEEL "C" CHANNEL										
40 CORRUGATED METAL CANOPY (OVERHANG)										
41 VINYL FLOORING										
42 STEEL WINDOWS										
43 METAL MESH WINDOW GUARDS										
44 BRICK										
45 1X6 DOUGLAS FIR SHEATHING										
46 METAL ROOF ACCESS LADDER										
47 TILE FLOORING										
48 STEEL DOOR										
49 2X8 ROOF RAFTER										
50 CIP CONCRETE HORIZ. BANDING										
51 RECESSED WALL CAVITY W/ UTILITY CONNECTION (APPROX. 7.5" DEEP)										

TREATMENT PLAN

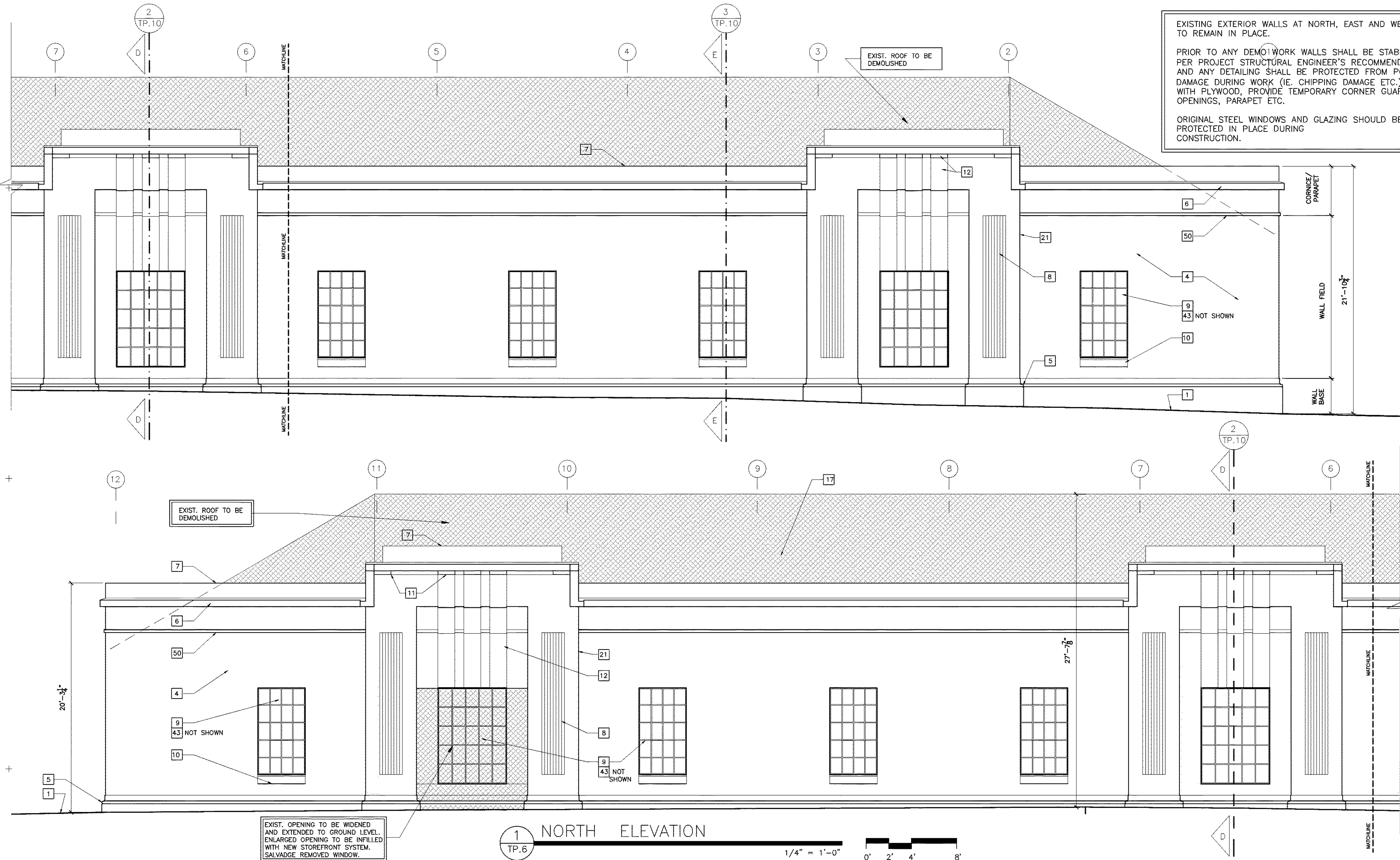
PROJECT : THE PACIFIC TELEPHONE & TELEGRAPH CO. GARAGE
HRB #1355 TREATMENT PLAN
660 10TH AVE. AND 559 9TH AVE., SAN DIEGO, CA 92101
CLIENT : CISTERRA PARTNERS, LLC
3590 CARMEL MOUNTAIN ROAD, STE. # 460, SAN DIEGO, CA 92130

ARCHITECT: UNION ARCHITECTURE, INC.
JOHN H. EISENHART, ARCHITECT
EVA THORN, INTERIORS
344 22ND STREET, SAN DIEGO, CA 92102
TELEPHONE: 619.788.2882 / WWW.UNIONARCH.COM

PROJECT NO: 5937.00
FILE NAME: BHM 300/5537-00-Cisterra-2018.3.rvt
DRAWN BY: Author
CHECKED BY: Checker
PLOT DATE: 6/9/2020 2:03:50 PM
TITLE:

DATE: 09.13.2019
REV: 10.10.2019
REV: 03.20.2020
TP. 5
SHEET 7 OF 12

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NOTE:

- THE 1932 BUILDING IS ORIGINAL EXCEPT FOR:
1. THE METAL OVERHEAD ROLL-UP DOORS.
2. THE WIRE MESH WINDOW GUARDS
- CIP : ABBR. CAST-IN PLACE CONCRETE

KEYNOTES

- CONCRETE SIDEWALK / GRADE
- KEYSTONE TYPE DECORATIVE CAST-IN PLACE CONCRETE MOULDING
- CONCRETE FLOOR
- BOARD FORMED (CAST-IN-PLACE) CONCRETE WALL, PAINTED
- CONCRETE CAST-IN-PLACE (CIP) BASE/TORUS MOULDING DETAIL, SEE 5/TP.10

- CONCRETE (CIP) MOULDED CORNICE BAND
- PARAPET (CIP CONCRETE)
- "FLUTING" DETAIL AT PIER (CIP CONCRETE)
- STEEL SASH WINDOWS W/ AWNING FUNCTION (SEE DTLS. 1 + 2/TP.9)
- BEAD MOULDING CONCRETE WINDOW SILL (CIP)
- RECTANGULAR BLOCK CIP CONCRETE
- CIP VERTICAL CONCRETE STRIP DECORATIVE RELIEF (PROTRUDING FROM WALL PLANE)

- DECORATIVE METAL VENT
- CONCRETE CIP DOOR TRIM (JAMB & HEAD)
- OVERHEAD STEEL ROLL-UP DOOR W/ METAL WALL CHANNEL
- "BULLET" BOLLARD
- BUILT-UP ROOFING
- ROOF OVERFLOW DRAIN
- ROOF DRAIN / DOWNSPOUT
- METAL SASH SKYLIGHT

- CONCRETE CIP PIER
- CONCRETE CIP BEAM / CORBEL
- METAL HANDRAIL
- 2X4 STUD WALL @ 16" O.C.
- 2x CEILING JOIST @ 24" O.C.
- 2X10 FLOOR JOIST @ 16" O.C.
- PLASTER FINISH WALL
- 7/8" WOOD SHEATHING

- 2x14 ROOF RAFTERS @ 2'-0" O.C.
- (2) 3X14 WOODEN "BOW" TRUSSES
- 3/4" STEEL TIE RODS
- STEEL ANGLE
- 3X8 ROOF RAFTER
- 2X6 WOOD CROSS BRIDGING
- WOOD DOOR
- 2x4 ACOUSTIC LAY IN CEILING (WITH PLASTER CEILING ABOVE)

- 1X WOODEN TREAD AT STAIR
- WROUGHT IRON RAILING
- STEEL "C" CHANNEL
- CORRUGATED METAL CANOPY (OVERHANG)
- VINYL FLOORING
- STEEL WINDOWS
- METAL MESH WINDOW GUARDS
- BRICK

- 1X6 DOUGLAS FIR SHEATHING
- METAL ROOF ACCESS LADDER
- TILE FLOORING
- STEEL DOOR
- 2X8 ROOF RAFTER
- CIP CONCRETE HORIZ. BANDING
- RECESSED WALL CAVITY W/ UTILITY CONNECTION (APPROX. 7.5" DEEP)

EXISTING EXTERIOR WALLS AT NORTH, EAST AND WEST ARE TO REMAIN IN PLACE.

PRIOR TO ANY DEMO WORK WALLS SHALL BE STABILIZED PER PROJECT STRUCTURAL ENGINEER'S RECOMMENDATION AND ANY DETAILING SHALL BE PROTECTED FROM POTENTIAL DAMAGE DURING WORK (IE. CHIPPING DAMAGE ETC.), COVER WITH PLYWOOD, PROVIDE TEMPORARY CORNER GUARDS AT OPENINGS, PARAPET ETC.

ORIGINAL STEEL WINDOWS AND GLAZING SHOULD BE PROTECTED IN PLACE DURING CONSTRUCTION.

TREATMENT PLAN

PROJECT :
THE PACIFIC TELEPHONE & TELEGRAPH CO. GARAGE
HRB #1355 TREATMENT PLAN
660 10TH AVE. AND 559 9TH AVE., SAN DIEGO, CA 92101
CLIENT :
CISTERRA PARTNERS, LLC
3580 CARMEL MOUNTAIN ROAD, STE. # 460, SAN DIEGO, CA 92130

ARCHITECT:
UNION ARCHITECTURE, INC.

JOHN H. EISENHART, ARCHITECT
EVA THORN, INTERIORS
3444 22ND STREET, SAN DIEGO, CA 92102
TELEPHONE: 619.788.2863 / WWW.UNIONARCH.COM

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DATE: 09.13.2019
REV: 03.20.2020

TP. 6

SHEET 8 OF 12

9G PLANNED DEVELOPMENT PERMIT PACKAGE

3rd SUBMITTAL
6.9.20

ISSUES:

PROJECT NO:
5937.00
FILE NAME:
BIM 300/5537-00-Carter
-03-ARCH - 03-05-2018.3.rvt
DRAWN BY: CHECKED BY:
Author Checker
PLOT DATE:
6/9/2020 2:03:51 PM
TITLE:

TP.6

SHEET 8 OF 12

7.20.20

54

+

- 1 CONCRETE SIDEWALK / GRADE
- 2 KEYSTONE TYPE DECORATIVE
CAST-IN PLACE CONCRETE MOULDING
- 3 CONCRETE FLOOR
- 4 BOARD FORMED (CAST-IN-PLACE)
CONCRETE WALL, PAINTED
- 5 CONCRETE CAST-IN-PLACE (CIP)
BASE/TORUS MOULDING DETAIL, SEE
5/TP.10

- | | |
|----|--|
| 13 | DECORATIVE METAL VENT |
| 14 | CONCRETE CIP DOOR TRIM (JAMB |
| 15 | OVERHEAD STEEL ROLL-UP DOOR
W/ METAL WALL CHANNEL |
| 16 | "BULLET" BOLLARD |
| 17 | BUILT-UP ROOFING |
| 18 | ROOF OVERFLOW DRAIN |
| 19 | ROOF DRAIN / DOWNSPOUT |
| 20 | METAL SASH SKYLIGHT |

- | | |
|----|--|
| 28 | 2x14 ROOF RAFTERS @2'-0" O.C. |
| 30 | (2) 3X14 WOODEN "BOW" TRUSSES |
| 31 | ¾" STEEL TIE RODS |
| 32 | STEEL ANGLE |
| 33 | 3X8 ROOF RAFTER |
| 34 | 2X6 WOOD CROSS BRIDGING |
| 35 | WOOD DOOR |
| 36 | 2x4 ACOUSTIC LAY IN CEILING (WITH PLASTER CEILING ABOVE) |

- | | |
|----|--|
| 45 | 1X6 DOUGLAS FIR SHEATHING |
| 46 | METAL ROOF ACCESS LADDER |
| 47 | TILE FLOORING |
| 48 | STEEL DOOR |
| 49 | 2X8 ROOF RAFTER |
| 50 | CIP CONCRETE HORIZ. BANDING |
| 51 | RECESSED WALL CAVITY W/ UTILITY CONNECTION (APPROX. 7.5" DEEP) |

ARCHITECT:
UNION ARCHITECTURE, INC.
JOHN H. EISENHART, ARCHITECT
EVA THORN, INTERIORS
3434 22ND STREET, SAN DIEGO, CA 92102
TELEPHONE: 619.788.2882 / WWW.UNIONARCH.COM

DATE: 09.13.2019
REV: 03.20.2020

TP. 9

SHEET 11 OF 12

**9G
PLANNED DEVELOPMENT PERMIT
PACKAGE**

185 W F St suite 500, San Diego, CA 92101
phone 619.239.2353

ISSUES:

PROJECT NO:
5937.00

FILE NAME:
BIM 360://5937.00-Cisterra
-9G/ARCH_Cisterra9G_2018.3.rvt

DRAWN BY: CHECKED BY:
Author Checker

PLOT DATE:
6/9/2020 2:03:54 PM

TITLE:

TP.9

7.20.20

57

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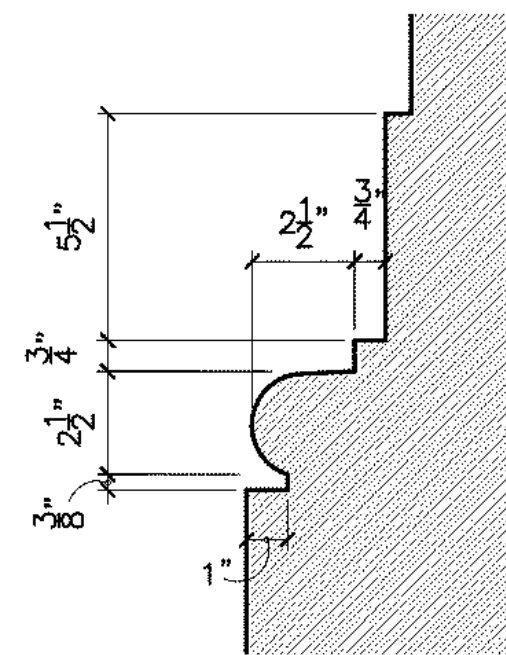
EXISTING EXTERIOR WALLS AT NORTH, EAST AND WEST ARE TO REMAIN IN PLACE.

PRIOR TO ANY DEMO WORK
WALLS SHALL BE STABILIZED
PER PROJECT STRUCTURAL
ENGINEER'S RECOMMENDATION
AND ANY DETAILING SHALL BE
PROTECTED FROM POTENTIAL
DAMAGE DURING WORK (I.E.
CHIPPING DAMAGE ETC.), COVER
WITH PLYWOOD. PROVIDE
TEMPORARY CORNER GUARDS AT
OPENINGS, PARAPET ETC.

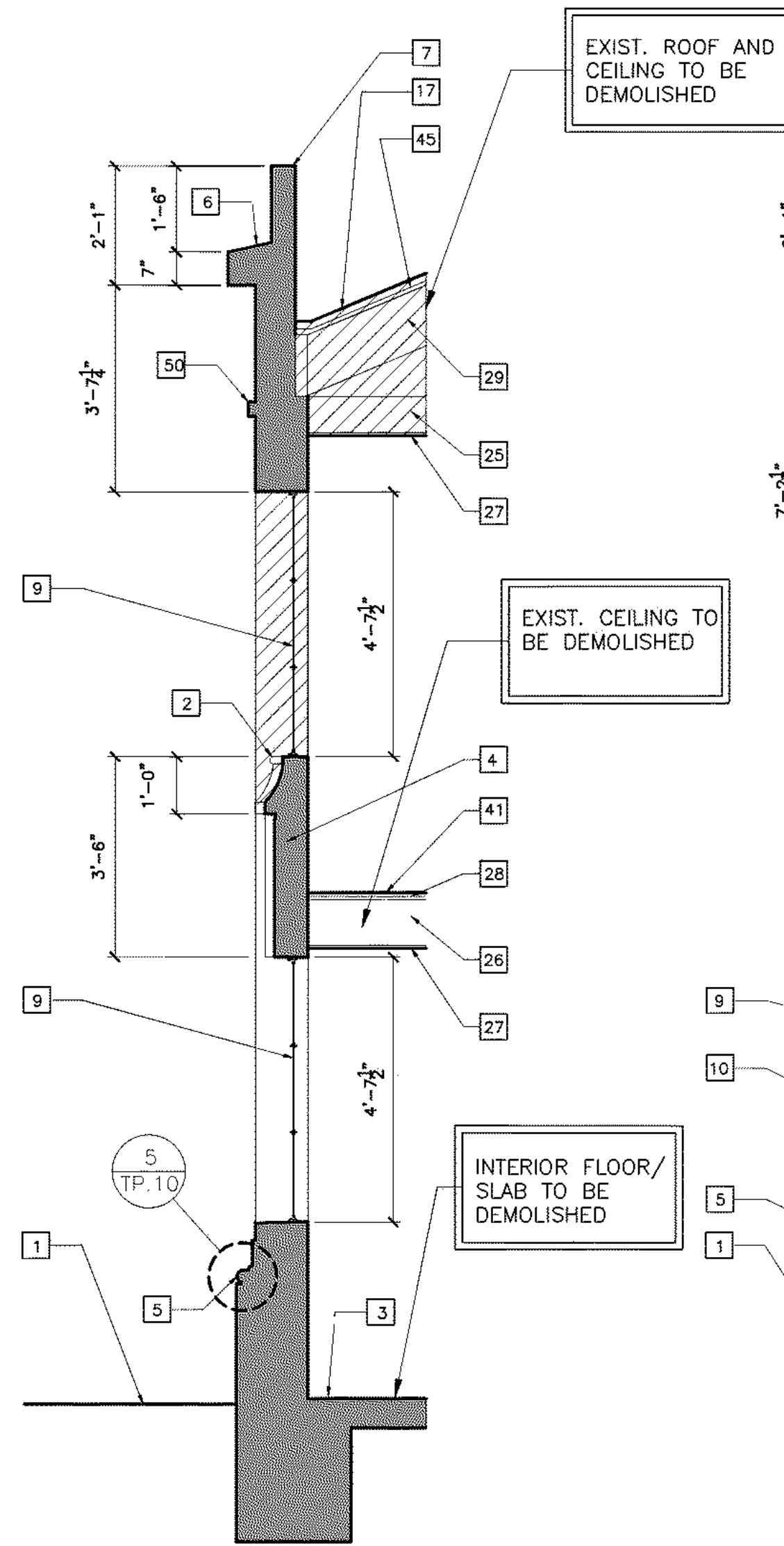
ORIGINAL STEEL WINDOWS AND GLAZING SHOULD BE PROTECTED IN PLACE DURING CONSTRUCTION.

FOR CONCRETE REHABILITATION
WORK SEE NATIONAL PARK
SERVICE PRESERVATION BRIEF
#15.

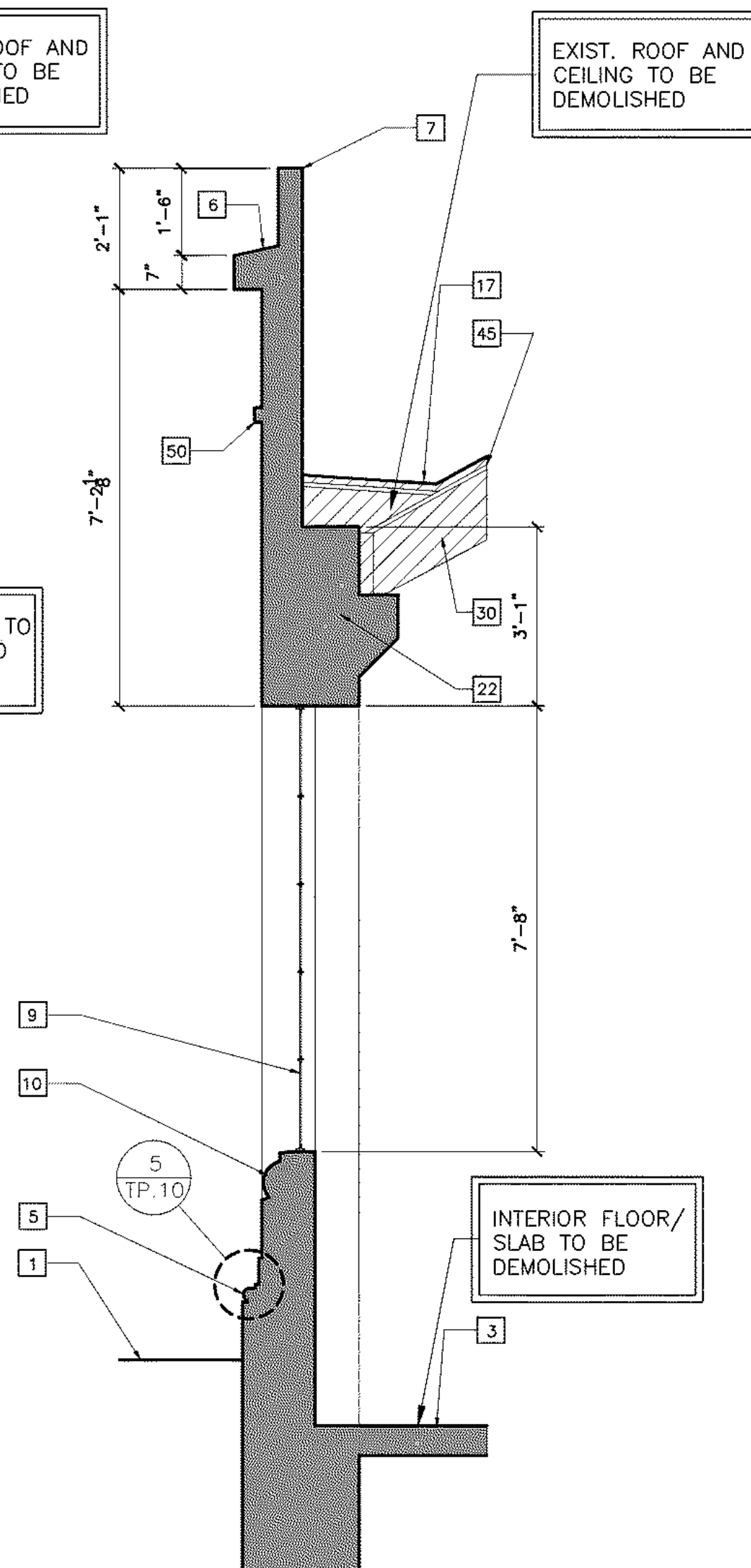
FOR STEEL WINDOW
REHABILITATION WORK SEE
NATIONAL PARK SERVICE
PRESERVATION BRIEF #13.



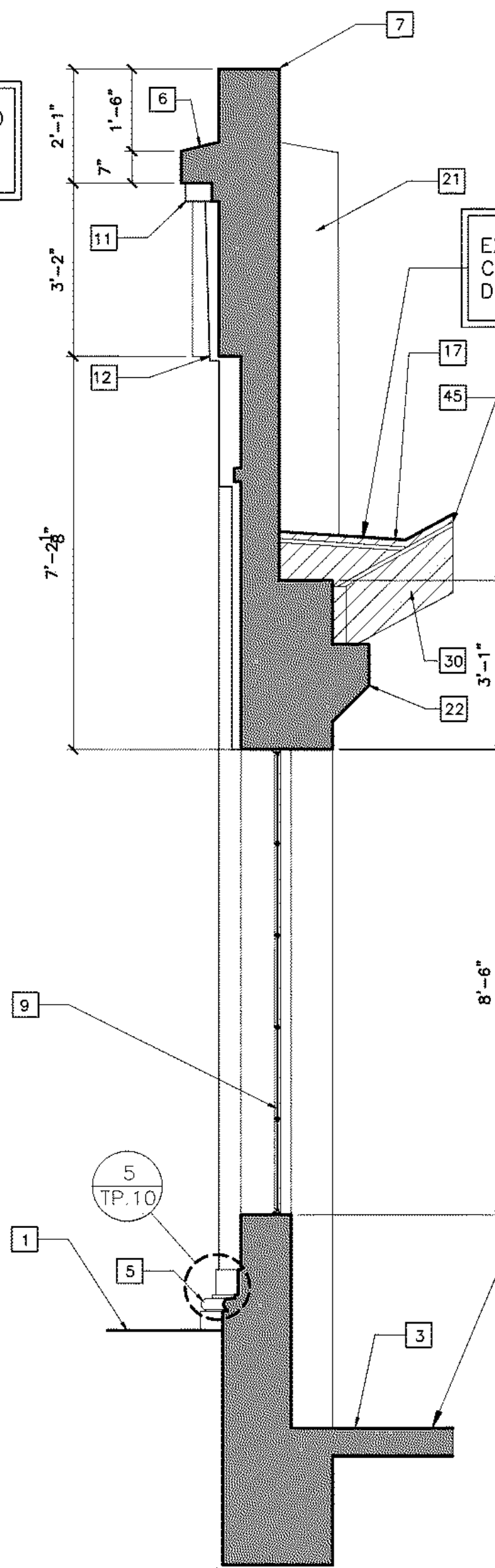
5 TYP. WALL BASE
TP.10
3" = 1'-0"



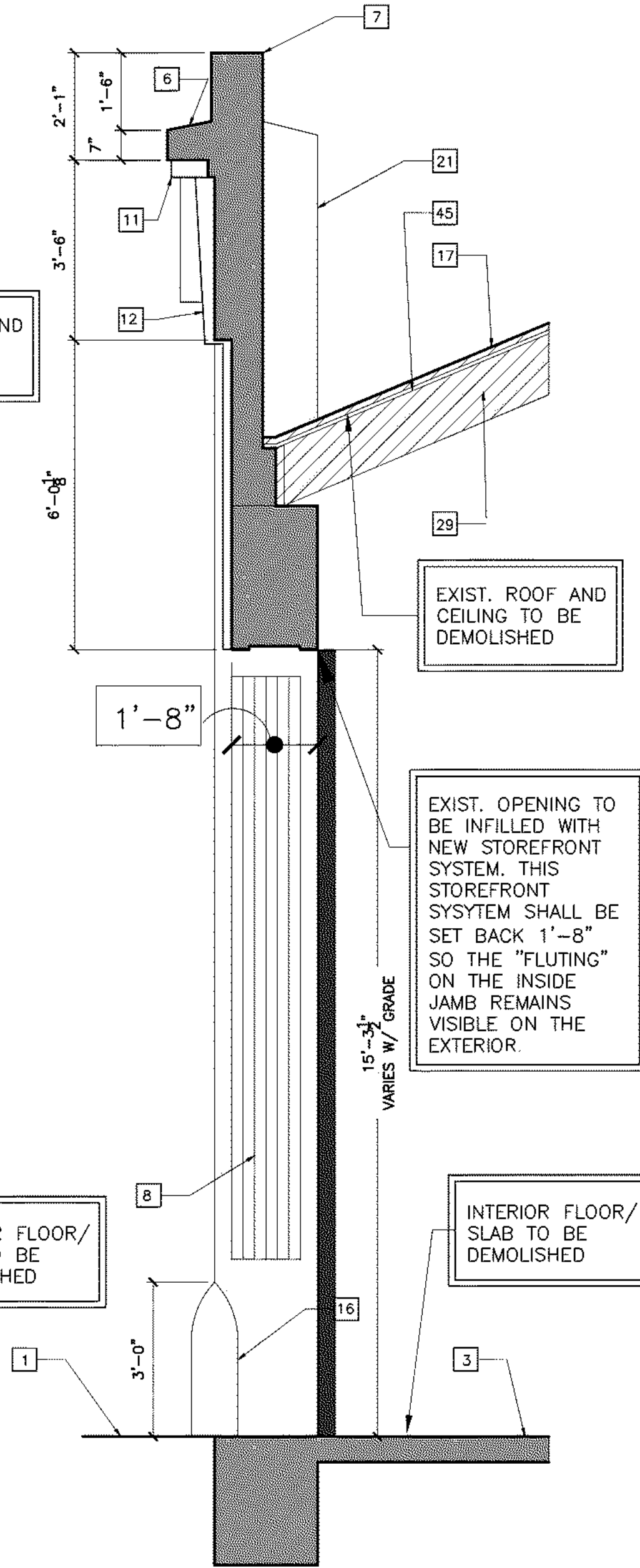
4 WALL SECTION F
TP.10 1/2" = 1'-0"



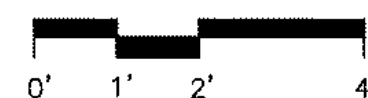
3 WALL SECTION E
TP.10 1/2" = 1'-0"



2 WALL SECTION D
TP.10 1/2" = 1'-0"



1 WALL SECTION C
TP.10 1/2" = 1'-0"



NOTE:

- A. THE 1932 BUILDING IS ORIGINAL EXCEPT FOR:
1. THE METAL OVERHEAD ROLL-UP DOORS.
 2. THE WIRE MESH WINDOW GUARDS
- B. CIP : ABBR. CAST-IN PLACE CONCRETE

KEYNOTES

- 1 CONCRETE SIDEWALK / GRADE
- 2 KEYSTONE TYPE DECORATIVE
CAST-IN PLACE CONCRETE MouldING
- 3 CONCRETE FLOOR
- 4 BOARD FORMED (CAST-IN-PLACE)
CONCRETE WALL, PAINTED
- 5 CONCRETE CAST-IN-PLACE (CIP)
BASE/TORUS MouldING DETAIL, SEE
5/TP.10

- | | |
|----|--|
| 6 | CONCRETE (CIP) MOULDED CORNICE BAND |
| 7 | PARAPET (CIP CONCRETE) |
| 8 | "FLUTING" DETAIL AT PIER (CIP CONCRETE) |
| 9 | STEEL SASH WINDOWS W/ AWNING FUNCTION (SEE DTLS. 1 + 2/TP.9) |
| 10 | BEAD MOULDING CONCRETE WINDOW SILL (CIP) |
| 11 | RECTANGULAR BLOCK CIP CONCRETE |
| 12 | CIP VERTICAL CONCRETE STRIP DECORATIVE RELIEF (PROTRUDING FROM WALL PLANE) |

- | | |
|----|--|
| 13 | DECORATIVE METAL VENT |
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| 16 | "BULLET" BOLLARD |
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| 18 | ROOF OVERFLOW DRAIN |
| 19 | ROOF DRAIN / DOWNSPOUT |
| 20 | METAL SASH SKYLIGHT |

- | | |
|----|-----------------------------|
| 21 | CONCRETE CIP PIER |
| 22 | CONCRETE CIP BEAM / CORBEL |
| 23 | METAL HANDRAIL |
| 24 | 2X4 STUD WALL @ 16" O.C. |
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| 26 | 2X10 FLOOR JOIST @ 16" O.C. |
| 27 | PLASTER FINISH WALL |
| 28 | 7/8" WOOD SHEATHING |

- | | |
|----|--|
| 29 | 2x14 ROOF RAFTERS @2'-0" O.C. |
| 30 | (2) 3x14 WOODEN "BOW" TRUSSES |
| 31 | ¾" STEEL TIE RODS |
| 32 | STEEL ANGLE |
| 33 | 3X8 ROOF RAFTER |
| 34 | 2X6 WOOD CROSS BRIDGING |
| 35 | WOOD DOOR |
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- 37) 1X WOODEN TREAD AT STAIR
- 38) WROUGHT IRON RAILING
- 39) STEEL "C" CHANNEL
- 40) CORRUGATED METAL CANOPY (OVERHANG)
- 41) VINYL FLOORING
- 42) STEEL WINDOWS
- 43) METAL MESH WINDOW GUARDS
- 44) BRICK

- 45 1X6 DOUGLAS FIR SHEATHING
46 METAL ROOF ACCESS LADDER
47 TILE FLOORING
48 STEEL DOOR
49 2X8 ROOF RAFTER
50 CIP CONCRETE HORIZ. BANDING
51 RECESSED WALL CAVITY W/ UTILITY CONNECTION (APPROX. 7.5" DEEP)

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DATE: 09.13.2019
REV: 03.20.2020

TP.10

SHEET 12 OF 12

TREATMENT PLAN

PROJECT : THE PACIFIC TELEPHONE & TELEGRAPH CO. GARAGE
HRB #1355 TREATMENT PLAN
660 10TH AVE. AND 559 9TH AVE., SAN DIEGO, CA 92101
CLIENT : CISTERRA PARTNERS, LLC
35580 CARMEL MOUNTAIN ROAD, STE. # 460, SAN DIEGO, CA 92130

ARCHITECT: UNION ARCHITECTURE, INC.
JOHN H. EISENHART, ARCHITECT
EVA THORN, INTERIORS
4344 22ND STREET, SAN DIEGO, CA 92102
TELEPHONE: 619 798 2862 / WWW.UNIONARCH.COM

3rd SUBMITTAL
6.9.20

ISSUES:

PROJECT NO.
5937 00

FILE NAME:

BIM 360://5937.00-Cisterra
-9G/ARCH_Cisterra9G_2018.3.rvt

DRAWN BY: **CHECKED BY:**
Author Checker

PLOT DATE:
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TITLE:

TP.10

7.20.20

58

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Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AWP-9554-OE

Issued Date: 09/12/2019

Paul Thometz
Cisterra
3580 Carmel Mountain road
San Diego, CA 92130

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building 9G Tower
Location: San Diego, CA
Latitude: 32-42-44.80N NAD 83
Longitude: 117-09-22.85W
Heights: 47 feet site elevation (SE)
321 feet above ground level (AGL)
368 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does exceed obstruction standards but would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, red lights - Chapters 4,5(Red),&12.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

☐ At least 10 days prior to start of construction (7460-2, Part 1)
☒ Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

This determination expires on 03/12/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, with



Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AWP-9907-OE

Issued Date: 09/25/2019

Paul Thometz
Cisterra
3580 Carmel Mountain road
San Diego, CA 92130

****DETERMINATION OF NO HAZARD TO AIR NAVIGATION FOR TEMPORARY STRUCTURE****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Crane 9G Tower
Location: san diego, CA
Latitude: 32-42-43.57N NAD 83
Longitude: 117-09-21.89W
Heights: 45 feet site elevation (SE)
365 feet above ground level (AGL)
410 feet above mean sea level (AMSL)

This aeronautical study revealed that the temporary structure does exceed obstruction standards but would not be a hazard to air navigation provided the condition(s), if any, in this letter is (are) met:

****SEE ATTACHMENT FOR ADDITIONAL CONDITION(S) OR INFORMATION****

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of a structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this temporary structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

A copy of this determination will be forwarded to the Federal Aviation Administration Flight Procedures Office if the structure is subject to the issuance of a Notice To Airman (NOTAM).

6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (424) 405-7643, or karen.mcdonald@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AWP-9554-OE.

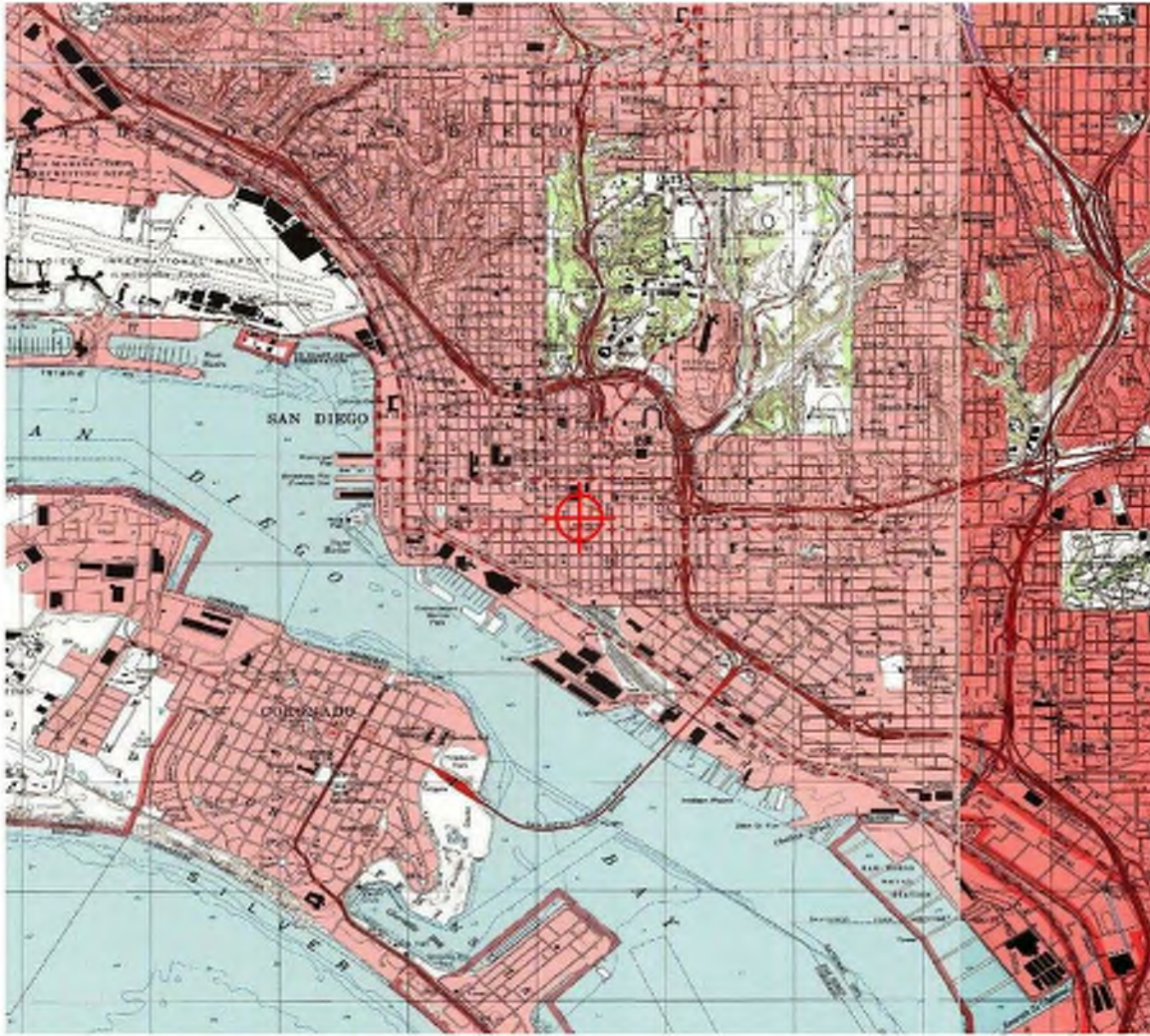
Signature Control No: 414607185-417125244

Karen McDonald
Specialist

Attachment(s)
Map(s)

(EBO)

TOPO Map for ASN 2019-AWP-9554-OE



Additional Condition(s) or Information for ASN 2019-AWP-9907-OE

Proposal: To construct and/or operate a(n) Crane to a height of 365 feet above ground level, 410 feet above mean sea level.

Location: The structure will be located 2.13 nautical miles southeast of SAN Airport reference point.

Part 77 Obstruction Standard(s) Exceeded and Aeronautical Impacts, if any:

Section 77.17 (a) (2) by 165 feet - a height that exceeds 245 feet above mean sea level within 2.13 nautical miles of SAN.

Section 77.17 (a) (5) a height that affects an Airport Surface by penetrating:

Section 77.19 (a) Horizontal Surface by 244 feet as applied to SAN.

Preliminary FAA study indicates that the above mentioned structure would:

have no effect on any existing or proposed arrival, departure, or en route instrument/visual flight rules (IFR/ VFR) minimum flight altitudes.
not exceed traffic pattern airspace
have no physical or electromagnetic effect on the operation of air navigation and communications facilities.
have no effect on any airspace and routes used by the military.

Based on this aeronautical study, the structure would not constitute a substantial adverse effect on aeronautical operations or procedures because it will be temporary. The temporary structure would not be considered a hazard to air navigation provided all of the conditions specified in this determination are strictly met.

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, flags/red lights - Chapters 3(Marked),4,5(Red),&12.

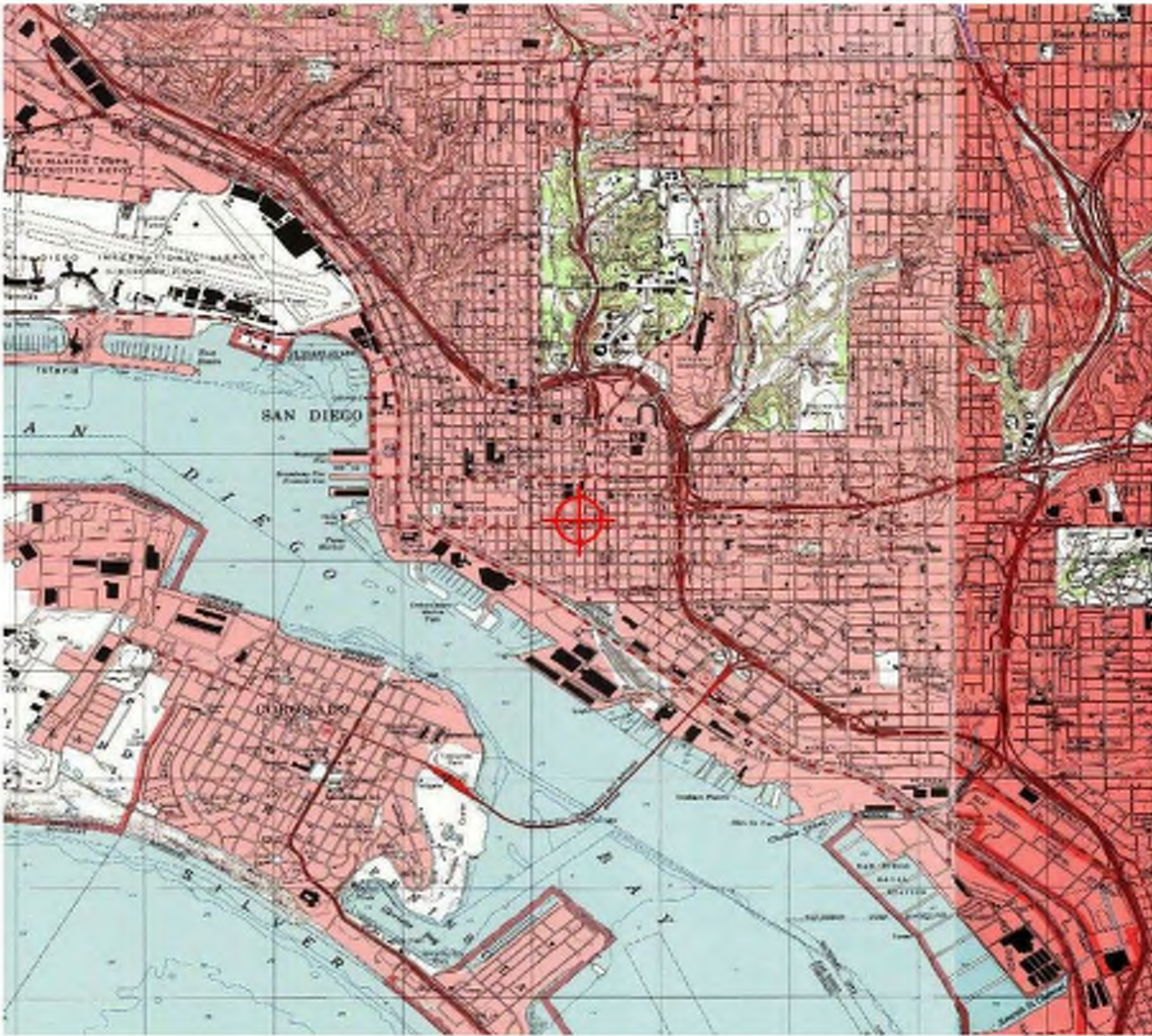
Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that the manager of SAN DIEGO INTL @ (619) 400-2761 be notified at least 3 business days prior to the temporary structure being erected and again when the structure is removed from the site.

This determination expires on 03/25/2021 unless extended, revised, or terminated by the issuing office.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

TOPO Map for ASN 2019-AWP-9907-OE



3rd SUBMITTAL
6.9.20

ISSUES:

PROJECT NO:
5937.00
FILE NAME:
B:\M 5937.00-Cisterra
-95047CH - Cisterra\05_2018.3.rvt
DRAWN BY: CHECKED BY:
Author Checker
PLOT DATE:
6/9/2020 2:03:56 PM
TITLE:

FAA

59

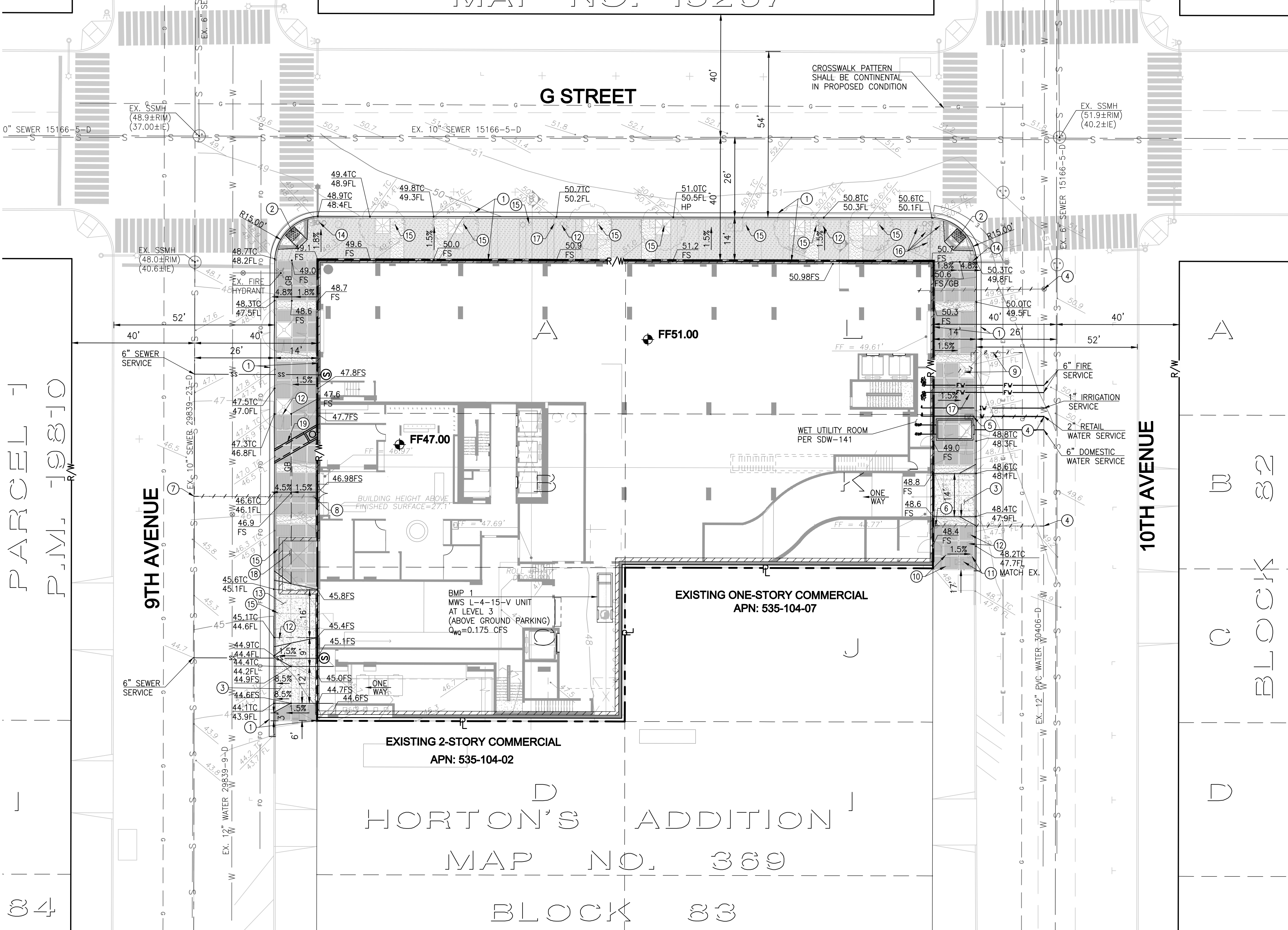
9G
PLANNED DEVELOPMENT PERMIT
PACKAGE



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LEGAL DESCRIPTION

THE LAND REFERRED TO HEREIN BELOW IS SITUATED IN THE CITY OF SAN DIEGO, IN THE COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOW:

LOTS A, B, C, K AND L IN BLOCK 83 OF HORTON'S ADDITION, IN THE CITY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO MAP THEREOF ON FILE IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY.

APN: 535-136-01-00

GRADING QUANTITIES

TOTAL SITE AREA: 0.575 AC
DISTURBED AREA: 0.575 AC
AMOUNT OF CUT: 21,500 CY
MAX. DEPTH OF CUT: 23 FT
AMOUNT OF FILL: 0 CY
MAX. DEPTH OF FILL: 0 FT
AMOUNT OF EXPORT: 21,500 CY

GRADING QUANTITIES ARE APPROXIMATE AND SUBJECT TO CHANGE BASED ON FINAL DESIGN. QUANTITIES SHALL NOT BE USED FOR BIDDING PURPOSES.

THE BASIS OF BEARINGS

THE BASIS OF BEARINGS OF THIS SURVEY IS THE CENTERLINE OF "G" STREET AS SHOWN ON MAP 15267, I.E. N 89°51'44"W

TOPOGRAPHY SOURCE

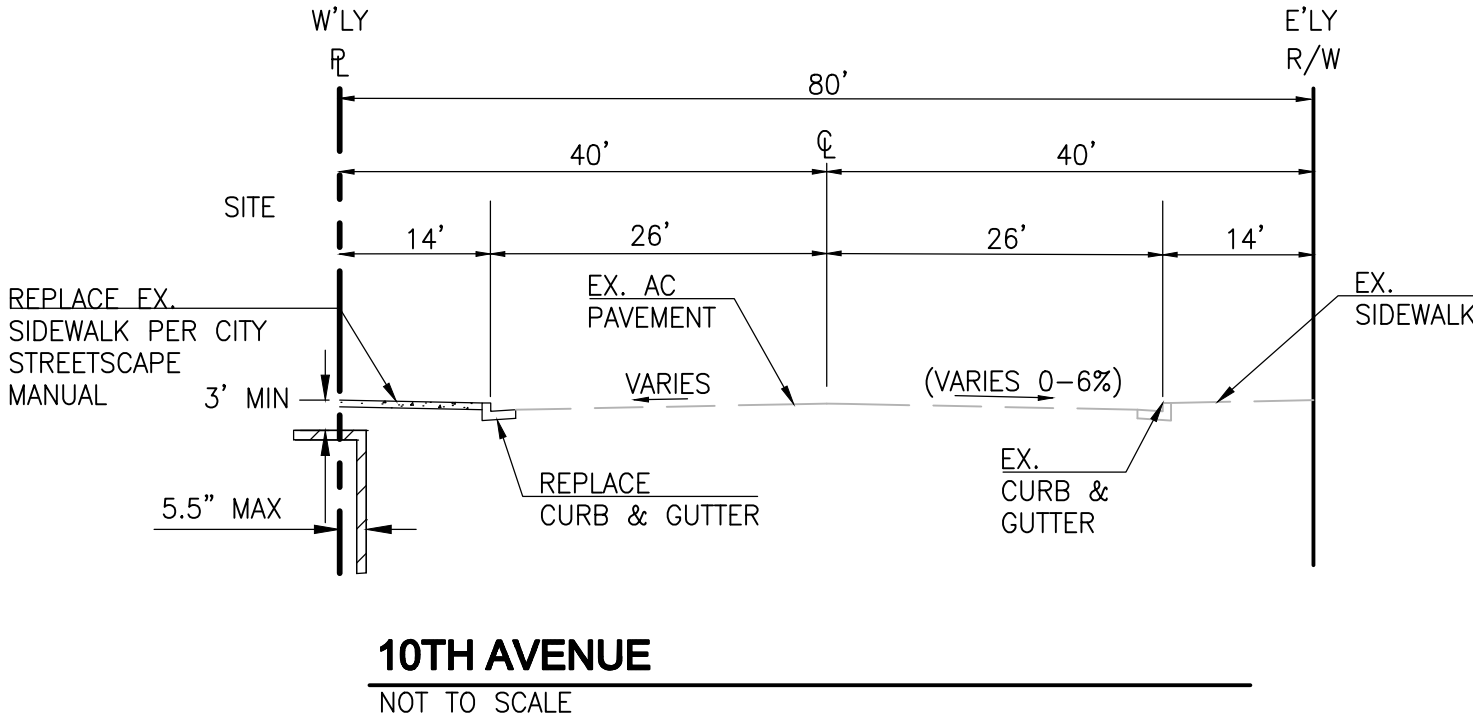
SAN DIEGO LAND SURVEYING & ENGINEERING, INC
9665 CHESAPEAKE DRIVE, SUITE 445
SAN DIEGO, CA 92123
ALTA/NSPS LAND TITLE SURVEY DATED MAY 23, 2019

LEGEND

PROPERTY LINE/ TM BOUNDARY	---
RIGHT OF WAY	---
EXISTING LOT LINE	---
CENTERLINE	---
EXISTING IMPROVEMENTS	
WATER	---
SEWER	---
GAS	---
FIBER OPTICS	---
ELECTRICAL	---
TRAFFIC SIGNAL	---

PROPOSED IMPROVEMENTS

WATER SERVICE	---
IRRIGATION SERVICE	---
SEWER LATERAL	---
STORM DRAIN	---
FIRE SERVICE	---
CURB AND GUTTER	---
BACKFLOW PREVENTER	---
WATER METER VAULT WITH DUAL 4" METERS	---
TREE GRATE	---
STREET LIGHT	---

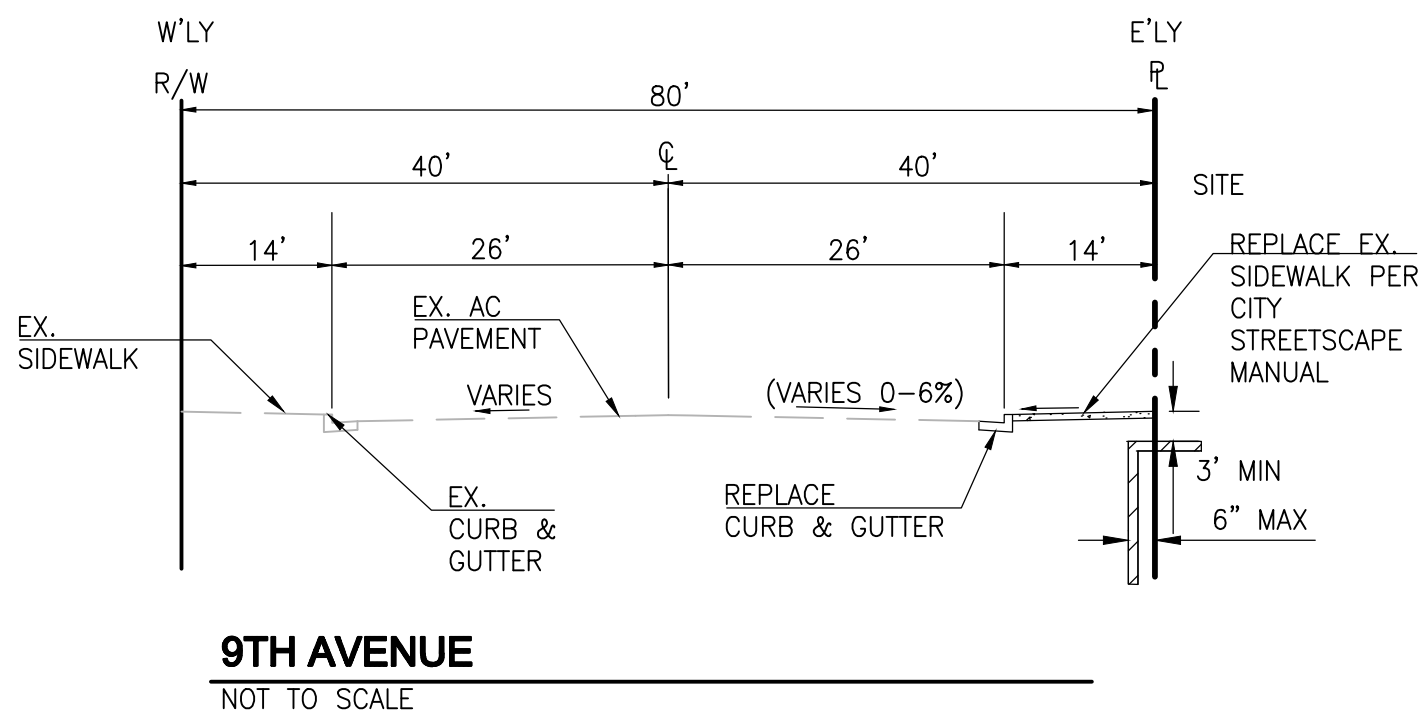
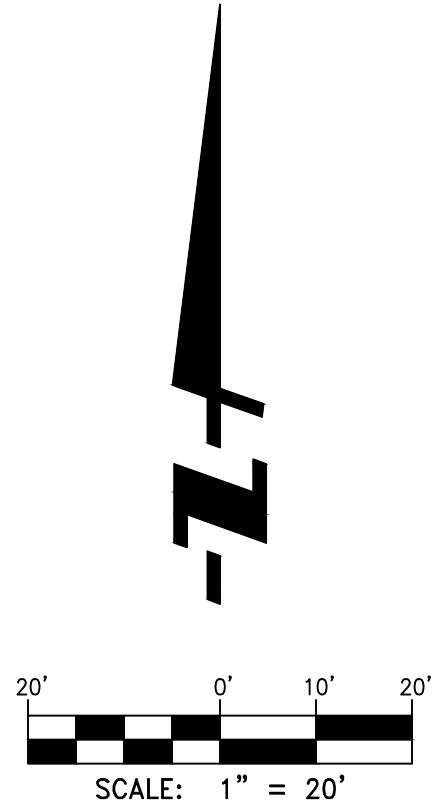
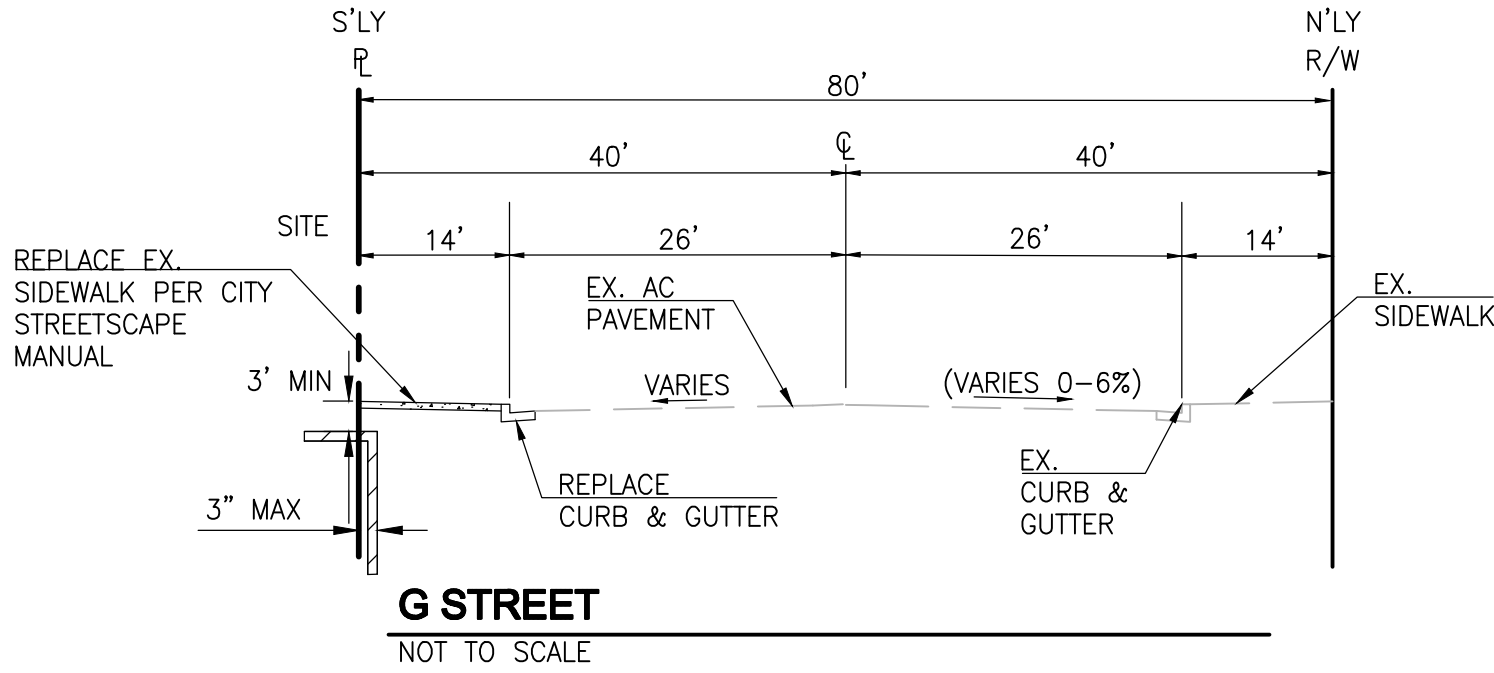


NOTES

- FOR STREETSCAPE, STREET LIGHTS, AND STREET TREES REFER TO ARCHITECT DRAWINGS
- IF A 3" OR LARGER METER IS REQUIRED FOR THIS PROJECT, THE OWNER/PERMITEE SHALL CONSTRUCT A NEW METER VAULT BELOW GROUND WITHIN THE PUBLIC ROW IN A MANNER SATISFACTORY TO THE PUBLIC UTILITIES DIRECTOR AND CITY ENGINEER
- ALL PROPOSED WATER AND SEWER FACILITIES (PUBLIC AND PRIVATE) WITHIN THE PUBLIC ROW OR PUBLIC EASEMENT MUST BE DESIGNED, CONSTRUCTED, OR ABANDONED IN ACCORDANCE WITH THE CRITERIA ESTABLISHED WITHIN THE CITY OF SAN DIEGO'S CURRENT WATER AND SEWER FACILITY DESIGN GUIDELINES, REGULATIONS, STANDARDS, AND PRACTICES PERTAINING THERETO.
- ALL WATER LINES SERVICING THIS DEVELOPMENT MUST PASS THROUGH A PERMITTED, PRIVATE, ABOVE GROUND, BACKFLOW PREVENTION DEVICE (BFPD)
- THE OWNER/PERMITEE SHALL BE RESPONSIBLE FOR ANY DAMAGE CAUSED TO CITY OF SAN DIEGO WATER AND SEWER FACILITIES IN THE VICINITY OF THE PROJECT SITE, DUE TO THE CONSTRUCTION ACTIVITIES ASSOCIATED WITH THIS PROJECT, IN ACCORDANCE WITH MUNICIPAL CODE SECTION 142.0607 IN THE EVENT THAT ANY SUCH FACILITY LOSES INTEGRITY THEN, THE OWNER/PERMITEE SHALL REPAIR OR RECONSTRUCT ANY DAMAGED PUBLIC WATER AND SEWER FACILITY IN A MANNER SATISFACTORY TO THE PUBLIC UTILITIES DIRECTOR AND THE CITY ENGINEER
- THE PROPOSED PROJECT WILL COMPLY WITH ALL THE REQUIREMENTS OF THE CURRENT CITY OF SAN DIEGO STORM WATER STANDARDS MANUAL BEFORE A GRADING OR BUILDING PERMIT IS ISSUED. IT IS THE RESPONSIBILITY OF THE OWNER/DESIGNER/APPLICANT TO ENSURE THAT THE CURRENT STORM WATER PERMANENT BMP DESIGN STANDARDS ARE INCORPORATED INTO THE PROJECT.
- PRIOR TO THE ISSUANCE OF ANY CONSTRUCTION PERMIT, THE OWNER/PERMITEE SHALL ENTER INTO A MAINTENANCE AGREEMENT FOR THE ONGOING PERMANENT BMP MAINTENANCE, SATISFACTORY TO THE CITY ENGINEER.
- PRIOR TO THE ISSUANCE OF ANY CONSTRUCTION PERMIT, THE OWNER/PERMITEE SHALL INCORPORATE ANY CONSTRUCTION BEST MANAGEMENT PRACTICES NECESSARY TO COMPLY WITH CHAPTER 14, ARTICLE 2, DIVISION 1 (GRADING REGULATIONS) OF THE SAN DIEGO MUNICIPAL CODE, INTO THE CONSTRUCTION PLANS OR SPECIFICATIONS.
- PRIOR TO THE ISSUANCE OF ANY CONSTRUCTION PERMIT, THE OWNER/PERMITEE SHALL SUBMIT A WATER POLLUTION CONTROL PLAN (WPCP). THE WPCP SHALL BE PREPARED IN ACCORDANCE WITH THE GUIDELINES IN PART 2 CONSTRUCTION BMP STANDARDS CHAPTER 4 OF THE CITY'S STORM WATER STANDARDS.

KEYNOTES

- REPLACE EXISTING CURB, GUTTER, AND SIDEWALK PER CITY STREETSCAPE MANUAL
- REPLACE EXISTING CURB RAMP PER SDG-132
- DRIVEWAY PER SDG-159, WIDTH PER PLAN
- EXISTING WATER SERVICE TO BE KILLED AT MAIN
- EXISTING WATER METER TO BE REMOVED
- EXISTING FDC TO BE REMOVED
- EXISTING SEWER LATERAL, TO BE ABANDONED AT THE PL
- EXISTING SEWER CLEANOUT TO BE REMOVED
- EXISTING GAS SERVICE AND METER TO BE REMOVED
- EXISTING SDG&E TRANSFORMERS TO BE REMOVED/RELOCATED
- EXISTING COMMUNICATION AND ELECTRICAL BOX TO REMOVED/RELOCATED
- EXISTING STREETLIGHT AND BOX TO BE RELOCATED
- DRIVEWAY PER SDG-163, WIDTH PER PLAN
- EXISTING TRAFFIC SIGNAL TO REMAIN, PROTECT IN PLACE
- EXISTING PARKING METER TO BE RELOCATED
- EXISTING ELECTRICAL AND COMMUNICATION VAULTS AND ELECTRICAL RISER TO REMAIN, PROTECT IN PLACE
- EXISTING SIGN TO BE RELOCATED
- UNDERGROUND SDG&E VAULT
- CURB OUTLET PER SDRSD D-25A



ISSUES:
PRELIMINARY NOT FOR CONSTRUCTION

PROJECT NO:
1459-002
FILE NAME:
DRAWN BY: BC
CHECKED BY: BS
PLOT DATE:
07/02/2020
TITLE:

CONCEPTUAL
GRADING &
UTILITY
PLAN

Sheet C1 of C3

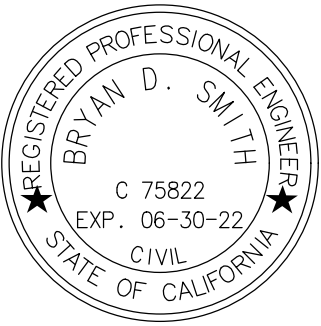
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C-1

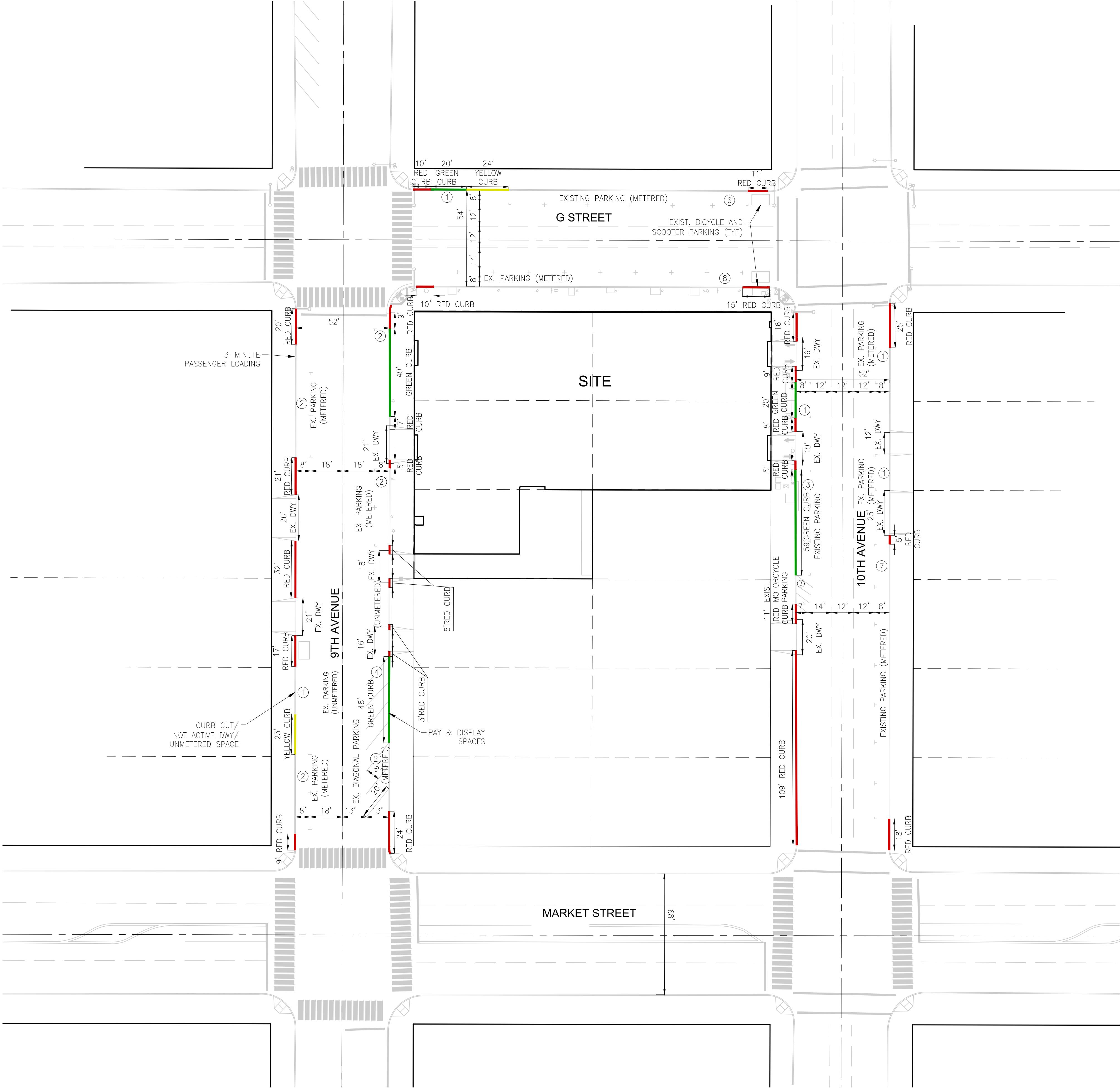
CARRIER JOHNSON + CULTURE
architecture + environments + brand strategy + graphics

185 W F St suite 500, San Diego, CA 92101
phone 619.239.2353

TENTATIVE MAP NO. 2365584
CONDITIONAL USE PERMIT NO. 2358007
SITE DEVELOPMENT PERMIT NO. 2358005
FUSCOE
ENGINEERING
6390 Greenwich Drive, Suite 170
San Diego, California 92122
tel 858.554.1500 • fax 858.597.0335
www.fuscoe.com



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CONCEPTUAL EXISTING CURB USAGE PLAN

LEGEND

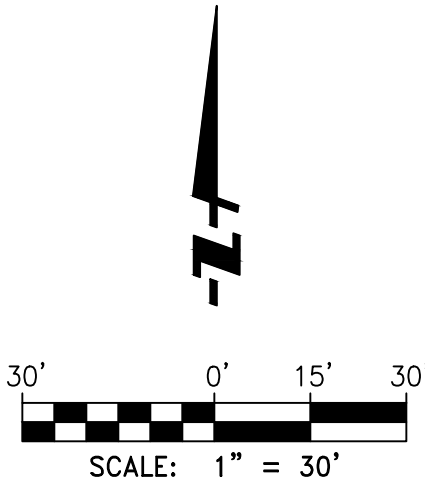
- PROPERTY LINE
- STREET CENTERLINE
- PARKING COUNT
- EXISTING DRIVEWAY

PARKING SUMMARY TABLE

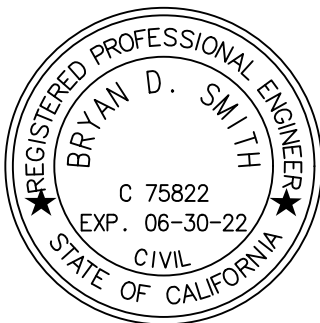
EXISTING PARKING SUMMARY				
STREET	METERED	UNMETERED	PARKING TYPE	
			TIME RESTRICTED (GREEN CURB)	TOTAL
G STREET	14	0	1	15
9TH AVENUE	8	2	6	16
10TH AVENUE	9	0	4	13
TOTAL PARKING COUNT	31	2	11	44

CURB DESIGNATIONS

- NO PARKING
- COMMERCIAL LOADING 20 MIN MAX
- TIME RESTRICTED (30 MIN, UNLESS OTHERWISE NOTED)



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San Diego, California 92122
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ISSUES: _____
PRELIMINARY NOT FOR CONSTRUCTION

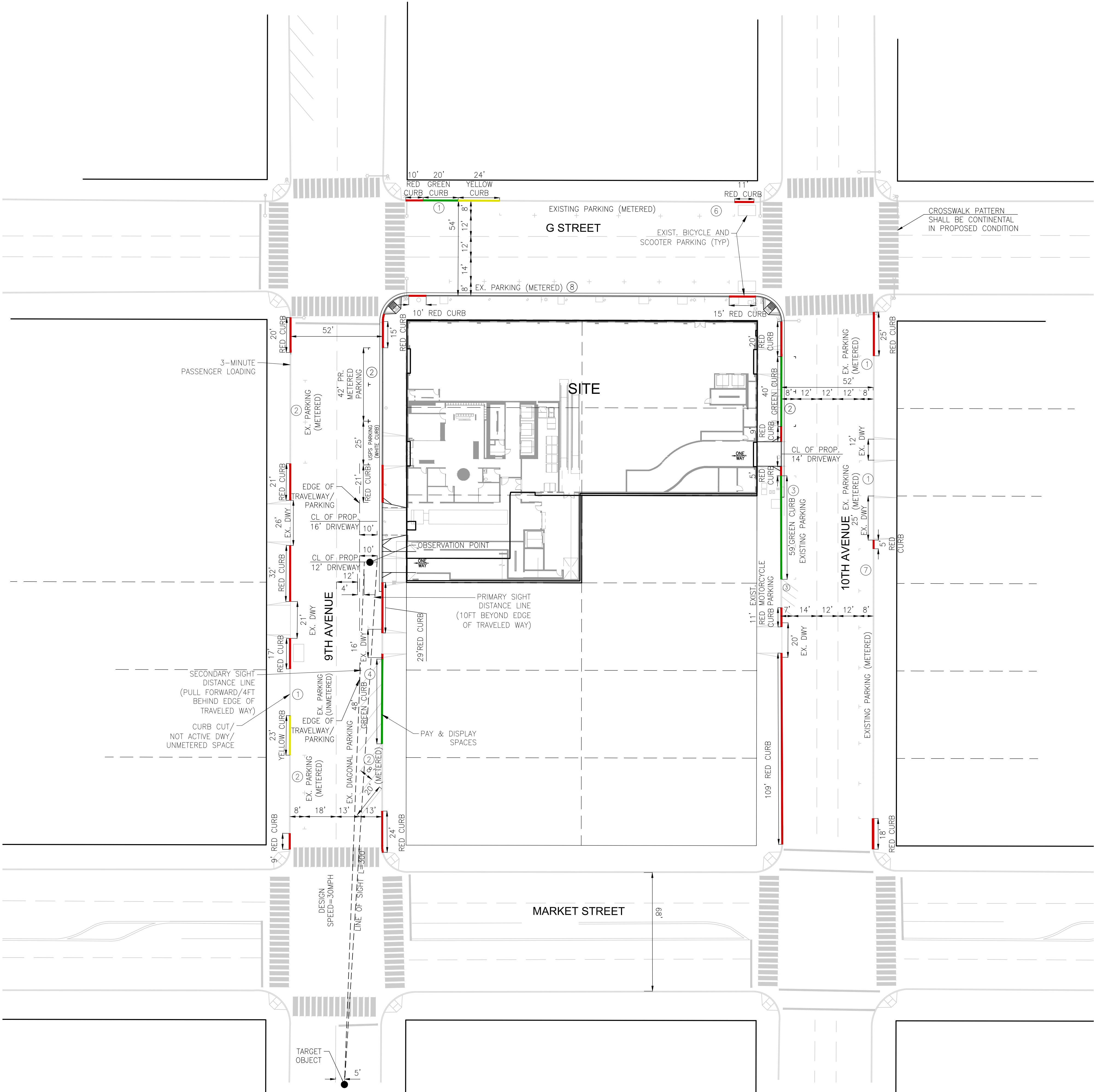
PROJECT NO: 1459-002
FILE NAME: _____
DRAWN BY: DM CHECKED BY: BS
PLOT DATE: 07/02/2020
TITLE: _____

CONCEPTUAL EXISTING CURB USAGE PLAN

Sheet C2 of C3

C-2
60a

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CONCEPTUAL PROPOSED CURB USAGE PLAN

LEGEND

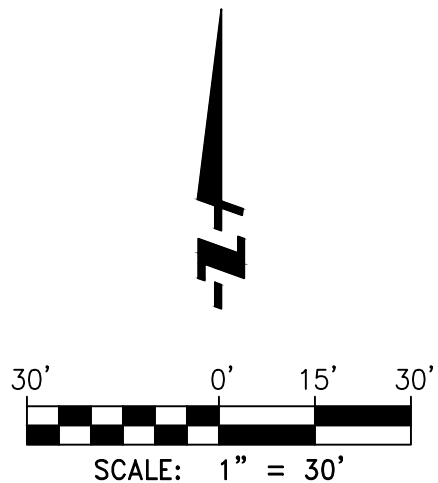
- PROPERTY LINE
- STREET CENTERLINE
- LINE OF SIGHT
- PARKING COUNT
- EXISTING DRIVEWAY

PARKING SUMMARY TABLE

PROPOSED PARKING SUMMARY				
STREET	METERED	UNMETERED	PARKING TYPE	
			TIME RESTRICTED (GREEN CURB)	TOTAL
G STREET	14	0	1	15
9TH AVENUE	8	2	4	14
10TH AVENUE	9	0	5	14
TOTAL PARKING COUNT	31	2	10	43

CURB DESIGNATIONS

- NO PARKING
- COMMERCIAL LOADING 20 MIN MAX
- TIME RESTRICTED (30 MIN, UNLESS OTHERWISE NOTED)



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San Diego, California 92122
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ISSUES:
PRELIMINARY NOT FOR CONSTRUCTION

PROJECT NO:
1459-002
FILE NAME:
DRAWN BY: DM
CHECKED BY: BS
PLOT DATE:
07/02/2020
TITLE:

CONCEPTUAL PROPOSED CURB USAGE PLAN

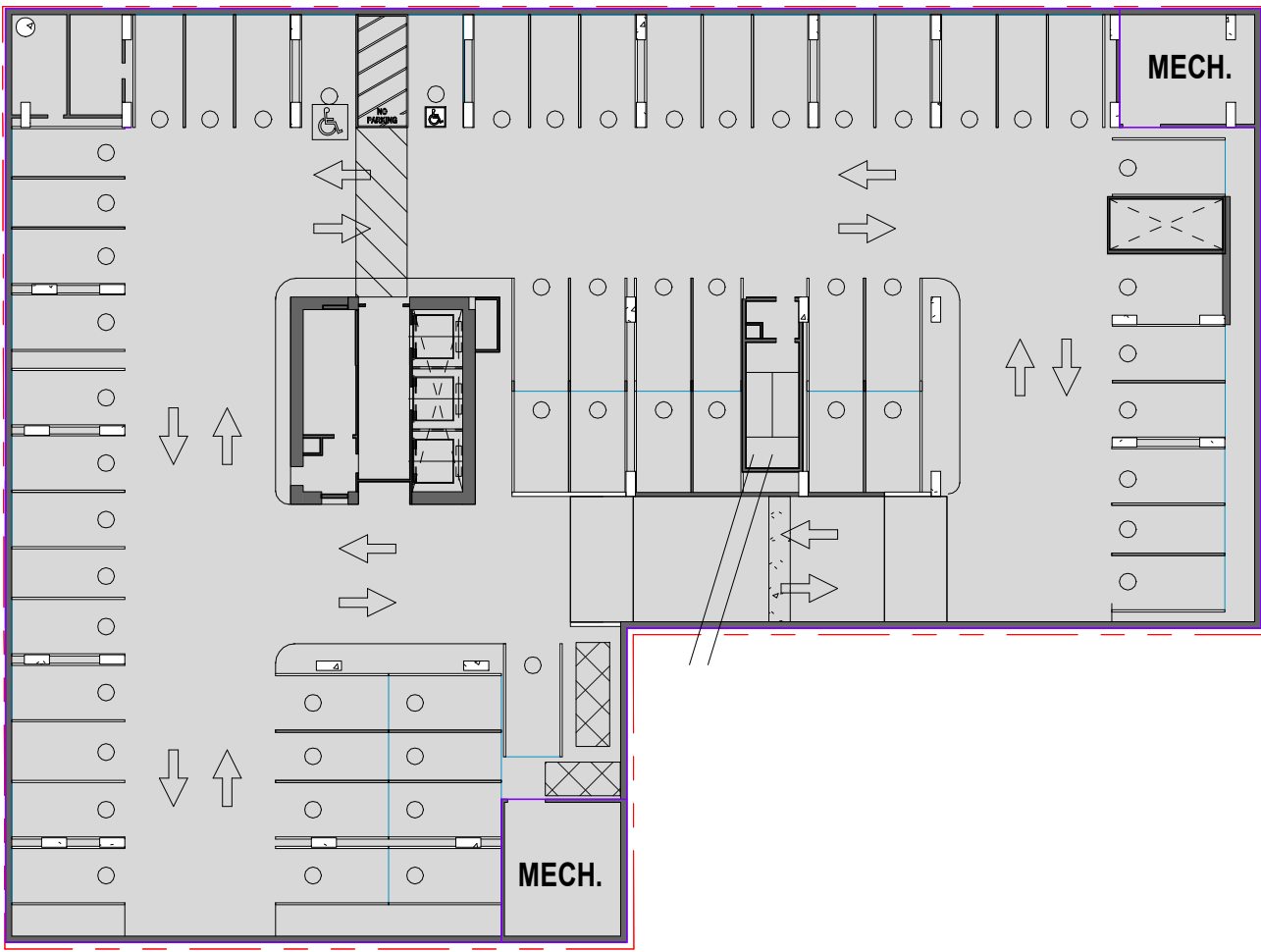
Sheet C3 of C3

C-3
60b

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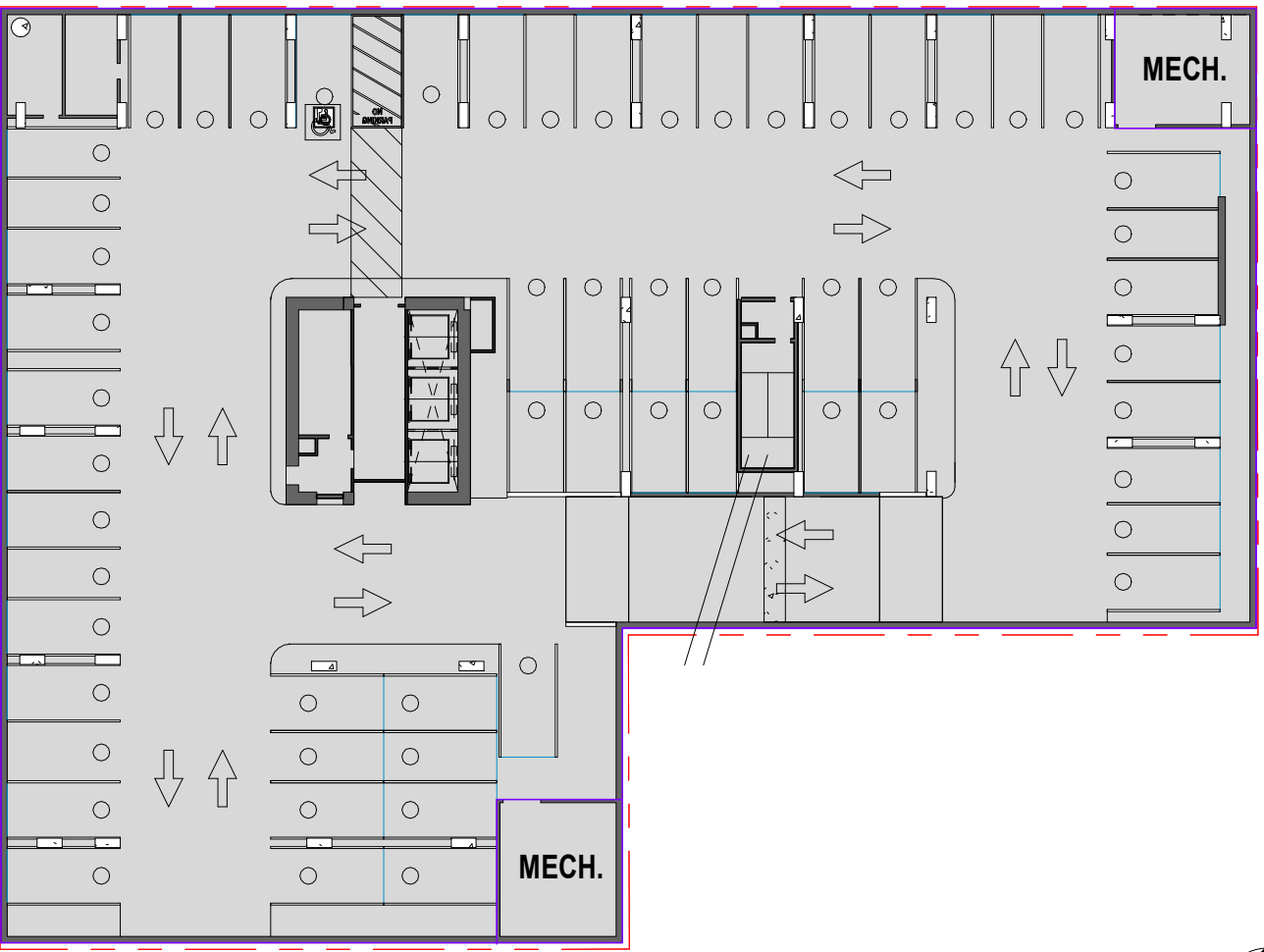
Level Breakdown
Level 3

Parking/Garage - EXEMPT



Level Breakdown
Level 4-5

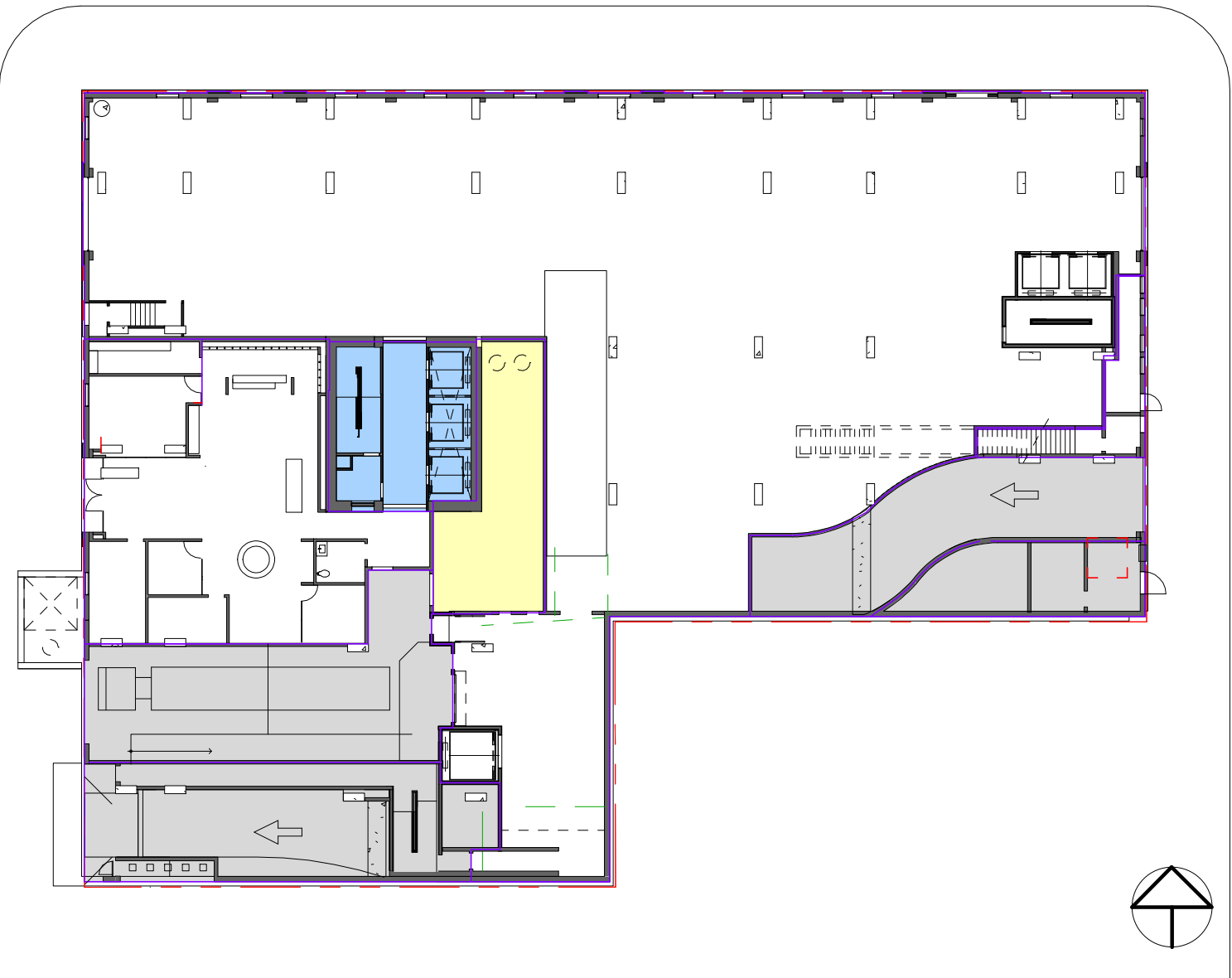
Parking/Garage - EXEMPT



Level Breakdown
Level 1

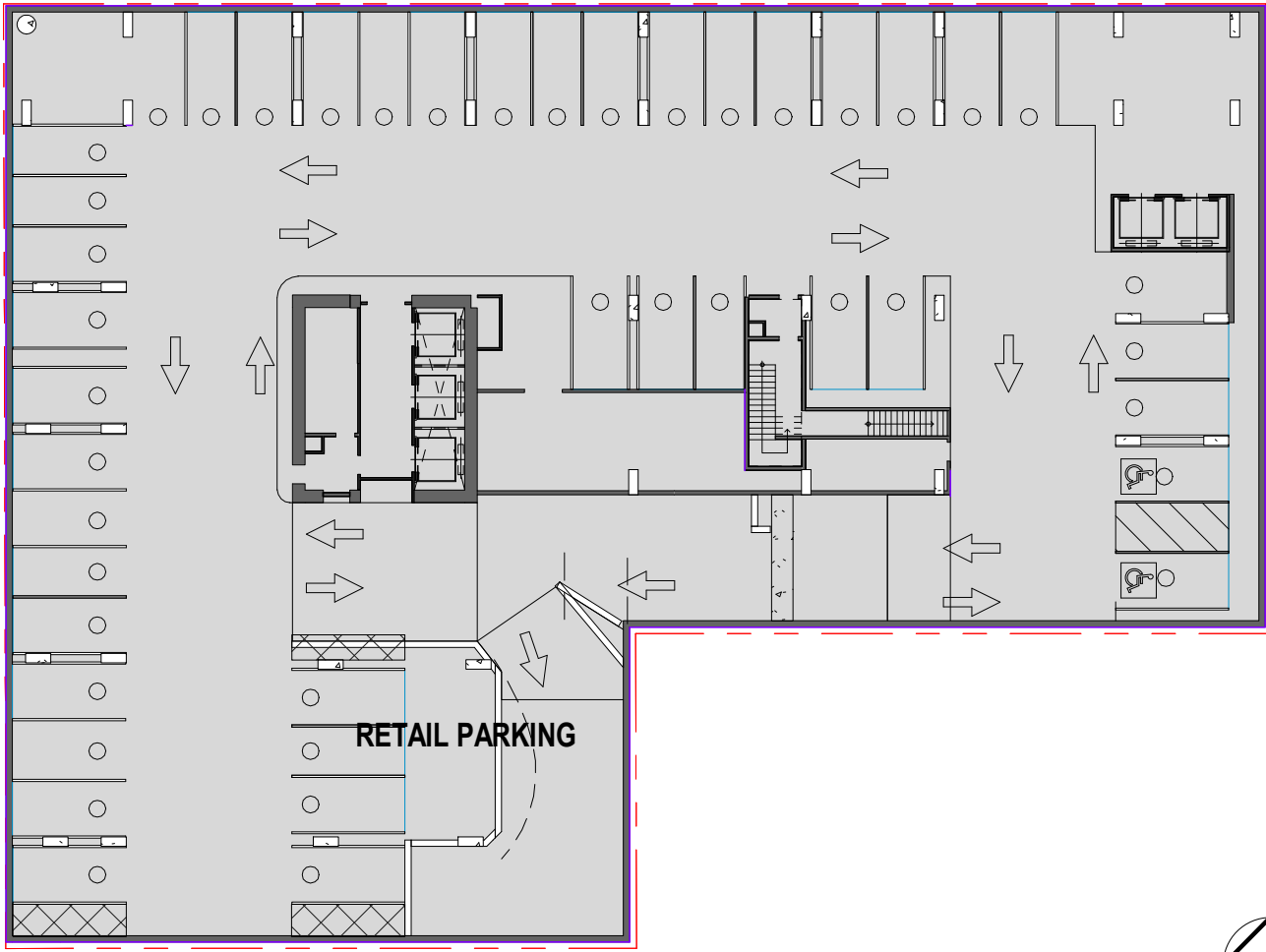
Retail/Office - 14,554 sf
Residential Lobby - 3,804 sf
Utilities - 1,872 sf
Parking/Garage - EXEMPT

Total - 22,112 sf



Level Breakdown
Level 2

Parking/Garage - EXEMPT



3rd SUBMITTAL
6.9.20

ISSUES:

PROJECT NO:
5937.00
FILE NAME:
B:\M 300\5937 00-Clients
-ARCH- C-Plans\05_2018.3.rvt
DRAWN BY: CHECKED BY:
Author Checker
PLOT DATE:
6/9/2020 2:04:03 PM
TITLE:

FAR Bonus
Incentives

9G
PLANNED DEVELOPMENT PERMIT
PACKAGE



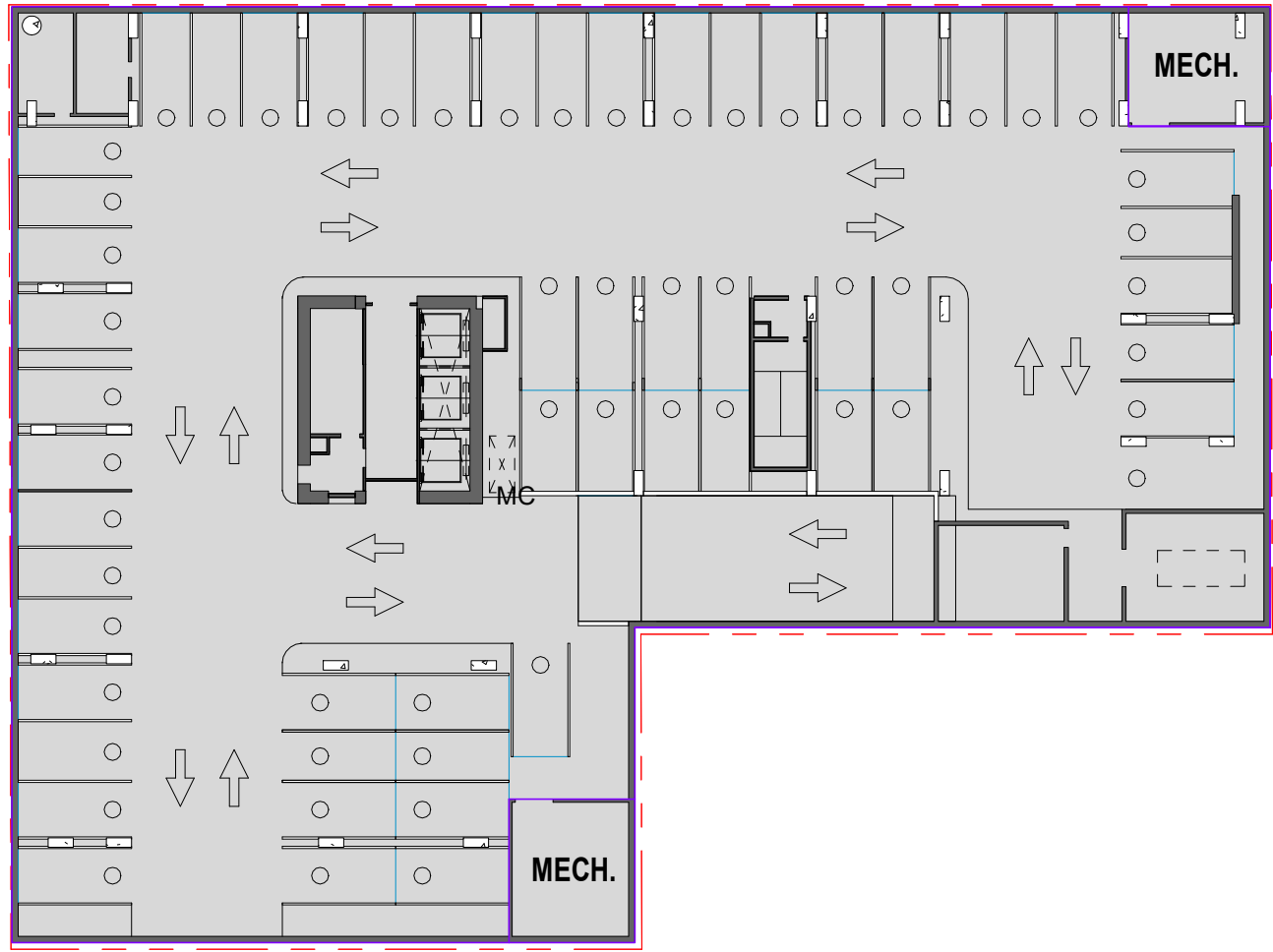
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Level Breakdown
Level 6

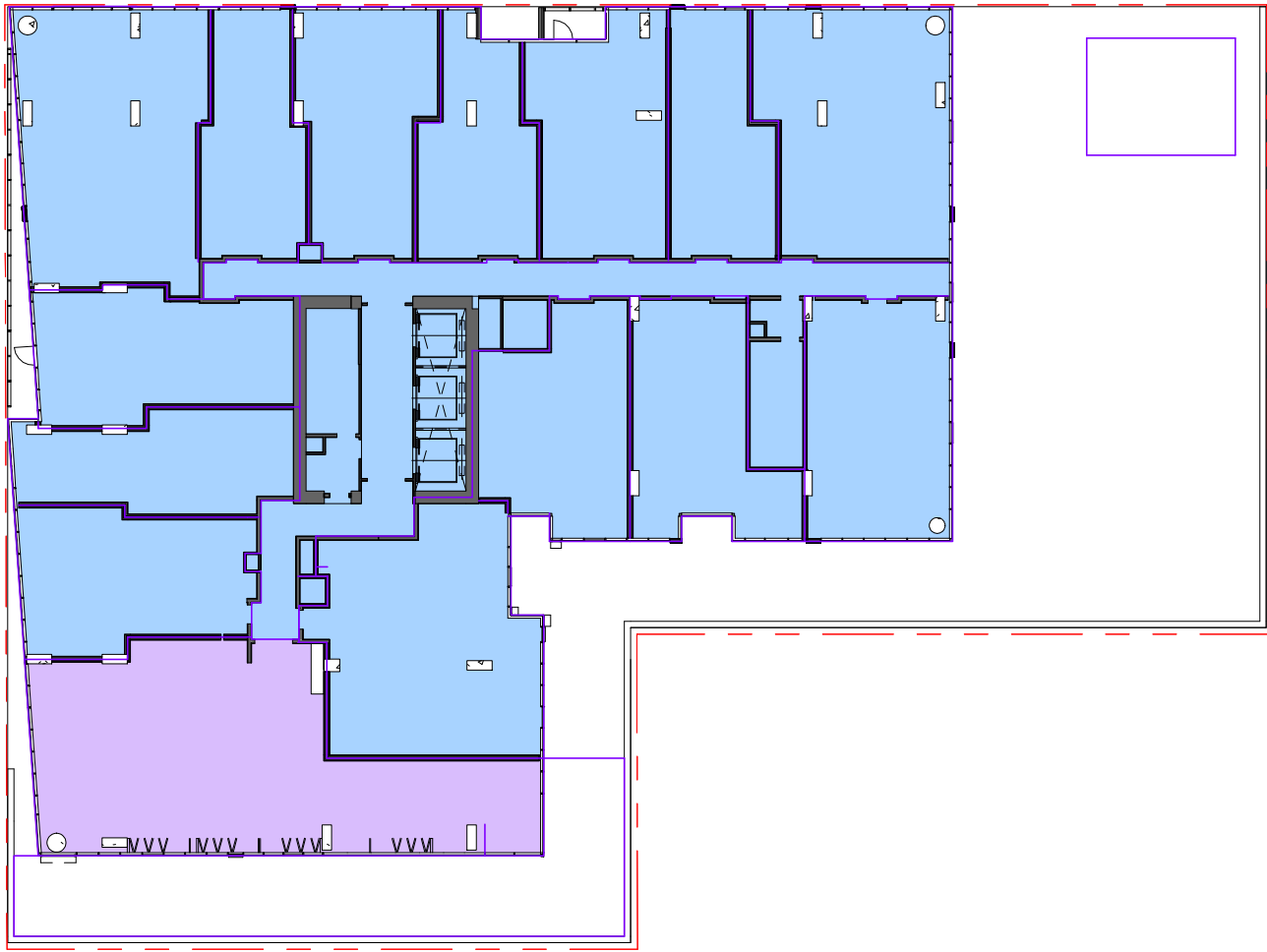
Parking/Garage - EXEMPT



Level Breakdown
Level 7

Residential - 14,360 sf
Amenities - 2,108 sf

Total - 16,468 sf



Level Breakdown
Level 8-19

Residential - 16,473 sf

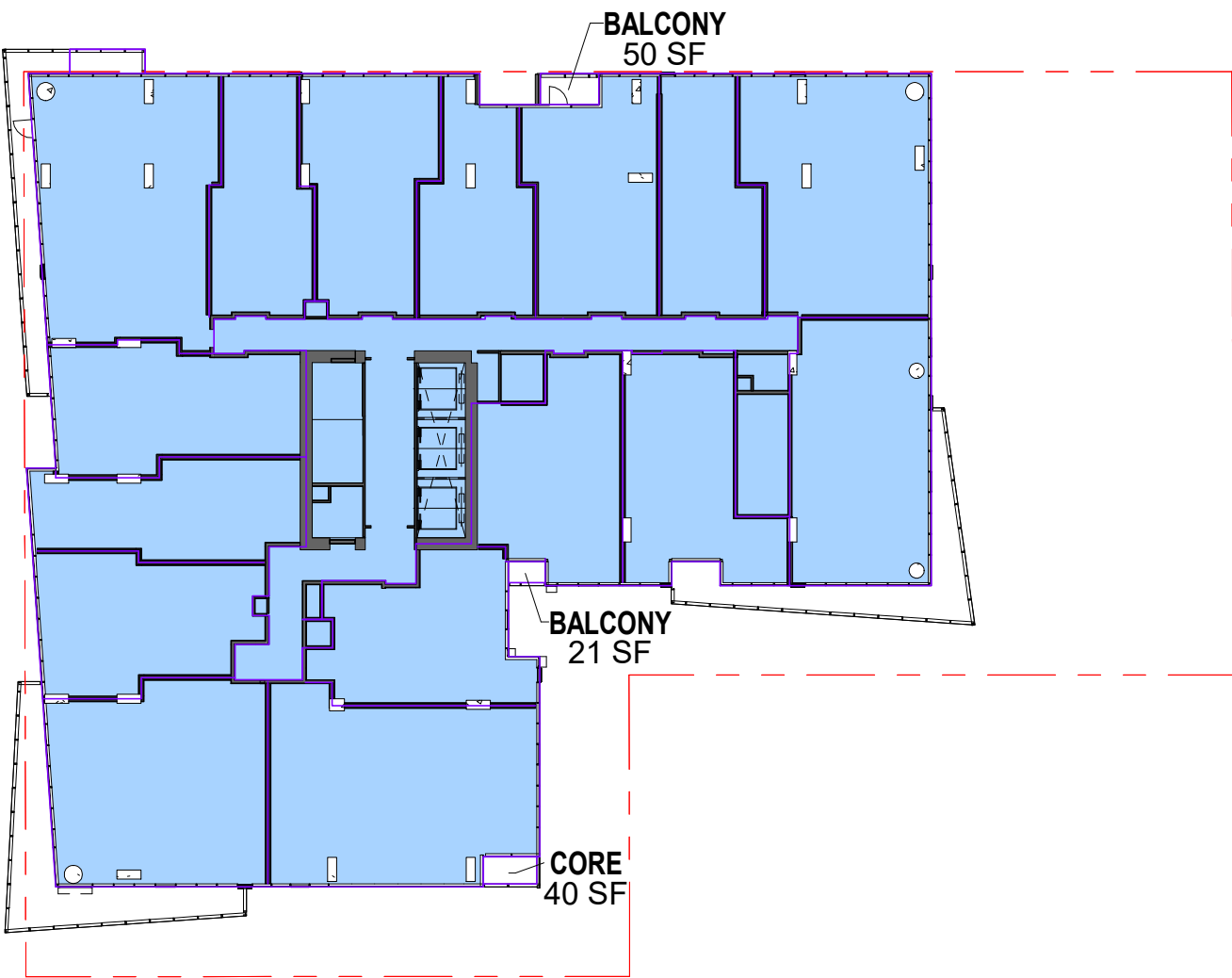
Total - 16,473 sf



Level Breakdown
Level 20

Residential - 16,423 sf

Total - 16,423 sf



9G
PLANNED DEVELOPMENT PERMIT
PACKAGE

3rd SUBMITTAL
6.9.20

ISSUES:

PROJECT NO:
5937.00
FILE NAME:
B:\300\5937\00-Clients
-SEARCH - C:\temp\5937_2018.3.rvt
DRAWN BY: Author
CHECKED BY: Checker
PLOT DATE:
6/9/2020 2:04:09 PM
TITLE:

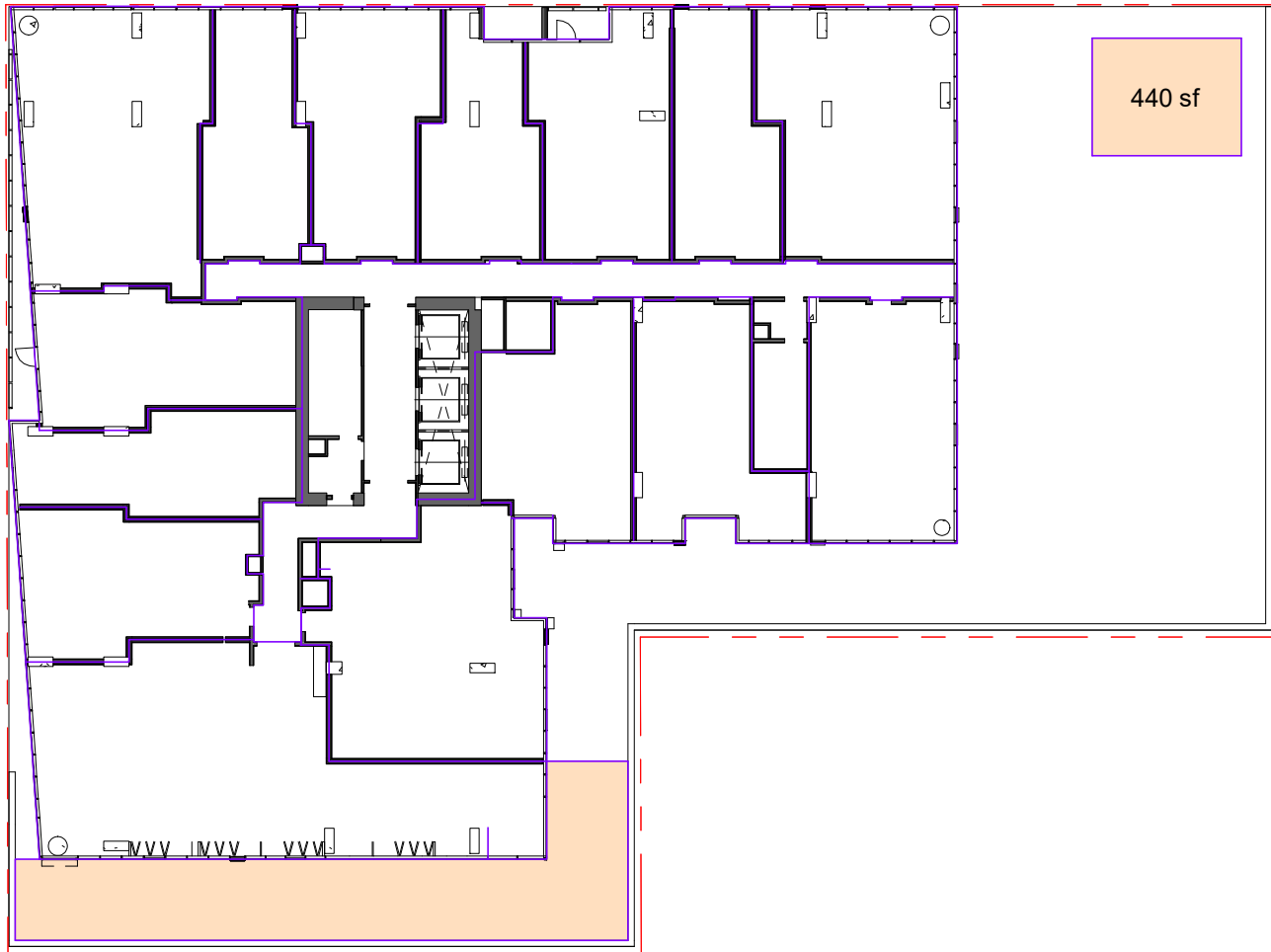
FAR Bonus
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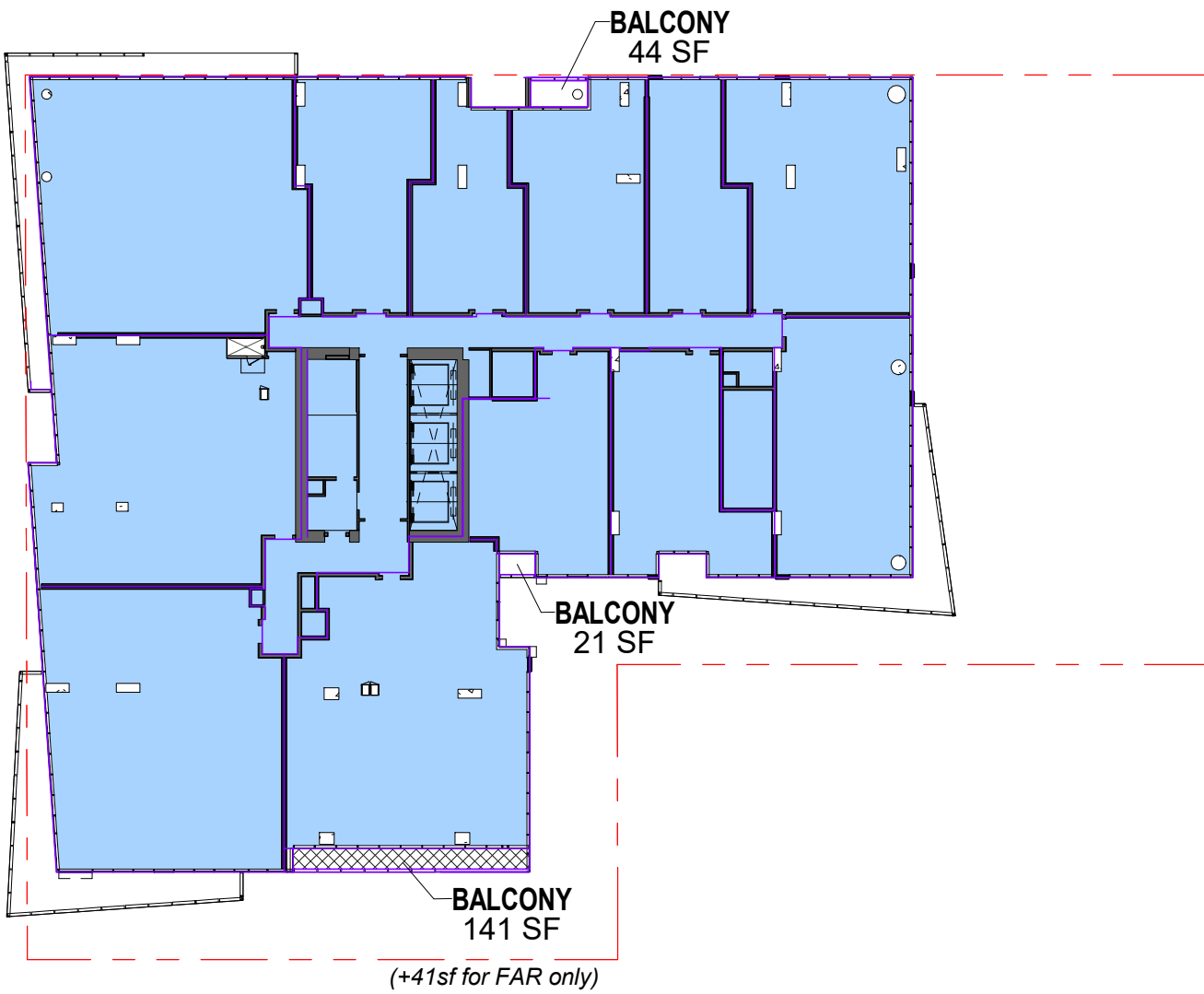
Level Breakdown
Level 7 - Exterior Common Area

- Open Area - 1,444 sf
 - Pet Area - 440 sf
- Total - 1,884 sf



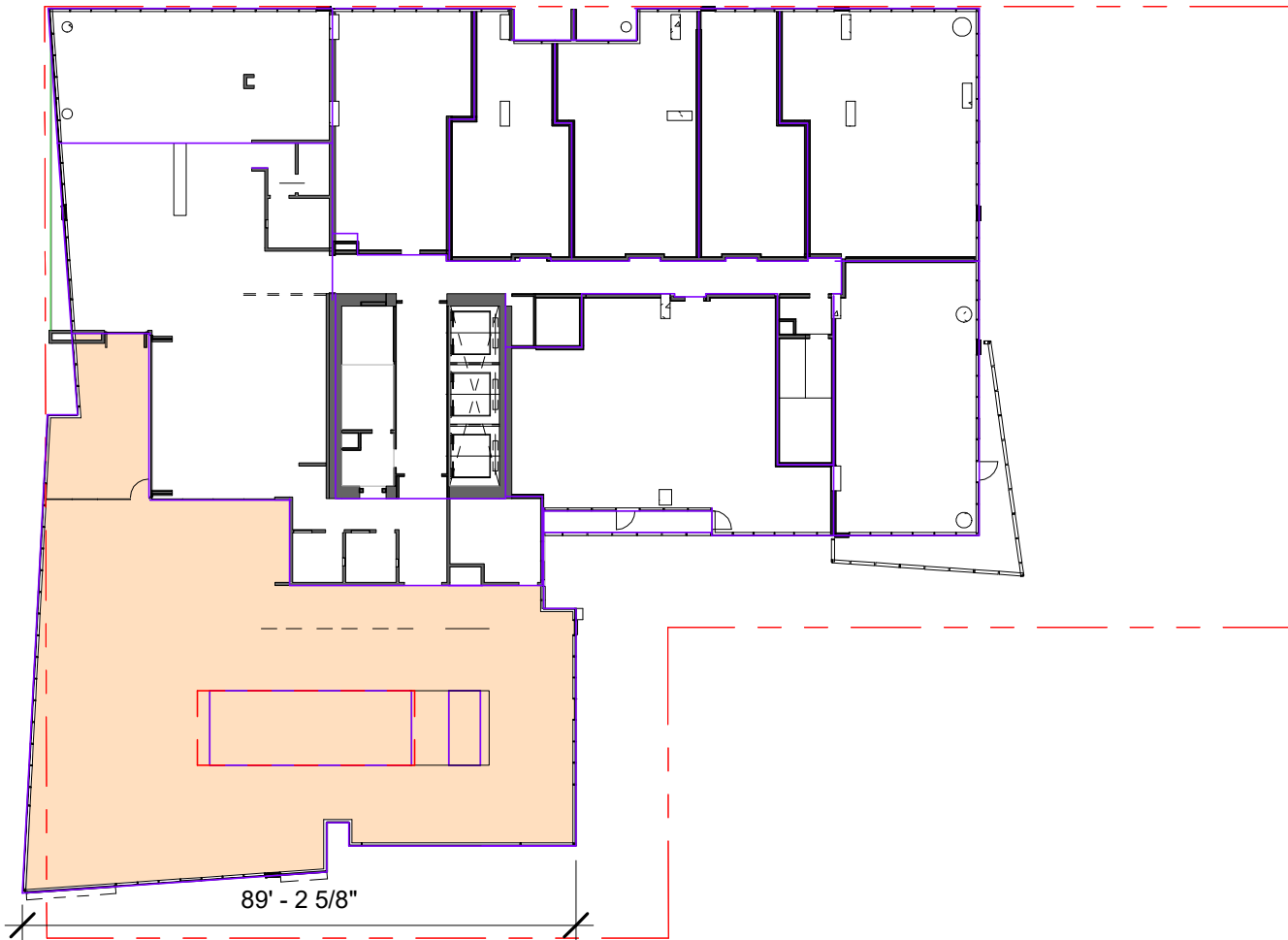
Level Breakdown
Level 21

- Residential - 16,308 sf
 - Residential Balcony (first 100sf not included in FAR) - 41sf
- Total - 16,349 sf



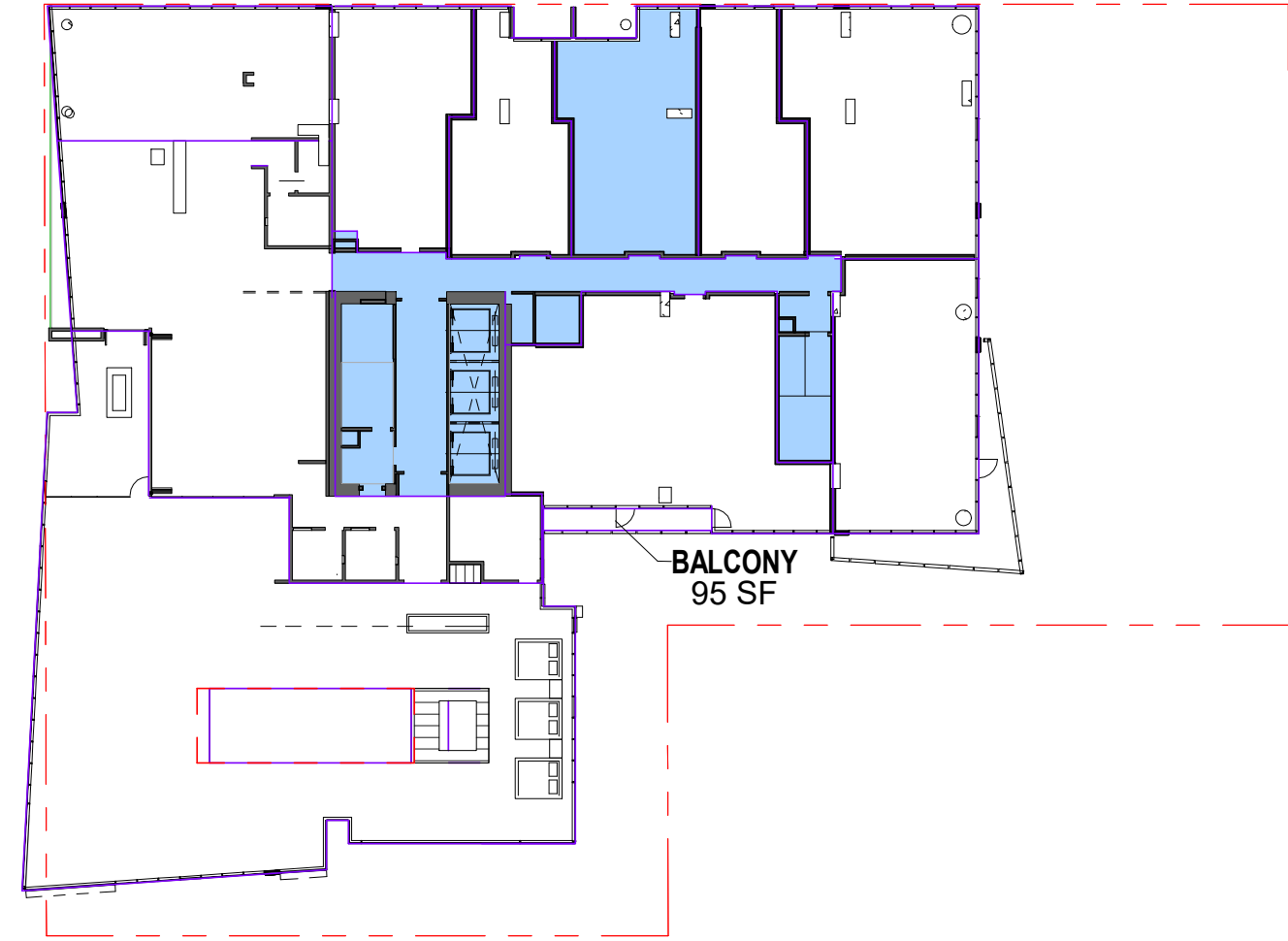
Level Breakdown
Level 22 - Exterior Common Area

- Open Area - 4,250 sf
- Total - 4,250 sf



Level Breakdown
Level 22

- Residential - 8,447 sf
 - Amenities - 3,639 sf
- Total - 12,086 sf



9G
PLANNED DEVELOPMENT PERMIT
PACKAGE

3rd SUBMITTAL
6.9.20

ISSUES:

PROJECT NO:
5937.00
FILE NAME:
B:\M 300\5937 00-Carter\2020\ARCH - Cisterra\6.9.20\18.3.rvt
DRAWN BY: Author
CHECKED BY: Checker
PLOT DATE:
6/9/2020 2:04:15 PM
TITLE:

FAR Bonus
Incentives and
Exterior
Common Areas



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