



THE CITY OF SAN DIEGO

Report to the Planning Commission

DATE ISSUED: September 17, 2020 REPORT NO. PC-20-058
HEARING DATE: September 24, 2020
SUBJECT: COSTA VERDE REVITALIZATION. Process Five Decision
PROJECT NUMBER: [477943](#)
OWNER/APPLICANT: Regency Centers, L.P., and MetroPlan, LLC

SUMMARY

Issue(s): Should the Planning Commission recommend to the City Council approval of a General Plan Amendment, Community Plan Amendment, Specific Plan Amendment; Planned Development Permit, Site Development Permit and Neighborhood Development Permit amending Planned Development Permit and Planned Commercial Development Permit; Vesting Tentative Map; and Public Right-of-Way and Easement Vacations that would allow for modifications of and additions to a 13.9-acre-site, currently developed with a retail and commercial shopping center, located at 8650 Genesee Avenue within the University Community Plan area?

Staff Recommendation(s):

1. Recommend the City Council CERTIFY [Environmental Impact Report No. 477943](#) (SCH No. 2016071031) and ADOPT Mitigation Monitoring, and Reporting Program (MMRP) and Findings and Statement of Overriding Considerations; and
2. Recommend the City Council APPROVE General Plan Amendment, University Community Plan Amendment, Costa Verde Specific Plan Amendment No. 1677935, Site Development Permit No. 2413426, Planned Development Permit No. 1677936, and Neighborhood Development Permit No. 2411303 amending Planned Development Permit No. 90-1109 and Planned Commercial Development Permit No. 85-0783, Vesting Tentative Map No. 2411304, and Public Right-of Way Vacation No. 2464871 and Easement Vacation No. 2413763 to add approximately 40,000 square feet of commercial office use; 360,000 square feet of research and development use; a 200-room hotel; and to vacate an existing public right-of-way of Esplanade Court, located at the northwest corner of Nobel Dr and Genesee Ave.

Community Planning Group Recommendation: Pending, as of the publication of this report,

as the University Community Planning Group opted to wait for publication of the final EIR before voting.

Environmental Review:

An Environmental Impact Report (EIR), project No. 477943 (SCH No. 2016071031) has been prepared for the project in accordance with the State of California Environmental Quality Act (CEQA) Guidelines. A Mitigation, Monitoring, and Reporting Program (MMRP) has been prepared and will be implemented which will fully mitigate significant Noise impacts, but the project would result in significant and unmitigated impacts to Transportation/Traffic Circulation. All other impacts were determined to be less than significant.

Fiscal Impact Statement: None. All costs related to processing this project are covered by a deposit account maintained by the applicant.

Code Enforcement Impact: None.

Housing Impact Statement

The project site is designated Neighborhood and Community Commercial by the University Community Plan. The community plan amendment would allow the reintroduction of visitor accommodations and addition of office and research and development uses on the project site. The project would not decrease or increase the amount of land designated for residential use and would; therefore, have a neutral impact on housing. The University Community Planning Area currently has approximately 27,000 residential units, including 360 deed-restricted affordable units.

BACKGROUND

The project site is developed with the Costa Verde Shopping Center, constructed in conformance with the Costa Verde Specific Plan (CVSP) that was approved by the City of San Diego City Council on June 10, 1986. The CVSP applies to approximately 50 acres of land north of Nobel Drive, east of Regents Road, south of La Jolla Village Drive, and west of Genesee Avenue. The overall site contains a mix of residential, including a variety of multi-unit developments and a retirement community, and commercial/retail uses. The overall site is split by Costa Verde Boulevard, which runs north/south, generally in the center of the site. The west half is dominated by residential uses, while the east half is dominated by the Costa Verde Shopping Center, which is the subject of the actions before the Planning Commission.

As discussed in more detail below in the General Plan/Community Plan discussion, the project site is located in the Central Subarea of the University Community. The areas immediately surrounding the CVSP include a variety of uses, including the Westfield UTC Shopping Center to the east; residential uses to the south and west, and residential, office, hotel, and commercial uses to the north. The Blue Line Trolley is under construction to the east of the site.

The CVSP intended for a mix of uses to be co-located on the entire site, which are consistent with the surrounding development. As the site has been built out over the past 35 years, the site has been developed consistent with that specific plan. As noted in more detail below, a plan amendment was approved that re-designated a portion of the specific plan from Visitor Commercial to High Density Residential, and allowed for a maximum of 2,740 dwelling units to be located within the CVSP, and removed a site anticipated for a hotel use.

On March 26, 2015, the City of San Diego Planning Commission initiated an amendment to the University Community Plan and Costa Verde Specific Plan to increase neighborhood and community commercial use on the 13.9-acre site by 125,000 square feet; to redesignate one acre of the site from Neighborhood Commercial to Visitor Commercial to allow a hotel use; and any technical amendment that may be needed to implement the proposed amendments (Planning Commission Resolution No. PC-4676). As the project was presented to staff and the community, the applicant developed the final project scope that was analyzed in the project EIR.

DISCUSSION

Project Description:

The Project entails the reconfiguration and expansion of the existing Costa Verde Center that provides neighborhood services, retail shops, restaurants, office, research and development uses, a hotel, and community gathering spaces. The Project proposes to retain the current amount (approximately 178,000 square feet [SF]) of commercial/retail uses, and add approximately 360,000 SF of research and development (R&D), 40,000 SF of office uses, and re-designate an approximately one-acre portion of the Project site as Visitor Commercial to reintroduce a hotel use to the Costa Verde Specific Plan (CVSP) area to allow for a 200-room hotel. The hotel would be up to 10 stories in height and would encompass approximately 125,000 SF. The maximum building heights would be 45 feet for commercial/retail structures, and 135 feet for commercial/office/research and development and hotel uses. The re-designation of the land use requires the approval of the General Plan, Community Plan, and Specific Plan amendments. The addition of the 360,000 SF of R&D and 40,000 SF of commercial office uses also require amendments to the Community Plan and Specific Plan.

The northern portion of the center sits approximately 14 feet higher in elevation (approximately 360 feet above mean sea level [AMSL]) than the southern portion of the site (approximately 350 feet AMSL, to approximately 335 feet AMSL). A uniform podium level of approximately 360 feet AMSL would be established across the entire site to provide a more cohesive experience and facilitate mobility throughout the site. The majority of parking would be provided beneath this podium level. At the southern portion of the site, the base of two commercial/retail structures would be located at an elevation similar to the existing ground elevation, but lower than the podium level, due to the difference in elevation across the site.

The northern portion of the center would consist of a pedestrian-orientated promenade. The promenade would extend southward from a circular style cul-de-sac at the end of Esplanade Court, which is proposed to be vacated as a public street and be incorporated into the Costa Verde Center

as a private street. It would be lined with retail, restaurant, and office, research and development buildings, as well as a central lawn and gathering area, outdoor seating and dining areas, decorative planters, site furniture, landscaping, and accent paving. Elevators and stairs would provide connections to the Trolley Station platform.

The southern portion of the center would be oriented around a surface parking lot. This area is intended for essential neighborhood services, such as a grocery store, pharmacy, and banks. Landscaping and sidewalks would be provided.

The architecture of the center would consist of modern design and materials, consistent with the character of the community's urban core. This would include clean lines and materials such as cast-in-place concrete, fiber cement panels, metal panels, paint over smooth plaster, brick veneer, and wood siding.

Required Actions:

General Plan Amendment (GPA): A GPA is required to amend the land use designation of approximately one acre from Neighborhood Commercial to Visitor Commercial. Although no text and/or graphic changes to the General Plan would occur as part of this action, community plans are a component of the Land Use Element of the General Plan.

Community Plan Amendment (CPA): A CPA is required to amend the land use designation as noted above and to add approximately 360,000 SF of research and development (R&D), 40,000 SF of office uses, and re-designate an approximately one-acre portion of the Project site as Visitor Commercial to reintroduce a hotel use to the Costa Verde Specific Plan (CVSP) area to allow for a 200-room hotel..

Specific Plan Amendment (SPA): A SPA is required to amend the land use designation as noted above and add approximately 360,000 SF of research and development (R&D), 40,000 SF of office uses, and re-designate an approximately one-acre portion of the Project site as Visitor Commercial to reintroduce a hotel use to the Costa Verde Specific Plan (CVSP) area to allow for a 200-room hotel.

Site Development Permit (SDP): A Process 5 SDP is required in accordance with [San Diego Municipal Code \(SDMC\) Section 126.0502 \(e\)\(4\)](#) for development within the Airport Land Use Compatibility Overlay Zone proposing development that includes a rezone or land use plan approval. On April 30, 2020, The San Diego Regional Airport Authority, which oversees the preparation of the MCAS Airport Land Use Compatibility Plan (ALUCP), completed their evaluation of the application and supplemental information provided by the City of San Diego. The Airport Land Use Commission notified the City of San Diego that the application did not meet the criteria for their review as the City of San Diego is implementing the requirements of the ALUCP.

Planned Development Permit (PDP): A PDP is required per [SDMC Section 126.0602](#) for development that complies with the applicable land use plan but contains uses that are not permitted in the underlying base zone. The Project's underlying base zone, RS-1-14 allows for single residential uses, and does not permit commercial, office, research and development, or hotels. Those uses do,

however, comply with the community and specific plans. The community plan will be amended to specifically designate approximately one-acre of land as Visitor Commercial for the proposed hotel, which is a use that had been anticipated with the original CVSP. A PDP is also required to amend Planned Development Permit No. 90-1109 and Planned Commercial Development Permit (PCD 85-0783), as the PCD is not a permit that the City utilizes anymore. The amendments to the General Plan, Community Plan, and Specific Plan would allow the hotel use, in addition to the commercial and office uses that are already consistent with the applicable land use plans.

Neighborhood Development Permit (NDP): The project is proposing Tandem Parking. Because the project is not in a Residential Tandem Parking Overlay zone, an NDP is required in accordance with [SDMC Section 126.0402\(d\)](#) for commercial development proposing tandem parking as described in Section 142.0555(b).

Vesting Tentative Map: A VTM is requested for the subdivision of the property, allowing for 4 lots and 20 commercial condominiums. This is requested in accordance with [SDMC Section 144.0201](#), and conditions are provided to ensure compliance with all of the applicable subdivision regulations.

Public Right-of-Way Vacation: This vacation is requested pursuant to [SDMC 125.0910](#) to vacate an existing public street (Esplanade Court) to the applicant/underlying fee owner. The vacation cannot be summarily granted, due to public utilities within the right-of-way, however the right-of-way does lie within one ownership and does not touch the property of another owner. An access easement will be provided to adjacent property to ensure that access to Genesee Avenue will be maintained. The adjacent property has access to another public street, Costa Verde Boulevard, that would not be impacted by any of this project actions.

Easement Vacation: The applicant has requested that existing public service easements be vacated in accordance with [SDMC Section 125.1010](#). In conjunction with the proposed right-of-way vacation, the easement vacation would privatize existing utilities within the current public right-of-way, and become privately owned and maintained.

General Plan/Community Plan Analysis:

The General Plan's Land Use Element includes the City of Villages land use strategy. Since the majority of the City is developed, infill development and redevelopment play an increasingly significant role in providing needed housing, jobs, and services in our communities. The City of Villages strategy is intended to focus growth into mixed-use activity centers that are pedestrian-friendly, centers of community, and linked to the regional transit system. The strategy focuses on the long-term economic, environmental, and social health of the City and its many communities. Implementation of the City of Villages strategy is an important component of the City's strategy to reduce local contributions to greenhouse gas emissions because the strategy makes it possible for larger numbers of people to make fewer and shorter auto trips. The City of Villages strategy identifies several village types and their characteristics and includes [Figure LU-1 which is the Village Propensity map](#) (Attachment 3). This map illustrates areas that already exhibit village characteristics and areas that may have a propensity to develop as village areas.

The project site is in an area with a high village propensity. The project area is considered an Urban Village which is higher density/intensity areas located in subregional employment districts. They are characterized by a cluster of more intensive employment, residential, regional and subregional commercial uses that maximize walkability and support transit. Amendments to the University Community Plan (UCP) and the CVSP in the last several years have increased the allowed intensity of residential, commercial and industrial development in and around the Urban Node of the University Community. The proposed revitalization of existing commercial services, reintroduction of visitor accommodations and addition of 400,000 square feet of office and research and development (R&D) uses would be consistent with the City of Villages strategy and the Economic Prosperity Element's goals to both retain and grow employment uses within subregional employment areas.

The project site is adjacent to the Mid-Coast Light Rail Transit (LRT) extension of the Blue Line Trolley and would have a direct connection to the UTC terminus station via new pedestrian bridges located south of Esplanade Court within the center median of Genesee Avenue. The proposed hotel, office and R&D use will have direct access to transit via the trolley and existing transit station on the ground level at UTC directly across Genesee Avenue providing connections to major employment areas such as Torrey Pines Mesa, Campus Point, Eastgate Technology Park, Sorrento Valley and downtown. The transit will also provide connections to the VA Medical Center, Scripps Memorial Hospital and the UC San Diego campus via trolley stops at the VA Medical Center and on the UC San Diego campus at Pepper Canyon and Voigt Drive.

The redesign of the center will integrate pedestrian friendly connections to adjacent uses and the trolley to make walking a viable choice for nearby residents. The center would convert an auto-oriented environment to a safe and comfortable pedestrian environment consistent with the General Plan's goals of creating walkable communities.

The proposed community plan amendment would help provide additional quality job opportunities and secondary employment in an area where a significant investment in transit has been made. This in turn, would help increase employment within a Transit Priority Area, consistent with Climate Action Plan Strategies and would further the City's trajectory towards meeting its goals to reduce greenhouse gas emissions.

During the Planning Commission Hearing for the community plan amendment initiation, commissioners requested that the provision of on-site residential uses be analyzed. In early stages of the project design, 120 multi-family residential units were included as a potential future phase of the revitalization of the commercial center. The adopted community plan does not permit residential use within the shopping center. Instead it designates High Density Residential on the neighboring property in the Costa Verde Specific Plan. In September 2007, a community plan amendment re-designated the adjacent site within the specific plan from Visitor Commercial to High Density Residential. Over 70% of the land area in the Costa Verde Specific Plan is devoted to high-rise residential use, allowing a maximum total of 2,740 dwelling units. Most of the residential units in the specific plan have been built with a portion currently being constructed.

The area surrounding the shopping center represents one of the densest residential areas in the City of San Diego outside of downtown. Approximately 18,000 dwelling units are within a 1-mile

radius of the Costa Verde Shopping Center and the transit stop in Genesee Ave. It is not anticipated that every project can or should include residential land uses to address regional housing needs. The proposed project, viewed as part of the UCP and CVSP, fulfills the region's goals to balance residential and employment uses near transit.

The project site is located in the Central Subarea of the University Community which is the most urban of the four subareas of the community. The UCP identifies a portion the Central Subarea as an Urban Node which is characterized by a relatively high density, mixed use core in the area of La Jolla Village Drive and Genesee Avenue. Activities such as retail, professional office, medium to high density residential and entertainment are currently concentrated within the Urban Node surrounding to the project site. The UCP Central Subarea's main objective is to improve the community's urban form and cohesiveness as new construction activity continues. Two previous amendments to the UCP and CVSP (Monte Verde & Westfield UTC) included improvements to the pedestrian realm along both La Jolla Village Drive and Genesee Avenue. These improvements included wider sidewalks, trees, and landscaping, informal seating areas, as well as thoughtful and direct pedestrian connections into each project. Architecture for both projects includes pedestrian oriented features at the ground level to help create a pedestrian friendly environment.

The proposed project would be the last piece of the puzzle for the area along Genesee Avenue to design and create a cohesive urban environment with direct connections to surrounding uses and transit. As mentioned previously, the redesign of the center would place less focus on functioning as an auto-oriented commercial center and create a mixed-use environment where walking, biking and transit become viable modes of transportation. The proposed hotel, office and R&D uses would help balance the mix of uses within the Urban Node and be well served by the adjacent neighborhood, community and regional commercial uses as well as the trolley extension and transit center across the street at UTC.

The University Community Plan Update began in 2018 and is anticipated to be complete in 2022. Although future land uses in and around the project site have not been formally determined, it is anticipated that the current mix of land uses would be retained, and the proposed amendment would not conflict with the community plan update. It is anticipated that residential capacity could be significantly increased as part of the community plan update through introduction of mixed-use land uses in five focus areas that have been identified in the process thus far. More information on the community plan update process can be found here: <https://www.planuniversity.org>

Environmental Analysis:

The project site is located in a fully developed area of the city. City staff analysis of the project identified the potential for the project to result in impacts to number of issues under CEQA, with significant impacts identified related to noise and transportation/traffic circulation. The project noise impacts can be fully mitigated. The project will also implement a number of mitigation measures related to transportation/traffic circulation which have been incorporated into the SDP/PDP/NDP for the project (Attachment 6). However, not all of the identified transportation/traffic circulation impacts can be mitigated to below a level of significance. Therefore, the applicant has provided Draft Candidate Finding's and Statement of Overriding Consideration to allow the decision maker to adopt

the Project with significant unmitigated and unavoidable direct impacts related to Transportation/Traffic Circulation (Attachment 10).

Conclusion:

Staff has reviewed the proposed Project and all issues identified through the review process have been resolved in conformance with adopted City Council policies and regulations of the Land Development Code. The Project meets all applicable regulations and policy documents, and staff supports the determination that the Project is consistent with the recommended land use plans, design guidelines, and development standards in effect for this site per the SDMC, the University Community Plan, the Costa Verde Specific Plan, and the General Plan. Thus, staff recommends the Planning Commission recommend to the City Council to approve the Project as proposed.

ALTERNATIVES

1. Recommend the City Council CERTIFY Environmental Impact Report No. 477943 (SCH No. 2016071031) and ADOPT Mitigation Monitoring and Reporting Program (MMRP) and Findings and Statement of Overriding Considerations and APPROVE General Plan Amendment, University Community Plan Amendment, Costa Verde Specific Plan Amendment No. 1677935, Site Development Permit No. 2413426, Planned Development Permit No. 1677936, and Neighborhood Development Permit No. 2411303 amending Planned Commercial Development Permit No. 90-1109, Vesting Tentative Map No. 2411304, and Public Right-of Way Vacation No. 2464871 and Easement Vacation No. 2413763, with modifications.
2. Recommend the City Council NOT CERTIFY Environmental Impact Report No. 477943 (SCH No. 2016071031) and NOT ADOPT Mitigation Monitoring and Reporting Program (MMRP) and Findings and Statement of Overriding Considerations and DENY General Plan Amendment, University Community Plan Amendment, Costa Verde Specific Plan Amendment No. 1677935, Site Development Permit No. 2413426, Planned Development Permit No. 1677936, and Neighborhood Development Permit No. 2411303 amending Planned Commercial Development Permit No. 90-1109, Vesting Tentative Map No. 2411304, and Public Right-of Way Vacation No. 2464871 and Easement Vacation No. 2413763, if the findings required to approve the project cannot be affirmed.

Respectfully submitted,



PJ Fitzgerald
Deputy Director
Development Services Department



Martha Blake
Development Project Manager
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Program Manager
Planning Department

EL/MB

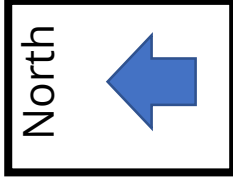
Attachments:

1. Aerial Photographs
2. Community Plan Land Use Map
3. Village Propensity Map
4. Project Location Map
5. Draft Permit Resolution with Findings
6. Draft Permit with Conditions
7. Draft Map Resolution with Findings
8. Draft Map Conditions
9. Draft Environmental Resolution with MMRP
10. Draft Statement of Findings and Overriding Conditions
11. Draft Community Plan Amendment Documents
12. Map Exhibit-Tentative/Vesting Tentative Map /Map Waiver/Project Plans
13. Community Planning Group Recommendation
14. Ownership Disclosure

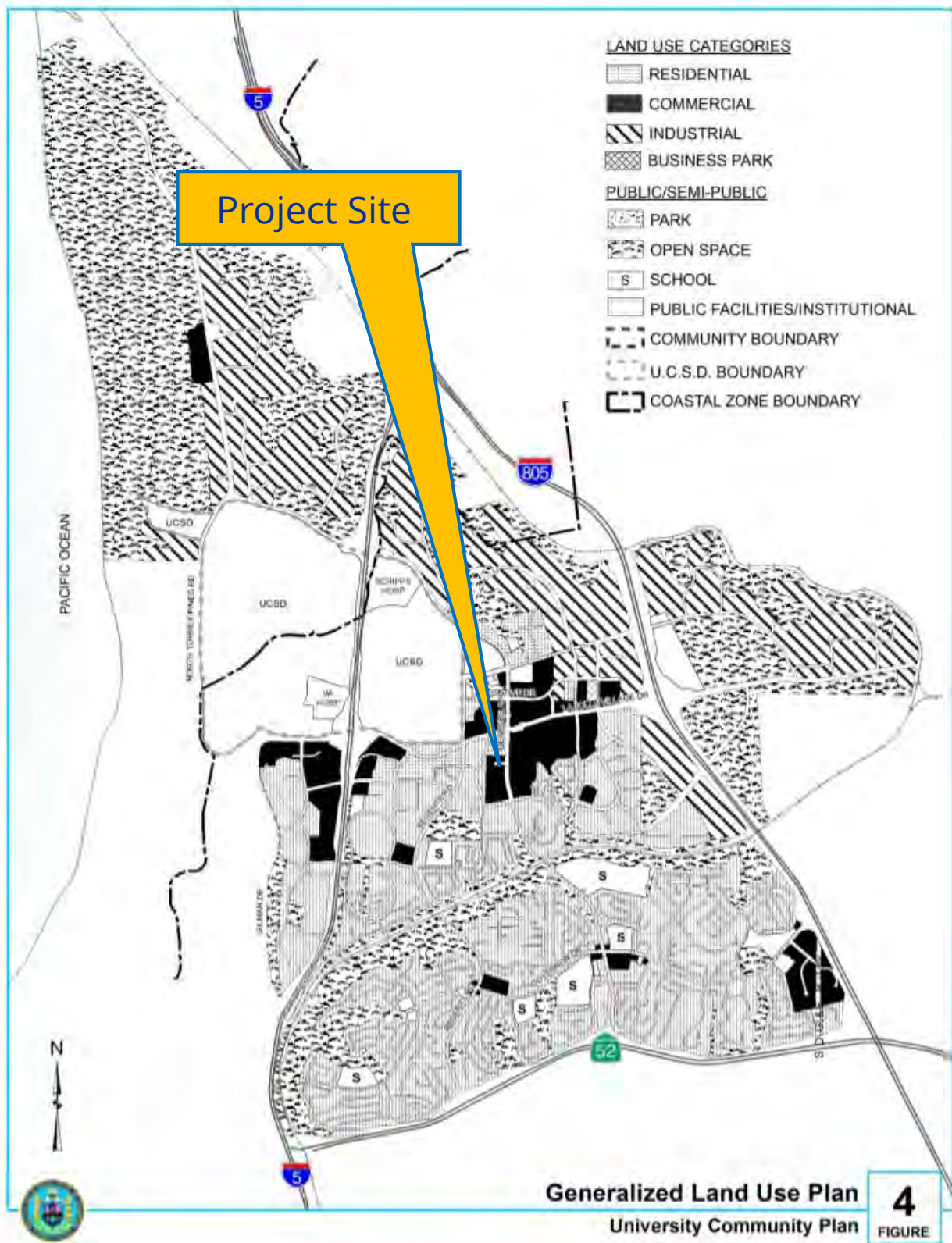


Aerial Photos

Costa Verde Revitalization.
PROJECT NO. 477943

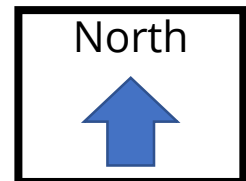






Community Plan Land Use Map

Costa Verde Revitalization.
PROJECT NO. 477943





THE CITY OF SAN DIEGO
General Plan
Land Use and Community
Planning Element

Using Geographic Information Systems (GIS), this figure shows existing areas that already exhibit village characteristics and areas that may have a propensity to develop as village areas based on having certain existing or planned characteristics; it does not take the place of site-specific planning. These characteristics include the location of parks, fire stations, transit routes, and existing and community plan designated land uses. Actual village locations will be designated in community plans with the input from recognized community planning groups and the use of locational criteria established under the policies section. Community plans will also house site-specific design guidelines to ensure the successful implementation of each site. Many community plans already identify sites suitable for mixed-use and provide extensive design and development policy guidance for development of those sites.

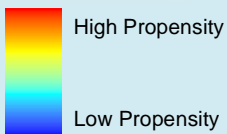
Pacific Ocean

San Diego Bay

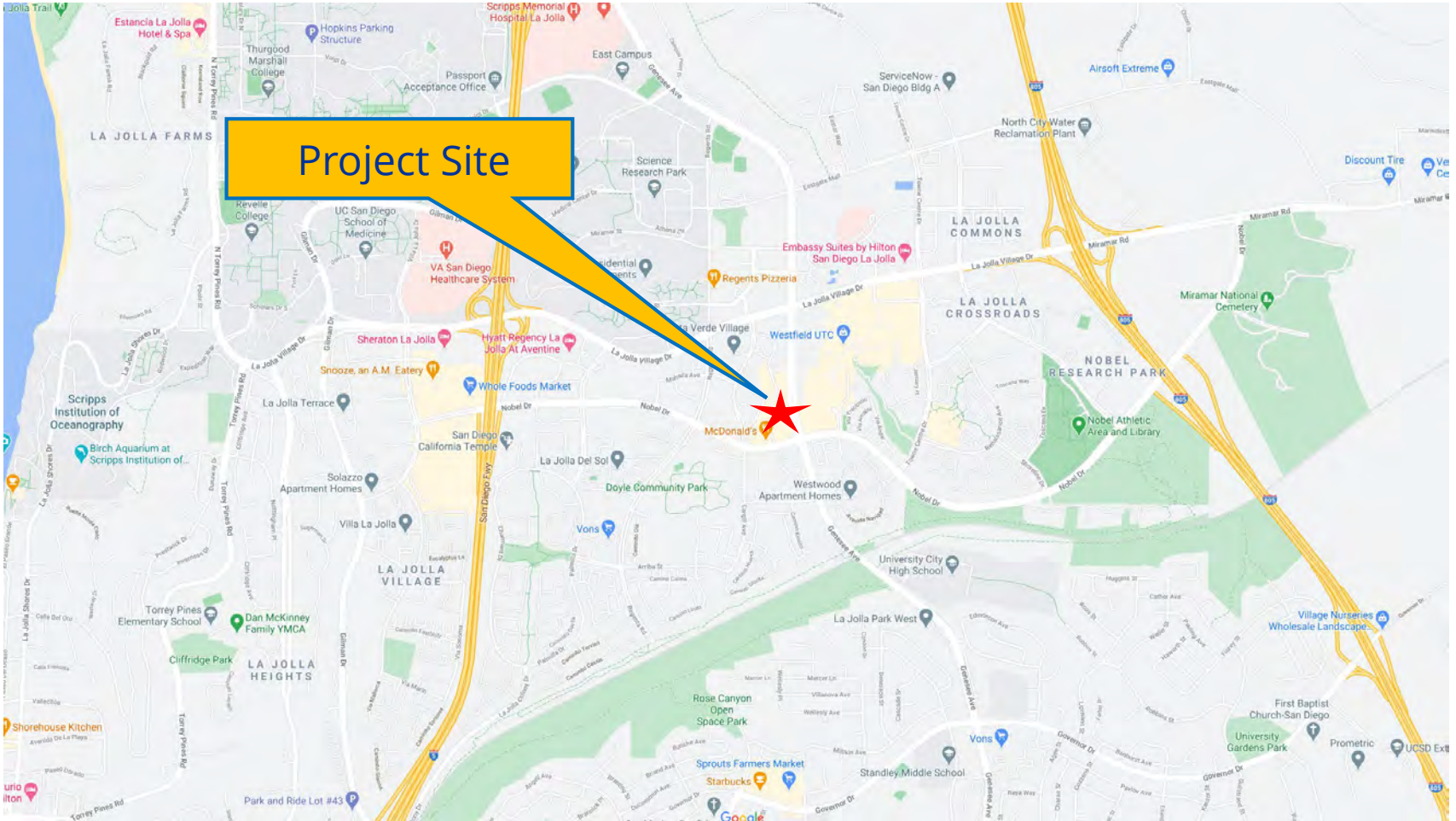
MEXICO

Figure LU-1
Village Propensity

Value



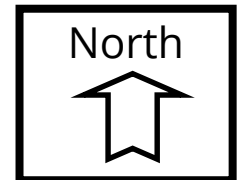
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Project Location Map

Costa Verde Revitalization

PROJECT NO. 477943



CITY COUNCIL RESOLUTION NO. [REDACTED]
SITE DEVELOPMENT PERMIT NO. 2413426, PLANNED DEVELOPMENT PERMIT NO. 1677936,
and NEIGHBORHOOD DEVELOPMENT PERMIT NO. 2411303
COSTA VERDE REVITALIZATION - PROJECT NO. 477943
[MMRP]

WHEREAS, Regency Shopping Centers, Limited Partnership, a California Limited Partnership, Owner/Permittee, filed an application with the City of San Diego for a Site Development Permit, Planned Development Permit, and Neighborhood Development Permit to reconfigure and expand an existing commercial shopping center known as the Costa Verde Revitalization project (Project), located at the northwest corner of Genesee Avenue and Nobel Drive in the RS-1-14 zone and the Community Plan Implementation Overlay Zone (CPIOZ) Type A within the Urban Node of the Central Subarea of the University Community Plan. The project site is legally described as: Lots 13 – 14 inclusive of Costa Verde, City of San Diego, County of San Diego, State of California, According to Map thereof No. 12045, filed in the office of the County Recorder of San Diego County, April 18, 1988; and

WHEREAS, on September 24, 2020, the Planning Commission of the City of San Diego considered, Site Development Permit No. 2413426 (SDP), Planned Development Permit No. 1677936 (PDP), and Neighborhood Development Permit No. 2411303 (NDP), and pursuant to Resolution No. XXXX-PC voted to recommend approval/disapproval of the Permits; and

WHEREAS, under Charter section 280(a)(2) this resolution is not subject to veto by the Mayor because this matter requires the City Council to act as a quasi-judicial body and where a public hearing was required by law implicating due process rights of individuals affected by the decision and where the Council was required by law to consider evidence at the hearing and to make legal findings based on the evidence presented; and

WHEREAS, the matter was set for public hearing on _____, testimony having been heard, evidence having been submitted, and the City Council having fully considered the matter and being fully advised concerning the same; NOW, THEREFORE,

BE IT RESOLVED, by the City Council, that it adopts the following findings with respect to Site Development Permit No. 2413426 (SDP), Planned Development Permit No. 1677936 (PDP), and Neighborhood Development Permit No. 2411303 (NDP):

A. SITE DEVELOPMENT PERMIT [SDMC Section 126.0505]

1. Findings for all Site Development Permits:

a. The proposed development will not adversely affect the applicable land use plan.

The approximately 14-acre site of the Costa Verde Center, which is located at the northwest corner of Nobel Drive and Genesee Avenue, is designated by the University Community Plan as Neighborhood Commercial and Community Commercial. To implement the Project, the following land use plan amendments would be required:

- Re-designate approximately 1.0 acre from a Neighborhood Commercial to a Visitor Commercial land use designation in the community plan to allow a 200-room hotel and associated ancillary uses.
- Amend the Development Intensity Element-Table 3 of the community plan. The table currently allows development of 178,000 square feet of Community/Neighborhood Commercial Uses. Approval of an amendment would permit the addition of a 200-room hotel and 400,000 square feet of Research & Development/Office Uses.
- Technical implementing amendments to the Costa Verde Specific Plan (CVSP) that are necessary to address permitted uses, zoning regulations, design guidelines or policies.
- The proposal would also amend the General Plan as all changes to community plans are considered amendments to the citywide document.

The Project, with the concurrent approval of amendments to the General Plan, University Community Plan and Costa Verde Specific Plan, will not adversely affect any of the long-range planning documents.

- The Strategic Framework, Land Use & Community Planning Element, Economic Prosperity Element and Urban Design Element of the General Plan provide a series of goals and policies that guide the revitalization of Costa Verde Center. The

elements promote redevelopment that efficiently combines a diversity of land uses, fosters mobility options particularly near transit stations and improves the local economy by expanding employment opportunities in the visitor industry, commercial retail services and technology sectors.

- As evaluated in the Final Project EIR, Table 5.1.1, the proposed changes to Costa Verde Center are in conformance with the University Community Plan with approval of the amendments. The site is within the Urban Node of the Central Subarea, one of four subareas identified by the community plan. The Central Subarea is “one of the major residential, commercial and office nodes in the City”.

The amendment, which provides for a hotel, restores the original vision of the 1986 Specific Plan, which included a hotel within the planning area until 2007. In the last several decades, the growth of visitor-serving accommodations in the University Community has not kept pace with the demand. The five hotels in the University Town Center (UTC) submarket have consistently out-performed the national annual occupancy for the United States during the last six years, despite the newest hotel in the area opening in 1989. A new hotel at Costa Verde Center would support visitors to the surrounding community, University of California San Diego (UCSD), Scripps Research, the Birch Aquarium, the expanding bio-tech/research & development sector, and the Veterans Affairs (VA) Medical Center.

- The CVSP implemented many of the Smart Growth concepts that decades later became the cornerstone of the Strategic Framework Element. The proposed Project represents an update of the underlying concepts of the plan. One which reflects current conditions including the soon to be opened Mid-Coast Trolley Terminus Station immediately next to the site above Genesee Avenue, the growing demand in the technology/bio-technology service sector for clustered space and the changing nature of retailing. Although an expansion of commercial retail uses is not being proposed, the re-configuration of the center allows for a revised mix of existing and new community/ neighborhood tenants. Approval of the proposal furthers the CVSP goal to: “Provide a complete range of goods and services for the residents of the University community.”

In accordance with Municipal Code Section 126.0502 (e) (4), a Site Development Permit is required if development within the Airport Land Use Compatibility Overlay Zone requests a rezone or land use approval. The application for the Costa Verde Center Revitalization requires amending several land use plans (General Plan, University Community Plan, and Costa Verde Specific Plan) in order to implement the project as proposed.

The purpose of the Airport Land Use Compatibility Overlay Zone is to implement adopted Airport Land Use Compatibility Plans (ALUCP), in accordance with state law, as applicable to property within the City. The intent of these supplemental regulations is to ensure that new development or expansion of existing development located within an Airport Influence Area (AIA) is compatible with respect to airport-related noise, public safety, airspace protection, and aircraft overflight areas. This overlay zone applies to properties such as the project site that are located within an AIA as identified in an adopted ALUCP for a public use or military airport.

The Marine Corps Air Station (MCAS) Miramar airfield is approximately two miles from the project site and located within the AIA for MCAS Miramar. The Project area is within "Review Area 2", which consists of locations that are within the airspace protection and/or overflight areas on the associated maps in the MCAS Miramar Airport Land Use Compatibility Plan (ALUCP). Since the site is within the overflight area for MCAS Miramar, the Project would be subject to review under FAA Part 77 Noticing Area requirements. Building height and obstruction restrictions apply around the installation to ensure that no object would interfere with the safe operation of aircraft or impact the air installation operations. The ALUCP contains criteria for determining airspace obstruction compatibility. The FAA issued a Determination of No Hazard to Air Navigation on March 26, 2020.

Issues in Review Area 2 include projects that create objects in a High Terrain Zone, projects that create electrical or visual hazards to airplanes in flight, and projects that have the potential to cause an increase in birds or wildlife. The Project site is not located within a High Terrain Zone. The Project also does not propose uses that would create electrical hazards to aircraft, and it does not propose the use of neon lights that could be mistaken for airport lighting or interfere with night vision goggles used by military pilots. The Project does not include large water features or propose uses that would attract wildlife such as birds that would interfere with aircraft operations. Based on mandatory compliance with FAA regulatory criteria, potential impacts from aircraft-related hazards were concluded to be less than significant.

The Project site is located outside of the 60 CNEL noise contour as shown on the Compatibility Policy Map: Noise of the MCAS Miramar ALUCP. Noise levels from the airport were incorporated into transportation noise levels for on-site noise-sensitive land uses and impacts were evaluated as to being less than significant.

On April 30, 2020, The San Diego Regional Airport Authority, which oversees the preparation of the MCAS Airport Land Use Compatibility Plan (ALUCP), completed their evaluation of the application and supplemental information provided by the City of San Diego. The Airport Land Use Commission notified the City of San Diego that the application did not meet the criteria for their review as the City of San Diego is implementing the requirements of the ALUCP.

b. The proposed development will not be detrimental to the public health, safety, and welfare.

The Project will not be detrimental to the public health, safety, and welfare because the permits controlling the development and use of the site requires compliance with City codes, policies, regulations and other regional, state and federal regulations. Construction plans will be reviewed by City Staff to ensure compliance with all building code regulations. All Uniform Building, Fire, Plumbing, Electrical and Mechanical Code regulations and permitting requirements governing the construction and continued operation of the development apply to this Project. The Project will be inspected by certified building and engineering inspectors to assure construction is in accordance with approved plans and regulations.

The Project is required to comply with seismic requirements of the California Building Code. Implementation of proper engineering design and utilization of standard construction practices

(including recommendations contained with the Geotechnical Investigation) verified at the building permit stage would ensure that the potential for impacts from regional geologic hazards, including fault rupture would be less than significant.

The Project Environmental Impact Report (EIR) identified that the project could result in significant impacts to both noise and traffic. Mitigation measures have been identified and will be implemented with the project to avoid (noise) or reduce (traffic) impacts.

The Project is subject to the Federal Aviation Regulation, Part 77 which requires that projects submit applications to the Federal Aviation Administration to determine if the proposed buildings would be a hazard to air navigation. The FAA issued a Determination of No Hazard to Air Navigation on March 26, 2020.

For the above cited reasons, the development will not be detrimental to the public, health, safety and welfare.

c. The proposed development will comply with the regulations of the Land Development Code including any allowable deviations pursuant to the Land Development Code.

The proposed development is governed by several long-range planning documents (the General Plan, University Community Plan and Costa Verde Specific Plan), the Land Development Code, several implementing discretionary permits (Planned Development Permit, Site Development Permit, Neighborhood Use Permit), a Vesting Tentative Parcel Map and Public Right-of-Way Easement Vacation.

The Costa Verde Specific Plan has regulatory controls and development guidelines for the site and surrounding residential community, beginning with the earliest site approvals in 1986. The existing mixed-used community could not have been achieved in strict conformance with the existing base zone which is intended for single-family homes.

The existing base zone for the site (Residential-Single Unit Zone, RS-1-14) and overlay zones (Airport Land Use Compatibility Overlay Zone, the Community Plan Implementation Overlay Zone (CPIOZ-A), the Parking Impact Overlay Zone and the Residential Tandem Parking Overlay Zone) are part of the Land Development Code. These regulations are further supplemented by the Costa Verde Specific Plan which allows for a more tailored approach to the proposed Project.

The Specific Plan notes that "Should any inconsistency arise between the development regulations of the base zone or the general regulations of the Municipal Code and the development guidelines of the Costa Verde Specific Plan and/or any implementing Planned Development Permit, the guidelines of the Specific Plan and/or Planned Development Permits shall govern over those of the base zone, the General Regulations of the Municipal Code (Chapter 14) or Land Development Procedures (Chapter 11)."

A Planned Development Permit is required to implement the development standards contained in the proposed CVSP amendment. The Project has been reviewed by City Staff for conformance with the Land Development Code including the Landscape Regulations and Parking Regulations and

complies with both portions of the Municipal Code. Implementation of the design standards of the Specific Plan are subject to conformance with the CPIOZ-A. As the project would conform to the applicable policies and standards of the CVSP (as amended) and Municipal Code, no variances or deviations from the Land Development Code are required. As amended, the Specific Plan provides design guidelines which can be implemented by a Planned Development Permit, without any deviations.

B. PLANNED DEVELOPMENT PERMIT [SDMC Section 126.0605]

1. Findings for all Planned Development Permits:

- a. The proposed development will not adversely affect the applicable land use plan.**

See above for Site Development Permit Finding A.1.a.

- b. The proposed development will not be detrimental to the public health, safety, and welfare.**

See above for Site Development Permit Finding A.1.b.

- c. The proposed development will comply with the regulations of the Land Development Code including any proposed deviations pursuant to Section 126.0602(b)(1) that are appropriate for this location and will result in a more desirable project than would be achieved if designed in strict conformance with the development regulations of the applicable zone, and any allowable deviations that are otherwise authorized pursuant to the Land Development Code..**

See above for Site Development Permit Finding A.1.c.

C. NEIGHBORHOOD DEVELOPMENT PERMIT SDMC Section 126.0404

1. Findings for all Neighborhood Development Permits:

- a. The proposed development will not adversely affect the applicable land use plan.**

See above for Site Development Permit Finding A.1.a.

- b. The proposed development will not be detrimental to the public health, safety, and welfare.**

See above for Site Development Permit Finding A.1.b.

- c. The proposed development will comply with the regulations of the Land Development Code including any allowable deviations pursuant to the Land Development Code.**

See above for Site Development Permit Finding A.1.c.

The above findings are supported by the minutes, maps and exhibits, all of which are incorporated herein by this reference.

[APPROVED] BE IT FURTHER RESOLVED, that Site Development Permit No. 2413426 (SDP), Planned Development Permit No. 1677936 (PDP), and Neighborhood Development Permit No. 2411303 (NDP), is granted to Regency Shopping Centers, Limited Partnership, a California Limited Partnership, Owner/Permittee, under the terms and conditions set forth in the attached permit which is made a part of this resolution, and contingent upon final passage of R-_____ approving amendments to the General Plan, University Community Plan, and Costa Verde Specific Plan.

[DENIED] BE IT FURTHER RESOLVED, that Site Development Permit No. 2413426 (SDP), Planned Development Permit No. 1677936 (PDP), and Neighborhood Development Permit No. 2411303 (NDP), is denied.

RECORDING REQUESTED BY
CITY OF SAN DIEGO
DEVELOPMENT SERVICES
PERMIT INTAKE, MAIL STATION
501

WHEN RECORDED MAIL TO
CITY CLERK
MAIL STATION 2A

INTERNAL ORDER NUMBER: 24006554

SPACE ABOVE THIS LINE FOR RECORDER'S USE

SITE DEVELOPMENT PERMIT NO. 2413426, PLANNED DEVELOPMENT PERMIT NO. 1677936, AND NEIGHBORHOOD DEVELOPMENT PERMIT NO. 2411303 to AMEND PLANNED DEVELOPMENT PERMIT NO. 90-1109 and PLANNED COMMERCIAL DEVELOPMENT PERMIT NO. 85-0783

COSTA VERDE REVITALIZATION - PROJECT NO. 477943 MMRP
CITY COUNCIL

This Site Development Permit No. 2413426, Planned Development Permit No. 1677936, and Neighborhood Development Permit No. 2411303 amending Planned Development Permit No. 90-1109 and Planned Commercial Development Permit 85-0783 is granted by the Council of the City of San Diego to Regency Centers, Limited Partnership, a California Limited Partnership, Owner/Permittee, pursuant to San Diego Municipal Code [SDMC] sections 126.0502, 126.0602 and 126.0402. The 13.9-acre site is located on the north west corner of Nobel Drive and Genesee Avenue, in the RS-1-14 Zone within the Costa Verde Specific Plan area of the University Community Plan. The project site is legally described as: Lots 13 – 14 inclusive of Costa Verde, City of San Diego, County of San Diego, State of California, According to Map thereof No. 12045, filed in the office of the County Recorder of San Diego County, April 18, 1988.

Subject to the terms and conditions set forth in this Permit, permission is granted to Owner/Permittee to remodel an existing neighborhood commercial shopping center described and identified by size, dimension, quantity, type, and location on the approved exhibits [Exhibit "A"] dated XXXXX XX, 2020, on file in the Development Services Department.

The project shall include:

- a. Reconfiguration and expansion of an existing 178,000-square foot commercial shopping center. Expansion to include the addition of approximately 40,000 square feet of commercial office, 360,000 square feet of research and development uses, and the addition of a 10-story, 200-room hotel, comprised of approximately 125,000-square feet on the 13.9acre property;
- b. Landscaping (planting, irrigation and landscape related improvements);
- c. Off-street parking;

- d. Site walls, fences, walkways, and exterior lighting; and
- e. Public and private accessory improvements determined by the Development Services Department to be consistent with the land use and development standards for this site in accordance with the adopted community plan, Costa Verde Specific Plan, the California Environmental Quality Act [CEQA] and the CEQA Guidelines, the City Engineer's requirements, zoning regulations, conditions of this Permit, and any other applicable regulations of the SDMC.

STANDARD REQUIREMENTS:

1. This permit must be utilized within thirty-six (36) months after the date on which all rights of appeal have expired. If this permit is not utilized in accordance with Chapter 12, Article 6, Division 1 of the SDMC within the 36-month period, this permit shall be void unless an Extension of Time has been granted. Any such Extension of Time must meet all SDMC requirements and applicable guidelines in effect at the time the extension is considered by the appropriate decision maker. This permit must be utilized by XXXXX XX, 2023.
2. No permit for the construction, occupancy, or operation of any facility or improvement described herein shall be granted, nor shall any activity authorized by this Permit be conducted on the premises until:
 - a. The Owner/Permittee signs and returns the Permit to the Development Services Department; and
 - b. The Permit is recorded in the Office of the San Diego County Recorder.
3. While this Permit is in effect, the subject property shall be used only for the purposes and under the terms and conditions set forth in this Permit unless otherwise authorized by the appropriate City decision maker.
4. This Permit is a covenant running with the subject property and all of the requirements and conditions of this Permit and related documents shall be binding upon the Owner/Permittee and any successor(s) in interest.
5. The continued use of this Permit shall be subject to the regulations of this and any other applicable governmental agency.
6. Issuance of this Permit by the City of San Diego does not authorize the Owner/Permittee for this Permit to violate any Federal, State or City laws, ordinances, regulations or policies including, but not limited to, the Endangered Species Act of 1973 [ESA] and any amendments thereto (16 U.S.C. § 1531 et seq.).
7. The Owner/Permittee shall secure all necessary building permits. The Owner/Permittee is informed that to secure these permits, substantial building modifications and site improvements

may be required to comply with applicable building, fire, mechanical, and plumbing codes, and State and Federal disability access laws.

8. Construction plans shall be in substantial conformity to Exhibit "A." Changes, modifications, or alterations to the construction plans are prohibited unless appropriate application(s) or amendment(s) to this Permit have been granted.

9. All of the conditions contained in this Permit have been considered and were determined necessary to make the findings required for approval of this Permit. The Permit holder is required to comply with each and every condition in order to maintain the entitlements that are granted by this Permit.

If any condition of this Permit, on a legal challenge by the Owner/Permittee of this Permit, is found or held by a court of competent jurisdiction to be invalid, unenforceable, or unreasonable, this Permit shall be void. However, in such an event, the Owner/Permittee shall have the right, by paying applicable processing fees, to bring a request for a new permit without the "invalid" condition(s) back to the discretionary body which approved the Permit for a determination by that body as to whether all of the findings necessary for the issuance of the proposed permit can still be made in the absence of the "invalid" condition(s). Such hearing shall be a hearing de novo, and the discretionary body shall have the absolute right to approve, disapprove, or modify the proposed permit and the condition(s) contained therein.

10. The Owner/Permittee shall defend, indemnify, and hold harmless the City, its agents, officers, and employees from any and all claims, actions, proceedings, damages, judgments, or costs, including attorney's fees, against the City or its agents, officers, or employees, relating to the issuance of this permit including, but not limited to, any action to attack, set aside, void, challenge, or annul this development approval and any environmental document or decision. The City will promptly notify Owner/Permittee of any claim, action, or proceeding and, if the City should fail to cooperate fully in the defense, the Owner/Permittee shall not thereafter be responsible to defend, indemnify, and hold harmless the City or its agents, officers, and employees. The City may elect to conduct its own defense, participate in its own defense, or obtain independent legal counsel in defense of any claim related to this indemnification. In the event of such election, Owner/Permittee shall pay all of the costs related thereto, including without limitation reasonable attorney's fees and costs. In the event of a disagreement between the City and Owner/Permittee regarding litigation issues, the City shall have the authority to control the litigation and make litigation related decisions, including, but not limited to, settlement or other disposition of the matter. However, the Owner/Permittee shall not be required to pay or perform any settlement unless such settlement is approved by Owner/Permittee.

11. This Permit may be developed in phases. Each phase shall be constructed prior to sale or lease to individual owners or tenants to ensure that all development is consistent with the conditions and exhibits approved for each respective phase per the approved Exhibit "A."

ENVIRONMENTAL/MITIGATION REQUIREMENTS:

12. Mitigation requirements in the Mitigation, Monitoring, and Reporting Program [MMRP] shall apply to this Permit. These MMRP conditions are hereby incorporated into this Permit by reference.

13. The mitigation measures specified in the MMRP and outlined in Environmental Impact Report, No. 477943, shall be noted on the construction plans and specifications under the heading ENVIRONMENTAL MITIGATION REQUIREMENTS.

14. The Owner/Permittee shall comply with the MMRP as specified in Environmental Impact Report, No. 477943, to the satisfaction of the Development Services Department and the City Engineer. Prior to issuance of any construction permit, all conditions of the MMRP shall be adhered to, to the satisfaction of the City Engineer. All mitigation measures described in the MMRP shall be implemented for the following issue areas:

Noise, Transportation/Circulation

WASTE MANAGEMENT PLAN REQUIREMENTS:

15. The Owner/Permittee shall comply with the Waste Management Plan dated January 2020 and shall be enforced and implemented to the satisfaction of the Environmental Services Department.

CLIMATE ACTION PLAN REQUIREMENTS:

16. Owner/Permittee shall comply with the Climate Action Plan (CAP) Consistency Checklist stamped as Exhibit "A." Prior to issuance of any construction permit, all CAP strategies shall be noted within the first three (3) sheets of the construction plans under the heading "Climate Action Plan Requirements" and shall be enforced and implemented to the satisfaction of the Development Services Department.

AIRPORT REQUIREMENTS:

17. Prior to issuance of construction permit, the Owner/Permittee shall provide a valid "Determination of No Hazard to Air Navigation" issued by the Federal Aviation Administration [FAA].

ENGINEERING REQUIREMENTS:

18. Prior to issuance of the Certificate of Occupancy, the Owner/Permittee shall dedicate an additional Right-of-Way, east of the existing driveway, to provide a 14-foot curb-to-property-line distance, adjacent to the site on Nobel Drive and Genesee Avenue, satisfactory to the City Engineer.

19. Prior to the issuance of any building permit, the Owner/Permittee shall assure by permit and bond the construction of a new City standard 26-foot driveway, adjacent to the site on Nobel Drive, satisfactory to the City Engineer.

20. Prior to the issuance of any building permit, the Owner/Permittee shall assure by permit and bond the construction of a new City standard 30-foot driveway, adjacent to the site on Genesee Avenue, satisfactory to the City Engineer.

21. Prior to the issuance of any building permit, the Owner/Permittee shall assure by permit and bond the construction of a new City standard 24-foot driveway, adjacent to the site on Genesee Avenue, satisfactory to the City Engineer.
22. Prior to the issuance of any building permit, the Owner/Permittee shall assure by permit and bond to reconstruct the existing curb with City Standard curb and gutter, adjacent to the site on Genesee Avenue at the right-turn pocket only, satisfactory to City Engineer.
23. Prior to the issuance of any building permits, the Owner/Permittee shall assure by permit and bond the replacement of the existing sidewalk with current City Standard 12 foot wide sidewalk, maintaining the existing sidewalk scoring pattern and preserving the contractor's stamp, adjacent to the site on Nobel Drive and Genesee Avenue, satisfactory to the City Engineer.
24. Prior to the issuance of any building permits, the Owner/Permittee shall assure, by permit and bond, to reconstruct two existing curb ramps, per exhibit 'A', with current City Standard directional curb ramps Standard Drawing SDG-130 and SDG-132 with Detectable/Tactile Warning Tile, on Genesee Avenue, satisfactory to the City Engineer.
25. Prior to the issuance of any building permit, the Owner/Permittee shall obtain an Encroachment Maintenance Removal Agreement for all enhanced pavers, street trees, landscape and irrigation located in the City's right-of-way, satisfactory to the City Engineer.
26. The drainage system proposed for this development, as shown on the site plan, is subject to approval by the City Engineer.
27. Prior to the issuance of any building permit, the Owner/Permittee shall privatize public storm drainpipes on Esplanade Court, satisfactory to the City Engineer.
28. Prior to the issuance of any building permits, the Owner/Permittee shall obtain a bonded grading permit for the grading proposed for this project. All grading shall conform to the requirements of the City of San Diego Municipal Code in a manner satisfactory to the City Engineer.
29. The project proposes to export 198,014 cubic yards of material from the project site. All excavated material listed to be exported, shall be exported to a legal disposal site in accordance with the Standard Specifications for Public Works Construction (the "Green Book"), 2015 edition and Regional Supplement Amendments adopted by Regional Standards Committee.
30. Prior to the issuance of any construction permit, the Owner/Permittee shall incorporate any construction Best Management Practices necessary to comply with Chapter 14, Article 2, Division 1 (Grading Regulations) of the SDMC, into the construction plans or specifications.
31. Prior to the issuance of any construction permit, the applicant shall submit a Technical Report that will be subject to final review and approval by the City Engineer, based on the Storm Water Standards in effect at the time of the construction permit issuance.

32. Prior to the issuance of any building permit, the Owner/Permittee shall enter into a Maintenance Agreement for the ongoing permanent BMP maintenance, satisfactory to the City Engineer.

33. Development of this project shall comply with all storm water construction requirements of the State Construction General Permit, Order No. 2009-0009DWQ, or subsequent order, and the Municipal Storm Water Permit, Order No. R9-2013-0001, or subsequent order. In accordance with Order No. 2009-0009DWQ, or subsequent order, a Risk Level Determination shall be calculated for the site and a Storm Water Pollution Prevention Plan (SWPPP) shall be implemented concurrently with the commencement of grading activities.

34. Prior to issuance of a grading or a construction permit, a copy of the Notice of Intent (NOI) with a valid Waste Discharge ID number (WDID#) shall be submitted to the City of San Diego as a proof of enrollment under the Construction General Permit. When ownership of the entire site or portions of the site changes prior to filing of the Notice of Termination (NOT), a revised NOI shall be submitted electronically to the State Water Resources Board in accordance with the provisions as set forth in Section II.C of Order No. 2009-0009-DWQ and a copy shall be submitted to the City.

LANDSCAPE REQUIREMENTS:

35. Prior to issuance of any grading permit, the Owner/Permittee shall submit complete construction documents for the revegetation and hydro seeding of all disturbed land in accordance with the City of San Diego Landscape Standards, Storm Water Design Manual, and to the satisfaction of the Development Services Department. All plans shall be in substantial conformance to this permit (including Environmental conditions) and Exhibit "A," on file in the Development Services Department.

36. Prior to issuance of any public improvement permit, the Owner/Permittee shall submit complete landscape construction documents for right-of-way improvements to the Development Services Department for approval. Improvement plans shall show, label, and dimension a 40-square-foot area around each tree which is unencumbered by utilities. Driveways, utilities, drains, water and sewer laterals shall be designed so as not to prohibit the placement of street trees.

37. Prior to issuance of any construction permit (including shell), the Owner/Permittee shall submit complete landscape and irrigation construction documents, which are consistent with the Landscape Standards, to the Development Services Department for approval. The construction documents shall be in substantial conformance with Exhibit "A," Landscape Development Plan, on file in the Development Services Department. Construction plans shall provide a 40-square-foot area around each tree that is unencumbered by hardscape and utilities unless otherwise approved per §142.0403(b)5.

38. In the event that a foundation only permit is requested by the Owner/Permittee, a site plan or staking layout plan, shall be submitted to the Development Services Department identifying all landscape areas consistent with Exhibit "A," Landscape Development Plan, on file in the Development Services Department. These landscape areas shall be clearly identified with a distinct symbol, noted with dimensions, and labeled as "landscaping area."

39. The Owner/Permittee shall be responsible for the maintenance of all landscape improvements shown on the approved plans, including in the right-of-way, unless long-term maintenance of said landscaping will be the responsibility of another entity approved by the Development Services Department. All required landscape shall be maintained consistent with the Landscape Standards in a disease, weed, and litter free condition at all times. Severe pruning or "topping" of trees is not permitted.

40. If any required landscape (including existing or new plantings, hardscape, landscape features, etc.) indicated on the approved construction documents is damaged or removed, the Owner/Permittee shall repair and/or replace in kind and equivalent size per the approved documents to the satisfaction of the Development Services Department within 30 days of damage or Certificate of Occupancy.

PLANNING/DESIGN REQUIREMENTS:

41. The automobile, motorcycle and bicycle parking spaces must be constructed in accordance with the requirements of the SDMC. All on-site parking stalls and aisle widths shall be in compliance with requirements of the City's Land Development Code and shall not be converted and/or utilized for any other purpose, unless otherwise authorized in writing authorized by the appropriate City decision maker in accordance with the SDMC.

42. A topographical survey conforming to the provisions of the SDMC may be required if it is determined, during construction, that there may be a conflict between the building(s) under construction and a condition of this Permit or a regulation of the underlying zone. The cost of any such survey shall be borne by the Owner/Permittee.

43. All signs associated with this development shall be consistent with sign criteria established by either the approved Exhibit "A" or City-wide sign regulations.

44. All private outdoor lighting shall be shaded and adjusted to fall on the same premises where such lights are located and in accordance with the applicable regulations in the SDMC.

45. The Owner/Permittee may not convert the research and development designated land use into multi-tenant office space or any other use authorized by the Costa Verde Specific Plan if such conversion would result in overall trip generation for the 360,000 sq. ft. designated as research and development that exceeds 8 average daily trips / ksf of Gross Floor Area.

TRANSPORTATION REQUIREMENTS

46. Prior to the issuance of the first building permit, the Owner/Permittee shall assure by permit and bond the construction of a one-way cycle track along Nobel Drive from Genesee Avenue to Regents Road as shown on Exhibit "A"), satisfactory to the City Engineer.

47. Prior to the issuance of the first building permit, the Owner/Permittee shall assure by permit and bond the reconfiguration of the eastbound approach of the Genesee Avenue/Esplanade Court intersection to provide two dedicated left-turn lanes, a through lane, and a dedicated right-turn lane; install an eastbound right-turn overlap phase; and modification of the traffic signal in conjunction with the changed lane designations, satisfactory to the City Engineer. These improvements shall be completed and operational prior to first occupancy.

48. Prior to the issuance of the first building permit, the Owner/Permittee shall assure by permit and bond the upgrade and/or repair of the signal interconnect communications, detection, and controller equipment on Genesee Avenue between Esplanade Court and Governor Drive, satisfactory to the City Engineer. These improvements shall be completed and operational prior to first occupancy.

49. Prior to the issuance of the first building permit, the Owner/Permittee shall assure by permit and bond the installation of a traffic signal to allow for protected northbound left turns at the Genesee Avenue/State Route 52 westbound ramps intersection, satisfactory to Caltrans and the City Engineer.

50. Prior to the issuance of the first building permit, the Owner/Permittee shall assure by permit and bond the installation at the Genesee Avenue/SR 52 eastbound ramps intersection of a right-turn overlap phasing on the westbound approach and associated traffic signal modification, satisfactory to Caltrans and the City Engineer.

51. Prior to the issuance of the first building permit, Owner/Permittee shall prepare a Transportation Demand Management Program for the site, to include at a minimum the following measures:

- To encourage the use of transit, the project will provide a 25% transit subsidy to hourly employees working on the property. The subsidy value will be limited to the equivalent value of 25% of the cost of an MTS "Regional Adult Monthly/30-Day Pass" (currently \$72 for a subsidy value of \$18 per month). Subsidies will be available to 75% of the hourly employees. The subsidy will be offered at the Opening Day of the project and will be provided for a period of three (3) years.
- Charge salaried employees market-rate for single-occupancy vehicle parking and provide reserved, discounted, or free spaces for registered carpools or vanpools
- Provide carpool/vanpool parking spaces as part of the overall project parking requirements at the project site. These spaces will be signed and striped "carpool/vanpool only."
- Provide showers and locker facilities located within the parking structure adjacent to the security office, as shown on Exhibit "A."
- Maintain an employer network in the SANDAG iCommute program for all tenants/employees
- Provide on-site carsharing and/or bike sharing.
- Provide transit pass sales at the site's concierge.
- Provide a shuttle for workers in the research and development and office buildings to access other properties within the community that are owned by the same entity. If a public zero-emission shuttle is established in the community in the future, provide a stop within the project site.

- Implement smart parking technologies to provide real-time space availability, carpool/vanpool priority, and the option to reserve spaces in advance.
- Install micromobility parking to accommodate a variety of micromobility forms, near the elevators to the trolley.
- Provide additional bicycle and micromobility amenities, such as tire pump/repair stands as well as electric bike and scooter charging stations.
- Consider enhanced wayfinding investments as part of the final design process.

The Transportation Demand Management Program shall state that all measures will be implemented at first occupancy. These measures will be monitored by Owner/Permittee for a period of no less than 3 years with surveys and traffic counts taken annually with a report provided to the City Engineer.

52. Prior to the issuance of the first building permit, the Owner/Permittee shall assure by permit and bond to restripe the westbound approach at the Genesee Avenue/Decoro Street intersection to include a shared through left-turn lane and an exclusive right-turn lane, along with associated traffic signal modifications, satisfactory to the City Engineer. These improvements must be completed and operational prior to first occupancy.

53. Prior to the issuance of the first building permit, the Owner/Permittee shall assure by permit and bond to widen the westbound approach to provide a dedicated left-turn lane and restripe the northbound approach to provide a dedicated right-turn at the Costa Verde Boulevard/Loop Road (South) intersection, satisfactory to the City Engineer. These improvements must be completed and operational prior to first occupancy.

54. Prior to the issuance of the first building permit, the Owner/Permittee shall assure by permit and bond to restripe the southbound approach to provide a dedicated right-turn lane, with associated signal modification at the intersection of Nobel Drive/Costa Verde Boulevard, satisfactory to the City Engineer. These improvements must be completed and operational prior to first occupancy.

GEOLOGY REQUIREMENTS:

55. Prior to the issuance of any construction permits (either grading or building), the Owner/Permittee shall submit a geotechnical investigation report or update letter prepared in accordance with the City's "Guidelines for Geotechnical Reports" that specifically addresses the proposed construction plans. The geotechnical investigation report or update letter shall be reviewed for adequacy by the Geology Section of the Development Services Department prior to issuance of any construction permits.

56. The Owner/Permittee shall submit an as-graded geotechnical report prepared in accordance with the City's "Guidelines for Geotechnical Reports" following completion of the grading. The as-graded geotechnical report shall be reviewed for adequacy by the Geology Section of the Development Services Department prior to exoneration of the bond and grading permit close-out.

PALEONTOLOGY REQUIREMENTS:

57. Prior to issuance of any construction permits, including but not limited to, the first Grading Permit, Demolition Plans/Permits and Building Plans/Permits or a Notice to Proceed for Subdivisions, but prior to the first preconstruction meeting, whichever is applicable, the Assistant Deputy Director (ADD) Environmental designee shall verify that the requirements for Paleontological Monitoring have been noted on the appropriate construction documents in "verbatim" as referenced in the City of San Diego Land Development Manual for Paleontological Resources.

58. The applicant shall submit a letter of verification to Mitigation Monitoring Coordination (MMC)/Environmental Designee (ED) identifying the Principal Investigator (PI), Monitors, and all persons for the project and the names of all persons involved in the paleontological monitoring program, as identified in City of San Diego Land Development Manual for Paleontological Resources.

59. MMC/ED will provide a letter to the applicant confirming the qualifications of the PI, Monitors, and all persons involved in the paleontological monitoring of the project. Professional Qualifications shall meet the following standards:

- a) The Principal Investigator (PI) should have a Ph.D. or M.S. in a field related to paleontology, such as geology or biology with an emphasis in paleobiology. Four cumulative years of full-time professional field, research, and museum experience in working with the geological formations of Southern California is required. Two of the four years must be in a supervisory capacity (crew chief or above).
- b) Paleontological Monitors should have a B.S. in a field related to paleontology, such as geology or biology with an emphasis in paleobiology. Two cumulative years of full-time professional field, research, and museum experience in working with the geological formations of Southern California is required.

60. Prior to the start of work, the applicant shall obtain approval from MMC for any personnel changes associated with the monitoring program.

PUBLIC UTILITIES DEPARTMENT REQUIREMENTS:

61. Prior to issuance of any Construction Permit, any private improvements within the public ROW (including but not limited to: landscaping, enhanced paving, private non-irrigation utilities, or structures of any kind) that could inhibit the City's right to access, maintain, repair, or replace its public water and sewer utilities must be removed unless the Owner/Permittee has a City approved County Recorded Encroachment and Maintenance Removal Agreement (EMRA) which authorizes that specific private improvement to be placed in that specific location.

62. Prior to issuance of the first Construction Permit, any damages caused to the City of San Diego's public water and sewer facilities, which are due to the activities associated with this project, shall be repaired or reconstructed by the Owner/Permittee in a manner satisfactory to the Public Utilities Director and the City Engineer in accordance with Municipal Code section 142.0607.

63. Prior to the issuance of any Construction Permit, the Owner/Permittee shall ensure that all associated easements have been dedicated or vacated as provided for in Exhibit "A".

INFORMATION ONLY:

- The issuance of this discretionary permit alone does not allow the immediate commencement or continued operation of the proposed use on site. Any operation allowed by this discretionary permit may only begin or recommence after all conditions listed on this permit are fully completed and all required ministerial permits have been issued and received final inspection.
- Any party on whom fees, dedications, reservations, or other exactions have been imposed as conditions of approval of this Permit, may protest the imposition within ninety days of the approval of this development permit by filing a written protest with the City Clerk pursuant to California Government Code-section 66020.
- This development may be subject to impact fees at the time of construction permit issuance.

APPROVED by the Council of the City of San Diego on XXXXX XX,2020, by Resolution No. ____.

DRAFT

PDP No. 1677936, SDP 2413426 & NDP No. 2411303
Date of Approval: XXXXX XX, 2020

AUTHENTICATED BY THE CITY OF SAN DIEGO DEVELOPMENT SERVICES DEPARTMENT

Martha Blake
Development Project Manager

**NOTE: Notary acknowledgment
must be attached per Civil Code
section 1189 et seq.**

The undersigned Owner/Permittee, by execution hereof, agrees to each and every condition of
this Permit and promises to perform each and every obligation of Owner/Permittee hereunder.

Regency Centers, Limited Partnership
Owner/Permittee

By _____
John Murphy
Managing Member

**NOTE: Notary acknowledgments
must be attached per Civil Code
section 1189 et seq.**

CITY COUNCIL RESOLUTION NUMBER R-_____
VESTING TENTATIVE PARCEL MAP NO. 2411304,
PUBLIC RIGHT-OF-WAY VACATION NO. 2464871, and EASEMENT
VACATION No. 2413763
COSTA VERDE REVITALIZATION – PROJECT NO. 477943 [MMRP]

WHEREAS, REGENCY CENTER, L.P., a Florida limited partnership, Subdivider, and Kimley-Horn and Associates, Inc. Civil Engineer/Surveyor, submitted an application to the City of San Diego for Vesting Tentative Parcel Map (Map), Public Right-of Way Vacation, and Easement Vacation to subdivide an approximately 13.9-acre site with two (2) existing lots into four (4) lots for the reconfiguration and expansion of an existing 178,000-square-foot commercial shopping center to include a 200-room hotel, 360,000 square feet of Research & Development, and 40,000 square feet of Office use for the Costa Verde Revitalization project (project). The project site is located at the northwest corner of Nobel Drive and Genesee Avenue in the University Community planning area and Costa Verde Specific Plan, and is zoned RS-1-14 zone. The property is legally described as; Lots 13 – 14 inclusive of Costa Verde, City of San Diego, County of San Diego, State of California, According to Map thereof No. 12045, filed in the office of the County Recorder of San Diego County, April 18, 1988; and

WHEREAS, the Map proposes the subdivision of two existing lots (Parcel 1-1.00 acre and Parcel 2-12.230 acres) and vacated right-of-way into four lots (Parcel 1-2.024 acres, Parcel 2-5.149 acres, Parcel 3-5.850 acres and Parcel 4-0.902 acres); and

WHEREAS, the Public Right-of-Way Easement Vacation would allow for Esplanade Court, a cul-de-sac and main entry to the project to become private, and the Easement Vacation would allow for the privatization of existing public utilities. The area of Esplanade Court to be vacated totals 0.698

acres, which would increase the total site area from approximately 13.2 to approximately 13.9 acres;
and

WHEREAS, the project complies with the requirements of a preliminary soils and/or geological reconnaissance report pursuant to Subdivision Map Act sections 66490 and 66491(b)-(f) and San Diego Municipal Code section 144.0220; and

WHEREAS, on September 24, 2020 the Planning Commission of the City of San Diego considered Vesting Tentative Parcel Map No. 2411304 and Public Right-of-Way No. 2413763, and pursuant to Resolution No. [Insert Planning Commission Resolution Number], the Planning Commission voted to recommend City Council approval/denial of the Vesting Tentative Parcel Map and Public Right-of-Way Easement Vacation; and

WHEREAS, under Charter section 280(a) (2) this resolution is not subject to veto by the Mayor because this matter requires the City Council to act as a quasi-judicial body and where a public hearing was required by law implicating due process rights of individuals affected by the decision and where the Council was required by law to consider evidence at the hearing and to make legal findings based on the evidence presented; and

WHEREAS, on [Insert Date], the Council of the City of San Diego considered Vesting Tentative Parcel Map No. 2411304 and Public Right-of-Way Easement Vacation No. 2413763, and pursuant to San Diego Municipal Code section(s) 125.0540 (Approval Requirements for a Parcel Map), 125.0530 (Decision Process for a Parcel Map), 125.0440 (Findings for a Tentative Map), 125.0941 (Findings for Public Right-of-Way Vacation Approval), and Subdivision Map Act section 66428, received for its consideration written and oral presentations, evidence having been submitted, and testimony having been heard from all interested parties at the public hearing, and

the City Council having fully considered the matter and being fully advised concerning the same;

NOW THEREFORE,

BE IT RESOLVED by the Council of the City of San Diego, that it adopts the following findings with respect to Vesting Tentative Parcel Map No. 2411304 and Public Right-of-Way Easement Vacation 2413763:

TENTATIVE MAP-SAN DIEGO MUNICIPAL CODE SECTION (SDMC) 125.0440

(a) The proposed subdivision and its design or improvement is consistent with the policies, goals, and objectives of the applicable land use plan.

The Costa Verde Center Vesting Tentative Parcel Map proposes to subdivide a 13.925-acre property (including vacated Public Right-of-Way) from two to four parcels. Approval of the request also allows for the potential future sale of commercial condominiums and maintenance of common areas within the project.

The subdivision would accommodate the reconfiguration and expansion of an existing commercial center in conformance with the policy goals contained in adopted long-range planning documents. The project site is located in the University Community Plan and Costa Verde Specific Plan, which designates the site for Community Commercial and Neighborhood Commercial Use. The Project proposes General Plan, Community Plan, and Specific Plan amendments to add: (1) 360,000 square feet (SF) of Research & Development and 40,000 SF of Office uses; (2) re-designate approximately one acre from Neighborhood/Community Commercial to Visitor Commercial to allow a 200-room hotel; and (3) adopt incidental technical implementing amendments to address permitted uses, design standards and policies.

The original Costa Verde Specific Plan implemented many of the Smart Growth concepts that decades later became the cornerstone of the Strategic Framework Element of the General Plan. The project focuses growth in a pedestrian-friendly, mixed-use activity center that is linked to the regional transit system. The proposed project represents an update of the fundamental underlying concepts of the currently adopted specific plan and that reflects current conditions including: (1) the soon to be opened Mid-Coast Trolley Terminus Station immediately next to the site above Genesee Ave.; (2) the growing demand in the technology/bio-technology service sectors for clustered employment spaces; (3) the need for a new hotel in a region that currently only contains limited visitor accommodations; and (4) the opportunity to benefit the neighborhood by offering new goods and services during a changing retailing environment.

These proposed amendments would concurrently take place through the project approval process, which would result in future development being consistent with the revised land use designations. Table 5.1-1 of the Costa Verde Center Revitalization Environmental Impact Report (Project No. 477943/SCH No. 201607103) provides a consistency evaluation of the project with the various

elements of the City of San Diego General Plan, the University Community Plan and the Costa Verde Specific Plan demonstrating the subdivision is consistent with the policies, goals and objectives contained in these documents.

(b) The proposed subdivision complies with the applicable zoning and development regulations of the Land Development Code, including any allowable deviations pursuant to the Land Development Code.

The proposed development is governed by several long-range planning documents (the General Plan, University Community Plan and Costa Verde Specific Plan), the Land Development Code, several implementing discretionary permits (Planned Development Permit, Site Development Permit, Neighborhood Use Permit), a Vesting Tentative Parcel Map and Public Right-of-Way Easement Vacation.

The Costa Verde Specific Plan has regulatory controls and development guidelines for the site and surrounding residential community, beginning with the earliest site approvals in 1986. The existing mixed-used community could not have been achieved in strict conformance with the existing base zone which is intended for single-family homes.

The existing base zone for the site (Residential-Single Unit Zone, RS-1-14) and overlay zones (Airport Land Use Compatibility Overlay Zone, the Community Plan Implementation Overlay Zone (CPIOZ-A), the Parking Impact Overlay Zone and the Residential Tandem Parking Overlay Zone) are part of the Land Development Code. These regulations are further supplemented by the Costa Verde Specific Plan which allows for a more tailored approach to the proposed Project.

The Specific Plan notes that "Should any inconsistency arise between the development regulations of the base zone or the general regulations of the Municipal Code and the development guidelines of the Costa Verde Specific Plan and/or any implementing Planned Development Permit, the guidelines of the Specific Plan and/or Planned Development Permits shall govern over those of the base zone, the General Regulations of the Municipal Code (Chapter 14) or Land Development Procedures (Chapter 11)."

A Planned Development Permit is required to implement the development standards contained in the proposed CVSP amendment. The Project has been reviewed by City Staff for conformance with the Land Development Code including the Landscape Regulations and Parking Regulations and complies with both portions of the Municipal Code. Implementation of the design standards of the Specific Plan are subject to conformance with the CPIOZ-A. As the project would conform to the applicable policies and standards of the CVSP (as amended) and Municipal Code, no variances or deviations from the Land Development Code are required. As amended, the Specific Plan provides design guidelines which can be implemented by a Planned Development Permit, without any deviations.

(c) The site is physically suitable for the type and density of development.

The entirety of the approximately 13.925-acre site is developed with an approximately 178,000 square foot shopping center. The site layout is designed with the majority of shops built in a linear

fashion along the western edge of the project site. Associated parking and multiple stand-alone buildings can be found to the east and south of the main shops.

Topography on the site ranges from approximately 335 feet above mean sea level (AMSL) at the southeastern corner of the project site (with the majority of the southern portion of the site at approximately 350 feet AMSL) to approximately 365 feet AMSL at the northwestern corner. There are no natural steep slopes within the project site, and the existing topographical changes are incorporated into the current development. Physical constraints such as steep slopes or natural vegetation or habitats do not exist on the fully developed site.

Local access to the project site is provided by Genesee Avenue, Nobel Drive, Costa Verde Boulevard, and Esplanade Court. Genesee Avenue and Nobel Drive are both classified as six-lane major roadways with raised or fixed medians. Costa Verde Boulevard and Esplanade Court are both four-lane local roadways with raised or fixed medians. Five entrances provide access to the internal roadways and parking areas in the existing center, including two on Esplanade Court, one on Genesee Avenue, one on Nobel Drive, and one on Costa Verde Boulevard.

Vehicular access to the Project would be available from Genesee Avenue, Nobel Drive, Costa Verde Boulevard, and Esplanade Court. The main project access would be provided from an entry at the signalized intersection of Genesee Avenue and Esplanade Court. Esplanade Court would become a private drive and be widened to include two inbound lanes and four outbound lanes. In the center of the road, ramps would provide access to and from the parking structure. Vehicular access will be readily available for the type and density of development envisioned by the project.

Pedestrian circulation would be provided throughout the site by a network of sidewalks, pathways, plazas, and public spaces. These pedestrian facilities would provide connections between the proposed uses within the project, and would connect to existing sidewalks along Genesee Avenue, Nobel Drive, Costa Verde Boulevard, Las Palmas Square, and Esplanade Court. Access to the Trolley Station under construction above Genesee Avenue would be provided with stairs, elevators, and pedestrian bridges. This, in turn, would provide an additional pedestrian connection to the Westfield UTC regional shopping center and UTC Transit Station across Genesee Avenue. Pedestrian connections to Las Palmas Square and the existing adjacent pocket park to the west of the site would also be provided, to enhance connectivity of residences to the west with the Costa Verde Center,

Trolley Station, and UTC Transit Station. Sidewalks along Genesee Avenue and Nobel Drive would be improved to urban parkway configurations, with a 12-foot wide sidewalk, tree grates, and 2 feet of private landscaping within the parkway. Benches would also be provided along Genesee Avenue to enhance pedestrian comfort. High-visibility crosswalk striping would be included at the intersection of Genesee Avenue and Esplanade Court. These project-implemented improvements will improve pedestrian access within the Central Subarea of the University Community Plan and link to the new trolley station above Genesee Ave.

The site is in a highly urbanized area of the City marked by high-density residential development, a regional mall and high-rise commercial development. Immediately surrounding existing land uses include a high-rise continuing care retirement community and high-rise multi-family residential uses to the west, multi-family residential uses to the south all along Nobel Drive, the Westfield UTC regional shopping center to the east, and surface parking and the Monte Verde residential project

under construction to the north. Further north across La Jolla Village Drive are high-rise office buildings and hotels, and more multi-family residential uses. Nearby institutional uses include the UCSD campus and medical center to the northwest of the site, and La Jolla Country Day School and multiple churches and synagogues to the north. A San Diego Police Department (SDPD) substation and City park are also located to the north. While the project is proposing an expansion of the existing development, including a 200-room hotel, 360,000 sq. ft. of Research & Development and 40,000 sq. ft. of commercial/office, the intensity of the proposed development is consistent with the existing uses and intensity of the Central Subarea of the University Community Planning Area. All public facilities and services are available and have the capacity to serve the project.

The existing approximately 13.3-acre shopping center reflects the type of automobile-centric development that was common in the 1980's. Portions of the site could be characterized as under-utilized as they contain large expanses of surface parking. The site's physical conditions do not present any type of constraints (topography, biological resources, access, public facilities and services or incompatibility with adjacent uses) that would preclude the type or density of land uses that are being proposed.

(d) The design of the subdivision or the proposed improvements are not likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat.

Approval of the Vesting Tentative Parcel Map would create two new parcels which would allow for commercial condominium ownership on one lot and joint maintenance of common areas. The developed project site is located in an urban in-fill area and does not contain, nor is adjacent to, any sensitive environmental resources, Multiple Habitat Planning Area (MHPA) Lands, Environmentally Sensitive Lands or existing fish or wildlife habitats. The closest MHPA land is located approximately 1,350 feet to the south, within a small canyon that connects with Rose Canyon.

The Costa Verde Revitalization Environmental Impact Report (Project No. 477943/SCH. No. 2016071031) has been prepared for this project in accordance with State of California Environmental Quality Act (CEQA) Guidelines. A Mitigation, Monitoring and Reporting Program would be implemented with this project to mitigate impacts related to Transportation/Circulation and Noise. A Statement of Overriding Considerations has been adopted for Transportation/Circulation impacts that cannot be mitigated below a level of significance.

(e) The design of the subdivision or the type of improvement will not be detrimental to the public health, safety, and welfare.

The proposed project will not be detrimental to public health, safety and welfare in that the permit controlling the grading and future development of the site contains specific conditions addressing compliance with the City's codes, policies, and regulations, as well as other regional, state, and federal regulations to prevent detrimental impacts to the health, safety and general welfare of persons residing and/or working in the area. Prior to issuance of any grading permits for the proposed project, the plans will be reviewed for compliance with all conditions of approval.

All Uniform Building, Fire, Plumbing, Electrical, Mechanical Code and other regulations governing construction, continued operation and health/life/safety requirements apply to this project. The

project has been conditioned to construct public improvements including new City Standard curbs, gutters, new water meters, new water lateral, new back flow preventers and sidewalks, enhanced crosswalks. As such, the design of the subdivision and related improvements will not be detrimental to the public, health, safety and welfare of the community.

(f) The design of the subdivision or the type of improvements will not conflict with easements acquired by the public at large for access through or use of property within the proposed subdivision.

The Vesting Tentative Parcel Map includes Esplanade Court, an existing five-lane divided public right-of-way cul-de-sac, which is to be vacated and privatized. To allow continued access for the public from Genesee Ave and Brooke Court, 24-foot general utility and emergency vehicle access easements will be dedicated for the improved Esplanade Court.

In conjunction with the trolley station infrastructure, minor portions of an existing Genesee Ave easement will be adjusted by the San Diego Association of Governments (SANDAG) based on their final design. The design of the subdivision will accommodate SANDAG improvements facilitating public access to the new trolley station.

(g) The design of the proposed subdivision provides, to the extent feasible, for future passive or natural heating and cooling opportunities.

The site's linear characteristic integrates the proposed buildings' north/south orientation to minimize excessive exposure to southern heat gain. Project design including building mass, street design and open space has considered the potential impacts for solar access to major areas of the site as well as surrounding uses.

The development proposes to take advantage of the local climate utilizing site specific analysis to identify sustainable design opportunities. Glazing, window to wall ratio, thermal barriers, skylights and exterior shading solutions will be evaluated to maximize energy performance through a preliminary Life Cycle Analysis performance model during Design Development.

A number of landscape treatments utilized in the project also take advantage of cooling opportunities. The project will provide canopy trees on-site and along Genesee Ave, Costa Verde Boulevard, and Nobel Drive. A portion of the existing trees to be removed are Palms which offer less shade than the replacement species that will be implemented by the project. Compliance with the City's current requirements for landscaping in Vehicular Use Areas and the use of green screens by the adjacent senior community will offer shaded cooling areas.

Future development of the site must comply with all applicable building and zoning reviews and requirements, including setbacks, building materials, site orientation, architectural treatments, and landscaping to allow for future passive or natural heating and cooling opportunities.

(h) The decision maker has considered the effects of the proposed subdivision on the housing needs of the region and that those needs are balanced against the needs for public services and the available fiscal and environmental resources.

Costa Verde Center is governed by two long-range planning documents. The approximately 13.9-acre shopping center is situated within the University Community Plan (UCP) and is a portion of the larger 54-acre Costa Verde Specific Plan. The original 1986 specific plan was an early advocate for employing complimentary land uses. The proposed planning program was described in the specific plan as, "An urbanized mixture of residential, commercial and professional office uses is provided by this project, located in the high intensity core or the University Community." The mix of residential and commercial uses envisioned by the two plans has developed over the last several decades on neighboring properties within the planning area. The uses proposed for the project are consistent with approval of community plan amendment requests to re-designate an approximately 1.0-acre site to Visitor Commercial and modify the Land Use and Intensity Table. The project upholds the original vision for Costa Verde in a contemporary context. The program updates an aging shopping center, adds a new hotel in an underserved market, and provides space for the employment of technology workers immediately next to a transit station.

As requested by the Planning Commission during the Community Plan Amendment Initiation, the applicant studied the potential for providing residential units by including 120 mixed-use residential units in a project submittal. While the feasibility study indicated a market existed for the proposed dwelling units, other factors were present, including input from neighbors. During public outreach, the community and particularly immediate neighbors voiced opposition to additional residential use as part of this proposed project.

The adopted community plan does not permit residential use within the shopping center instead designating High Density Residential on the neighboring property in the Costa Verde Specific Plan. In September 2007, a community plan amendment re-designated the adjacent site within the specific plan from Visitor Commercial to High Density Residential. Over 70% of the land area in the Costa Verde Specific Plan is devoted to high-rise residential use, allowing a maximum total of 2,740 dwelling units. The vast majority of the residential units in the specific plan have been built with a portion currently being constructed. The area surrounding the shopping center represents one of the densest residential areas in the City of San Diego outside of downtown. Approximately 18,000 dwelling units are within a 1-mile radius of the Costa Verde Shopping Center. It is not anticipated that every project can or should include residential land uses to address regional housing needs. The proposed project, viewed as part of the University Community Plan and Costa Verde Specific Plan, fulfills the region's goals to balance residential and employment uses near transit.

The above findings are supported by the minutes, maps, and exhibits, all of which are herein incorporated by reference.

BE IT FURTHER RESOLVED, the Esplanade Court Public Right-of-Way easement located within the project boundaries shall be vacated, contingent upon the recordation of the approved Vesting

Tentative Parcel Map for the project, and that the following findings are supported by the minutes, maps, and exhibits, all of which are herein incorporated by reference:

PUBLIC RIGHT-OF-WAY EASEMENT VACATION-SDMC SECTION 125.0941

(a) There is no present or prospective public use for the public right-of-way, either for the facility for which it was originally acquired or for any other public use of a like nature that can be anticipated.

The Esplanade Court public right-of-way, consisting of approximately 30,425 square feet, was granted to the City of San Diego and dedicated for street purposes per Document No. RR-268818, dated July 13, 1987. Esplanade Court is a cul-de-sac that provides access to the parking, circulation, and shopping areas of the Costa Verde Center, a retail shopping center. The existing public right-of-way provides one of the primary means of access to the Costa Verde Center and also connects to a private easement which provides access to the 8875 Towers at Costa Verde, an adjacent multi-family residential development. The existing private easement is not the only access point for the 8875 Towers as the development has frontage on Costa Verde Boulevard.

The Esplanade Court public right-of-way would be replaced by a private easement providing access to both the Costa Verde Center and the 8875 Towers. This private easement would not only continue to provide access from Genesee Avenue to the Costa Verde shopping center, but also will also run in favor of the adjacent residential development off of Brooke Court. Brooke Court is also accessed from Costa Verde Boulevard, a public street. Therefore, there is no present or prospective public use for the public right-of-way either for the facility for which it was originally acquired or for any other public use of a like nature that can be anticipated.

(b) The public will benefit from the action through improved use of the land made available by the vacation.

Vacation of the public right-of-way and subsequent dedication of a private easement will allow for more creative and beneficial uses of the property including enhanced landscaping and maintenance, a superior pedestrian experience, improved paving, signage, and aesthetics while maintaining public and emergency access.

Furthermore, the City of San Diego and the County of San Diego will benefit from the increase of property value by increasing the area of privately-owned land subject to property tax and the incremental increase to the tax base. With the tax revenue increase, the general public will benefit by the vacation of the existing right-of-way through the increase of available public funds. The public will benefit from the reduction of liability to the City of San Diego from land it controls. The City will also be relieved of landscape and street maintenance responsibilities, and the general public will benefit from the associated savings.

(c) The vacation does not adversely affect any applicable land use plan.

Esplanade Court is not listed on the General Plan Land Use and Street System Map, nor is it featured in the University City Community Plan as a feature of the Recommended Street Network. Neither of these plans will be negatively impacted by the vacation of the public right-of-way.

(d) The public facility for which the public right-of-way was originally acquired will not be detrimentally affected by the vacation.

The Esplanade Court public right-of-way was granted to the City of San Diego and dedicated for street purposes per Document No. RR-268818, dated July 13, 1987. While the public right-of-way simply consists of a cul-de-sac, it does provide access to the Costa Verde Center and the 8875 Towers. However, this right-of-way would be replaced by a private easement that would provide access to both sites on the existing Esplanade Court location. Therefore, the public facility for which the public right-of-way was originally acquired would not be detrimentally affected by the vacation.

EASEMENT VACATION – SDMC 125.1040**(a) There is no present or prospective public use for the easement, either for the facility or purpose for which it was originally acquired or for any other public use of a like nature that can be anticipated.**

The existing easement to be vacated is located within an existing public right-of-way that is also proposed to be vacated. The water and sewer easement were acquired for public utilities within a right-of-way to serve the property that is owned by the entity to which the vacation would be granted. The utilities within the vacation serve the existing shopping center. There is no present or prospective public use for the easement proposed to be vacated, and the public would also cease to be responsible for maintenance of the water and sewer services as the easement vacation would privatize existing utilities within the current public right-of-way and become privately owned and maintained.

(b) The public will benefit from the action through improved utilization of the land made available by the vacation.

The area of the easement vacation is within an existing public street that is also proposed to be vacated. The City of San Diego is responsible for the maintenance of public street and utilities. The privatization of utilities within the easement will benefit the public because the public will no longer bear the cost to maintain the utilities that serve the Costa Verde Center. In addition, as privately-owned property, the land would be subject to property taxes that benefit the public.

(c) The vacation is consistent with any applicable land use plan.

The proposed vacation will allow for the private ownership and maintenance of water and sewer utilities. The vacation of the easement is consistent with the existing land use plan, as the vacation itself does not result in any revisions to that plan. In addition, there are companion items to this easement vacation that propose amendments to both the University Community Plan and the Costa Verde Specific Plan to allow the expansion and modification of the existing shopping center. The modifications include development within the area of the easement vacation, and vacation of the

easement would allow for redevelopment of the project site consistent with the proposed land use plan amendments.

(d) The public facility or purpose for which the easement was originally acquired will not be detrimentally affected by the vacation or the purpose for which the easement was acquired no longer exists.

The easement for water and sewer exists to serve the existing shopping center. The privatization of those utilities will eliminate the need for that easement, as the City would no longer be providing maintenance for those utilities. Therefore, the purpose for which the easement exists is no longer required.

BE IT FURTHER RESOLVED, that based on the Findings hereinbefore adopted by the City Council, Vesting Tentative Parcel Map No. 2411304 and Public Right-of-Way Easement Vacation No. 2413763 are hereby granted to Regency Center, L.P., a Florida limited partnership subject to the attached conditions which are made a part of the resolution by reference.

APPROVED: MARA W. ELLIOTT, City Attorney

By _____
[Attorney]
Deputy City Attorney

[Initials]:[Initials]
[Month]/[Day]/[Year]
Or.Dept:[Dept]
R-R-[Reso Code]

ATTACHMENT: Vesting Tentative Parcel Map Conditions and Public Right-of-Way Conditions
Internal Order No. 24006554

CITY COUNCIL
CONDITIONS FOR TENTATIVE MAP NO. 2411304, PUBLIC RIGHT-OF-WAY VACATION NO. 2464871,
AND EASEMENT VACATION NO. 2413763

COSTA VERDE REVITALIZATION - PROJECT NO. 477943 [MMRP]

ADOPTED BY RESOLUTION NO. R-_____ ON _____

GENERAL

1. This Tentative Parcel Map will expire _____, 2023.
2. Compliance with all of the following conditions shall be completed and/or assured, to the satisfaction of the City Engineer, prior to the recordation of the Parcel Map, unless otherwise noted.
3. Prior to the Tentative Map expiration date, a Parcel Map to consolidate the existing lots into one lot shall be recorded in the Office of the San Diego County Recorder.
4. Prior to the recordation of the Parcel Map, taxes must be paid on this property pursuant to Subdivision Map Act section 66492. To satisfy this condition, a tax certificate stating that there are no unpaid lien conditions against the subdivision must be recorded in the Office of the San Diego County Recorder.
5. The Tentative Map shall conform to the provisions of Site Development Permit No. 2413426, Planned Development Permit No. 1677936, and Neighborhood Development Permit No. 2411303.
6. The Subdivider shall defend, indemnify, and hold the City (including its agents, officers, and employees [together, "Indemnified Parties"]) harmless from any claim, action, or proceeding, against the City and/or any Indemnified Parties to attack, set aside, void, or annul City's approval of this project, which action is brought within the time period provided for in Government Code section 66499.37. City shall promptly notify Subdivider of any claim, action, or proceeding and shall cooperate fully in the defense. If City fails to promptly notify Subdivider of any claim, action, or proceeding, or if City fails to cooperate fully in the defense, Subdivider shall not thereafter be responsible to defend, indemnify, or hold City and/or any Indemnified Parties harmless. City may participate in the defense of any claim, action, or proceeding if City both bears its own attorney's fees and costs, City defends the action in good faith, and Subdivider is not required to pay or perform any settlement unless such settlement is approved by the Subdivider.

AIRPORT

7. Prior to recordation of the Parcel Map, the Subdivider shall provide a valid "Determination of No Hazard to Air Navigation" issued by the Federal Aviation Administration [FAA].

ENGINEERING

8. The following will be conditions of the Tentative Map Resolution that the subdivider will need to satisfy/assure before the Final Map is recorded.
9. The Subdivider shall assure, by permit and bond, the dedication of additional Right-of-Way to provide a 14-foot curb-to-property-line distance, adjacent to the site on Nobel Drive and Genesee Avenue, satisfactory to the City Engineer.
10. The Subdivider shall assure by permit and bond the construction of a new City standard 20 ft driveway, adjacent to the site on Nobel Drive, satisfactory to the City Engineer.
11. The Subdivider shall assure by permit and bond the construction of a new City standard 30 ft driveway, adjacent to the site on Genesee Avenue, satisfactory to the City Engineer.
12. The Subdivider shall assure by permit and bond the construction of a new City standard 24 ft driveway, adjacent to the site on Genesee Avenue, satisfactory to the City Engineer.
13. The Subdivider shall assure by permit and bond to reconstruct the existing curb with City Standard curb and gutter, adjacent to the site on Nobel Drive and Genesee Avenue, satisfactory to City Engineer.
14. The Subdivider shall assure by permit and bond the replacement of the existing sidewalk with current City Standard 12 ft sidewalk, maintaining the existing sidewalk scoring pattern and preserving the contractor's stamp, adjacent to the site on Nobel Drive and Genesee Avenue, satisfactory to the City Engineer.
15. The Subdivider shall assure, by permit and bond, to reconstruct existing curb ramp, per exhibit 'A', with current City Standard directional curb ramps Standard Drawing SDG-130 and SDG-132 with Detectable/Tactile Warning Tile, satisfactory to the City Engineer.

16. The Subdivider shall obtain an Encroachment Maintenance Removal Agreement for all enhanced pavers, street trees, landscape and irrigation located in the City's right-of-way, satisfactory to the City Engineer.
17. The drainage system proposed for this development, as shown on the site plan, is subject to approval by the City Engineer.
18. The Subdivider shall privatize public storm drain pipes on Esplanade Court, satisfactory to the City Engineer.
19. The Subdivider shall obtain a bonded grading permit for the grading proposed for this project. All grading shall conform to the requirements of the City of San Diego Municipal Code in a manner satisfactory to the City Engineer.
20. The project proposes to export 198,014 cubic yards of material from the project site. All excavated material listed to be exported, shall be exported to a legal disposal site in accordance with the Standard Specifications for Public Works Construction (the "Green Book"), 2015 edition and Regional Supplement Amendments adopted by Regional Standards Committee.
21. The Subdivider shall incorporate any construction Best Management Practices necessary to comply with Chapter 14, Article 2, Division 1 (Grading Regulations) of the SDMC, into the construction plans or specifications.
22. The Subdivider shall submit a Technical Report that will be subject to final review and approval by the City Engineer, based on the Storm Water Standards in effect at the time of the construction permit issuance.
23. The Subdivider shall enter into a Maintenance Agreement for the ongoing permanent BMP maintenance, satisfactory to the City Engineer.

24. Development of this project shall comply with all storm water construction requirements of the State Construction General Permit, Order No. 2009-0009DWQ, or subsequent order, and the Municipal Storm Water Permit, Order No. R9-2013-0001, or subsequent order. In accordance with Order No. 2009-0009DWQ, or subsequent order, a Risk Level Determination shall be calculated for the site and a Storm Water Pollution Prevention Plan (SWPPP) shall be implemented concurrently with the commencement of grading activities.
25. Prior to issuance of a grading or a construction permit, a copy of the Notice of Intent (NOI) with a valid Waste Discharge ID number (WDID#) shall be submitted to the City of San Diego as a proof of enrollment under the Construction General Permit. When ownership of the entire site or portions of the site changes prior to filing of the Notice of Termination (NOT), a revised NOI shall be submitted electronically to the State Water Resources Board in accordance with the provisions as set forth in Section II.C of Order No. 2009-0009-DWQ and a copy shall be submitted to the City.
26. The Subdivider shall underground any new service run to any new or proposed structures within the subdivision.
27. The Subdivider shall ensure that all existing onsite utilities serving the subdivision shall be undergrounded with the appropriate permits. The Subdivider shall provide written confirmation from applicable utilities that the conversion has taken place, or provide other means to assure the undergrounding, satisfactory to the City Engineer.
28. Conformance with the "General Conditions for Tentative Subdivision Maps," filed in the Office of the City Clerk under Document No. 767688 on May 7, 1980, is required. Only those exceptions to the General Conditions which are shown on the Tentative Map and covered in these special conditions will be authorized. All public improvements and incidental facilities shall be designed in accordance with criteria established in the Street Design Manual, filed with the City Clerk as Document No. RR-297376.

MAPPING

29. "Basis of Bearings" means the source of uniform orientation of all measured bearings shown on the map. Unless otherwise approved, this source shall be the California Coordinate System, Zone 6, North American Datum of 1983 [NAD 83].
30. "California Coordinate System" means the coordinate system as defined in Section 8801 through 8819 of the California Public Resources Code. The specified zone for San Diego County is "Zone 6," and the official datum is the "North American Datum of 1983."
31. The Parcel Map shall:

ATTACHMENT 8

- a. Use the California Coordinate System for its "Basis of Bearing" and express all measured and calculated bearing values in terms of said system. The angle of grid divergence from a true median (theta or mapping angle) and the north point of said map shall appear on each sheet thereof. Establishment of said Basis of Bearings may be by use of existing Horizontal Control stations or astronomic observations.
 - b. Show two measured ties from the boundary of the map to existing Horizontal Control stations having California Coordinate values of First Order accuracy. These tie lines to the existing control shall be shown in relation to the California Coordinate System (i.e., grid bearings and grid distances). All other distances shown on the map are to be shown as ground distances. A combined factor for conversion of grid-to-ground distances shall be shown on the map.
32. This project proposes the subdivision of 13.9 acres of property into four Parcels and the vacation of Esplanade Court. The proposed Parcels 1 and 3 will be subdivided into 20 commercial condominium units and thus requires a Tentative Map which may be approved, conditionally approved, or denied in accordance with Process 5 pursuant to the City of San Diego Land Development Code section 125.0430.
33. Prior to the recordation of the Parcel Map, taxes must be paid or bonded for this property pursuant to section 66492 of the Subdivision Map Act. A current original tax certificate, recorded in the office of the San Diego County Recorder, must be provided to satisfy this condition.
34. If a tax bond is required as indicated in the tax certificate, ensure that it is paid or posted at the County Clerk of the Board of Supervisors Office and supply proof prior to the recordation of the Parcel Map.
35. Prior to the recordation of the Parcel Map, all private streets and drives shall be shown with bearings and distances along the centerline and width of the streets shown on a non-title sheet on the Parcel Map. The street names shall be submitted to BDR-Street Name Coordinator for approval and published on the Parcel Map.
36. The Parcel Map shall be based on field survey and all lot corners must be marked with durable survey monuments pursuant to Section 144.0311(d) of the City of San Diego Land Development Codes and Subdivision Map Act Section 66495.
37. All survey monuments shall be set prior to the recordation of the Parcel Map, unless the setting of monuments is deemed impractical due to the proposed improvements and/or grading associated with the project, in which case, delayed monumentation may be applied on the Parcel Map in accordance with Section 144.0130 of the City of San Diego Land Development Codes.
38. The easements show on the Tentative Map exhibit will be vacated pursuant to Section 66445(j) of the Subdivision Map Act.

WATER & WASTEWATER

39. Prior to the vacation of any public ROW or public easement being approved by the City and recorded with the County, all work within the public ROW that is required to privatize the water main associated with the ROW or easement vacation shall be complete to the satisfaction of the Public Utilities Director and the City Engineer.

GEOLOGY

40. Prior to the issuance of a grading permit, the Subdivider shall submit a geotechnical report prepared in accordance with the City of San Diego's "Guidelines for Geotechnical Reports," satisfactory to the City Engineer.

INFORMATION:

- The approval of this Tentative Map by the City Council of the City of San Diego does not authorize the subdivider to violate any Federal, State, or City laws, ordinances, regulations, or policies including but not limited to, the Federal Endangered Species Act of 1973 and any amendments thereto (16 USC § 1531 et seq.).
- If the Subdivider makes any request for new water and sewer facilities (including services, fire hydrants, and laterals), the Subdivider shall design and construct such facilities in accordance with established criteria in the most current editions of the City of San Diego water and sewer design guides and City regulations, standards and practices pertaining thereto. Off-site improvements may be required to provide adequate and acceptable levels of service and will be determined at final engineering.
- Subsequent applications related to this Vesting Tentative Map will be subject to fees and charges based on the rate and calculation method in effect at the time of payment.
- Any party on whom fees, dedications, reservations, or other exactions have been imposed as conditions of approval of the Vesting Tentative Map, may protest the imposition within ninety days of the approval of this Vesting Tentative Map by filing a written protest with the San Diego City Clerk pursuant to Government Code sections 66020 and/or 66021.
- Where in the course of development of private property, public facilities are damaged or removed, the Subdivider shall at no cost to the City, obtain the required permits for work in the public right-of-way, and repair or replace the public facility to the satisfaction of the City Engineer (San Diego Municipal Code § 142.0607).

Internal Order No. 24006554

RESOLUTION NUMBER R- _____

ADOPTED ON _____

WHEREAS, on April 12, 2016, Regency, LP submitted an application to Development Services Department for a General Plan Amendment, Community Plan Amendment, Specific Plan Amendment, Site Development Permit, Planned Development Permit, Neighborhood Development Permit, Vesting Tentative Map, Public Right-of-Way Vacation, and Easement Vacation for the Costa Verde Revitalization project (Project); and

WHEREAS, on September 24, 2020, the Planning Commission of the City of San Diego considered the Project, and pursuant to Resolution No. XXX voted to recommend City Council XXX of the Project; and

WHEREAS, the matter was set for a public hearing to be conducted by the Council of the City of San Diego (Council); and

WHEREAS, the issue was heard by the Council on [DATE]; and

WHEREAS, the Council considered the issues discussed in Environmental Impact Report No. 477943 / SCH No. 2016071031 (Report) prepared for this Project; and

WHEREAS, under Charter section 280(a)(2) this resolution is not subject to veto by the Mayor because this matter requires the City Council to act as a quasi-judicial body, a public hearing is required by law implicating due process rights of individuals affected by the decision, and the Council is required by law to consider evidence at the hearing and to make legal findings based on the evidence presented; NOW THEREFORE,

BE IT RESOLVED, by the Council that it is certified that the Report has been completed in compliance with the California Environmental Quality Act of 1970 (CEQA) (Public Resources Code Section 21000 et seq.), as amended, and the State CEQA Guidelines thereto (California Code of Regulations, Title 14, Chapter 3, Section 15000 et seq.), that the Report reflects the independent judgment of the City of San Diego as Lead Agency and that the information contained in said Report, together with any comments received during the public review process, has been reviewed and considered by the Council in connection with the approval of the Project.

BE IT FURTHER RESOLVED, that pursuant to CEQA Section 21081 and State CEQA Guidelines Section 15091, the Council hereby adopts the Findings made with respect to the Project, which are attached hereto as Exhibit A.

BE IT FURTHER RESOLVED, that pursuant to State CEQA Guidelines Section 15093, the Council hereby adopts the Statement of Overriding Considerations with respect to the Project, which is attached hereto as Exhibit A.

BE IT FURTHER RESOLVED, that pursuant to CEQA Section 21081.6, the Council hereby adopts the Mitigation Monitoring and Reporting Program, or alterations to implement the changes to the Project as required by this Council in order to mitigate or avoid significant effects on the environment, which is attached hereto as Exhibit B.

BE IT FURTHER RESOLVED, that the Report and other documents constituting the record of proceedings upon which the approval is based are available to the public at the office of the City Clerk, 202 C Street, San Diego, Ca 92101.

BE IT FURTHER RESOLVED, that the City Clerk is directed to file a Notice of Determination with the Clerk of the Board of Supervisors for the County of San Diego regarding the Project.

APPROVED: MARA W. ELLIOTT, City Attorney

By: _____
[NAME], [DEPUTY CITY ATTORNEY or DEVELOPMENT PROJECT MANAGER]

ATTACHMENT(S): Exhibit A, Findings and Statement of Overriding Considerations
 Exhibit B, Mitigation Monitoring and Reporting Program

EXHIBIT B**MITIGATION, MONITORING, AND REPORTING PROGRAM**

SITE DEVELOPMENT PERMIT NO. 2413426, PLANNED DEVELOPMENT PERMIT NO. 1677936, AND NEIGHBORHOOD DEVELOPMENT PERMIT NO. 2411303 to AMEND PLANNED DEVELOPMENT PERMIT NO. 90-1109 and PLANNED COMMERCIAL DEVELOPMENT PERMIT NO. 85-0783

COSTA VERDE REVITALIZATION - PROJECT NO. 477943 MMRP

This Mitigation Monitoring and Reporting Program is designed to ensure compliance with Public Resources Code Section 21081.6 during implementation of mitigation measures. This program identifies at a minimum: the department responsible for the monitoring, what is to be monitored, how the monitoring shall be accomplished, the monitoring and reporting schedule, and completion requirements. A record of the Mitigation Monitoring and Reporting Program will be maintained at the offices of the Land Development Review Division, 1222 First Avenue, Fifth Floor, San Diego, CA, 92101. All mitigation measures contained in the Environmental Impact Report No. 477943 / SCH No. 2016071031 shall be made conditions of Site Development Permit No. 2413426, Planned Development Permit No. 1677936, and Neighborhood Development Permit No. 2411303 to Amend Planned Development Permit No. 90-1109 and Planned Commercial Development Permit No. 85-0783 as may be further described below.

A. GENERAL REQUIREMENTS – PART IPlan Check Phase (Prior to Permit Issuance)

1. Prior to the issuance of a Notice to Proceed (NTP) for a subdivision, or any construction permits, such as Demolition, Grading or Building, or beginning any construction related activity on-site, the Development Services Department (DSD) Director's Environmental Designee (ED) shall review and approve all Construction Documents (CD) (plans, specification, details, etc.) to ensure the MMRP requirements are incorporated into the design.
2. In addition, the ED shall verify that the MMRP Conditions/Notes that apply ONLY to the construction phases of this project are included VERBATIM, under the heading, "ENVIRONMENTAL/MITIGATION REQUIREMENTS."
3. These notes must be shown within the first three (3) sheets of the construction documents in the format specified for engineering construction document templates as shown on the City website:

<http://www.sandiego.gov/development-services/industry/standtemp.shtml>
4. The Title Index Sheet must also show on which pages the "Environmental/Mitigation Requirements" notes are provided.
5. Surety and Cost Recovery – The Development Services Director or City Manager may require appropriate surety instruments or bonds from private Permit Holders to ensure the long-term performance or implementation of required mitigation measures or programs. The City is authorized to recover its cost to offset the salary, overhead, and expenses for City personnel and programs to monitor qualifying projects.

B. GENERAL REQUIREMENTS – PART IIPost Plan Check (After Permit Issuance/Prior to Start of Construction)

1. Pre construction meeting is required ten (10) working days prior to beginning any work on this project. The Permit Holder/Owner is responsible to arrange and perform this meeting by contacting the City Resident Engineer (RE) of the Field Engineering Division and City staff from Mitigation Monitoring Coordination (MMC). Attendees must also include the Permit holder's Representative(s), Job Site Superintendent, and the following consultants:

Qualified Acoustician

NOTE: Failure of all responsible Permit Holder's representatives and consultants to attend shall require an additional meeting with all parties present.

Contact Information:

- a) The Primary Point of Contact is the RE at the Field Engineering Division – 858-627-3200
 - b) For Clarification of environmental requirements, it is also required to call RE and MMC at 858-627-3360
2. MMRP Compliance: This Project, Project Tracking System (PTS) #477943 and/or Environmental Document # 477943, shall conform to the mitigation requirements contained in the associated Environmental Document and implemented to the satisfaction of the DSD's Environmental Designee (MMC) and the City Engineer (RE). The requirements may not be reduced or changed but may be annotated (i.e., to explain when and how compliance is being met and location of verifying proof, etc.). Additional clarifying information may also be added to other relevant plan sheets and/or specifications as appropriate (i.e., specific locations, times of monitoring, methodology, etc.).

NOTE: Permit Holder's Representatives must alert RE and MMC if there are any discrepancies in the plans or notes, or any changes due to field conditions. All conflicts must be approved by RE and MMC BEFORE the work is performed.

3. Other Agency Requirements: Evidence of compliance with all other agency requirements or permits shall be submitted to the RE and MMC for review and acceptance prior to the beginning of work or within one week of the Permit Holder obtaining documentation of those permits or requirements. Evidence shall include copies of permits, letters of resolution or other documentation issued by the responsible agency.

NOTE: Confirmation of NPDES compliance from the State Water Resources Control Board (SWRCB) during and following construction.

4. Monitoring Exhibits: All consultants are required to submit, to RE and MMC, a monitoring exhibit on a 11x17 reduction of the appropriate construction plan, such as site plan, grading, landscape, etc., marked to clearly show the specific areas including the limit of work, scope of that discipline's work, and notes indicating when in the construction schedule that work will be performed. When necessary for clarification, a detailed methodology of how the work will be performed shall be included.

NOTE: Surety and Cost Recovery – When deemed necessary by the Development Services Director or City Manager, additional surety instruments or bonds from the private Permit Holder may be required to ensure the long-term performance or implementation of required mitigation measures or programs. The City is authorized to recover its cost to offset the salary, overhead, and expenses for City personnel and programs to monitor qualifying projects.

5. Other Submittals and Inspections: The Permit Holder/Owner’s representative shall submit all required documentation, verification letters, and requests for all associated inspections to the RE and MMC for approval per the following schedule:

Document Submittal/Inspection Checklist		
Issue Area	Document Submittal	Associated Inspection/ Approvals/Notes
General	Consultant Qualification Letters	Prior to Preconstruction Meeting
General	Consultant Construction Monitoring Exhibits	Prior to or at Preconstruction Meeting
Noise	Acoustical Reports	Noise Mitigation Features Inspection
Bond Release	Request for Bond Release Letter	Final MMRP Inspections Prior to Bond Release Letter

C. SPECIFIC MMRP ISSUE AREA CONDITIONS/REQUIREMENTS FROM EIR

TRANSPORTATION/CIRCULATION

Existing Conditions Plus Project Impacts - Intersections

TRA-1 Genesee Avenue/Esplanade Court

Prior to the issuance of the first construction permit, the Owner/Permittee shall assure by permit and bond the following improvements, satisfactory to the City Engineer to mitigate the Project’s impact to the Genesee Avenue/Esplanade Court intersection:

- Reconfigure the eastbound approach to provide two dedicated left-turn lanes, a through lane and a dedicated right-turn lane. Install an eastbound right-turn overlap phase. Modify the traffic signal in conjunction with the changed lane designations.
- All improvements shall be completed and operational prior to first occupancy.

TRA-2 Genesee Avenue/Governor Drive

Prior to the issuance of the first construction permit, the Owner/Permittee shall assure by permit and bond the following improvement to mitigate the Project’s impact to the Genesee Avenue/ Governor Drive intersection:

- Install right-turn overlap phasing on the southbound approach and modify traffic signal accordingly.

However, the installation of right-turn overlap would prohibit access to the parcel in the northwest corner of the intersection due to the inability to make U-turns. Therefore, this impact is considered significant and unmitigated.

- As partial mitigation, the Project will upgrade and/or repair signal interconnect, communications, detection, and controller equipment on Genesee Avenue between Esplanade Court and Governor Drive.

TRA-3 Genesee Avenue/SR 52 Westbound Ramps

Prior to the issuance of the first construction permit, the Owner/Permittee shall assure by permit and bond the installation of a traffic signal to allow for protected northbound left turns to mitigate the Project's impact to the Genesee Avenue/SR 52 westbound ramps intersection, satisfactory to the City Engineer.

- Install a traffic signal at this intersection to allow for protected northbound left turns.

Although the identified improvements would fully mitigate the impact, the Project's impact to this intersection is considered significant and unmitigated because the timing of the identified improvements are not within the Applicant's or the City's control as it requires Caltrans approval.

TRA-4 Genesee Avenue/SR 52 Eastbound Ramps

Prior to the issuance of the first construction permit, the Owner/Permittee shall assure by permit and bond the installation of the following improvements, satisfactory to the City Engineer to mitigate the Project's impact to the Genesee Avenue/SR 52 eastbound ramps intersection:

- Right-turn overlap phasing on the westbound approach and associated traffic signal modification, satisfactory to Caltrans and the City Engineer.

Although the identified improvements would fully mitigate the impact, the Project's impact to this intersection is considered significant and unmitigated because the timing of the identified improvements are not within the Applicant's or the City's control as it requires Caltrans approval.

Existing Conditions Plus Project Impacts – Freeway Segments**TRA-5 I-5: Gilman Drive to Nobel Drive**

Addition of managed lanes on I-5 between I-8 and La Jolla Village Drive, as identified in SANDAG's 2050 Revenue Constrained RTP, would improve freeway operations. However, there is currently no funding in place at this time and no guarantee that the improvements would occur. As partial mitigation, the Project proposes the following TDM measures to use alternate forms of transportation other than single- occupancy vehicles. The City's Environmental Designee shall verify that the TDM measures listed below are included on the project Construction drawings prior to the issuance of building permits, and that the requirements are implemented.

- Implement a parking management plan, which will charge salaried employees market-rate for single-occupancy vehicle parking and provide reserved, discounted, or free spaces for registered carpools or vanpools.
- Provide carpool/vanpool parking spaces as a part of the overall project parking requirements at the project site. These spaces will be signed and striped "carpool/vanpool parking only."
- Provide shower and locker facilities. These showers and lockers will be located in the parking structure adjacent to the security office.
- Maintain an employer network in the SANDAG iCommute program (which replaces the previous RideMatcher service) to tenants/ employees.
- Provide on-site carsharing vehicle(s) and/or bikesharing.
- Provide a 25 percent transit subsidy to hourly employees working on the property. The subsidy value will be limited to the equivalent value of 25 percent of the cost of a Metropolitan Transit System "Regional Adult Monthly/30-Day Pass" (currently \$72 for a subsidy value of \$18 per month). Subsidies will be

available to 75 percent of the hourly employees. The subsidy will be offered at the Opening Day of the project and will be provided for a period of three years.

- Provide transit pass sales at the site's concierge.
- Provide a shuttle for workers in the research and development and office buildings to access other properties within the community that are owned by the same entity. If a public zero-emission shuttle is established in the community in the future, provide a stop within the project site.
- Implement smart parking technologies to provide real-time space availability, carpool/vanpool priority, and the option to reserve spaces in advance.
- Install micromobility parking to accommodate a variety of micromobility forms, near the elevators to the trolley.
- Provide additional bicycle and micromobility amenities, such as tire pump/repair stands as well as electric bike and scooter charging stations.
- Consider enhanced wayfinding investments as part of the final design process.

In addition, the Project applicant shall prepare a TDM Monitoring and Reporting Program to assess the estimated net reduction in project trips due to the proposed TDM measures. Traffic counts and data relating to paid parking, non-vehicular usage and carpool/vanpool usage shall be collected using on-site person surveys, field visits, and coordination with the property owners and tenants, among others. The Project applicant shall conduct the monitoring program annually for a period of three years. Annual TDM Reports shall be prepared and submitted to the satisfaction of the City Engineer.

Impacts remain significant and unmitigated in the Existing Plus Project scenario.

TRA-6 I-805: Governor Drive to Nobel Drive

Currently, there is one managed lane of I-805 between SR 52 and I-5, which was Stage I of the I-805 North Managed Lanes Project. Stages II through IV of the I-805 North Managed Lanes project would construct the second carpool lane in the median from just north of SR 52 to just north of La Jolla Village Drive. Additionally, the Nobel Drive Direct Access Ramp (DAR) and the Nobel Drive Park & Ride and Transit Station would be constructed and the Governor Drive interchange would be reconfigured. The addition of managed lanes and a new DAR on Nobel Drive would further improve freeway operations on the I-805. The construction start dates for these improvements are pending as there is no funding in place to guarantee that these improvements would be completed. As partial mitigation, the Project proposes several TDM measures (as shown in TRA-5) to incentivize use of alternate forms of transportation other than single-occupancy vehicles. Impacts remain significant and unmitigated in the Existing Plus Project scenario.

TRA-7 SR 52: Genesee Avenue to I-805

The addition of a third lane in each direction along SR 52 between I-5 and I-805, as identified in SANDAG's 2050 Unconstrained Network RTP, would improve freeway operations. However, there is currently no funding in place at this time and no guarantee that the improvements would occur. As partial mitigation, the Project proposes several TDM measures (as shown in TRA-5) to incentivize use of alternate forms of transportation other than single-occupancy vehicles. Impacts remain significant and unmitigated in the Existing Plus Project scenario.

Existing Conditions Plus Project Impacts – Metered Freeway On Ramps**TRA-8 I-805/Nobel Drive Interchange Southbound On-ramp**

Stages II through IV of the I-805 North Managed Lanes (as discussed above), the Nobel Drive DAR, the Nobel Drive Park & Ride and Transit Station, and the reconfiguration of the Governor Drive interchange would relieve the congestion and delay at the freeway ramp meter and improve overall freeway operations, but there is no funding in place to ensure that the improvements would occur. Therefore, impacts at this freeway ramp meter remain significant and unmitigated in the Existing Plus Project scenario. As partial mitigation, the Project proposes TDM measures (as shown in TRA-5) to incentivize use of alternate forms of transportation other than single-occupancy vehicles.

Near-Term (Opening Day 2023) Plus Project Impacts – Intersections**TRA-9 Genesee Avenue/Esplanade Court**

Implementation of TRA-1, as outlined above, would mitigate the Project-related significant impact at the Genesee Avenue/Esplanade Court intersection for the Near Term (Opening Day 2023) Plus Project scenario to a less than significant level.

TRA-10 Genesee Avenue/Decoro Street

Prior to the issuance of the first construction permit, the Owner/Permittee shall assure by permit and bond the following improvements to the satisfaction of the City Engineer to mitigate the Project's impact to the Genesee Avenue/Decoro Street intersection:

- Restripe the westbound approach to include a shared through left-turn lane and an exclusive right-turn lane, along with associated traffic signal modifications. This improvement would require the removal of approximately six on-street parking spaces on the westbound approach.
- All improvements must be completed and operational prior to first occupancy.

TRA-11 Genesee Avenue/Governor Drive

Implementation of TRA-2 would reduce the Project-related significant impact at the Genesee Avenue/Governor Drive intersection for the Near Term (Opening Day 2023) Plus Project scenario to a less than significant level. However, the installation of southbound right-turn overlap would prohibit access to the northwest corner of the intersection due to the inability to make eastbound U-turns. Therefore, this impact is considered significant and unmitigated. As partial mitigation, the Project will upgrade and/or repair signal interconnect, communications, detection and controller equipment on Genesee Avenue between Esplanade Court and Governor Drive.

TRA-12 Genesee Avenue/SR 52 Westbound Ramps

Implementation of TRA-3 would reduce the Project-related significant impact at the Genesee Avenue/SR 52 westbound ramps intersection for the Near Term (Opening Day 2023) Plus Project scenario to a less than significant level. Although the identified improvements would fully mitigate the impact, the Project's impact to this intersection is considered significant and unmitigated because the timing of the identified improvements are not within the Applicant's or City's control as they require Caltrans approval.

TRA-13 Genesee Avenue/SR 52 Eastbound Ramps

Implementation of TRA-4 would reduce the Project-related significant impact to the Genesee Avenue/SR 52 eastbound ramps intersection for the Near-Term (Opening Day 2023) Plus Project scenario to a less than significant level. Although the identified improvements would fully mitigate the impact, the Project's impact to this intersection

is considered significant and unmitigated because the timing of the identified improvements are not within the Applicant's or City's control as they require Caltrans approval.

Near-Term (Opening Day 2023) Plus Project Impacts – Roadway Segments

TRA-14 Genesee Avenue from Decoro Street to Governor Drive

Per the University Community Plan Amendment (December 5, 2016), the widening of Genesee Avenue to six lanes is deemed infeasible. As partial mitigation, the Project will upgrade and/or repair signal interconnect, communications, detection, and controller equipment on Genesee Avenue between Esplanade Court and Governor Drive.

TRA-15 I-5: Gilman Drive to Nobel Drive

Addition of managed lanes on I-5 between I-8 and La Jolla Village Drive, as identified in SANDAG's 2050 Revenue Constrained RTP, would improve freeway operations. However, there is currently no funding in place at this time and no guarantee that the improvements would occur. Implementation of TRA-5 project TDM measures would partially mitigate the Project's impact. Impacts remain significant and unmitigated in the Near-Term (Opening Day 2023) Plus Project scenario.

TRA-16 I-805: Governor Drive to Nobel Drive

Currently, there is one managed lane of I-805 between SR 52 and I-5, which was Stage I of the I-805 North Managed Lanes Project. Stages II through IV of the I-805 North Managed Lanes project would construct the second carpool lane in the median from just north of SR 52 to just north of La Jolla Village Drive. Additionally, the Nobel Drive Direct Access Ramp (DAR) and the Nobel Drive Park & Ride and Transit Station would be constructed and the Governor Drive interchange would be reconfigured. The addition of managed lanes and a new DAR on Nobel Drive would further improve freeway operations on the I-805. The construction start dates for these improvements are pending as there is no funding in place to guarantee that these improvements would be completed. Implementation of TRA-6 project TDM measures would partially mitigate the Project's impact. Impacts remain significant and unmitigated in the Near-Term (Opening Day 2023) Plus Project scenario.

TRA-17 SR 52: Genesee Avenue to I-805

The addition of a third lane in each direction along SR 52 between I-5 and I-805, as identified in SANDAG's 2050 Unconstrained Network RTP, would improve freeway operations. However, there is currently no funding in place at this time and no guarantee that the improvements would occur. Implementation of TRA-7 would partially mitigate the Project's impact. Impacts remain significant and unmitigated in the Near-Term (Opening Day 2023) Plus Project scenario.

Near-Term (Opening Day 2023) Plus Project Impacts –Metered Freeway On-ramps

TRA-18 I-805/Nobel Drive Interchange Southbound On-ramp

Stages II through IV of the I-805 North Managed Lanes (as discussed above), the Nobel Drive DAR, the Nobel Drive Park & Ride and Transit Station, and the reconfiguration of the Governor Drive interchange would relieve the congestion and delay at the ramp meter and improve overall freeway operations, but there is no funding in place to ensure that the improvements would occur. Therefore, impacts at this freeway ramp meter remain significant and unmitigated in the Near-Term (Opening Day 2023) Plus Project scenario. As partial mitigation, the Project proposes TDM measures (as shown in TRA-5) to incentivize use of alternate forms of transportation other than single occupancy vehicles.

Cumulative Impacts – Intersections***TRA-19 La Jolla Village Drive/Genesee Avenue***

Widening the westbound approach to provide a second dedicated right-turn lane is, a condition of approval for the Monte Verde project as included in that project's EIR transportation mitigation measures and permit conditions. The required improvement is currently permitted and bonded by Monte Verde. Therefore, the Project's impact in the Year 2035 scenario at this location is considered less than significant.

TRA-20 Costa Verde Boulevard/Loop Road (South)

Prior to the issuance of the first construction permit, the Owner/Permittee shall assure by permit and bond the following improvements to the satisfaction of the City Engineer to mitigate the Project's cumulative impact to the Costa Verde Boulevard/Loop Road (South) intersection:

- Widen the westbound approach to provide a dedicated left-turn lane. To accommodate the additional lane, approximately 10 feet of widening of the roadway would be required. The additional 10 feet of widening can be accomplished by widening 5 feet on both sides of the driveway.
- Restripe the northbound approach to provide a dedicated right-turn lane.
- All improvements must be completed and operational prior to first occupancy.

TRA-21 Genesee Avenue/Esplanade Court

Implementation of TRA-1, as outlined above, would mitigate the Project's contribution to a significant cumulative impact at the Genesee Avenue/Esplanade Court intersection for the Year 2035 (Community Buildout) Plus Project scenario to a less than significant level.

TRA-22 Nobel Drive/Costa Verde Boulevard

Prior to the issuance of the first construction permit, the Owner/Permittee shall assure by permit and bond the following improvements to the satisfaction of the City Engineer to mitigate the Project's cumulative impact to the Nobel Drive/Costa Verde Boulevard intersection:

- Restripe the southbound approach to provide a dedicated right-turn lane, with associated signal modification.
- All improvements must be completed and operational prior to first occupancy.

TRA-23 Nobel Drive/Genesee Avenue

Prior to the issuance of the first construction permit, the Owner/Permittee shall assure by permit and bond the following improvements to the satisfaction of the City Engineer to mitigate the Project's cumulative impact to the Nobel Drive/Genesee Avenue intersection:

- Install a right-turn overlap phasing on the eastbound approach, with associated signal modification.

However, the installation of an eastbound right-turn overlap would restrict access to the residential development on the west side of Genesee Avenue, south of Nobel Drive due to the inability to make northbound U-turns. Therefore, this impact is considered significant and unmitigated. As partial mitigation, the Project will upgrade and/or repair signal interconnect, communications, detection and controller equipment on Genesee Avenue between Esplanade Court and Governor Drive.

TRA-24 Genesee Avenue/Decoro Street

Implementation of TRA-10, as outlined above, would mitigate the Project's contribution to a significant cumulative impact at the Genesee Avenue/Decoro Street intersection for the Year 2035 (Community Buildout) Plus Project scenario to a less than significant level.

TRA-25 Genesee Avenue/Governor Drive

Implementation of TRA-2, as outlined above, would reduce the Project's cumulative impact at the Genesee Avenue/Governor Drive intersection for the Year 2035 (Community Buildout) Plus Project scenario. However, the installation of southbound right-turn overlap would prohibit access to the northwest corner of the intersection due to the inability to make eastbound U-turns. Therefore, this impact is considered significant and unmitigated. As partial mitigation, the Project will upgrade and/or repair signal interconnect, communications, detection, and controller equipment on Genesee Avenue between Esplanade Court and Governor Drive.

TRA-26 Genesee Avenue/SR 52 Westbound Ramps

Implementation of TRA-3 would reduce the Project's significant impact at the Genesee Avenue/ SR 52 westbound ramps intersection for the Year 2035 (Community Buildout) Plus Project scenario to a less than significant level. Although the identified improvements would fully mitigate the impact, the Project's impact to this intersection is considered significant and unmitigated because the timing of the identified improvements are not within the Applicant's or City's control as they require Caltrans approval.

TRA-27 Genesee Avenue/SR 52 Eastbound Ramps

Implementation of TRA-4 would reduce the Project's significant impact to the Genesee Avenue/SR 52 eastbound ramps intersection for the Year 2035 (Community Buildout) Plus Project scenario to less than significant. Although the identified improvements would fully mitigate the impact, the Project's impact to this intersection is considered significant and unmitigated because the timing of the identified improvements are not within the Applicant's or City's control as they require Caltrans approval.

Cumulative Impacts - Roadway Segments***TRA-28 Genesee Avenue: Nobel Drive to Decoro Street, Decoro Street to Centurion Square, Centurion Square to Governor Drive, Governor Drive to SR 52***

Per the University Community Plan Amendment (December 5, 2016), the widening of Genesee Avenue to 6-lanes was deemed infeasible. As partial mitigation, the Project will upgrade and/or repair signal interconnect, communications, detection and controller equipment on Genesee Avenue between Esplanade Court and Governor Drive.

Cumulative Impacts - Freeway Segments***TRA-29 I-5: Gilman Drive to Nobel Drive***

Addition of managed lanes on I-5 between I-8 and La Jolla Village Drive, as identified in SANDAG's 2050 Revenue Constrained RTP, would improve freeway operations. However, there is currently no funding in place at this time and no guarantee that the improvements would occur. Implementation of TRA-5 project TDM measures would partially mitigate the Project's impact. Impacts remain significant and unmitigated in the cumulative condition.

TRA-30 I-805: Governor Drive to Nobel Drive

Currently, there is one managed lane of I-805 between SR 52 and I-5, which was Stage I of the I-805 North Managed Lanes Project. Stages II through IV of the I-805 North Managed Lanes project would construct the second

carpool lane in the median from just north of SR 52 to just north of La Jolla Village Drive. Additionally, the Nobel Drive Direct Access Ramp (DAR) and the Nobel Drive Park & Ride and Transit Station would be constructed and the Governor Drive interchange would be reconfigured. The addition of managed lanes and a new DAR on Nobel Drive would improve freeway operations on the I-805. The construction start dates for these improvements are pending as there is no funding in place to guarantee that these improvements would be completed. Implementation of TRA-6 project TDM measures would partially mitigate the Project's impact. Impacts remain significant and unmitigated in the cumulative condition.

TRA-31 SR 52: Genesee Avenue to I-805

The addition of a third lane in each direction along SR 52 between I-5 and I-805, as identified in SANDAG's 2050 Unconstrained Network RTP, would improve freeway operations. However, there is currently no funding in place at this time and no guarantee that the improvements would occur. Implementation of TRA-7 project TDM measures would partially mitigate the Project's impact. Impacts remain significant and unmitigated in the cumulative condition.

Cumulative Impacts –Metered Freeway On-ramps

TRA-32 I-805/Nobel Drive Interchange Southbound Ramps

Stages II through IV of the I-805 North Managed Lanes (as discussed above), the Nobel Drive DAR, the Nobel Drive Park & Ride and Transit Station, and the reconfiguration of the Governor Drive interchange would relieve the congestion and delay at the freeway ramp meter and improve overall freeway operations, but there is no funding in place to ensure that the improvements would occur. Therefore, impacts at this freeway ramp meter remain significant and unmitigated in the cumulative condition. As partial mitigation, the Project proposes several TDM measures (as shown in TRA-5) to incentivize use of alternate forms of transportation other than single-occupancy vehicles.

TRA-33 I-5/La Jolla Village Drive Interchange Northbound On-Ramp

The UTC Revitalization project is conditioned to construct a HOV lane at the I-5/La Jolla Village Drive northbound on-ramp. As of January 2020, this improvement is currently under construction and is expected to be completed prior to Year 2035.

NOISE

NOI-1 Event Plaza Noise Barrier

Noise levels from operational noise generated by the Project shall meet the arithmetic mean of the City noise ordinance standards between a commercial and multi-family residential use. This standard is 60 dBA L_{EQ} during the hours between 7:00 a.m. and 7:00 p.m., 55 dBA L_{EQ} during the hours between 7:00 p.m. and 10:00 p.m., and 52.5 dBA L_{EQ} during the hours between 10:00 p.m. and 7:00 a.m. Noise reduction may be accomplished through on-site sound barriers or use restrictions.

To reduce noise levels from live music performances within the Project's event plaza, all performances with amplified sound shall be directed to the east. A moveable or permanent bandshell shall be erected as a noise barrier. The barrier shall be at least 6 feet high and shall be located between the performers and the off-site receptors to the west. If amplified sound is used, any amplification equipment (e.g., speakers) shall not extend above or around the sound barrier, as viewed from the off-site receptors to the west. Non-amplified (acoustic) live music performances shall be permitted without the requirement of a noise barrier.

All sound barriers shall be solid. They shall be constructed of masonry, wood, plastic, fiberglass, steel, or a combination of those materials, with no cracks or gaps through or below the walls. Any seams or cracks shall be filled or caulked. If wood is used, it shall be tongue and groove and must be at least one-inch total thickness or have a density of at least 3.5 pounds per square foot. Where architectural or aesthetic factors allow, glass or clear plastic

3/8-inch thick or thicker may be used. Sheet metal of 18-gauge (minimum) may be used, if it meets the other criteria and is properly supported and stiffened so that it does not rattle or create noise itself from vibration or wind.

Prior to the first outdoor event with amplified sound, the Owner/Permittee shall engage a qualified acoustician to perform and certify a sound test to confirm that noise levels meet the specified standards. The City's Environmental Designee and MMC shall review the test methods and findings and confirm to their satisfaction that sound attenuation meets the specified standards. The noise level needed to ensure compliance shall be noted and the maximum volume level of the speakers shall be identified in Costa Verde Center standard operating procedures, leases, and future event contracts.

NOI-2 HVAC Noise Barriers

Noise levels from operational noise generated by rooftop equipment shall meet the arithmetic mean of the nighttime City noise ordinance standards between a commercial and multi-family residential use. This standard is 52.5 dBA L_{EQ} during the hours between 10:00 p.m. and 7:00 a.m. Noise reduction shall be accomplished through on-site noise barriers.

Sound barriers shall be constructed surrounding the rooftop HVAC units on all Project buildings. On Building B, the barriers shall be incorporated into the proposed 14-foot mechanical screens. On Building T1, the barriers shall be incorporated into the proposed 25-foot mechanical screens. The barriers shall be at least two feet higher than the tallest noise-generating rooftop equipment on all other structures. Barrier construction requirements are the same as those specified in Mitigation Measure NOI-1. The City's Environmental Designee and MMC shall verify the inclusion of these features on project plans prior to the issuance of building permits.

NOI-3 Indoor Music Use Noise Analysis

Prior to issuance of a Conditional Use Permit (CUP) for indoor music use (if and when such use is proposed), a noise analysis shall be completed to assess operational noise sources associated with the indoor music use. Appropriate noise attenuation measures identified in the noise analysis shall be incorporated into the project design to ensure compliance with the City Noise Ordinance limits between a commercial use and multi-family residential use of 60 dBA L_{EQ} during the hours between 7:00 a.m. and 7:00 p.m., 55 dBA L_{EQ} during the hours between 7:00 p.m. and 10:00 p.m., and 52.5 dBA L_{EQ} during the hours between 10:00 p.m. and 7:00 a.m. Methods for ensuring compliant noise levels may include, but not be limited to, the following:

- Restricting music-generating equipment to indoor locations;
- Constructing the building so that the entry doors face away from the adjacent off-site receivers;
- Including a double set of entry doors that are offset to limit noise transmission through the doors; and
- Ensuring that any side or rear doors remain securely closed when music is playing.

NOI-4 Parking Garage Demolition Noise Barriers

Prior to issuance of demolition, grading, or building permits, the City's Environmental Designee and MMC shall ensure the following notes are included on the Project plans. For demolition of the underground parking garage and ground level slabs, if a breaker is used within 145 feet or if a concrete saw is used within 139 feet of the pocket park, a temporary 12-foot-high noise control barrier shall be erected between the breaker and concrete saw and the pocket park to reduce noise levels below the City Noise Ordinance construction threshold of 75 dBA L_{EQ} (12 hour). If applicable, a construction safety barrier may be enhanced to act as a noise control barrier by meeting the specifications listed below.

The temporary noise control barrier shall be tall enough to break the line of sight between the breaker and concrete saw and the sensitive receptor. The sound attenuation barrier shall be solid. It shall be constructed of wood, plywood, or flexible vinyl curtains that meet a rating of Sound Transmission Class (STC) 19, with no cracks or gaps

through or below the wall. Any seams or cracks shall be filled or caulked. If wood or plywood is used, it can be tongue and groove and shall be at least 5/8-inch total thickness or have a density of at least 3.5 pounds per square foot.

Alternative methods (including, but not limited to the use of alternative sound barriers, noise attenuation devices/modifications to construction equipment, limiting hours of operation, or a combination of these measures) may be employed to reduce noise levels below the City Noise Ordinance construction threshold of 75 dBA L_{EQ} (12 hour). For example, for residences located on floors higher than 12 feet at off-site residences facing the project site to the west, noise barriers placed on balconies would reduce noise levels. Where architectural or aesthetic factors allow, glass or clear plastic 3/8 of an inch thick or thicker may be used, if it is desirable to preserve a view. Noise-attenuating materials may be placed on off-site balconies if they meet the criteria listed above for ground-level sound barriers and are properly supported and stiffened so that they do not rattle or create noise itself from vibration or wind. Alternate measures shall be evaluated by a qualified acoustician and approved by the City's Environmental Designee and MMC prior to the initiation of construction activities to ensure that they will reduce noise levels to within City standards. The following additional requirements also will be implemented:

- All construction equipment shall have properly operating and maintained mufflers;
- The construction contractor shall post notices, legible at a distance of 50 feet, at the project construction site. All notices shall indicate the dates and duration of construction activities, as well as provide a contact name and a telephone number where area residents can inquire about the construction process and register complaints;
- An on-site coordinator shall be employed by the project applicant/contractor. The coordinator's duties shall include fielding and documenting noise complaints, determining the source of the complaint (e.g., piece of construction equipment), determining whether noise levels are within acceptable limits and according to City standards, and reporting complaints to the City. The coordinator shall contact nearby noise-sensitive receptors, advising them of the construction schedule; and
- Where feasible during construction, the construction contractor shall place stationary construction equipment in locations where the emitted noise is away from sensitive noise receivers.

NOI-5 Buildings Demolition, Grading, and Building Construction Noise Barriers

Prior to issuance of demolition, grading, or building permits, the City's Environmental Designee and MMC shall ensure the following notes are included on the Project plans. A temporary 12-foot high noise control barrier shall be erected between the construction equipment and residentially zoned property lines within the following distances to reduce noise levels below the City Noise Ordinance construction threshold of 75 dBA L_{EQ} (12 hour):

- 70 feet for demolition and grading using a dozer, loader, and off-highway truck;
- 65 feet for demolition and grading using an excavator, loader, and off-highway truck;
- 41 feet for building construction using a drill;
- 40 feet for building construction using a concrete truck; and
- 49 feet for building construction using a crane.

If applicable, a construction safety barrier may be enhanced to act a noise control barrier by meeting the specifications listed in Mitigation Measure NOI-4.

The temporary noise control barrier shall be tall enough to break the line of sight between the pieces of equipment and the adjacent residentially zoned property. The sound barrier specifications, alternative compliance procedures, and additional requirements shall be the same as those described in Noise Mitigation Measure NOI-4.

DRAFT CANDIDATE FINDINGS AND STATEMENT OF OVERRIDING CONSIDERATIONS
for
COSTA VERDE CENTER REVITALIZATION PROJECT
Project No. 477943
SCH No. 2016071031

I. INTRODUCTION

a. Findings of Fact and Statement of Overriding Considerations

The following Candidate Findings are made for the Costa Verde Center Revitalization Project (Project). The environmental effects of the Project are addressed in the Final Environmental Impact Report (Final EIR) dated September 2020, which is incorporated by reference herein.

The California Environmental Quality Act (CEQA) [Section 21081(a)] and the State CEQA Guidelines [Section 15091(a)] require that no public agency shall approve or carry out a project for which an EIR has been completed which identifies one or more significant effects thereof, unless such public agency makes one or more of the following findings:

1. Changes or alterations have been required in, or incorporated into, the Project which mitigate or avoid the significant effects on the environment;
2. Those changes or alterations are within the responsibility and jurisdiction of another public agency and have been or can or should be adopted by that other agency; or
3. Specific economic, legal, social, technological, or other considerations, including considerations for the provision of employment opportunities for highly trained workers, make infeasible the mitigation measures or alternatives identified in the Final EIR.

CEQA also requires that the findings made pursuant to Section 15091 be supported by substantial evidence in the record (Section 15091(b) of the State CEQA Guidelines). Under CEQA, substantial evidence means that enough relevant information has been provided (and reasonable inferences from this information may be made) that a fair argument can be made to support a conclusion, even though other conclusions might also be reached. Substantial evidence may include facts, reasonable assumptions predicted upon facts, and expert opinion supported by facts (Section 15384 of the State CEQA Guidelines).

CEQA further requires the decision-making agency to balance, as applicable, the economic, legal, social, technological, or other benefits of a proposed project against its unavoidable environmental effects when determining whether to approve the project. If the specific economic, legal, social, technological, or other benefits of a proposed project outweigh the unavoidable adverse environmental effects, the adverse environmental effects may be considered "acceptable" (Section 15093(a) of the State CEQA Guidelines). When the lead agency approves a project which will result in the occurrence of significant effects which are identified in the Final EIR but are not avoided or substantially lessened, the agency shall state in writing the specific reasons to support its actions based on the Final EIR and/or other information in the record.

b. Record of Proceedings

For purposes of CEQA and these Findings and Statement of Overriding Considerations, the Record of Proceedings for the Project consists of the following documents and other evidence:

- The Notice of Preparation (NOP) and all other public notices issued by the City in conjunction with the Project;
- All responses to the NOP received by the City;
- The Final EIR;
- The Draft EIR;
- All written comments submitted by agencies or members of the public during the public review comment period on the Draft EIR;
- All responses to the written comments included in the Final EIR;
- All written and oral public testimony presented during a noticed public hearing for the Project at which such testimony was taken;
- The Mitigation Monitoring and Reporting Program;
- The reports and technical memoranda included or referenced in any responses to comments in the Final EIR;
- All documents, studies, EIRs, or other materials incorporated by reference in, or otherwise relied upon during the preparation of, the EIR;
- Matters of common knowledge to the City, including, but not limited to, federal, state, and local laws and regulations;
- Any documents expressly cited in these Findings and Statement; and
- Any other relevant materials required to be in the record of proceedings by Public Resources Code Section 21167.6(e).

c. Custodian and Location of Records

The documents and other materials which constitute the record of proceedings for the City's actions on the Project are located at the City's Development Services Department (DSD), 1222 1st Avenue, 5th Floor, San Diego, CA 92101. The City's DSD is the custodian of the Project's administrative record. Copies of the document that constitute the record of proceedings are on the City's website and at all relevant times have been available upon request at the offices of the City's DSD. The Draft EIR was also placed on the City's website at www.sandiego.gov/ceqa/draft, and the Final EIR was placed on the City's website at www.sandiego.gov/final. This information is provided in compliance with the Public Resources Code 21081.6(a)(2) and State CEQA Guidelines 15091(e).

II. PROJECT SUMMARY

a. Project Objectives

The objectives of the Costa Verde Center Revitalization Project include the following:

- Revitalize an aging shopping center to better serve present and future community needs by enhancing and diversifying neighborhood/community-serving retail, dining, and commercial opportunities and local services.
- Integrate new land uses (such as commercial office/research and development and visitor accommodations) to create a more vibrant activity center that contributes to the City's goals of smart growth.
- Provide a hotel in a transit-accessible location to serve visitors and the community's research, business, and educational hub.
- Implement transit-supportive land uses and a built environment embracing the Blue Line Trolley Station, which will be located in the center of Genesee Avenue within a Transit Priority Area.
- Increase mobility options by providing pedestrian and bicycle linkages to improve connectivity within the Costa Verde Specific Plan (CVSP) Area and between the center and adjacent neighborhood.
- Provide a place for gathering spots for the public that promote social interaction between University community residents, students, seniors, visitors, and workers.
- Improve the environmental sustainability of the existing retail center through the implementation of features such as energy conservation, sustainable landscape, water conservation, and support for alternative transportation, consistent with the City's Climate Action Plan (CAP).

b. Project Description

The Project entails the reconfiguration and expansion of the existing Costa Verde Center to create a local, walkable hub that provides neighborhood services, retail shops, restaurants, office/research and development uses, a hotel, and community gathering spaces. The Project proposes to retain the current amount (approximately 178,000 square feet [SF]) of commercial/retail uses, add approximately 360,000 SF of research and development, and 40,000 SF of office uses, and re-designate an approximately one-acre portion of the Project site as Visitor Commercial to reintroduce a hotel use to the CVSP area. A 200-room hotel would serve residents, visitors, and the community's research, business, and educational hub. The hotel would be up to 10 stories in height and would encompass approximately 125,000 SF. The maximum building heights would be 45 feet for commercial/retail structures, and 135 feet for commercial/office/research and development and hotel uses.

The northern portion of the center sits approximately 14 feet higher in elevation (approximately 360 feet above mean sea level [AMSL]) than the southern portion of the site (approximately 350 feet AMSL, to approximately 335 feet AMSL). A uniform podium level of approximately 360 feet AMSL would be established across the entire site to provide a more cohesive experience and facilitate mobility throughout the site. The majority of parking would be provided beneath this podium level. At the southern portion of the site, the base of two commercial/retail structures would be located at an elevation similar to the existing ground elevation, but lower than the podium level, due to the

difference in elevation across the site.

The northern portion of the center would consist of a pedestrian-orientated promenade. The promenade would extend southward from a circular style cul-de-sac at the end of Esplanade Court. It would be lined with retail, restaurant, and office/research and development buildings, as well as a central lawn and gathering area, outdoor seating and dining areas, decorative planters, site furniture, landscaping, and accent paving. Elevators and stairs would provide connections to the Trolley Station platform.

The southern portion of the center would be oriented around a surface parking lot. This area is intended for essential neighborhood services, such as a grocery store, pharmacy, and banks. Landscaping and sidewalks would be provided.

The architecture of the center would consist of modern design and materials, consistent with the character of the community's urban core. This would include clean lines and materials such as cast-in-place concrete, fiber cement panels, metal panels, paint over smooth plaster, brick veneer, and wood siding.

III. ENVIRONMENTAL REVIEW PROCESS AND PUBLIC PARTICIPATION

The lead agency approving the Project and conducting environmental review under CEQA (California Public Resources Code Sections 21000, et seq., and the Guidelines promulgated thereunder in California Code of Regulations, Title 14, Sections 15000 et seq. (CEQA Guidelines)), shall be the City of San Diego (City). The City as lead agency shall be primarily responsible for carrying out the Project. In compliance with Section 15082 of the State CEQA Guidelines, the City published a Notice of Preparation on July 12, 2016, which began a 30-day period for comments on the appropriate scope of the EIR. Consistent with CEQA Section 21083.9, the City held a public agency scoping meeting on July 28, 2016 at the Costa Verde Center Hi Neighborhood Room. The purpose of this meeting was to seek input and concerns from the public regarding the environmental issues that may potentially result from the Project.

A previous iteration of the Project was evaluated, and the related environmental effects disclosed in a Draft EIR that was circulated for public review on January 31, 2018; however, since that time, the Project has been redesigned. The City published a Draft EIR addressing the revised project on March 12, 2020 in compliance with CEQA. Pursuant to State CEQA Guidelines Section 15085, upon publication of the Draft EIR, the City filed a Notice of Completion with the Governor's Office of Planning and Research, State Clearinghouse, indicating that the Draft EIR had been completed and was available for review and comment by the public. The City also posted a Notice of Availability of the Draft EIR at this time pursuant to State CEQA Guidelines Section 15087. During the public review period, the City received comments on the environmental document. After the close of public review period, the City provided responses in writing to all comments received on the Draft EIR.

The Final EIR for the Project was published on September 4, 2020. The Final EIR has been prepared in accordance with CEQA and the State CEQA Guidelines.

IV. SUMMARY OF IMPACTS

Impacts associated with specific issue areas (e.g., transportation and noise) resulting from approval of the Project and future implementation are discussed below.

The Final EIR concludes the Project will have **no impacts** with respect to the following issue areas:

- Agriculture and Forestry Resources
- Biological Resources
- Hazardous Materials
- Historical Resources
- Mineral Resources
- Tribal Cultural Resources

The Final EIR concludes the Project will have a **less than significant impact** and require no mitigation measures with respect to the following issue areas:

- Air Quality
- Energy
- Geology and Soils
- Greenhouse Gas Emissions
- Hydrology and Water Quality
- Land Use
- Paleontological Resources
- Population and Housing
- Public Utilities
- Public Services and Facilities
- Visual Effects/Neighborhood Character

The Final EIR concludes the Project will potentially have a **significant impact but mitigated to below a level of significance** with respect to the following issue areas:

- Noise
- Transportation/Circulation (Direct impacts to two intersections; cumulative impacts to five intersections, one metered freeway ramp)

The Final EIR concludes the Project will potentially have a **significant unmitigated impact** and no feasible mitigation measures are available to reduce impacts to below a level of significance for the following issue area:

- Transportation/Circulation (Direct impacts to three intersections, two roadway segment, three

freeway segments, one metered freeway ramp; cumulative impacts to four intersections, six roadway segments, three freeway segments, one metered freeway ramp)

As of July 1, 2020, the City of San Diego was required to change how a project's transportation impacts are evaluated under CEQA by switching from the Level of Service (LOS) metric to the Vehicle Miles Traveled (VMT) metric per Senate Bill 743 (SB 743). The Draft EIR was circulated for public comment prior to July 1, 2020, and prior to the City's adoption of the VMT metric. Pursuant to the CEQA Guidelines, the City policies and standards with regard to transportation analysis in place at the time of public circulation remain applicable to the Project EIR after July 1, 2020. In addition to the applicable Level of Service or "LOS" metric used to evaluate the project's transportation impacts, the Project's transportation/circulation impacts were also evaluated using VMT methodology per the City's draft Transportation Study Manual guidelines (dated 6/10/20). Under the City's VMT methodology, the Project is presumed to have less than significant transportation impacts. Although impacts were presumed to be less than significant, a detailed analysis was completed which concluded that impacts related to VMT would be less than significant. However, the Draft EIR concluded the Project would have significant unmitigated transportation impacts utilizing the City's LOS standards applicable at the time of Draft EIR public circulation.

V. FINDINGS REGARDING SIGNIFICANT IMPACTS

The Findings incorporate the facts and discussions in the Final EIR for the Project as fully set forth therein.

a. Findings Regarding Impacts that Can Be Mitigated to Below a Level of Significance

The City, having independently reviewed and considered the information contained in the Final EIR and the record of proceedings, finds pursuant to CEQA Section 21081(a)(1) and State CEQA Guidelines Section 15091(a)(1) AND adopts the following findings regarding the significant effects of the Project, as follows:

Changes or alterations have been required in, or incorporated into, the Project that mitigate, or avoid, or substantially lessen the significant effects on the environment as identified in the Final EIR. The basis for this conclusion follows.

i. NOISE (Operations)

Impact: Noise levels from Project operations at off-site noise sensitive land uses (NSLUs) would exceed the San Diego Municipal Code (SDMC) noise standards.

Facts in Support of Finding: Operations associated with the Project would include noise generated by heating, ventilation, and air conditioning (HVAC) units, truck deliveries at loading docks, trash compaction, vehicles entering/exiting the parking structure, and indoor/outdoor music events. The noise analysis included an assumption that operational noise levels would occur simultaneously and determined that these activities would result in combined noise levels at NSLUs of up to 61.0 A-weighted decibels over a one-hour average (dBA L_{EQ}). As a result, operational noise levels associated

with the Project could exceed the City's noise level standards that range between 52.5 and 60 dBA L_{EQ} , depending on the time of day or night, which would result in a potentially significant impact. Specifically, potentially significant impacts were identified for outdoor amplified music events, operation of HVAC units, and indoor music events.

Mitigation Measures: Three mitigation measures were identified to address operational noise impacts associated with the Project. Mitigation Measure NOI-1 would require a moveable or permanent bandshell at least 6 feet in height be located between the performers and off-site areas west of the Project during amplified outdoor music events. Prior to an amplified outdoor music event, a sound test by a qualified acoustician is required to demonstrate compliance with the City's applicable noise level standards. The results of the sound test must be reviewed and accepted by the City's Environmental Designee and Mitigation Monitoring Coordination (MMC). Mitigation Measure NOI-2 would require that noise barriers be identified on Project plans around all rooftop HVAC units prior to the issuance of building permits. Lastly, Mitigation Measure NOI-3 specifies that if a Conditional Use Permit (CUP) is obtained for indoor music events, a noise analysis must be completed and demonstrate compliance with the City Noise Ordinance at off-site NSLUs prior to the issuance of the CUP.

Finding: Implementation of mitigation measures NOI-1, NOI-2, and NOI-3 would reduce operational noise impacts to a less than significant level.

Reference: See EIR Section 5.7 for a complete discussion of operational noise impacts associated with the Project.

ii. NOISE (Construction)

Impact: Noise levels from Project construction associated with at off-site NSLUs would exceed the SDMC construction noise standards.

Facts in Support of Finding: Construction associated with the Project includes noise associated with demolition of the underground parking garage, building demolition and grading adjacent to the western property line, and building construction of Buildings A, B, C, D, and L. Demolition of the parking garage would involve the use of a breaker and concrete saw; building demolition and grading would involve the simultaneous use of a dozer or excavator, in addition to a loader and off-highway truck; and building construction would involve the use of an excavator-mounted drill, cement truck, and crane. These three components of construction activity would each exceed the City's construction noise standard of 75 dBA L_{EQ} over a 12-hour period, resulting in a temporary significant impact.

Mitigation Measures: Two mitigation measures were identified to address construction noise impacts associated with the Project. Mitigation Measure NOI-4 would require that Project plans for demolition of the underground parking garage include a note that noise control is required if a breaker or concrete saw is used within 145 or 139 feet of the pocket park, respectively. If an alternate method is desired, it must be reviewed and accepted by the City's Environmental Designee and MMC. Similarly, Mitigation Measure NOI-5 would require that Project plans for building demolition and grading, and building construction include a note that noise control is required if specific types of construction equipment are

within a specific distance of residentially zoned property lines. Distances range between 40 and 70 feet from construction equipment to residentially zoned property lines.

Finding: Implementation of mitigation measures NOI-4 and NOI-5 would reduce construction noise impacts to a less than significant level.

Reference: See EIR Section 5.7 for a complete discussion of construction noise impacts associated with the Project.

iii. TRANSPORTATION/CIRCULATION

Impact: Traffic associated with the Project would result in significant impacts that could be mitigated to a level of below significance under Existing Plus Project conditions at one intersection, Near-Term 2023 Plus Project (Opening Day) scenario at two intersections, and cumulative impacts under Year 2035 (Community Buildout) Plus Project scenario at five intersections and one metered freeway on-ramp at the following locations:

Intersections

- Genesee Avenue/Esplanade Court (Existing Plus Project, Near-Term 2023 Plus Project (Opening Day), and Year 2035 Plus Project);
- Genesee Avenue/Decoro Street (Near-Term 2023 Plus Project (Opening Day) and Year 2035 Plus Project);
- La Jolla Village Drive/Genesee Avenue (Year 2035 Plus Project);
- Costa Verde Boulevard/Loop Road South (Year 2035 Plus Project); and
- Nobel Drive/Costa Verde Boulevard (Year 2035 Plus Project).

Metered Freeway On-ramp

- I-5 Northbound On-Ramp/La Jolla Village Drive (Year 2035 Plus Project)

Intersections

Genesee Avenue/Esplanade Court

Facts in Support of Finding: The intersection of Esplanade Court at Genesee Avenue operates at unacceptable levels under Existing conditions (level of service [LOS] E during the a.m. peak hour and LOS F during the p.m. peak hour) and is expected to remain at an unacceptable LOS under Near-Term 2023 (LOS F during the p.m. peak hour) and Year 2035 (LOS E during the a.m. peak hour and LOS F during the p.m. peak hour) scenarios without the Project. With the addition of Project traffic under Existing, Near-Term 2023, and Year 2035 scenarios, intersection operations would be significantly impacted.

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Mitigation Measures: Mitigation Measure TRA-1 (as well as mitigation measures TRA-9 and TRA-21, both of which refer to Mitigation Measure TRA-1) would be required. The Project would be required to reconfigure the eastbound approach to provide two dedicated left-turn lanes, a through lane, a dedicated right-turn lane and install an eastbound right-turn overlap phase, and modify the traffic signal in conjunction with the changed lane designations.

Finding: With implementation of Mitigation Measures TRA-1, TRA-9, and TRA-21, impacts at Genesee Avenue/Esplanade Court would be reduced to less than significant.

Reference: See EIR Section 5.2 for a complete discussion of transportation impacts associated with the Project.

Genesee Avenue/Decoro Street

Facts in Support of Finding: The intersection of Genesee Avenue/Decoro Street is expected to operate at unacceptable levels in Near-Term 2023 conditions (LOS E during the a.m. peak hour) and Year 2035 scenario (LOS F during a.m. and p.m. peak hours) and is expected to remain at unacceptable levels under Near-Term 2023 (LOS F during p.m. peak hour) and Year 2035 (LOS F during the a.m. and p.m. peak hour) scenarios without the Project. With the addition of Project traffic under Near-Term 2023 Plus Project (Opening Day) and Year 2035 Plus Project scenarios, intersection operations would be significantly impacted.

Mitigation Measures: Mitigation Measure TRA-10 (as well as Mitigation Measure TRA-24, which refers to Mitigation Measure TRA-10) would be required. The Project would be required to restripe of the westbound approach to include a shared through left-turn lane and an exclusive right-turn lane, along with associated traffic signal modifications, and would require the removal of approximately six on-street parking spaces on the westbound approach.

Finding: With implementation of Mitigation Measures TRA-10 and TRA-24, impacts at Genesee Avenue/Decoro Street would be reduced to less than significant.

Reference: See EIR Section 5.2 for a complete discussion of transportation impacts associated with the Project.

La Jolla Village Drive/Genesee Avenue

Facts in Support of Finding: The intersection of La Jolla Village Drive/Genesee Avenue operates at unacceptable levels under Existing conditions (LOS F during the a.m. peak hour and LOS E during the p.m. peak hour) and is expected to remain at unacceptable levels under Near-Term 2023 (LOS E during a.m. and p.m. peak hours) and Year 2035 (LOS F during the a.m. peak hour and LOS E during the p.m. peak hour) without the Project. With the addition of Project traffic, intersection operations would result in a cumulative significant impact under Year 2035 Plus Project scenario only.

Mitigation Measure: Mitigation Measure TRA-19 would be required. Mitigation Measure TRA-19, which

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identifies widening the westbound approach to provide a second dedicated right-turn lane, is a condition of approval for the Monte Verde project as included in that project's EIR transportation mitigation measures and permit conditions. The required improvement is currently permitted and bonded by Monte Verde.

Finding: With implementation of Mitigation Measure TRA-19, impacts at La Jolla Village Drive/Genesee Avenue would be reduced to less than significant.

Reference: See EIR Section 5.2 for a complete discussion of transportation impacts associated with the Project.

Costa Verde Boulevard/Loop Road (South)

Facts in Support of Finding: The intersection of Costa Verde Boulevard/Loop Road South is expected to operate at unacceptable levels under Year 2035 conditions (LOS F during the p.m. peak hour) without the Project. With the addition of Project traffic, intersection operations would be significantly impacted under the Year 2035 Plus Project scenario.

Mitigation Measure: Mitigation Measure TRA-20 would be required. The Project would be required to widen the westbound approach to provide a dedicated left-turn lane. To accommodate the additional lane, approximately 10 feet of widening can be accomplished by widening 5 feet on both sides of the driveway). The Project would also restripe the northbound approach to provide a dedicated right-turn lane.

Finding: With implementation of Mitigation Measure TRA-20, impacts at Costa Verde Boulevard/Loop Road South would be reduced to less than significant.

Reference: See EIR Section 5.2 for a complete discussion of transportation impacts associated with the Project.

Nobel Drive/Costa Verde Boulevard

Facts in Support of Finding: The intersection of Nobel Drive/Costa Verde Boulevard is expected to operate at unacceptable levels under Year 2035 scenario (LOS E during the a.m. and p.m. peak hours) without the Project. With the addition of Project traffic, intersection operations would be significantly impacted.

Mitigation Measure: Mitigation Measure TRA-22 would be required. The Project would be required to restripe the southbound approach to provide a dedicated right-turn lane, with associated signal modification.

Finding: With implementation of Mitigation Measure TRA-22, impacts at Nobel Drive/Costa Verde Boulevard would be reduced to less than significant.

Reference: See EIR Section 5.2 for a complete discussion of transportation impacts associated with the

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Project.

Metered Freeway On-ramps

I-5/La Jolla Village Drive Northbound On-Ramp

Facts in Support of Finding: The northbound metered on-ramp at I-5/La Jolla Village Drive is expected to operate at unacceptable levels under Year 2035 conditions (i.e., experiencing delays in excess of 15 minutes) without the Project. With the addition of Project traffic, metered on-ramp operations would be significantly impacted.

Mitigation Measure: Mitigation Measure TRA-33 would be required. The UTC Revitalization project is conditioned to construct a high-occupancy vehicle (HOV) lane at the I-5/La Jolla Village Drive northbound on-ramp. This improvement has been completed and is open to traffic.

Finding: With implementation of Mitigation Measure TRA-33, which is expected to be completed prior to 2035, the impact at the I-5/La Jolla Drive northbound on-ramp would be reduced to less than significant.

Reference: See EIR Section 5.2 for a complete discussion of transportation impacts associated with the Project.

b. Findings Regarding Impacts that Are Found to be Significant and Unavoidable

The City, having reviewed and considered the information contained in the Final EIR and the Record of Proceedings and pursuant to Public Resource Code §21081(a)(3) and State CEQA Guidelines §15091(a)(3), makes the following findings regarding transportation/circulation.

Specific economic, legal, social, technological, or other considerations, including considerations of the provision of employment opportunities for highly trained workers, make infeasible the mitigation measures or alternatives identified in the Final EIR (Project No. 477943 / SCH No. 2016071031) as described below.

“Feasible” is defined in Section 15364 of the CEQA Guidelines to mean “capable of being accomplished in a successful manner within a reasonable period of time, taking into account economic, environmental, legal, social, and technological factors.” The CEQA statute (Section 21081) and Guidelines (Section 15019(a)(3)) also provide that “other” considerations may form the basis for a finding of infeasibility.

These findings are based on the discussion of impacts in Section 5.2 of the EIR.

i. Transportation/Circulation

Impact: Traffic associated with the Project would result in significant and unavoidable impacts at two intersections, six roadway segments, three freeway segments, and one metered freeway on-ramp at the following locations:

Intersections

- Genesee Avenue/Governor Drive (Existing Plus Project, Near-Term 2023 Plus Project (Opening Day), and Year 2035 Plus Project); and
- Nobel Drive/Genesee Avenue (Year 2035 Plus Project).

Roadway Segments

- La Jolla Village Drive from Genesee Avenue to Executive Way (Year 2035 Plus Project);
- Genesee Avenue from La Jolla Village Drive to Esplanade Court (Year 2035 Plus Project);
- Genesee Avenue from Nobel Drive to Decoro Street (Year 2035 Plus Project);
- Genesee Avenue from Decoro Street to Centurion Square (Near-Term and Year 2035);
- Genesee Avenue from Centurion Square to Governor Drive (Near-Term 2023 Plus Project (Opening Day) and Year 2035 Plus Project); and
- Genesee Avenue from Governor Drive to State Route SR 52 (Year 2035 Plus Project).

Freeway Segments

- I-5: Gilman Drive to Nobel Drive (Existing Plus Project, Near-Term 2023 Plus Project (Opening Day), and Year 2035 Plus Project);
- I-805: Governor Drive to Nobel Drive (Existing Plus Project, Near-Term 2023 Plus Project (Opening Day), and Year 2035 Plus Project); and
- SR 52: Genesee Avenue to I-805 (Existing Plus Project, Near-Term 2023 Plus Project (Opening Day), and Year 2035 Plus Project).

Metered Freeway On-ramps

- I-805/Nobel Drive interchange southbound on-ramp (Existing Plus Project, Near-Term 2023 Plus Project (Opening Day), and Year 2035 Plus Project)

Intersections

Genesee Avenue/Governor Drive

Facts in Support of Finding: The configuration of the southbound approach of this intersection includes a right-turn only lane onto westbound Governor Drive and the eastbound approach includes two left-turn only lanes onto northbound Genesee Avenue. This intersection currently operates at LOS F during the a.m. peak hour and LOS E during the p.m. peak hour. These conditions would further deteriorate with implementation of the Project, ultimately to LOS F in the p.m. peak hour under the Year 2035 Plus

Project scenario.

Mitigation Measures: Implementation of Mitigation Measure TRA-2 (as well as Mitigation Measure TRA-11 and TRA-25) would require the Project to install right-turn overlap phasing on the southbound approach and modify the traffic signal accordingly. However, the installation of southbound right-turn overlap would prohibit access to the parcel in the northwest corner of the intersection due to the inability to make eastbound U-turns. Therefore, this mitigation measure is determined to be infeasible. As partial mitigation, the Project will upgrade and/or repair the signal interconnect, communications, detection, and controller equipment on Genesee Avenue between Esplanade Court and Governor Drive.

Finding: This impact would remain significant and unavoidable in the Existing Plus Project, Near-Term 2023 Plus Project (Opening Day), and Year 2035 Plus Project scenarios.

Reference: See EIR Section 5.2 for a complete discussion of transportation impacts associated with the Project.

Nobel Drive/Genesee Avenue

Facts in Support of Finding: The configuration of the eastbound approach of this intersection includes a right-turn only lane onto southbound Genesee Avenue. This intersection currently operates at LOS D in the a.m. and p.m. peak hours. The LOS would be expected to degrade to LOS F in the a.m. peak hour and LOS E in the p.m. peak hour without the Project in the Year 2035 scenario. With the addition of Project traffic, the LOS would be expected to remain the same, but the increase in delay would result in a significant cumulative impact.

Mitigation Measure: Implementation of Mitigation Measure TRA-23 would require the Project to install right-turn overlap phasing from the eastbound approach with associated traffic signal modification. However, the installation of an eastbound right-turn overlap would restrict access to the residential development on the west side of Genesee Avenue, south of Nobel Drive, due to the inability to make northbound U-turns. Therefore, this mitigation measure is determined to be infeasible. As partial mitigation, the Project will upgrade and/or repairs to the signal interconnect, communications, detection, and controller equipment on Genesee Avenue between Esplanade Court and Governor Drive.

Finding: This impact would remain significant and unavoidable in the Year 2035 Plus Project scenario.

Reference: See EIR Section 5.2 for a complete discussion of transportation impacts associated with the Project.

Roadway Segments

La Jolla Village Drive from Genesee Avenue to Executive Way

Facts in Support of Finding: The functional classification of this roadway segment is a 6-lane Major Arterial and includes street parking. This roadway currently operates at LOS D, but would be expected to

operate at LOS F in the Year 2035 scenario either with or without the Project. The volume-to-capacity ratio increase that would result from the Project would result in cumulative significant impact.

Mitigation Measure: As part of the approvals for the University Community Plan Amendment (CPA) Final Program EIR, the City Council in December 2016 deemed repurposing the segment of La Jolla Village Drive between Genesee Avenue and Executive Way to a 6-lane Prime Arterial to be infeasible as it was determined that on-street parking would remain. As such, the Project's contribution to significant cumulative impacts along La Jolla Village Drive between Genesee Avenue and Executive Way would not be mitigated to a less than significant level..

Finding: This impact would remain significant and unmitigated in the Year 2035 Plus Project scenario.

Reference: See EIR Section 5.2 for a complete discussion of transportation impacts associated with the Project.

Genesee Avenue from La Jolla Village Drive to Esplanade Court

Facts in Support of Finding: The functional classification of this roadway segment is currently a 4-lane Major Arterial and includes a driveway serving the UTC mall. It will be reconstructed back to six lanes upon completion of the Mid-Coast Trolley construction. This roadway currently operates at LOS C, but would be expected to deteriorate to LOS E in the Year 2035 scenario either with or without the Project. The volume-to-capacity ratio increase that would result from the Project would result in a significant cumulative impact.

Mitigation Measure: Per the University Community Plan Amendment (December 5, 2016), the repurposing of this segment to a 6-lane Prime Arterial was deemed infeasible given that the existing conditions include a loading driveway serving the UTC mall. As such, the Project's significant cumulative impacts along Genesee Avenue between La Jolla Village Drive and Esplanade Court would not be mitigated to a less than significant level.

Finding: This impact would remain significant and unmitigated in the Year 2035 Plus Project scenario.

Reference: See EIR Section 5.2 for a complete discussion of transportation impacts associated with the Project.

Genesee Avenue from Nobel Drive to Decoro Street

Facts in Support of Finding: The functional classification of this roadway segment is a 4-lane Major Arterial. This roadway currently operates at LOS D, but would be expected to operate at LOS F in the Year 2035 scenario either with or without the Project. The volume-to-capacity ratio increase would result in a significant cumulative impact.

Mitigation Measure: As part of the approvals for the University CPA , Final Program EIR (SCH: 2015121011), the City Council in December 2016 rejected the widening of Genesee Avenue between

Nobel Drive and the SR 52 westbound ramps to six lanes as infeasible as it would not substantially reduce the significant impacts from the CPA project. Furthermore, the repurposing of Genesee Avenue right-of-way to provide for a modified six lane arterial was also rejected as it would require modification of the existing street design along this segment, including removal of the center median, resulting in a loss of trees, which would be inconsistent with CAP Strategy 5. As partial mitigation, the Project will upgrade and/or repair the signal interconnect, communications, detection and controller equipment on Genesee Avenue between Esplanade Court and Governor Drive. As such, the Project's significant cumulative impacts along Genesee Avenue between Nobel Drive and Decoro Street would not be mitigated to a less than significant level.

Finding: This impact would remain significant and unmitigated in the Year 2035 Plus Project scenario.

Reference: See EIR Section 5.2 for a complete discussion of transportation impacts associated with the Project.

Genesee Avenue from Decoro Street to Governor Drive

Facts in Support of Finding: The functional classification of this roadway segment is a 4-lane Major Arterial. This roadway currently operates at LOS D, but would be expected to operate at LOS E in the Near-Term 2023 scenario and to LOS F in the Year 2035 scenario either with or without the Project. The volume-to-capacity ratio increase that would result from the Project's traffic would result in a significant direct impact and a significant cumulative impact.

Mitigation Measure: As part of the approvals for the University CPA, Final Program EIR (SCH: 2015121011), the City Council in December 2016 rejected the widening of Genesee Avenue between Nobel Drive and the SR 52 westbound ramps to six lanes as infeasible as it would not substantially reduce the significant impacts from the CPA project. Furthermore, the repurposing of Genesee Avenue right-of-way to provide for a modified six lane arterial was also rejected as it would require modification of the existing street design along this segment, including removal of the center median, resulting in a loss of trees, which would be inconsistent with CAP Strategy 5. As partial mitigation, the Project would upgrade and/or repair the signal interconnect, communications, detection, and controller equipment on Genesee Avenue between Esplanade Court and Governor Drive, . As such, the Project's significant direct impact and significant cumulative impact along Genesee Avenue between Decoro Street and Governor Drive would not be mitigated to a less than significant level.

Finding: This impact would remain significant and unmitigated in the Near-Term 2023 Plus Project (Opening Day) and Year 2035 Plus Project scenarios.

Reference: See EIR Section 5.2 for a complete discussion of transportation impacts associated with the Project.

Genesee Avenue from Governor Drive to SR 52

Facts in Support of Finding: The functional classification of this roadway segment is a 4-lane Major

Arterial. This roadway currently operates at LOS D, but would be expected to operate at LOS F in the Year 2035 scenario either with or without the Project. The volume-to-capacity ratio increase would result in a significant cumulative impact.

Mitigation Measure: As part of the approvals for the University CPA, Final Program EIR (SCH: 2015121011), the City Council in December 2016 rejected the widening of Genesee Avenue between Nobel Drive and the SR 52 westbound ramps to six lanes as infeasible as it would not substantially reduce the significant impacts from the CPA project. Furthermore, the repurposing of Genesee Avenue right-of-way to provide for a modified six lane arterial was also rejected as it would require modification of the existing street design along this segment, including removal of the center median, resulting in a loss of trees, which would be inconsistent with CAP Strategy 5. As partial mitigation, the Project would upgrade and/or repair the signal interconnect, communications, detection, and controller equipment on Genesee Avenue between Esplanade Court and Governor Drive, as part of Mitigation Measure TRA-28. As such, the Project's significant cumulative impact along Genesee Avenue between Governor Drive and SR 52 would not be mitigated to a less than significant level.

Finding: This impact would remain significant and unmitigated in the Year 2035 Plus Project scenario.

Reference: See EIR Section 5.2 for a complete discussion of transportation impacts associated with the Project.

Freeway Segments

I-5: Gilman Drive to Nobel Drive

Facts in Support of Finding: Under both Existing and Near-Term 2023 scenarios without the Project, this freeway segment would be expected to operate at LOS E northbound during the p.m. peak hour and southbound during the a.m. peak hour, and at LOS F southbound during the p.m. peak hour. In the Existing Plus Project and Near-Term 2023 Plus Project (Opening Day) scenarios, LOS would remain the same, but a significant direct impact would be expected to occur to the southbound direction during the p.m. peak hour due to the reduction in speed exceeding the allowable threshold. In the Year 2035 scenario without the Project, this freeway segment would be expected to operate at LOS E northbound in the a.m. peak hour and at LOS F northbound during the p.m. peak hour and southbound during both the a.m. and p.m. peak hours. With the Project, LOS would be expected to remain the same, but a significant cumulative impact would occur to the southbound direction during the p.m. peak hour due to the reduction in speed exceeding the allowable threshold.

Mitigation Measures: The addition of managed lanes on I-5 between I-8 and La Jolla Village Drive, as identified in the San Diego Association of Governments (SANDAG) 2050 Revenue Constrained RTP, would improve freeway operations. However, there is currently no funding in place at this time and no guarantee that the improvements would occur.

- As partial mitigation, Mitigation Measure TRA-5 (as well as TRA-15 and TRA-29, which reference TRA-5) requires the following transportation demand management (TDM) measures to

incentivize use of alternate forms of transportation other than single-occupancy vehicles: Provide a 25 percent transit subsidy to hourly employees working on the property. The subsidy value will be limited to the equivalent value of 25 percent of the cost of a Metropolitan Transit System “Regional Adult Monthly/30-Day Pass” (currently \$72 for a subsidy value of \$18 per month). Subsidies will be available to 75 percent of the hourly employees. The subsidy will be offered at the Opening Day of the project and will be provided for a period of three years.

- Implement a parking management plan, which will charge salaried employees market-rate for single-occupancy vehicle parking and provide reserved, discounted, or free spaces for registered carpools or vanpools.
- Provide carpool/vanpool parking spaces as a part of the overall project parking requirements at the project site. These spaces will be signed and striped “carpool/vanpool parking only.”
- Provide shower and locker facilities. These showers and lockers will be located in the parking structure adjacent to the security office.
- Maintain an employer network in the SANDAG iCommute program (which replaces the previous RideMatcher service) to tenants/employees.
- Provide on-site carsharing vehicle(s) and/or bikesharing.
- Provide transit pass sales at the site’s concierge.
- Provide a shuttle for workers in the research and development and office buildings to access other properties within the community that are owned by the same entity. If a public zero-emission shuttle is established in the community in the future, provide a stop within the project site.
- Implement smart parking technologies to provide real-time space availability, carpool/vanpool priority, and the option to reserve spaces in advance.
- Install micromobility parking to accommodate a variety of micromobility forms, near the elevators to the trolley.
- Provide additional bicycle and micromobility amenities, such as tire pump/repair stands as well as electric bike and scooter charging stations.
- Consider enhanced wayfinding investments as part of the final design process.

Finding: Impacts would remain significant and unmitigated in the Existing Plus Project, Near-Term 2023 Plus Project (Opening Day), and Year 2035 Plus Project scenarios.

Reference: See EIR Section 5.2 for a complete discussion of transportation impacts associated with the Project.

I-805: Governor Drive to Nobel Drive

Facts in Support of Finding: Under both Existing and Near-Term 2023 scenarios without the Project, this freeway segment would be expected to operate at LOS F northbound during the a.m. peak hour and southbound during the p.m. peak hour. In the Existing Plus Project and Near-Term 2023 Plus Project (Opening Day) scenarios, LOS would be expected to remain the same, but a significant direct impact would be expected to the northbound direction during the a.m. peak hour due to the reduction in speed exceeding the allowable threshold. In the Year 2035 condition with or without the Project, this freeway

segment would be expected to operate at LOS F northbound in the a.m. peak hour and southbound during the p.m. peak hour. A significant cumulative impact would occur to the northbound direction during the a.m. peak hour due to the reduction in speed exceeding the allowable threshold.

Mitigation Measures: Currently, there is one managed lane of I-805 between SR 52 and I-5, which was constructed as Stage I of the I-805 North Managed Lanes Project. Stages II through IV of the I-805 North Managed Lanes project would construct the second carpool lane in the median from just north of SR 52 to just north of La Jolla Village Drive. Additionally, the Nobel Drive Direct Access Ramp (DAR) and the Nobel Drive Park & Ride and Transit Station would be constructed and the Governor Drive interchange would be reconfigured. The addition of managed lanes and a new DAR on Nobel Drive would improve freeway operations on the I-805. The construction start dates for these improvements are pending as there is no funding in place to guarantee that these improvements would be completed. As partial mitigation, TRA-6 (as well as TRA-16 and TRA-30, which reference TRA-6) requires TDM measures (as indicated above for the impact to I-5: Gilman Drive to Nobel Drive) to incentivize use of alternate forms of transportation other than single-occupancy vehicles.

Finding: Impacts would remain significant and unmitigated in the Existing Plus Project, Near-Term 2023 Plus Project (Opening Day), and Year 2035 Plus Project scenarios.

Reference: See EIR Section 5.2 for a complete discussion of transportation impacts associated with the Project.

SR 52: Genesee Avenue to I-805

Facts in Support of Finding: Under Existing conditions both with and without the Project, this freeway segment would be expected to operate at LOS F eastbound during the a.m. and p.m. peak hours, and at LOS E westbound during the a.m. peak hour. Although the LOS would not change, a significant direct impact would occur to the westbound direction during the a.m. peak hour and eastbound during the p.m. peak hour due to the reduction in speed exceeding the allowable threshold. Under Near-Term 2023 conditions with and without the Project, this freeway segment would be expected to operate at LOS F eastbound during the a.m. and p.m. peak hours, as well as westbound during the a.m. peak hour. Although the LOS would not be expected to change, a significant direct impact would occur to the westbound direction during the a.m. peak hour and eastbound during the p.m. peak hour due to the reduction in speed exceeding the allowable threshold. Under 2035 conditions with and without the Project, this freeway segment would be expected to operate at LOS F eastbound during the a.m. and p.m. peak hours and westbound during the a.m. peak hour, as well as at LOS E westbound during the p.m. peak hour. Although the LOS would not change, the Project's impact to the westbound direction during the a.m. peak hour and eastbound during the p.m. peak hour would result in a cumulative significant impact due to the reduction in speed exceeding the allowable threshold.

Mitigation Measures: The addition of a third lane in each direction along SR 52 between I-5 and I-805, as identified in SANDAG's 2050 Unconstrained Network RTP, would improve freeway operations. However, there is currently no funding in place at this time and no guarantee that the improvements would occur. As partial mitigation, TRA-7 (as well as TRA-17 and TRA-31, which reference TRA-7) requires

TDM measures (as indicated above for the impact to I-5: Gilman Drive to Nobel Drive) to incentivize use of alternate forms of transportation other than single-occupancy vehicles.

Finding: Impacts would remain significant and unmitigated in the Existing Plus Project, Near-Term 2023 Plus Project, and Year 2035 Plus Project scenarios.

Reference: See EIR Section 5.2 for a complete discussion of transportation impacts associated with the Project.

Metered Freeway On-ramps

I-805/Nobel Drive Interchange Southbound On-Ramp

Facts in Support of Finding: Under Existing, Near-Term 2023, and 2035 scenarios both with and without the Project, delay at this on-ramp would exceed the City's threshold of 15 minutes. The Project's contribution to the delay would result in significant direct and cumulative impacts.

Mitigation Measures: Stages II through IV of the I-805 North Managed Lanes (as discussed above), the Nobel Drive DAR, the Nobel Drive Park & Ride and Transit Station, and the reconfiguration of the Governor Drive interchange would relieve the congestion and delay at the freeway ramp meter and improve overall freeway operations, but there is no funding in place to ensure that the improvements would occur. As partial mitigation, TRA-8 (as well as TRA-18 and TRA-32) requires TDM measures (as indicated above for the impact to I-5: Gilman Drive to Nobel Drive) to incentivize use of alternate forms of transportation other than single-occupancy vehicles.

Finding: Impacts at this freeway ramp meter would remain significant and unmitigated in the Existing Plus Project, Near-Term 2023 Plus Project (Opening Day), and Year 2035 Plus Project scenarios.

Reference: See EIR Section 5.2 for a complete discussion of transportation impacts associated with the Project.

VI. FINDINGS REGARDING MITIGATION MEASURES WHICH ARE THE RESPONSIBILITY OF ANOTHER AGENCY (CEQA 21081(a)(2) AND CEQA GUIDELINES 15091 (a)(2))

The City, having reviewed and considered the information contained in the Final EIR and the Record of Proceedings, finds pursuant to CEQA §21081(a)(2) and CEQA Guidelines §15091(a)(2) that there are changes or alterations which could reduce significant impacts that are within the responsibility and jurisdiction of another public agency, and that such changes can and should be adopted by such other agency and/or approved for the Project to implement.

Impact: Traffic associated with the Project would result in significant and unavoidable impacts at two intersections where improvements would require California Department of Transportation (Caltrans) approval, including:

- Genesee Avenue/SR 52 Westbound Ramps (Existing Plus Project, Near-Term 2023 Plus Project (Opening Day), and Year 2035 Plus Project); and
- Genesee Avenue/SR 52 Eastbound Ramps (Existing Plus Project, Near-Term 2023 Plus Project (Opening Day), and Year 2035 Plus Project).

Genesee Avenue/SR 52 Westbound Ramp

Facts in Support of Finding: Under Existing and Near-Term 2023 scenarios with or without the Project, Genesee Avenue/SR 52 Westbound Ramp intersection is expected to operate at LOS F during the p.m. peak hour. In Year 2035 with or without the Project, it is expected to operate at LOS E during the a.m. peak hour and LOS F during the p.m. peak hour. Although the Project would not change the LOS, the addition of Project traffic would exceed the City's thresholds for additional delay at this intersection and cause significant direct and cumulative impacts.

Mitigation Measures: The significant direct and cumulative impacts would be reduced to less than significant through implementation of Mitigation Measure TRA-3 (as well as TRA-12 and TRA-26, which reference TRA-3), which requires that the Owner/Permittee assures by permit and bond the installation of a traffic signal to allow for protected northbound left turns, satisfactory to the California Department of Transportation (Caltrans) and the City Engineer.

Finding: Although the identified improvements would fully mitigate the impact, the Project's impact to this intersection is considered significant and unmitigated because the timing of the identified improvements is not within the Applicant's or the City's control as it requires Caltrans approval.

Reference: See EIR Section 5.2 for a complete discussion of transportation impacts associated with the Project.

Genesee Avenue/SR 52 Eastbound Ramp

Facts in Support of Finding: Under Existing conditions with or without the Project, Genesee Avenue/SR 52 Eastbound Ramp intersection operates and is expected to operate at LOS F during the p.m. peak hour. Although the Project would not change the LOS, the addition of Project traffic would exceed the City's thresholds for additional delay at this intersection, resulting in a significant direct impact. In the Near-Term 2023 scenario, the intersection would continue to operate at LOS with or without the Project in the p.m. peak hour, but the addition of Project traffic would result in degradation from LOS D to LOS E during the a.m. peak hour. Both the increase in delay during the p.m. peak hour and degradation of LOS during the a.m. peak hour would be considered significant direct impacts. In Year 2035 with or without the Project, the intersection would be expected to operate at LOS F during the a.m. and p.m. peak hours. Although the Project would not change the LOS, the addition of Project traffic would exceed the City's thresholds for additional delay at this intersection and the Project would have significant cumulative impact.

Mitigation Measures: The significant direct and cumulative impacts would be reduced to less than significant through implementation of Mitigation Measure TRA-4 (as well as TRA-13 and TRA-27 reference TRA-4), which requires that the Owner/Permittee assures by permit and bond the installation of right-turn overlap phasing on the westbound approach and associated traffic signal modification satisfactory to Caltrans and the City Engineer.

Finding: Although the identified improvements would fully mitigate the impact, the Project's impact to this intersection is considered significant and unmitigated because the timing of the identified improvements are not within the Applicant's or the City's control as it requires Caltrans approval.

Reference: See EIR Section 5.2 for a complete discussion of transportation impacts associated with the Project.

VII. FINDINGS REGARDING PROJECT ALTERNATIVES

In accordance with Section 15126.6(a) of the Guidelines, an EIR must contain a discussion of "a range of reasonable alternatives to a project, or the location of a project, which would feasibly attain most of the basic objectives of the project but would avoid or substantially lessen any of the significant effects of the project, and evaluate the comparative merits of the alternatives." Section 15126.6(f) further states that "the range of alternatives in an EIR is governed by the 'rule of reason' that requires the EIR to set forth only those alternatives necessary to permit a reasoned choice." Thus, the following discussion focuses on project alternatives that are capable of eliminating significant environmental impacts or substantially reducing them as compared to the proposed Project, even if the alternative would impede the attainment of some project objectives, or would be more costly. In accordance with Section 15126.6(f)(1) of the Guidelines, among the factors that may be taken into account when addressing the feasibility of alternatives are: (1) site suitability; (2) economic viability; (3) availability of infrastructure; (4) general plan consistency; (5) other plans or regulatory limitations; (6) jurisdictional boundaries; and (7) whether the proponent can reasonably acquire, control, or otherwise have access to the alternative site.

As required in Section 15126.6(a), in developing the alternatives to be addressed in this section, consideration was given to an alternative's ability to meet most of the basic objectives of the project. Because the Project will cause potentially significant environmental effects unless mitigated, the City must consider the feasibility of any environmentally superior alternatives to the project, evaluating whether these alternatives could avoid or substantially lessen the potentially significant environmental effects while achieving most of the objectives of the project.

The City, having reviewed and considered the information contained in the Final EIR and the Record of Proceedings, and pursuant to Public Resource Code §21081(a)(3) and State CEQA Guidelines §15091(a)(3), makes the following findings with respect to the alternatives identified in the Final EIR (Project No. 477943/ SCH No. 2016071031):

Specific economic, legal, social, technological, or other considerations, including considerations of the provision of employment opportunities for highly trained workers, make infeasible the mitigation

measures or alternatives identified in the Final EIR (Project No. 442880 / SCH No. 2016031026) as described below.

“Feasible” is defined in Section 15364 of the CEQA Guidelines to mean “capable of being accomplished in a successful manner within a reasonable period of time, taking into account economic, environmental, legal, social, and technological factors.” The CEQA statute (Section 21081) and Guidelines (Section 15019(a)(3)) also provide that “other considerations” may form the basis for a finding of infeasibility.

a) Alternatives under Consideration

The project alternatives are summarized below along with the findings relevant to each alternative.

1. No Project Alternative

CEQA Guidelines, Section 15126.6(e), requires that an EIR evaluate a “no project” alternative along with its impact. The purpose of describing and analyzing a no project alternative is to allow a lead agency to compare the impacts of approving the project to the impacts of not approving it. Under the No Project/No Development Alternative, the project would not be implemented, and the site would remain in its current condition. Accordingly, the No Project Alternative assumes that the Project would not be adopted, no redevelopment of the existing retail uses would be implemented, and no new hotel, office, or research and development uses would be constructed. With completion of the Monte Verde towers currently under construction, the existing CVSP area will be completely built out, and no additional work would occur to fulfill the existing plan. The pedestrian bridges planned to extend from the Trolley station would connect to elevators and stairways that extend into a landscaped area at the eastern edge of the Costa Verde Center, in accordance with plans developed by SANDAG. Modifications to the Costa Verde Center and off-site improvements to improve connectivity between transit, bicycle, and pedestrian modes would not occur.

Potentially Significant Effects:

The No Project Alternative would avoid the significant and unmitigated (or unavoidable) impacts to transportation/circulation (traffic congestion) as well as short-term construction and long-term operational noise identified for the Project. It also would incrementally reduce impacts to paleontological resources, public utilities, and public services and facilities, which would be less than significant for the Project. This alternative would not generate additional fees to address existing deficiencies in public facilities. It would be similar to the Project with regard to geology. This alternative would not require plan amendments but would be less preferred than the Project with regard to consistency with the environmental goals and objectives of applicable land use plans. It also would be less preferred with regard to alternative transportation modes, aesthetics, and hydrology/water quality, due to the retention of existing conditions as opposed to the upgrades that are proposed by the Project. With regard to air quality, GHG, and energy, this alternative would result in reduced

impacts on a site-specific basis. It would not, however, implement strategies designed to reduce these impacts on a regional, long-term basis.

Facts In Support of Finding:

While the No Project Alternative would reduce the significant environmental effects associated with Project related to construction noise and operational noise and transportation/circulation, it would not meet any of the Project objectives. The No Project Alternative would not revitalize an aging shopping center, integrate new land uses to better serve present and future community needs, or create a more vibrant activity center that contributes to the goals of smart growth and supports transit (Objectives 1 through 4). It also would not increase mobility options by providing improved pedestrian and bicycle linkages between the center and the adjacent neighborhood (Objective 5), provide a place for public gathering spots that promote social interaction (Objective 6), or improve the sustainability of the existing center through features consistent with the City's CAP (Objective 7).

Finding:

The No Project Alternative is rejected because specific economic, social, or other considerations including matters of public policy make this alternative infeasible.

Rationale:

This alternative is rejected because it could not feasibly accomplish the basic objectives of the Project.

Reference:

See EIR Section 8.4.1 for a complete analysis of this alternative.

2. Retail, Hotel, and Residential Alternative

The Retail, Hotel, and Residential Alternative reflects the project as submitted to the City in March 2016 and circulated for public review in January 2018. This alternative would involve increasing the development intensity of commercial/retail uses by approximately 125,000 SF for a total of approximately 303,000 SF distributed among a total of 15 new and existing buildings and redesignating an approximately one-acre portion of the project site to Visitor Commercial to reintroduce a hotel use to the CVSP area. A 200-room hotel would serve residents, visitors, and the community's research, business, and educational hub. Additionally, a mixed-use residential component, consisting of ground floor retail and six floors of multi-family residential use (with the top floor incorporating a mezzanine level) totaling 120 units would be incorporated as a future project phase.

The hotel would be up to 10 stories in height, up to a maximum of 125 feet, and would encompass approximately 125,000 SF. The maximum height of commercial structures would

be 90 feet and the mixed-use residential component would total a maximum height of 100 feet.

The redesigned shopping center generally would be comprised of two areas due, in part, to site topography. The northern portion of the center sits approximately 15 feet higher in elevation than the southern portion of the site. A parking structure would be provided in each of these two areas.

The northern portion of the center would consist of a pedestrian-orientated "Main Street." The Main Street would extend from a gateway entry at Genesee Avenue and Esplanade Court to a circular style cul-de-sac and a central thoroughfare. It would be lined with commercial/retail and restaurant buildings, an outdoor living room, a central plaza, pedestrian walkways, decorative planters, site furniture, landscaping, and accent paving. Other amenities would include a rooftop park open to the community, rooftop gardens, green roofs, a community meeting room, and direct connections to the planned Trolley Station and off-site community facilities and uses.

The lower-elevation, southern, portion of the center would primarily consist of neighborhood convenience services generally within free-standing buildings separated by surface parking lots. This area is intended for essential neighborhood services, such as a grocery store, pharmacy, and banks. The future mixed-use residential component would also be located in this portion of the site. Landscaping, sidewalks, and parking facilities would be provided. Pedestrian connections between the northern and southern portions of the center would be provided primarily from the central plaza along Main Street.

Potentially Significant Effects:

The Retail, Hotel, and Residential Alternative would increase significant and unmitigated direct and cumulative transportation/circulation impacts to street segments, while decreasing impacts at intersections, freeway segments, and ramp meters. This alternative would incrementally reduce significant operational noise impacts from HVAC operations. Potentially significant, but mitigable, impacts related to demolition and construction noise would be similar under this alternative as for the Project. This alternative would incrementally reduce impacts to land use (noise compatibility), aesthetics, air quality, energy, paleontological resources, public utilities, and public services and facilities, which would be less than significant for the Project. It would be similar to the Project with regard to greenhouse gas emissions, hydrology/water quality, and geology.

Facts in Support of Finding:

The Retail, Hotel, and Residential Alternative would fulfill all of the Project objectives, including revitalization of an aging shopping center by expanding, enhancing, and diversifying neighborhood/community-serving retail, dining, and commercial opportunities and local services (Objective 1) and integrating some new land uses (visitor accommodations

and residential uses) to create a more vibrant activity center (Objective 2). It also would provide a hotel in a transit-accessible location (Objective 3), implement transit-supportive land uses (Objective 4), increase mobility options by providing improved pedestrian and bicycle linkages between the center and the adjacent neighborhood (Objective 5), provide a place for public gathering spots that promote social interaction (Objective 6), and improve the sustainability of the existing center through features consistent with the City's CAP (Objective 7). It would not, however, integrate research and development uses to the site as an element of Objective 2. As described above, it would decrease some impacts, while increasing others.

Finding:

The Retail, Hotel, and Residential Alternative is rejected because specific economic, social, or other considerations including matters of public policy make this alternative infeasible.

Rationale:

This alternative would not provide employment opportunities for highly trained workers, as the Project would through the addition of research and development uses to the site.

Reference:

See EIR Section 8.4.2 for a complete analysis of this alternative.

3. Retail, Hotel, Office, and Reduced Research and Development Alternative

The Retail, Hotel, Office, and Reduced Research and Development Alternative would construct 210,000 SF of research and development, which is 150,000 SF less than the Project. It also proposes to revitalize the 178,000 SF of existing retail space and add a hotel and 40,000 SF of office space, similar to the Project. The mobility improvements and community facilities, as well as sustainable design features, proposed as part of the Project would occur under this alternative.

Potentially Significant Effects:

The Retail, Hotel, Office, and Reduced Research and Development Alternative would reduce significant, direct and cumulative transportation/circulation (traffic congestion) impacts, although significant and unmitigated impacts would still occur. Potentially significant, but mitigable, impacts related to demolition and construction noise would be the same under this alternative as for the Project, while operational noise impacts would be incrementally reduced. It would slightly reduce impacts related to aesthetics, air quality, energy, GHG, paleontological resources, public utilities, and public facilities and services, which also would be less than significant under the Project. Less-than-significant impacts to land use, hydrology/water quality, and geology would be similar to the Project. This alternative is

identified as the environmentally superior alternative among the build alternatives because it would reduce significant and unmitigated transportation / circulation impacts, as well as reduce significant but mitigable operational noise impacts. Specifically, it would result in the least amount of traffic generation of any of the build alternatives.

Facts in Support of Finding:

This alternative would revitalize an aging shopping center by expanding, enhancing, and diversifying neighborhood/community-serving retail, dining, and commercial opportunities and local services (Objective 1). It also would provide a hotel in a transit-accessible location (Objective 3), increase mobility options by providing improved pedestrian and bicycle linkages between the center and the adjacent neighborhood (Objective 5), provide a place for public gathering spots that promote social interaction (Objective 6), and improve the sustainability of the existing center through features consistent with the City's CAP (Objective 7). While this alternative would create a built environment that would embrace the Trolley Station, it would implement transit-supportive land uses within a Transit Priority Area (Objective 4) and integrate new land uses to create a more vibrant activity center that contributes to the goals of smart growth (Objective 2) to a lesser degree than the Project. In summary, this alternative would fulfill five and partially fulfill two of the seven Project objectives.

Finding:

The Retail, Hotel, Office, and Reduced Research and Development Alternative is rejected because specific economic, social, or other considerations including matters of public policy make this alternative infeasible.

Rationale:

The reduction in research and development uses under this alternative would reduce the availability of employment opportunities for highly trained workers, relative to what would occur through implementation of the Project. This alternative would also not as fully implement transit-supportive land uses within a Transit Priority Area and integrate new land uses to create a more vibrant activity center that contributes to the goals of smart growth.

Reference:

See EIR Section 8.4.3 for a complete analysis of this alternative.

4. Retail and Office/Research and Development Alternative

The Retail and Office/Research and Development Alternative proposes to revitalize the 178,000 SF of existing retail space and add 360,000 SF of research and development and 40,000 SF of office uses, similar to the Project. This alternative would not, however, include

development of a 200-room hotel at the site. It is anticipated that two restaurants would operate at the site where a hotel would be located under the Project. The mobility improvements and community facilities, as well as sustainable design features, proposed as part of the Project would occur under this alternative.

Potentially Significant Effects:

The Retail and Office/Research and Development Alternative would reduce significant, direct and cumulative transportation/circulation (traffic congestion) impacts. Potentially significant, but mitigable, impacts related to demolition and construction noise would be the same under this alternative as for the Project, while operational noise impacts would be incrementally reduced. It would slightly reduce impacts related to land use (related to noise compatibility), aesthetics, air quality, energy, GHGs, paleontological resources, public utilities, and public facilities and services, which also would be less than significant under the Project. Less-than-significant impacts to hydrology/water quality and geology would be similar to the Project.

Facts in Support of Finding:

The Retail and Office/Research and Development Alternative would revitalize an aging shopping center by expanding, enhancing, and diversifying neighborhood/community-serving retail, dining, and commercial opportunities and local services (Objective 1). It also would increase mobility options by providing improved pedestrian and bicycle linkages between the center and the adjacent neighborhood (Objective 5), provide a place for public gathering spots that promote social interaction (Objective 6), and improve the sustainability of the existing center through features consistent with the City's CAP (Objective 7). While this alternative would create a built environment that would embrace the Trolley Station, it would implement transit-supportive land uses within a Transit Priority Area (Objective 4) and integrate new land uses to create a more vibrant activity center that contributes to the goals of smart growth (Objective 2) to a lesser degree than the Project. It also would not provide a hotel in a transit-accessible location (Objective 3). In summary, this alternative would fulfill four, partially fulfill two, and not fulfill one of the seven Project objectives.

Finding:

The Retail and Office/Research and Development Alternative is rejected because specific economic, social, or other considerations including matters of public policy make this alternative infeasible,

Rationale:

This alternative would not provide a hotel in a transit-accessible location, which would serve researchers, academicians, business travelers, and other visitors to the community. By not providing a hotel, this alternative would not as fully implement transit-supportive land uses within a Transit Priority Area and integrate new land uses to create a more vibrant activity

center that contributes to the goals of smart growth. It also would not generate Transient Occupancy Tax to fund City infrastructure and service needs.

Reference:

See EIR Section 8.4.4 for a complete analysis of this alternative.

VIII. FINDINGS REGARDING OTHER CEQA CONSIDERATIONS

a. Growth Inducement

Section 15126.2(e) of the CEQA Guidelines mandates that the growth-inducing impact of a project be discussed. This discussion is presented in Section 7.2 of the EIR. The City finds that the Project would not result in short- or long-term growth-inducing impacts.

Short-Term Growth Inducement

During the Project construction, demand for various construction trade skills and labor would increase. It is anticipated that this demand would be met by the local labor force and would not require importation of a substantial number of workers that could cause an increased demand for temporary or permanent housing in this area.

Long-Term Growth Inducement

The Project would contribute to long-term growth through the redevelopment of existing commercial retail space, and addition of 360,000 SF of research and development, 40,000 SF of office uses, and a 200-room hotel. The completed development would create additional part-time and full-time employment, involving a wide variety of jobs ranging from low to high wage scales. None of the anticipated uses is expected to require the importation of a specialized work force that is not already present in the region. The labor pool within the project area is adequate. While the Project has the potential to foster economic growth for the City through expanded retail sales and research and development/office jobs, it is expected to have a limited effect on regional population growth because it would draw from the local population for jobs. The Project would not directly or indirectly increase population growth in the region. No significant pressure on local housing supply or demand is expected to result from development of the Project.

The Project site is currently developed and is designated for urban uses and surrounded by existing and planned urban development and infrastructure. The Project would not require the extension or expansion of roadways, public services, utilities, or infrastructure into areas currently without service. It would be compatible with long-range plans for mass transit through expansion of the neighboring transit center and extension of the Metropolitan Transit System Blue Line Trolley. As a result, development of the Project would not remove any physical barriers to growth. Therefore, growth inducement would not be significant as a result of the Project.

b. Significant Irreversible Environmental Changes that will be Caused by the Project

Section 15126.2(d) of the CEQA Guidelines requires an evaluation of significant irreversible environmental change that may occur as a result of project implementation. This discussion is presented in Section 7.4 of the EIR. Irreversible environmental changes typically fall into three categories: (1) primary impacts, such as the use of nonrenewable resources (i.e., biological habitat, agricultural land, mineral deposits, water bodies, energy resources and cultural resources); (2) secondary impacts, such as road improvements which provide access to previously inaccessible areas; and (3) environmental accidents potentially associated with the project. Section 15126.2(d) of the State CEQA Guidelines states that irretrievable commitments of resources should be evaluated to assure that current consumption of such resources is justified.

As the site is currently developed with urban uses, implementation of the Project would not result in significant irreversible impacts to biological resources, historical resources, agricultural or forestry lands, or mineral resources. In addition, no water bodies are located on or adjacent to the site that would be impacted by the Project.

The Project would entail the commitment of energy and non-renewable resources, such as energy in the form of electricity, energy derived from fossil fuels, natural gas, construction materials (i.e., concrete, asphalt, sand and gravel, petrochemicals, steel, and lumber and forest products), potable water, and labor during the construction phase. The Project features a number of sustainability elements to minimize its consumption of energy and non-renewable resources and associated impacts would be less than significant. Nevertheless, use of these resources on any level would have an incremental effect on the regional consumption of these commodities, and therefore result in long-term, irretrievable losses of non-renewable resources, such as fuel and energy.

Paleontological resources which could be disturbed would be salvaged, as necessary, and data recovered in accordance with City standards. Impacts to paleontological resources would not be a reversible change to the resource. Lastly, the Project would not involve road or highway improvements that would provide access to previously inaccessible areas. Further, no major environmental accidents or hazards are anticipated to occur as a result of Project implementation.

IX. FINDINGS REGARDING RESPONSES TO COMMENTS AND REVISIONS IN THE FINAL EIR

The Final EIR includes the comments received on the Draft EIR and responses to those comments. The focus of the responses to comments is on the disposition of significant environmental issues that are raised in the comments, as specified by CEQA Guidelines Section 15088(c).

Finding/Rationale: Responses to comments made on the Draft EIR and revisions in the Final EIR merely clarify and amplify the analysis presented in the document, and do not trigger the need to recirculate per CEQA Guidelines Section 15088.5(b).

**STATEMENT OF OVERRIDING CONSIDERATIONS
(PUBLIC RESOURCES CODE SECTION 21081(b))**

Pursuant to Section 21081(b) of CEQA and CEQA Guidelines §15093 and §15043, CEQA requires the decision-making agency to balance, as applicable, the economic, legal, social, technological, or other benefits of a proposed project against its unavoidable environmental risks when determining whether to approve the project.

If the specific economic, legal, social, technological, or other benefits outweigh the unavoidable adverse environmental effects, the adverse environmental effects may be considered acceptable pursuant to Public Resources Code §21081. CEQA further requires that when the lead agency approves a project which will result in the occurrence of significant effects which are identified in the Environmental Impact Report (EIR) but are not avoided or substantially lessened, the agency shall state in writing the specific reasons to support its action based on the EIR and/or other information in the record.

Pursuant to the Public Resources Code §21081(b) and Guidelines § 15093, the City Council, having considered all of the foregoing, finds that the following specific overriding economic, legal, social, technological, or other benefits associated with the project outweigh unavoidable adverse direct impacts related to transportation/circulation.

The City Council declares that it has adopted all feasible mitigation measures to reduce the proposed environmental impacts to an insignificant level; considered the entire administrative record, including the EIR; and weighed the proposed benefits against its environmental impacts. This determination is based on the following specific benefits, each of which is determined to be, by itself and independent of the other project benefits, a basis for overriding and outweighing all unavoidable adverse environmental impacts identified in the Final EIR. Substantial evidence supports the various benefits, and can be found in the preceding sections (which are incorporated by reference into this section), the Final EIR, or in documents that comprise the Record of Proceedings for this matter.

1. The Project would create Base Sector Employment uses in the UTC/Sorrento Mesa Subregional Employment Area (City General Plan Economic Prosperity Element Figure EP-2) in accordance with the City's General Plan Economic Prosperity Element. Specifically, the Project would support the policies of this Element by:
 - Encouraging a broader geographic distribution of high-technology businesses throughout the City (Policy EP-A.2);
 - Encouraging large regional employers to locate and expand in a Subregional Employment Area (Policy EP-A.3);
 - Including base sector uses appropriate to an office setting in a village context (Policy EP-A.4);
 - Providing for the establishment and retention of non-base sector employment uses near housing to serve base sector industries and

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 Draft Candidate Findings and Statement of Overriding Considerations
 Costa Verde Center Revitalization Project
 May 15, 2020

community needs and encourage the development of small businesses (Policy EP-A.6);

- Increasing the allowable intensity of employment uses in a Subregional Employment Area where transit exists (Policy EP-A.7);
 - Concentrating more intense office development in a Subregional Employment Area with transit access (Policy EP-A.8);
 - Efficiently utilizing employment lands through increased intensity in an “urban village” and Subregional Employment Area (Policy EP-A.9); and
 - Locating compatible employment uses on an infill site and supporting job growth in an existing urban area (Policy EP-A.10).
2. The Project would implement the City of Villages Strategy in the City’s General Plan, which will sustain the long-term economic, environmental, and social health of the City, and implement the regional planning goals of the San Diego Association of Governments (SANDAG) Regional Plan. In particular, the Project would implement transit-supportive land uses and a built environment embracing the Blue Line Trolley Station, which is projected to start operations in late 2021. In addition to the approximately 17,800 housing units that exist within one mile of the Costa Verde Center, the Project would establish an employment center that is accessible by Trolley. The introduction of the research and development/office use supports the potential for daily commute of employees, furthering the economic viability of the Trolley. The Project would also provide pedestrian and bicycle linkages to improve connectivity between the Costa Verde Center, the adjacent neighborhood, and the Trolley. These design features, combined with other Transportation Demand Management (TDM) measures that the Project is committed to implementing, would decrease Vehicle Miles Traveled.
 3. Approval of this Project would represent another significant step toward achieving the City’s goal of encouraging scientific research and, in particular, the biotechnology industry, to locate in San Diego. In 2015, for example, Mayor Faulconer announced the results of “The Economic Impact of San Diego’s Research Institutions,” which revealed that San Diego’s scientific nonprofit, research institutes, and university centers have a \$4.6 billion total economic impact on the regional economy. He stated, “I look forward to continuing to work with these scientists, entrepreneurs and research institutions to ensure San Diego remains a global pioneer in scientific discovery.” The Project would reinforce San Diego’s valuable identity as a leader in the field of biotechnical research. The Project would be located in the University Community, which already supports a number of companies involved in biotechnology research and manufacturing. The site is ideally situated near the University of California, San Diego, Scripps Clinic, and Salk Institute.

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Draft Candidate Findings and Statement of Overriding Considerations
Costa Verde Center Revitalization Project
May 15, 2020

4. The Project would revitalize a shopping center that was constructed in 1989 and is suffering from outdated design, deteriorating facilities, and resulting vacancies. The revitalized center would better serve present and future needs of the surrounding community, providing space for key services such as a grocery store, banks, dry cleaners, post office, and affordable dining establishments. Introduction of office, hotel, and research and development uses to the site would help to create a more vibrant activity center. This mix of uses would be supportive of retail uses in the current economic environment, minimize the trips that office employees need to make during the day (for food, banking, pharmacy, etc.), and increase the amount of public activity in the center for improved security.
5. The Project will include a new pharmacy that does not currently exist in the local neighborhood. Immediately behind the Project is the continuing care retirement community called the Vi. This new amenity will now be in walking distance to the Vi residential village.
6. The Project would provide a hotel in a transit-accessible location, which would serve researchers, academicians, family members of UCSD students and Vi residents, business travelers, and other visitors to the community. The construction of a hotel in this key location adjacent to the Blue Line Trolley, that also connects to UCSD and is in proximity to numerous research institutions, would help to enhance San Diego's role as a center for academic and scientific research collaboration.
7. The Project would support transit by building a dense, mixed-use project with employment uses adjacent to both the new Blue Line Trolley and the existing UTC Transit Center bus terminal. The Project is situated in a Transit Priority Area (TPA), which is ideal for denser projects that will facilitate transit ridership, improve the City's overall transit mode share, and reduce greenhouse gas emissions.
8. The Project will result in an improved pedestrian experience by leveling the site along the "Main Street," removing the need to use escalators to traverse the site, and creating a more welcoming entrance to visitors entering the site from neighborhoods to the west.

The City Council finds in accordance with Public Resources Code §21081(b) and 21081.5, and CEQA Guidelines §§15093 and 15043, that any, or any combination of, the Statement of Overriding Consideration benefits noted above would be sufficient to reach the conclusion that overriding findings justify the significant, unmitigable impacts that were found.

(R-2016-)

CITY COUNCIL RESOLUTION NO.
GENERAL PLAN AMENDMENT, COMMUNITY PLAN AMENDMENT AND SPECIFIC PLAN AMENDMENT
NO. 1677935

COSTA VERDE REVITALIZATION - PROJECT NO. 477943
[MMRP]

ADOPTED ON _____

WHEREAS, Regency Centers, LP a Florida Limited Partnership, requested an amendment to the 2008 General Plan (General Plan), the University Community Plan, and the Costa Verde Specific Plan to amend the University Community Plan, to revise the "Community Commercial" acreage from 30 to 29 acres and "Visitor Commercial" from 46 to 47 acres, and to amend the Land Use and Development Intensity, allowing for 178,000 SF of Neighborhood/Community Commercial and 2,740 Dwelling units to add 360,000 SF of Research & Development Office; 40,000 SF of Professional Office; and 200 Hotel Rooms; and

WHEREAS, the site is legally described as Lots 13 - 14 inclusive of Costa Verde, City of San Diego, County of San Diego, State of California, According to Map thereof No. 12045, filed in the office of the County Recorder of San Diego County, April 18, 1988; and

WHEREAS, an amendment to the University Community Plan and the Costa Verde Specific Plan requires an amendment to the General Plan due to the University Community Plan and the Costa Verde Specific Plan being part of the Land Use Element of the General Plan; and

WHEREAS, on September 24, 2020, the Planning Commission of the City of San Diego found the proposed amendments consistent with the General Plan, University Community Plan, and Costa Verde Specific Plan and voted _____ to recommend City Council approval of the amendments; and

WHEREAS, on _____, the Council of the City of San Diego (City Council) held a public hearing for the purpose of considering an amendment to the General Plan, the University Community Plan, and the Costa Verde Specific Plan; and

WHEREAS, the City Council has considered the Planning Commission record and recommendation, as well as all maps, exhibits, and written documents contained in the file for this project on record in the City of San Diego, and has considered the oral presentations given at the public hearing; and

WHEREAS, under Charter section 280(a)(2) this ordinance is not subject to veto by the Mayor because this matter requires the Council to act as a quasi-judicial body and where a public hearing was required by law implicating due process rights of individuals affected by the decision and where the Council was required by law to consider evidence at the hearing and to make legal findings based on evidence presented; NOW, THEREFORE,

BE IT RESOLVED, by the City Council, that it adopts the amendments to the General Plan, University Community Plan, and Costa Verde Specific Plan, a copy of which is on file in the office of the City Clerk as Document No. RR-_____.

BE IT FURTHER RESOLVED, by the City Council, that it adopts and amendment to the General Plan a copy of which is on file in the office of the City Clerk as Document No. RR-_____..

APPROVED: MARA ELLIOT, City Attorney

By _____

Deputy City Attorney

MJL:pev

INSERT Date

Or.Dept:DSD

R-2016- INSERT

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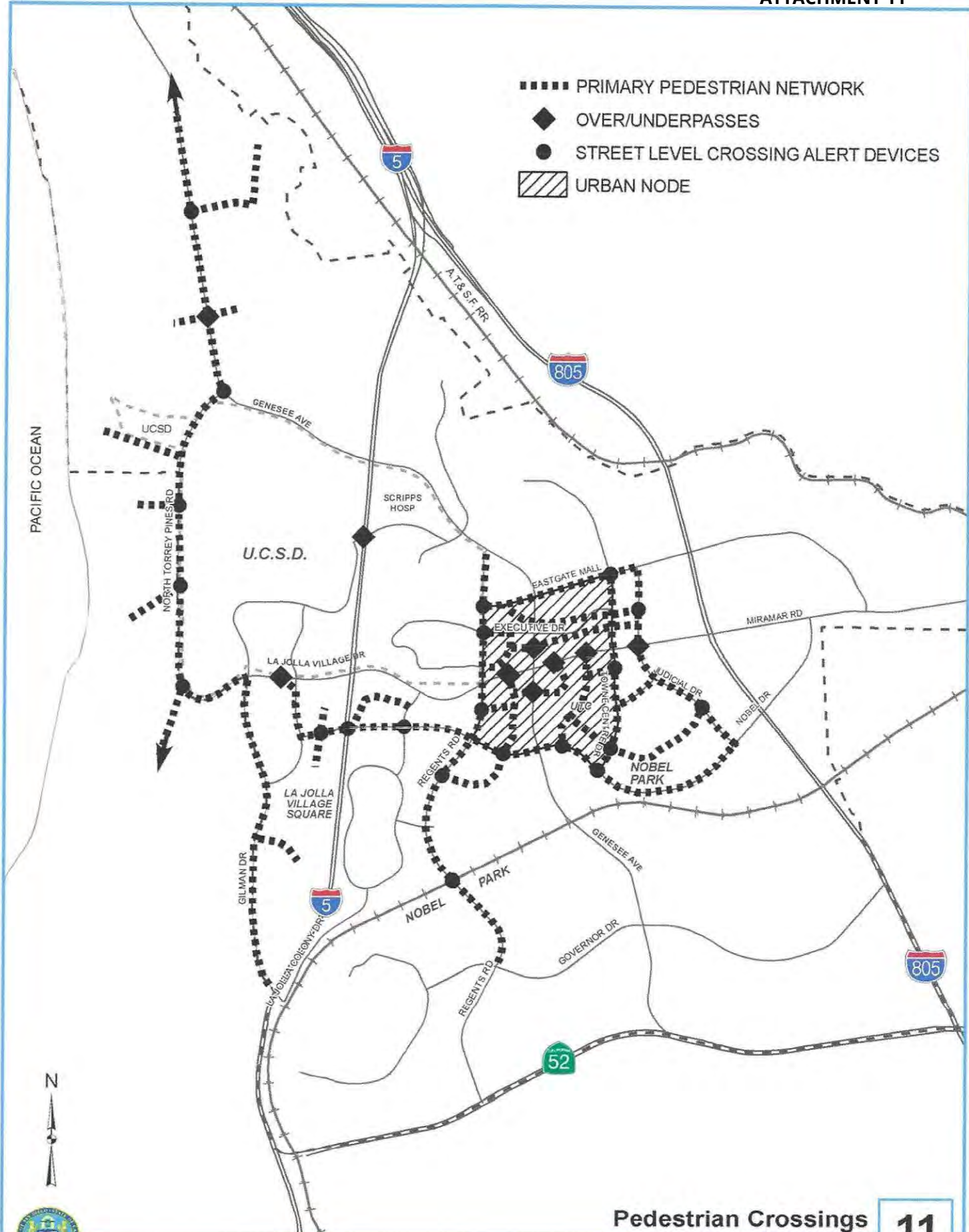
G. Progress Guide and General Plan

This Plan includes a consistency analysis, describing how the Plan conforms to the General Plan. This analysis is in the **General Plan Consistency Element** of this Plan.

**TABLE 1
UNIVERSITY COMMUNITY PLAN LAND USE SUMMARY**

Category	Use Description	Acreage	Dwelling Units
Residential		(1,562)	
	5-10 Units/Acre	718	6,018
	10-15 Units/Acre	100	1,446
	15-30 Units/Acre	547	12,245
	30-45 Units/Acre	99	4,284
	45-75 Units/Acre	98	6,424
Commercial		(392)	
	Neighborhood	36	
	Community	30 29	
	Regional	103	
	Visitor	46 47	
	Office	178	
Life Sciences/Research		(697)	
	Scientific Research	630	
	Hospitals	67	
Industrial		(580)	
	Restricted	347	
	Business/Industrial Park	233	
Parks/Open Space		(2,811)	
	Neighborhood	34 usable	
	Community	29 usable	
	Sports Complex	21 usable	
	Joint Use	18 usable	
	Golf	359	
	Resource-Based	394	
	Open Space	1,119	
	State Park	837	
Schools		(1,233)	
	Elementary	61	
	Junior High	28	
	Senior High	40	
	UCSD	1,104	
Public Facilities		(36)	
Other	Freeway Rights-of-Way, etc.	(1,201)	
	Total Community	8,512	
	Total Community Dwelling Units		30,417

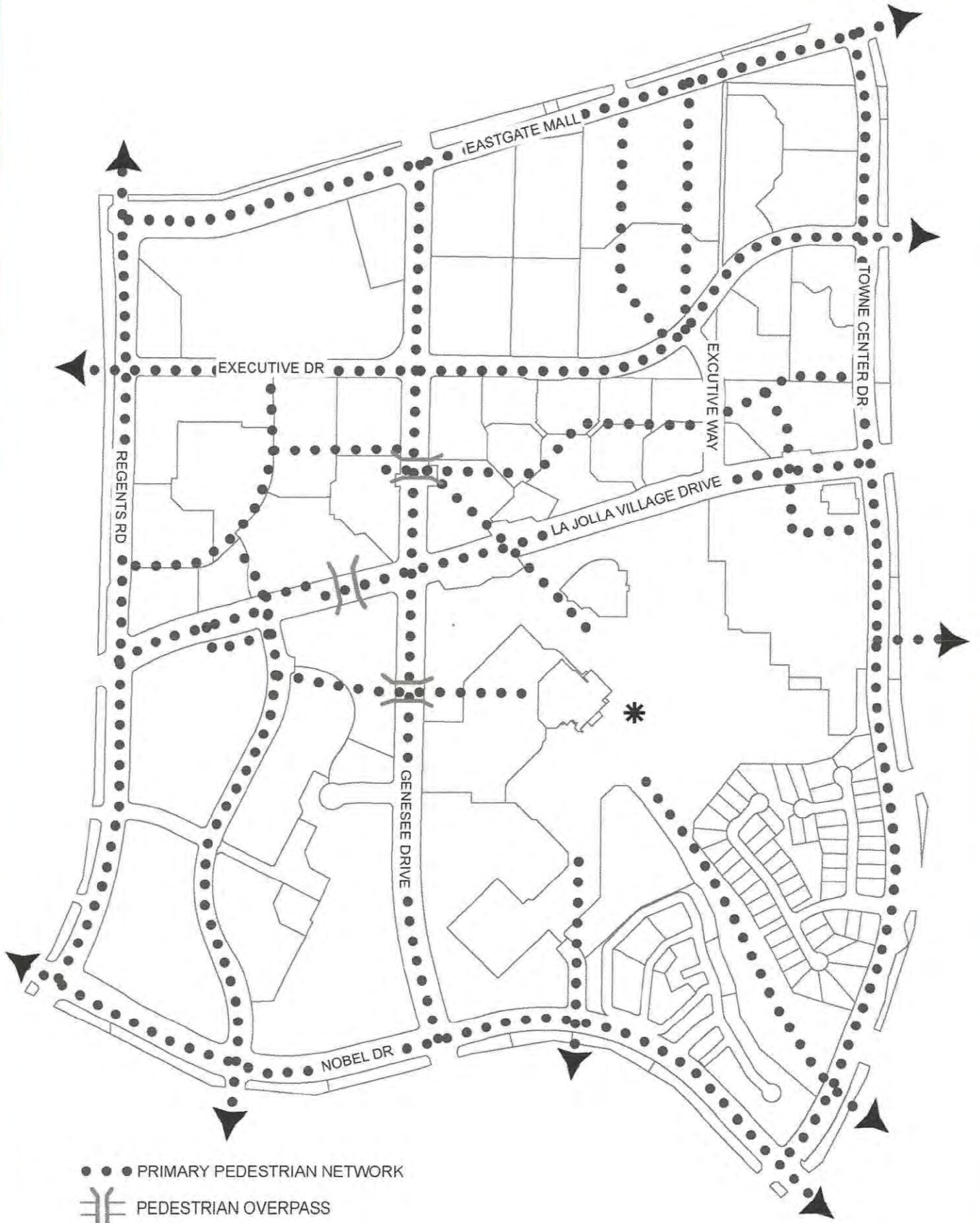
- PRIMARY PEDESTRIAN NETWORK
- ◆ OVER/UNDERPASSES
- STREET LEVEL CROSSING ALERT DEVICES
- ▨ URBAN NODE



Pedestrian Crossings
University Community Plan

11
FIGURE





- ● ● PRIMARY PEDESTRIAN NETWORK
- ⌘ PEDESTRIAN OVERPASS
- ▭ PARCELS



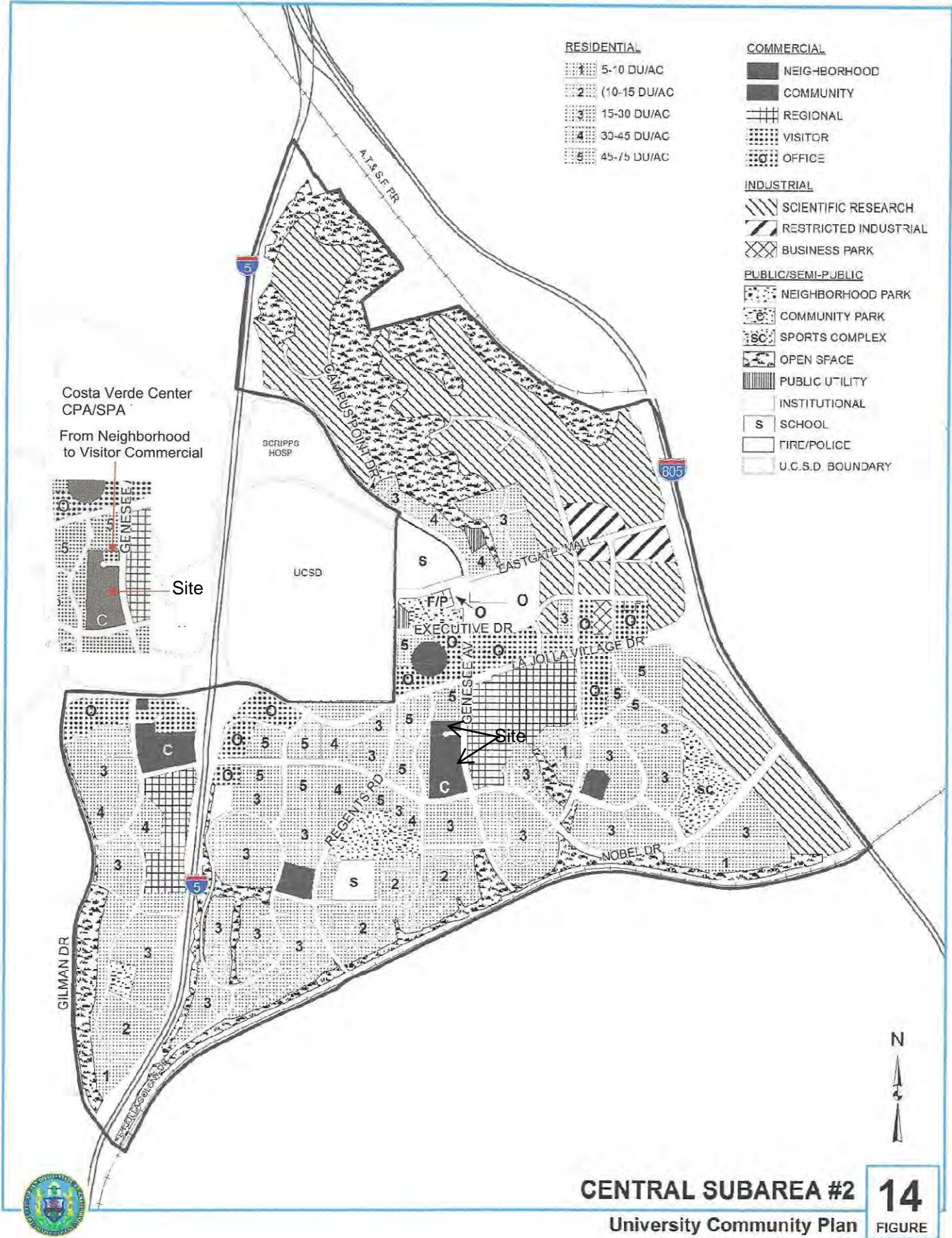


TABLE 3 (continued)
LAND USE AND DEVELOPMENT INTENSITY

Any changes to this table for properties in the Coastal Zone shall require an amendment to the Local Coastal Program.

Subarea/Name	Gross Acres	Land Use and Development Intensity
35. La Jolla Centre I (PCD) ^(7b)	3.17	143,400 SF - Office
36. Neighborhood Park	30.00	
37. City Ownership	56.5	18,000 SF/AC - Scientific Research
Alexandria (PDP)	42.60	8,657 ADT- Scientific Research ⁽¹⁰⁾
Open Space	2.75	
38. Towne Centre Apartments (PRD)	23.79	256 DU
39. City Ownership	7 – 8	30 DU/AC
40. La Jolla Crossroads ⁽⁸⁾	33.80	33.8 AC - Residential, 1,809 DU
41. Renaissance La Jolla (PDR & PCD)	112.96	2,500 DU 50,000 SF - Neighborhood Commercial
Open Space Easement	15.06	
42. La Jolla Gateway (PCD) ^{7c}	14.17	396,305 SF - Office
Congregation Beth Israel ^{7c}		2,165SF – Chapel 62,931 SF – Sanctuary/Temple School
43. University Towne Centre	75.35	1,811,409 SF - Regional Commercial GLA 300 DU ⁽⁹⁾
44. Vista La Jolla/University Pines	12.26	257 DU
45. Vista La Jolla	14.84	56 DU
46. Nobel Terrace (PRD)	41.05	716 DU
47. Costa Verde Specific Plan ⁽⁸⁾	54.00	178,000 SF-Neighborhood/Community Commercial 360,000 SF-Research & Development Office 40,000 SF-Professional Office 250 Room-Hotel 2,740 DU
48. La Jolla Highlands Torrey Heights La Jolla Pines Village Green	17.42	474 DU
49. Genesee Highlands Unit 2	17.87	246 DU
50. Genesee Highlands Unit 3 Open Space Easement	8.61 13.60	211 DU

(7a) ADT's from Irvine Company owned parcels 343-122-40-43, 45-52, & 60-64, Subarea 12 (PID 90-0892); 345-012-09, Subarea 35 (PCD 83-0131); 345-011-15, 16-, & 23, Subarea 42 (PCD 82-0707); and 345-120-17, Subarea 67 (PRD 96-0638) have been shifted to La Jolla Centre III Subarea 29, APN 345-012-10.

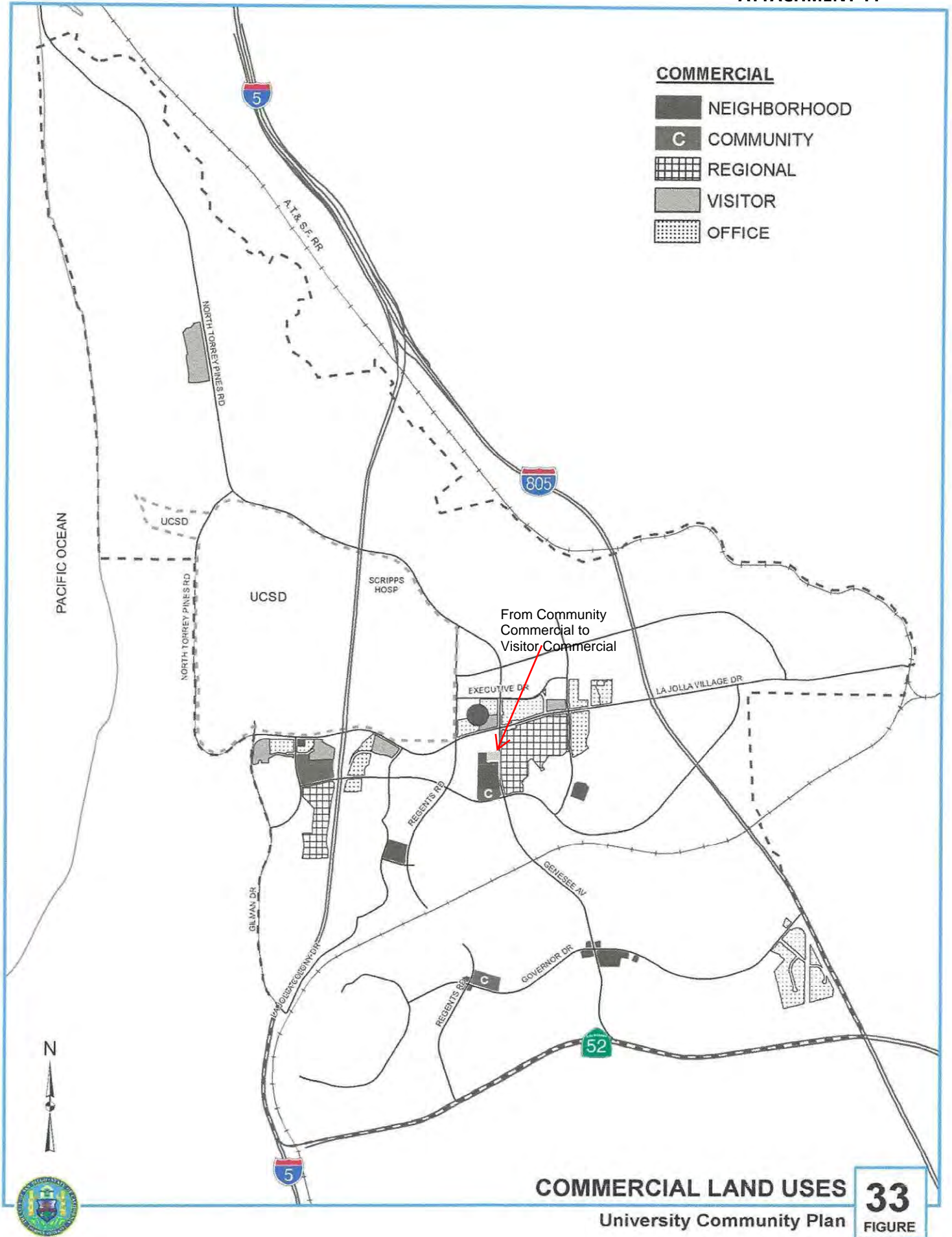
(7b) ADT's from Irvine Company owned parcel 345-012-09, Subarea 35 (PCD 83-0131) have been shifted to La Jolla Centre III Subarea 29, APN 345-012-10.

(7c) ADT's from Irvine Company owned parcels 345-011-15 & 16 Subarea 42 (PCD 82-0707) have been shifted to La Jolla Centre III Subarea 29, APN 345-012-10. Congregation Beth Israel not a part of ADT Shift.

(8) After 558 ADT transferred from Subarea 47 to Subarea 40, La Jolla Crossroads, and 987 ADT transferred from Subarea 47 to Subarea 37, Alexandria, 1,615 unused ADT remain with Costa Verde Specific Plan Area.

(9) This property is subject to an approved Master Planned Development Permit (MPDP), which permits adjustment to the levels of retail and residential development (up to 300 units) within the intensity envelope for the property defined by the MPDP.

(10) This property is subject to an approved Planned Development Permit (PDP), which allows adjustment to square footage for uses permitted in the IP-1-1 zone so long as maximum trip generation does not exceed 8,657 ADT.



COMMERCIAL LAND USES
University Community Plan





COSTA VERDE CENTER REVITALIZATION

PREPARED BY:	rdc.
NAME:	
ADDRESS:	245 EAST THIRD STREET LONG BEACH, CA 90802
PHONE:	562.628.8000

ORIGINAL DATE:	3/15/2016 (BY CALLISONRTKL)
REVISION DATE:	7/28/2016 (BY CALLISONRTKL)
REVISION DATE:	12/12/2016 (BY CALLISONRTKL)
REVISION DATE:	06/10/2019 (BY GENSLER)
REVISION DATE:	10/11/2019 (BY RDC)
REVISION DATE:	09/01/2020 (BY RDC)

SHEET TITLE:	COVER PAGE
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SHEET NO:	ENT- 00	1 OF 65
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PTS #:	477943
PROJECT #:	477943

Regency Centers.	REGENCY CENTERS 420 STEVENS AVE SUITE 320 SOLANA BEACH, CA 92075
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• This perspective has been included for illustrative purposes only. The intent of this exhibit is to convey community-level design concepts rather than specific architectural or landscape information. For project details, please refer to the other sheets in this plan set.

VESTING TENTATIVE PARCEL MAP NO. XXXX

FOR COMMERCIAL CONDOMINIUM PURPOSES

PARCEL SUMMARY TABLE	
PARCEL #	AREA (ACRES)
EXISTING LOT 13	1.000
EXISTING LOT 14	12.230
TOTAL EXISTING AREA:	13.230*
PROPOSED PARCEL 1	2.024
PROPOSED PARCEL 2	5.149
PROPOSED PARCEL 3	5.850
PROPOSED PARCEL 4	0.902
TOTAL PROPOSED AREA:	13.925*

LOT SUMMARY NOTES:
 1. TOTAL LOT ACREAGE INCREASED DUE TO VACATION OF ESPLANADE COURT (0.698 ACRES).
 2. LOT 3 TO BE CONDOMINIUMIZED IN THE FUTURE UP TO 20 UNITS

- PROPOSED EASEMENTS**
1. PROPOSED PRIVATE RECIPROCAL AGREEMENT FOR ACCESS PER SEPARATE DOCUMENT
 2. FUTURE SDGE EASEMENT DEDICATED PER SEPARATE DOCUMENT
 3. FUTURE PUBLIC STREET DEDICATION
 4. 15' PUBLIC WATER EASEMENT DEDICATED HEREON
 5. PUBLIC WATER METER EASEMENT DEDICATED HEREON

- EXISTING EASEMENTS**
- 36' GENERAL UTILITY EASEMENT PER DOC 87-500962 REC. 9/3/1987
 - EASEMENT AGREEMENT FOR PARKING PER DOC 90-537240 REC. 10/2/1990
 - PUBLIC STREET EASEMENT GRANTED TO SANDAG FOR LRT STATION INFRASTRUCTURE AND PUBLIC ACCESS PER DOC 2016-0538712 REC. 10/07/2016 TO BE VACATED
 - SLOPE AND DRAINAGE EASEMENT PER DOC 76232 REC. 5/1/1969 AND DOC 76233 REC. 5/1/1969
 - 46' GENERAL UTILITY EASEMENT PER 23300-D (OFFSITE)
 - RECIPROCAL AGREEMENT FOR ACCESS RECORDED 10/2/1990 AS RECORD NO. 90-537242
 - 20' SURFACE AND SUBSURFACE EASEMENT PER DOC 90-260802 REC. 5/11/1990

LEGEND:

- EXISTING RIGHT OF WAY / PROPERTY BOUNDARY
- - - EXISTING SANDAG PUBLIC STREET EASEMENT TO BE VACATED
- - - EXISTING EASEMENT
- - - EXISTING CENTERLINE
- PROPOSED LOT LINE
- - - PROPOSED EASEMENT
- STREET CENTERLINE
- FUTURE PUBLIC STREET DEDICATION

DATUM DISCLAIMER:
 ALL ELEVATIONS REFERENCED IN THIS PLAN SET ARE CONSISTENT WITH NATIONAL GEODETIC VERTICAL DATUM OF 1929 (NGVD - 29) AS REQUIRED BY THE CITY OF SAN DIEGO.

LEGAL DESCRIPTION

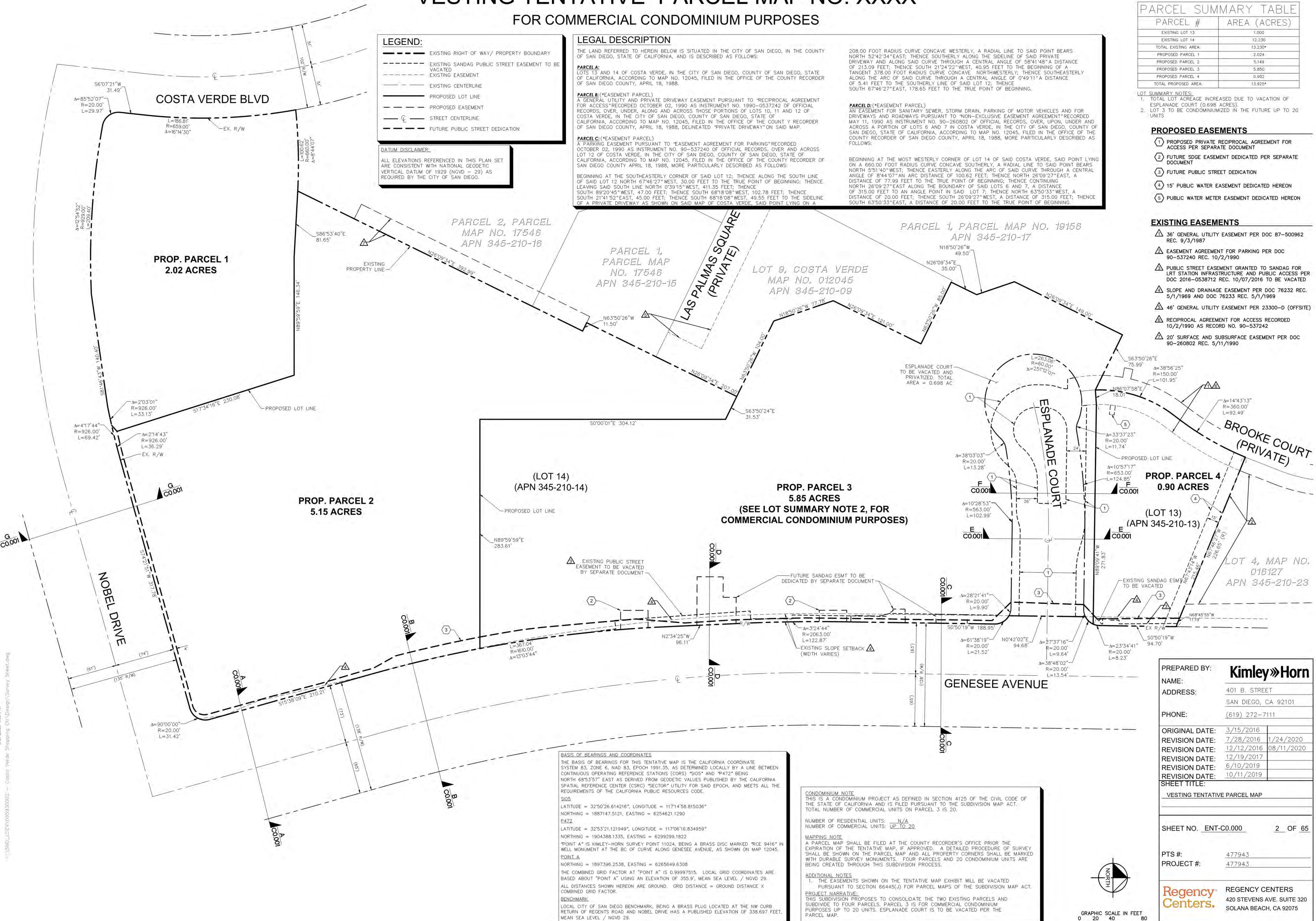
THE LAND REFERRED TO HEREIN BELOW IS SITUATED IN THE CITY OF SAN DIEGO, IN THE COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:

PARCEL A:
 LOTS 13 AND 14 OF COSTA VERDE, IN THE CITY OF SAN DIEGO, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO MAP NO. 12045, FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, APRIL 18, 1988.

PARCEL B: (*EASEMENT PARCEL)
 A GENERAL UTILITY AND PRIVATE DRIVEWAY EASEMENT PURSUANT TO "RECIPROCAL AGREEMENT FOR ACCESS" RECORDED OCTOBER 02, 1990 AS INSTRUMENT NO. 1990-0537242 OF OFFICIAL RECORDS, OVER, UNDER, ALONG AND ACROSS THOSE PORTIONS OF LOTS 10, 11 AND 12 OF COSTA VERDE, IN THE CITY OF SAN DIEGO, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO MAP NO. 12045, FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, APRIL 18, 1988, DELINEATED "PRIVATE DRIVEWAY" ON SAID MAP.

PARCEL C: (*EASEMENT PARCEL)
 A PARKING EASEMENT PURSUANT TO "EASEMENT AGREEMENT FOR PARKING" RECORDED OCTOBER 02, 1990 AS INSTRUMENT NO. 90-537240 OF OFFICIAL RECORDS, OVER AND ACROSS LOT 12 OF COSTA VERDE, IN THE CITY OF SAN DIEGO, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO MAP NO. 12045, FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY APRIL 18, 1988, MORE PARTICULARLY DESCRIBED AS FOLLOWS:
 BEGINNING AT THE SOUTHEASTERLY CORNER OF SAID LOT 12; THENCE ALONG THE SOUTH LINE OF SAID LOT 12 NORTH 67°46'27" WEST, 30.00 FEET TO THE TRUE POINT OF BEGINNING; THENCE LEAVING SAID SOUTH LINE NORTH 0°39'15" WEST, 411.35 FEET; THENCE SOUTH 89°20'45" WEST, 47.00 FEET; THENCE SOUTH 68°18'08" WEST, 302.78 FEET; THENCE SOUTH 21°41'52" EAST, 45.00 FEET; THENCE SOUTH 68°18'08" WEST, 49.55 FEET TO THE SIDELINE OF A PRIVATE DRIVEWAY AS SHOWN ON SAID MAP OF COSTA VERDE, SAID POINT LYING ON A 208.00 FOOT RADIUS CURVE CONCAVE WESTERLY, A RADIAL LINE TO SAID POINT BEARS NORTH 52°42'34" EAST; THENCE SOUTHERLY ALONG THE SIDELINE OF SAID PRIVATE DRIVEWAY AND ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 58°41'48" A DISTANCE OF 213.09 FEET; THENCE SOUTH 21°24'22" WEST, 40.95 FEET TO THE BEGINNING OF A TANGENT 378.00 FOOT RADIUS CURVE CONCAVE NORTHWESTERLY; THENCE SOUTHEASTERLY ALONG THE ARC OF SAID CURVE THROUGH A CENTRAL ANGLE OF 0°49'11" A DISTANCE OF 5.41 FEET TO THE SOUTHERLY LINE OF SAID LOT 12; THENCE SOUTH 67°46'27" EAST, 178.65 FEET TO THE TRUE POINT OF BEGINNING.

PARCEL D: (*EASEMENT PARCEL)
 AN EASEMENT FOR SANITARY SEWER, STORM DRAIN, PARKING OF MOTOR VEHICLES AND FOR DRIVEWAYS AND ROADWAYS PURSUANT TO "NON-EXCLUSIVE EASEMENT AGREEMENT" RECORDED MAY 11, 1990 AS INSTRUMENT NO. 90-260802 OF OFFICIAL RECORDS, OVER, UPON, UNDER AND ACROSS A PORTION OF LOTS 6 AND 7 IN COSTA VERDE, IN THE CITY OF SAN DIEGO, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO MAP NO. 12045, FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, APRIL 18, 1988, MORE PARTICULARLY DESCRIBED AS FOLLOWS:
 BEGINNING AT THE MOST WESTERLY CORNER OF LOT 14 OF SAID COSTA VERDE, SAID POINT LYING ON A 660.00 FOOT RADIUS CURVE CONCAVE SOUTHERLY, A RADIAL LINE TO SAID POINT BEARS NORTH 51°40' WEST; THENCE EASTERLY ALONG THE ARC OF SAID CURVE THROUGH A CENTRAL ANGLE OF 84°40' AN ARC DISTANCE OF 100.62 FEET; THENCE NORTH 26°09'27" EAST, A DISTANCE OF 77.99 FEET TO THE TRUE POINT OF BEGINNING; THENCE CONTINUING NORTH 26°09'27" EAST ALONG THE BOUNDARY OF SAID LOTS 6 AND 7, A DISTANCE OF 315.00 FEET TO AN ANGLE POINT IN SAID LOT 7; THENCE NORTH 63°50'33" WEST, A DISTANCE OF 20.00 FEET; THENCE SOUTH 26°09'27" WEST, A DISTANCE OF 315.00 FEET; THENCE SOUTH 63°50'33" EAST, A DISTANCE OF 20.00 FEET TO THE TRUE POINT OF BEGINNING.



BASIS OF BEARINGS AND COORDINATES

THE BASIS OF BEARINGS FOR THIS TENTATIVE MAP IS THE CALIFORNIA COORDINATE SYSTEM 83, ZONE 6, NAD 83, EPOCH 1991.35, AS DETERMINED LOCALLY BY A LINE BETWEEN CONTINUOUS OPERATING REFERENCE STATIONS (CORS) "S105" AND "P472" BEING NORTH 68°53'57" EAST AS DERIVED FROM GEODETIC VALUES PUBLISHED BY THE CALIFORNIA SPATIAL REFERENCE CENTER (CSRS) "SECTOR" UTILITY FOR SAID EPOCH, AND MEETS ALL THE REQUIREMENTS OF THE CALIFORNIA PUBLIC RESOURCES CODE.

S105
 LATITUDE = 32°50'26.614216", LONGITUDE = 117°14'58.815036"
 NORTHING = 1887147.5121, EASTING = 6254621.1290

P472
 LATITUDE = 32°53'21.121949", LONGITUDE = 117°06'16.834959"
 NORTHING = 1904388.1335, EASTING = 6299299.1822

"POINT A" IS KIMLEY-HORN SURVEY POINT 11024, BEING A BRASS DISC MARKED "ICE 9416" IN WELL MONUMENT AT THE BC OF CURVE ALONG GENESSEE AVENUE, AS SHOWN ON MAP 12045.

POINT A
 NORTHING = 1897396.2538, EASTING = 6265649.6308

THE COMBINED GRID FACTOR AT "POINT A" IS 0.99997515. LOCAL GRID COORDINATES ARE BASED ABOUT "POINT A" USING AN ELEVATION OF 355.9', MEAN SEA LEVEL / NGVD 29.

ALL DISTANCES SHOWN HEREON ARE GROUND. GRID DISTANCE = GROUND DISTANCE X COMBINED GRID FACTOR.

BENCHMARK
 LOCAL CITY OF SAN DIEGO BENCHMARK, BEING A BRASS PLUG LOCATED AT THE NW CURB RETURN OF REGENTS ROAD AND NOBELL DRIVE HAS A PUBLISHED ELEVATION OF 338.697 FEET, MEAN SEA LEVEL / NGVD 29.

CONDOMINIUM NOTE
 THIS IS A CONDOMINIUM PROJECT AS DEFINED IN SECTION 4125 OF THE CIVIL CODE OF THE STATE OF CALIFORNIA AND IS FILED PURSUANT TO THE SUBDIVISION MAP ACT. TOTAL NUMBER OF COMMERCIAL UNITS ON PARCEL 3 IS 20.

NUMBER OF RESIDENTIAL UNITS: N/A
 NUMBER OF COMMERCIAL UNITS: UP TO 20

MAPPING NOTE
 A PARCEL MAP SHALL BE FILED AT THE COUNTY RECORDER'S OFFICE PRIOR THE EXPIRATION OF THE TENTATIVE MAP, IF APPROVED. A DETAILED PROCEDURE OF SURVEY SHALL BE SHOWN ON THE PARCEL MAP AND ALL PROPERTY CORNERS SHALL BE MARKED WITH DURABLE SURVEY MONUMENTS. FOUR PARCELS AND 20 CONDOMINIUM UNITS ARE BEING CREATED THROUGH THIS SUBDIVISION PROCESS.

ADDITIONAL NOTES
 1. THE EASEMENTS SHOWN ON THE TENTATIVE MAP EXHIBIT WILL BE VACATED PURSUANT TO SECTION 66445(i) FOR PARCEL MAPS OF THE SUBDIVISION MAP ACT.

PROJECT NARRATIVE
 THIS SUBDIVISION PROPOSES TO CONSOLIDATE THE TWO EXISTING PARCELS AND SUBDIVIDE TO FOUR PARCELS. PARCEL 3 IS FOR COMMERCIAL CONDOMINIUM PURPOSES UP TO 20 UNITS. ESPLANADE COURT IS TO BE VACATED PER THE PARCEL MAP.

PREPARED BY: Kimley-Horn
NAME: 401 B. STREET
ADDRESS: SAN DIEGO, CA 92101
PHONE: (619) 272-7111

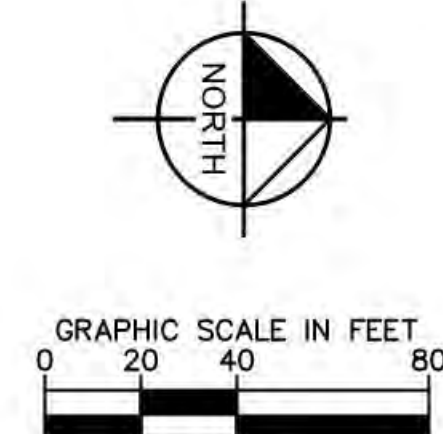
ORIGINAL DATE: 3/15/2016
REVISION DATE: 7/28/2016, 1/24/2020
REVISION DATE: 12/12/2016, 08/11/2020
REVISION DATE: 12/19/2017
REVISION DATE: 6/10/2019
REVISION DATE: 10/11/2019

SHEET TITLE:
 VESTING TENTATIVE PARCEL MAP

SHEET NO. ENT-C0.000 2 OF 65

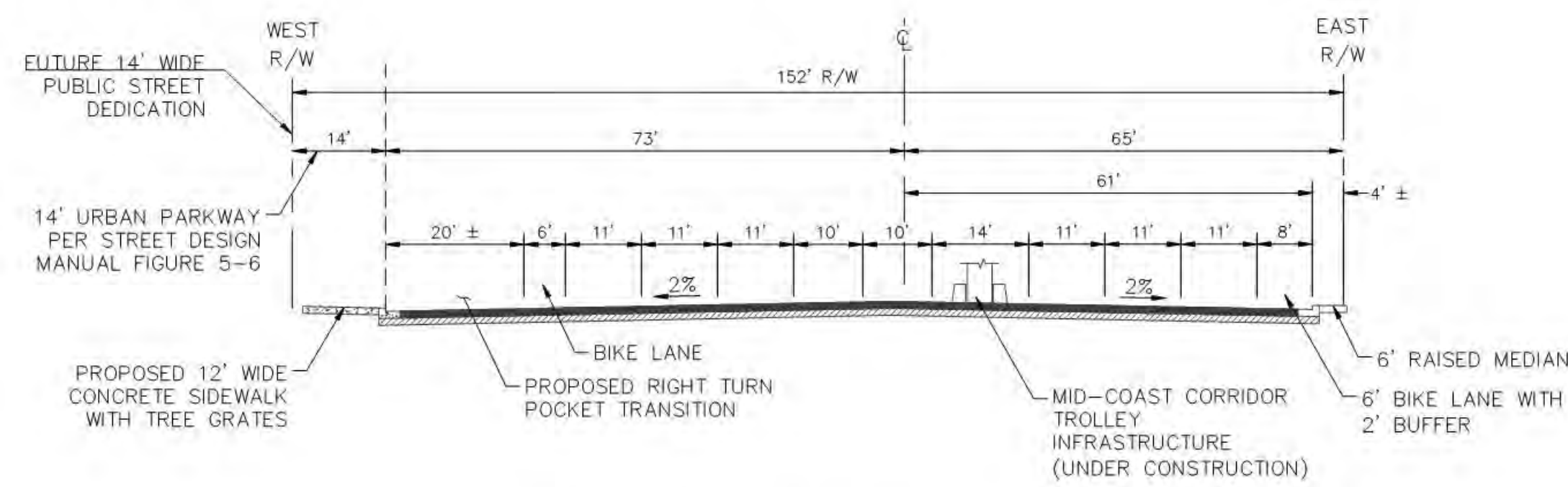
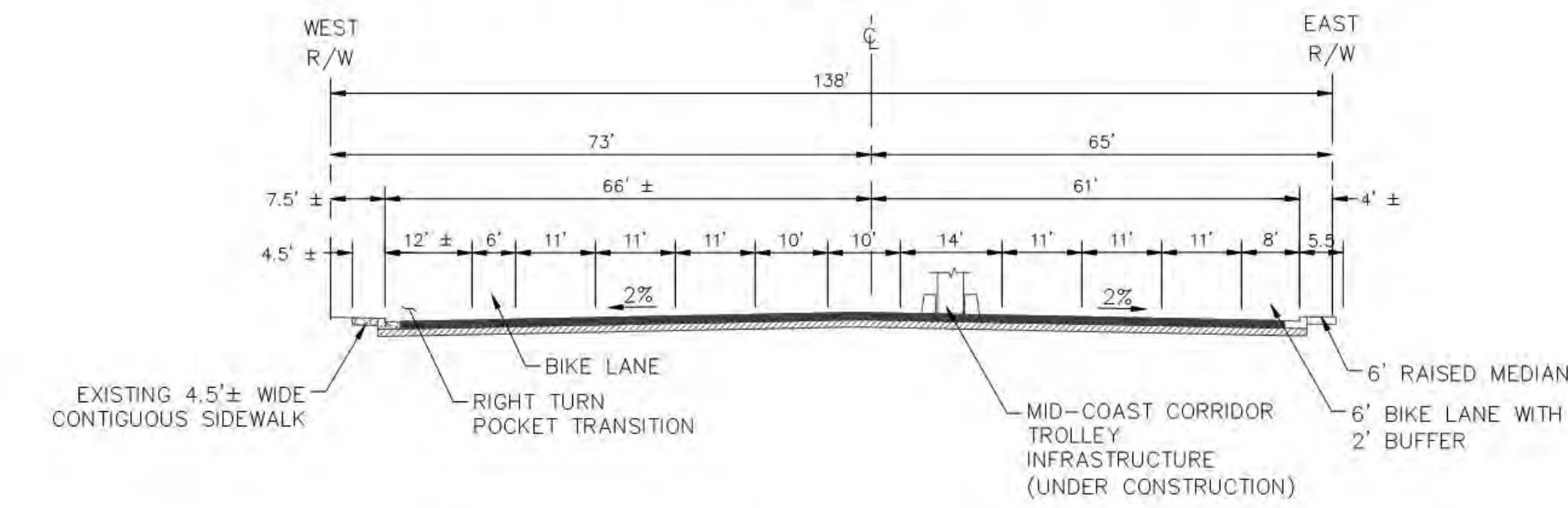
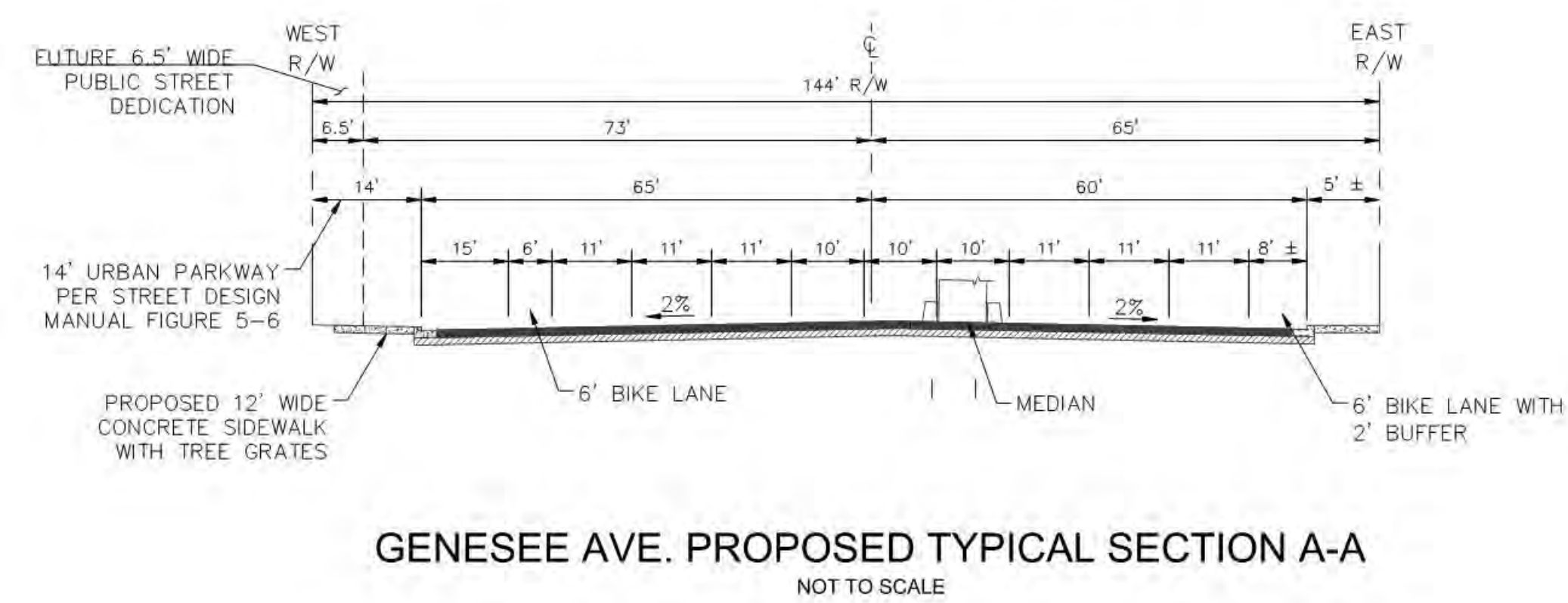
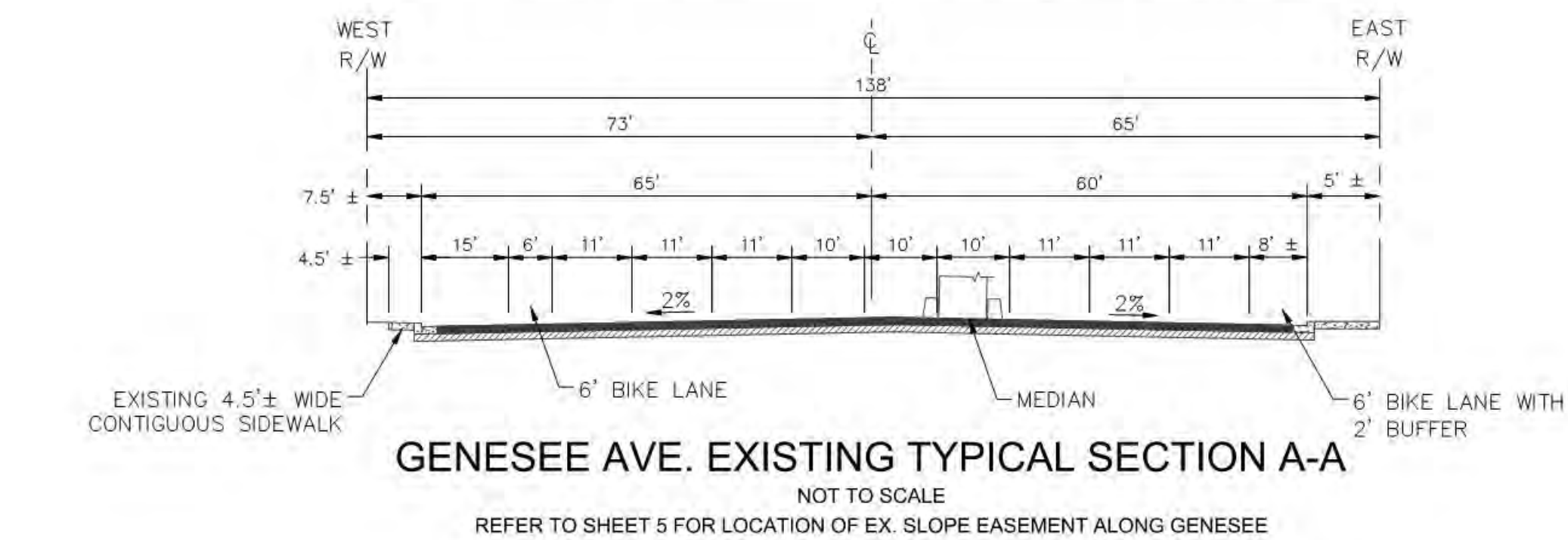
PTS #: 477943
PROJECT #: 477943

Regency Centers REGENCY CENTERS
 420 STEVENS AVE. SUITE 320
 SOLANA BEACH, CA 92075

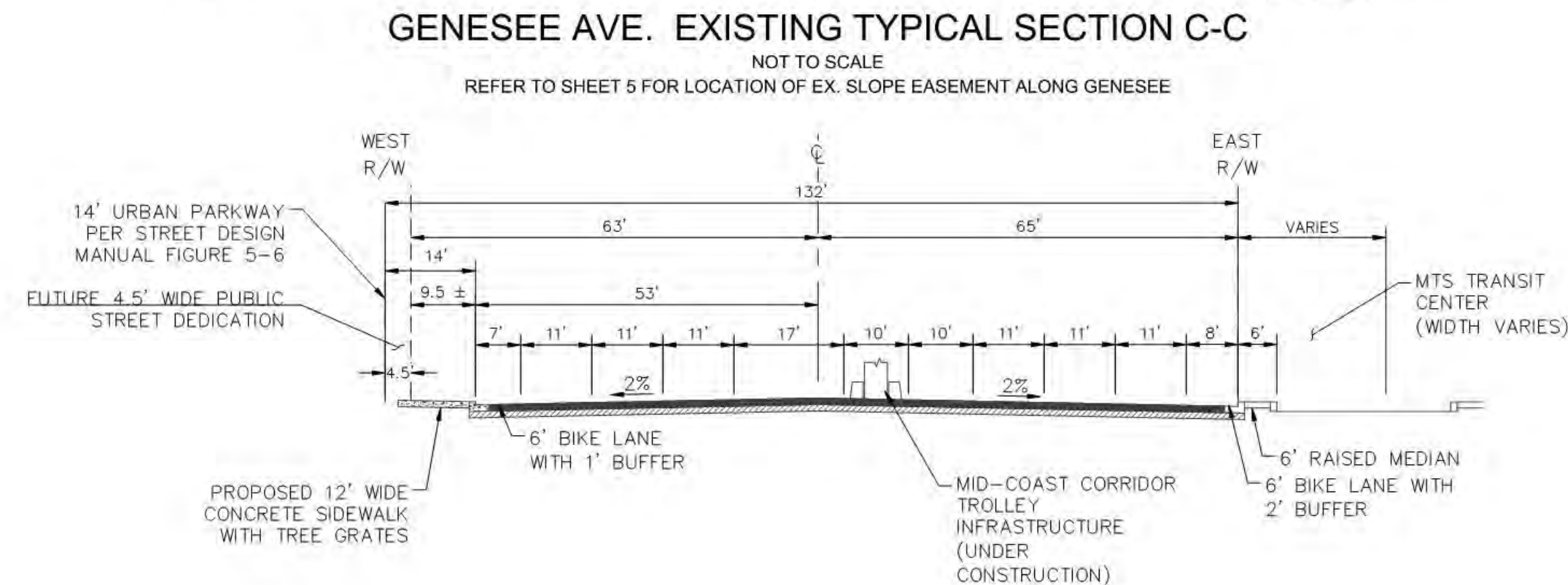
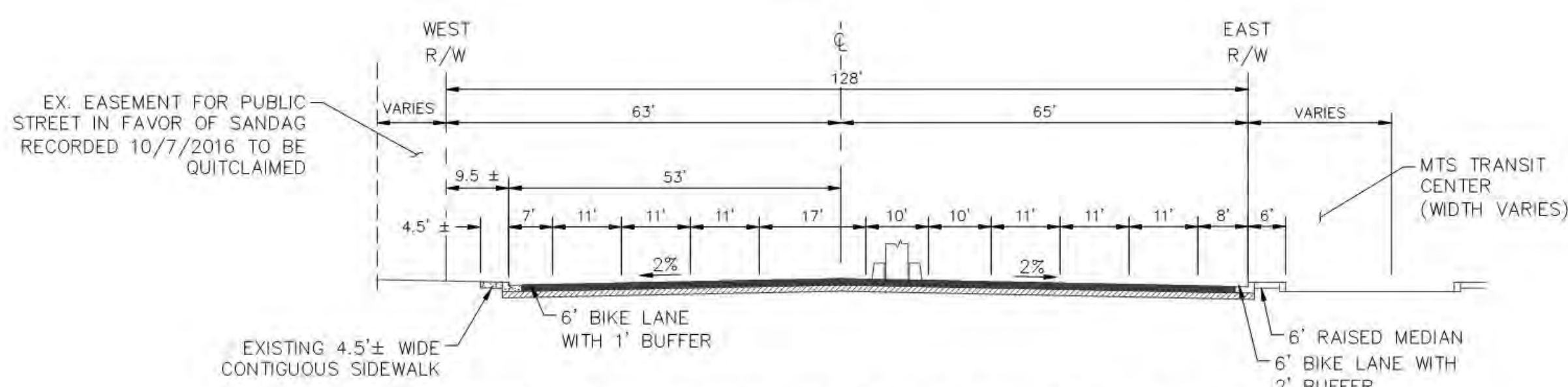


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GENESEE AVENUE

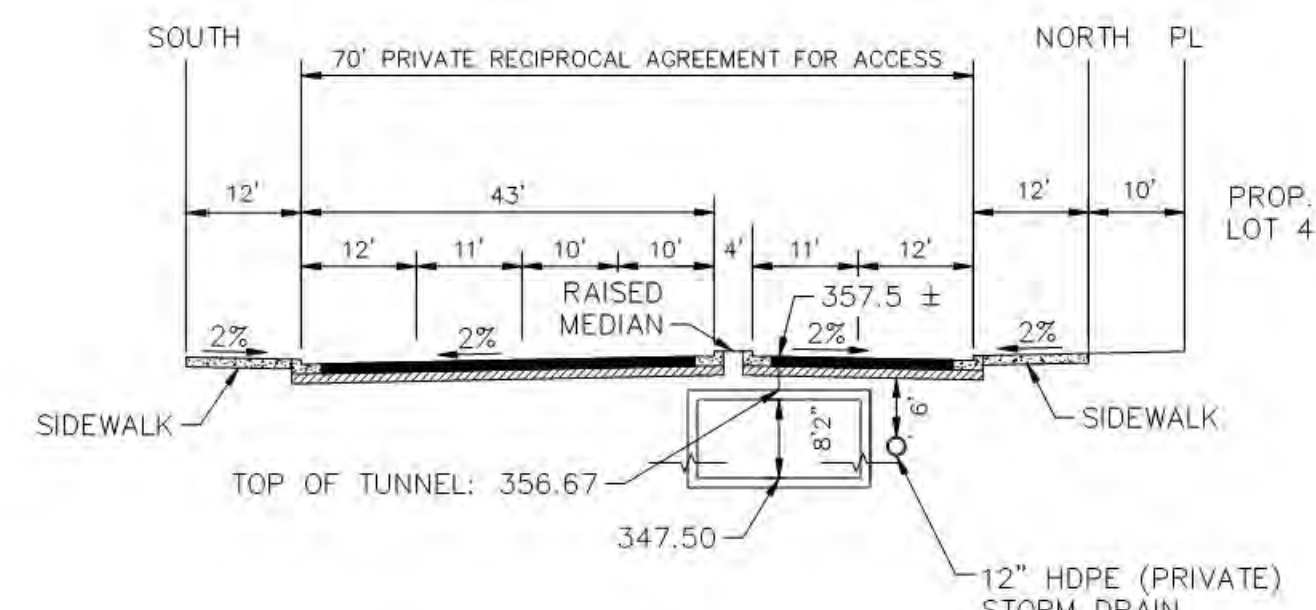
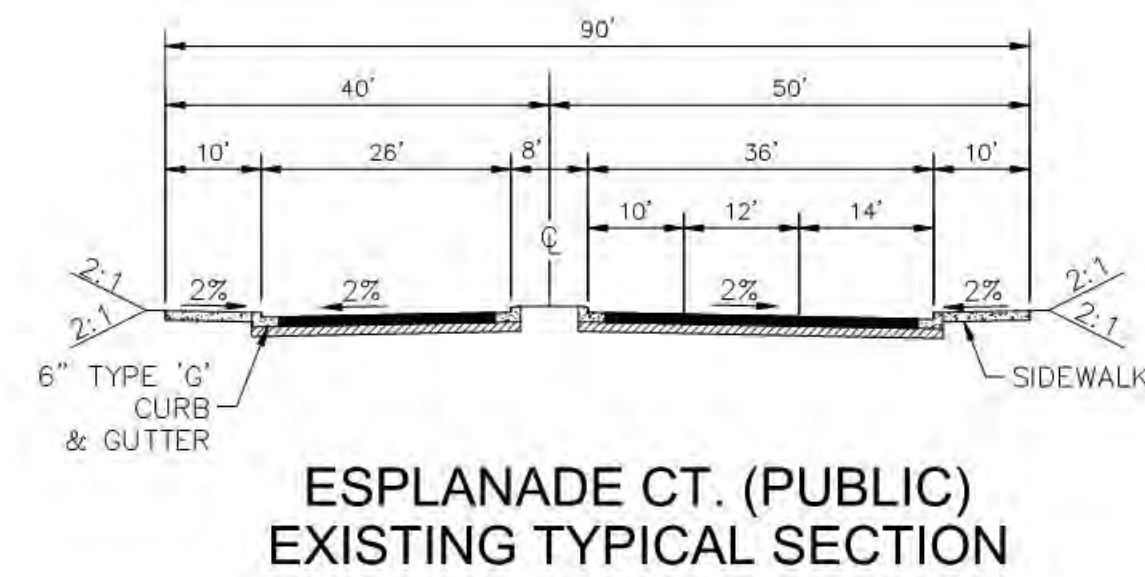


GENESEE AVE. PROPOSED TYPICAL SECTION B-B

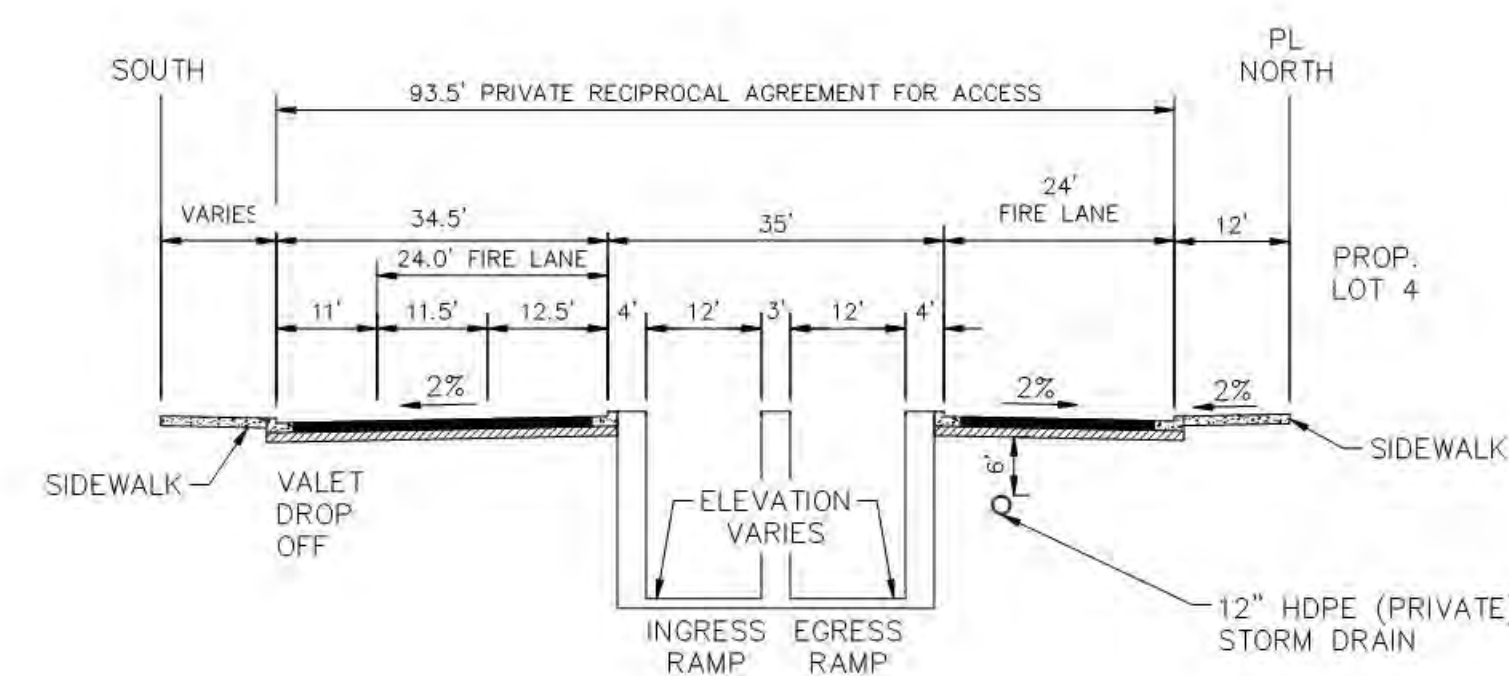


GENESEE AVE. PROPOSED TYPICAL SECTION C-C

ESPLANADE COURT

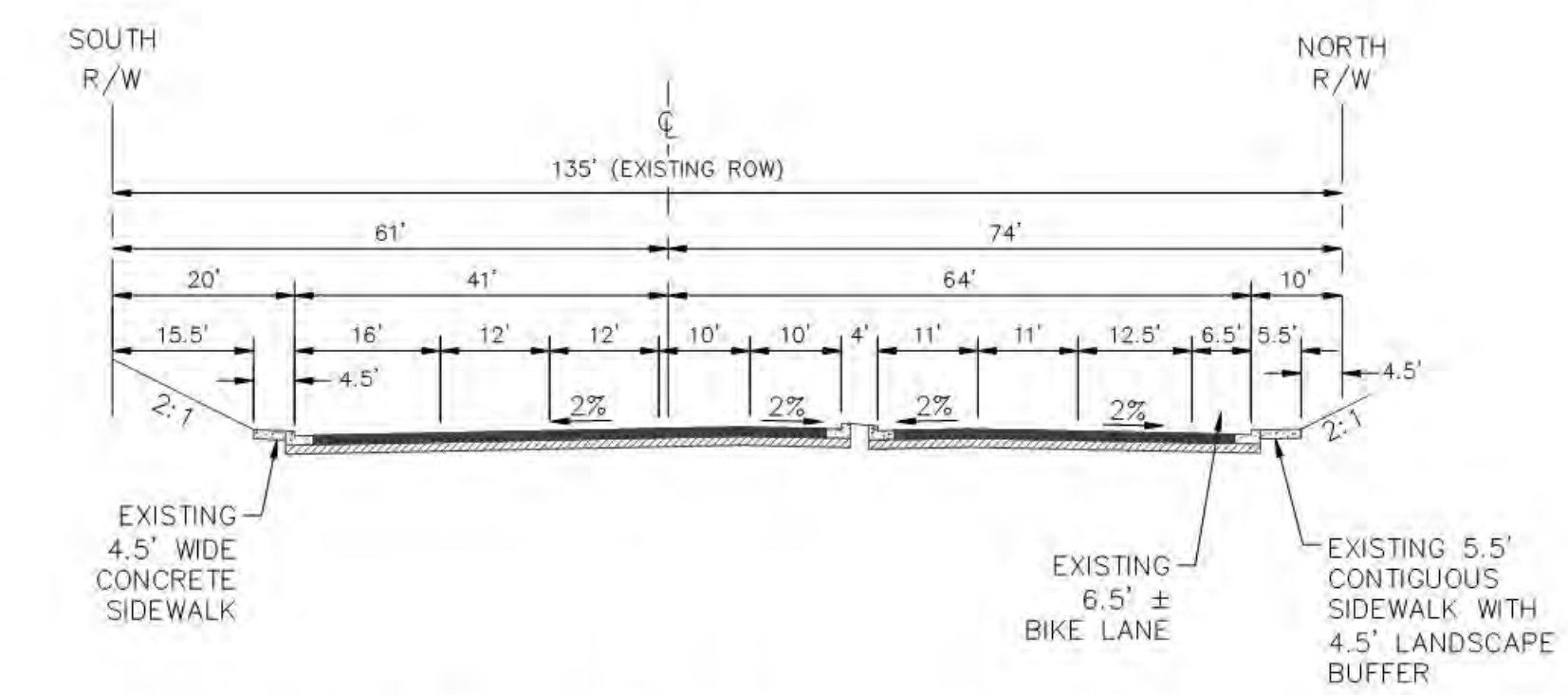


ESPLANADE CT. (PVT.) PROPOSED TYPICAL SECTION E-E

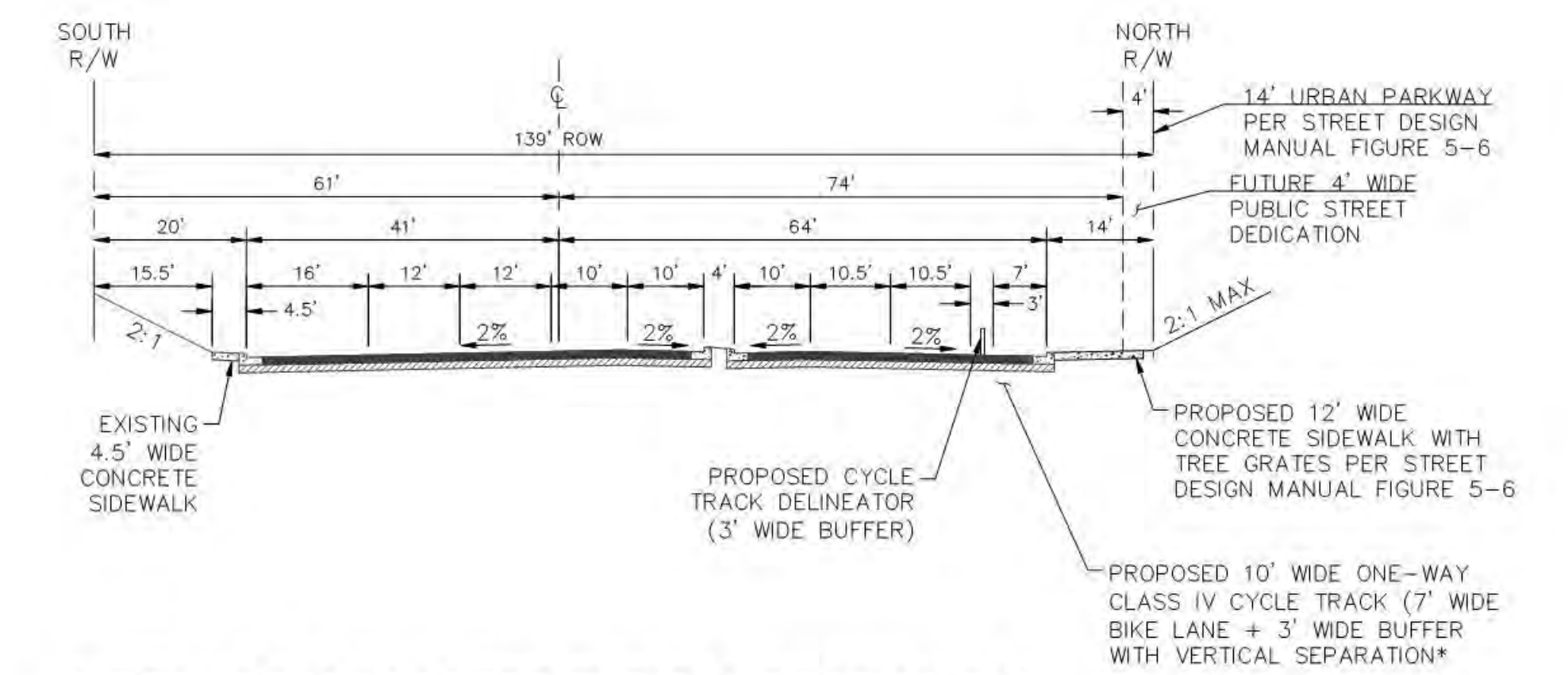


ESPLANADE CT. (PVT.) PROPOSED TYPICAL SECTION F-F

NOBEL DRIVE



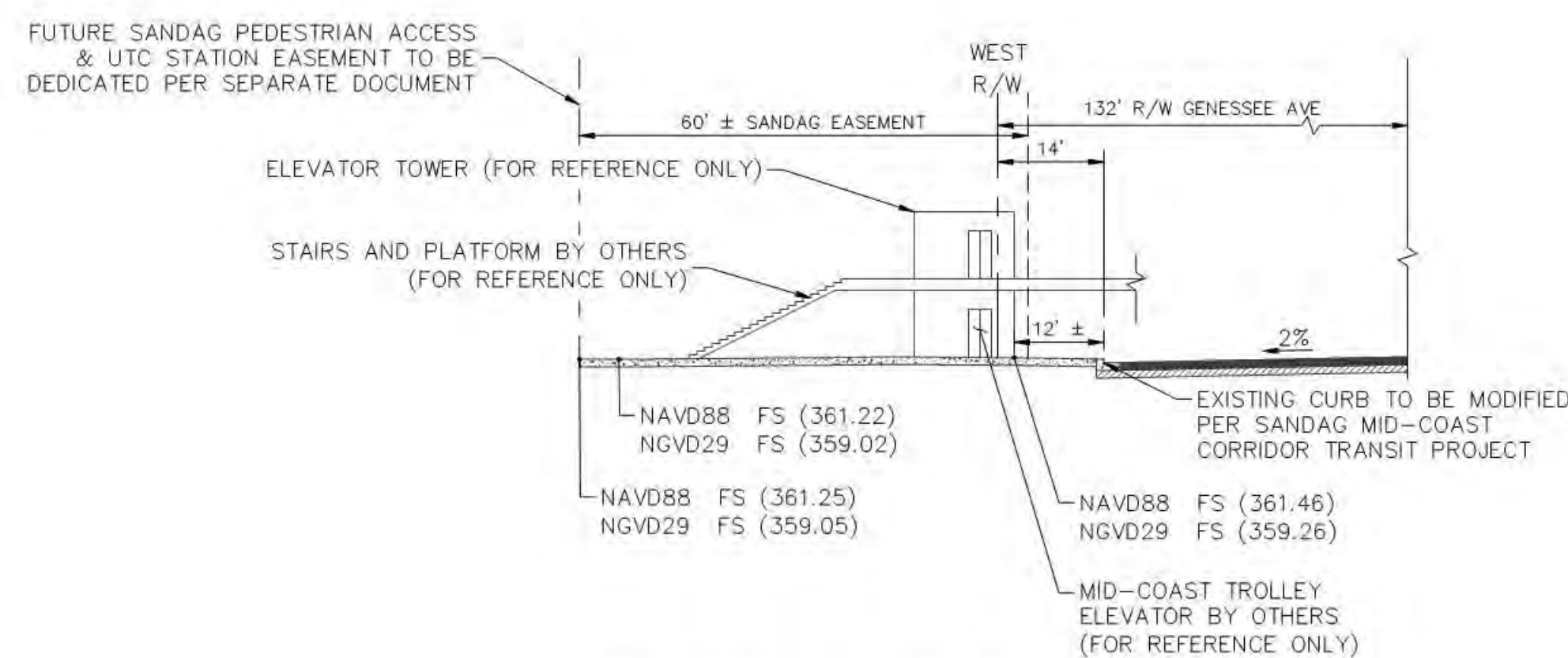
NOBEL DR. EXISTING TYPICAL SECTION G-G



NOBEL DR. PROPOSED TYPICAL SECTION G-G

*NOTE: PROPOSED CLASS IV CYCLE TRACK TO BE INSTALLED ALONG NORTHERLY HALF-WIDTH BETWEEN GENESEE AVENUE AND REGENTS ROAD.

GENESEE AVENUE



GENESEE AVE. PROPOSED SECTION D-D

NOTE:
1. ELEVATIONS REFERENCED HEREON ARE PROVIDED PER SANDAG MID-COAST CORRIDOR PROJECT DRAWING #CD441, DATED AUGUST 2019, WHICH WAS PREPARED ON NAVD-88 DATUM. THE COSTA VERDE PLANS ARE PREPARED ON NGVD-29 PER CITY OF SAN DIEGO REQUIREMENTS. THE ELEVATIONS SHIFT FROM NAVD-88 TO NGVD-29 IS 2.2' VERTICALLY DOWNWARD.
2. UTC STATION IMPROVEMENTS SHOWN FOR REFERENCE ONLY AND MAY NOT BE TO SCALE.

SEE SHEET 4 FOR VISIBILITY TRIANGLES

SIGHT DISTANCE CALCULATIONS

SPEED LIMIT ASSUMPTIONS:
GENESEE AVENUE - 45 MPH
NOBEL DRIVE - 40 MPH
COSTA VERDE BOULEVARD - 25 MPH
ESPLANADE COURT - 10 MPH

INTERSECTIONS WITH STOP CONTROL ON THE MINOR ROAD (CASE B1)

THE VERTEX (DECISION POINT) OF THE DEPARTURE SIGHT TRIANGLE ON THE MINOR ROAD SHOULD BE 14.5 FT FROM THE EDGE OF THE MAJOR ROAD TRAVELED WAY

PER TABLE 9-7, DESIGN INTERSECTION SIGHT DISTANCE - CASE B1, LEFT TURN FROM STOP
@ 45 MPH - 500'
@ 40 MPH - 445'
@ 25 MPH - 280'

PER TABLE 9-8, DESIGN INTERSECTION SIGHT DISTANCE - CASE B2, LEFT TURN FROM STOP
@ 45 MPH - 430'
@ 40 MPH - 385'
@ 25 MPH - 240'

SOURCE: A POLICY ON GEOMETRIC DESIGN OF HIGHWAYS AND STREETS, 7TH EDITION, 2018

SIGNALIZED INTERSECTIONS (CASE D - INTERSECTIONS WITH TRAFFIC SIGNAL CONTROL)

"AT SIGNALIZED INTERSECTIONS, THE FIRST VEHICLE STOPPED ON ONE APPROACH SHOULD BE VISIBLE TO THE DRIVER OF THE FIRST VEHICLE STOPPED ON EACH OF THE OTHER APPROACHES. LEFT-TURNING VEHICLES SHOULD HAVE SUFFICIENT SIGHT DISTANCE TO SELECT GAPS IN ONCOMING TRAFFIC AND COMPLETE LEFT TURNS. APART FROM THESE SIGHT CONDITIONS, THERE ARE GENERALLY NO OTHER APPROACH OR DEPARTURE SIGHT TRIANGLES NEEDED FOR SIGNALIZED INTERSECTIONS."

PREPARED BY: **Kimley»Horn**

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ORIGINAL DATE: 3/15/2016
REVISION DATE: 7/28/2016 1/24/2020
REVISION DATE: 12/12/2016
REVISION DATE: 12/19/2017
REVISION DATE: 6/10/2019
REVISION DATE: 10/11/2019

TYPICAL SECTIONS

SHEET NO. ENT-C0.001 3 OF 65

PTS #: 477943
PROJECT #: 477943

Regency Centers
REGENCY CENTERS
420 STEVENS AVE. SUITE 320
SOLANA BEACH, CA 92075

LEGEND:	
	EX. STORM DRAIN
	PROP. STORM DRAIN (PRIVATE)
	PROP. STORM DRAIN GRATE INLET (PRIVATE)
	PROP. STORM DRAIN CLEANOUT (PRIVATE)
	EX. SIGNALIZED INTERSECTION
	EX. CONTOURS
	PROP. CONTOURS
	SLOPE
	RETAINING WALL
	DAYLIGHT LINE
	BUILDING LIMITS
	CENTERLINE
	BUILDING POINT OF CONNECTION (PRIVATE)
	ULTIMATE RIGHT OF WAY
	LIMITS OF WORK
	LIMITS OF UNDERGROUND PARKING STRUCTURE
	STORM DIRECTION OF FLOW
	VISIBILITY TRIANGLES
	AASHTO SIGHT TRIANGLE
	UNDERGROUND PARKING STRUCTURE
	PROP. LANDSCAPE
	5' X 8' TREE WELL PER LANDSCAPE PLAN
	CLASS IV CYCLE TRACK DELINEATOR
	FUTURE PUBLIC STREET DEDICATION

ABBREVIATIONS:	
AC	ASBESTOS CONCRETE
BW	BOTTOM OF WALL
CL	CENTERLINE
DWY.	DRIVEWAY
EX.	EXISTING
FFE	FINISH FLOOR ELEVATION
FG	FINISH GRADE
FL	FLOW LINE
FS	FINISH SURFACE
H	HEIGHT
IE	INVERT ELEVATION
IMPS	IMPROVEMENTS
LP	LENGTH
LAT.	LATERAL
MAX.	MAXIMUM
PROP.	PROPOSED
R	RADIUS
RET. WALL	RETAINING WALL
R/W	RIGHT OF WAY
SWR	SEWER
SF	SQUARE FEET
TP	TOP OF PAVEMENT
TW	TOP OF WALL
TVL	TYPICAL VOLUME

- NOTES:**
- SEE ARCHITECTURAL PLANS FOR FINISH FLOOR ELEVATIONS.
 - PODIUM DRAINAGE IS SHOWN FOR CONCEPTUAL PURPOSES AND WILL BE DESIGNED BY OTHERS.
 - REFER TO ARCHITECTURAL PLANS FOR PODIUM LEVEL ELEVATIONS.
 - NO OBJECTS HIGHER THAN 36 INCHES ARE PROPOSED IN VISIBILITY AREAS. NO VEGETATION, OTHER THAN TREES, WILL EXCEED 24 INCHES IN HEIGHT WITHIN ANY PORTION OF THE VISIBILITY AND SIGHT DISTANCE AREAS THAT LIE WITHIN THE PUBLIC RIGHT-OF-WAY. SEE SHEET ENT-CO.001 FOR SIGHT DISTANCE CALCULATIONS.
 - ALL PROPOSED STORM DRAIN FACILITIES SHOWN HEREON ARE PRIVATE. NO PUBLIC STORM DRAIN FACILITIES ARE PROPOSED.
- SETBACKS:**
- PUBLIC STREET FRONTAGES (GENESEE AVE AND NOBEL DRIVE)-15 FT
 - INTERIOR STREET FRONTAGES - 10 FT
 - REAR YARD - 10 FEET WHEN ADJOINING RESIDENTIAL SITES IF WALLS FACING RESIDENCES HAVE OPENINGS (DOORS/WINDOWS) IF FACING WALLS DO NOT HAVE OPENINGS, REAR YARD MAY BE 5 FT.
 - NO DEVIATIONS FROM REQUIRED SETBACKS ARE BEING REQUESTED.

GRADING INFORMATION:

TOTAL SITE AREA: 13.86 ACRES
 GRADED AREA: 11.92 ACRES
 OUT QUANTITIES: 198,014 CY
 FILL QUANTITIES: 0 CY
 EXPORT: 198,014 CY
 SITE RETAINING WALLS: 630 LF

MAX. CUT DEPTH: ~25 FT
 MAX. CUT SLOPE RATIO: 2:1 MAX
 MAX. FILL DEPTH: 0 FT
 MAX. FILL SLOPE RATIO: 2:1 MAX
 MAX. WALL HEIGHT: 3.5 FT

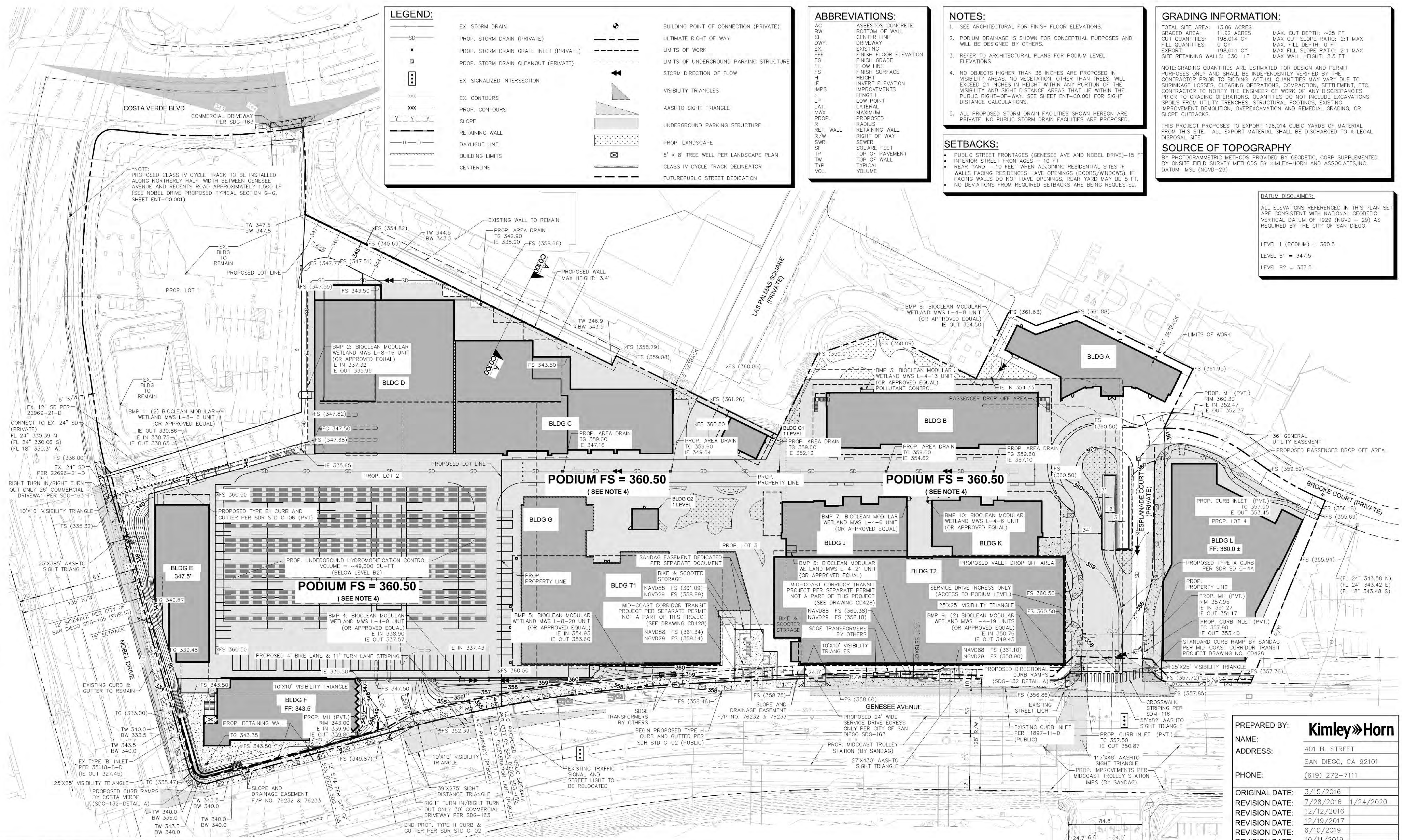
NOTE: GRADING QUANTITIES ARE ESTIMATED FOR DESIGN AND PERMIT PURPOSES ONLY AND SHALL BE INDEPENDENTLY VERIFIED BY THE CONTRACTOR PRIOR TO BIDDING. ACTUAL QUANTITIES MAY VARY DUE TO SHRINKAGE LOSSES, CLEARING OPERATIONS, COMPACTION, SETTLEMENT, ETC. CONTRACTOR TO NOTIFY THE ENGINEER OF WORK OF ANY DISCREPANCIES PRIOR TO GRADING OPERATIONS. QUANTITIES DO NOT INCLUDE EXCAVATIONS FROM UTILITY TRENCHES, STRUCTURAL FOOTINGS, EXISTING IMPROVEMENT DEMOLITION, OVEREXCAVATION AND REMEDIAL GRADING, OR SLOPE CUTBACKS.

THIS PROJECT PROPOSES TO EXPORT 198,014 CUBIC YARDS OF MATERIAL FROM THIS SITE. ALL EXPORT MATERIAL SHALL BE DISCHARGED TO A LEGAL DISPOSAL SITE.

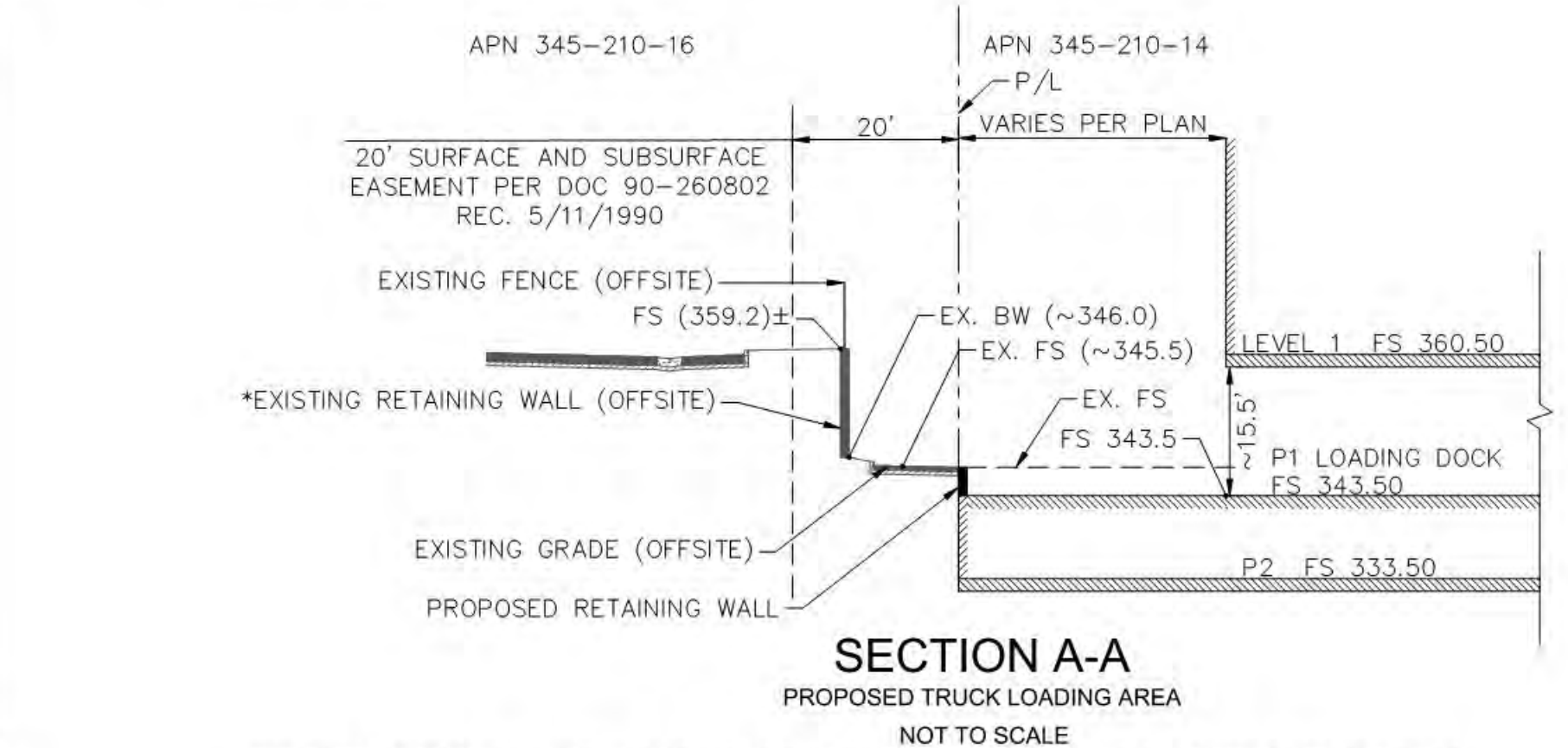
SOURCE OF TOPOGRAPHY
 BY PHOTOGRAMMETRIC METHODS PROVIDED BY GEODETIC, CORP SUPPLEMENTED BY ONSITE FIELD SURVEY METHODS BY KIMLEY-HORN AND ASSOCIATES, INC. DATUM: MSL (NGVD-29)

DATUM DISCLAIMER:
 ALL ELEVATIONS REFERENCED IN THIS PLAN SET ARE REQUIRED WITH NATIONAL GEODETIC VERTICAL DATUM OF 1928 (NGVD - 29) AS PRESENTED BY THE CITY OF SAN DIEGO.

LEVEL 1 (PODIUM) = 360.5
 LEVEL B1 = 347.5
 LEVEL B2 = 337.5



- STORM WATER NOTES:**
- PRIOR TO THE ISSUANCE OF ANY CONSTRUCTION PERMIT, THE OWNER/PERMITEE SHALL ENTER INTO A MAINTENANCE AGREEMENT FOR THE ONGOING PERMANENT BMP MAINTENANCE, SATISFACTORY TO THE CITY ENGINEER.
 - PRIOR TO THE ISSUANCE OF ANY CONSTRUCTION PERMIT, THE OWNER/PERMITEE SHALL INCORPORATE ANY CONSTRUCTION BEST MANAGEMENT PRACTICES NECESSARY TO COMPLY WITH CHAPTER 14, ARTICLE 2, DIVISION 1 (GRADING REGULATIONS) OF THE SAN DIEGO MUNICIPAL CODE, INTO THE CONSTRUCTION PLANS OR SPECIFICATIONS.
 - PRIOR TO THE ISSUANCE OF ANY CONSTRUCTION PERMIT, THE APPLICANT SHALL SUBMIT A TECHNICAL REPORT THAT WILL BE SUBJECT TO FINAL REVIEW AND APPROVAL BY THE CITY ENGINEER, BASED ON THE STORM WATER STANDARDS IN EFFECT AT THE TIME OF THE CONSTRUCTION PERMIT ISSUANCE.
 - DEVELOPMENT OF THIS PROJECT SHALL COMPLY WITH ALL STORM WATER CONSTRUCTION REQUIREMENTS OF THE STATE CONSTRUCTION GENERAL PERMIT, ORDER NO. 2009-00090DWO, OR SUBSEQUENT ORDER, AND THE MUNICIPAL STORM WATER PERMIT, ORDER NO. R9-2013-0001, OR SUBSEQUENT ORDER, IN ACCORDANCE WITH ORDER NO. 2009-00090DWO, OR SUBSEQUENT ORDER. A RISK LEVEL DETERMINATION SHALL BE CALCULATED FOR THE SITE AND A STORM WATER POLLUTION PREVENTION PLAN (SWPPP) SHALL BE IMPLEMENTED CONCURRENTLY WITH THE COMMENCEMENT OF GRADING ACTIVITIES.
 - PRIOR TO ISSUANCE OF A GRADING OR A CONSTRUCTION PERMIT, A COPY OF THE NOTICE OF INTENT (NOI) WITH A VALID WASTE DISCHARGE ID NUMBER (WDID#) SHALL BE SUBMITTED TO THE CITY OF SAN DIEGO AS A PROOF OF ENROLLMENT UNDER THE CONSTRUCTION GENERAL PERMIT. WHEN OWNERSHIP OF THE ENTIRE SITE OR PORTIONS OF THE SITE CHANGES PRIOR TO FILING OF THE NOTICE OF TERMINATION (NOT), A REVISED NOI SHALL BE SUBMITTED ELECTRONICALLY TO THE STATE WATER RESOURCES BOARD IN ACCORDANCE WITH THE PROVISIONS AS SET FORTH IN SECTION 11.C OF ORDER NO. 2009-0009-DWO AND A COPY SHALL BE SUBMITTED TO THE CITY.
 - PRIOR TO THE ISSUANCE OF ANY BUILDING PERMIT, THE OWNER/PERMITEE SHALL ENTER INTO A MAINTENANCE AGREEMENT FOR THE ONGOING PERMANENT BMP MAINTENANCE, SATISFACTORY TO THE CITY ENGINEER.
 - ALL PROPOSED STORM DRAIN LINES AND STRUCTURES ARE PRIVATE UNLESS OTHERWISE INDICATED.



PREPARED BY: **Kimley-Horn**

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 ADDRESS: SAN DIEGO, CA 92101
 PHONE: (619) 272-7111

ORIGINAL DATE: 3/15/2016
 REVISION DATE: 7/28/2016, 1/24/2020
 REVISION DATE: 12/12/2016
 REVISION DATE: 12/19/2017
 REVISION DATE: 6/10/2019
 REVISION DATE: 10/11/2019

SHEET TITLE: PRELIM. GRADING AND DRAINAGE PLAN

SHEET NO. ENT-CO.100 4 OF 65

PTS #: 477943
 PROJECT #: 477943

Regency Centers. REGENCY CENTERS
 420 STEVENS AVE. SUITE 320
 SOLANA BEACH, CA 92075

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*CONTRACTOR TO VERIFY SIZE AND DEPTH OF EXISTING FOOTING AND NOTIFY EOR OF DISCREPANCIES.

LEGEND:

	EX. STORM DRAIN (PER GRADING AND DRAINAGE PLAN)		PROP. SANITARY SEWER MANHOLE (PRIVATE)
	EXISTING CENTERLINE		PROP. SANITARY SEWER CLEANOUT (PRIVATE)
	BUILDING UTILITY POINT OF CONNECTION (PRIVATE)		EX. WATER LINE
	FIRE DEPARTMENT CONNECTION (FDC)		PROP. DOMESTIC WATER LINE (PRIVATE)
	POST INDICATOR VALVE (PIV)		PROP. WATER METER (WITHIN R/W, NOT TO SCALE)
	PROPOSED STANDPIPE PER MEP PLANS		UNDERGROUND PARKING STRUCTURE
	RIGHT OF WAY		PROPOSED BUILDING FOOTPRINTS (LEVEL 1)
	LIMITS OF UNDERGROUND PARKING STRUCTURE		UTILITIES TO BE ABANDONED AND OR RELOCATED
	EX. FIRE LINE (PRIVATE)		
	PROP. FIRE HYDRANT (PRIVATE)		
	EX. SANITARY SEWER LINE		
	PROP. SANITARY SEWER LINE (PRIVATE)		

ABBREVIATIONS:

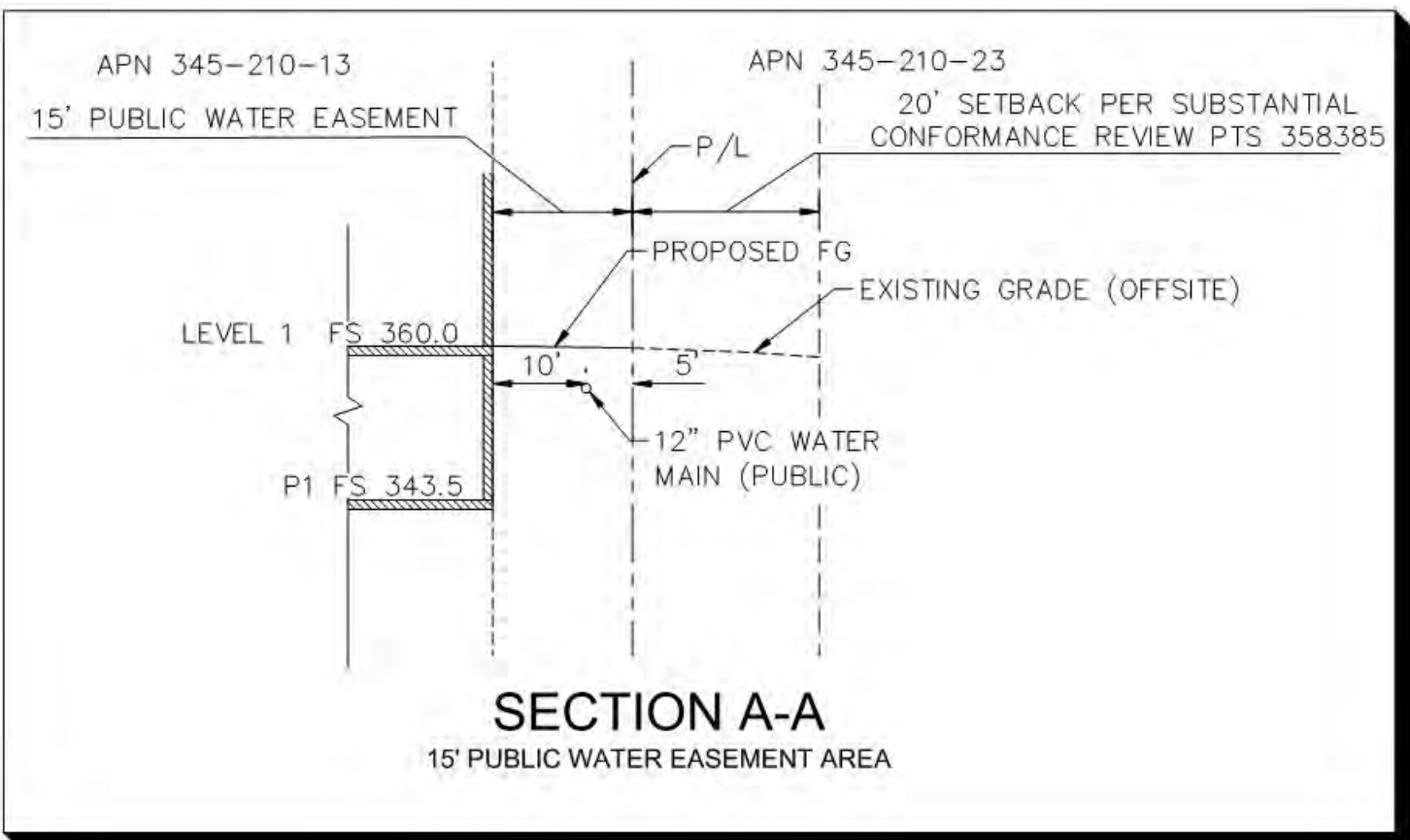
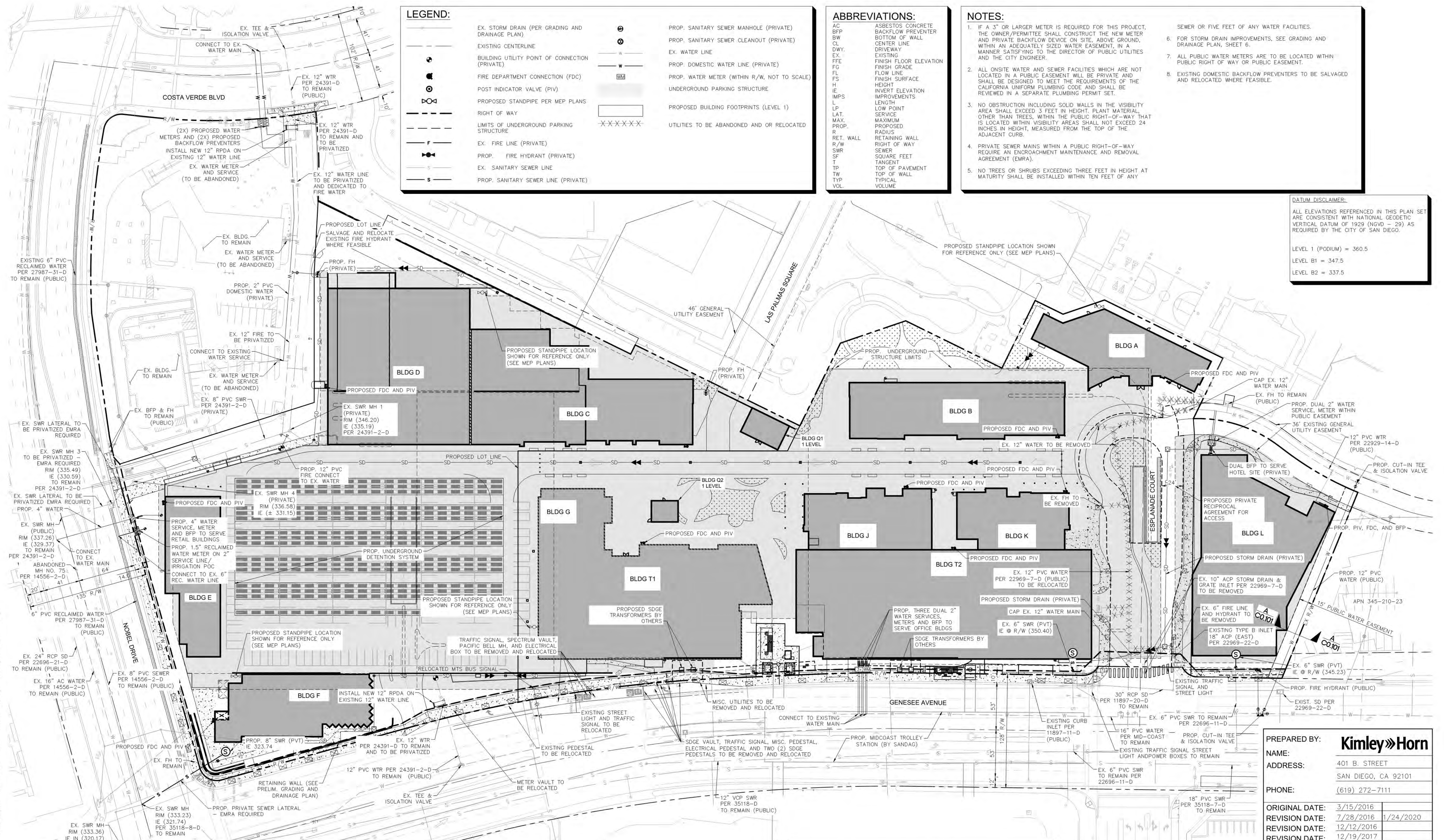
AC	ASBESTOS CONCRETE
BFP	BACKFLOW PREVENTER
BW	BOTTOM OF WALL
CL	CENTER LINE
DWY.	DRIVEWAY
EX.	EXISTING
FFE	FINISH FLOOR ELEVATION
FG	FINISH GRADE
FL	FLOW LINE
FS	FINISH SURFACE
H	HEIGHT
IE	INVERT ELEVATION
IMPS	IMPROVEMENTS
L	LENGTH
LP	LOW POINT
LAT.	SERVICE
MAX.	MAXIMUM
PROP.	PROPOSED
R	RADIUS
RET. WALL	RETAINING WALL
R/W	RIGHT OF WAY
SWR	SEWER
SF	SQUARE FEET
T	TANGENT
TP	TOP OF PAVEMENT
TW	TOP OF WALL
TYP	TYPICAL
VOL.	VOLUME

NOTES:

- IF A 3" OR LARGER METER IS REQUIRED FOR THIS PROJECT, THE OWNER/PERMITTEE SHALL CONSTRUCT THE NEW METER AND PRIVATE BACKFLOW DEVICE ON SITE, ABOVE GROUND, WITHIN AN ADEQUATELY SIZED WATER EASEMENT, IN A MANNER SATISFYING TO THE DIRECTOR OF PUBLIC UTILITIES AND THE CITY ENGINEER.
- ALL ONSITE WATER AND SEWER FACILITIES WHICH ARE NOT LOCATED IN A PUBLIC EASEMENT WILL BE PRIVATE AND SHALL BE DESIGNED TO MEET THE REQUIREMENTS OF THE CALIFORNIA UNIFORM PLUMBING CODE AND SHALL BE REVIEWED IN A SEPARATE PLUMBING PERMIT SET.
- NO OBSTRUCTION INCLUDING SOLID WALLS IN THE VISIBILITY AREA SHALL EXCEED 3 FEET IN HEIGHT. PLANT MATERIAL OTHER THAN TREES, WITHIN THE PUBLIC RIGHT-OF-WAY THAT IS LOCATED WITHIN VISIBILITY AREAS SHALL NOT EXCEED 24 INCHES IN HEIGHT, MEASURED FROM THE TOP OF THE ADJACENT CURB.
- PRIVATE SEWER MAINS WITHIN A PUBLIC RIGHT-OF-WAY REQUIRE AN ENCROACHMENT MAINTENANCE AND REMOVAL AGREEMENT (EMRA).
- NO TREES OR SHRUBS EXCEEDING THREE FEET IN HEIGHT AT MATURITY SHALL BE INSTALLED WITHIN TEN FEET OF ANY SEWER OR FIVE FEET OF ANY WATER FACILITIES.
- FOR STORM DRAIN IMPROVEMENTS, SEE GRADING AND DRAINAGE PLAN, SHEET 6.
- ALL PUBLIC WATER METERS ARE TO BE LOCATED WITHIN PUBLIC RIGHT OF WAY OR PUBLIC EASEMENT.
- EXISTING DOMESTIC BACKFLOW PREVENTERS TO BE SALVAGED AND RELOCATED WHERE FEASIBLE.

DATUM DISCLAIMER:
ALL ELEVATIONS REFERENCED IN THIS PLAN SET ARE CONSISTENT WITH NATIONAL GEODETIC VERTICAL DATUM OF 1929 (NGVD - 29) AS REQUIRED BY THE CITY OF SAN DIEGO.

LEVEL 1 (PODIUM) = 360.5
LEVEL B1 = 347.5
LEVEL B2 = 337.5



PREPARED BY: **Kimley»Horn**

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ORIGINAL DATE: 3/15/2016	
REVISION DATE: 7/28/2016	1/24/2020
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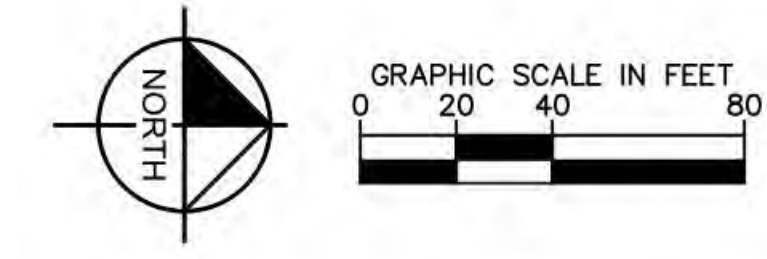
SHEET TITLE: UTILITY PLAN

SHEET NO. ENT-C0.101 5 OF 65

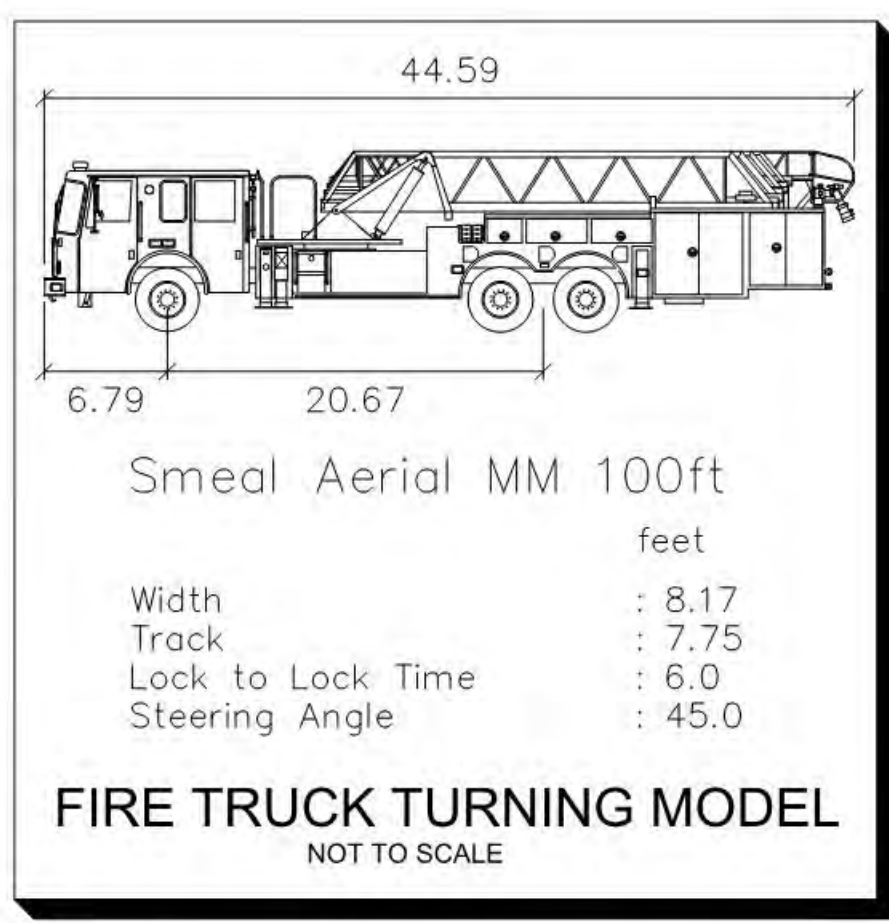
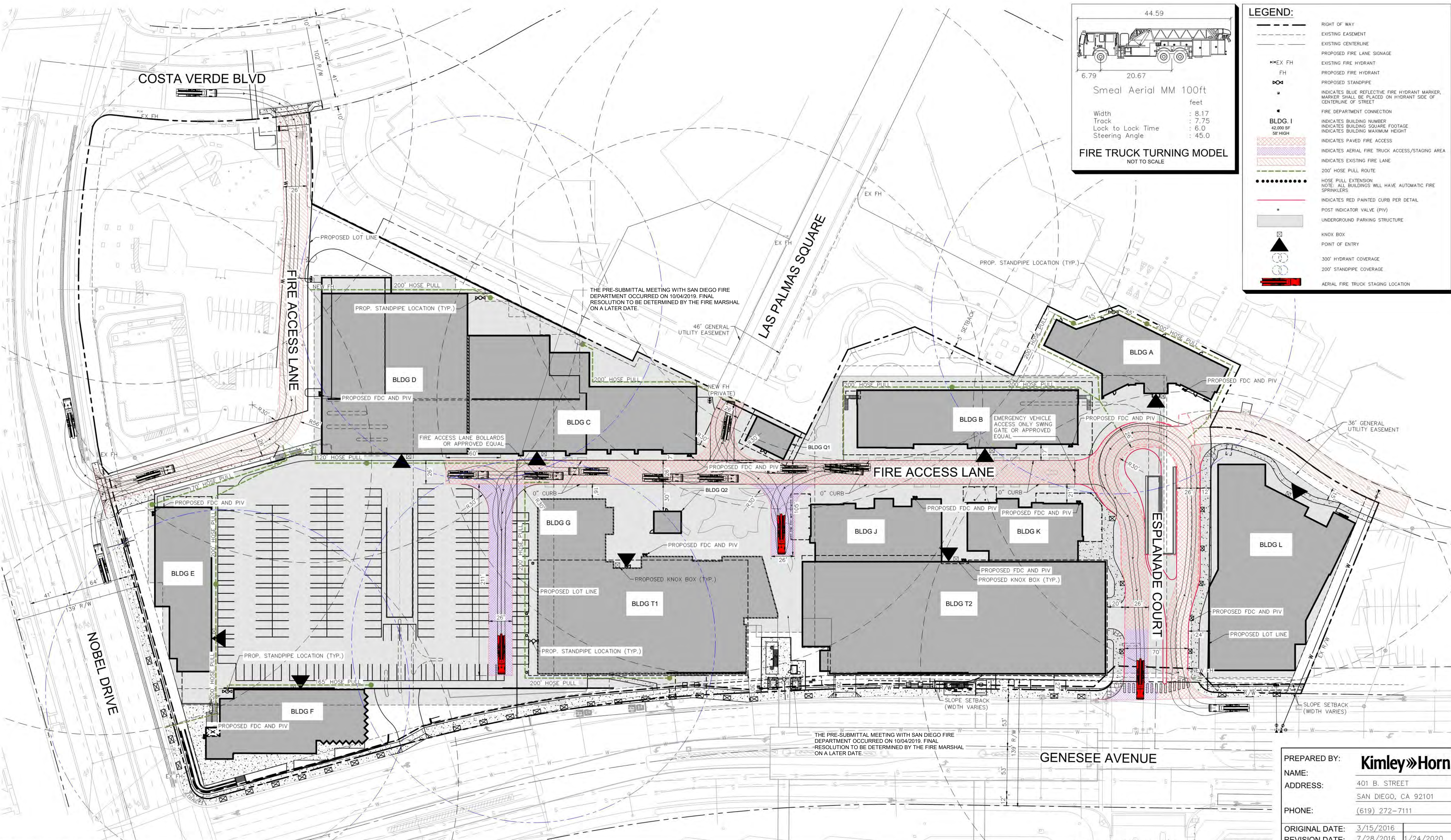
PTS #: 477943

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LEGEND:

- RIGHT OF WAY
- EXISTING EASEMENT
- EXISTING CENTERLINE
- PROPOSED FIRE LANE SIGNAGE
- EXISTING FIRE HYDRANT
- PROPOSED FIRE HYDRANT
- PROPOSED STANDPIPE
- INDICATES BLUE REFLECTIVE FIRE HYDRANT MARKER. MARKER SHALL BE PLACED ON HYDRANT SIDE OF CENTERLINE OF STREET
- FIRE DEPARTMENT CONNECTION
- INDICATES BUILDING NUMBER
- INDICATES BUILDING SQUARE FOOTAGE
- INDICATES BUILDING MAXIMUM HEIGHT
- INDICATES PAVED FIRE ACCESS
- INDICATES AERIAL FIRE TRUCK ACCESS/STAGING AREA
- INDICATES EXISTING FIRE LANE
- 200' HOSE PULL ROUTE
- HOSE PULL EXTENSION NOTE: ALL BUILDINGS WILL HAVE AUTOMATIC FIRE SPRINKLERS
- INDICATES RED PAINTED CURB PER DETAIL
- POST INDICATOR VALVE (PIV)
- UNDERGROUND PARKING STRUCTURE
- KNOX BOX
- POINT OF ENTRY
- 300' HYDRANT COVERAGE
- 200' STANDPIPE COVERAGE
- AERIAL FIRE TRUCK STAGING LOCATION

GENERAL FIRE NOTES:

- CITY OF SAN DIEGO FIRE SITE INSPECTIONS ARE REQUIRED FOR THIS PROJECT. PLEASE SCHEDULE ALL FIELD INSPECTIONS AT LEAST 48 HOURS IN ADVANCE. INSPECTIONS CANCELLED AFTER 1 P.M. ON THE DAY BEFORE THE SCHEDULED DATE WILL BE SUBJECT TO A RE-INSPECTION FEE. CALL CITY OF SAN DIEGO FIRE INSPECTION SCHEDULING AT (619) 446-5440.
- A LUMBER DROP INSPECTION SHALL BE PERFORMED PRIOR TO BRINGING COMBUSTIBLE MATERIALS ON-SITE (OR DRYWALL FOR STRUCTURES OF NON-COMBUSTIBLE CONSTRUCTION). ALL-WEATHER ACCESS ROADS CAPABLE OF SUPPORTING 95,000 LBS., TOPPED WITH ASPHALT, CONCRETE OR EQUIVALENT SHALL BE IN PLACE AND HYDRANTS OPERATIONAL AT TIME OF LUMBER DROP INSPECTION. USE THE FIRE MASTER PLAN SERVICE REQUEST NUMBER TO SCHEDULE THIS INSPECTION.
- FOR PROJECTS IN WILDLAND INTERFACE AREA, A VEGETATION CLEARANCE INSPECTION IS REQUIRED PRIOR TO A LUMBER DROP INSPECTION. USE THE FUEL MODIFICATION PLAN SERVICE REQUEST NUMBER TO SCHEDULE THE VEGETATION CLEARANCE INSPECTION.
- PHASE INSTALLATION OF FIRE ACCESS ROADS REQUIRES ADDITIONAL INSPECTIONS NOT COVERED BY THE FEES PAID AT PLAN SUBMITTAL. CONTACT INSPECTION NOT COVERED BY THE FEES PAID AT PLAN SUBMITTAL. CONTACT INSPECTION SCHEDULING TO ARRANGE FOR ADDITIONAL INSPECTIONS THAT MAY BE NEEDED AND ANY FEES THAT MAY BE DUE.
- AN ORIGINAL APPROVED, SIGNED, WET-STAMPED CITY OF SAN DIEGO FIRE MASTER PLAN SHALL BE AVAILABLE ON-SITE AT TIME OF INSPECTION.
- ACCESS ROADS AND HYDRANTS SHALL BE MAINTAINED AND REMAIN CLEAR OF OBSTRUCTIONS AT ALL ITEMS DURING AND AFTER CONSTRUCTION. AREAS WHERE PARKING IS NOT PERMITTED SHALL BE CLEARLY IDENTIFIED AT ALL TIMES.
- TEMPORARY FUEL TANKS OF 60 OR MORE GALLONS SHALL BE REVIEWED, INSPECTED, AND PERMITTED BY THE CITY OF SAN DIEGO FIRE PRIOR TO USE.
- THE PROJECT ADDRESS (OR TRACT NUMBER AND PROJECT NAME, WHERE STREET SIGNS ARE NOT INSTALLED) SHALL BE CLEARLY POSTED AND VISIBLE FROM THE PUBLIC ROAD DURING CONSTRUCTION.
- ALL GATES IN CONSTRUCTION FENCING SHALL BE EQUIPPED WITH EITHER A KNOX OR BREAKAWAY PADLOCK.
- FIRE LANE WIDTHS SHALL BE MEASURED FROM TOP FACE OF THE CURB TO TOP FACE OF THE CURB FOR FIRE LANES WITH STANDARD CURBS AND GUTTERS AND 6" FROM THE BACK OF CURB FOR FIRE LANES WITH MODIFIED CURB DESIGNS (E.G., ROLLED, RAMPED, ETC.).
- THE DEVELOPER IS RESPONSIBLE TO VERIFY THAT ALL APPROVED PUBLIC WORKS OR GRADING DEPARTMENT STREET IMPROVEMENT PLANS OR PRECISE GRADING PLANS CONFORM TO THE MINIMUM STREET WIDTH MEASUREMENTS PER THE APPROVED CITY OF SAN DIEGO FIRE AND CITY STANDARDS IDENTIFIED IN CITY OF SAN DIEGO FIRE FIRE ACCESS ROADWAYS POLICY.
- FIRE LANE SIGNS AND RED CURBS SHALL MEET THE SPECIFICATIONS SHOWN IN CITY OF SAN DIEGO FIRE FIRE ACCESS ROADWAYS POLICY AND SHALL BE INSTALLED AS DESCRIBED THEREIN. ADDITIONAL FIRE LANE MARKINGS MAY BE REQUIRED AT THE TIME OF INSPECTION DEPENDING ON FIELD CONDITIONS.
- ALL FIRE HYDRANTS SHALL HAVE A BLUE REFLECTIVE PAVEMENT MARKER INDICATING THEIR LOCATION PER THE CITY OF SAN DIEGO FIRE POLICY. ON PRIVATE PROPERTY, MARKERS ARE TO BE MAINTAINED IN GOOD CONDITION BY THE PROPERTY OWNER.
- ADDRESS NUMBERS SHALL BE LOCATED AND BE OF A COLOR AND SIZE SO AS TO BE PLAINLY VISIBLE AND LEGIBLE FROM THE ROADWAY FROM WHICH THE BUILDING IS ADDRESSED. ADDRESS LETTERING SHALL BE 12" IN HEIGHT WITH A STROKE OF 1 1/2". ADDRESS NUMBERS SHALL BE ILLUMINATED AT NIGHT.
- ACCESS GATES SHALL BE APPROVED PRIOR TO INSTALLATION AND SHALL BE IN COMPLIANCE WITH SECTION 902 OF THE 2007 CFC AND CITY OF SAN DIEGO FIRE POLICY.
- APPROVED ACCESS WALKWAYS SHALL BE PROVIDED TO ALL REQUIRED OPENINGS AND ALL RESCUE WINDOWS.
- VEGETATION SHALL BE SELECTED AND MAINTAINED IN SUCH A MANNER AS TO ALLOW IMMEDIATE ACCESS TO ALL HYDRANTS, VALVES, FIRE DEPARTMENT CONNECTIONS, PULL STATIONS, EXTINGUISHERS, SPRINKLER RISERS, ALARM

- CONTROL PANELS, RESCUE WINDOWS, AND OTHER DEVICES OR AREAS USED FOR FIREFIGHTING PURPOSES, VEGETATION OR BUILDING FEATURES SHALL NOT OBSTRUCT ADDRESS NUMBERS OR INHIBIT THE FUNCTIONING OF ALARM BELLS, HORNS, OR STROBES.
- ANY FUTURE MODIFICATION TO THE APPROVED FIRE MASTER PLAN OR APPROVED SITE PLAN, INCLUDING BUT NOT LIMITED TO ROAD WIDTH, GRADE, SPEED HUMPS, TURNING RADII, GATES OR OTHER OBSTRUCTIONS, SHALL REQUIRE REVIEW, INSPECTION, AND APPROVAL BY THE CITY OF SAN DIEGO FIRE.
- APPROVAL OF THIS PLAN SHALL NOT BE CONSIDERED AS APPROVAL OF ANY INFORMATION OR PROJECT CONDITIONS OTHER THAN THOSE ITEMS AND REQUIREMENTS IDENTIFIED IN CITY OF SAN DIEGO FIRE POLICY AND RELATED PORTIONS OF THE CFC AND CBC. THIS PROJECT MAY BE SUBJECT TO ADDITIONAL REQUIREMENTS NOT STATED HEREIN UPON EXAMINATION OF ACTUAL SITE AND PROJECT CONDITIONS OR DISCLOSURE OF ADDITIONAL INFORMATION.
- AUTOMATIC FIRE SPRINKLER SYSTEMS SHALL BE INSTALLED IN ACCORDANCE WITH APPLICABLE CODES AND LOCAL ORDINANCES, AMENDMENTS, AND GUIDELINES. A SEPARATE PLAN SUBMITTAL IS REQUIRED.
- REQUIRED FIRE HYDRANT SPACING IS AT 600' MAXIMUM AND 300' FROM END OF ACCESS ROADS. THIS HYDRANT SPACING ON THIS PROJECT AVERAGES 250'.
- FIRE APPARATUS ACCESS ROADS SHALL BE DESIGNED AND MAINTAINED TO SUPPORT THE IMPOSED LOADS OF FIRE APPARATUS AND SHALL BE SURFACED SO AS TO PROVIDE ALL WEATHER DRIVING CAPABILITIES. CFC 503.2.3.
- FIRE APPARATUS ACCESS ROADS AND WATER SUPPLIES FOR FIRE PROTECTION SHALL BE INSTALLED AND MADE SERVICEABLE PRIOR TO AND DURING TIME OF CONSTRUCTION. CFC 501.4.
- POST INDICATOR VALVES, FIRE DEPARTMENT CONNECTIONS, AND ALARM BELL ARE TO BE LOCATED ON THE ADDRESS/ACCESS SIDE OF THE STRUCTURE. 912.2.1.
- CBC SEC. 3002.4 - GENERAL STRETCHER REQUIREMENTS - ALL BUILDINGS AND STRUCTURES WITH ONE OR MORE PASSENGER SERVICE ELEVATORS SHALL BE PROVIDED WITH NOT LESS THAN ONE MEDICAL EMERGENCY SERVICE

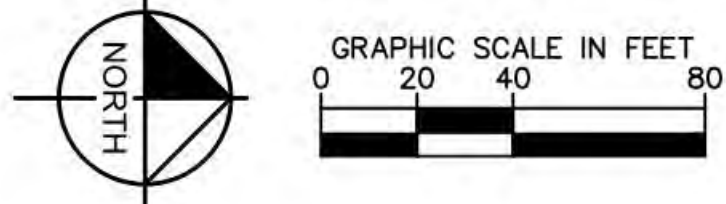
- ELEVATOR TO ALL LANDINGS MEETING THE PROVISIONS OF SECTION 3002.4A.
- SAN DIEGO MUNICIPAL CODE SECTION 55.0507 ITEM (C) HYDRANT LOCATIONS SHALL BE IDENTIFIED BY THE INSTALLATION OF REFLECTIVE BLUE COLORED MARKERS. SUCH MARKERS SHALL BE AFFIXED TO THE ROADWAY SURFACE APPROXIMATELY CENTERED BETWEEN CURBS, AND AT A RIGHT ANGLE TO THE HYDRANT.
- CFC 507.5.5 CLEAR SPACE AROUND HYDRANTS - A 3 FOOT CLEAR SPACE SHALL BE MAINTAINED AROUND THE CIRCUMFERENCE OF FIRE HYDRANTS, EXCEPT AS OTHERWISE REQUIRED OR APPROVED.
- CFC507.5.6 - PHYSICAL PROTECTION - IF ADDITIONAL HYDRANTS ARE REQUIRED AND WHERE FIRE HYDRANTS ARE SUBJECT TO IMPACT BY A MOTOR VEHICLE, GUARD POSTS OR OTHER APPROVED MEANS SHALL COMPLY WITH SECTION CFC 312.
- THE REQUIRED WIDTH OF ACCESS ROADWAYS SHALL NOT BE OBSTRUCTED IN ANY MANNER, INCLUDING THE PARKING OF VEHICLES. WHERE NO SPACE IS PROVIDED FOR PARKING ALONG ACCESS ROADWAYS, THEY SHALL BE KEPT CLEAR BY THE POSTING OF SIGNS OR THE PAINTING OF CURBS.
- AN ILLUMINATED DIRECTORY, IN ACCORDANCE WITH FHPS POLICY I-00-6, SHALL BE PROVIDED. DIRECTORY SHOULD BE AT BOTH ENTRANCES TO THE SITE.
- CFC 504.3 - NEW BUILDINGS FOUR OR MORE STORIES ABOVE GRADE PLANE, EXCEPT THOSE WITH A ROOF SLOPE GREATER THAN FOUR UNITS VERTICAL IN 12 UNITS HORIZONTAL, SHALL BE PROVIDED WITH A STAIRWAY TO THE ROOF IN ACCORDANCE WITH 101.12. SUCH STAIRWAY SHALL BE MARKED AT STREET AND FLOOR LEVELS WITH A SIGN INDICATING THAT THE STAIRWAY CONTINUES TO THE ROOF.
- EXTERIOR DOORS AND OPENINGS REQUIRED BY THIS CODE OR THE CBC SHALL BE MAINTAINED READILY ACCESSIBLE FOR EMERGENCY ACCESS BY THE FIRE DEPARTMENT. AN APPROVED ACCESS WALKWAY LEADING FROM FIRE APPARATUS ACCESS ROADS TO EXTERIOR OPENINGS SHALL BE PROVIDED WHEN REQUIRED BY THE FIRE CODE OFFICIAL.
- EVERY BUILDING FOUR STORES OR MORE IN HEIGHT SHALL BE PROVIDED WITH

- NOT LESS THAN ONE STANDPIPE FOR USE DURING CONSTRUCTION INSTALLED IN ACCORDANCE WITH CFC 3313.1 STANDPIPE SHALL BE INSTALLED WHEN THE PROGRESS OF CONSTRUCTION IS NOT MORE THAN 40 FEET IN HEIGHT ABOVE THE LOWEST LEVEL OF FIRE DEPARTMENT ACCESS. CFC 3313.1
- DECORATIVE MATERIALS SHALL BE MAINTAINED IN A FLAME RETARDANT CONDITION. CFC SEC. 804
- ALL BUILDINGS AND SITES UNDERGOING CONSTRUCTION, ALTERATION, OR DEMOLITION SHALL COMPLY WITH THE REQUIREMENTS OF CHAPTER 33 OF THE CFC.
- FIRE PROTECTION EQUIPMENT SHALL BE IDENTIFIED IN AN APPROVED MANNER. ROOMS CONTAINING CONTROLS FOR A/C SYSTEMS, SPRINKLER RISERS AND VALVES, OR OTHER FIRE DETECTION, SUPPRESSION OR CONTROL ELEMENTS SHALL BE IDENTIFIED FOR THE USE OF THE FIRE DEPARTMENT. APPROVED SIGNS REQUIRED TO IDENTIFY FIRE PROTECTION EQUIPMENT AND EQUIPMENT LOCATION SHALL BE CONSTRUCTED OF DURABLE MATERIALS, PERMANENTLY INSTALLED AND READILY VISIBLE.

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LEVEL 1 (PODIUM) = 360.5
LEVEL B1 = 347.5
LEVEL B2 = 337.5



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SHEET TITLE: FIRE ACCESS PLAN

SHEET NO. ENT-C0.102 6 OF 65

PTS #: 477943

PROJECT #: 477943

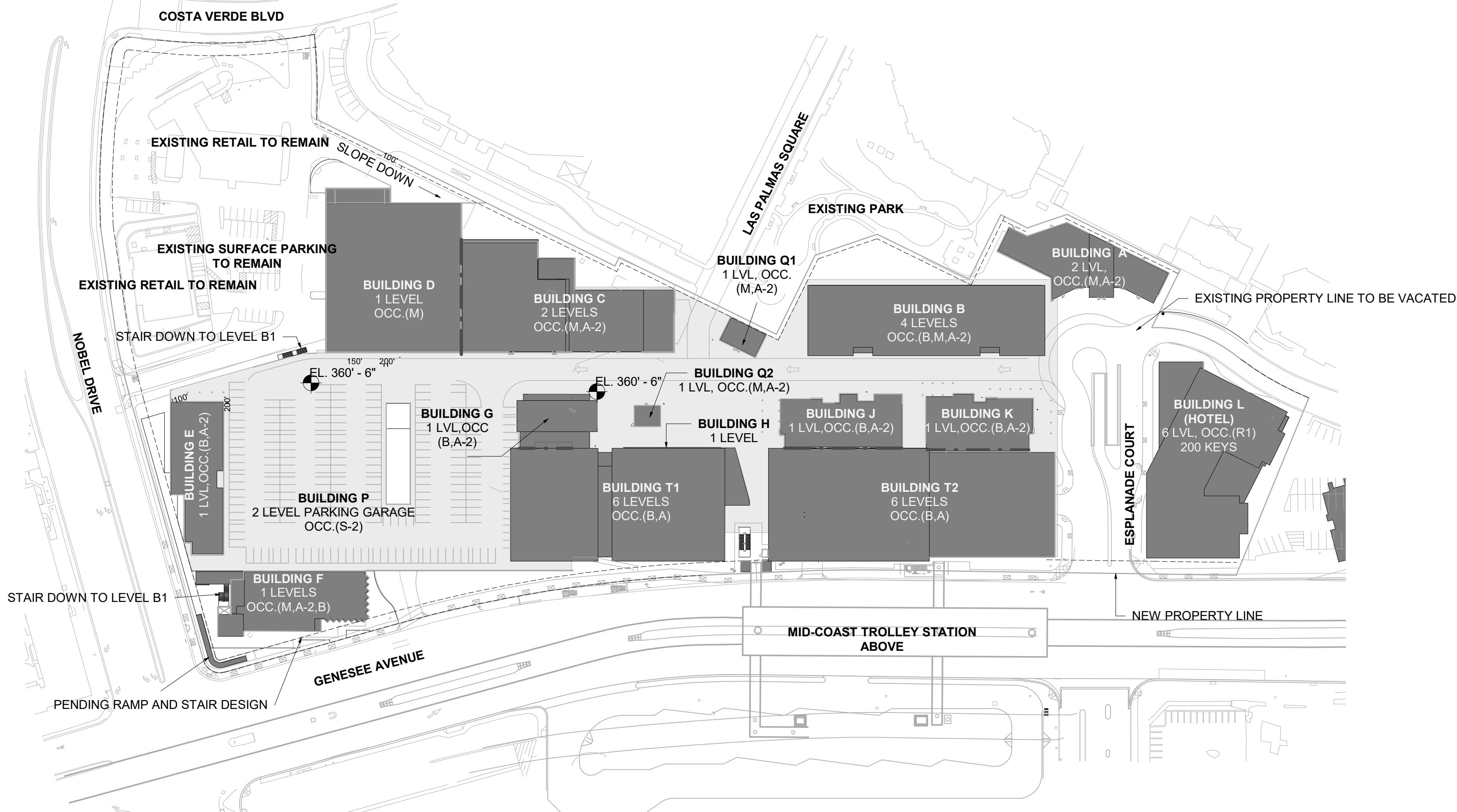
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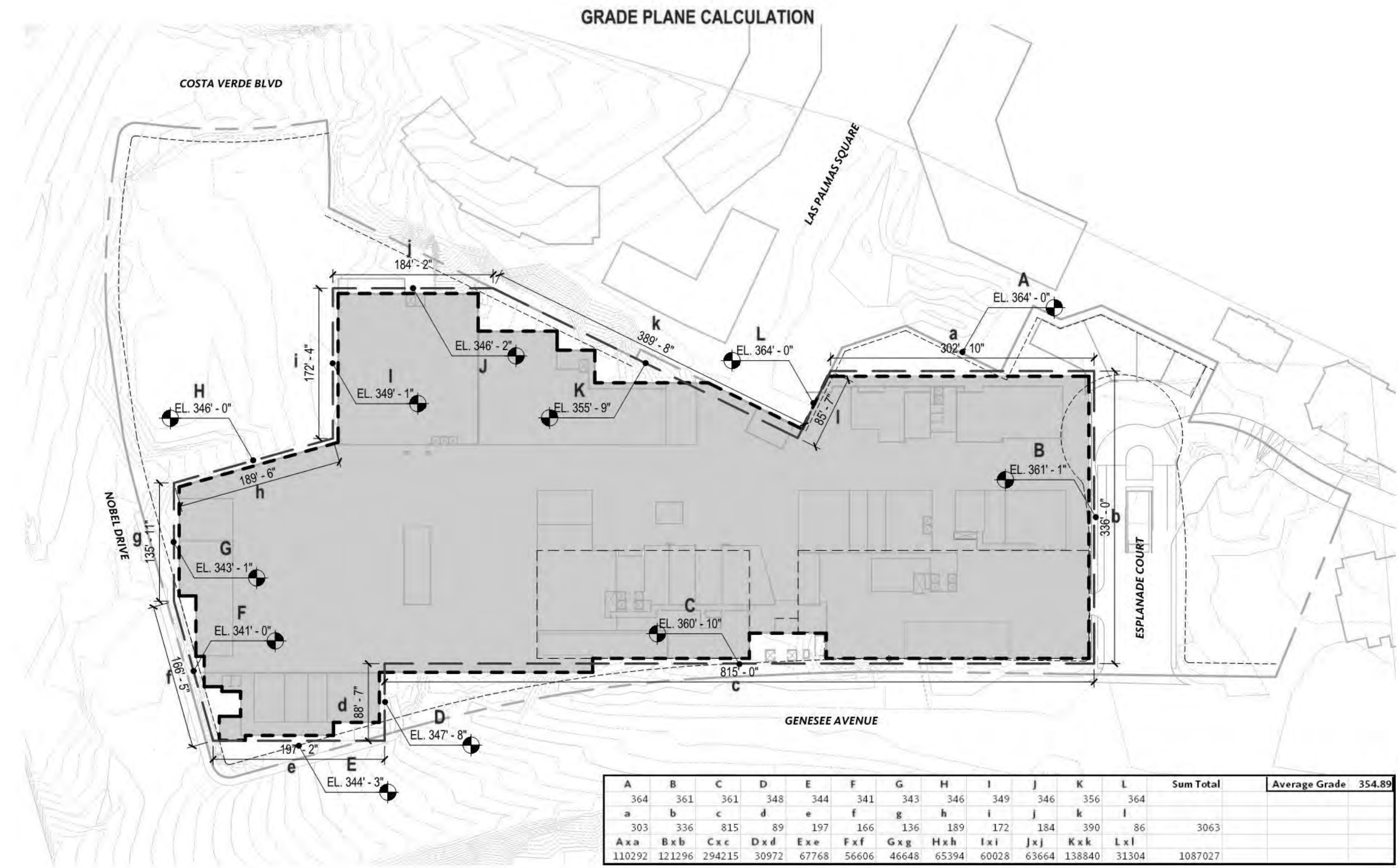
FIRE SEPARATION

FIRE SEPARATION BETWEEN occupancies	WALL RATING	WALL RATING LEGEND
BUILDING J (B) AND BUILDING T2(A)	1 HR	
BUILDING K(B) AND BUILDING T2(A)	1 HR	
BUILDING G(B) AND BUILDING T1(A)	1 HR	
BUILDING D(M) AND BUILDING G(A)	1 HR	
B1 LEVEL BUILDING D(M) AND BUILDING P(S2)	1 HR	
B1 LEVEL BUILDING F(M) AND BUILDING P(S2)	1 HR	

HORIZONTAL FIRE SEPARATION BETWEEN occupancies	
PODIUM P(S2) B1 Level LID and BUILDINGS A,B,C,D, E, F,G,H,J,K,Q1,Q2,T1,T2 FOR Occ(B,M,A)	3 HR



AVERAGE GRADE PLANE

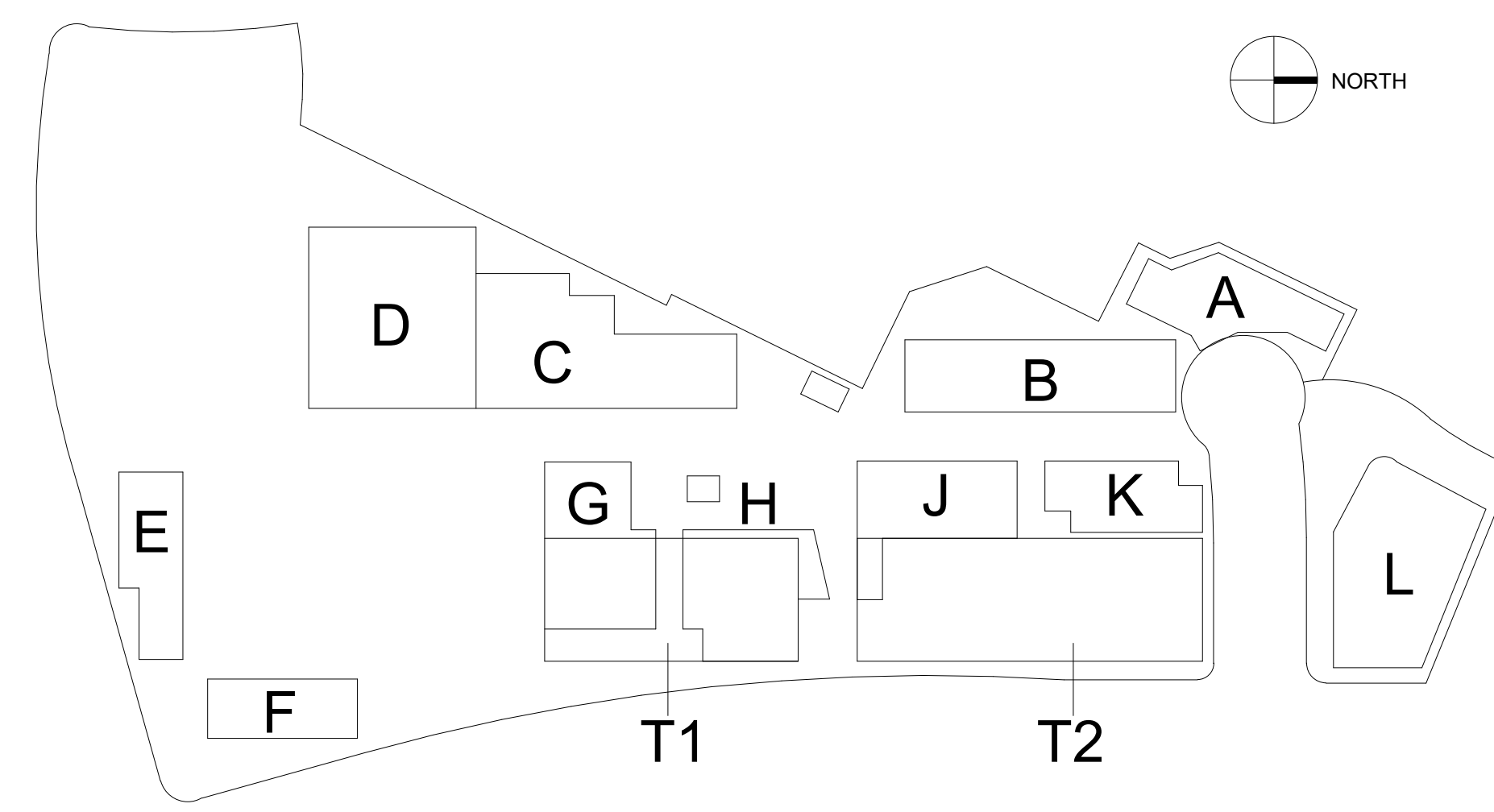


AVERAGE GRADE PLANE = 354.89'
 T.O. LID = 360.5'
 DELTA = -5.61'

CODE ANALYSIS

	BUILDING A	BUILDING B	BUILDING C	BUILDING D	BUILDING E	BUILDING F	BUILDING G	BUILDING H	BUILDING J	BUILDING K	BUILDING L	BUILDING P	BUILDING T1	BUILDING T2	BUILDING Q1	BUILDING Q2	
OCCUPANCY	M.A-2	B.M.A-2	M.A-2	M	B.A-2	M.A-2,B	B.A-2	M.A-2	B.A-2	B.A-2	R1	(2 LEVELS BELOW GRADE parking similar to HOTEL)	M.B.S2	M.B.A	B.A	M.A-2	M.A-2
CONSTRUCTION TYPE	TYPE V-A - '16 CBC W/ SPRINKLERS	TYPE IV - '16 CBC W/ SPRINKLERS	TYPE V-A - '16 CBC W/ SPRINKLERS	TYPE V-A - '16 CBC W/ SPRINKLERS	TYPE V-B - '16 CBC W/ SPRINKLERS	TYPE V-B - '16 CBC W/ SPRINKLERS	TYPE V-B - '16 CBC W/ SPRINKLERS	TYPE V-B - '16 CBC W/ SPRINKLERS	TYPE V-B - '16 CBC W/ SPRINKLERS	TYPE V-B - '16 CBC W/ SPRINKLERS	TYPE 1A - '16 CBC W/ SPRINKLERS	TYPE 1A - '16 CBC W/ SPRINKLERS	TYPE IB - '16 CBC W/ SPRINKLERS	TYPE IB - '16 CBC W/ SPRINKLERS	TYPE V-B - '16 CBC W/ SPRINKLERS	TYPE V-B - '16 CBC W/ SPRINKLERS	
BUILDING HEIGHT CBC 504.3																	
S (without area increase)	70'	85'	70'	70'	60'	60'	60'	60'	60'	60'	UL	UL	180'	180'	60'	60'	
S (with area increase)	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	
Proposed Height	32'	54'	32'	25'	27'	45'	24'	24'	24'	24'	150'	106'	106'	17'	17'		
Average grade plane - podium	-	354.89' (-5.61')	354.89' (-5.61')	354.89' (-5.61')	354.89' (-5.61')	354.89' (-5.61')	354.89' (-5.61')	354.89' (-5.61')	354.89' (-5.61')	354.89' (-5.61')	360.5'	354.89' (-5.61')	354.89' (-5.61')	354.89' (-5.61')	354.89' (-5.61')	354.89' (-5.61')	
Height from grade plane	-	74.61'	37.61'	30.61'	32.61'	50.61'	29.61'	29.61'	29.61'	29.61'	TBD	-23.00'	111.61'	111.61'	22.61'	22.61'	
NUMBER OF STORIES CBC 504.4																	
S (without area increase)	3	4	3	3	2	2	2	2	2	2	UL	UL	12	12	2	2	
S (with area increase)	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	
Proposed stories	2	4	2	1	1	1	2	1	1	1	6	-2	6	6	1	1	
ALLOWABLE FLOOR AREA CBC 506.2																	
S1	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	
SM	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	
SM (without height increase)	34,500	45,000	34,500	34,500	18,000	18,000	18,000	18,000	18,000	18,000	UL	UL	UL	UL	18,000	18,000	
SM (with height increase)	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	
Proposed Area (largest floor)	9,500	21,530	28,290	27,000	9,950	8,440	16,580	9,660	12,070	8,730	24,090	376,934	24,269	41,176	1,450	800	
Allowable Area per Floor	34,500	45,000	34,500	34,500	18,000	18,000	18,000	18,000	18,000	18,000	UL	UL	UL	UL	18,000	18,000	
Allowable Building Area CBC 506.2.3 or 506.2.4	103,500	135,000	103,500	103,500	36,000	36,000	36,000	36,000	36,000	36,000	UL	UL	UL	UL	36,000	36,000	
Total Proposed Building Area	11,240	71,835	28,290	27,000	9,950	8,440	20,290	9,660	12,070	8,730	125,000	714,408	131,345	210,000	1,450	800	

Note: All areas in terms of Gross Floor Area (GFA) not Gross Leasable Area (GLA) per CBC, Chp. 5 General building heights and Areas.



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ORIGINAL DATE: 3/15/2016 (BY CALLISONRTKL)
REVISION DATE: 7/28/2016 (BY CALLISONRTKL)
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REVISION DATE: 10/11/2019 (BY RDC)

SHEET TITLE:
 PROJECT STATISTICS

SHEET NO: ENT- G0.101 8 OF 65

PTS #: 477943
PROJECT #: 477943

Regency Centers. REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187

Proposed Location	CAL GREEN											Total (a+b+c+h+j+k)		
	Standard (a)	Tandem (b)	Motorcycle (not incl. in total count)	Bicycle Long Term (not incl. in total count)	Bicycle Short Term (not incl. in total count)	Micro-Mobility Parking (not incl. in total count)	E.V. Capable (c=d+e+f+g)	E.V. Capable Standard (d)	E.V. Acc. (e)	E.V. Van Acc. (f)	E.V. Amb. (g)		Low Emitting, Fuel Vehicle, Carpool (h)	Acc. (j)
Required %			2%	5%	5%		6%					7%		
Required Spaces			42	99	5		125		4	2	4	146	25	6
Plaza Existing Surface Lot	55	-	-	-	5	10	-	-	-	-	-	-	1	2
Level 01	200	24	-	-	15	30	28	28	-	-	-	-	6	2
B1 Level below grade	510	23	41	99	-	+	70	60	4	2	4	72	17	2
B2 level below grade	766	128	-	-	-	-	27	27	-	-	-	72	-	-
Building L (Hotel 200 Keys)	51	11	2	-	-	-	4	2	1	1	-	2	2	1
Total Parking Spaces Provided	1582	186	43	99	20	40	129	117	5	3	4	146	26	7

- Note:
- Bicycle Parking to be provided in accordance with Section 142.0530 (e).
 - Determined for Motorcycle, EV, LEV/carpool, Accessible Stalls per CBC 11B-502
 - In accordance with the Specific Plan, the total number of parking spaces shall range between 1,837-2,076. The maximum of the range excludes the off-site spaces that are the subject of an easement agreement for parking per Document 90-537240, recorded October 2, 1990.
 - Provide 11 shower stalls and 38 two-tier personal effects lockers per San Diego Climate Action Plan requirements

Minimum Required Parking Article 2: General Development Regulations Division 5: Parking Regulations				Total Spaces Provided
Land Use	Sq. Ft. / Rooms	Ratio (Per 1,000 sq. ft.)	Total Req'd Spaces	
COMMERCIAL / RETAIL	178,000 sq. ft.	4.3	765	
R&D	360,000 sq. ft.	2.1	756	
OFFICE	40,000 sq. ft.	2.9	116	
HOTEL	200 rooms	1 per room	200	
TOTAL	--	--	1,837	2,076

Note:
In accordance with the Specific Plan, the total number of parking spaces shall range between 1,837-2,076. The maximum of the range excludes the off-site spaces that are the subject of an easement agreement for parking per Document 90-537240, recorded October 2, 1990.

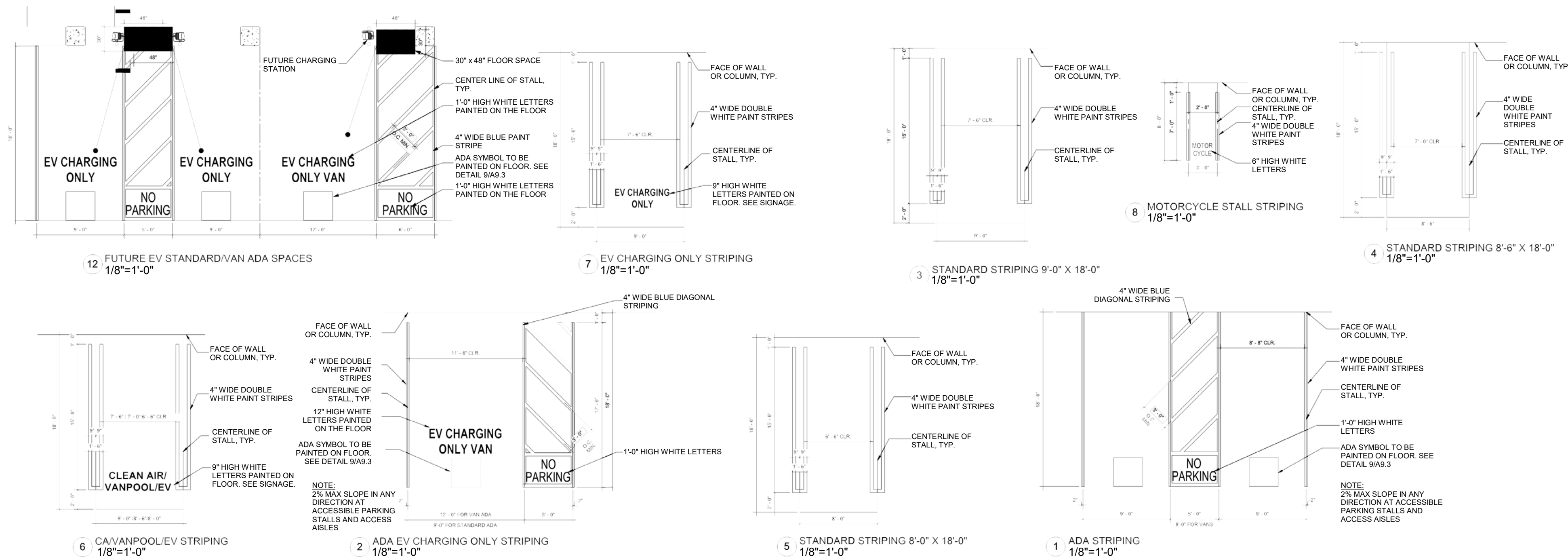
LOADING ZONE SPACES	Length	Width	Vertical Clearance	# of Off-Street Loading Spaces
T2 LOADING	67'-8"	38'-10"	15'-0"	5
RETAIL LOADING	115'-6"	209'-0"	22'-0"	7

BUILDING AREA SUMMARY	LEVEL B2	LEVEL B1	LEVEL 1	LEVEL 2	LEVEL 3	LEVEL 4	LEVEL 5	LEVEL 6	TOTAL
RETAIL AREA SUMMARY									
EXISTING			8,730						8,730
BUILDING A			9,500	1,740					11,240
BUILDING B			13,180						13,180
BUILDING C			24,440	3,850					28,290
BUILDING D		8,700	27,000						35,700
BUILDING E			9,950						9,950
BUILDING F		9,470	8,440						17,910
BUILDING G			16,580	3,710					20,290
BUILDING H			9,660						9,660
BUILDING J			12,070						12,070
BUILDING K			8,730						8,730
BUILDING Q1			1,450						1,450
BUILDING Q2			800						800
RETAIL GRAND TOTAL									178,000

OFFICE / R&D AREA SUMMARY	LEVEL B2	LEVEL B1	LEVEL 1	LEVEL 2	LEVEL 3	LEVEL 4	LEVEL 5	LEVEL 6	TOTAL
BUILDING B (OFFICE/R&D)	450	450	1,790	18,655	18,655	18,655			58,655
BUILDING T1 (R&D)	650	650	10,360	24,269	24,269	24,269	23,439	23,439	131,345
BUILDING T2 (R&D)	650	650	2,824	41,176	41,175	41,175	41,175	41,175	210,000
OFFICE GRAND TOTAL									400,000

HOTEL AREA SUMMARY	LEVEL B2	LEVEL B1	LEVEL 1	LEVEL 2	LEVEL 3	LEVEL 4	LEVEL 5	LEVEL 6	TOTAL
BUILDING L			24,214	24,708	19,188	18,964	18,963	18,963	125,000
HOTEL GRAND TOTAL									125,000

PROJECT TOTAL	LEVEL B2	LEVEL B1	LEVEL 1	LEVEL 2	LEVEL 3	LEVEL 4	LEVEL 5	LEVEL 6	TOTAL
									703,000



PREPARED BY: **rdc.**
 NAME: 245 EAST THIRD STREET
 ADDRESS: LONG BEACH, CA 90802
 PHONE: 562.628.8000

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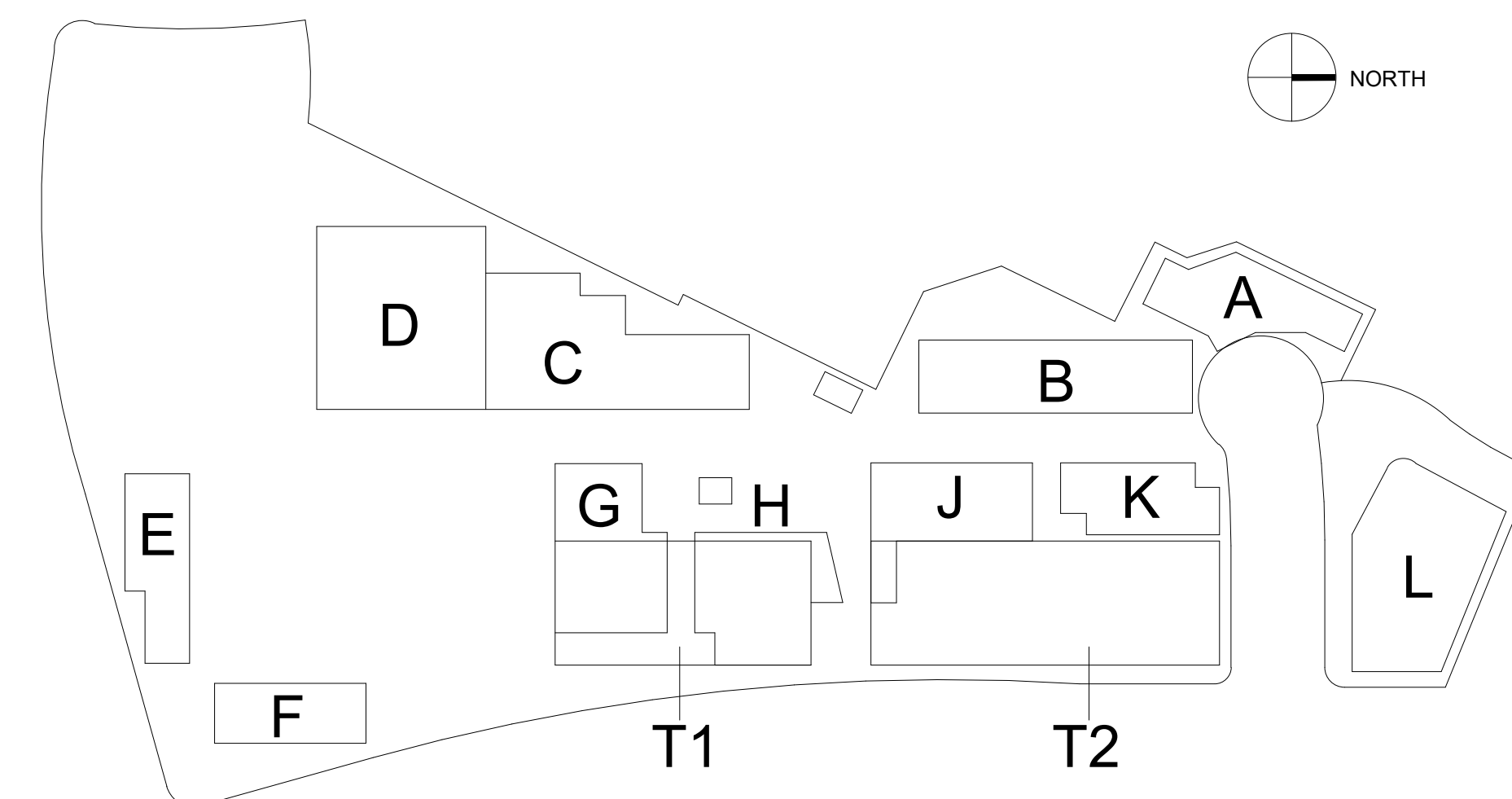
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 PROJECT STATISTICS

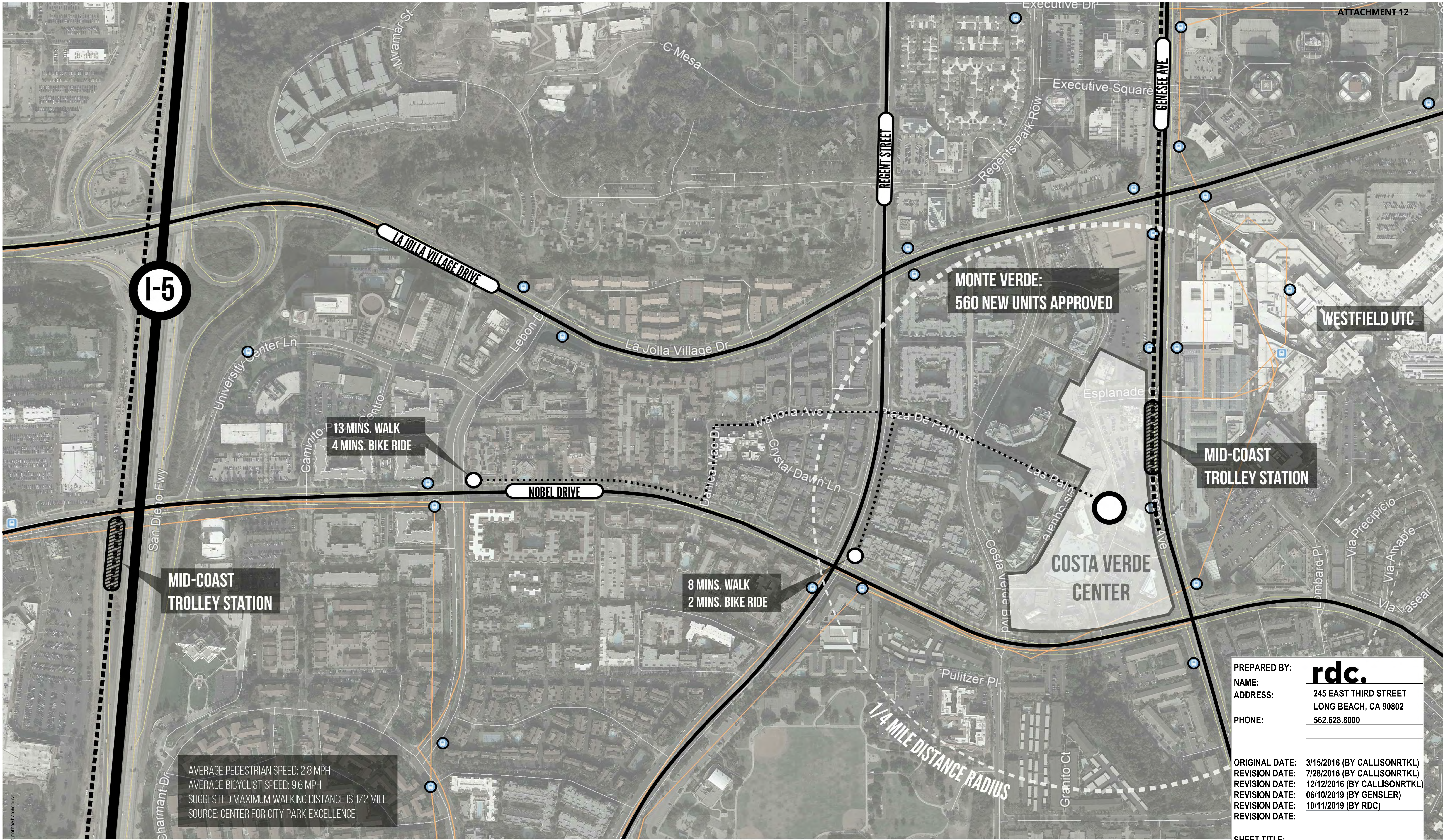
SHEET NO: **ENT-G0.102** 9 OF 65

PTS #: 477943
 PROJECT #: 477943

Regency Centers. REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187





AVERAGE PEDESTRIAN SPEED: 2.8 MPH
 AVERAGE BICYCLIST SPEED: 9.6 MPH
 SUGGESTED MAXIMUM WALKING DISTANCE IS 1/2 MILE
 SOURCE: CENTER FOR CITY PARK EXCELLENCE

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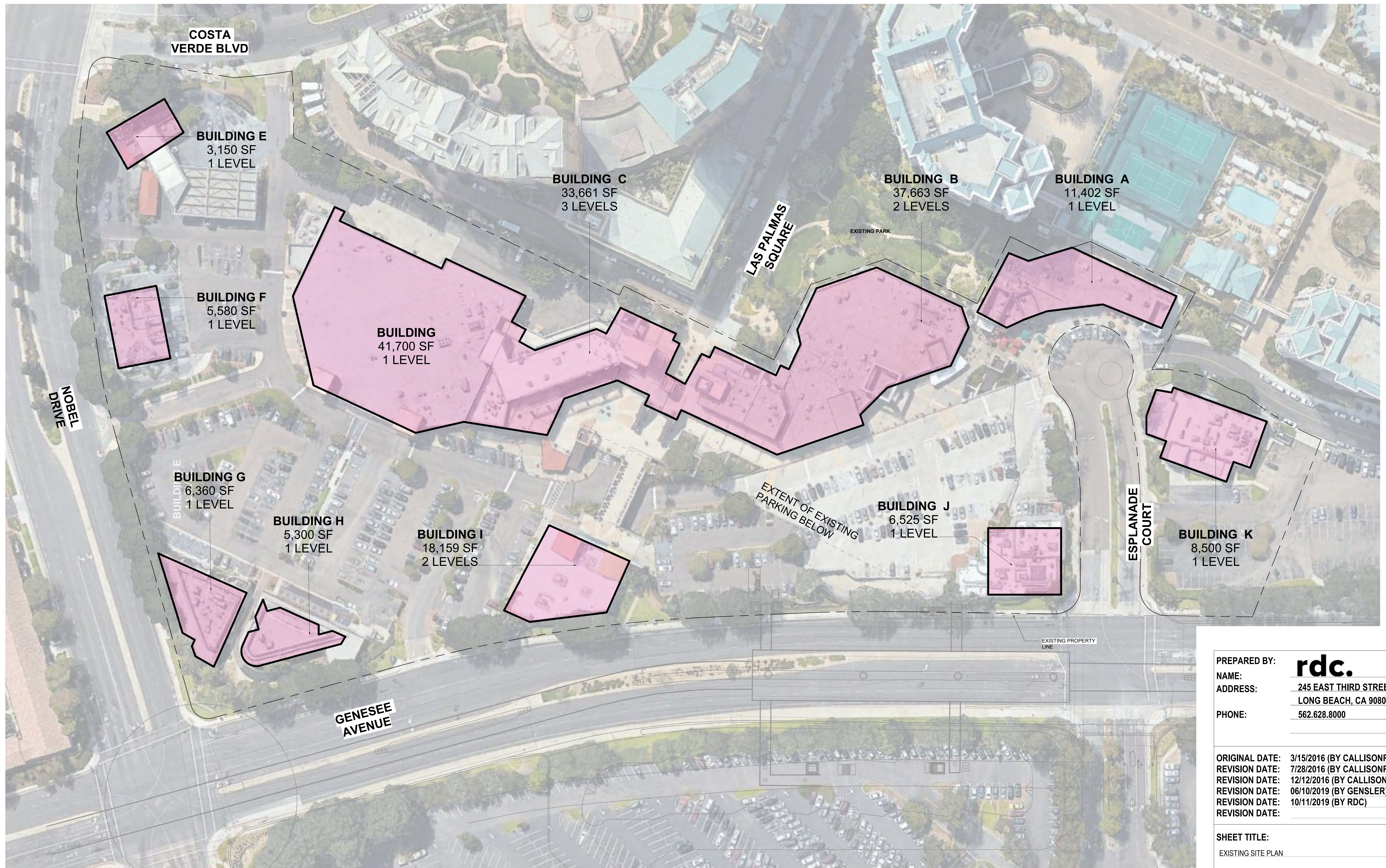
SHEET TITLE:
 VICINITY AND SERVICE MAP

SHEET NO: **ENT- G0.103** 10 OF 65

PTS #: 477943
 PROJECT #: 477943

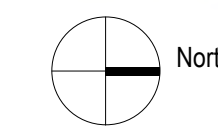
Regency Centers REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187



1 EXISTING SITE PLAN

SCALE: 1" = 40'-0"



PREPARED BY: **rdc.**
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 ADDRESS: LONG BEACH, CA 90802
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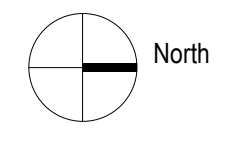
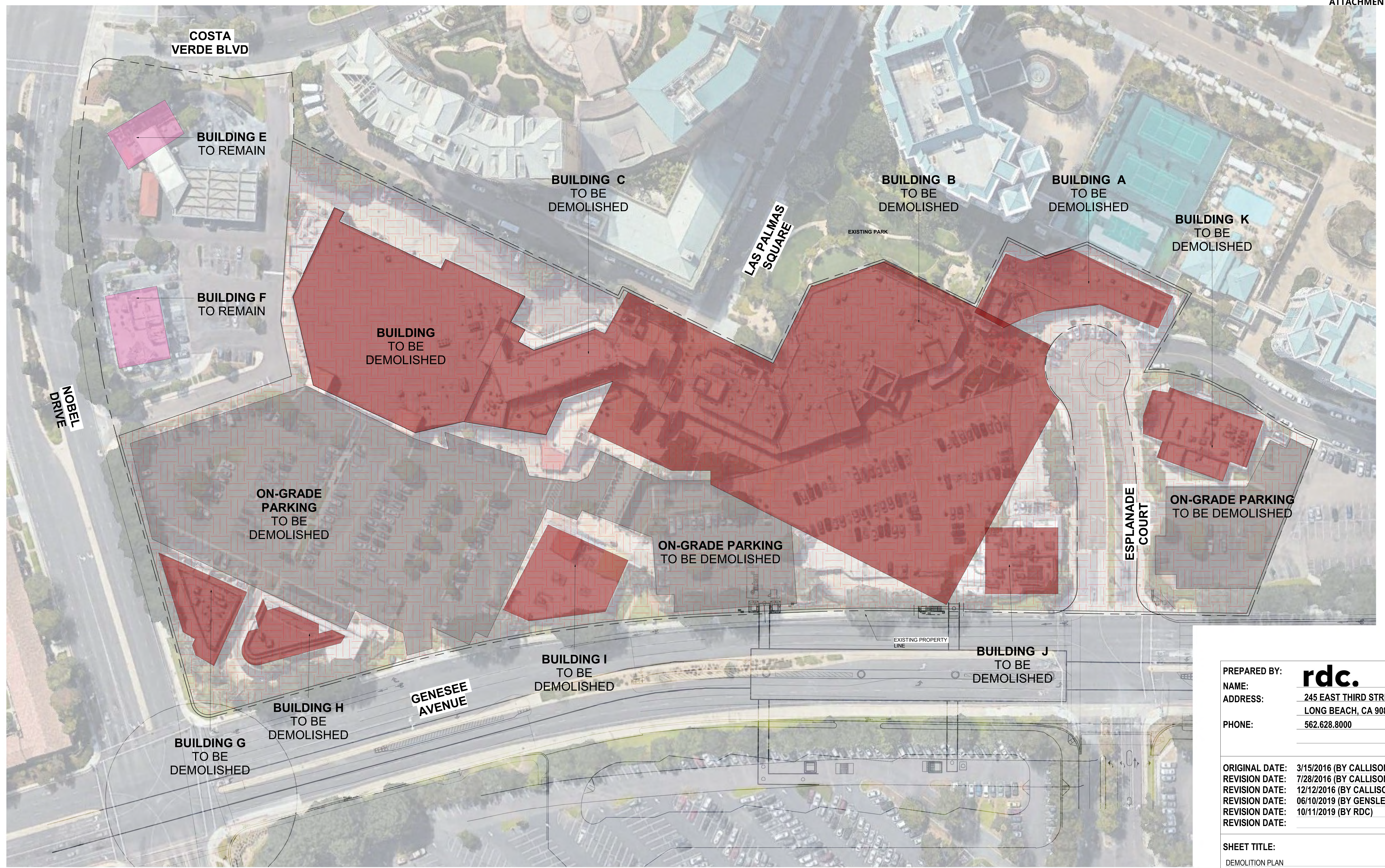
ORIGINAL DATE: 3/15/2016 (BY CALLISONRTKL)
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 REVISION DATE: 06/10/2019 (BY GENSLER)
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SHEET TITLE:
 EXISTING SITE PLAN

SHEET NO: **ENT1.000** 11 OF 65

PTS #: 477943
 PROJECT #: 477943

Regency Centers REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075



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SHEET TITLE:
 DEMOLITION PLAN

SHEET NO: **ENT1.001** 12 OF 65

PTS #: 477943
 PROJECT #: 477943

Regency Centers. REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187

1 DEMOLITION PLAN
 SCALE: 1" = 40'-0"



1 OVERALL SITE PLAN

SCALE: 1" = 40'-0"
 NOTE:
 1. FOR LOCATIONS OF EXISTING TRAFFIC SIGNALS, STREET LIGHTS, AND OTHER IMPROVEMENTS TO BE REMOVED OR RELOCATED ALONG GENESSEE AVENUE & NOBEL DRIVE, STRIPING AT THE PROPOSED DRIVEWAYS, PROPOSED RIGHT-TURN EXIT ONLY DRIVEWAY & LOCATION OF PROPOSED VALET DROP OFF

PREPARED BY: **rdc.**
 NAME: 245 EAST THIRD STREET
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 REVISION DATE: 09/01/2020 (BY RDC)

SHEET TITLE:
 SITE PLAN

SHEET NO: **ENT1.100** 13 OF 65

PTS #: 477943
 PROJECT #: 477943

Regency Centers. REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187

PLAN LEGEND

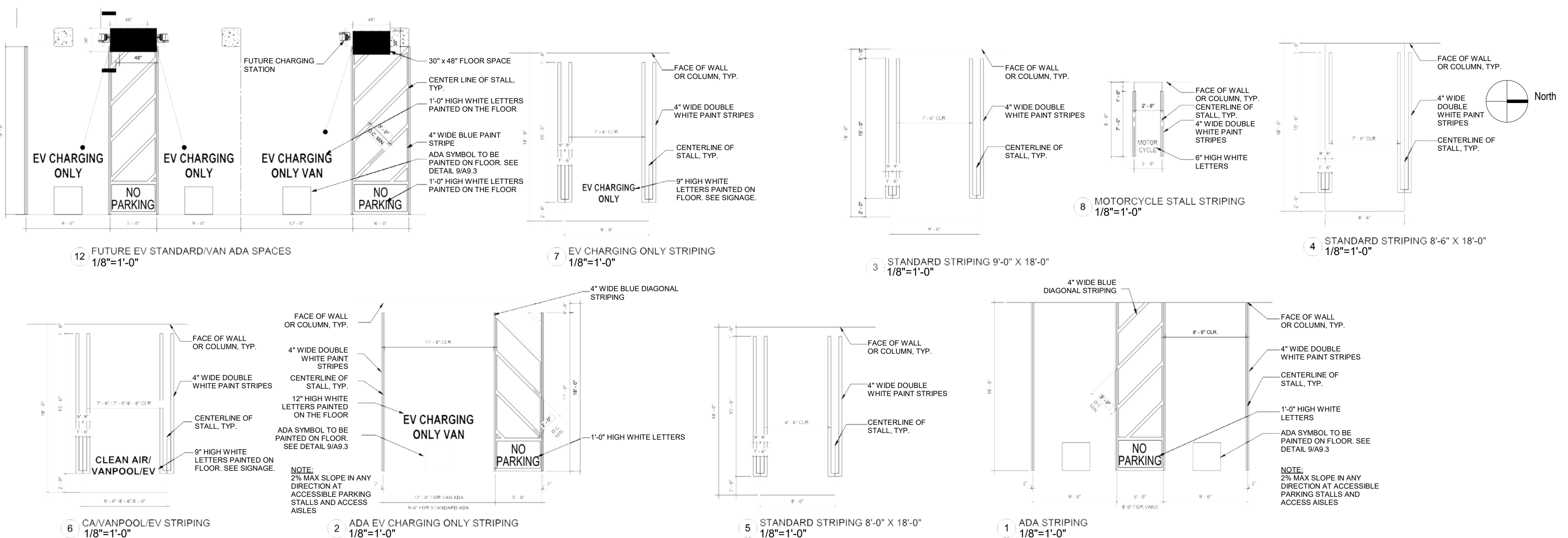
- Office
- Retail Circulation

XXXX TOTAL PARKING COUNT
 (See Sheet 9 G0.102, Parking Allocation Summary for required parking space allocation requirements per CBC 11B-502 Final allocation of parking space locations will be determined at time of Building Permit Submittal. Also see the Parking Details on G01.102 for typical parking space dimensions).

E Elevator



B2 OVERALL PLAN - LEVEL B2
 SCALE: 1" = 40'-0"



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SHEET TITLE:
 LEVEL B2 PLAN

SHEET NO: ENT1.200 14 OF 65

PTS #: 477943
PROJECT #: 477943

Regency Centers. REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187

COSTA VERDE BLVD



TURN AROUND SPACE 9'-6" X 18'-0" PER SANDIEGO MUNICIPAL CODE SECTION 142.0560 (D) (3). THE MINIMUM REQUIRED AREA IS 8'-0" X 18'-0"

PLAN LEGEND

- Retail
- Office
- Retail Circulation
- Retail Service

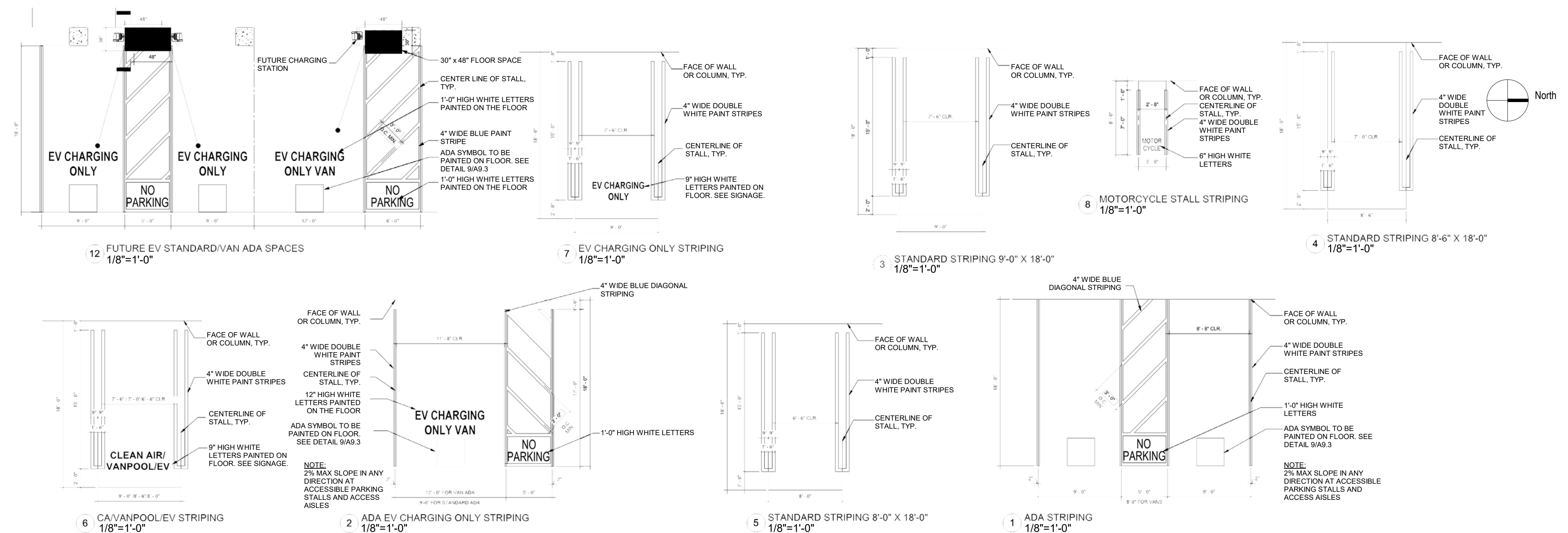
XXXX TOTAL PARKING COUNT
 (See Sheet 9 G0.102, Parking Allocation Summary for required parking space allocation requirements per CBC 11B-502 Final allocation of parking space locations will be determined at time of Building Permit Submittal. Also see the Parking Details on G01.102 for typical parking space dimensions).

E Elevator

NOBEL DRIVE

GENESEE AVENUE

B1 OVERALL PLAN - LEVEL B1
 SCALE: 1" = 40'-0"



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 REVISION DATE: 09/01/2020 (BY RDC)

SHEET TITLE:
 LEVEL B1 PLAN

SHEET NO: **ENT1.201** 15 OF 65

PTS #: 477943
 PROJECT #: 477943

Regency Centers. REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187

COSTA VERDE BLVD

PLAN LEGEND

- Retail
- Office
- Hotel
- Service
- Retail Circulation

XXXX TOTAL PARKING COUNT
 (See Sheet 9 G0.102, Parking Allocation Summary for required parking space allocation requirements per CBC 11B-502 Final allocation of parking space locations will be determined at time of Building Permit Submittal. Also see the Parking Details on G01.102 for typical parking space dimensions).



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 REVISION DATE: 09/01/2020 (BY RDC)

SHEET TITLE:
 LEVEL 1 PLAN

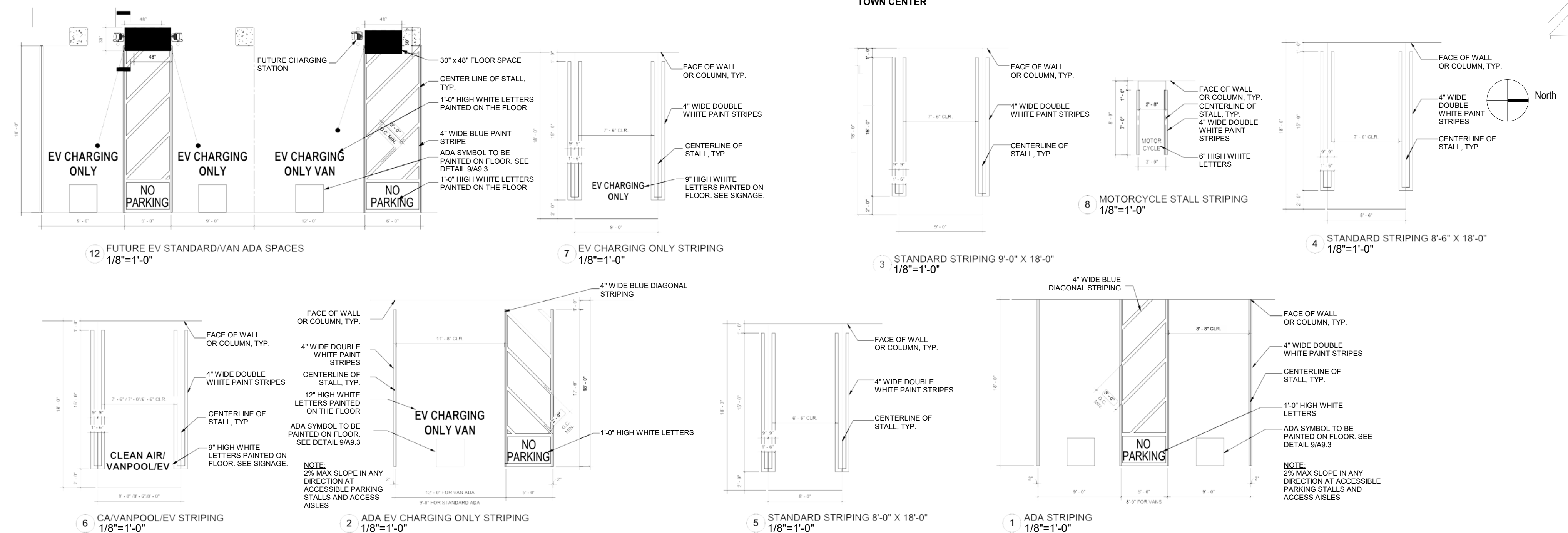
SHEET NO: **ENT1.202** 16 OF 65

PTS #: 477943
 PROJECT #: 477943

Regency Centers. REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187

1 OVERALL PLAN - LEVEL 1
 SCALE: 1" = 40'-0"



COSTA VERDE BLVD

PLAN LEGEND

- Retail
- Office
- Hotel
- Office Exterior Patio







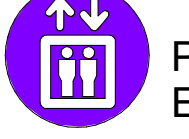
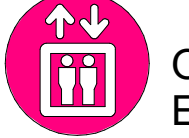



2 OVERALL PLAN - LEVEL 2
SCALE: 1" = 40'-0"

PREPARED BY:	rdc.
NAME:	245 EAST THIRD STREET
ADDRESS:	LONG BEACH, CA 90802
PHONE:	562.628.8000
ORIGINAL DATE:	3/15/2016 (BY CALLISONRTKL)
REVISION DATE:	7/28/2016 (BY CALLISONRTKL)
REVISION DATE:	12/12/2016 (BY CALLISONRTKL)
REVISION DATE:	06/10/2019 (BY GENSLER)
REVISION DATE:	10/11/2019 (BY RDC)
SHEET TITLE:	LEVEL 2 PLAN
SHEET NO:	ENT1.203 17 OF 65
PTS #:	477943
PROJECT #:	477943
Regency Centers.	REGENCY CENTERS 420 STEVENS AVE SUITE 320 SOLANA BEACH, CA 92075
PROJECT NUMBER:	19-187

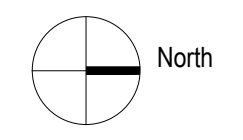
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COSTA VERDE BLVD

-  ACCESSIBILITY CIRCULATION
-  ACCESSIBILITY CROSSWALK
-  TURN AROUND SPACE 9'-6" x 18'-0" PER SANDIEGO MUNICIPAL CODE SECTION 142.0560 (D) (3). THE MINIMUM REQUIRED AREA IS 8'-0" X 18'-0"
-  PEDESTRIAN CIRCULATION
-  BUS STOP
-  GENERAL ACCESSIBLE PARKING LOCATIONS
-  PUBLIC ELEVATOR
-  OFFICE ELEVATOR
-  PUBLIC STAIRS

PLAN LEGEND

-  Retail
-  Office
-  Retail Circulation
-  Retail Service



B1 ACCESSIBILITY CIRCULATION MAP - LEVEL B1

SCALE: 1" = 40'-0"

PREPARED BY: **rdc.**
 NAME: 245 EAST THIRD STREET
 ADDRESS: LONG BEACH, CA 90802
 PHONE: 562.628.8000

ORIGINAL DATE: 3/15/2016 (BY CALLISONRTKL)
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 REVISION DATE: 06/10/2019 (BY GENSLER)
 REVISION DATE: 10/11/2019 (BY RDC)
 REVISION DATE: 09/01/2020 (BY RDC)

SHEET TITLE:
 CIRCULATION MAP - ACCESSIBILITY - LEVEL B1

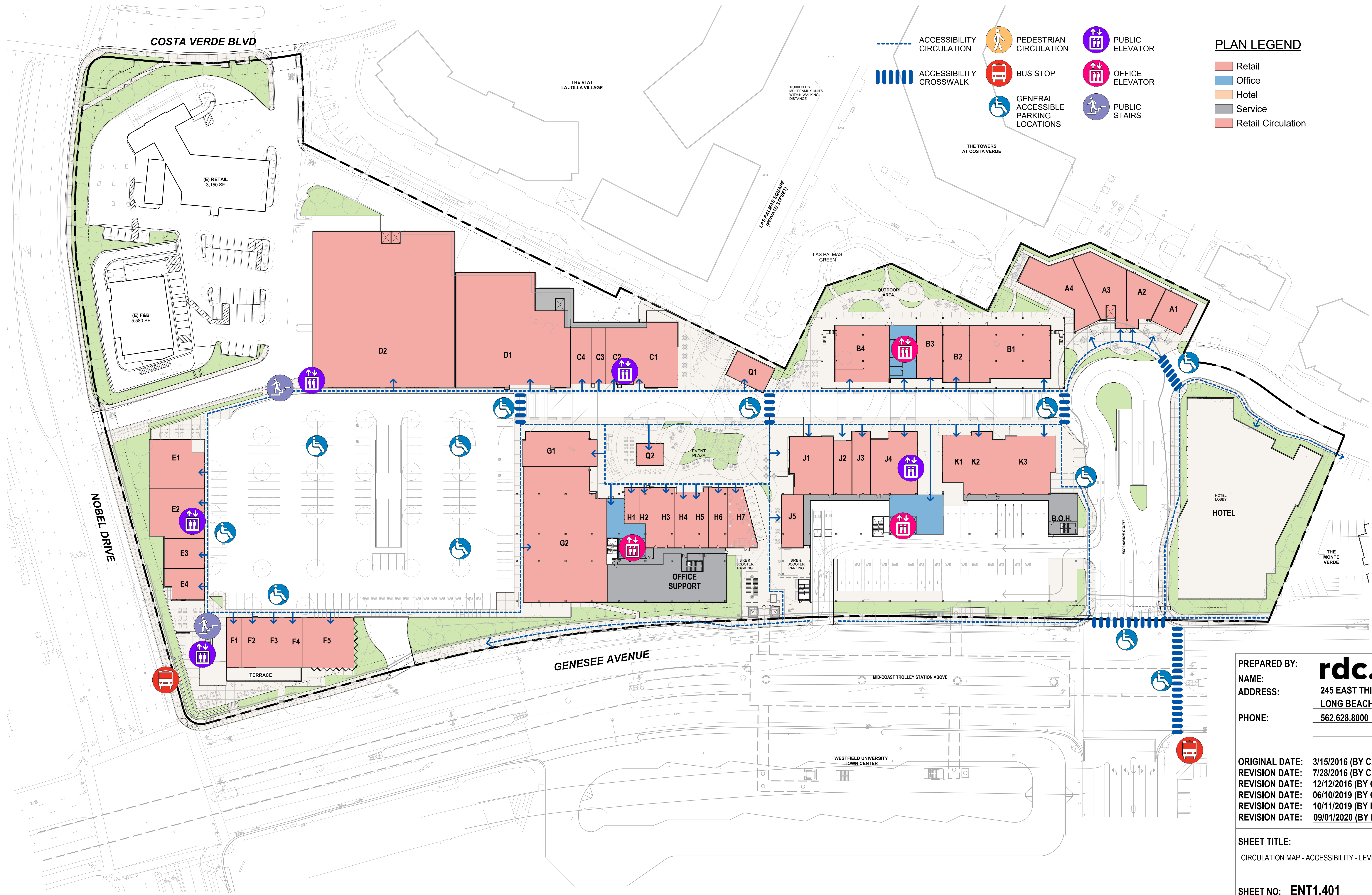
SHEET NO: **ENT1.400** 18 OF 65

PTS #: 477943
 PROJECT #: 477943

Regency Centers. REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187

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- ACCESSIBILITY CIRCULATION
- ACCESSIBILITY CROSSWALK
- PEDESTRIAN CIRCULATION
- BUS STOP
- GENERAL ACCESSIBLE PARKING LOCATIONS
- PUBLIC ELEVATOR
- OFFICE ELEVATOR
- PUBLIC STAIRS

- PLAN LEGEND**
- Retail
 - Office
 - Hotel
 - Service
 - Retail Circulation

1 ACCESSIBILITY CIRCULATION MAP LEVEL 1
SCALE: 1" = 40'-0"

PREPARED BY: **rdc.**
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SHEET TITLE:
 CIRCULATION MAP - ACCESSIBILITY - LEVEL 1

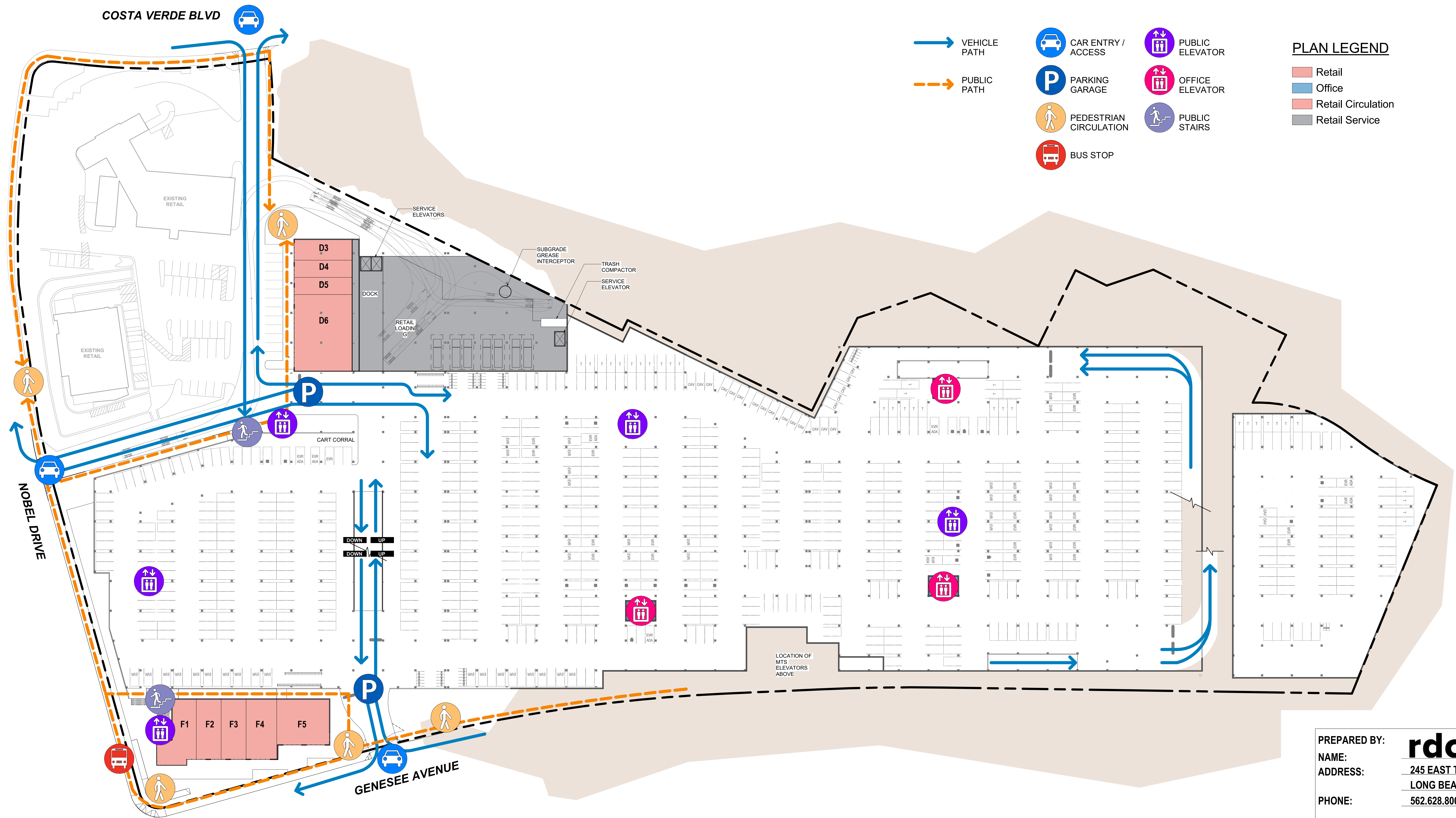
SHEET NO: **ENT1.401** 19 OF 65

PTS #: 477943
 PROJECT #: 477943

Regency Centers REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187

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- VEHICLE PATH
- PUBLIC PATH
- CAR ENTRY / ACCESS
- PARKING GARAGE
- PEDESTRIAN CIRCULATION
- BUS STOP
- PUBLIC ELEVATOR
- OFFICE ELEVATOR
- PUBLIC STAIRS

PLAN LEGEND

- Retail
- Office
- Retail Circulation
- Retail Service

B1 PUBLIC CIRCULATION MAP LEVEL B1
SCALE: 1" = 40'-0"

PREPARED BY: **rdc.**
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 REVISION DATE: 09/01/2020 (BY RDC)

SHEET TITLE:
CIRCULATION MAP - PUBLIC - LEVEL B1

SHEET NO: **ENT1.403** 20 OF 65

PTS #: 477943
 PROJECT #: 477943

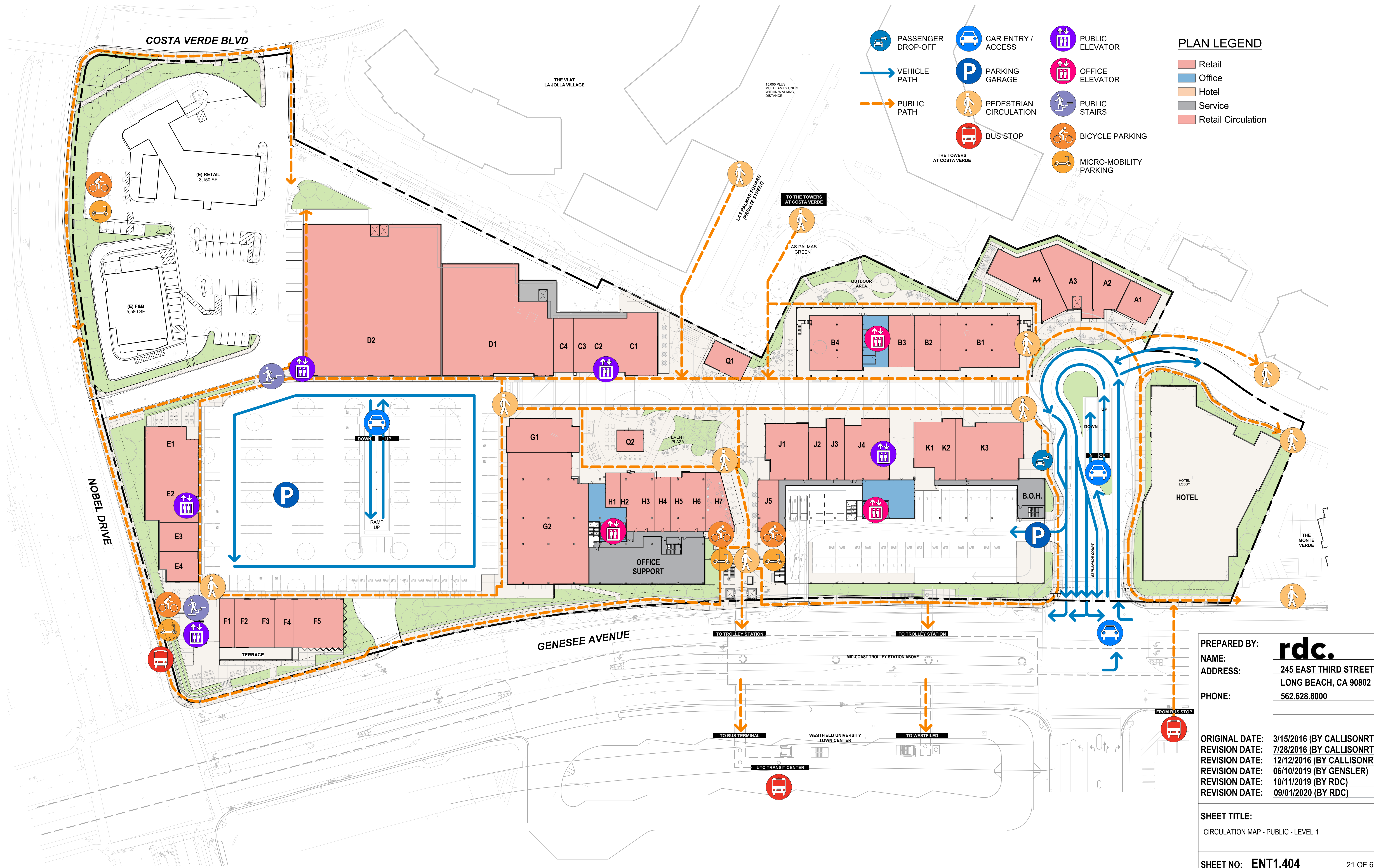
Regency Centers. REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187

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- PASSENGER DROP-OFF
- CAR ENTRY / ACCESS
- PUBLIC ELEVATOR
- VEHICLE PATH
- PARKING GARAGE
- OFFICE ELEVATOR
- PUBLIC PATH
- PEDESTRIAN CIRCULATION
- PUBLIC STAIRS
- BUS STOP
- BICYCLE PARKING
- MICRO-MOBILITY PARKING

- PLAN LEGEND**
- Retail
 - Office
 - Hotel
 - Service
 - Retail Circulation



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SHEET TITLE:
 CIRCULATION MAP - PUBLIC - LEVEL 1

SHEET NO: **ENT1.404** 21 OF 65

PTS #: 477943
 PROJECT #: 477943

Regency Centers. REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187

1 PUBLIC CIRCULATION MAP LEVEL 1
 SCALE: 1" = 40'-0"

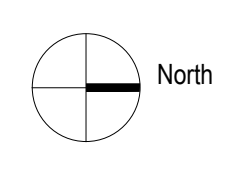
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COSTA VERDE BLVD



PLAN LEGEND

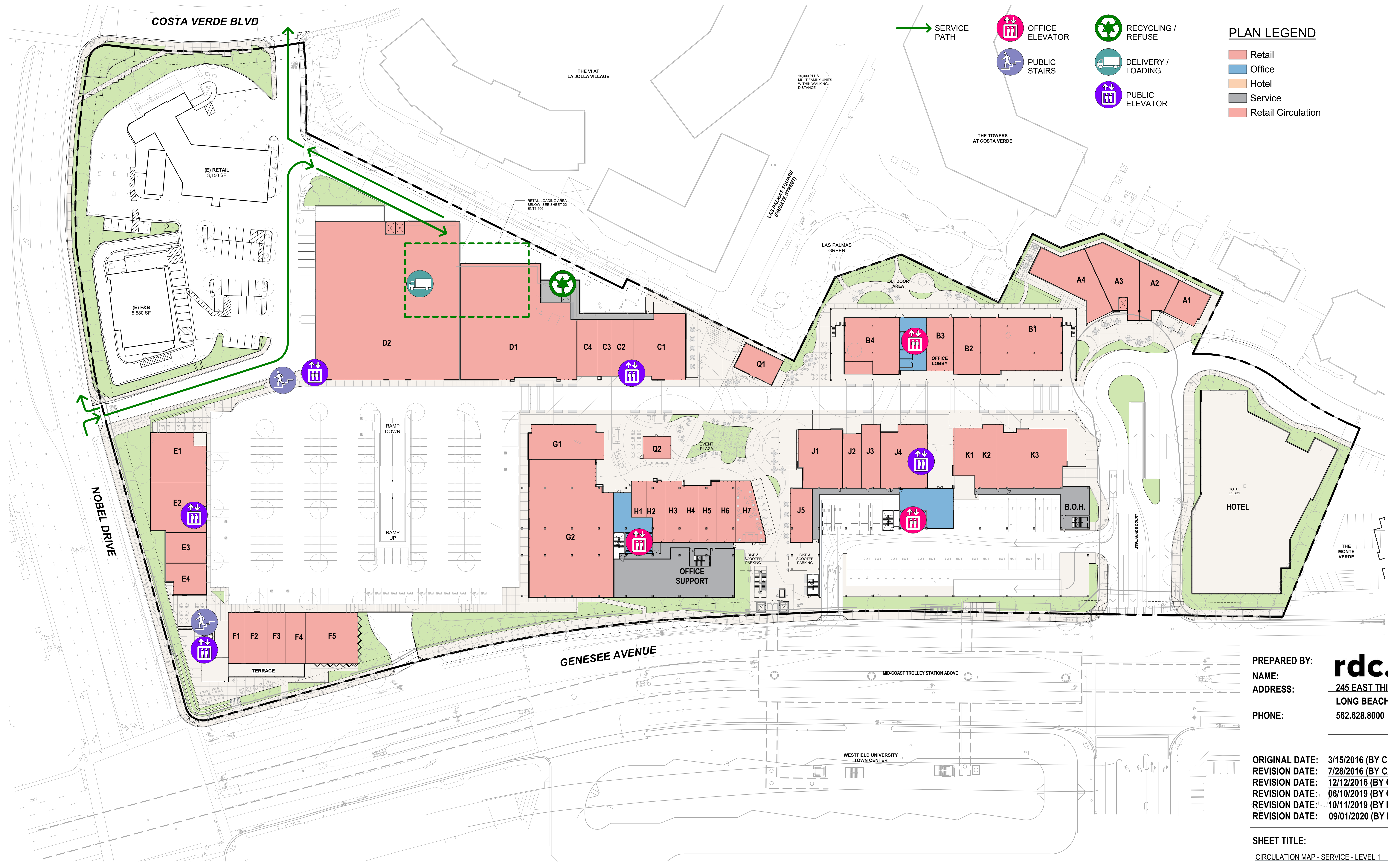
- Retail
- Office
- Retail Circulation
- Retail Service



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PHONE:	562.628.8000
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REVISION DATE:	09/01/2020 (BY RDC)
SHEET TITLE:	CIRCULATION MAP - SERVICE - LEVEL B1
SHEET NO:	ENT1.405 22 OF 65
PTS #:	477943
PROJECT #:	477943
Regency Centers.	REGENCY CENTERS 420 STEVENS AVE SUITE 320 SOLANA BEACH, CA 92075

B1 SERVICE CIRCULATION MAP LEVEL B1
SCALE: 1" = 40'-0"

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SERVICE PATH
 OFFICE ELEVATOR
 PUBLIC STAIRS
 RECYCLING / REFUSE
 DELIVERY / LOADING
 PUBLIC ELEVATOR

PLAN LEGEND

- Retail
- Office
- Hotel
- Service
- Retail Circulation

1 SERVICE CIRCULATION MAP - LEVEL 1
SCALE: 1" = 40'-0"

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REVISION DATE: 10/11/2019 (BY RDC)
REVISION DATE: 09/01/2020 (BY RDC)

SHEET TITLE:
 CIRCULATION MAP - SERVICE - LEVEL 1

SHEET NO: ENT1.406 23 OF 65

PTS #: 477943
PROJECT #: 477943

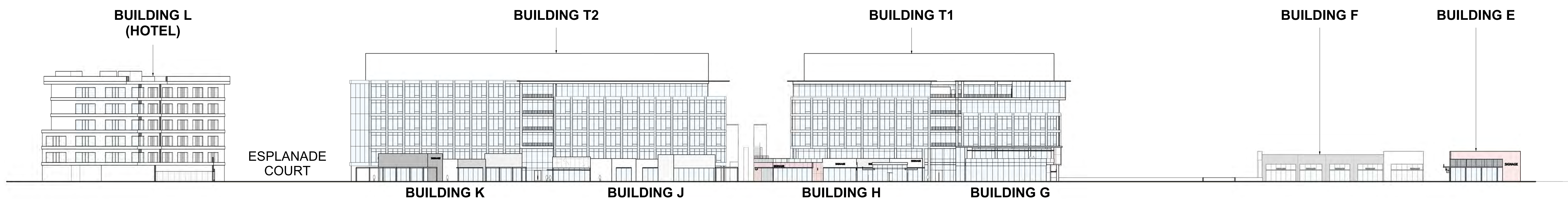
Regency Centers. REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187

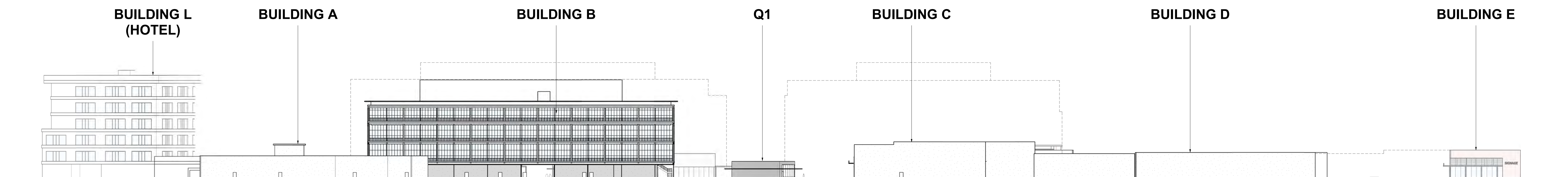
9/1/2020 9:45:18 AM C:\Local\Rev\ARCH\19187\19-Costa Verde Center Entitlement_RDC_sing.txd



1 OVERALL EAST ELEVATION FROM GENESEE AVENUE
SCALE: 1" = 40'-0"



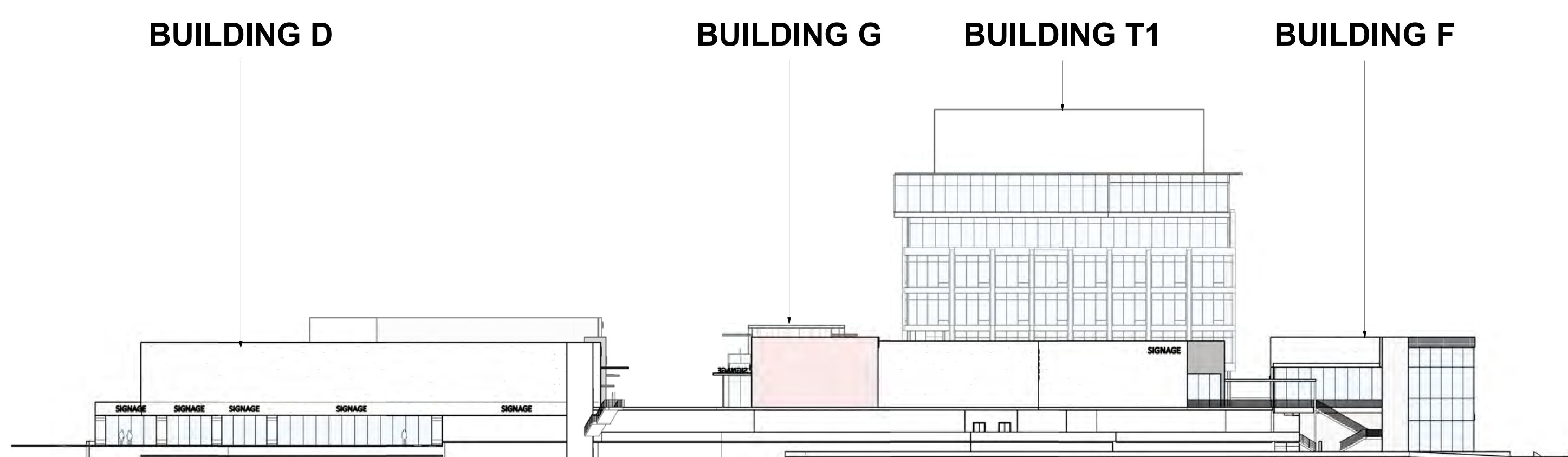
2 OVERALL WEST ELEVATION OF OFFICE TOWERS
SCALE: 1" = 40'-0"



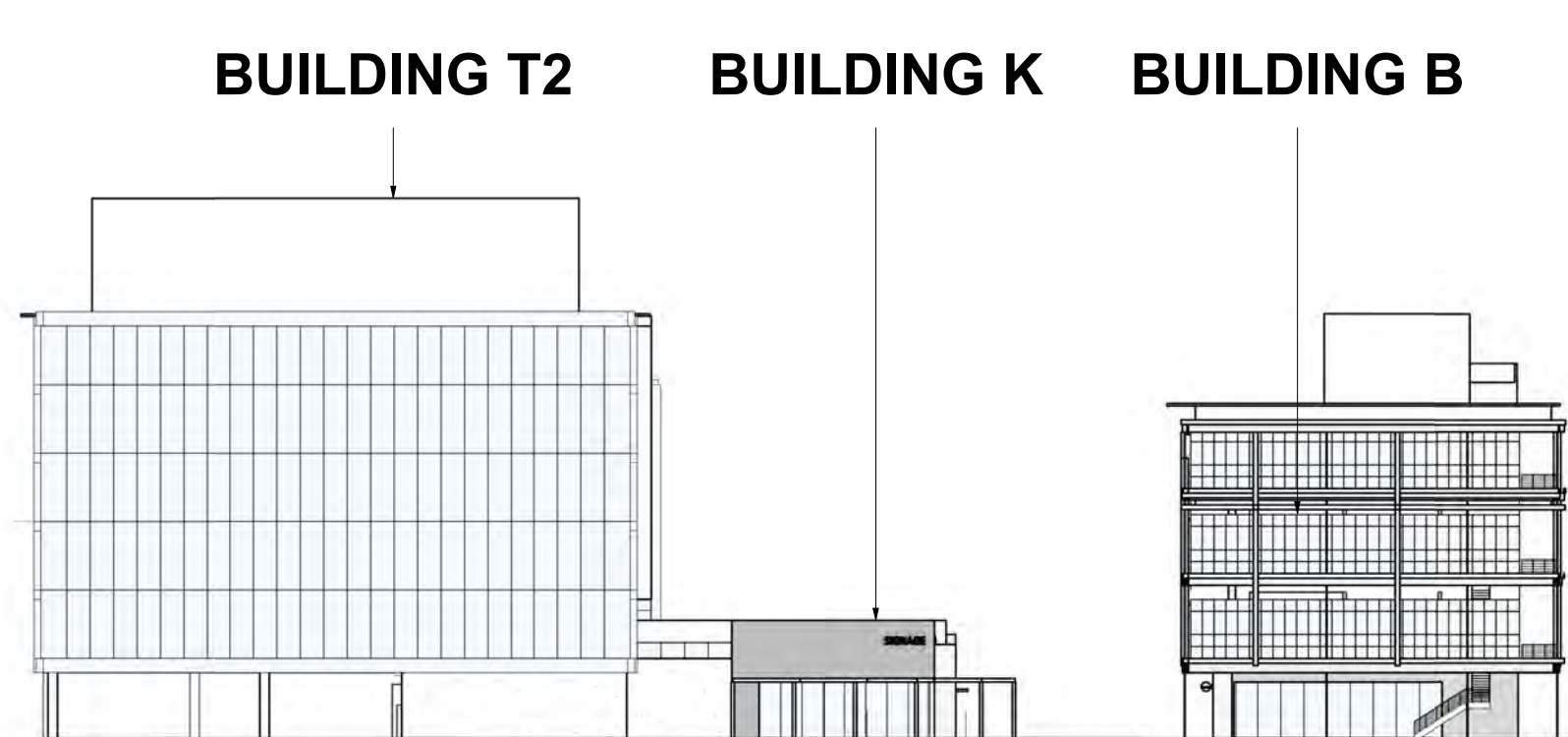
3 OVERALL WEST ELEVATION OF A, B, C, D
SCALE: 1" = 40'-0"



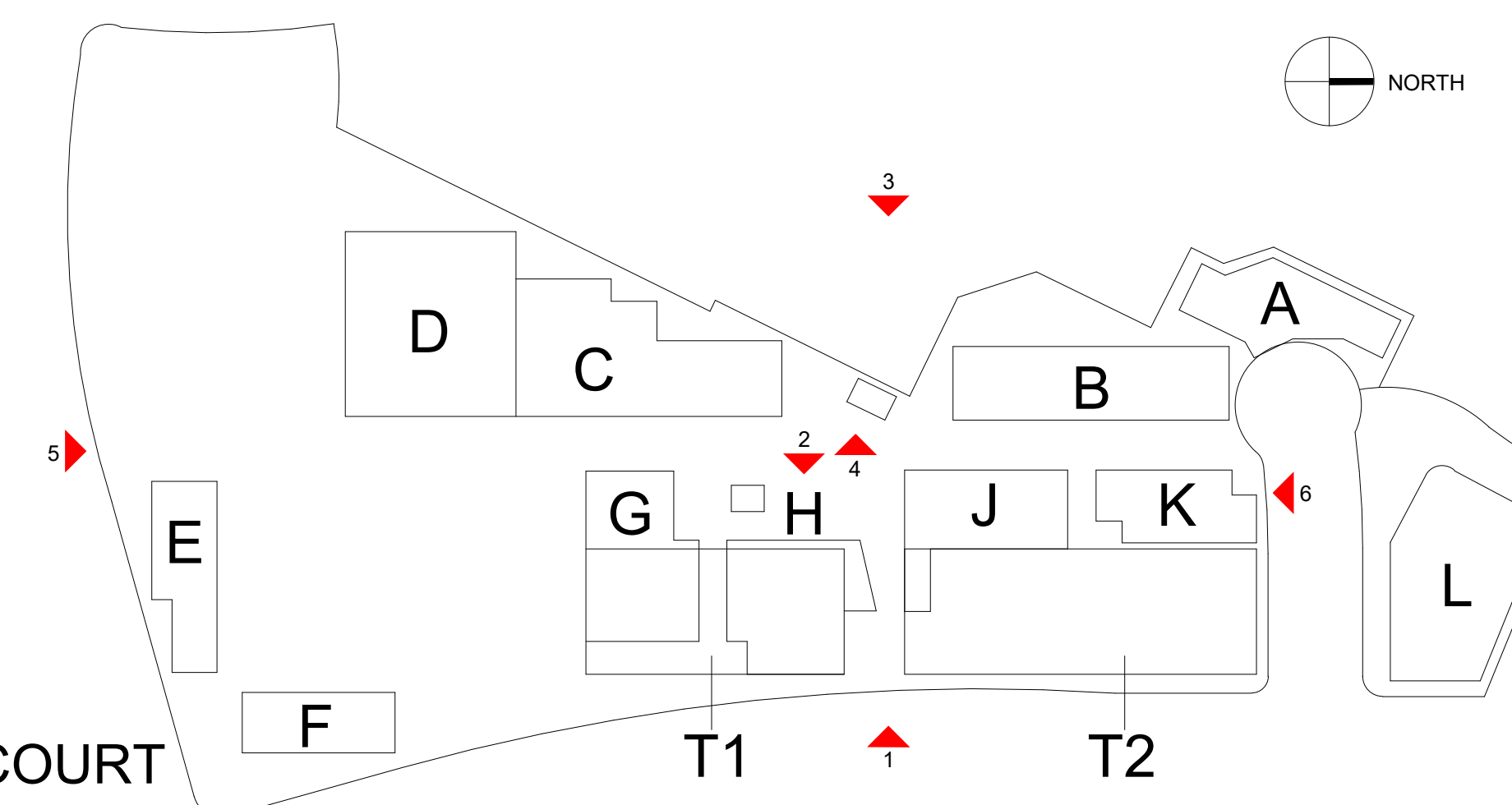
4 OVERALL EAST ELEVATION OF A, B, C, D
SCALE: 1" = 40'-0"



5 OVERALL SOUTH ELEVATION FROM NOBEL DRIVE
SCALE: 1" = 40'-0"



6 OVERALL NORTH ELEVATION FROM ESPLANADE COURT
SCALE: 1" = 40'-0"



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ORIGINAL DATE: 3/15/2016 (BY CALLISONRTKL)
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 REVISION DATE: 06/10/2019 (BY GENSLER)
 REVISION DATE: 10/11/2019 (BY RDC)

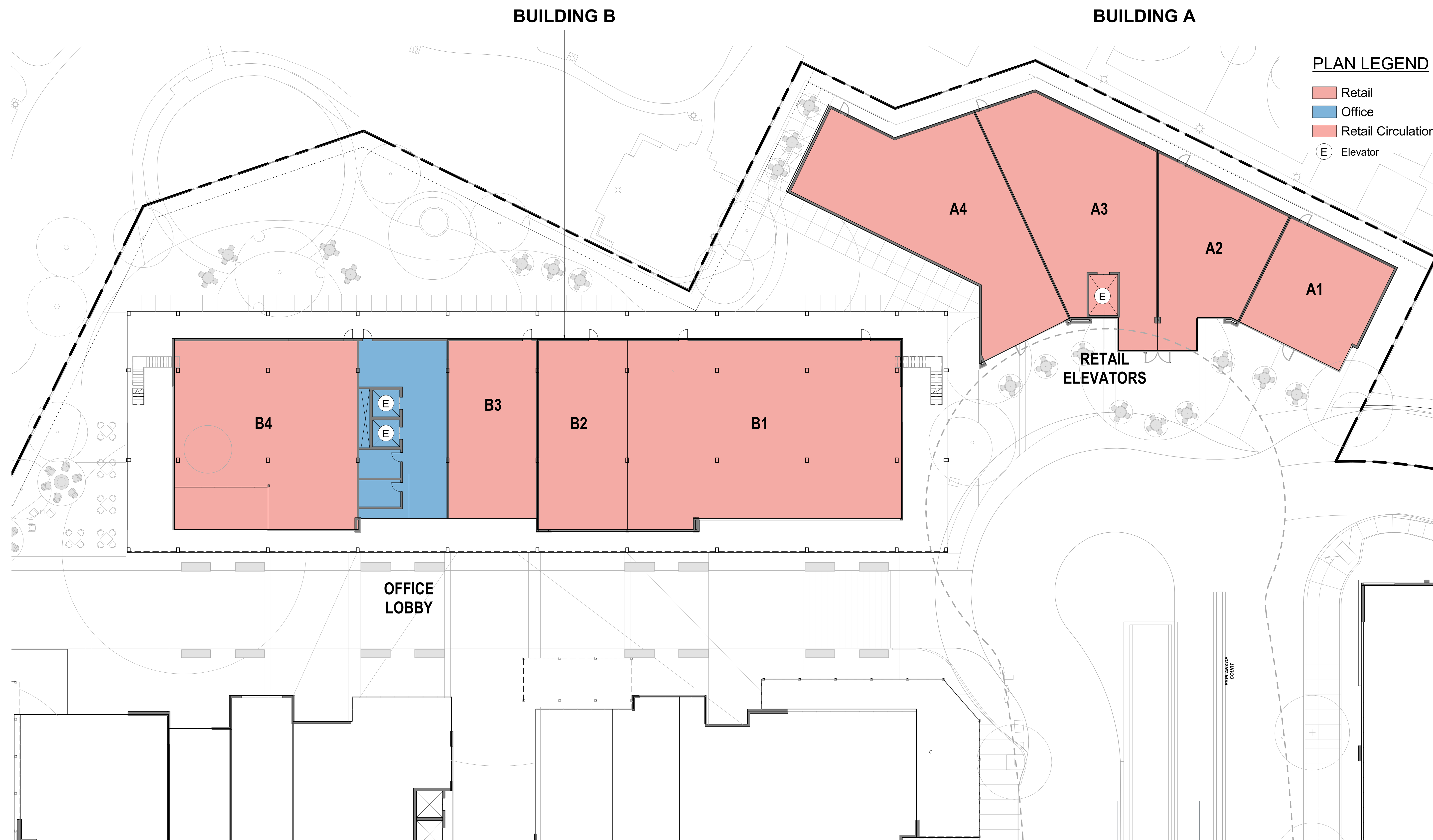
SHEET TITLE:
 OVERALL BUILDING ELEVATIONS

SHEET NO: **ENT2.100** 24 OF 65

PTS #: 477943
 PROJECT #: 477943

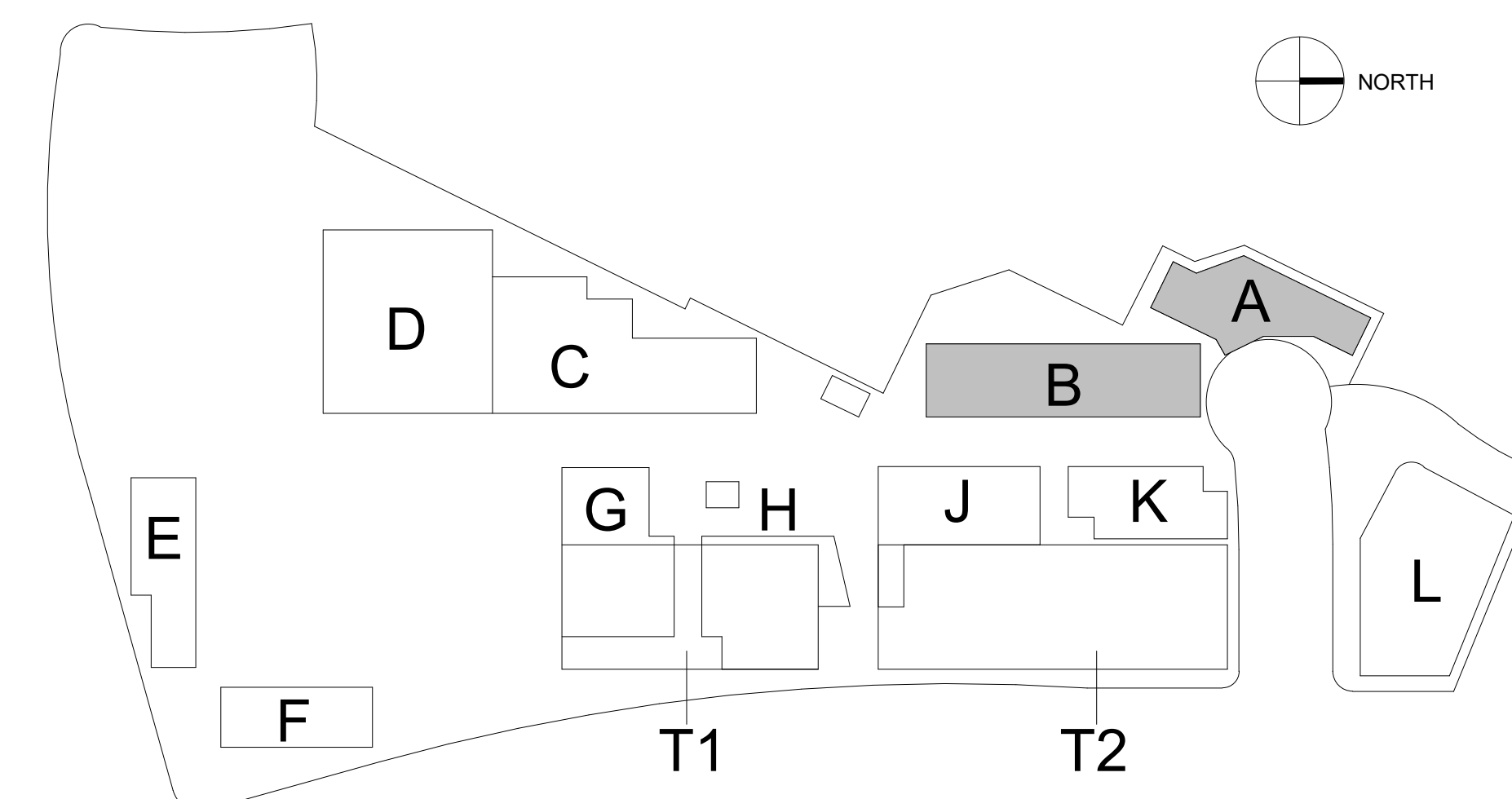
Regency Centers. REGENCY CENTERS
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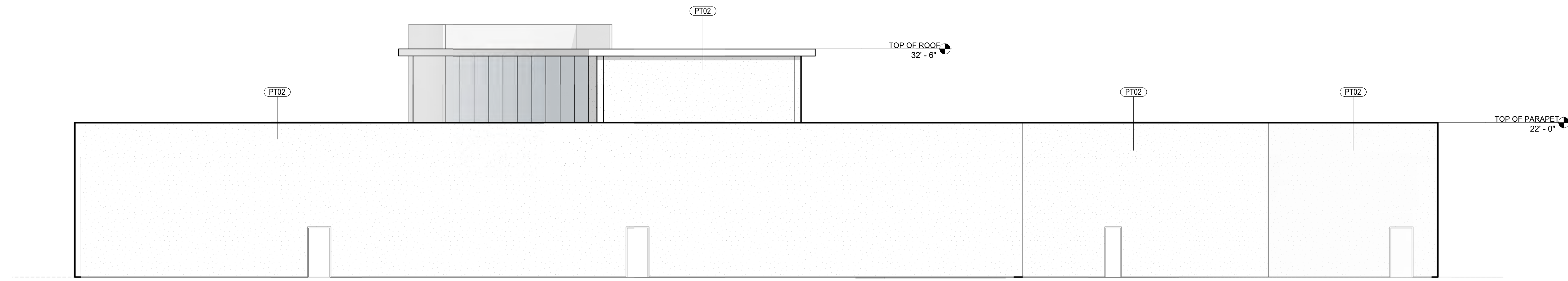
PROJECT NUMBER: 19-187



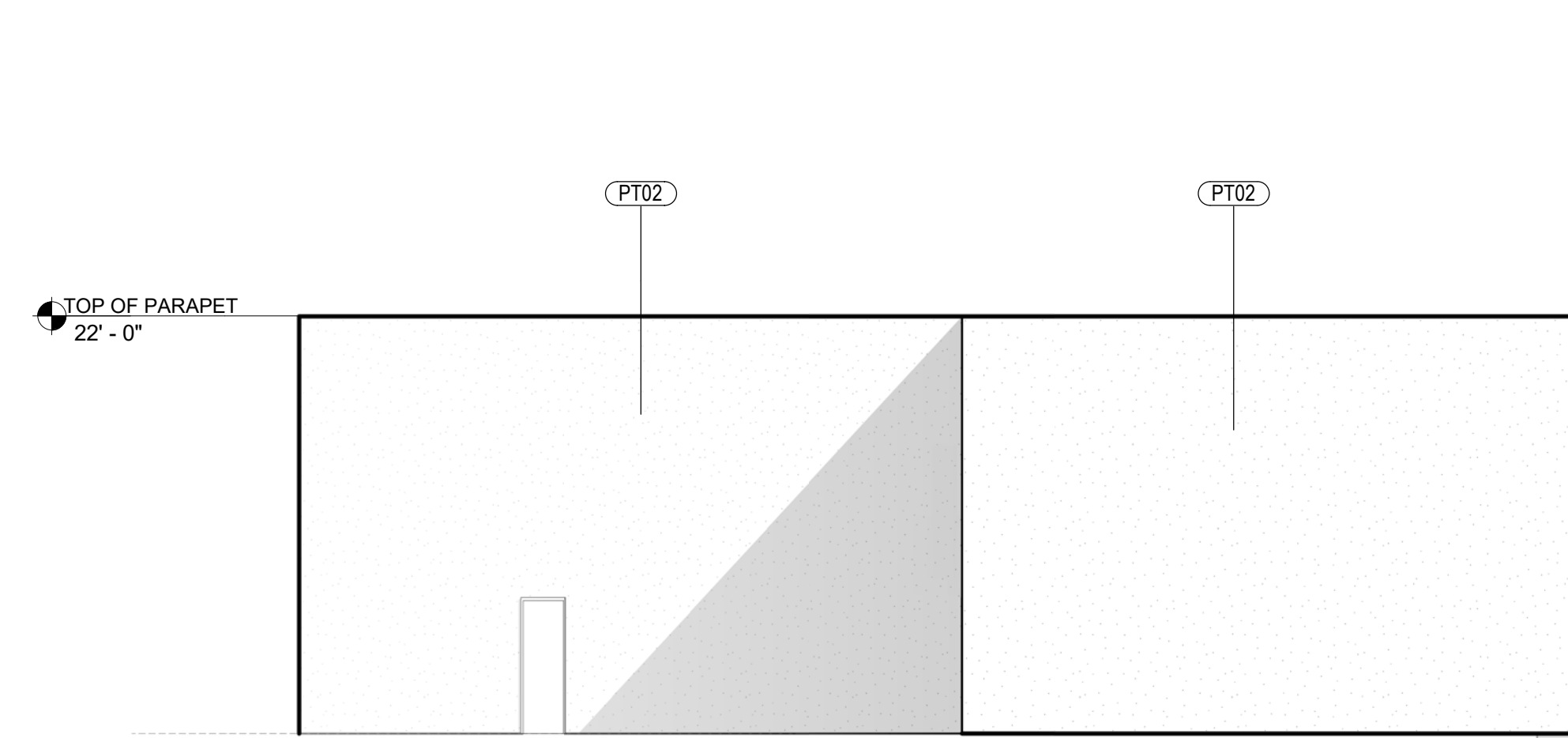
AB BUILDING PLAN A, B
SCALE: 1/16" = 1'-0"

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NAME:	
ADDRESS:	245 EAST THIRD STREET LONG BEACH, CA 90802
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REVISION DATE:	7/28/2016 (BY CALLISONRTKL)
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REVISION DATE:	06/10/2019 (BY GENSLER)
REVISION DATE:	10/11/2019 (BY RDC)
SHEET TITLE:	ENLARGED PLAN OF BUILDING A, B
SHEET NO:	ENT2.110 25 OF 65
PTS #:	477943
PROJECT #:	477943
Regency Centers.	REGENCY CENTERS 420 STEVENS AVE SUITE 320 SOLANA BEACH, CA 92075

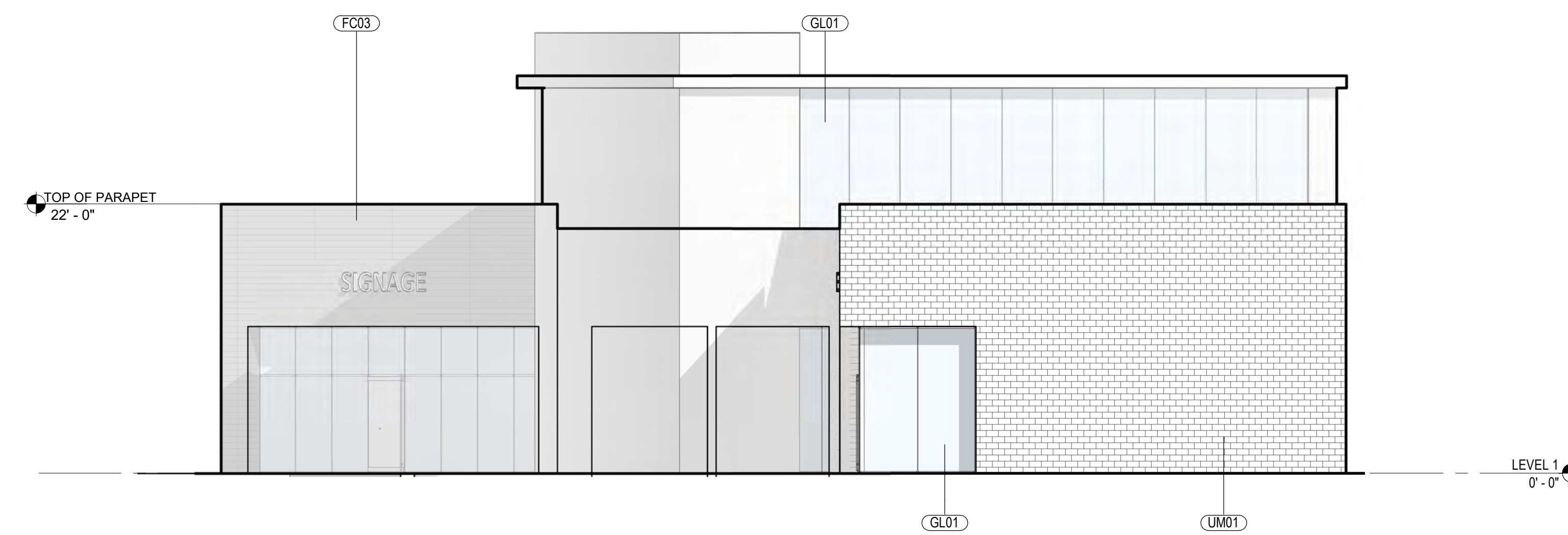




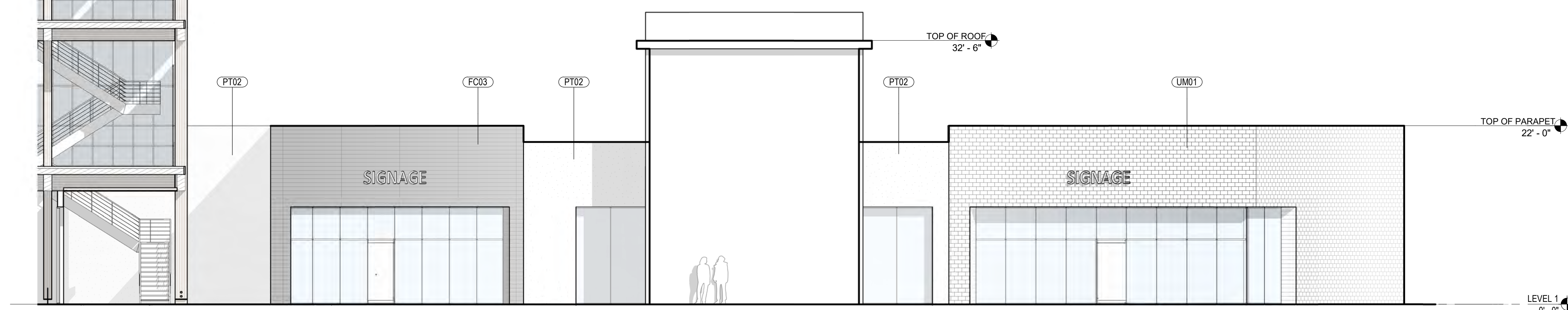
A-W BUILDING A - WEST ELEVATION
SCALE: 1/8" = 1'-0"



A-S BUILDING A - SOUTH ELEVATION
SCALE: 1/8" = 1'-0"



A-N BUILDING A - NORTH ELEVATION
SCALE: 1/8" = 1'-0"



A-E BUILDING A - EAST ELEVATION
SCALE: 1/8" = 1'-0"

MATERIAL LEGEND

CO01	CAST-IN-PLACE CONCRETE
FC01	FIBER CEMENT PANEL
FC02	FIBER CEMENT PANEL
FC03	FIBER CEMENT PANEL
GL01	TEMPERED INSULATED STOREFRONT
GL02	GLAZING
GL03	INSULATED VISION GLAZING
MT01	SPANDREL GLAZING
MT02	MATTE BLACK COMPOSITE METAL PANEL
MT03	PERFORATED METAL PANEL
MT04	COPPER PANEL
MT10	METAL
PT01	METAL GUARDRAIL
PT02	PAINT #1 OVER SMOOTH PLASTER
ST01	PAINT #2 OVER SMOOTH PLASTER
UM01	STONE PANEL CLADDING
UM02	FULL WIDTH WHITE BRICK VENEER
UM03	FULL WIDTH DARK BRICK VENEER
WD01	FULL WIDTH BRICK VENEER
WD02	WOOD SIDING
WD03	WOOD SIDING
WD04	WOOD PANEL
WD06	RANDOM LENGTH WOOD SIDING
WD07	SOLID WOOD SCREEN
	MASS TIMBER

NOTE: MATERIALS REFERENCED IN THIS DRAWING AND LEGEND ARE CONCEPTUAL. ALL EXTERIOR MATERIALS TO BE DETERMINED AT TIME OF BUILDING PERMIT.

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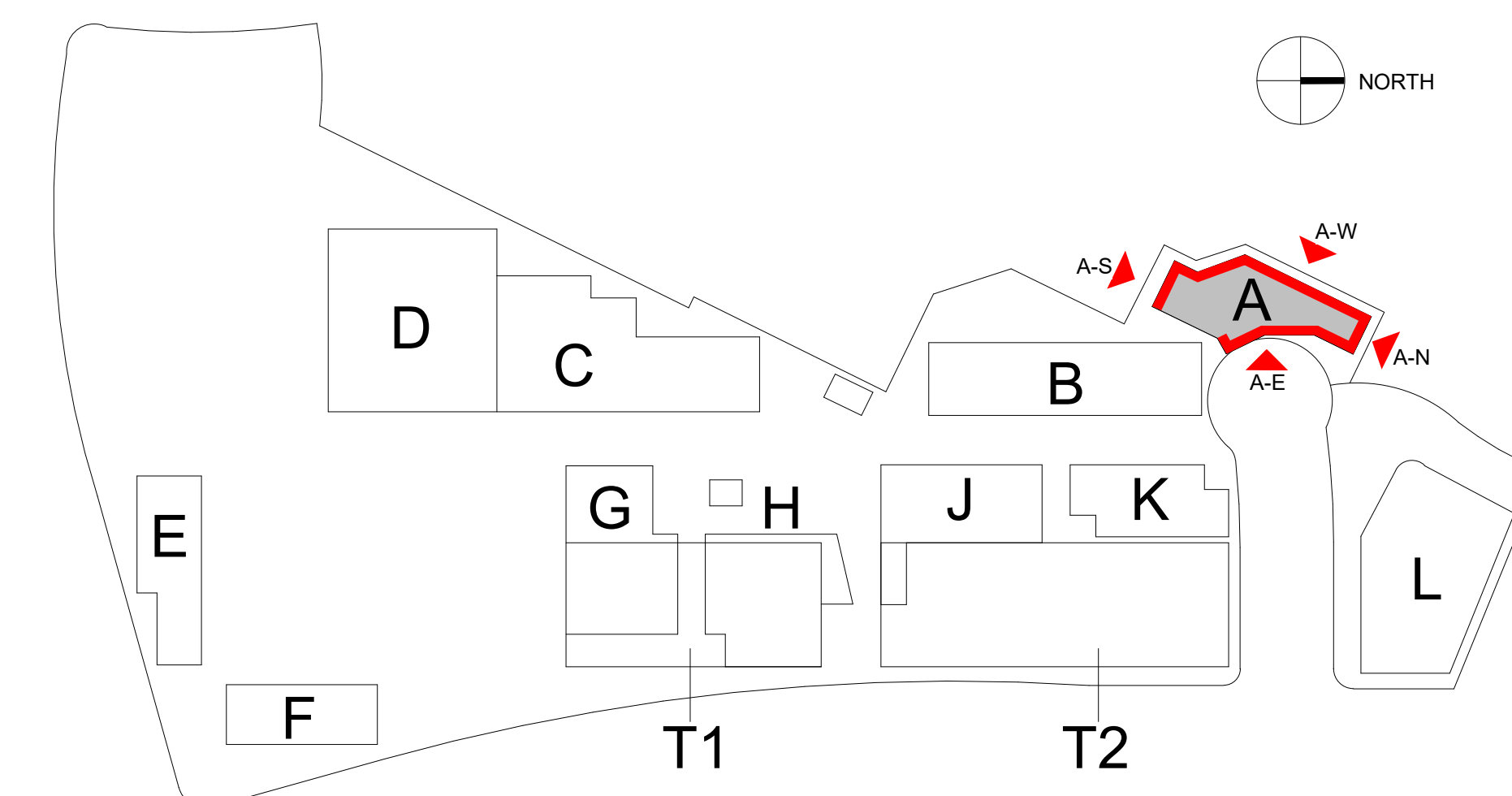
SHEET TITLE:
 ENLARGED ELEVATIONS BUILDING A

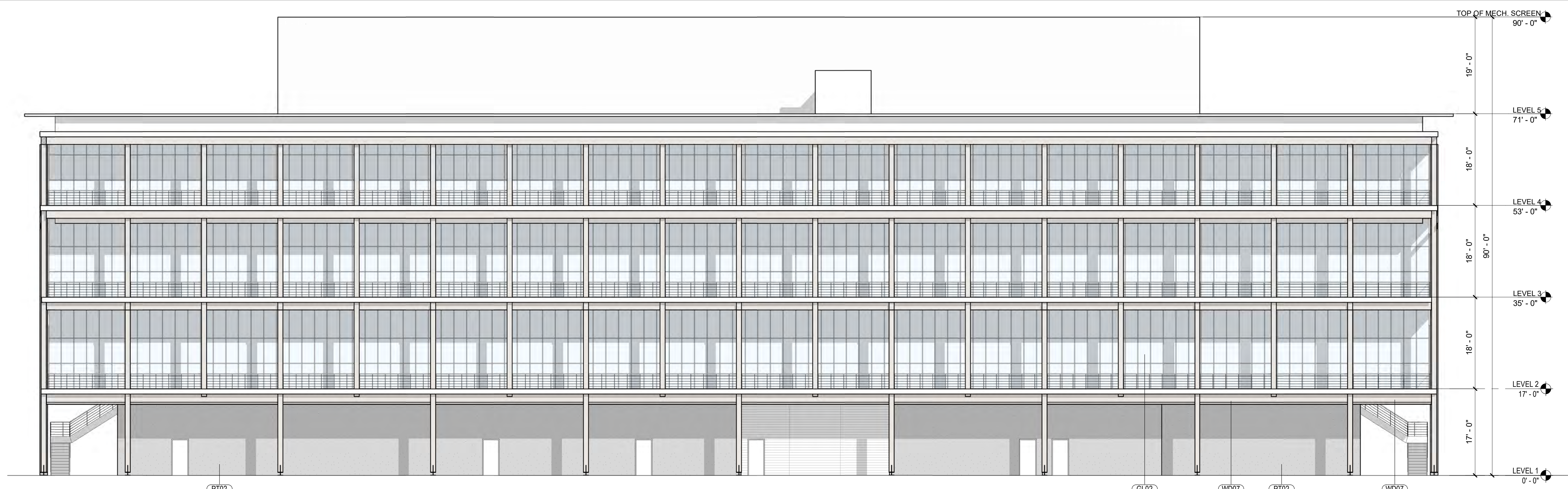
SHEET NO: ENT2.111 26 OF 65

PTS #: 477943
PROJECT #: 477943

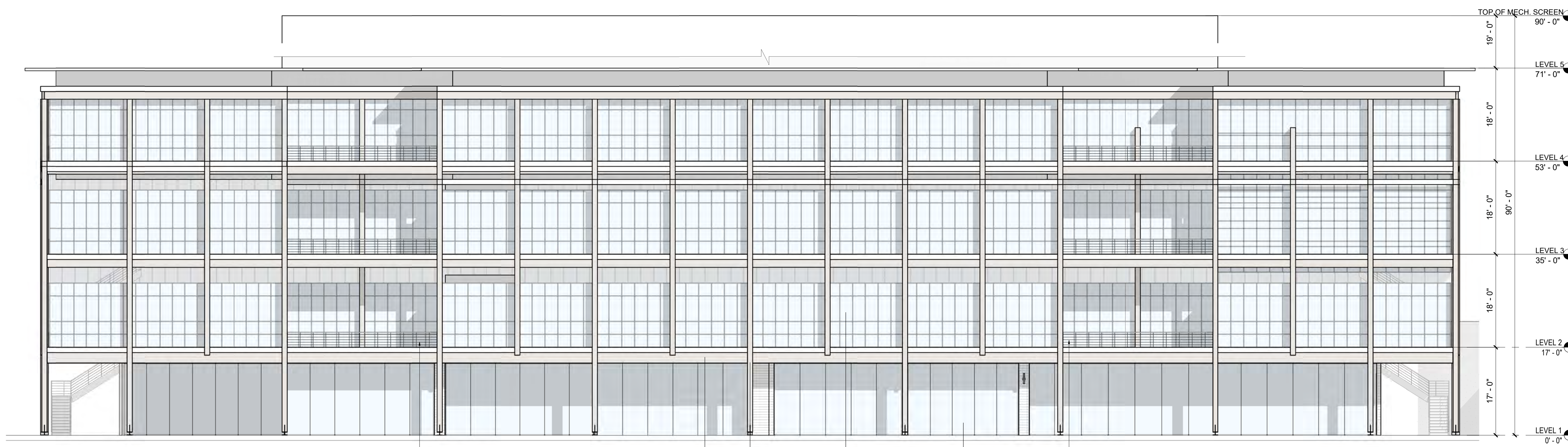
Regency Centers REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187





B-W BUILDING B - WEST ELEVATION
SCALE: 3/32" = 1'-0"

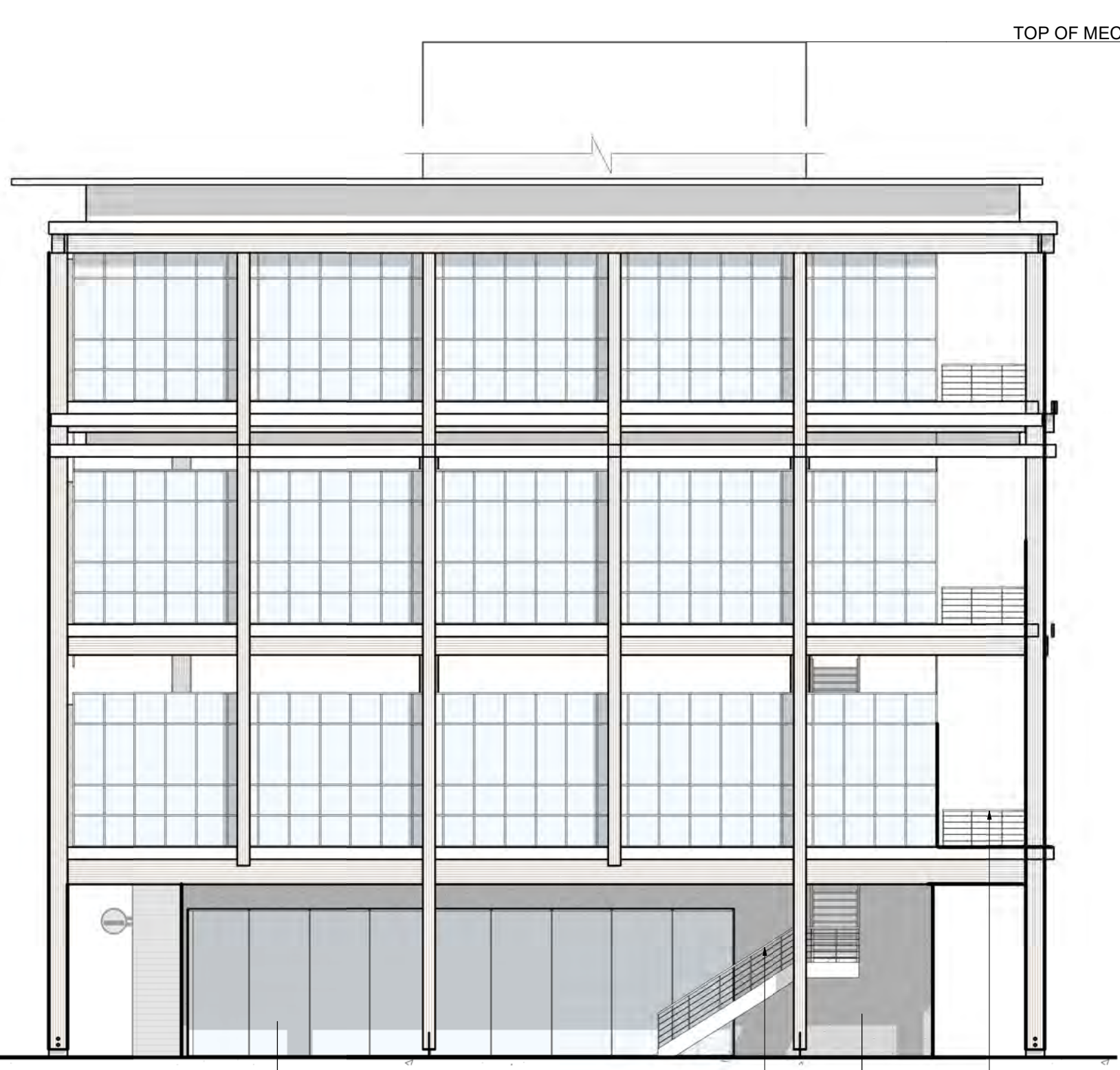
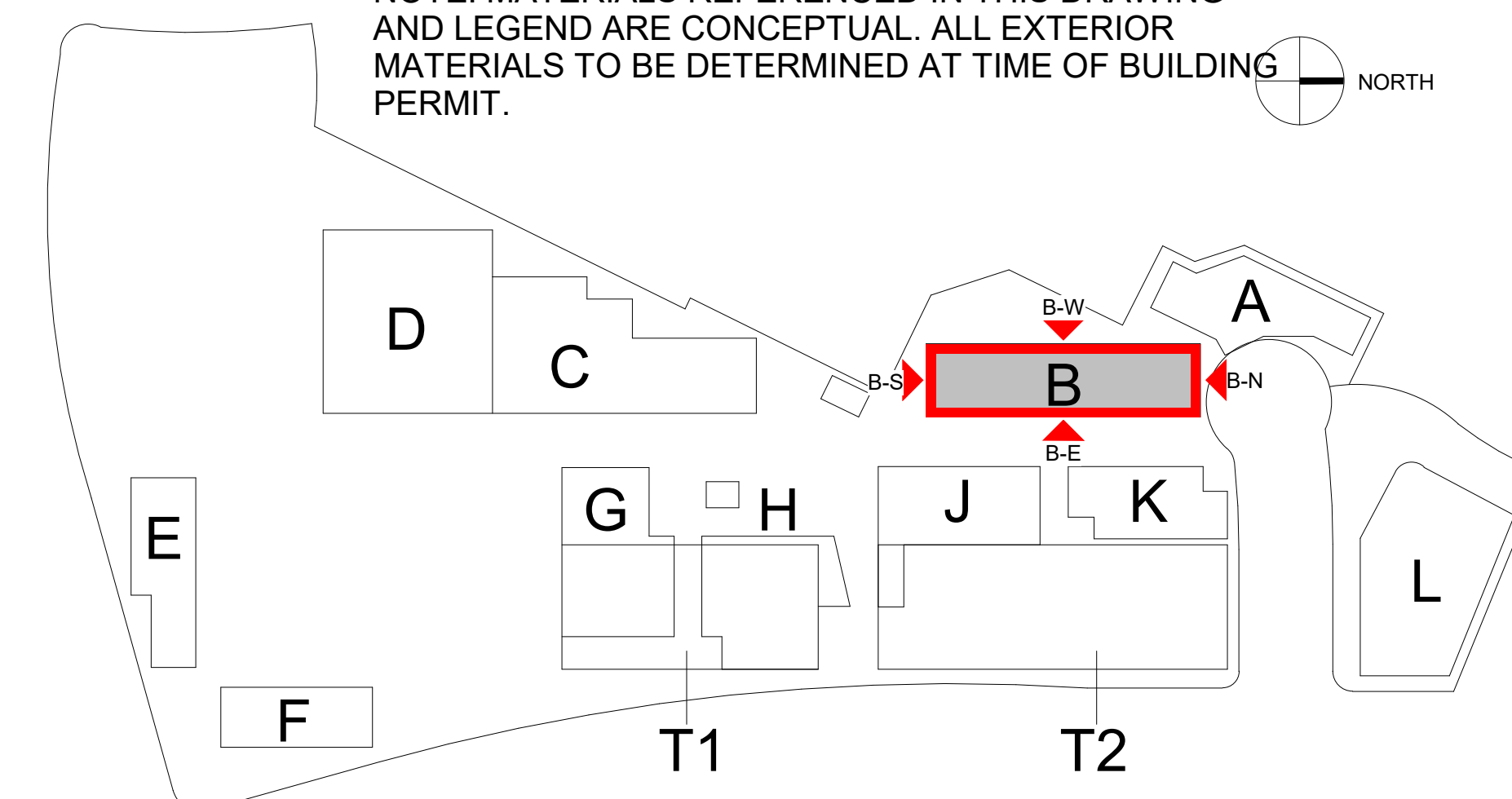


B-E BUILDING B - EAST ELEVATION
SCALE: 3/32" = 1'-0"

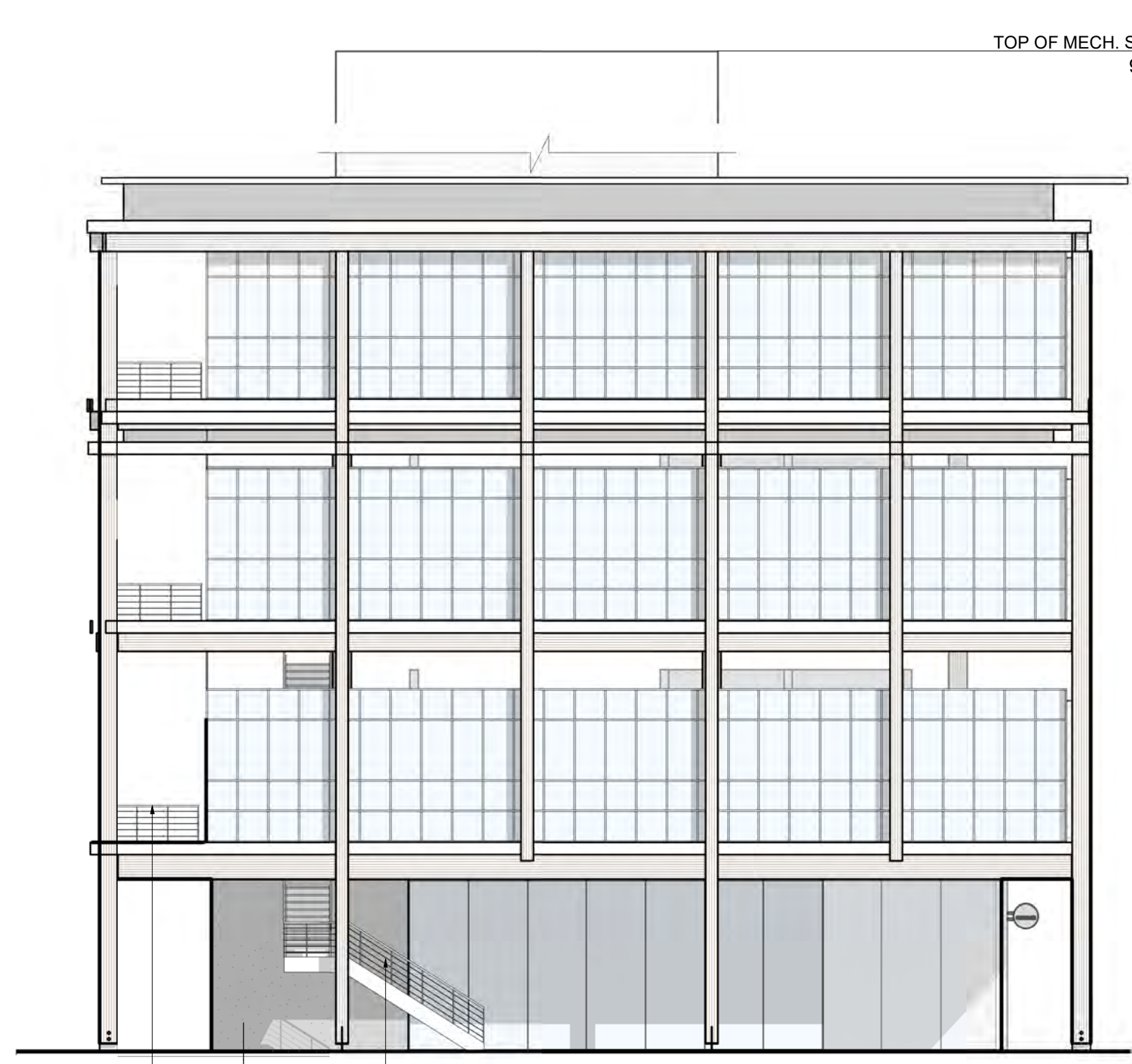
MATERIAL LEGEND

- GL01 TEMPERED INSULATED STOREFRONT
- GL02 GLAZING
- GL03 INSULATED VISION GLAZING
- MT01 SPANDREL GLAZING
- MT02 MATTE BLACK COMPOSITE METAL PANEL
- MT03 PERFORATED METAL PANEL
- MT04 COPPER PANEL
- MT10 METAL
- PT01 METAL GUARDRAIL
- PT02 PAINT #1 OVER SMOOTH PLASTER
- WD01 PAINT #2 OVER SMOOTH PLASTER
- WD02 WOOD SIDING
- WD03 WOOD SIDING
- WD04 WOOD PANEL
- WD06 RANDOM LENGTH WOOD SIDING
- WD07 SOLID WOOD SCREEN
- MASS TIMBER

NOTE: MATERIALS REFERENCED IN THIS DRAWING AND LEGEND ARE CONCEPTUAL. ALL EXTERIOR MATERIALS TO BE DETERMINED AT TIME OF BUILDING PERMIT.



B-N BUILDING B - NORTH ELEVATION
SCALE: 3/32" = 1'-0"



B-S BUILDING B - SOUTH ELEVATION
SCALE: 3/32" = 1'-0"

PREPARED BY: **rdc.**
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ORIGINAL DATE: 3/15/2016 (BY CALLISONRTKL)
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REVISION DATE: 06/10/2019 (BY GENSLER)
REVISION DATE: 10/11/2019 (BY RDC)

SHEET TITLE:
ENLARGED ELEVATIONS OF BUILDING B

SHEET NO: ENT2.112 27 OF 65

PTS #: 477943
PROJECT #: 477943

Regency Centers REGENCY CENTERS
420 STEVENS AVE SUITE 320
SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187

(E) RETAIL
EXISTING
RETAIL



RETAIL
ELEVATORS

BUILDING D

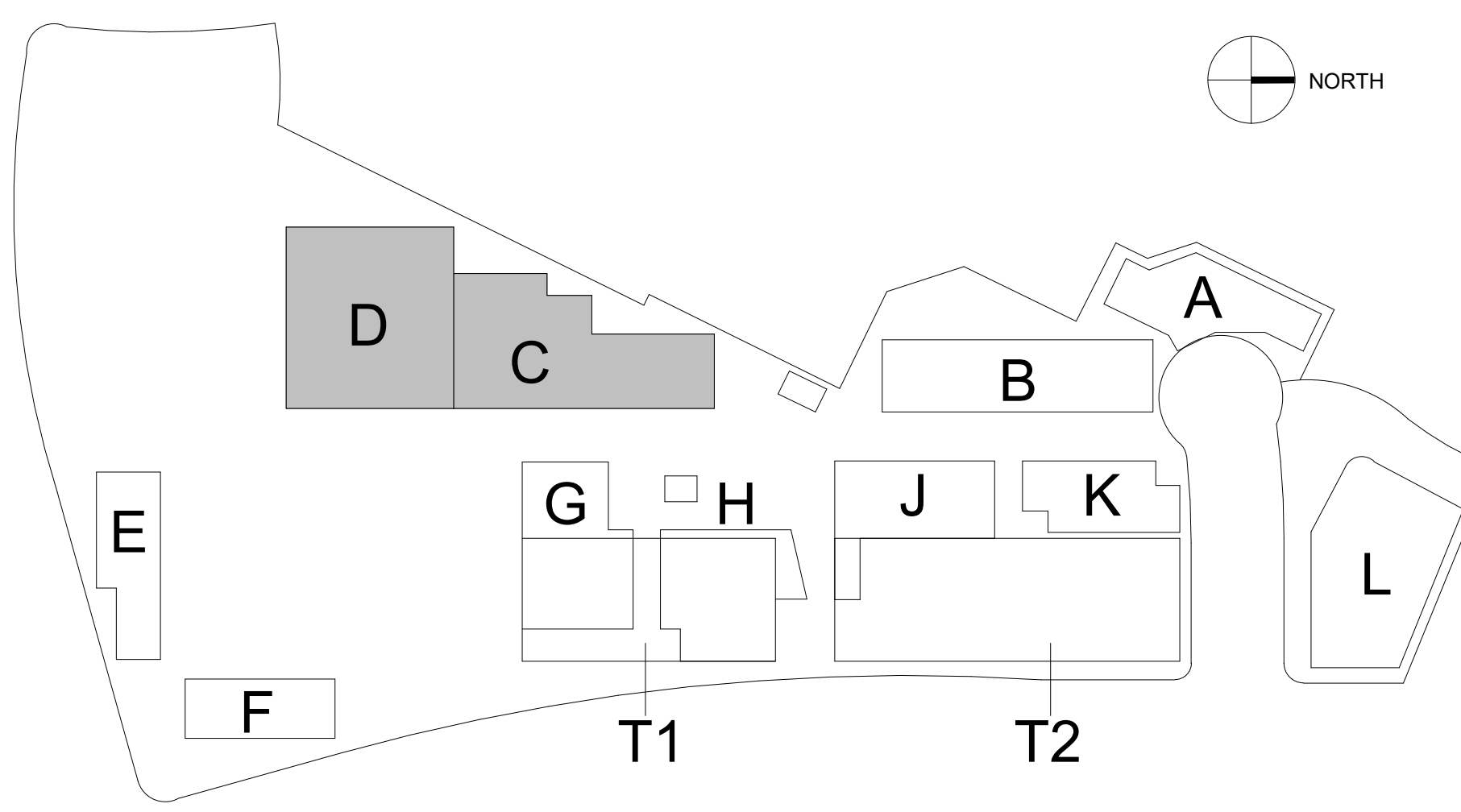
RETAIL
ELEVATORS
BUILDING C

PLAN LEGEND

- Retail
- Service
- Retail Circulation
- Elevator

CD BUILDING PLAN C, D

SCALE: 1/16" = 1'-0"



PREPARED BY: **rdc.**
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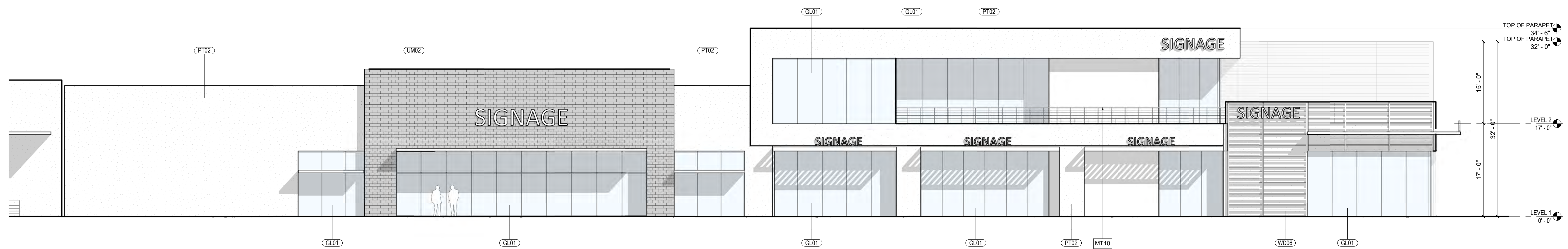
SHEET TITLE:
 ENLARGED PLAN OF BUILDING C, D

SHEET NO: **ENT2.120** 28 OF 65

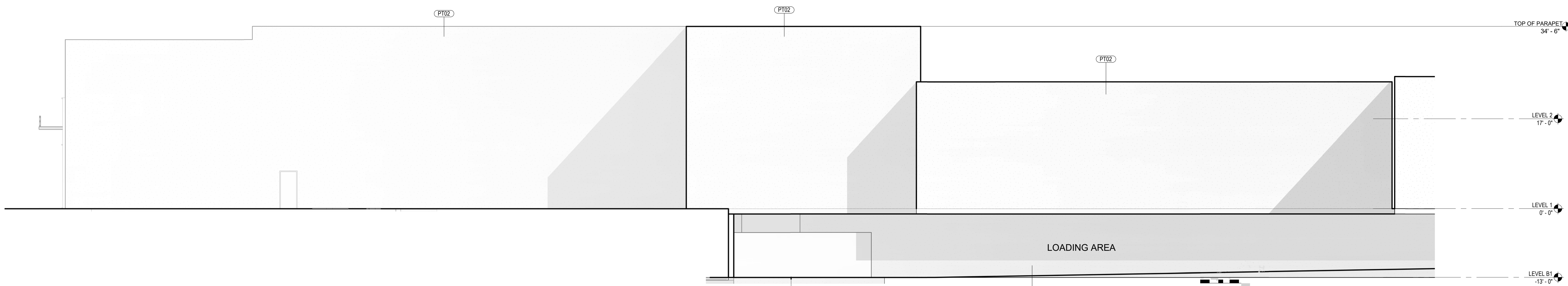
PTS #: 477943
 PROJECT #: 477943

Regency Centers REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187



C-E BUILDING C - EAST ELEVATION
SCALE: 1/8" = 1'-0"

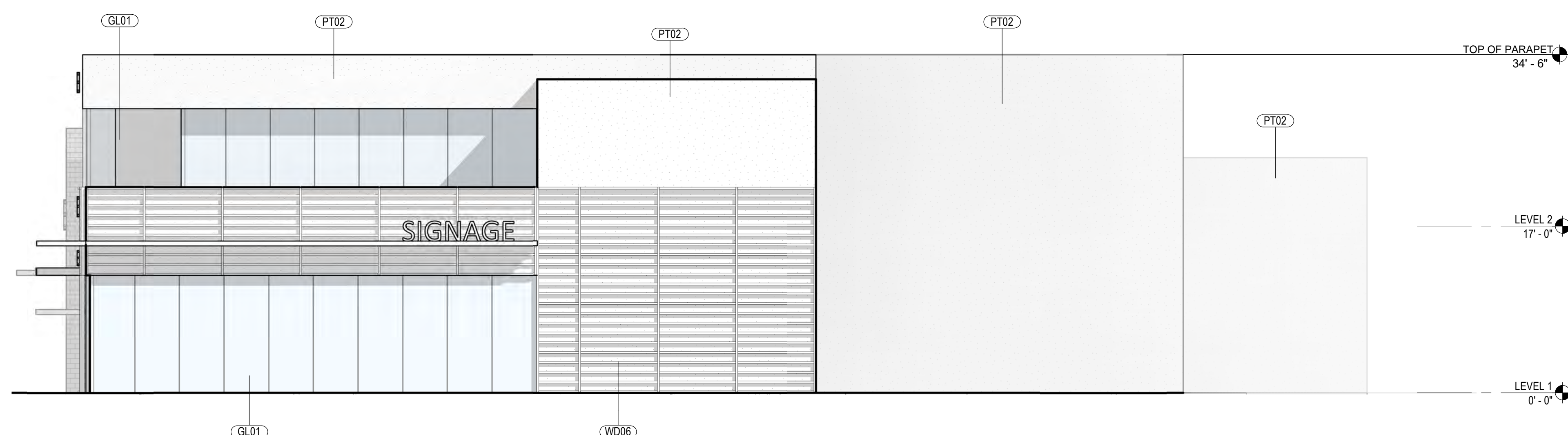
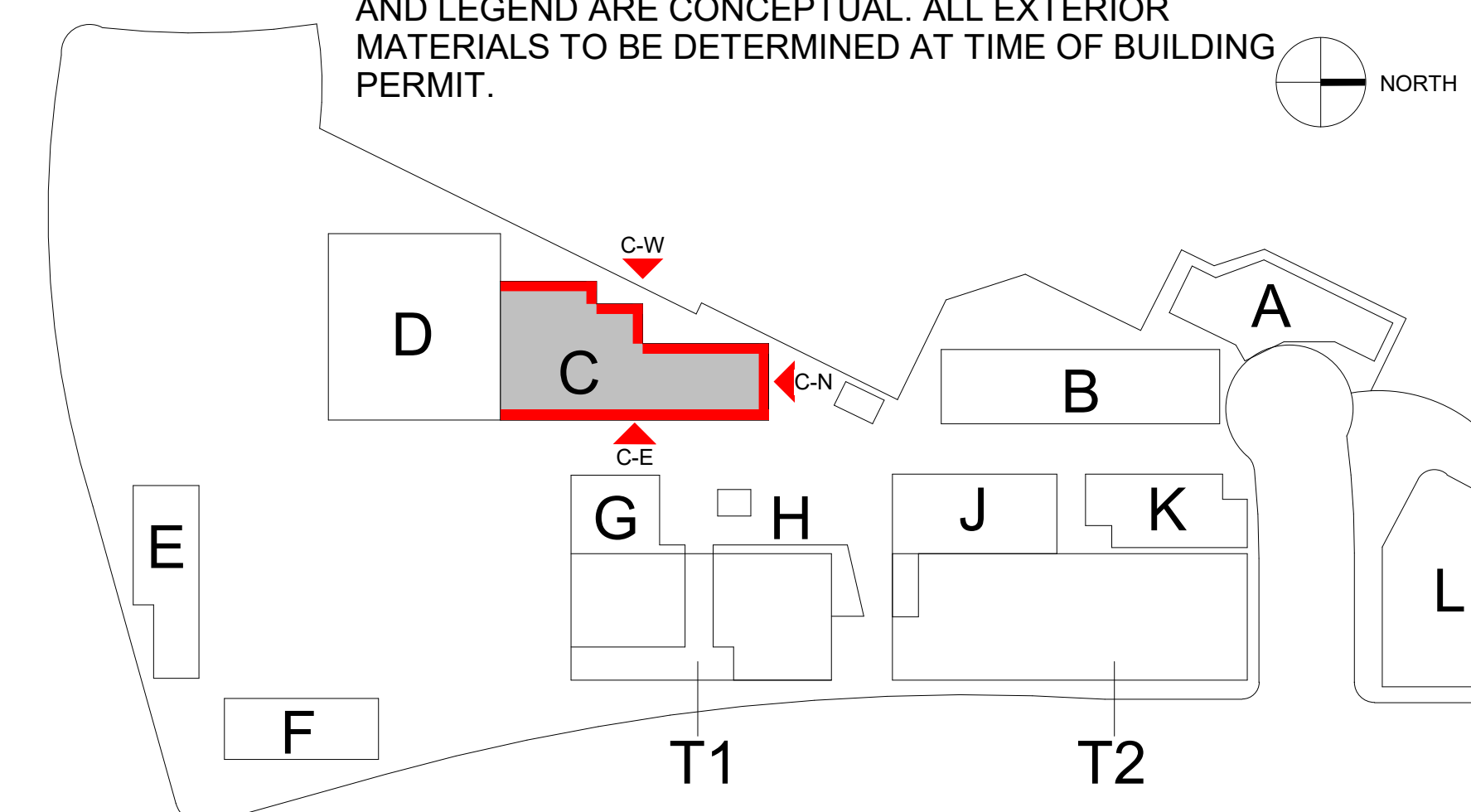


C-W BUILDING C - WEST ELEVATION
SCALE: 1/8" = 1'-0"

MATERIAL LEGEND

- CO01 CAST-IN-PLACE CONCRETE
- FC01 FIBER CEMENT PANEL
- FC02 FIBER CEMENT PANEL
- FC03 FIBER CEMENT PANEL
- GL01 TEMPERED INSULATED STOREFRONT
- GL02 GLAZING
- GL03 INSULATED VISION GLAZING
- MT01 SPANDREL GLAZING
- MT02 MATTE BLACK COMPOSITE METAL PANEL
- MT03 PERFORATED METAL PANEL
- MT04 COPPER PANEL
- MT10 METAL
- PT01 METAL GUARDRAIL
- PT02 PAINT #1 OVER SMOOTH PLASTER
- ST01 PAINT #2 OVER SMOOTH PLASTER
- UM01 STONE PANEL CLADDING
- UM02 FULL WIDTH WHITE BRICK VENEER
- UM03 FULL WIDTH DARK BRICK VENEER
- WD01 FULL WIDTH BRICK VENEER
- WD02 WOOD SIDING
- WD03 WOOD SIDING
- WD04 WOOD PANEL
- WD06 RANDOM LENGTH WOOD SIDING
- WD07 SOLID WOOD SCREEN
- MASS TIMBER

NOTE: MATERIALS REFERENCED IN THIS DRAWING AND LEGEND ARE CONCEPTUAL. ALL EXTERIOR MATERIALS TO BE DETERMINED AT TIME OF BUILDING PERMIT.



C-N BUILDING C - NORTH ELEVATION
SCALE: 1/8" = 1'-0"

PREPARED BY: **rdc.**
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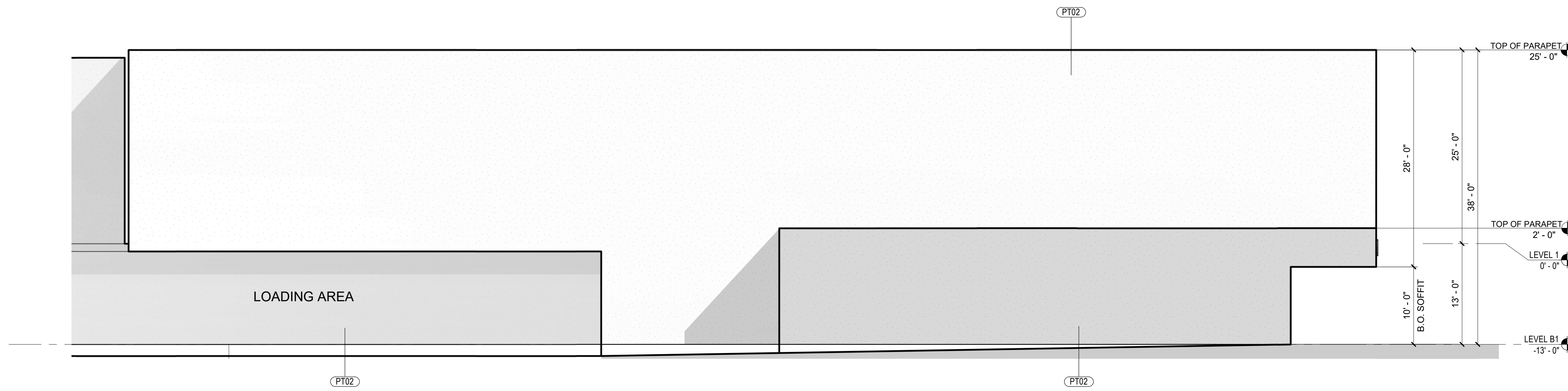
SHEET TITLE:
ENLARGED ELEVATIONS OF BUILDING C

SHEET NO: ENT2.121 29 OF 65

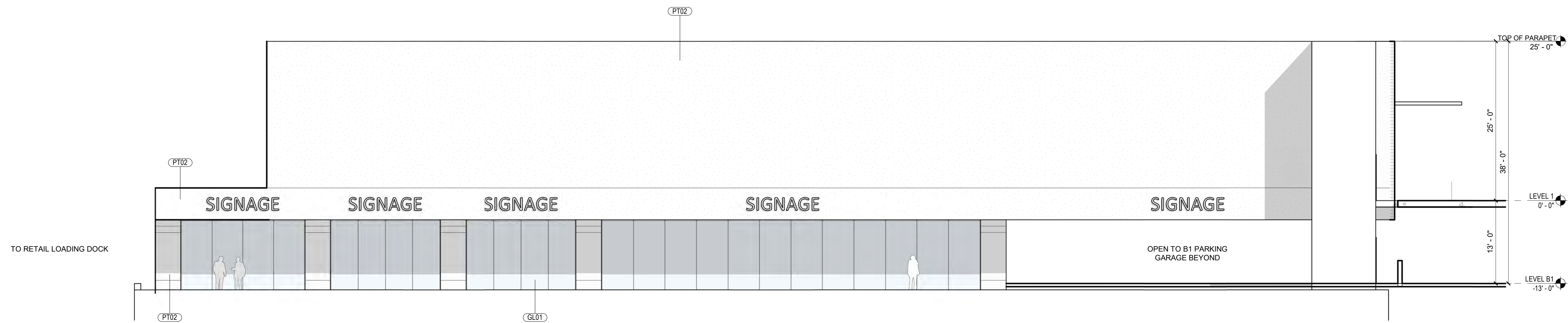
PTS #: 477943
PROJECT #: 477943

Regency Centers REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

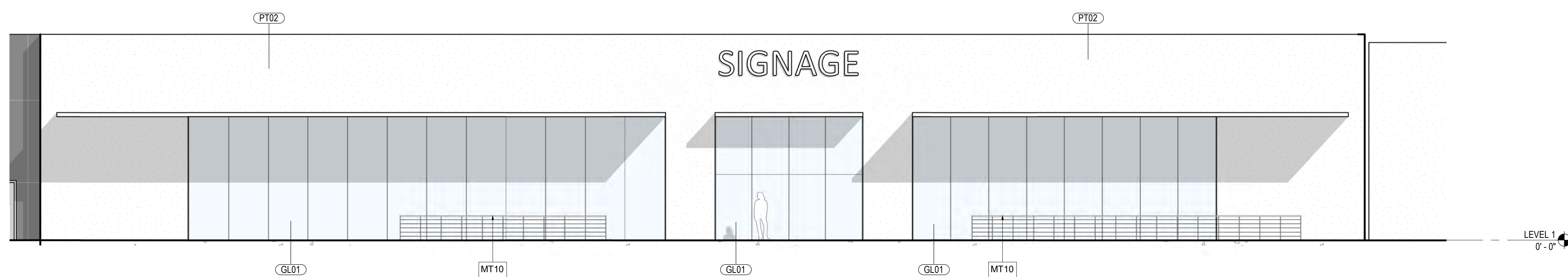
PROJECT NUMBER: 19-187



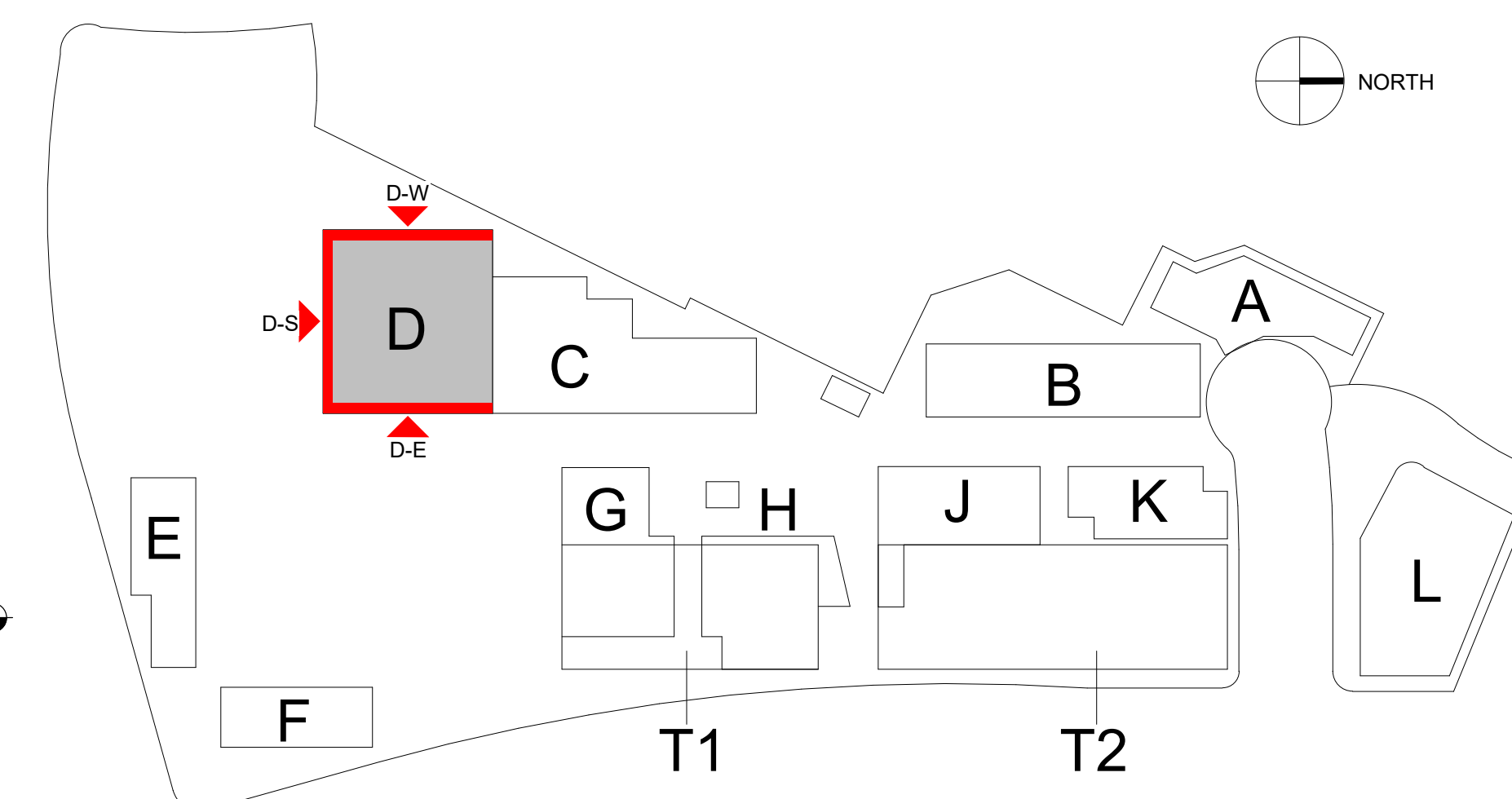
D-W BUILDING D - WEST ELEVATION
SCALE: 1/8" = 1'-0"



D-S BUILDING D - SOUTH ELEVATION
SCALE: 1/8" = 1'-0"



D-E BUILDING D - EAST ELEVATION
SCALE: 1/8" = 1'-0"



MATERIAL LEGEND

CO01	CAST-IN-PLACE CONCRETE
FC01	FIBER CEMENT PANEL
FC02	FIBER CEMENT PANEL
FC03	FIBER CEMENT PANEL
GL01	TEMPERED INSULATED STOREFRONT
GL02	GLAZING
GL03	INSULATED VISION GLAZING
MT01	SPANDREL GLAZING
MT02	MATTE BLACK COMPOSITE METAL PANEL
MT03	PERFORATED METAL PANEL
MT04	COPPER PANEL
MT10	METAL
PT01	METAL GUARDRAIL
PT02	PAINT #1 OVER SMOOTH PLASTER
ST01	PAINT #2 OVER SMOOTH PLASTER
UM01	STONE PANEL CLADDING
UM02	FULL WIDTH WHITE BRICK VENEER
UM03	FULL WIDTH DARK BRICK VENEER
WD01	FULL WIDTH BRICK VENEER
WD02	WOOD SIDING
WD03	WOOD SIDING
WD04	WOOD SIDING
WD06	RANDOM LENGTH WOOD SIDING
WD07	SOLID WOOD SCREEN
	MASS TIMBER

NOTE: MATERIALS REFERENCED IN THIS DRAWING AND LEGEND ARE CONCEPTUAL. ALL EXTERIOR MATERIALS TO BE DETERMINED AT TIME OF BUILDING PERMIT.

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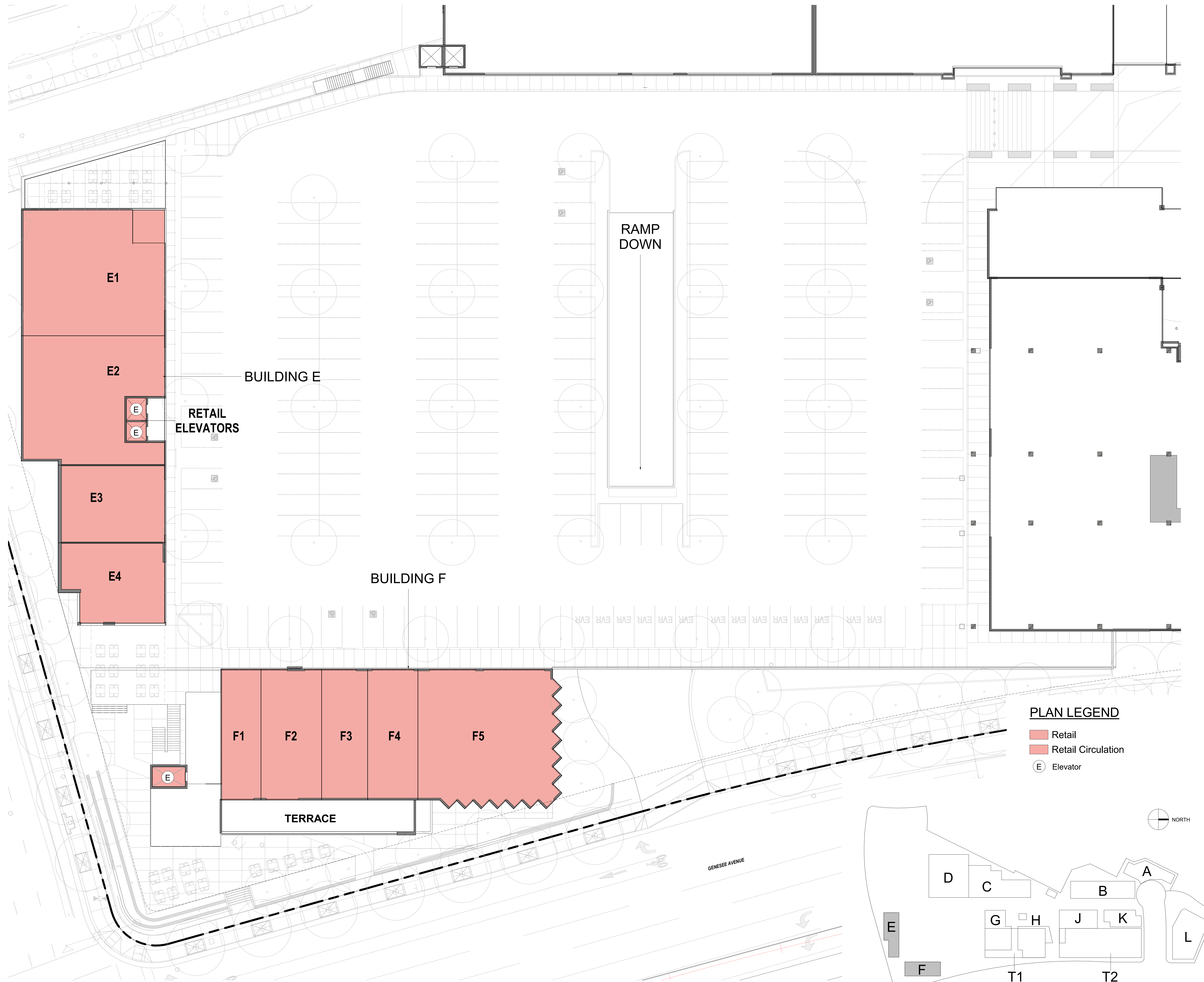
ORIGINAL DATE: 3/15/2016 (BY CALLISONRTKL)
REVISION DATE: 7/28/2016 (BY CALLISONRTKL)
REVISION DATE: 12/12/2016 (BY CALLISONRTKL)
REVISION DATE: 06/10/2019 (BY GENSLER)
REVISION DATE: 10/11/2019 (BY RDC)

SHEET TITLE:
ENLARGED ELEVATIONS OF BUILDING D

SHEET NO: ENT2.122 30 OF 65

PTS #: 477943
PROJECT #: 477943

Regency Centers. **REGENCY CENTERS**
420 STEVENS AVE SUITE 320
SOLANA BEACH, CA 92075



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 REVISION DATE: 10/11/2019 (BY RDC)
 REVISION DATE: 09/01/2020 (BY RDC)

SHEET TITLE:
 ENLARGED PLAN OF BUILDING E, F

SHEET NO: **ENT2.130** 31 OF 65

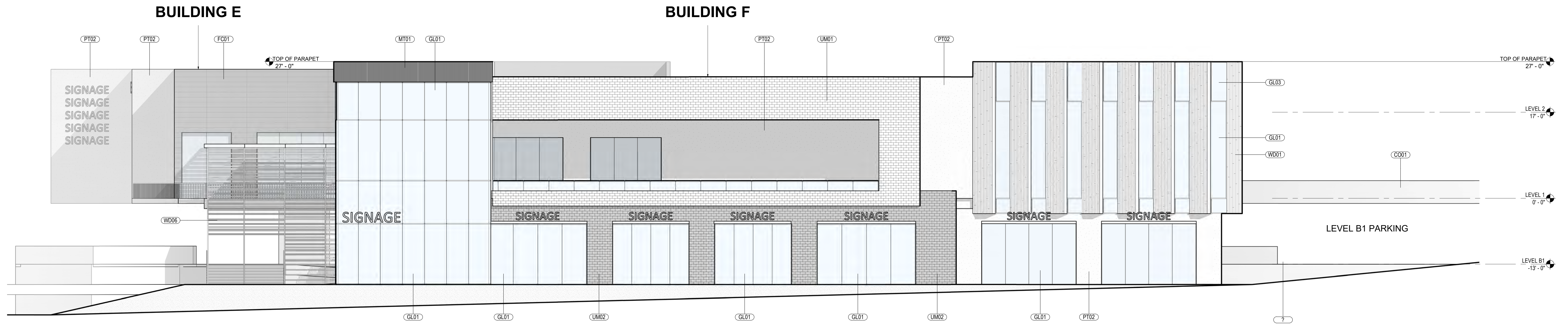
PTS #: 477943
 PROJECT #: 477943

Regency Centers. REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

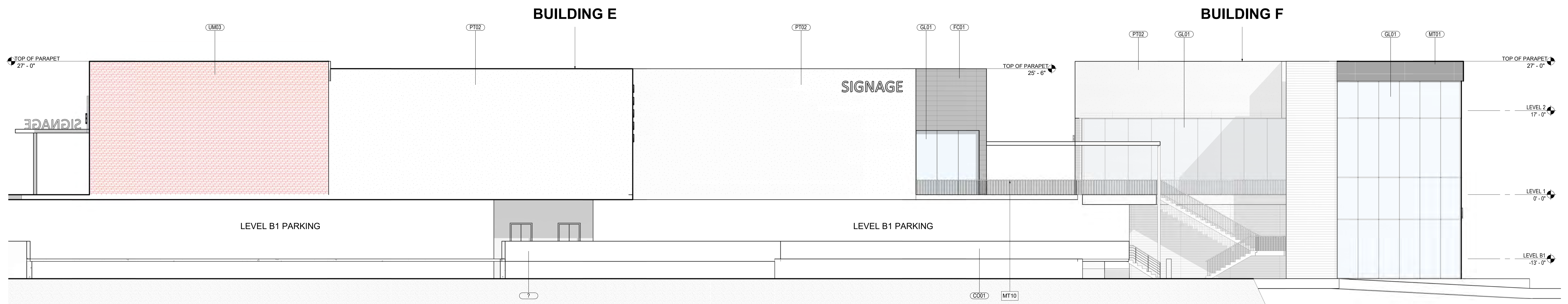
PROJECT NUMBER: 19-187

EF BUILDING PLAN E, F
 SCALE: 1/16" = 1'-0"

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EF-E BUILDING E, F - EAST ELEVATION
SCALE: 1/8" = 1'-0"

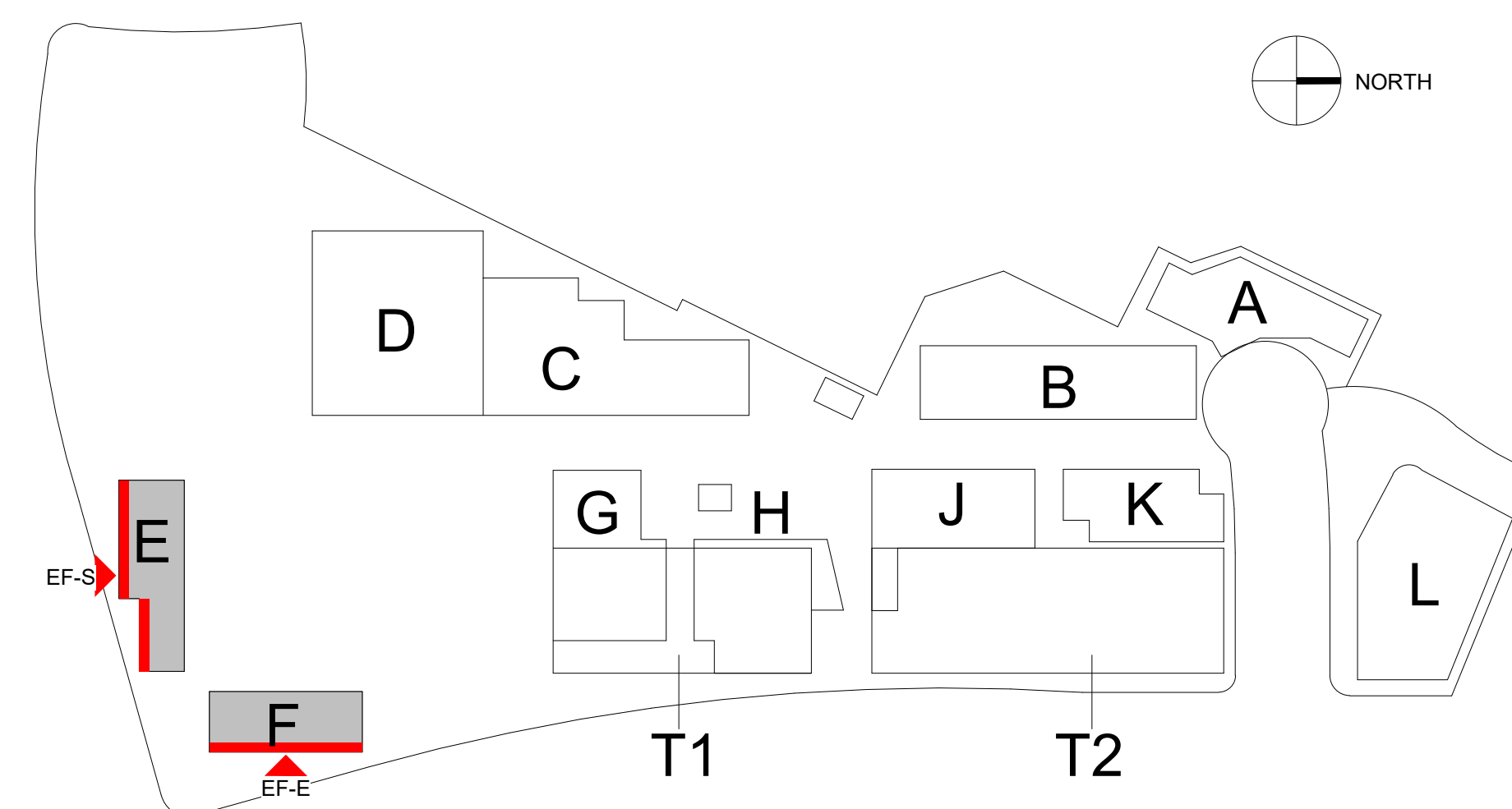


EF-S BUILDING E, F - SOUTH ELEVATION
SCALE: 1/8" = 1'-0"

MATERIAL LEGEND

- CO01 CAST-IN-PLACE CONCRETE
- FC01 FIBER CEMENT PANEL
- FC02 FIBER CEMENT PANEL
- FC03 FIBER CEMENT PANEL
- GL01 TEMPERED INSULATED STOREFRONT
- GL02 GLAZING
- GL03 INSULATED VISION GLAZING
- MT01 SPANDREL GLAZING
- MT02 MATTE BLACK COMPOSITE METAL PANEL
- MT03 PERFORATED METAL PANEL
- MT04 COPPER PANEL
- MT10 METAL
- PT01 METAL GUARDRAIL
- PT02 PAINT #1 OVER SMOOTH PLASTER
- ST01 PAINT #2 OVER SMOOTH PLASTER
- UM01 STONE PANEL CLADDING
- UM02 FULL WIDTH WHITE BRICK VENEER
- UM03 FULL WIDTH DARK BRICK VENEER
- WD01 FULL WIDTH BRICK VENEER
- WD02 WOOD SIDING
- WD03 WOOD SIDING
- WD04 WOOD PANEL
- WD06 RANDOM LENGTH WOOD SIDING
- WD07 SOLID WOOD SCREEN
- MASS TIMBER

NOTE: MATERIALS REFERENCED IN THIS DRAWING AND LEGEND ARE CONCEPTUAL. ALL EXTERIOR MATERIALS TO BE DETERMINED AT TIME OF BUILDING PERMIT.



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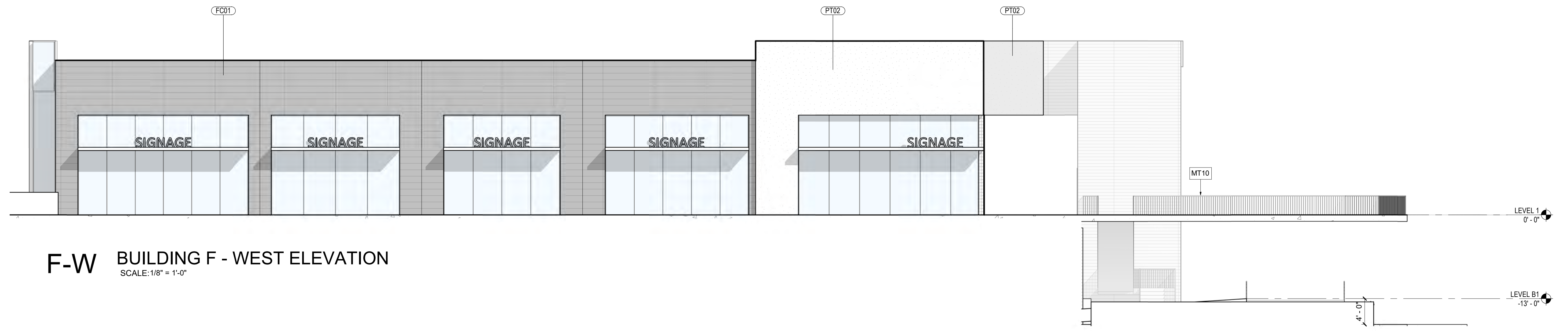
ORIGINAL DATE: 3/15/2016 (BY CALLISONRTKL)
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 REVISION DATE: 06/10/2019 (BY GENSLER)
 REVISION DATE: 10/11/2019 (BY RDC)

SHEET TITLE:
 ENLARGED ELEVATIONS OF BUILDING E, F

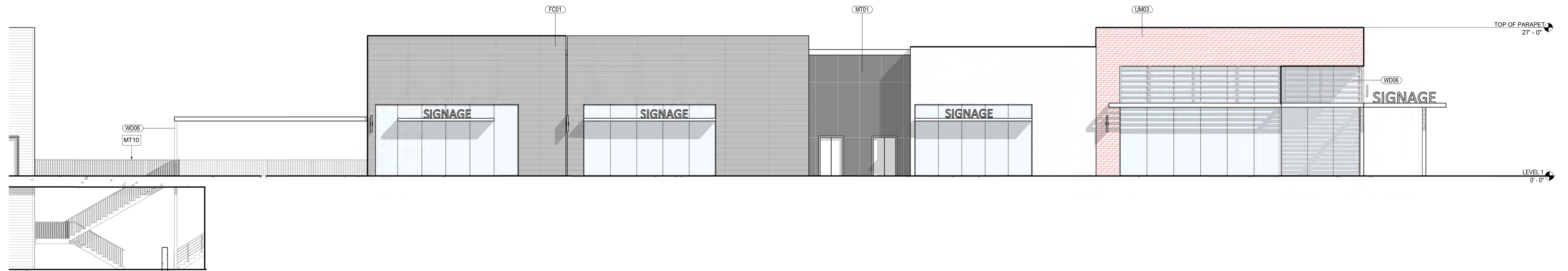
SHEET NO: **ENT2.131** 32 OF 65

PTS #: 477943
 PROJECT #: 477943

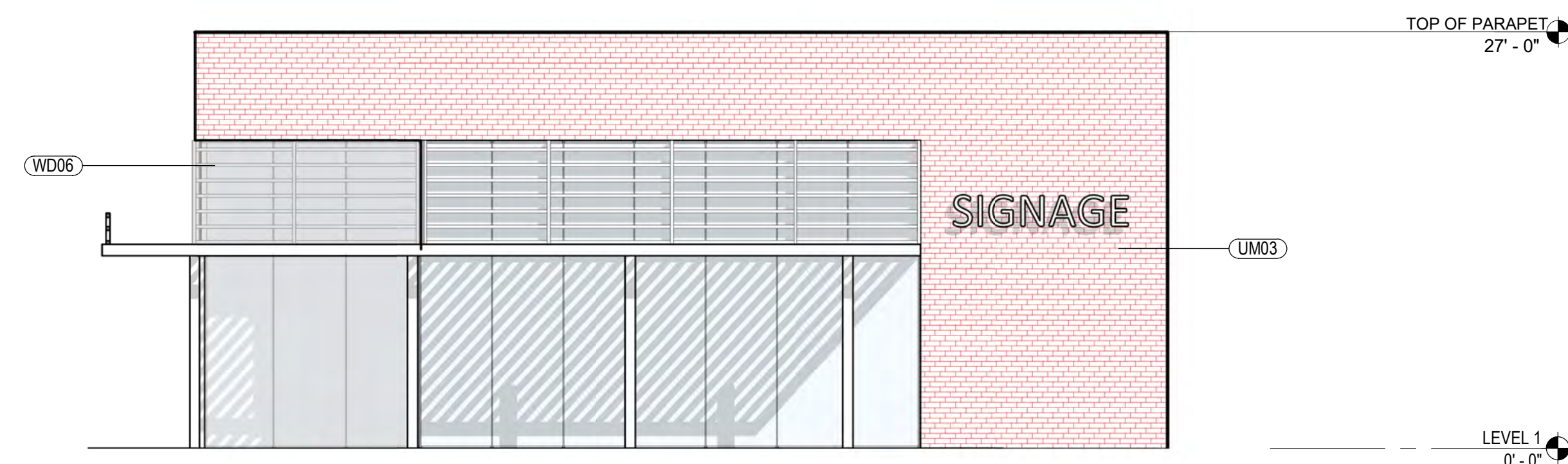
Regency Centers. REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075



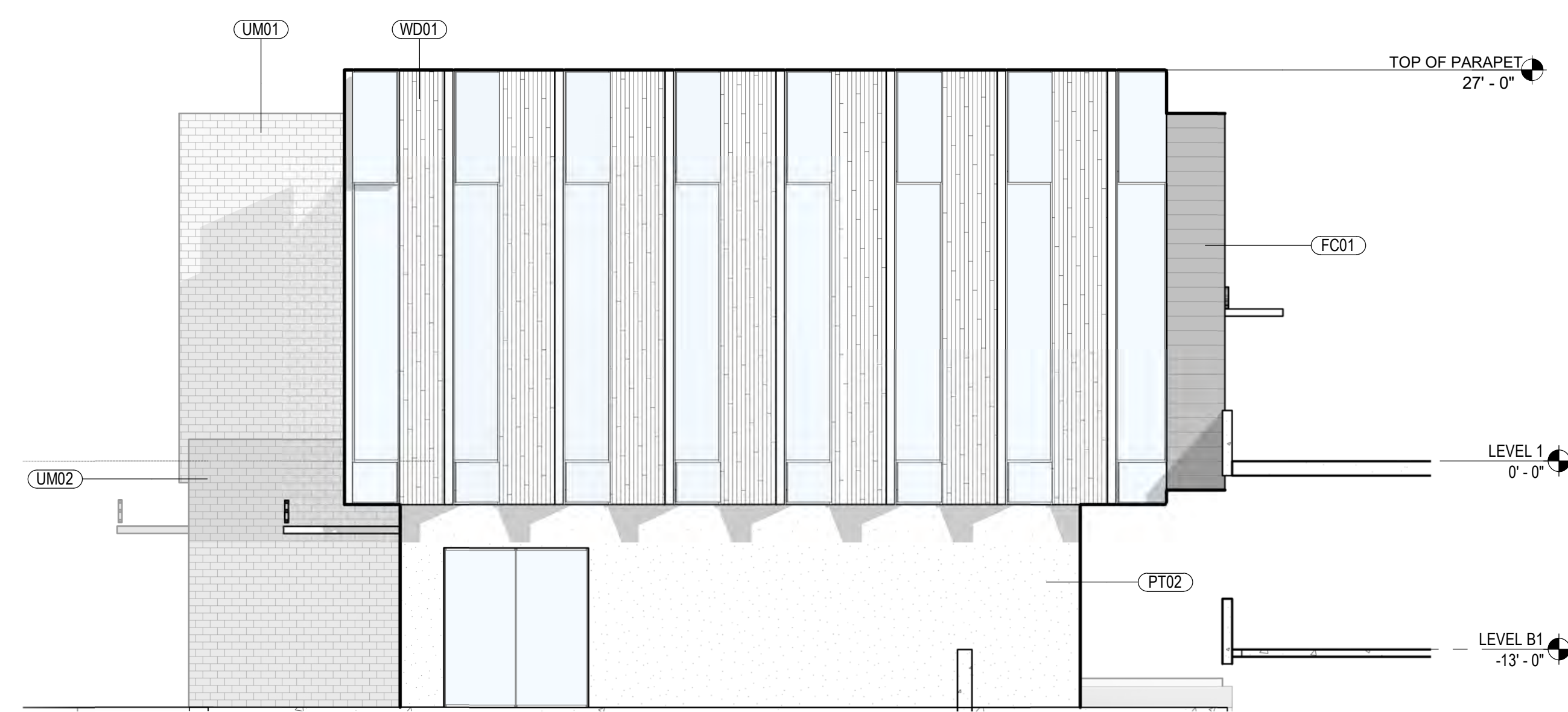
F-W BUILDING F - WEST ELEVATION
SCALE: 1/8" = 1'-0"



E-N BUILDING E - NORTH ELEVATION
SCALE: 1/8" = 1'-0"



E-W BUILDING E - WEST ELEVATION
SCALE: 1/8" = 1'-0"

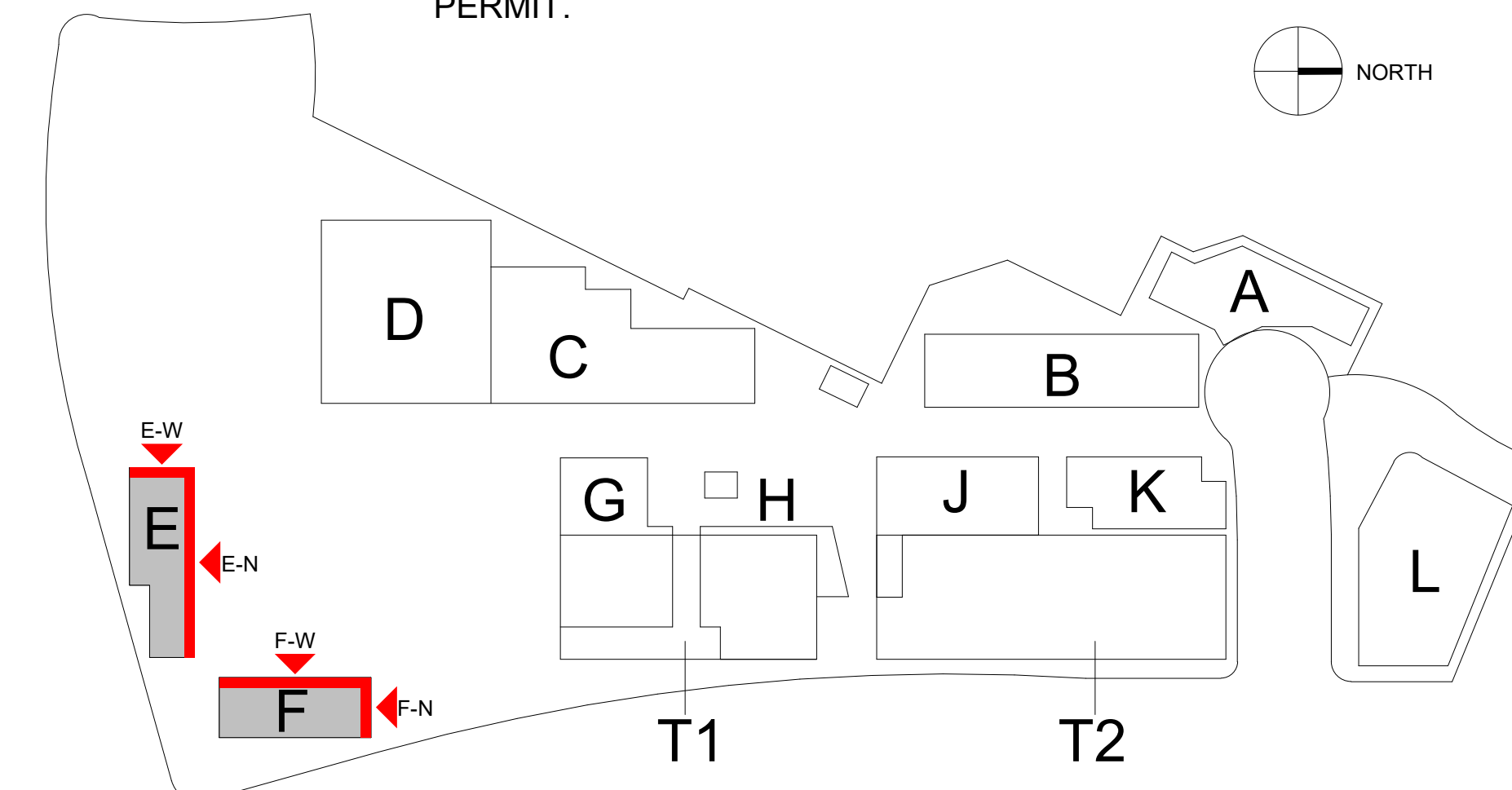


F-N BUILDING F - NORTH ELEVATION
SCALE: 1/8" = 1'-0"

MATERIAL LEGEND

- CO01 CAST-IN-PLACE CONCRETE
- FC01 FIBER CEMENT PANEL
- FC02 FIBER CEMENT PANEL
- FC03 FIBER CEMENT PANEL
- GL01 TEMPERED INSULATED STOREFRONT
- GL02 GLAZING
- GL03 INSULATED VISION GLAZING
- MT01 SPANDREL GLAZING
- MT02 MATTE BLACK COMPOSITE METAL PANEL
- MT03 PERFORATED METAL PANEL
- MT04 COPPER PANEL
- MT10 METAL
- PT01 METAL GUARDRAIL
- PT02 PAINT #1 OVER SMOOTH PLASTER
- ST01 PAINT #2 OVER SMOOTH PLASTER
- UM01 STONE PANEL CLADDING
- UM02 FULL WIDTH WHITE BRICK VENEER
- UM03 FULL WIDTH DARK BRICK VENEER
- WD01 FULL WIDTH BRICK VENEER
- WD02 WOOD SIDING
- WD03 WOOD SIDING
- WD04 WOOD PANEL
- WD06 RANDOM LENGTH WOOD SIDING
- WD07 SOLID WOOD SCREEN
- MASS TIMBER

NOTE: MATERIALS REFERENCED IN THIS DRAWING AND LEGEND ARE CONCEPTUAL. ALL EXTERIOR MATERIALS TO BE DETERMINED AT TIME OF BUILDING PERMIT.



PREPARED BY:	rdc.
NAME:	245 EAST THIRD STREET
ADDRESS:	LONG BEACH, CA 90802
PHONE:	562.628.8000
ORIGINAL DATE:	3/15/2016 (BY CALLISONRTKL)
REVISION DATE:	7/28/2016 (BY CALLISONRTKL)
REVISION DATE:	12/12/2016 (BY CALLISONRTKL)
REVISION DATE:	06/10/2019 (BY GENSLER)
REVISION DATE:	10/11/2019 (BY RDC)

SHEET TITLE:
ENLARGED ELEVATIONS OF BUILDING E, F

SHEET NO: ENT2.132 33 OF 65

PTS #: 477943
PROJECT #: 477943

Regency Centers. REGENCY CENTERS
420 STEVENS AVE SUITE 320
SOLANA BEACH, CA 92075

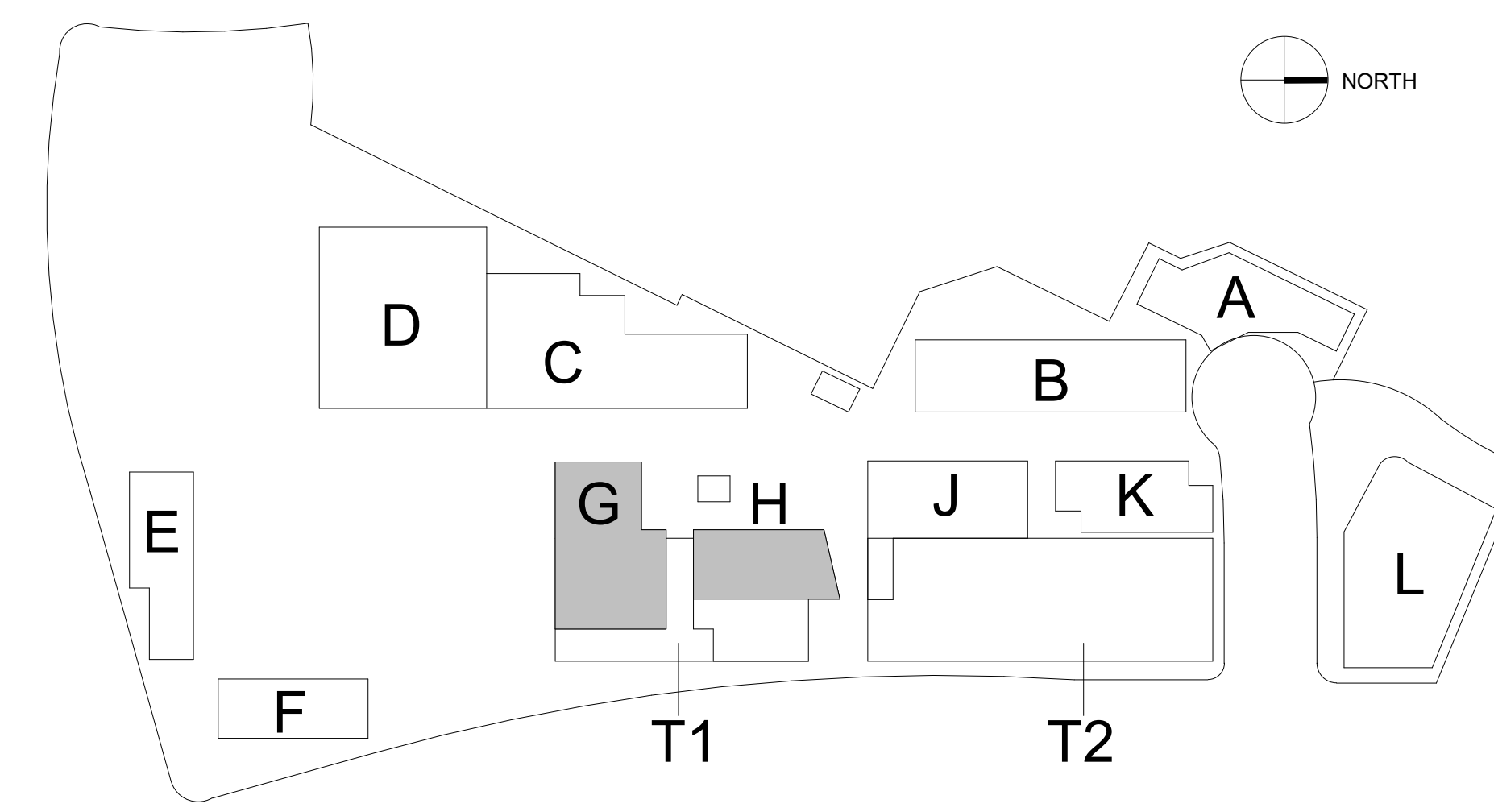
PROJECT NUMBER: 19-187



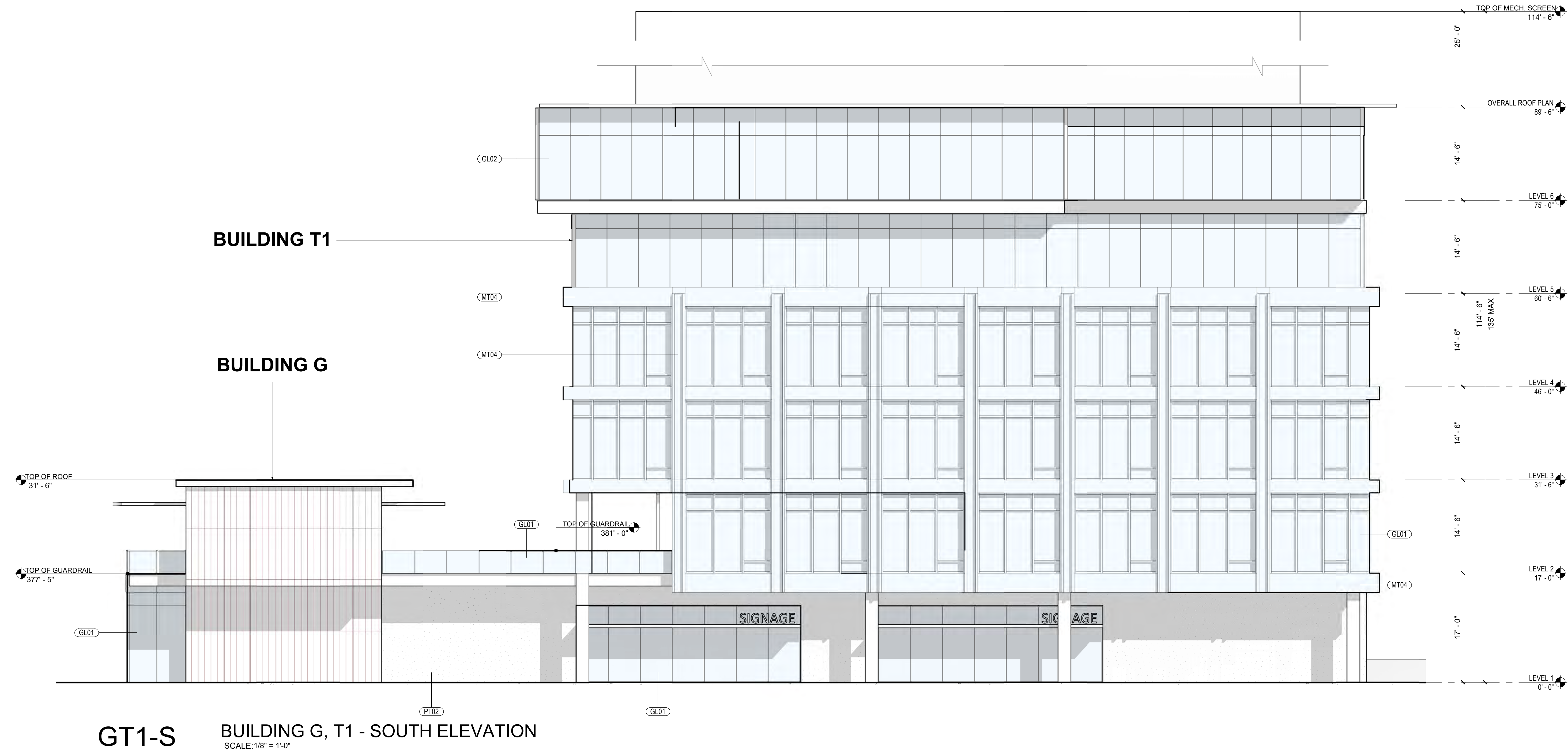
PLAN LEGEND

- Retail
- Office
- Service
- Elevator

GH BUILDING PLAN G, H
SCALE: 1/16" = 1'-0"



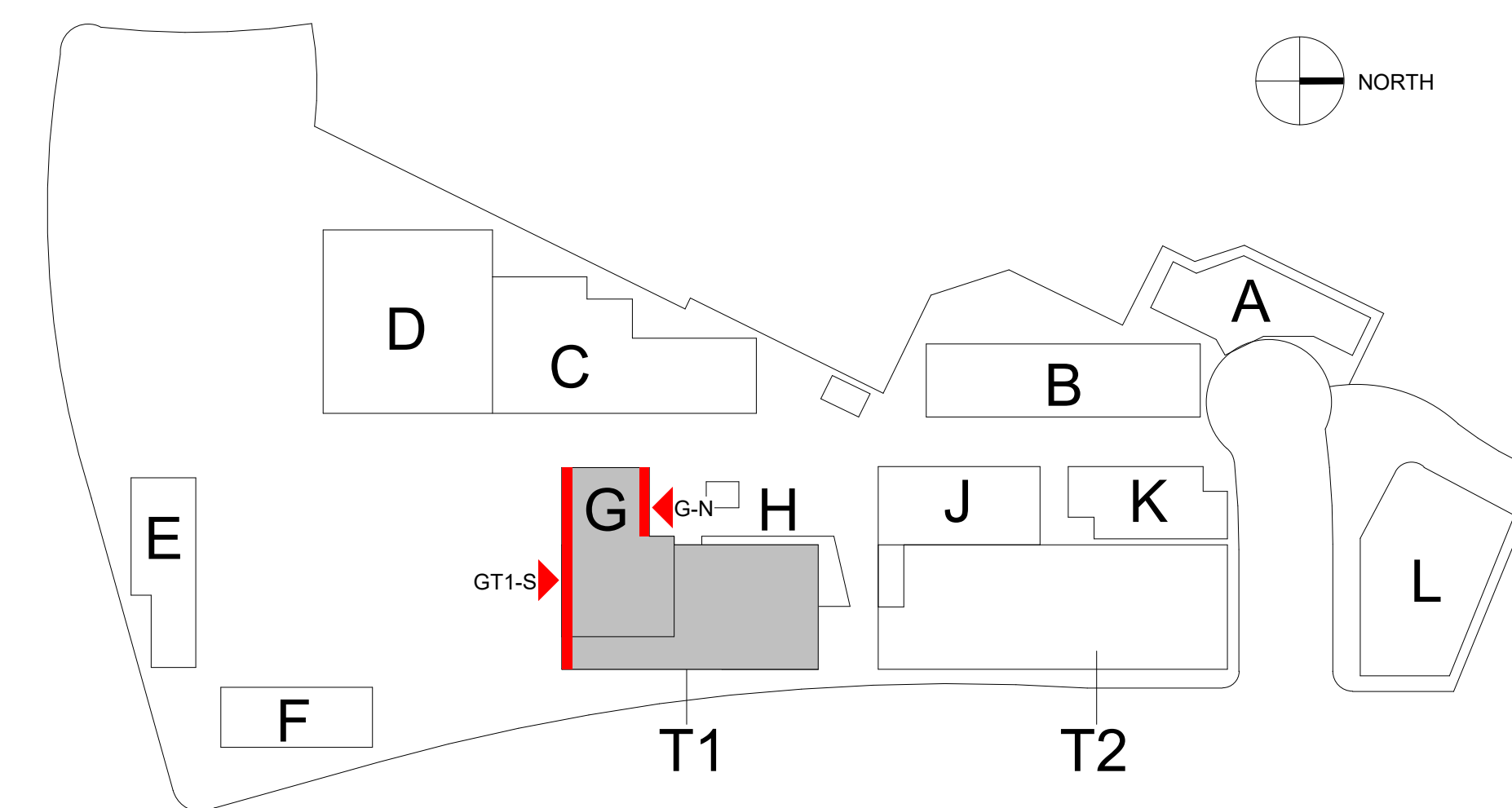
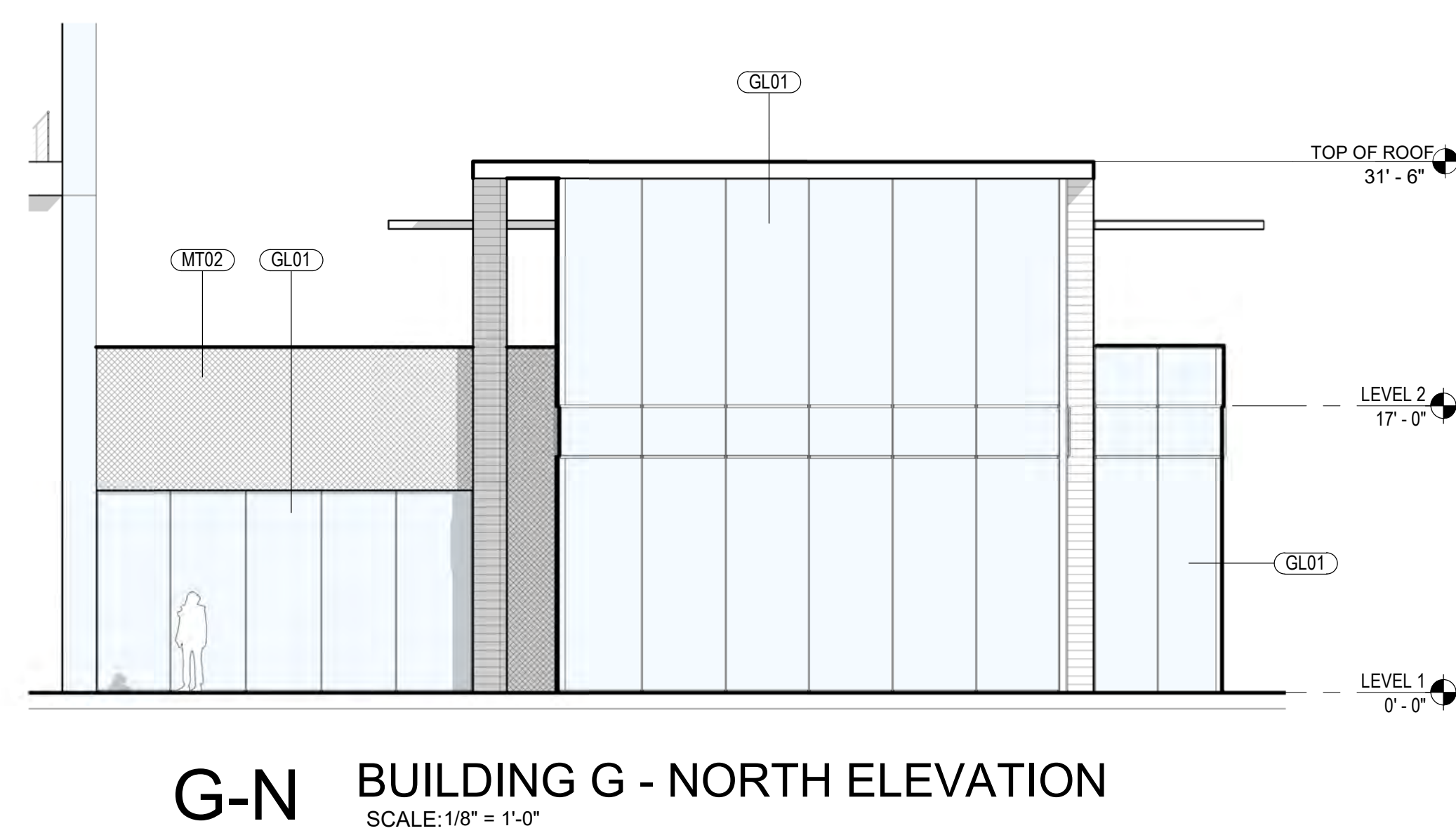
PREPARED BY:	rdc.
NAME:	245 EAST THIRD STREET
ADDRESS:	LONG BEACH, CA 90802
PHONE:	562.628.8000
ORIGINAL DATE:	3/15/2016 (BY CALLISONRTKL)
REVISION DATE:	7/28/2016 (BY CALLISONRTKL)
REVISION DATE:	12/12/2016 (BY CALLISONRTKL)
REVISION DATE:	06/10/2019 (BY GENSLER)
REVISION DATE:	10/11/2019 (BY RDC)
SHEET TITLE:	ENLARGED PLAN OF BUILDING G, H
SHEET NO:	ENT2.140 34 OF 65
PTS #:	477943
PROJECT #:	477943
Regency Centers.	REGENCY CENTERS 420 STEVENS AVE SUITE 320 SOLANA BEACH, CA 92075
PROJECT NUMBER:	19-187



MATERIAL LEGEND

CO01	CAST-IN-PLACE CONCRETE
FC01	FIBER CEMENT PANEL
FC02	FIBER CEMENT PANEL
FC03	FIBER CEMENT PANEL
GL01	TEMPERED INSULATED STOREFRONT
GL02	GLAZING
GL03	INSULATED VISION GLAZING
MT01	SPANDREL GLAZING
MT02	MATTE BLACK COMPOSITE METAL PANEL
MT03	PERFORATED METAL PANEL
MT04	COPPER PANEL
MT10	METAL
PT01	METAL GUARDRAIL
PT02	PAINT #1 OVER SMOOTH PLASTER
ST01	PAINT #2 OVER SMOOTH PLASTER
UM01	STONE PANEL CLADDING
UM02	FULL WIDTH WHITE BRICK VENEER
UM03	FULL WIDTH DARK BRICK VENEER
WD01	FULL WIDTH BRICK VENEER
WD02	WOOD SIDING
WD03	WOOD SIDING
WD04	WOOD PANEL
WD06	RANDOM LENGTH WOOD SIDING
WD07	SOLID WOOD SCREEN
	MASS TIMBER

NOTE: MATERIALS REFERENCED IN THIS DRAWING AND LEGEND ARE CONCEPTUAL. ALL EXTERIOR MATERIALS TO BE DETERMINED AT TIME OF BUILDING PERMIT.



PREPARED BY: **rdc.**

NAME: _____

ADDRESS: 245 EAST THIRD STREET
LONG BEACH, CA 90802

PHONE: 562.628.8000

ORIGINAL DATE: 3/15/2016 (BY CALLISONRTKL)

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REVISION DATE: 12/12/2016 (BY CALLISONRTKL)

REVISION DATE: 06/10/2019 (BY GENSLER)

REVISION DATE: 10/11/2019 (BY RDC)

SHEET TITLE:
ENLARGED ELEVATION OF BUILDING G, T1

SHEET NO: ENT2.141 35 OF 65

PTS #: 477943

PROJECT #: 477943

Regency Centers. REGENCY CENTERS
420 STEVENS AVE SUITE 320
SOLANA BEACH, CA 92075



T1-E BUILDING T1 - EAST ELEVATION
SCALE: 1/8" = 1'-0"

MATERIAL LEGEND

- CO01 CAST-IN-PLACE CONCRETE
- FC01 FIBER CEMENT PANEL
- FC02 FIBER CEMENT PANEL
- FC03 FIBER CEMENT PANEL
- GL01 TEMPERED INSULATED STOREFRONT
- GL02 GLAZING
- GL03 INSULATED VISION GLAZING
- MT01 SPANDREL GLAZING
- MT02 MATTE BLACK COMPOSITE METAL PANEL
- MT03 PERFORATED METAL PANEL
- MT04 COPPER PANEL
- MT10 METAL
- PT01 METAL GUARDRAIL
- PT02 PAINT #1 OVER SMOOTH PLASTER
- ST01 PAINT #2 OVER SMOOTH PLASTER
- UM01 STONE PANEL CLADDING
- UM02 FULL WIDTH WHITE BRICK VENEER
- UM03 FULL WIDTH DARK BRICK VENEER
- WD01 FULL WIDTH BRICK VENEER
- WD02 WOOD SIDING
- WD03 WOOD SIDING
- WD04 WOOD PANEL
- WD06 RANDOM LENGTH WOOD SIDING
- WD07 SOLID WOOD SCREEN
- MASS TIMBER

NOTE: MATERIALS REFERENCED IN THIS DRAWING AND LEGEND ARE CONCEPTUAL. ALL EXTERIOR MATERIALS TO BE DETERMINED AT TIME OF BUILDING PERMIT.

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 NAME:
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 LONG BEACH, CA 90802
 PHONE: 562.628.8000

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 REVISION DATE: 06/10/2019 (BY GENSLER)
 REVISION DATE: 10/11/2019 (BY RDC)

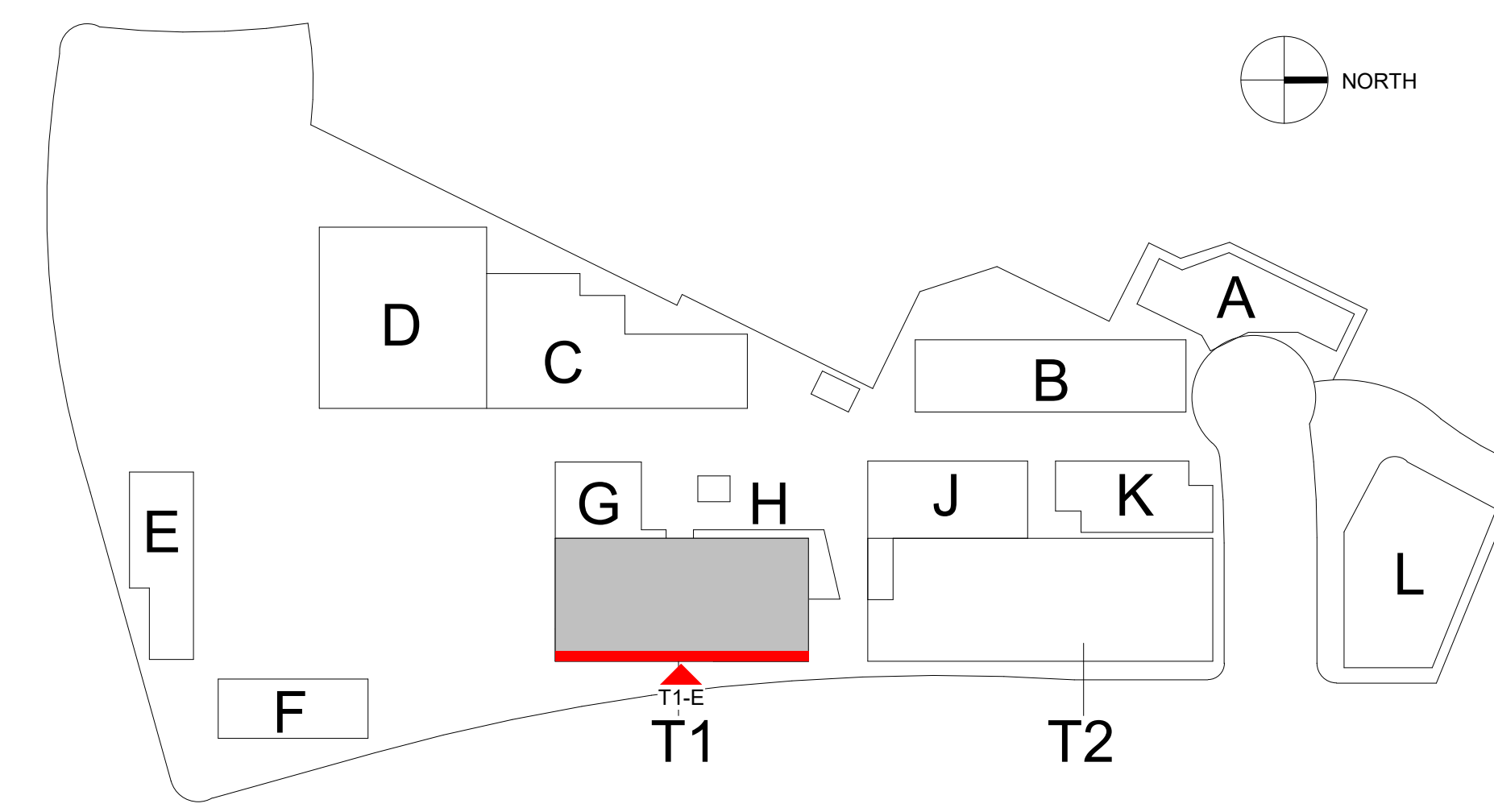
SHEET TITLE:
 ENLARGED ELEVATION OF BUILDING G, H, T1

SHEET NO: **ENT2.142** 36 OF 65

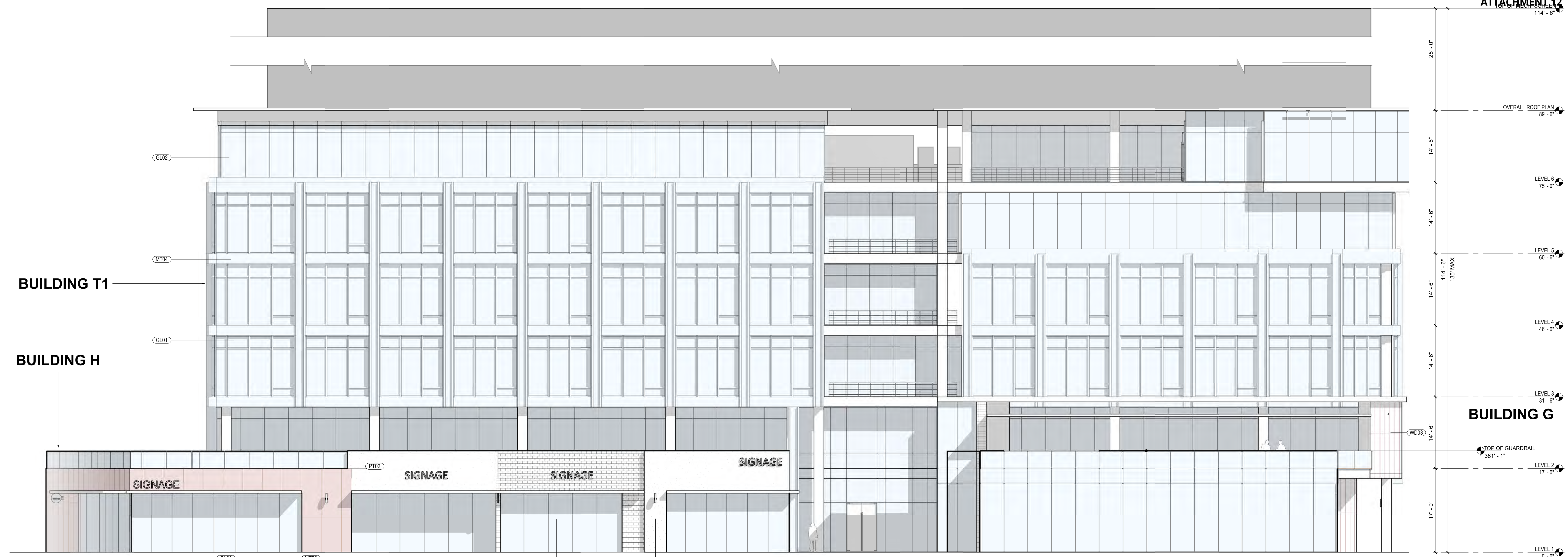
PTS #: 477943
 PROJECT #: 477943

Regency Centers. REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

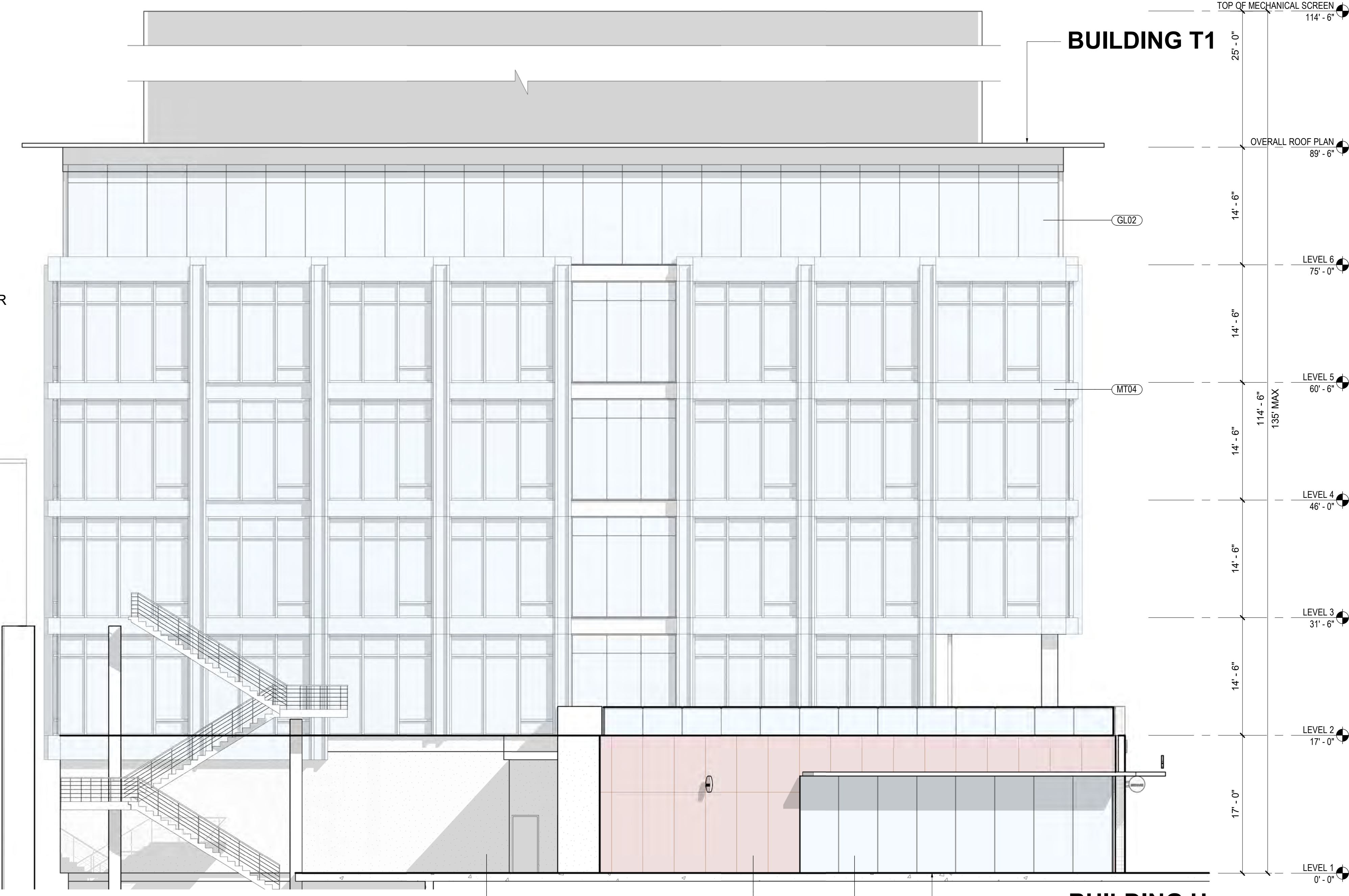
PROJECT NUMBER: 19-187



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GHT1-W BUILDING G, H, T1 - WEST ELEVATION
SCALE: 1/8" = 1'-0"



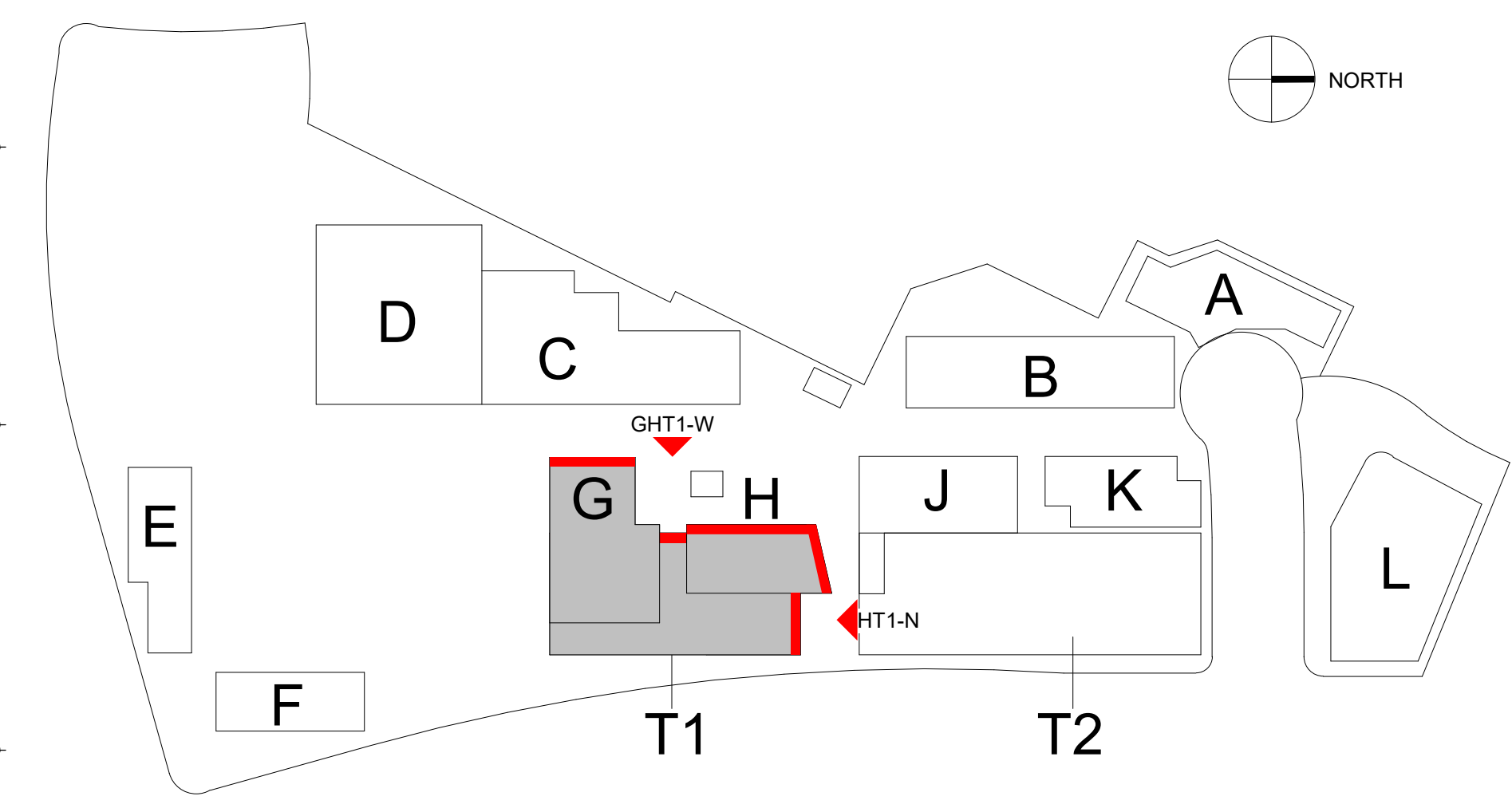
HT1-N BUILDING H, T1 - NORTH ELEVATION
SCALE: 1/8" = 1'-0"

2 STORY ELEVATOR
BY SAN DIEGO
METROPOLITAN
TRANSIT SYSTEM

MATERIAL LEGEND

- CO01 CAST-IN-PLACE CONCRETE
- FC01 FIBER CEMENT PANEL
- FC02 FIBER CEMENT PANEL
- FC03 FIBER CEMENT PANEL
- GL01 TEMPERED INSULATED STOREFRONT GLAZING
- GL02 GLAZING
- GL03 INSULATED VISION GLAZING
- MT01 SPANDREL GLAZING
- MT02 MATTE BLACK COMPOSITE METAL PANEL
- MT03 PERFORATED METAL PANEL
- MT04 COPPER PANEL
- MT10 METAL
- PT01 METAL GUARDRAIL
- PT02 PAINT #1 OVER SMOOTH PLASTER
- ST01 PAINT #2 OVER SMOOTH PLASTER
- UM01 STONE PANEL CLADDING
- UM02 FULL WIDTH WHITE BRICK VENEER
- UM03 FULL WIDTH DARK BRICK VENEER
- WD01 FULL WIDTH BRICK VENEER
- WD02 WOOD SIDING
- WD03 WOOD SIDING
- WD04 WOOD PANEL
- WD06 RANDOM LENGTH WOOD SIDING
- WD07 SOLID WOOD SCREEN

NOTE: MATERIALS REFERENCED IN THIS DRAWING AND LEGEND ARE CONCEPTUAL. ALL EXTERIOR MATERIALS TO BE DETERMINED AT TIME OF BUILDING PERMIT.



PREPARED BY: **rdc.**
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PHONE: 562.628.8000

ORIGINAL DATE: 3/15/2016 (BY CALLISONRTKL)
REVISION DATE: 7/28/2016 (BY CALLISONRTKL)
REVISION DATE: 12/12/2016 (BY CALLISONRTKL)
REVISION DATE: 06/10/2019 (BY GENSLER)
REVISION DATE: 10/11/2019 (BY RDC)

SHEET TITLE:
ENLARGED ELEVATIONS OF BUILDING G, H, T1

SHEET NO: **ENT2.143** 37 OF 65

PTS #: 477943
PROJECT #: 477943

Regency Centers REGENCY CENTERS
420 STEVENS AVE SUITE 320
SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187



PLAN LEGEND

- Retail
- Office
- Service
- Retail Circulation
- E Elevator

JK BUILDING PLAN J, K
SCALE: 1/16" = 1'-0"

PREPARED BY:	rdc.
NAME:	245 EAST THIRD STREET
ADDRESS:	LONG BEACH, CA 90802
PHONE:	562.628.8000

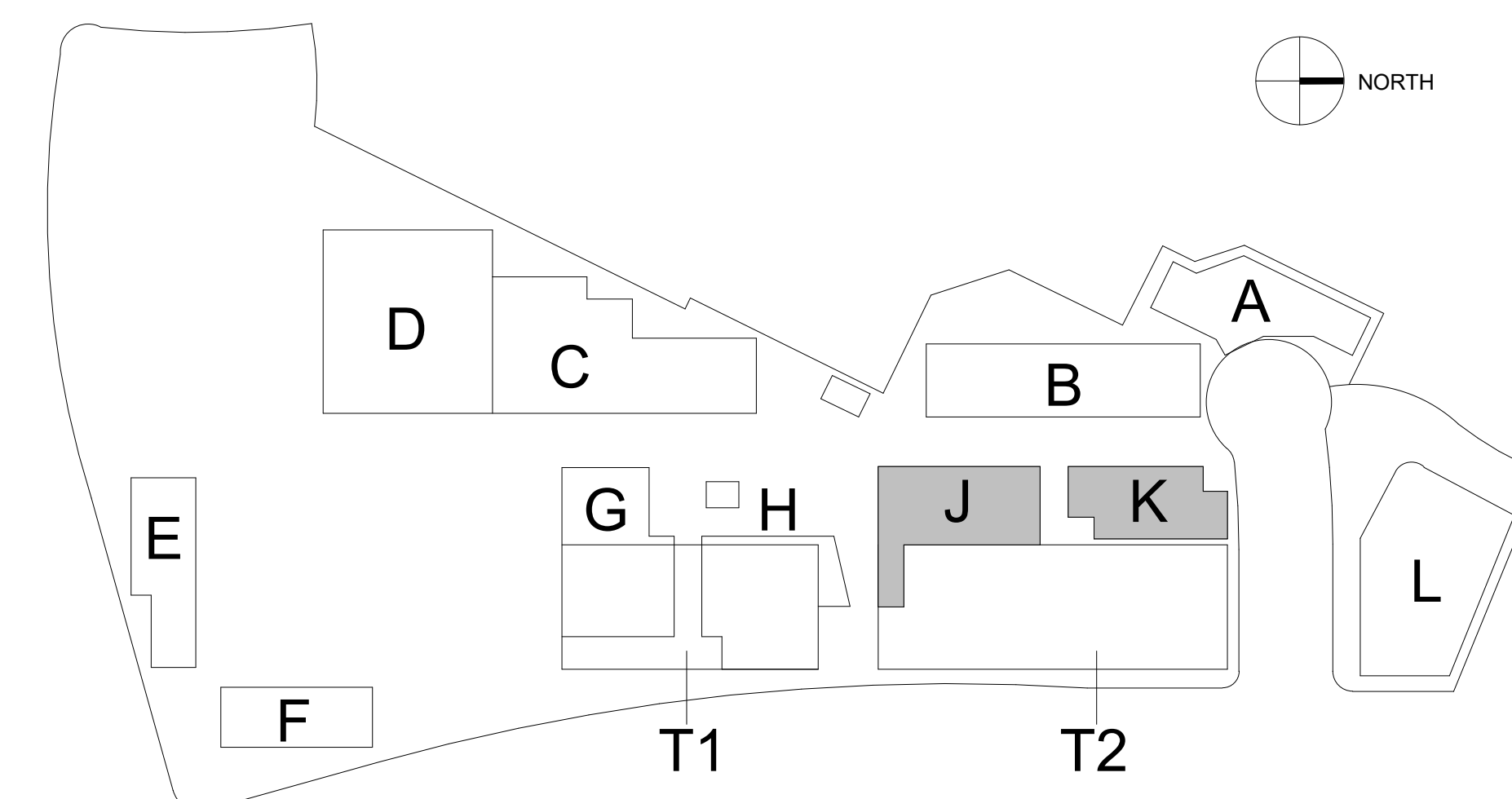
ORIGINAL DATE:	3/15/2016 (BY CALLISONRTKL)
REVISION DATE:	7/28/2016 (BY CALLISONRTKL)
REVISION DATE:	12/12/2016 (BY CALLISONRTKL)
REVISION DATE:	06/10/2019 (BY GENSLER)
REVISION DATE:	10/11/2019 (BY RDC)
REVISION DATE:	09/01/2020 (BY RDC)

SHEET TITLE:
ENLARGED PLAN OF BUILDING J, K

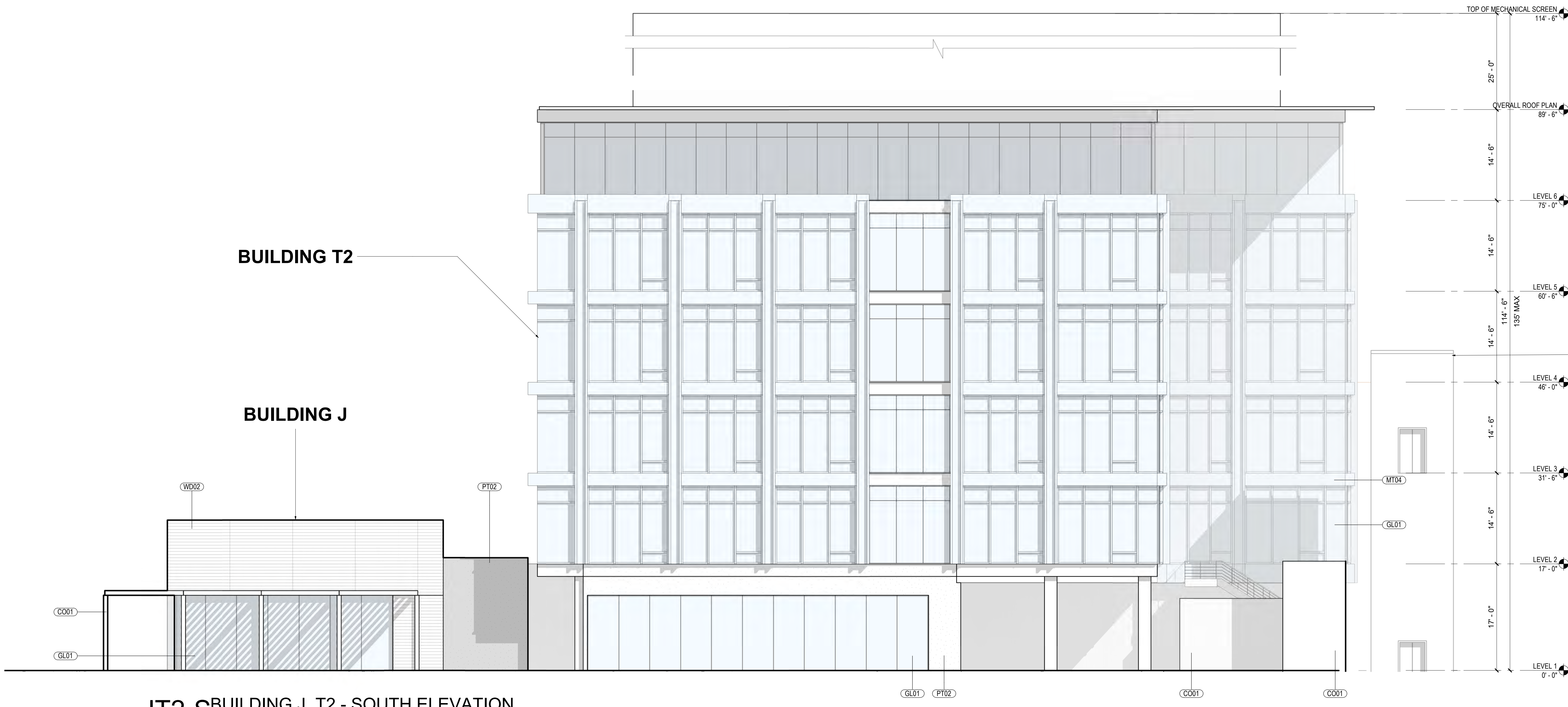
SHEET NO: ENT2.150 38 OF 65

PTS #: 477943
PROJECT #: 477943

Regency Centers REGENCY CENTERS
420 STEVENS AVE SUITE 320
SOLANA BEACH, CA 92075



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JT2-S BUILDING J, T2 - SOUTH ELEVATION
SCALE: 1/8" = 1'-0"

MATERIAL LEGEND

CO01	CAST-IN-PLACE CONCRETE
FC01	FIBER CEMENT PANEL
FC02	FIBER CEMENT PANEL
FC03	FIBER CEMENT PANEL
GL01	TEMPERED INSULATED STOREFRONT GLAZING
GL02	GLAZING
GL03	INSULATED VISION GLAZING
MT01	SPANDREL GLAZING
MT02	MATTE BLACK COMPOSITE METAL PANEL
MT03	PERFORATED METAL PANEL
MT04	COPPER PANEL
MT10	METAL
PT01	METAL GUARDRAIL
PT02	PAINT #1 OVER SMOOTH PLASTER
ST01	PAINT #2 OVER SMOOTH PLASTER
UM01	STONE PANEL CLADDING
UM02	FULL WIDTH WHITE BRICK VENEER
UM03	FULL WIDTH DARK BRICK VENEER
WD01	FULL WIDTH BRICK VENEER
WD02	WOOD SIDING
WD03	WOOD SIDING
WD04	WOOD PANEL
WD06	RANDOM LENGTH WOOD SIDING
WD07	SOLID WOOD SCREEN MASS TIMBER

NOTE: MATERIALS REFERENCED IN THIS DRAWING AND LEGEND ARE CONCEPTUAL. ALL EXTERIOR MATERIALS TO BE DETERMINED AT TIME OF BUILDING PERMIT.

PREPARED BY: **rdc.**

NAME: _____

ADDRESS: 245 EAST THIRD STREET
LONG BEACH, CA 90802

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REVISION DATE: 10/11/2019 (BY RDC)

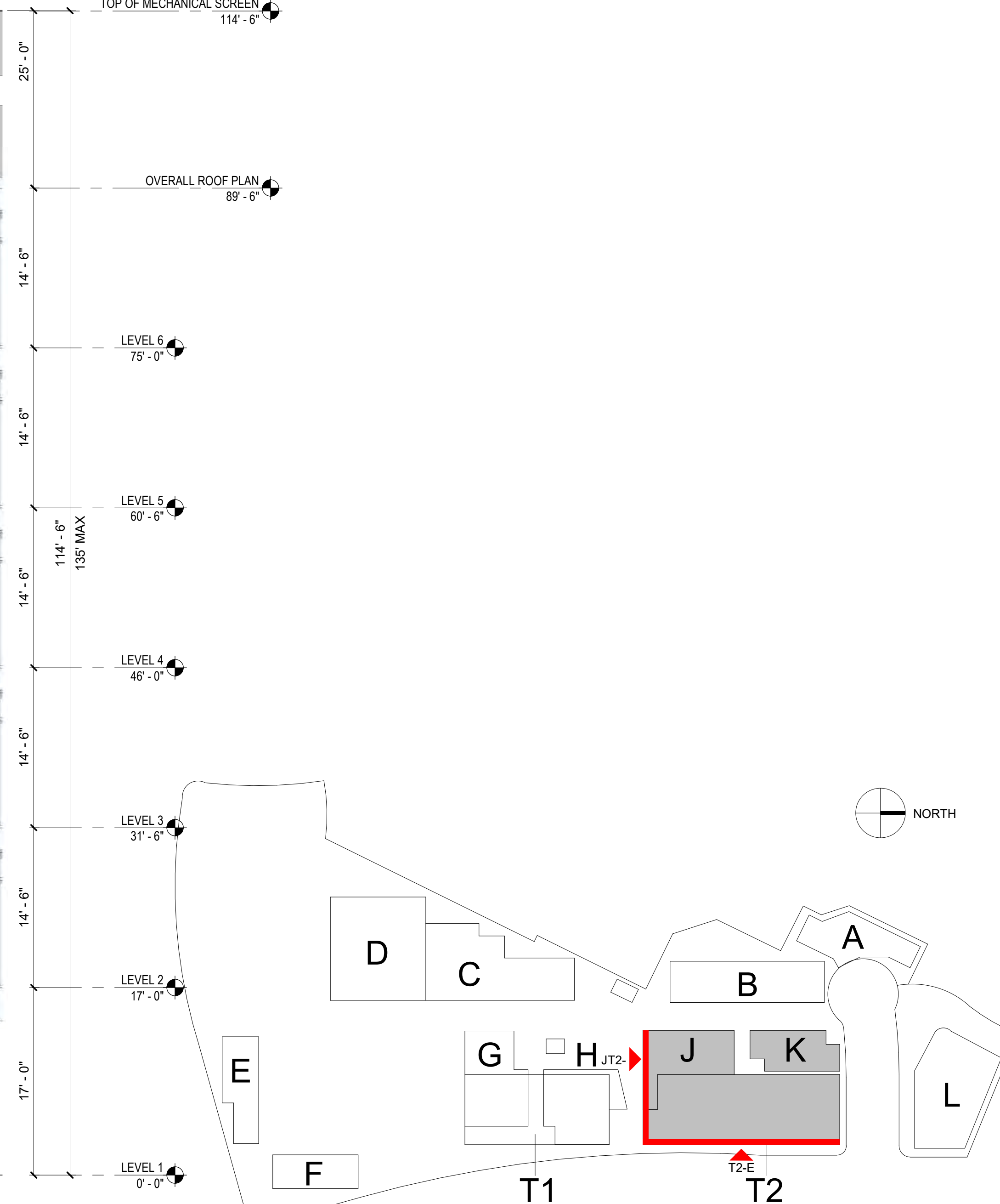
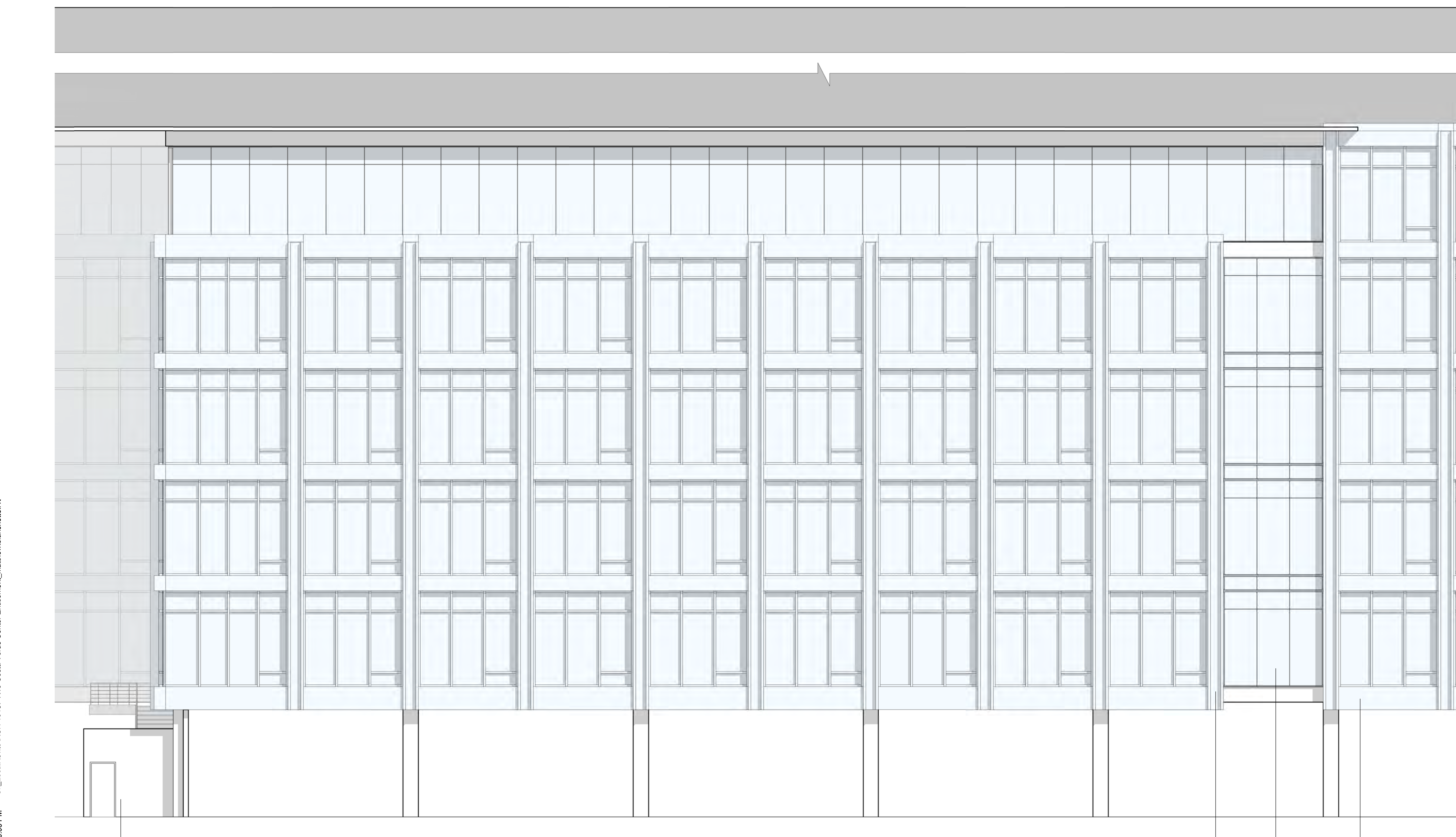
SHEET TITLE:
ENLARGED ELEVATION OF BUILDING J, T2

SHEET NO: ENT2.151 39 OF 65

PTS #: 477943
PROJECT #: 477943

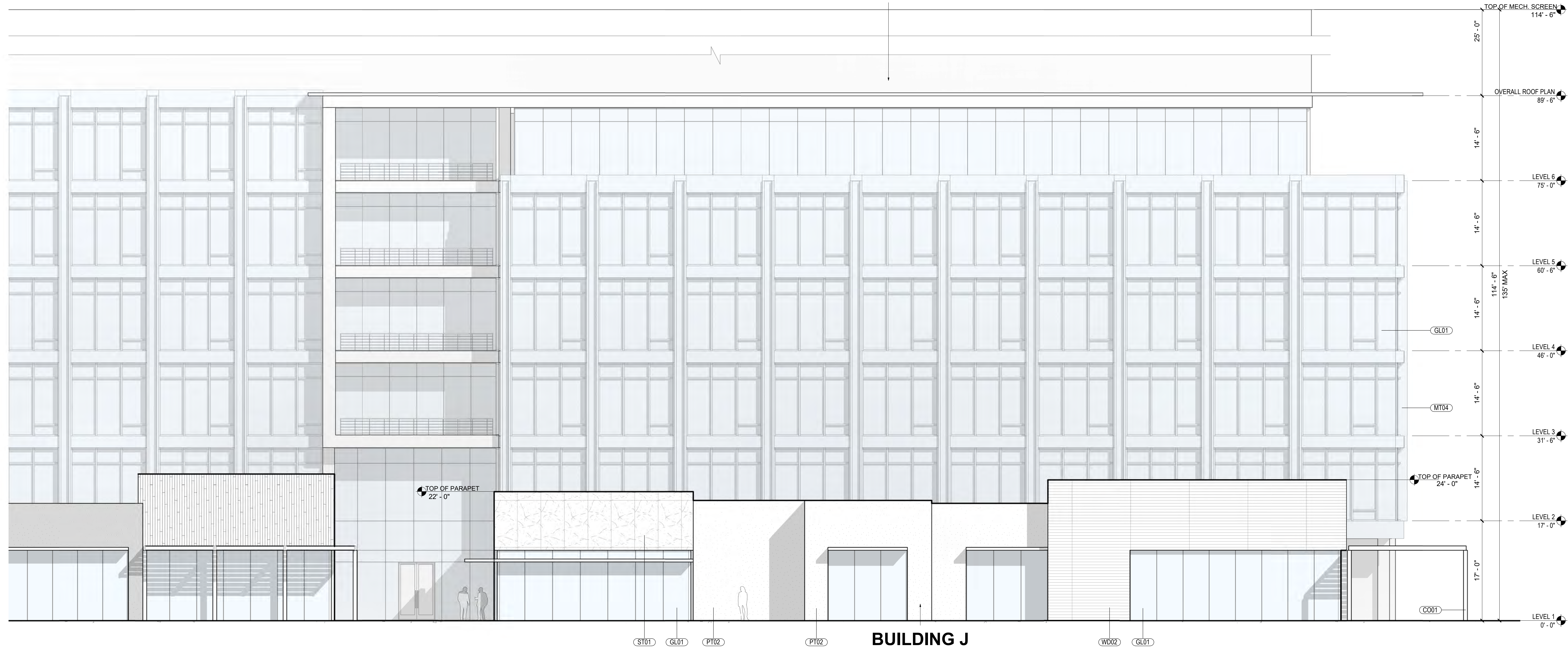
Regency Centers REGENCY CENTERS
420 STEVENS AVE SUITE 320
SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187



T2-E BUILDING T2 - EAST ELEVATION
SCALE: 1/8" = 1'-0"

BUILDING T2



MATERIAL LEGEND

CO01	CAST-IN-PLACE CONCRETE
FC01	FIBER CEMENT PANEL
FC02	FIBER CEMENT PANEL
FC03	FIBER CEMENT PANEL
GL01	TEMPERED INSULATED STOREFRONT GLAZING
GL02	GLAZING
GL03	INSULATED VISION GLAZING
MT01	SPANDREL GLAZING
MT02	MATTE BLACK COMPOSITE METAL PANEL
MT03	PERFORATED METAL PANEL
MT04	COPPER PANEL
MT10	METAL
PT01	METAL GUARDRAIL
PT02	PAINT #1 OVER SMOOTH PLASTER
ST01	PAINT #2 OVER SMOOTH PLASTER
UM01	STONE PANEL CLADDING
UM02	FULL WIDTH WHITE BRICK VENEER
UM03	FULL WIDTH DARK BRICK VENEER
WD01	FULL WIDTH BRICK VENEER
WD02	WOOD SIDING
WD03	WOOD SIDING
WD04	WOOD PANEL
WD06	RANDOM LENGTH WOOD SIDING
WD07	SOLID WOOD SCREEN
	MASS TIMBER

NOTE: MATERIALS REFERENCED IN THIS DRAWING AND LEGEND ARE CONCEPTUAL. ALL EXTERIOR MATERIALS TO BE DETERMINED AT TIME OF BUILDING PERMIT.

JT2-W BUILDING J, T2 - WEST ELEVATION
SCALE: 1/8" = 1'-0"

PREPARED BY: **rdc.**
 NAME:
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 PHONE: 562.628.8000

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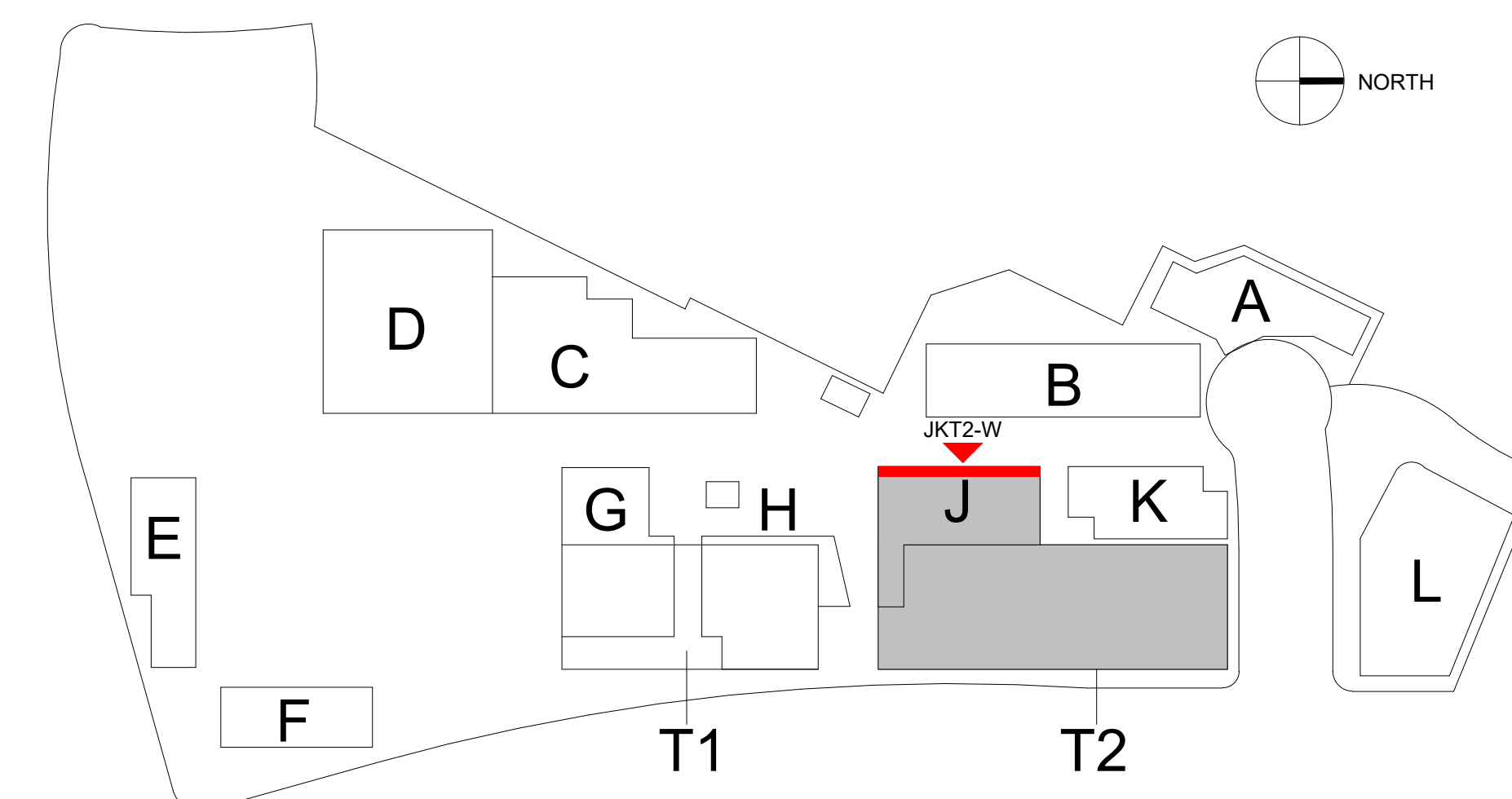
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 ENLARGED ELEVATION OF BUILDING J, T2

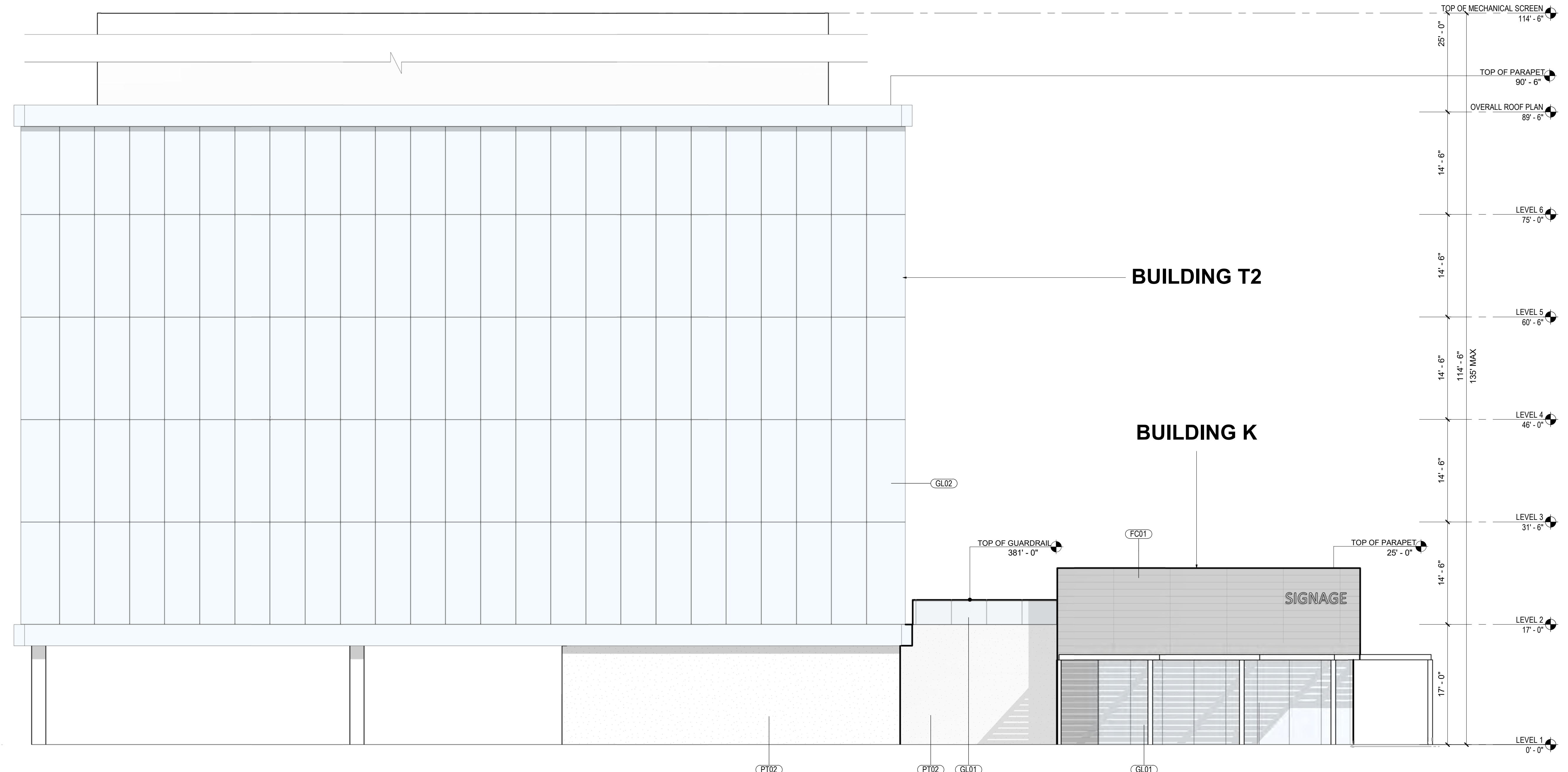
SHEET NO: ENT2.152 40 OF 65

PTS #: 477943
 PROJECT #: 477943

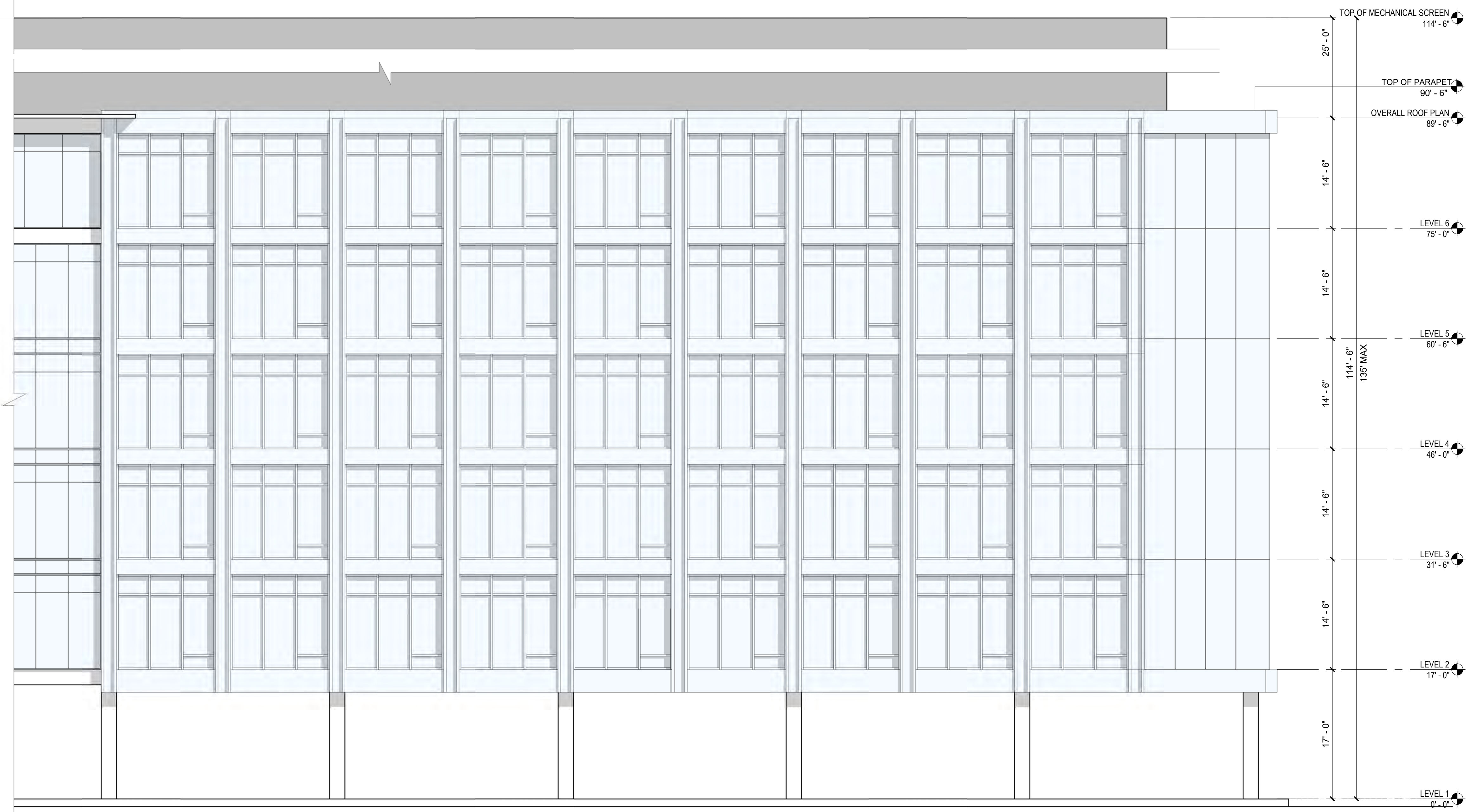
Regency Centers REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187





KT2-N BUILDING K, T2 - NORTH ELEVATION
SCALE: 1/8" = 1'-0"



T2-E BUILDING T2 - EAST ELEVATION
SCALE: 1/8" = 1'-0"

MATERIAL LEGEND

CO01	CAST-IN-PLACE CONCRETE
FC01	FIBER CEMENT PANEL
FC02	FIBER CEMENT PANEL
FC03	FIBER CEMENT PANEL
GL01	TEMPERED INSULATED STOREFRONT GLAZING
GL02	GLAZING
GL03	INSULATED VISION GLAZING
MT01	SPANDREL GLAZING
MT02	MATTE BLACK COMPOSITE METAL PANEL
MT03	PERFORATED METAL PANEL
MT04	COPPER PANEL
MT10	METAL
PT01	METAL GUARDRAIL
PT02	PAINT #1 OVER SMOOTH PLASTER
ST01	PAINT #2 OVER SMOOTH PLASTER
UM01	STONE PANEL CLADDING
UM02	FULL WIDTH WHITE BRICK VENEER
UM03	FULL WIDTH DARK BRICK VENEER
WD01	FULL WIDTH BRICK VENEER
WD02	WOOD SIDING
WD03	WOOD SIDING
WD04	WOOD PANEL
WD06	RANDOM LENGTH WOOD SIDING
WD07	SOLID WOOD SCREEN
	MASS TIMBER

NOTE: MATERIALS REFERENCED IN THIS DRAWING AND LEGEND ARE CONCEPTUAL. ALL EXTERIOR MATERIALS TO BE DETERMINED AT TIME OF BUILDING PERMIT.

PREPARED BY:	rdc.
NAME:	
ADDRESS:	245 EAST THIRD STREET LONG BEACH, CA 90802
PHONE:	562.628.8000

ORIGINAL DATE:	3/15/2016 (BY CALLISONRTKL)
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REVISION DATE:	06/10/2019 (BY GENSLER)
REVISION DATE:	10/11/2019 (BY RDC)

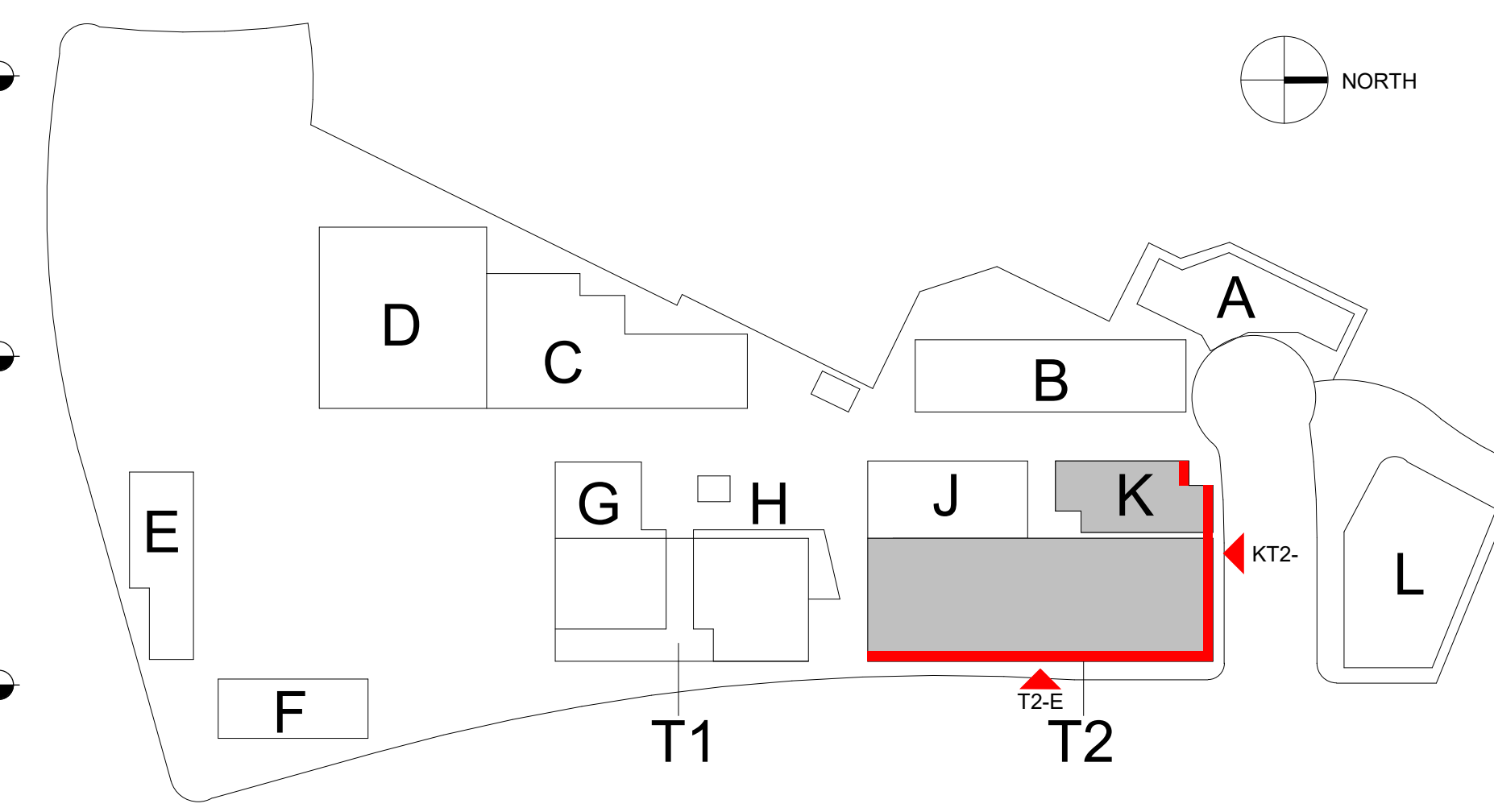
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ENLARGED ELEVATION OF BUILDING K, T2

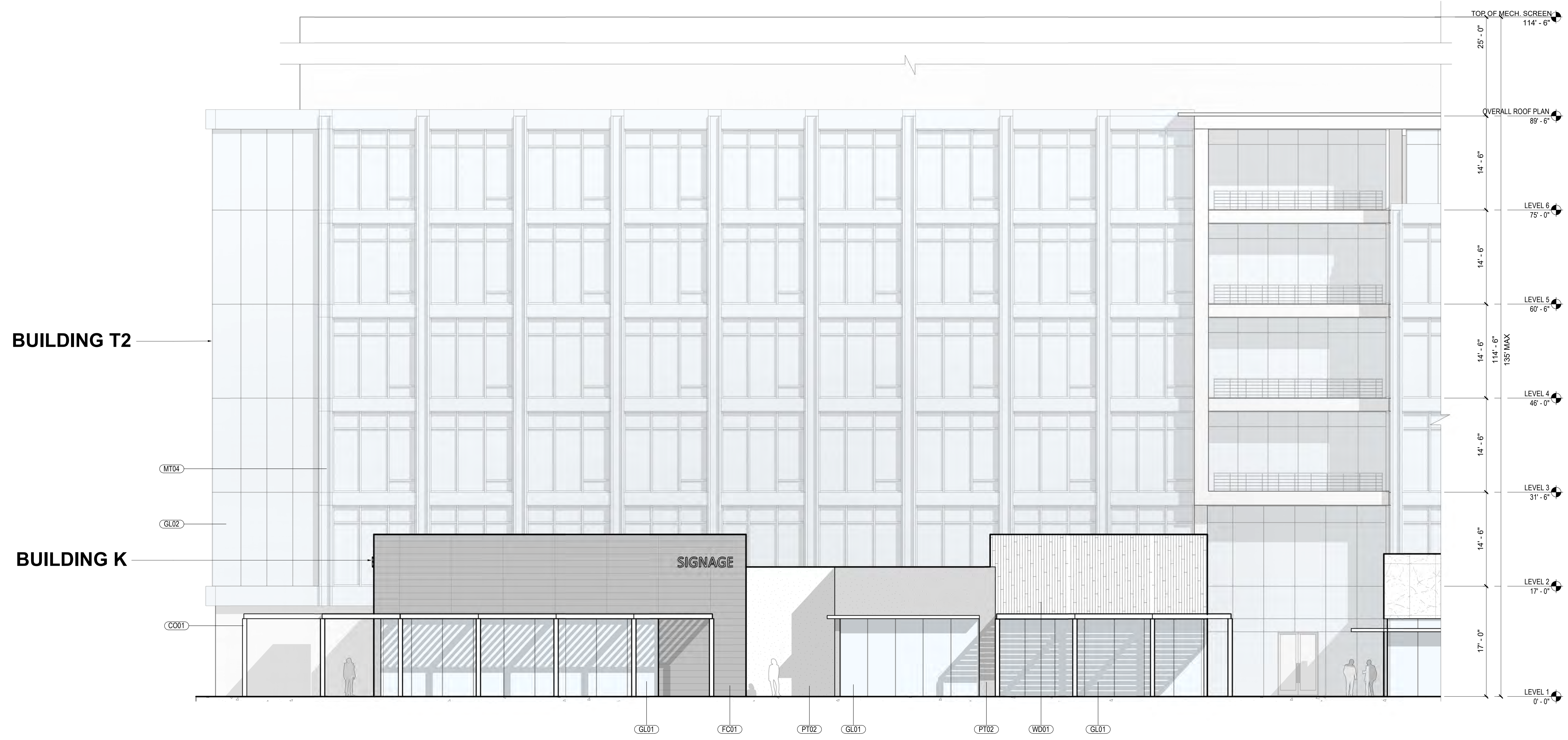
SHEET NO: ENT2.155 41 OF 65

PTS #: 477943
PROJECT #: 477943

Regency Centers REGENCY CENTERS
420 STEVENS AVE SUITE 320
SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187





MATERIAL LEGEND

CO01	CAST-IN-PLACE CONCRETE
FC01	FIBER CEMENT PANEL
FC02	FIBER CEMENT PANEL
FC03	FIBER CEMENT PANEL
GL01	TEMPERED INSULATED STOREFRONT GLAZING
GL02	INSULATED VISION GLAZING
GL03	SPANDREL GLAZING
MT01	MATTE BLACK COMPOSITE METAL PANEL
MT02	PERFORATED METAL PANEL
MT03	COPPER PANEL
MT04	METAL
MT10	METAL GUARDRAIL
PT01	PAIN #1 OVER SMOOTH PLASTER
PT02	PAIN #2 OVER SMOOTH PLASTER
ST01	STONE PANEL CLADDING
UM01	FULL WIDTH WHITE BRICK VENEER
UM02	FULL WIDTH DARK BRICK VENEER
UM03	FULL WIDTH BRICK VENEER
WD01	WOOD SIDING
WD02	WOOD SIDING
WD03	WOOD SIDING
WD04	WOOD PANEL
WD06	RANDOM LENGTH WOOD SIDING
WD07	SOLID WOOD SCREEN
	MASS TIMBER

NOTE: MATERIALS REFERENCED IN THIS DRAWING AND LEGEND ARE CONCEPTUAL. ALL EXTERIOR MATERIALS TO BE DETERMINED AT TIME OF BUILDING PERMIT.

KT2-W BUILDING K, T2 - WEST ELEVATION
SCALE: 1/8" = 1'-0"

PREPARED BY: **rdc.**
NAME:
ADDRESS: 245 EAST THIRD STREET
 LONG BEACH, CA 90802
PHONE: 562.628.8000

ORIGINAL DATE: 3/15/2016 (BY CALLISONRTKL)
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REVISION DATE: 06/10/2019 (BY GENSLER)
REVISION DATE: 10/11/2019 (BY RDC)

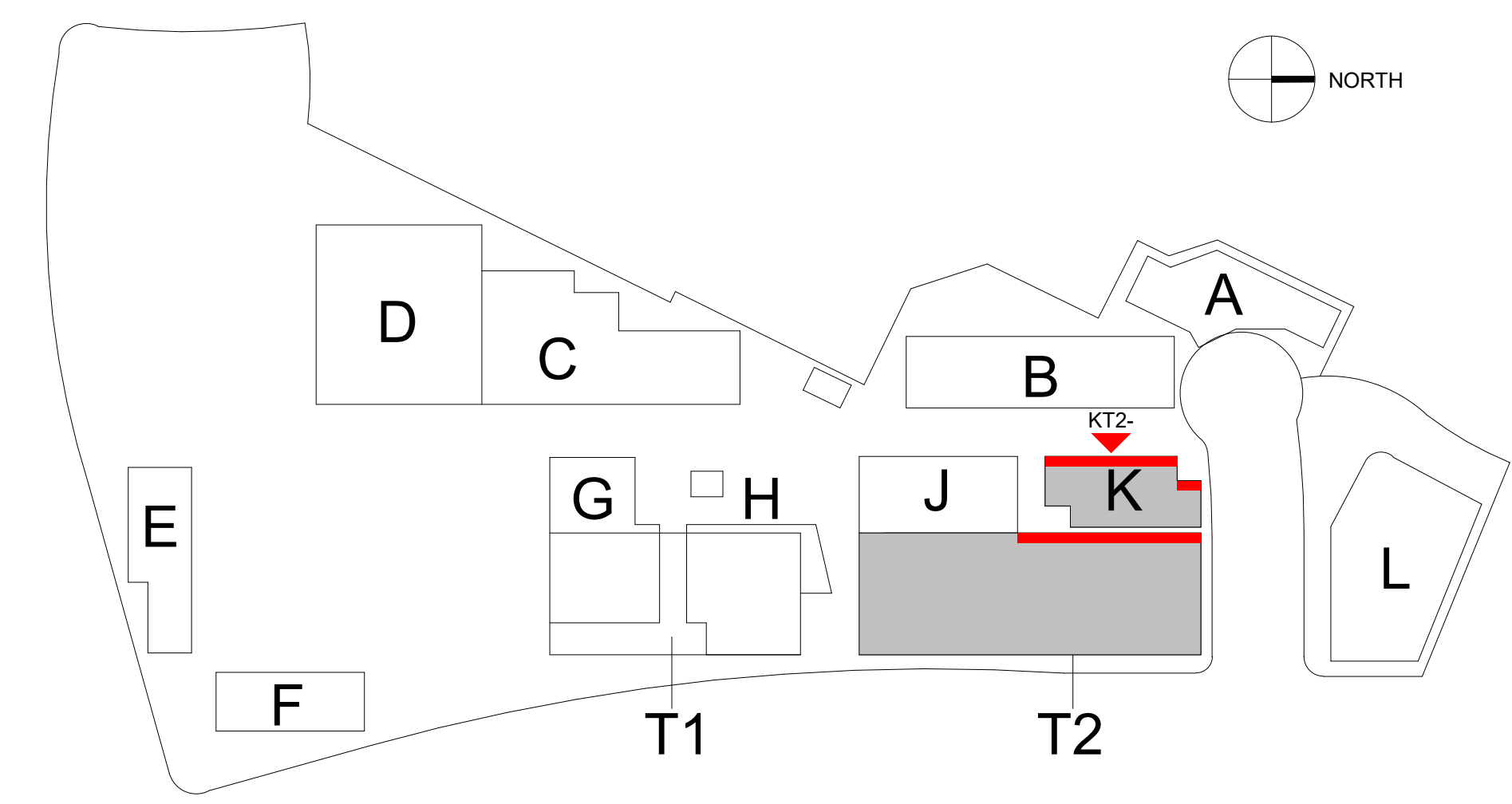
SHEET TITLE:
 ENLARGED ELEVATION OF BUILDING K

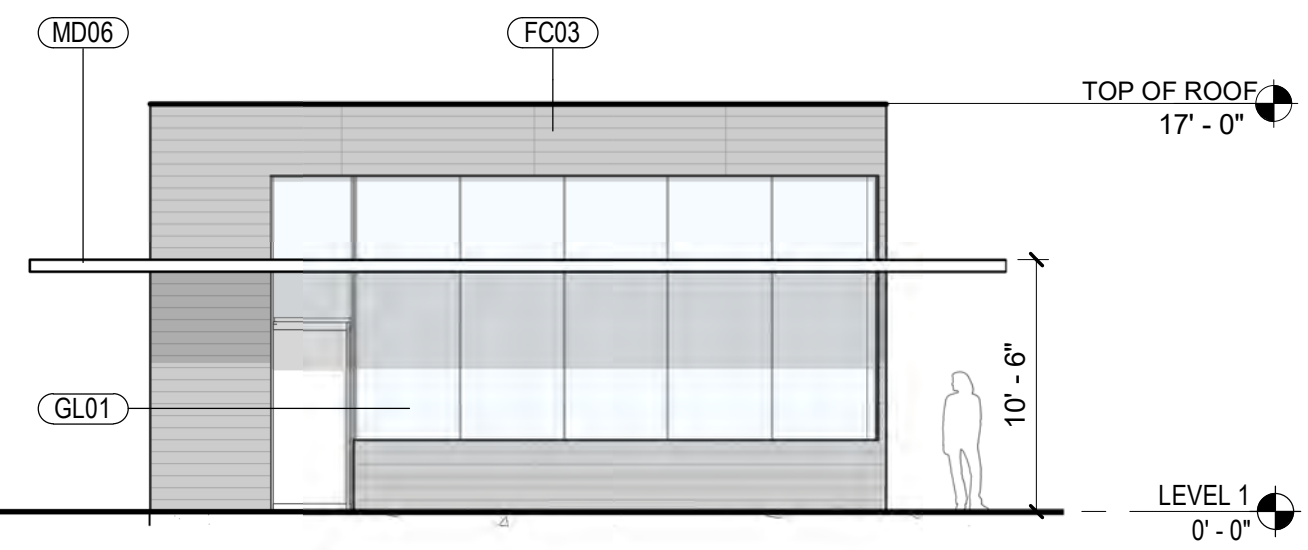
SHEET NO: ENT2.156 42 OF 65

PTS #: 477943
PROJECT #: 477943

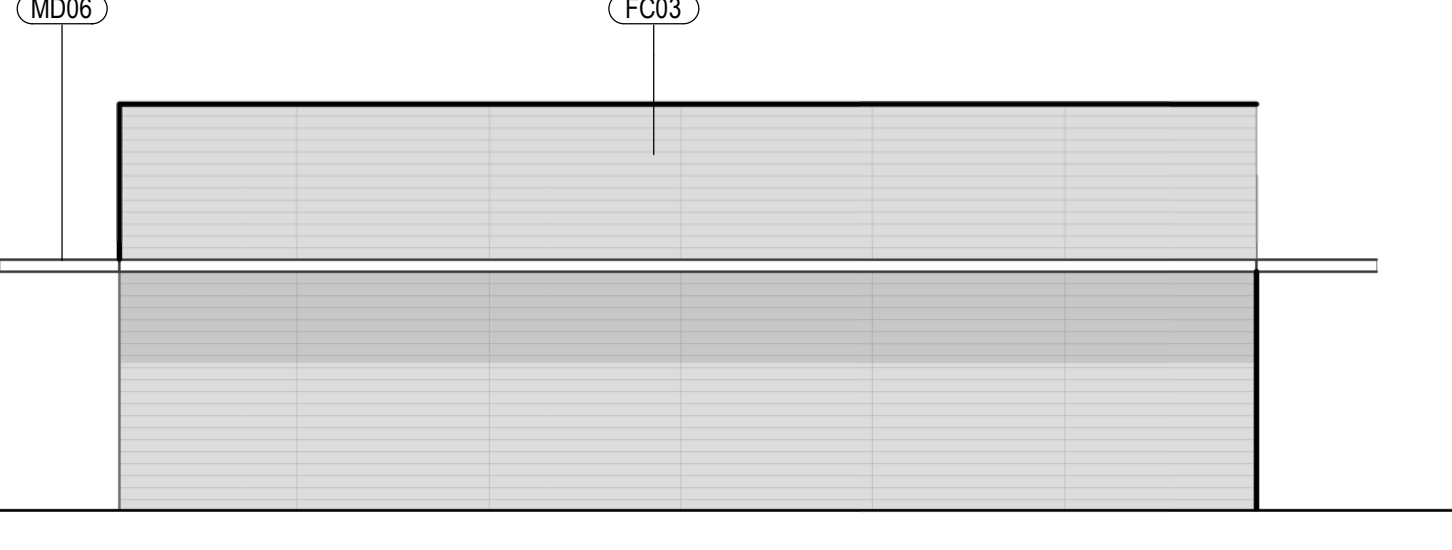
Regency Centers REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187

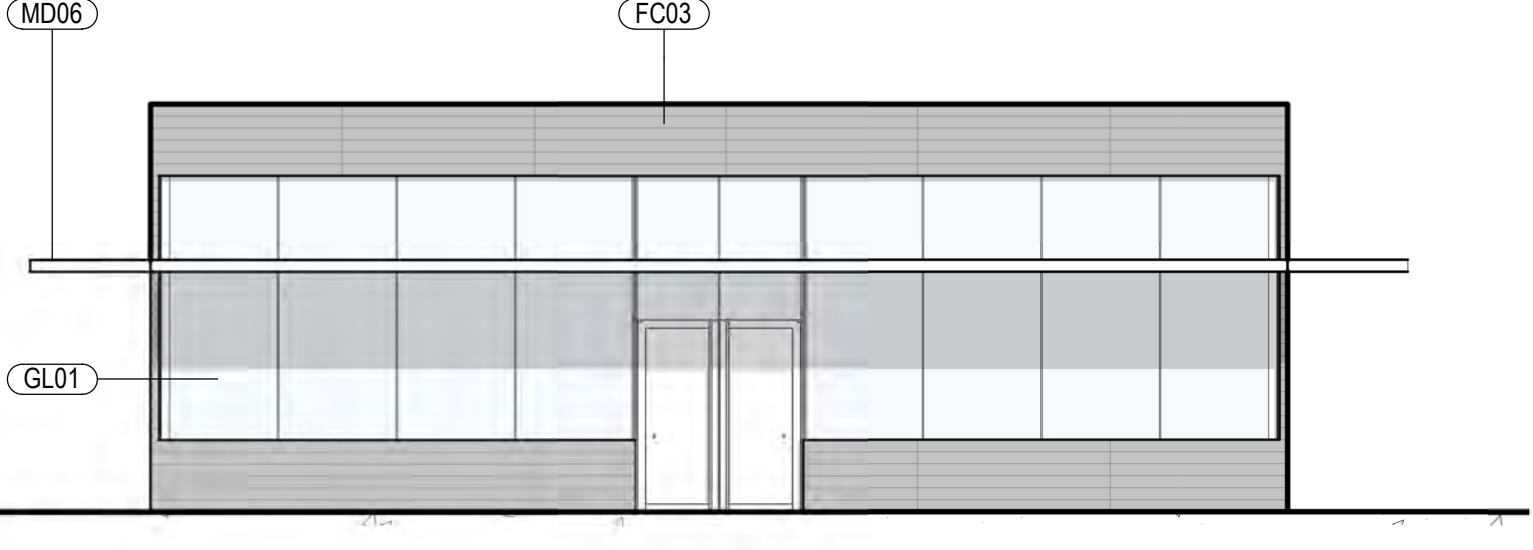




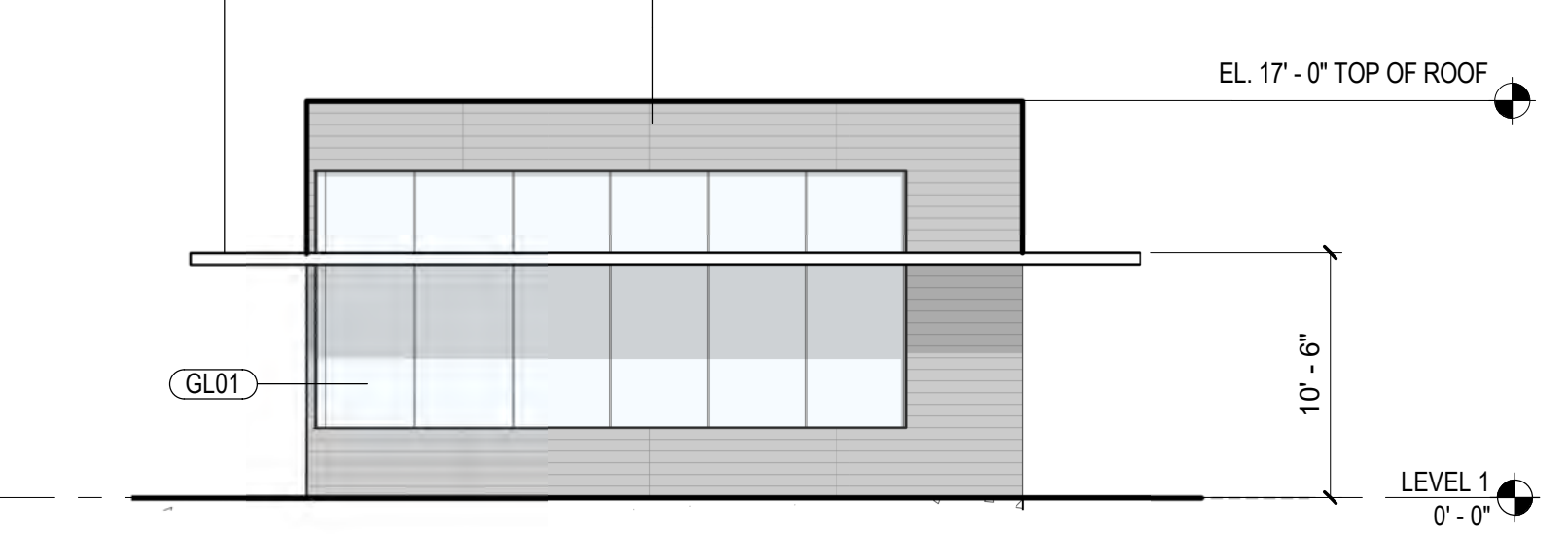
Q1-S BUILDING Q1 - SOUTH ELEVATION
SCALE: 1/8" = 1'-0"



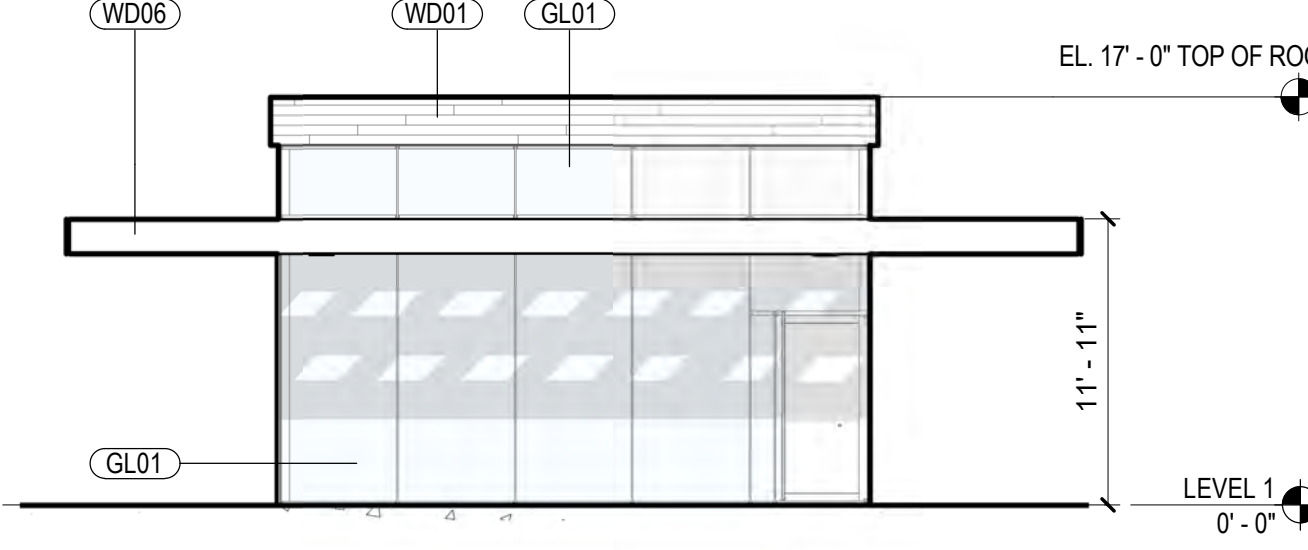
Q1-W BUILDING Q1 - WEST ELEVATION
SCALE: 1/8" = 1'-0"



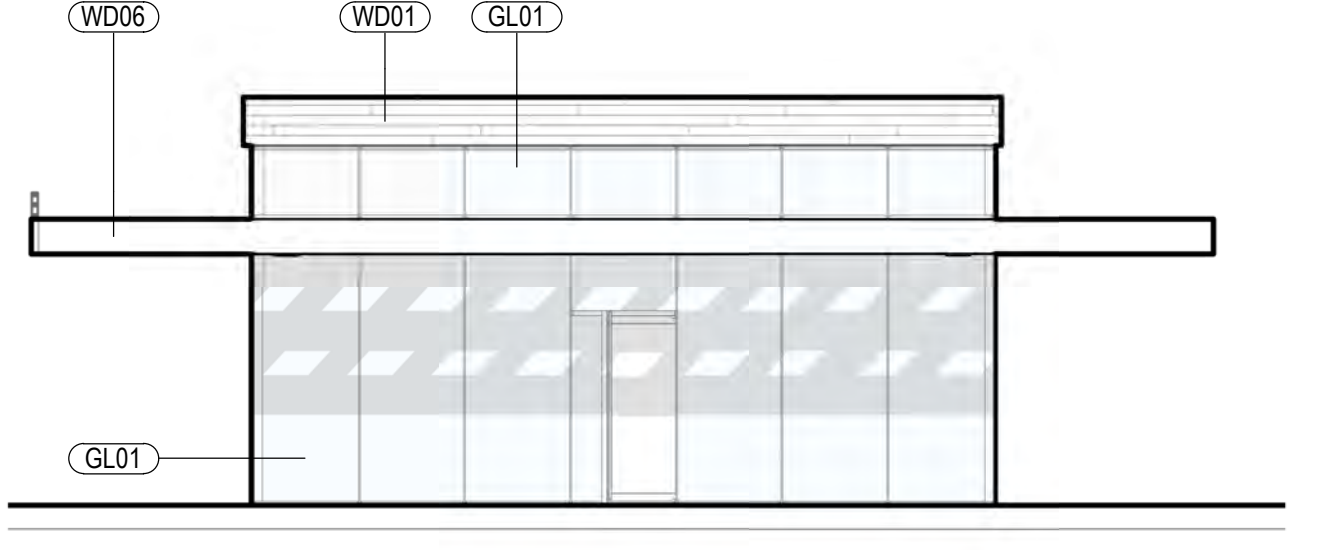
Q1-E BUILDING Q1 - EAST ELEVATION
SCALE: 1/8" = 1'-0"



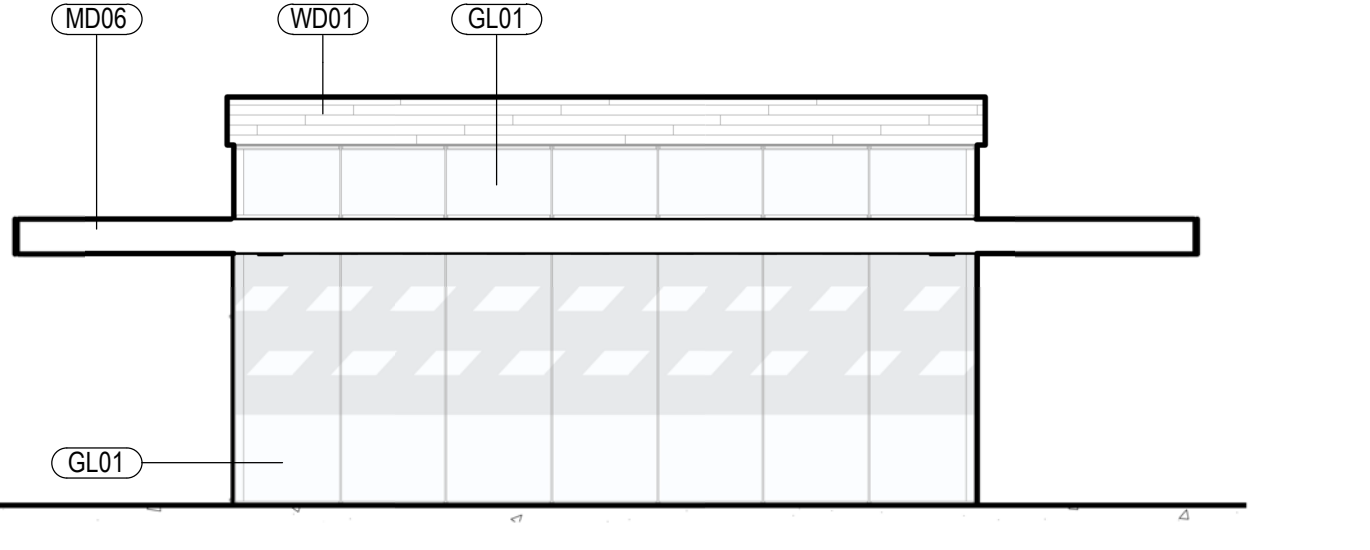
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SCALE: 1/8" = 1'-0"



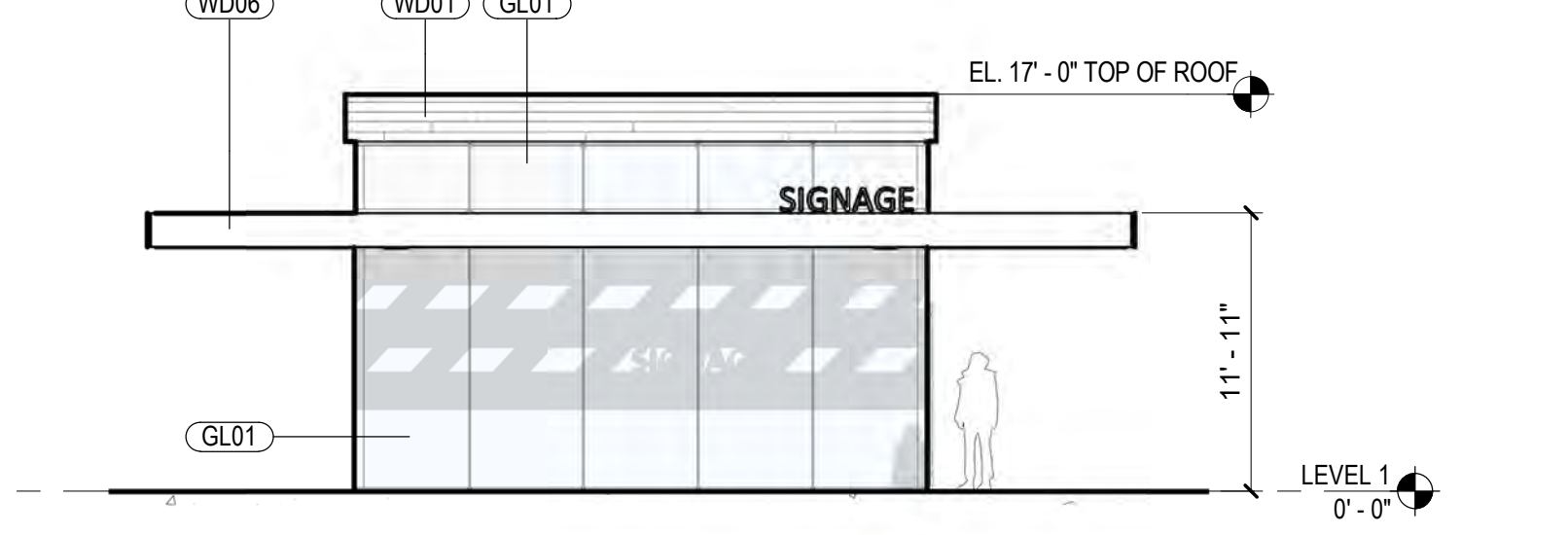
Q2-S BUILDING Q2 - SOUTH ELEVATION
SCALE: 1/8" = 1'-0"



Q2-W BUILDING Q2 - WEST ELEVATION
SCALE: 1/8" = 1'-0"



Q2-E BUILDING Q2 - EAST ELEVATION
SCALE: 1/8" = 1'-0"

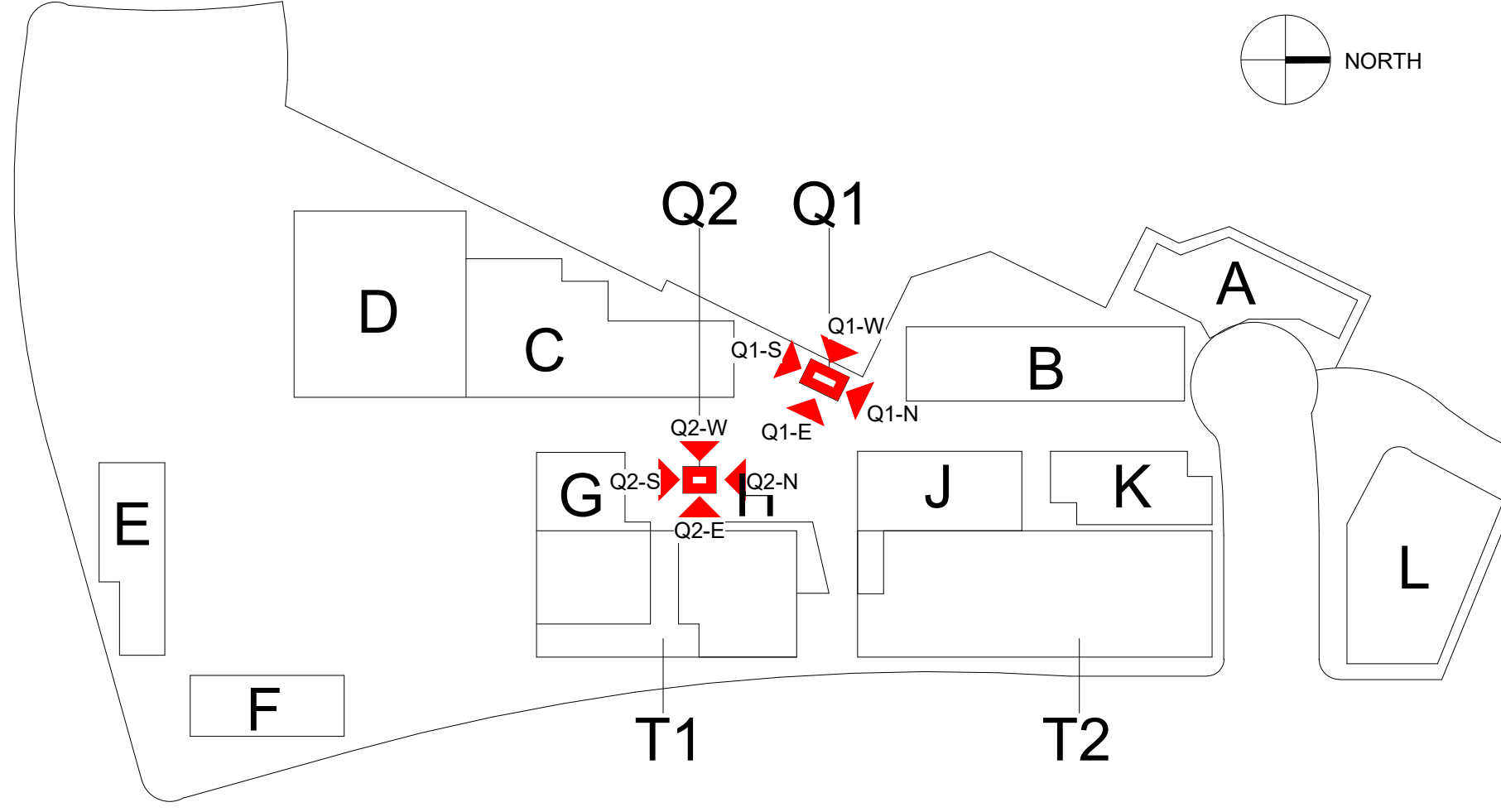


Q2-N BUILDING Q2 - NORTH ELEVATION
SCALE: 1/8" = 1'-0"

MATERIAL LEGEND

- CO01 CAST-IN-PLACE CONCRETE
- FC01 FIBER CEMENT PANEL
- FC02 FIBER CEMENT PANEL
- FC03 FIBER CEMENT PANEL
- GL01 TEMPERED INSULATED STOREFRONT
- GL02 GLAZING
- GL03 INSULATED VISION GLAZING
- MT01 SPANDREL GLAZING
- MT02 MATTE BLACK COMPOSITE METAL PANEL
- MT03 PERFORATED METAL PANEL
- MT04 COPPER PANEL
- MT10 METAL
- PT01 METAL GUARDRAIL
- PT02 PAINT #1 OVER SMOOTH PLASTER
- ST01 PAINT #2 OVER SMOOTH PLASTER
- UM01 STONE PANEL CLADDING
- UM02 FULL WIDTH WHITE BRICK VENEER
- UM03 FULL WIDTH DARK BRICK VENEER
- WD01 FULL WIDTH BRICK VENEER
- WD02 WOOD SIDING
- WD03 WOOD SIDING
- WD04 WOOD PANEL
- WD06 RANDOM LENGTH WOOD SIDING
- WD07 SOLID WOOD SCREEN
- MASS TIMBER

NOTE: MATERIALS REFERENCED IN THIS DRAWING AND LEGEND ARE CONCEPTUAL. ALL EXTERIOR MATERIALS TO BE DETERMINED AT TIME OF BUILDING PERMIT.



PREPARED BY: **rdc.**
 NAME:
 ADDRESS: 245 EAST THIRD STREET
 LONG BEACH, CA 90802
 PHONE: 562.628.8000

ORIGINAL DATE: 3/15/2016 (BY CALLISONRTKL)
 REVISION DATE: 7/28/2016 (BY CALLISONRTKL)
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 REVISION DATE: 06/10/2019 (BY GENSLER)
 REVISION DATE: 10/11/2019 (BY RDC)

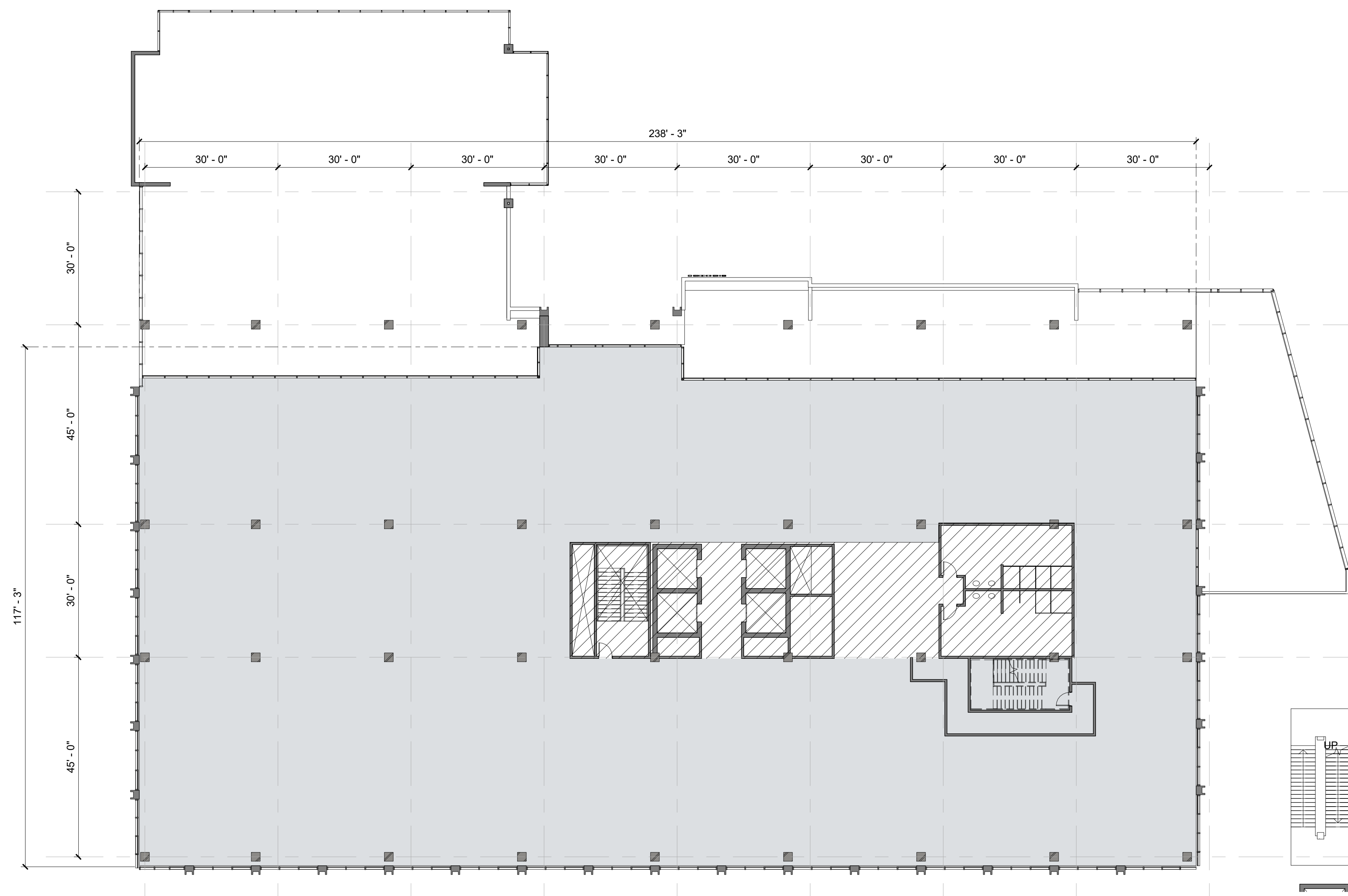
SHEET TITLE:
 ENLARGED PLAN AND ELEVATION OF BUILDING Q1 & Q2

SHEET NO: **ENT2.160** 43 OF 65

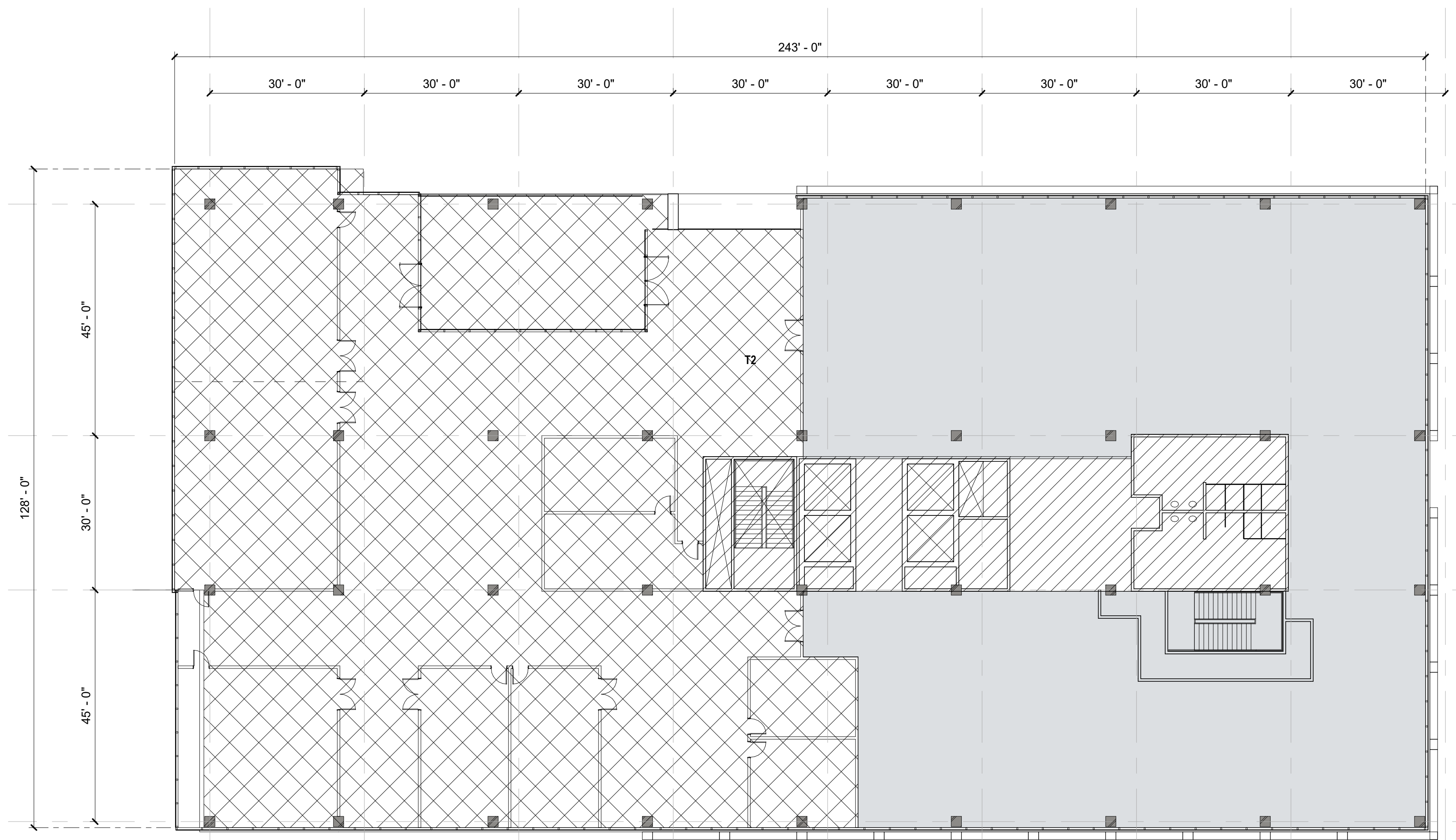
PTS #: 477943
 PROJECT #: 477943

Regency Centers REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

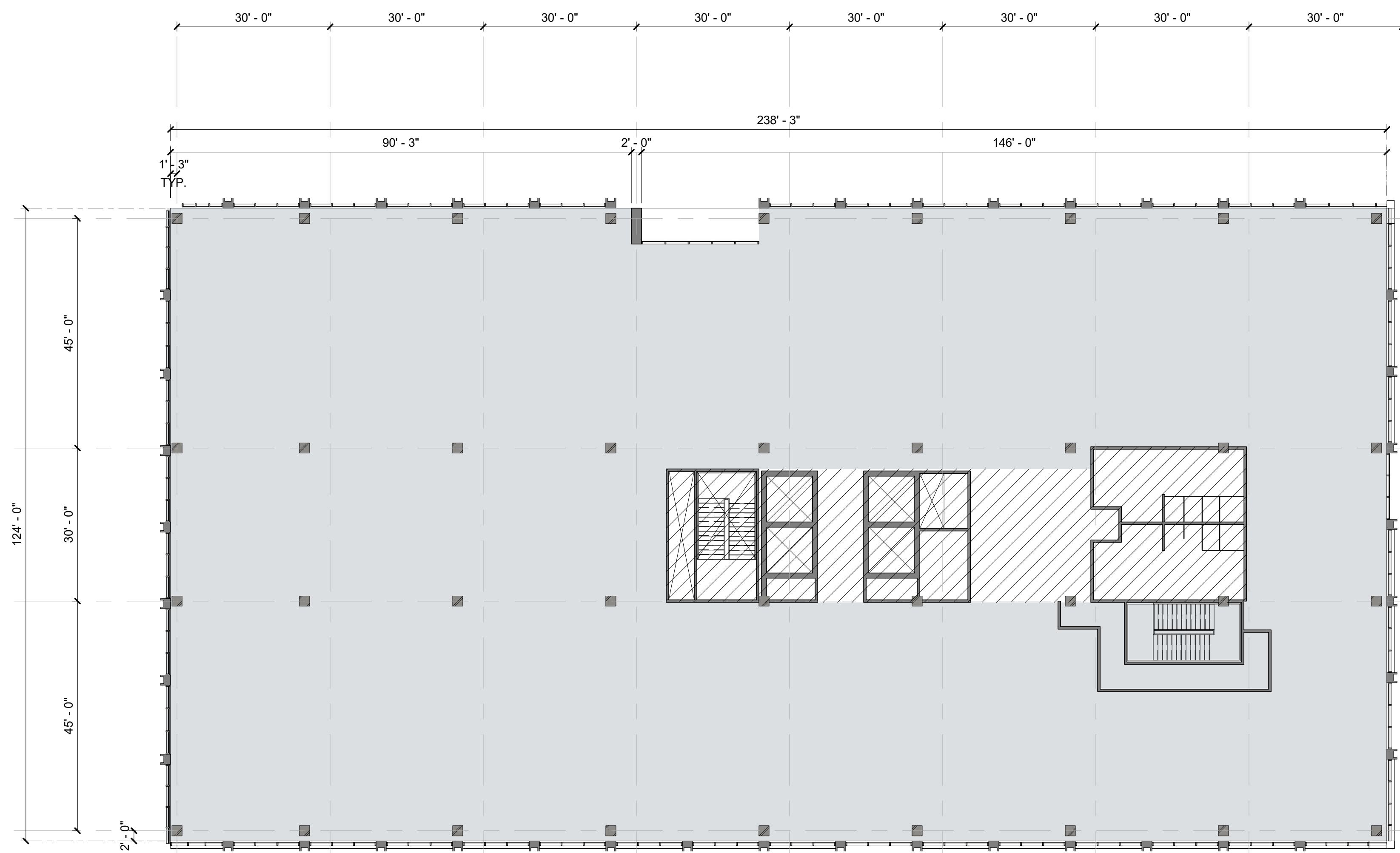
PROJECT NUMBER: 19-187



T1-2 OFFICE/R&D - LEVEL 2
SCALE: 1/16" = 1'-0"

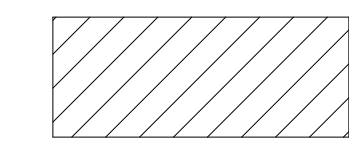

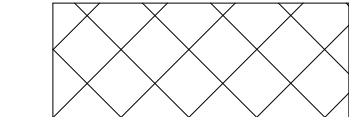


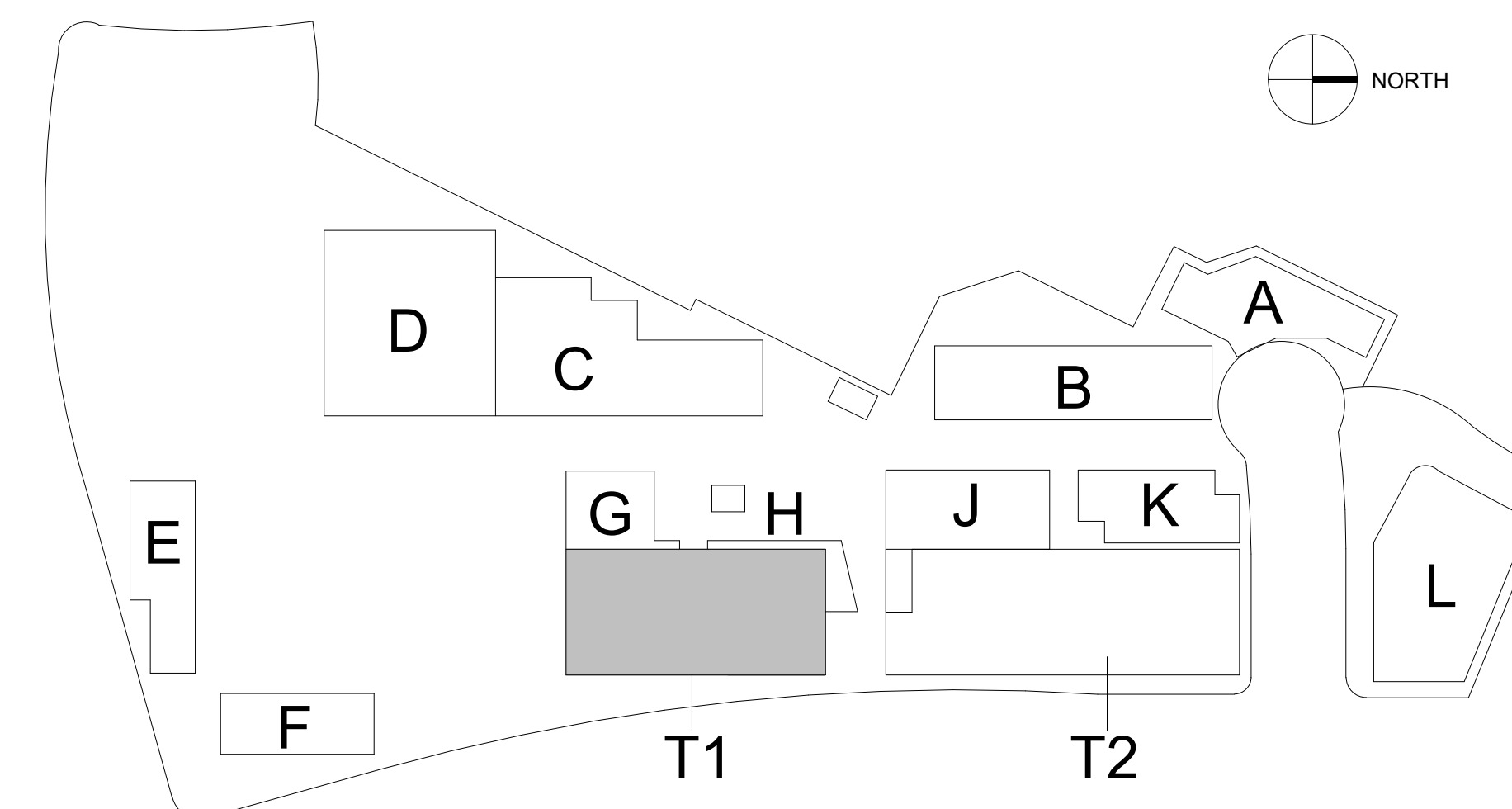
T1-6 LEVEL 6
SCALE: 1/16" = 1'-0"



T1-4 OFFICE/R&D TYPICAL PLAN - LEVEL 4
SCALE: 1/16" = 1'-0"

LEGEND

-  CORE AND COMMON SPACE
-  OPEN FLOOR PLATE
-  CONFERENCE



PREPARED BY: **rdc.**
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 REVISION DATE: 12/12/2016 (BY CALLISONRTKL)
 REVISION DATE: 06/10/2019 (BY GENSLER)
 REVISION DATE: 10/11/2019 (BY RDC)

SHEET TITLE:
 R&D PLANS - BUILDING T1

SHEET NO: **ENT2.170** 44 OF 65

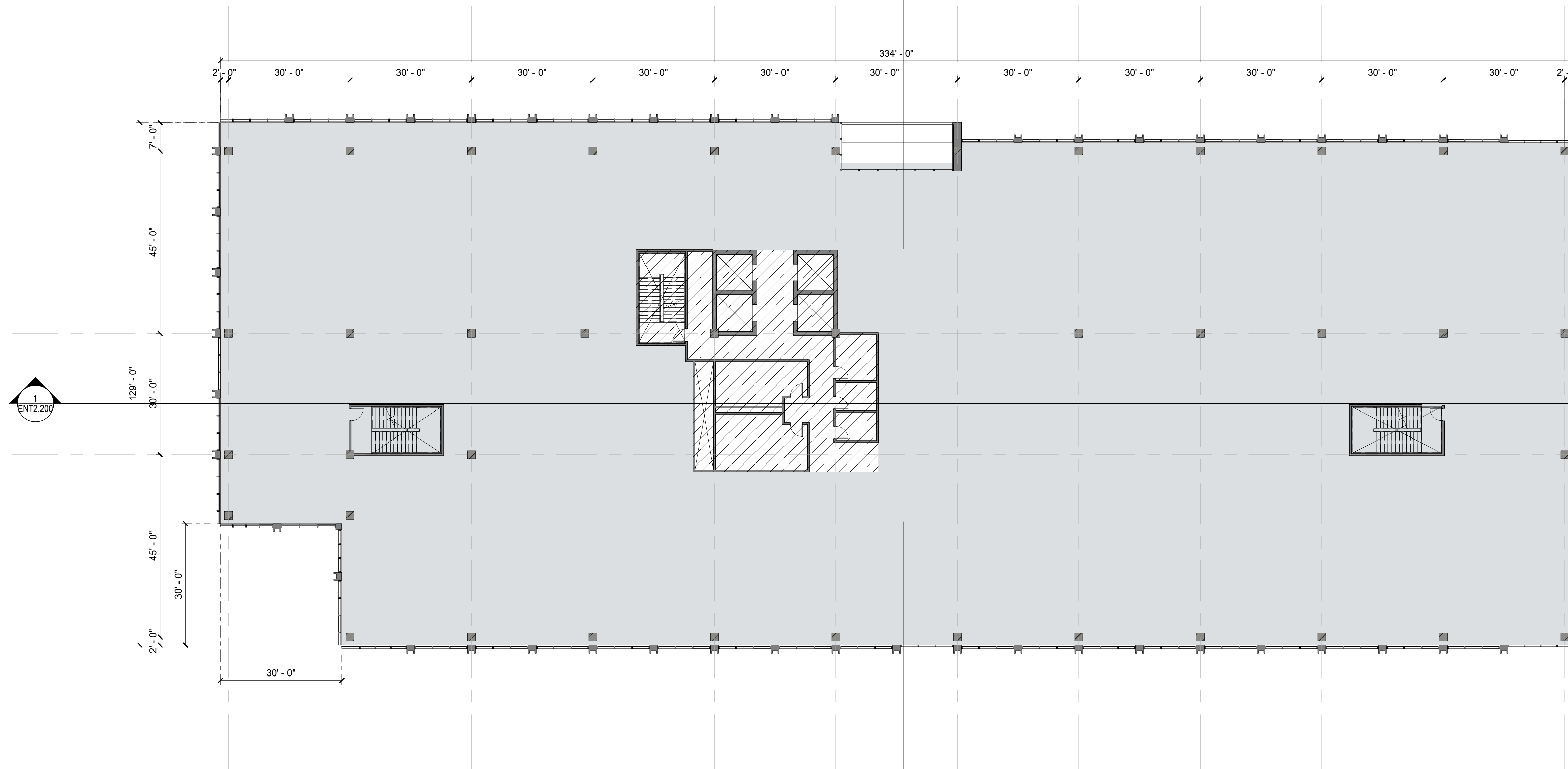
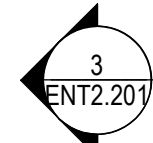
PTS #: 477943
 PROJECT #: 477943

Regency Centers. REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187

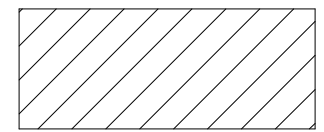

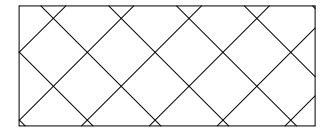


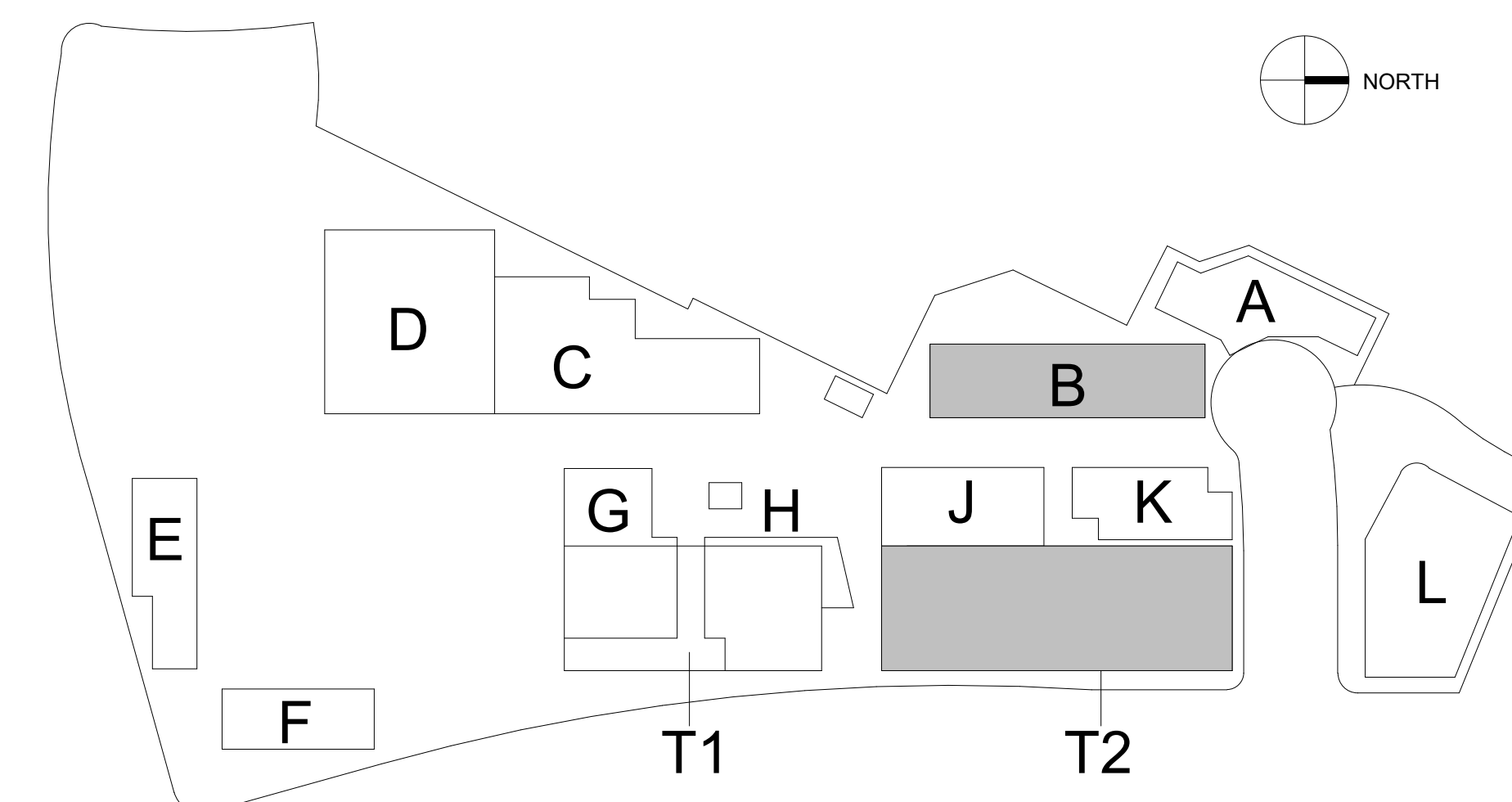
B OFFICE TYPICAL PLAN - LEVEL 3
SCALE: 1/16" = 1'-0"



T2 OFFICE/R&D TYPICAL PLAN - LEVEL 4
SCALE: 1/16" = 1'-0"

LEGEND

-  CORE AND COMMON SPACE
-  OPEN FLOOR PLATE
-  CONFERENCE



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SHEET TITLE:
 R&D PLANS / OFFICE PLAN - BUILDING B, T2

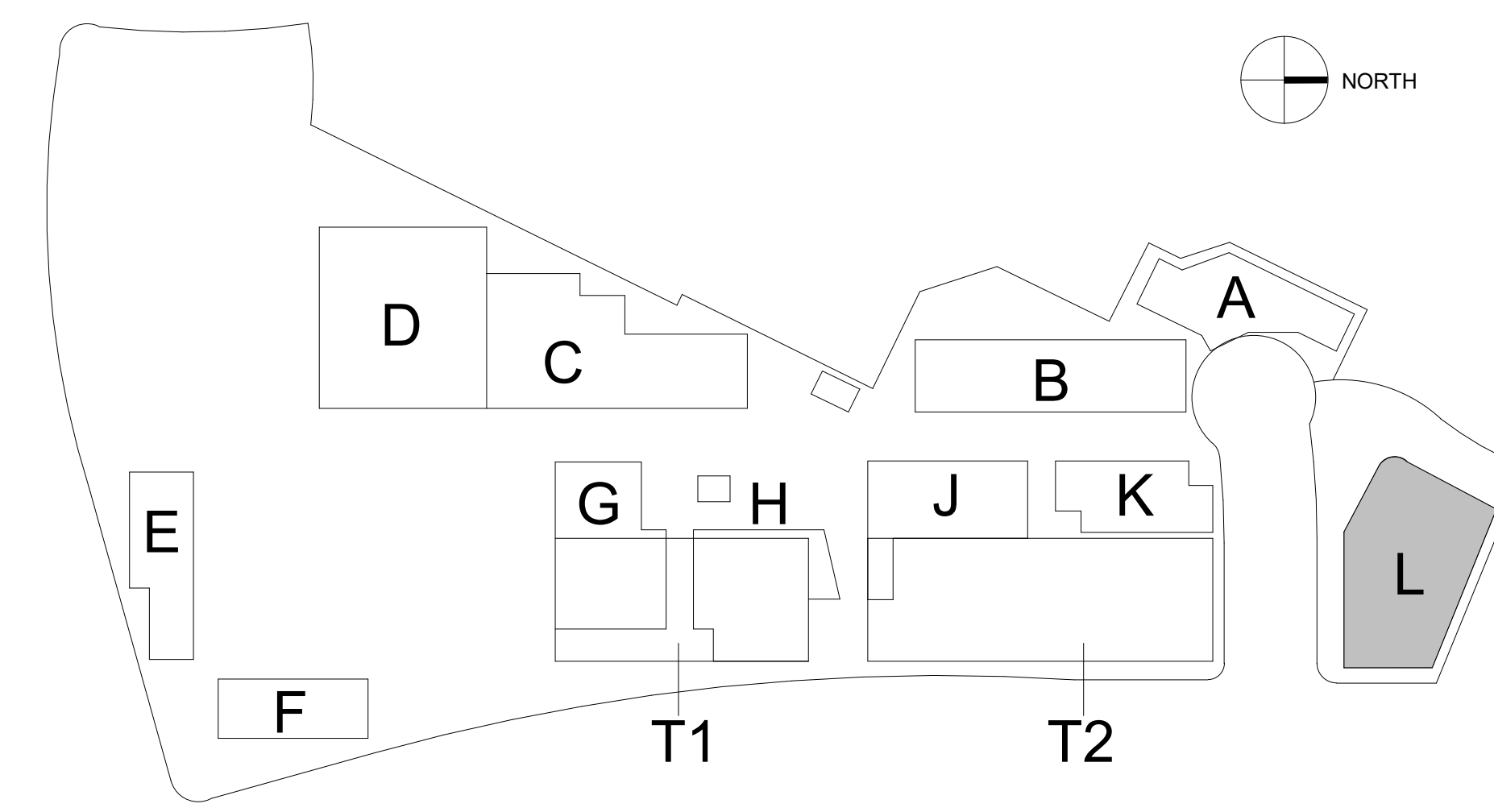
SHEET NO: **ENT2.180** 45 OF 65

PTS #: 477943
 PROJECT #: 477943

Regency Centers. REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075



L HOTEL
SCALE: 1/16" = 1'-0"



PREPARED BY: **rdc.**
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 REVISION DATE: 10/11/2019 (BY RDC)
 REVISION DATE: 09/01/2020 (BY RDC)

SHEET TITLE:
 ENLARGED PLAN OF HOTEL

SHEET NO: **ENT2.190** 46 OF 65

PTS #: 477943
 PROJECT #: 477943

Regency Centers. REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187

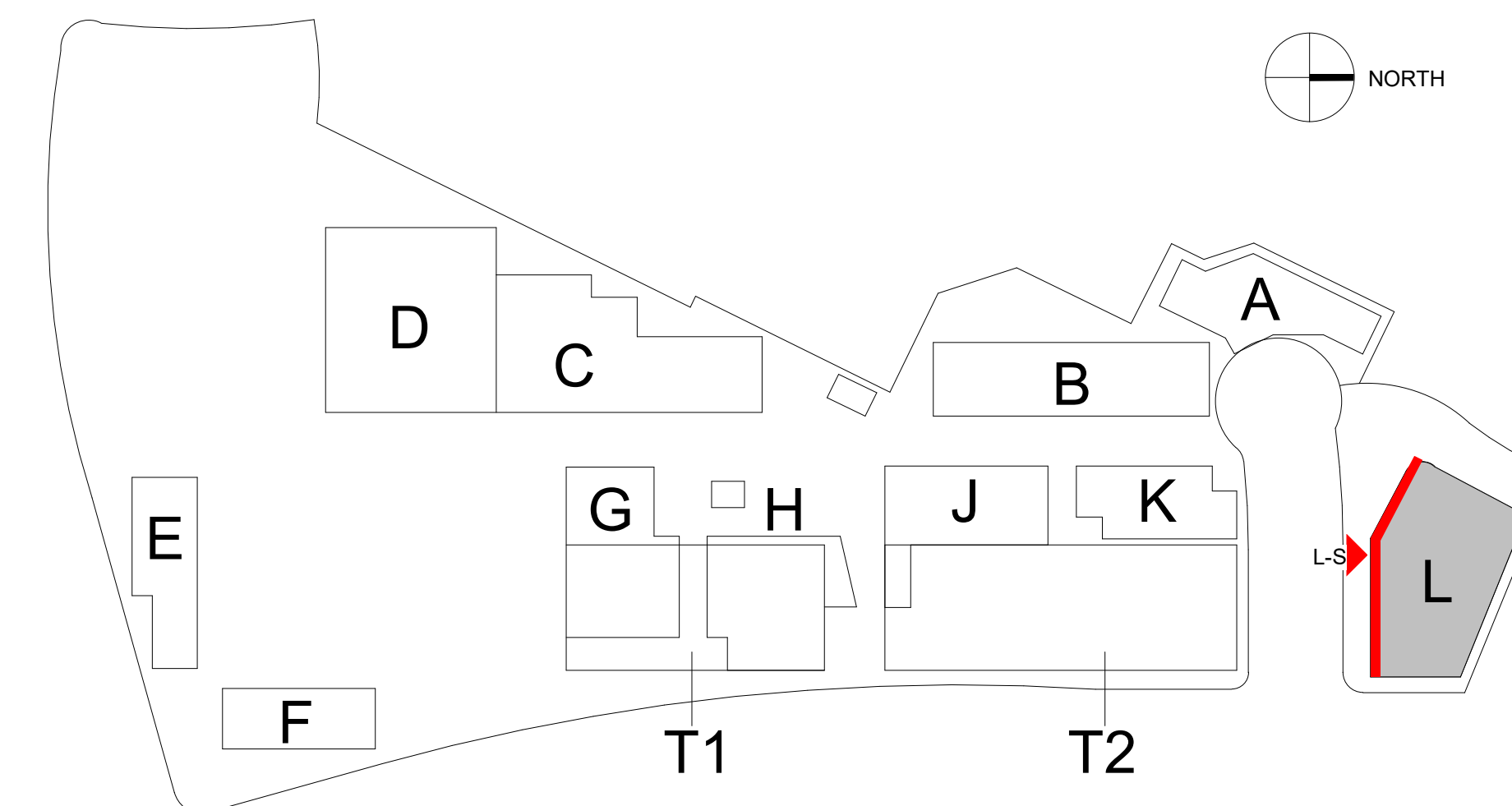


L-S HOTEL - SOUTH ELEVATION
SCALE: 1/8" = 1'-0"

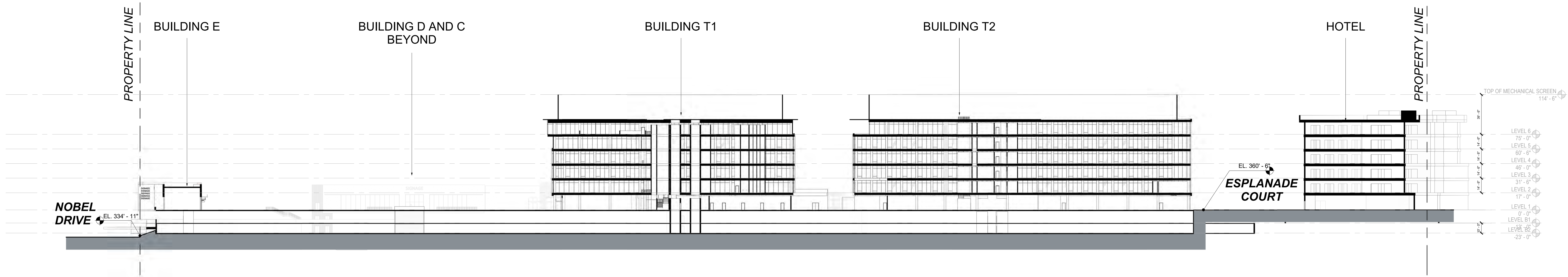
MATERIAL LEGEND

- CO01 CAST-IN-PLACE CONCRETE
- FC01 FIBER CEMENT PANEL
- FC02 FIBER CEMENT PANEL
- FC03 FIBER CEMENT PANEL
- GL01 TEMPERED INSULATED STOREFRONT
- GL02 GLAZING
- GL03 INSULATED VISION GLAZING
- MT01 SPANDREL GLAZING
- MT02 MATTE BLACK COMPOSITE METAL PANEL
- MT03 PERFORATED METAL PANEL
- MT04 COPPER PANEL
- MT10 METAL
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- UM01 STONE PANEL CLADDING
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- WD01 FULL WIDTH BRICK VENEER
- WD02 WOOD SIDING
- WD03 WOOD SIDING
- WD04 WOOD PANEL
- WD06 RANDOM LENGTH WOOD SIDING
- WD07 SOLID WOOD SCREEN
- MASS TIMBER

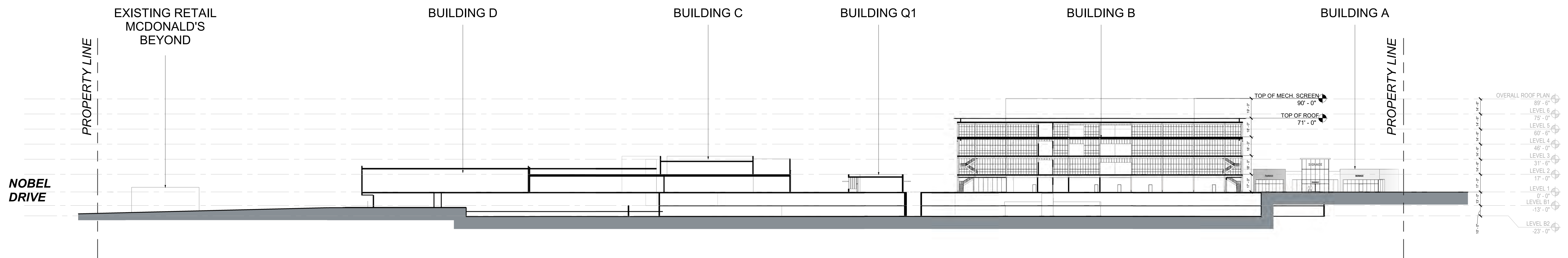
NOTE: MATERIALS REFERENCED IN THIS DRAWING AND LEGEND ARE CONCEPTUAL. ALL EXTERIOR MATERIALS TO BE DETERMINED AT TIME OF BUILDING PERMIT.



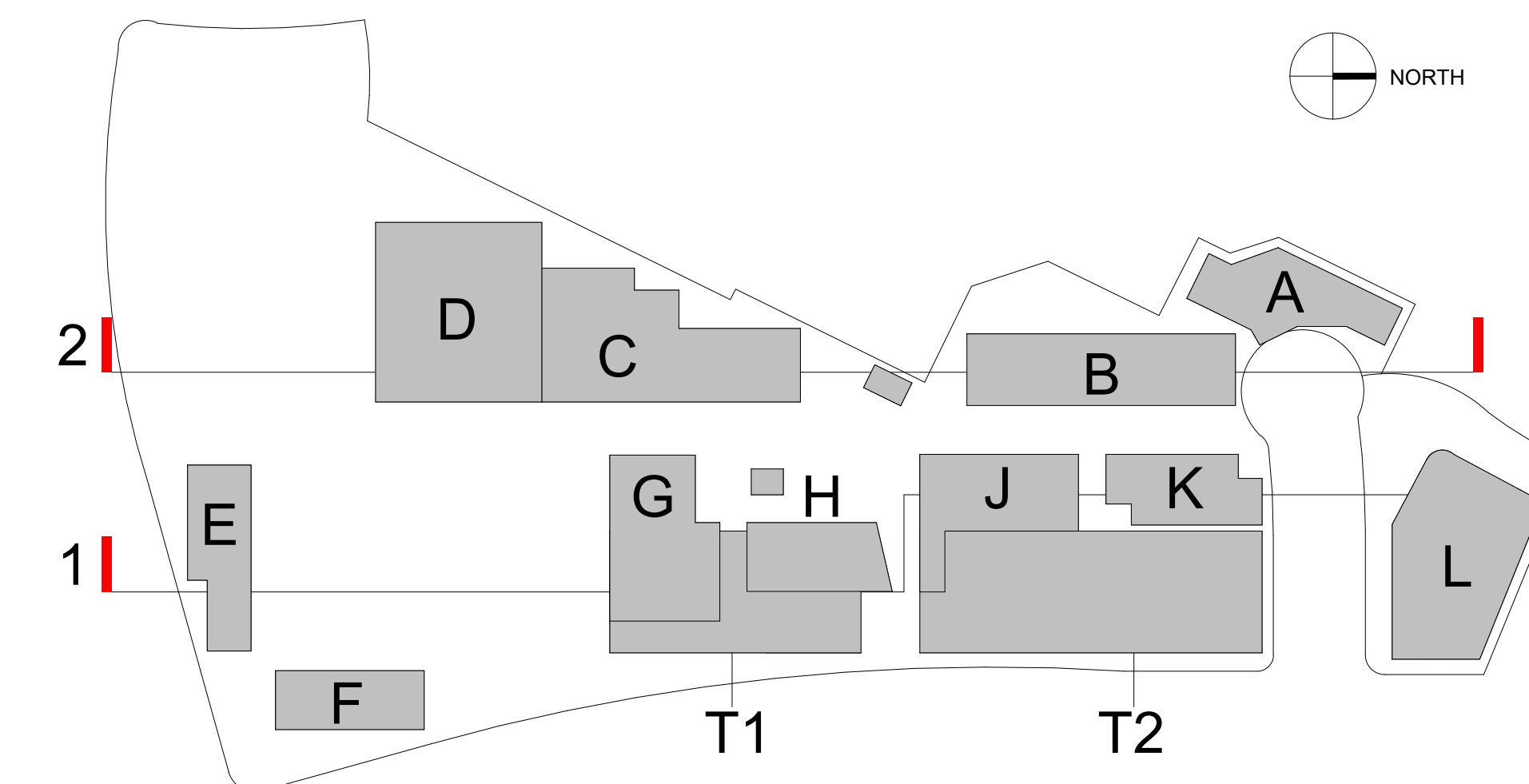
PREPARED BY:	rdc.
NAME:	245 EAST THIRD STREET
ADDRESS:	LONG BEACH, CA 90802
PHONE:	562.628.8000
ORIGINAL DATE: 3/15/2016 (BY CALLISONRTKL)	
REVISION DATE: 7/28/2016 (BY CALLISONRTKL)	
REVISION DATE: 12/12/2016 (BY CALLISONRTKL)	
REVISION DATE: 06/10/2019 (BY GENSLER)	
REVISION DATE: 10/11/2019 (BY RDC)	
SHEET TITLE: ENLARGED ELEVATION OF HOTEL	
SHEET NO: ENT2.191	47 OF 65
PTS #: 477943	
PROJECT #: 477943	
Regency Centers.	REGENCY CENTERS 420 STEVENS AVE SUITE 320 SOLANA BEACH, CA 92075
PROJECT NUMBER:	19-187



1 LONGITUDINAL SECTION - NORTH/SOUTH 1
SCALE: 1" = 40'-0"



2 LONGITUDINAL SECTION - NORTH/SOUTH 2
SCALE: 1" = 40'-0"



PREPARED BY: **rdc.**
 NAME:
 ADDRESS: 245 EAST THIRD STREET
 LONG BEACH, CA 90802
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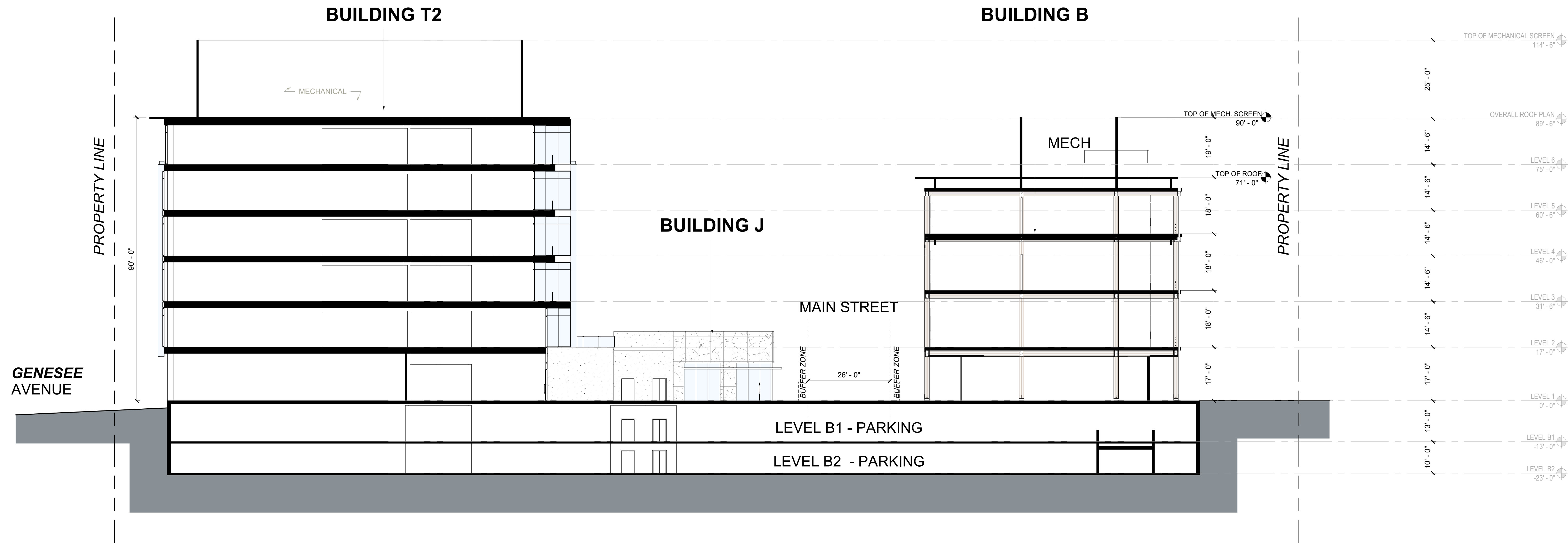
ORIGINAL DATE: 3/15/2016 (BY CALLISONRTKL)
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 REVISION DATE: 12/12/2016 (BY CALLISONRTKL)
 REVISION DATE: 06/10/2019 (BY GENSLER)
 REVISION DATE: 10/11/2019 (BY RDC)

SHEET TITLE:
 SECTION 1 & 2

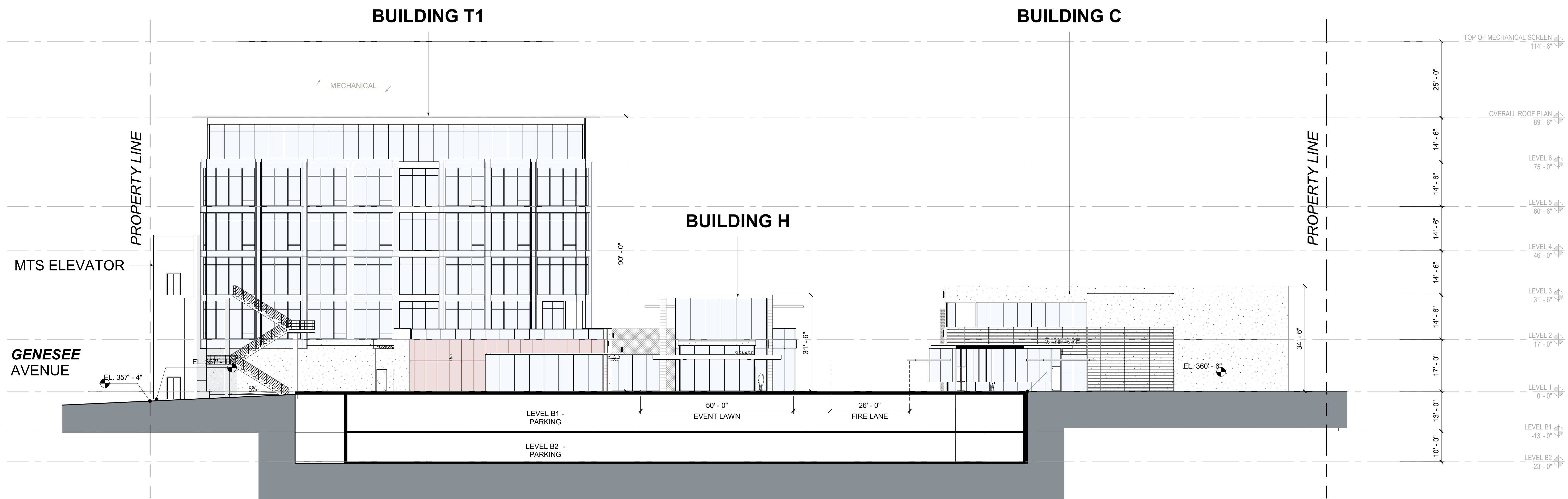
SHEET NO: **ENT2.200** 48 OF 65

PTS #: 477943
 PROJECT #: 477943

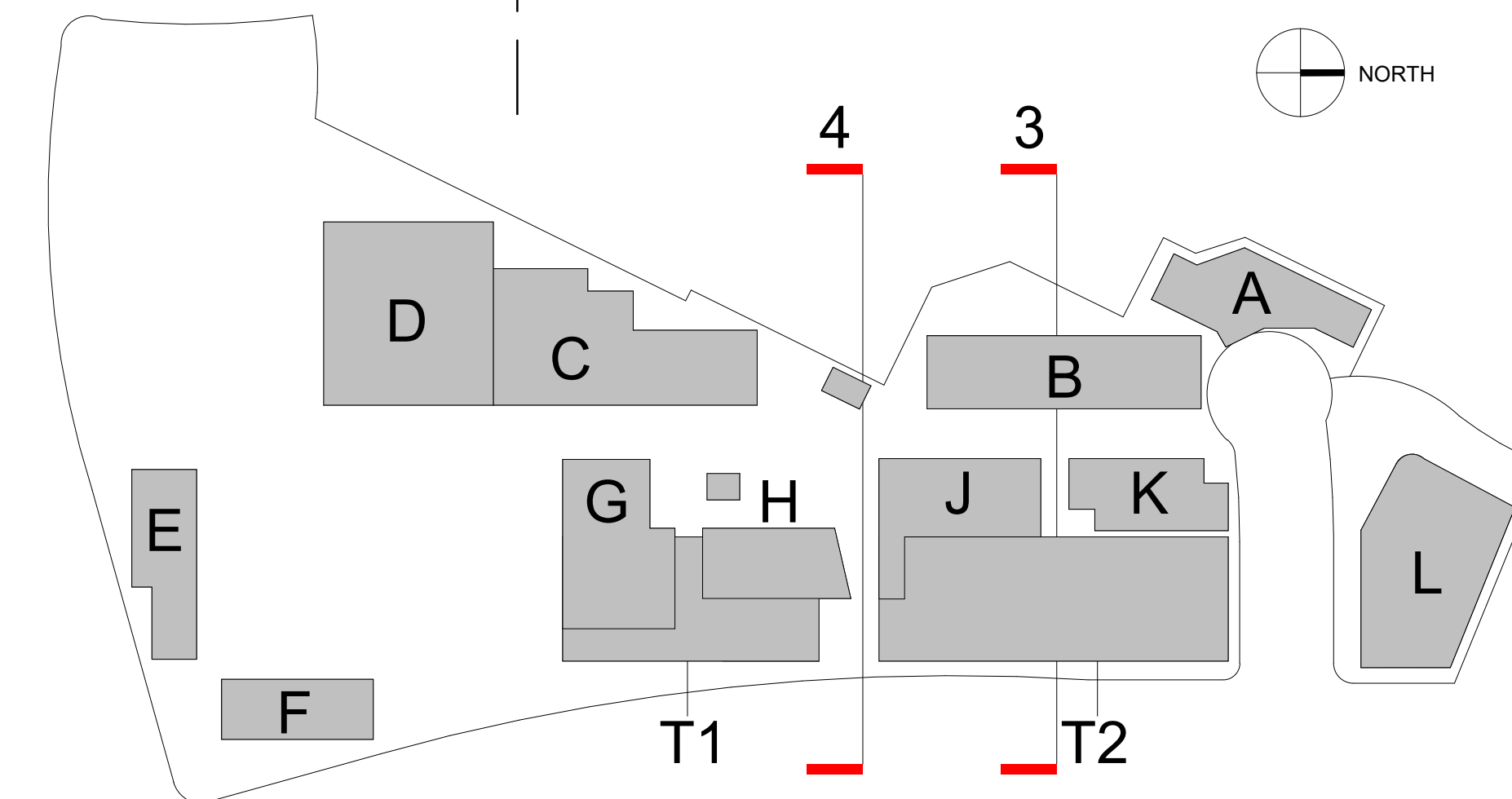
Regency Centers REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075



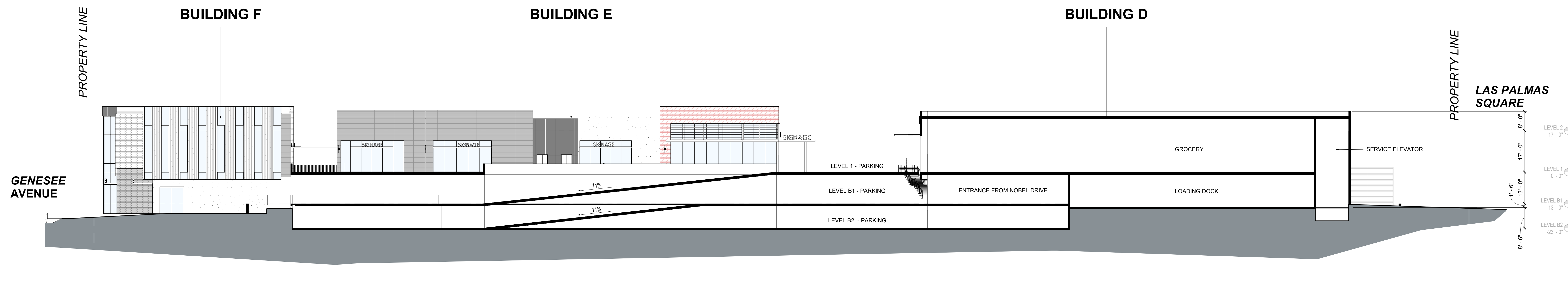
3 SECTION THROUGH MAIN STREET AND OFFICE LOBBY
SCALE: 1/16" = 1'-0"



4 SECTION AT METROPOLITAN TRANSIT SYSTEM PLATFORM
SCALE: 1/16" = 1'-0"



PREPARED BY:	rdc.
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ADDRESS:	LONG BEACH, CA 90802
PHONE:	562.628.8000
ORIGINAL DATE:	3/15/2016 (BY CALLISONRTKL)
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REVISION DATE:	06/10/2019 (BY GENSLER)
REVISION DATE:	10/11/2019 (BY RDC)
SHEET TITLE:	SECTION 3 & 4
SHEET NO:	ENT2.201 49 OF 65
PTS #:	477943
PROJECT #:	477943
Regency Centers	REGENCY CENTERS 420 STEVENS AVE SUITE 320 SOLANA BEACH, CA 92075



5 SECTION THROUGH NEIGHBORHOOD CENTER
SCALE: 1/16" = 1'-0"

PREPARED BY: **rdc.**
 NAME:
 ADDRESS: 245 EAST THIRD STREET
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 PHONE: 562.628.8000

ORIGINAL DATE: 3/15/2016 (BY CALLISONRTKL)
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 REVISION DATE: 06/10/2019 (BY GENSLER)
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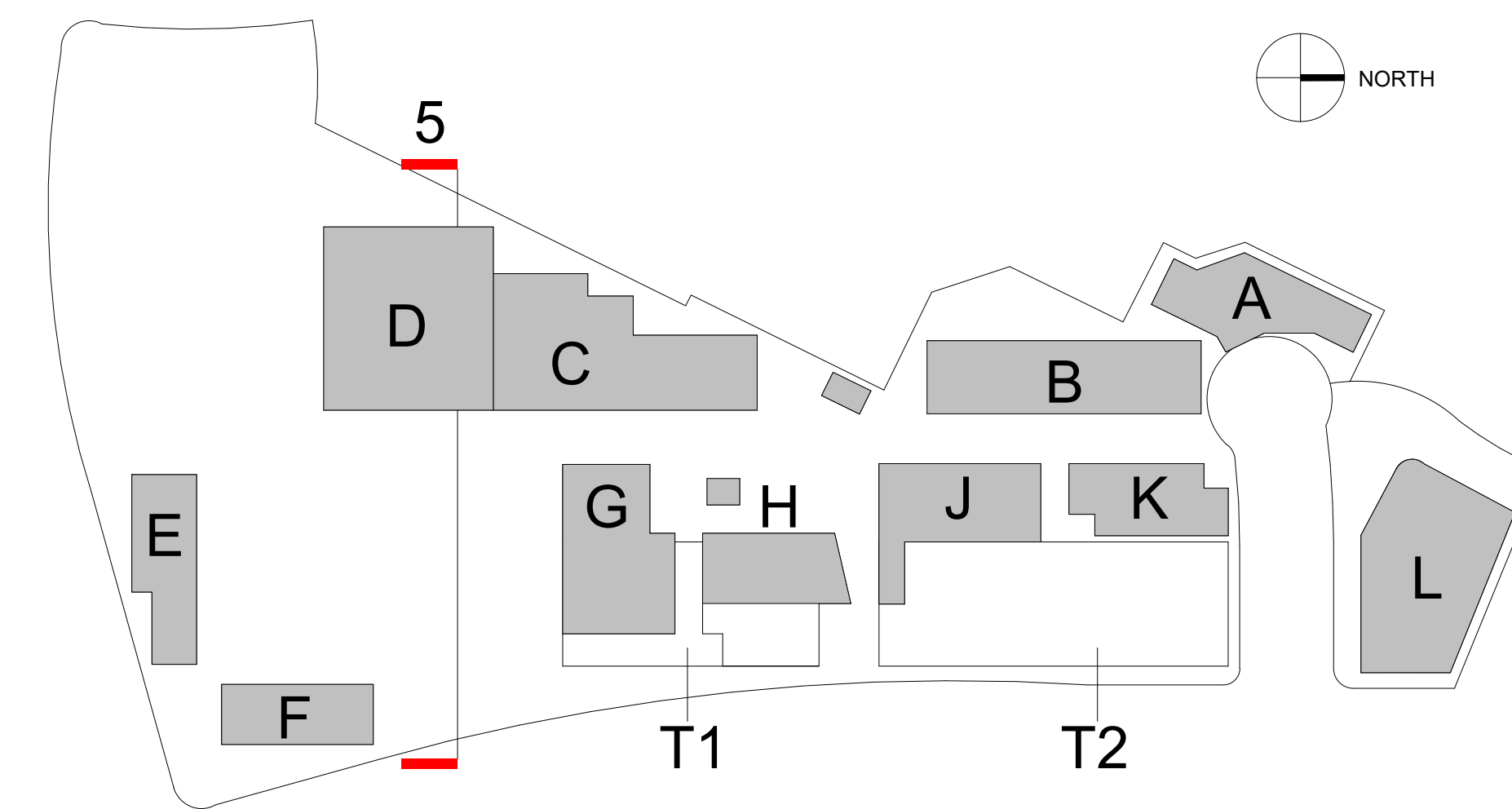
SHEET TITLE:
SECTION 5

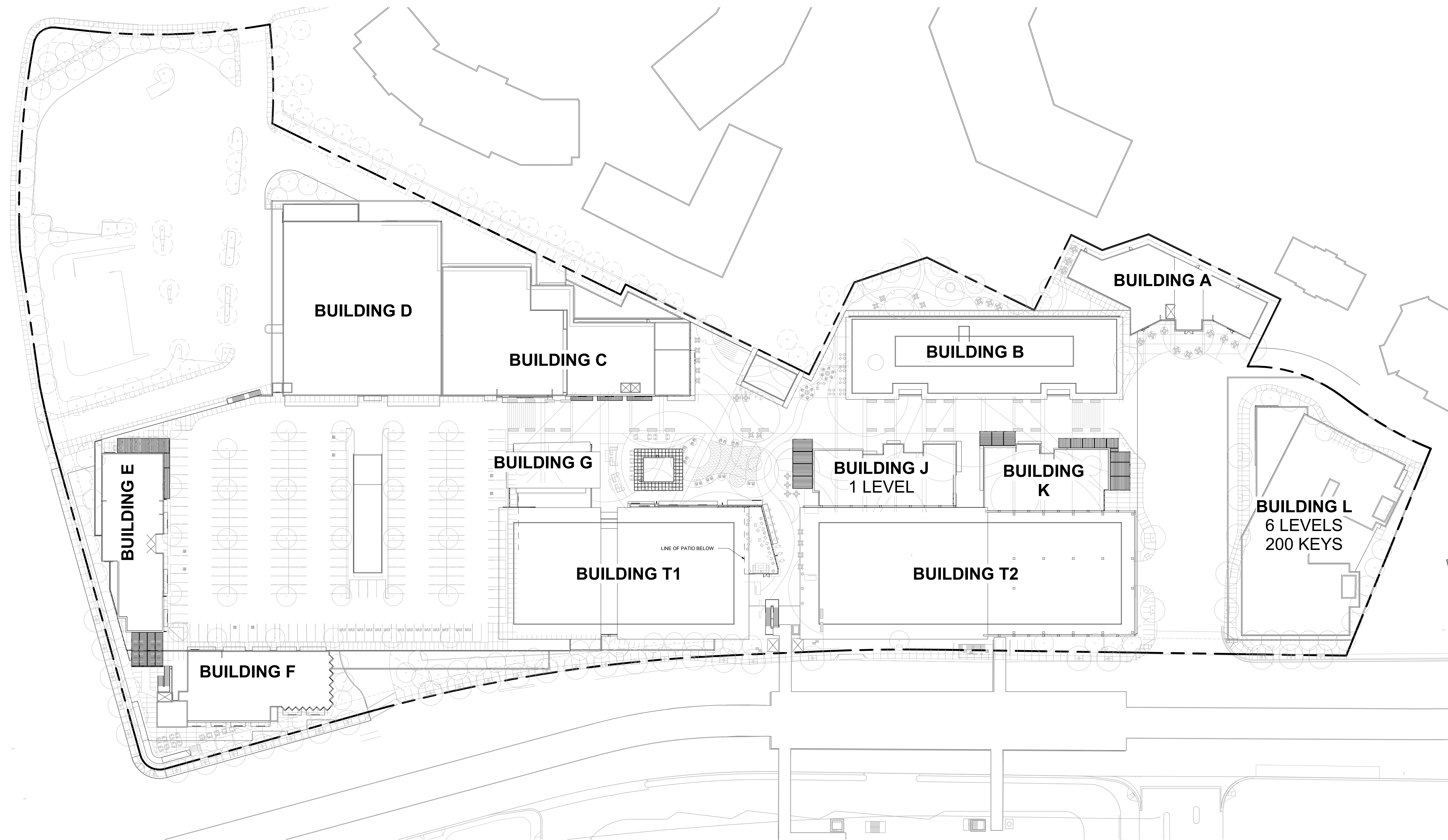
SHEET NO: **ENT2.202** 50 OF 65

PTS #: 477943
 PROJECT #: 477943

Regency Centers. REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187





1 OVERALL ROOF PLAN
SCALE: 1" = 50'-0"

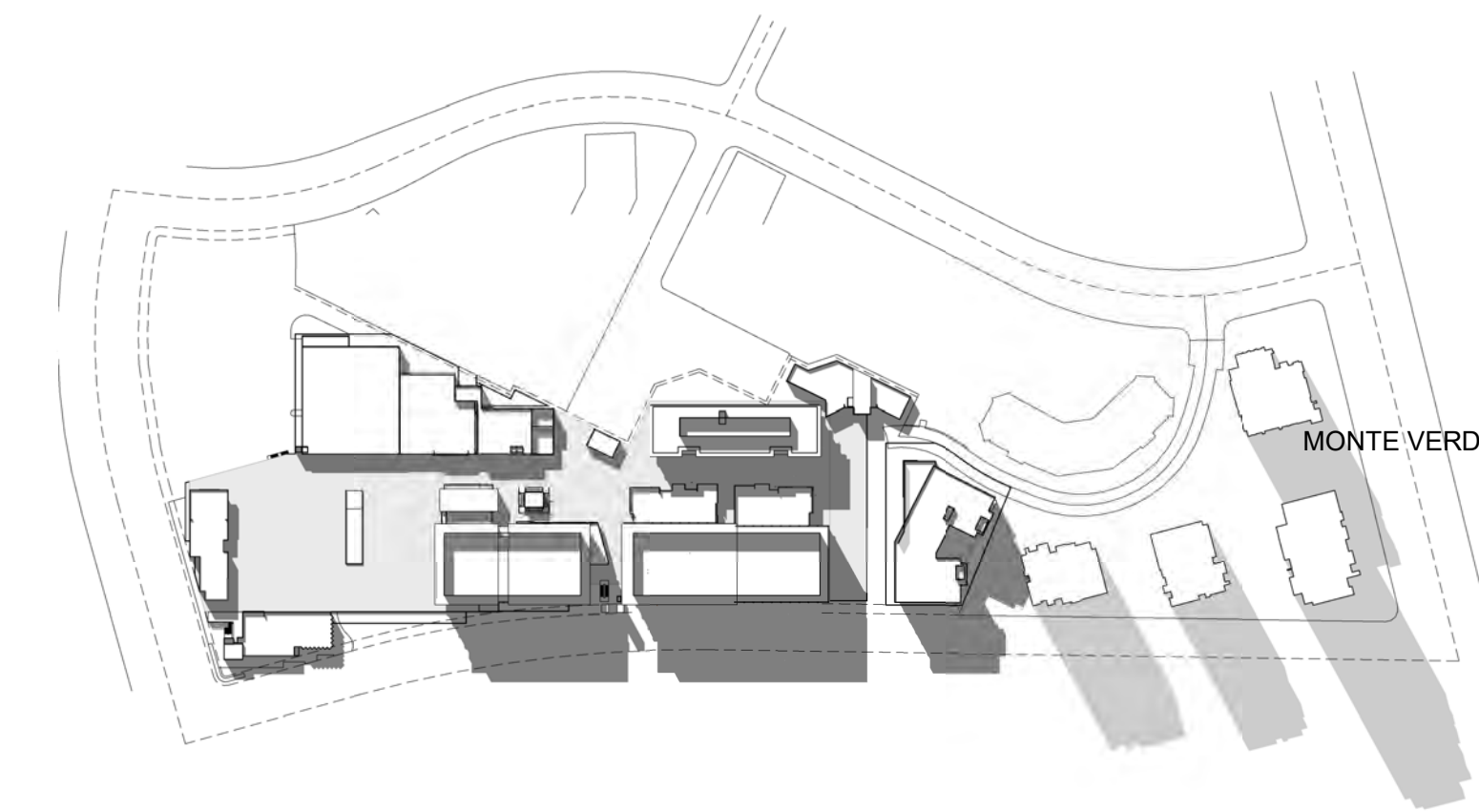
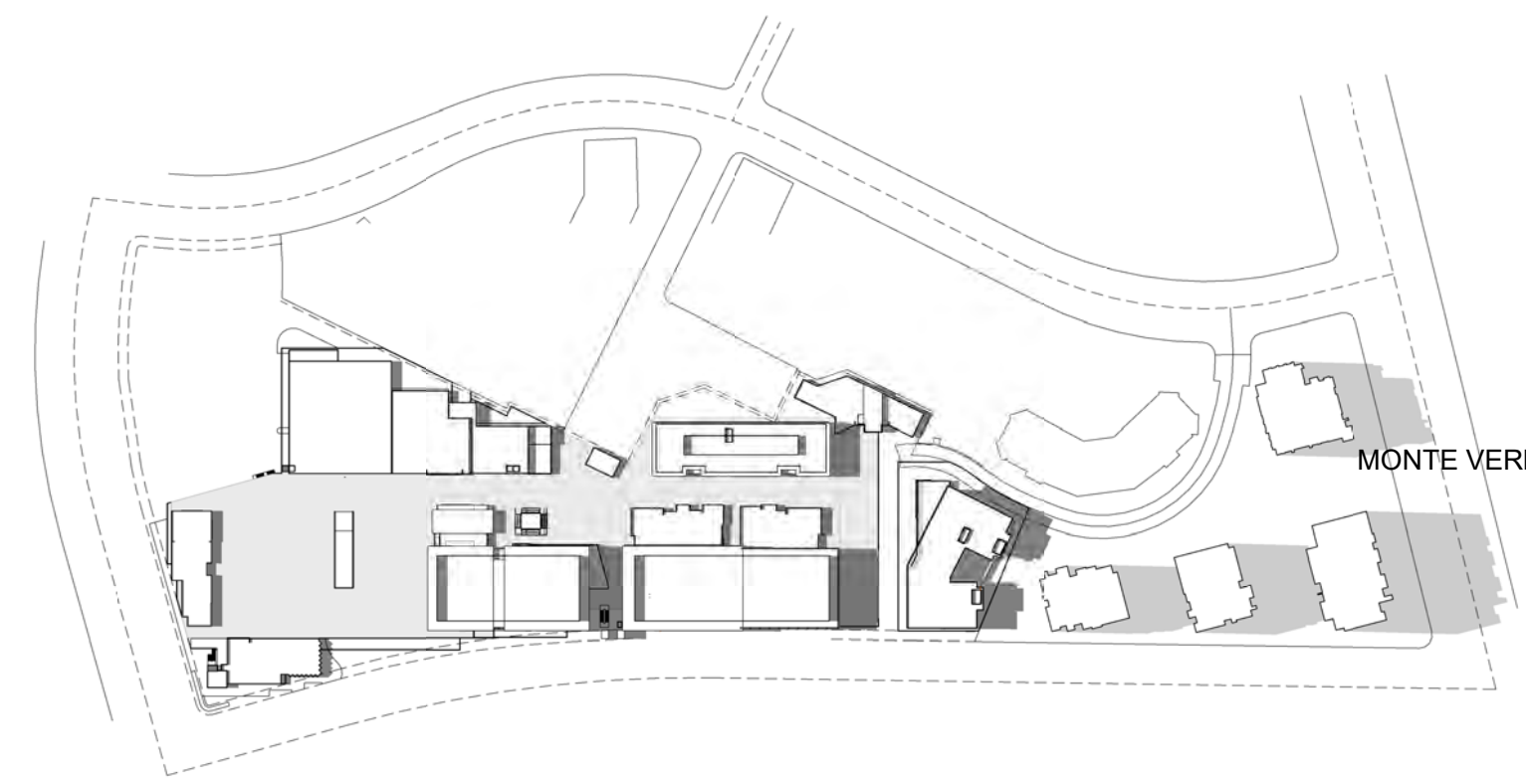
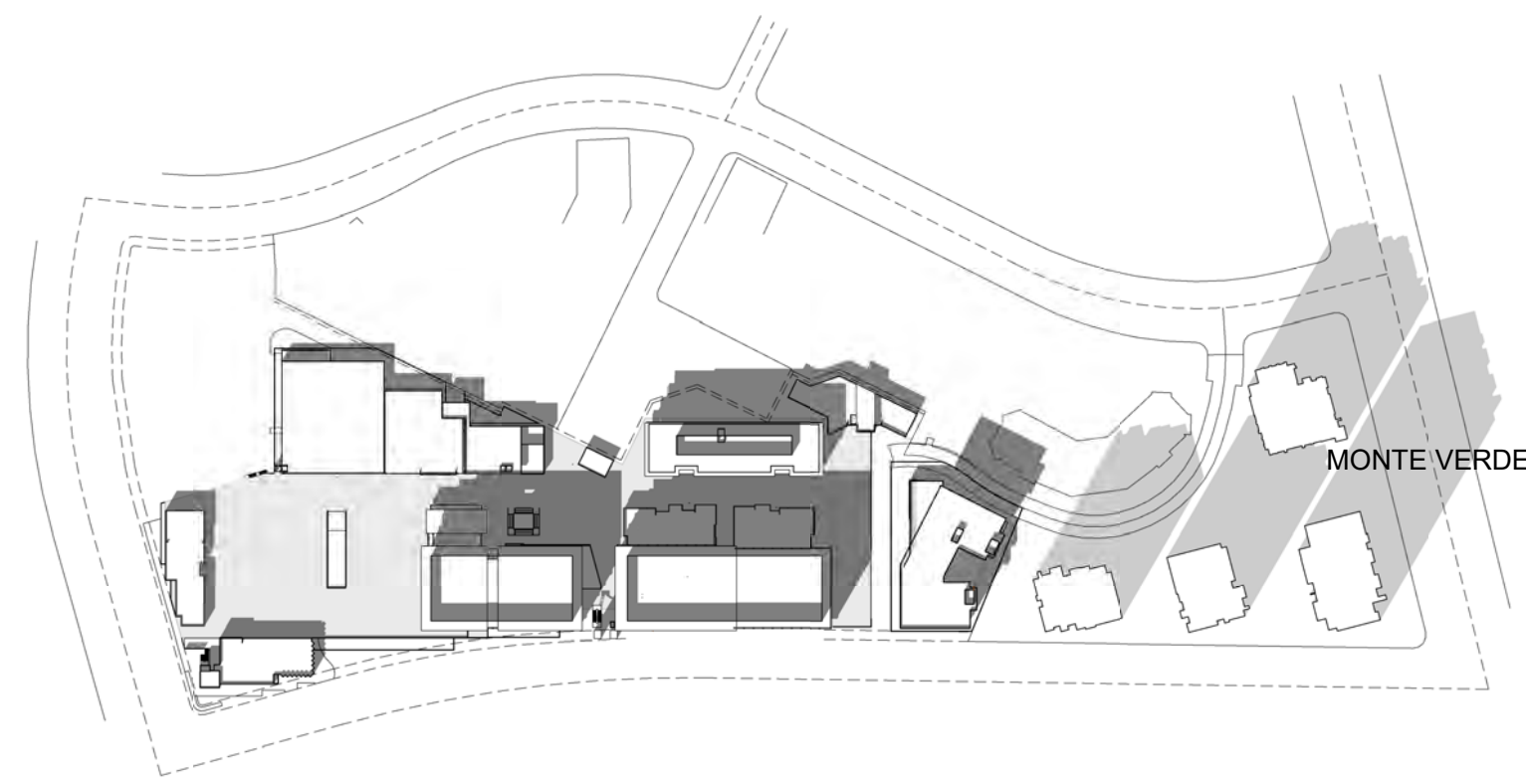
PREPARED BY:	rdc.
NAME:	
ADDRESS:	245 EAST THIRD STREET LONG BEACH, CA 90802
PHONE:	562.628.8000
ORIGINAL DATE:	3/15/2016 (BY CALLISONRTKL)
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REVISION DATE:	06/10/2019 (BY GENSLER)
REVISION DATE:	10/11/2019 (BY RDC)
REVISION DATE:	09/01/2020 (BY RDC)
SHEET TITLE:	OVERALL ROOF PLAN
SHEET NO:	ENT2.300 51 OF 65
PTS #:	477943
PROJECT #:	477943
Regency Centers.	REGENCY CENTERS 420 STEVENS AVE SUITE 320 SOLANA BEACH, CA 92075

9:00 AM

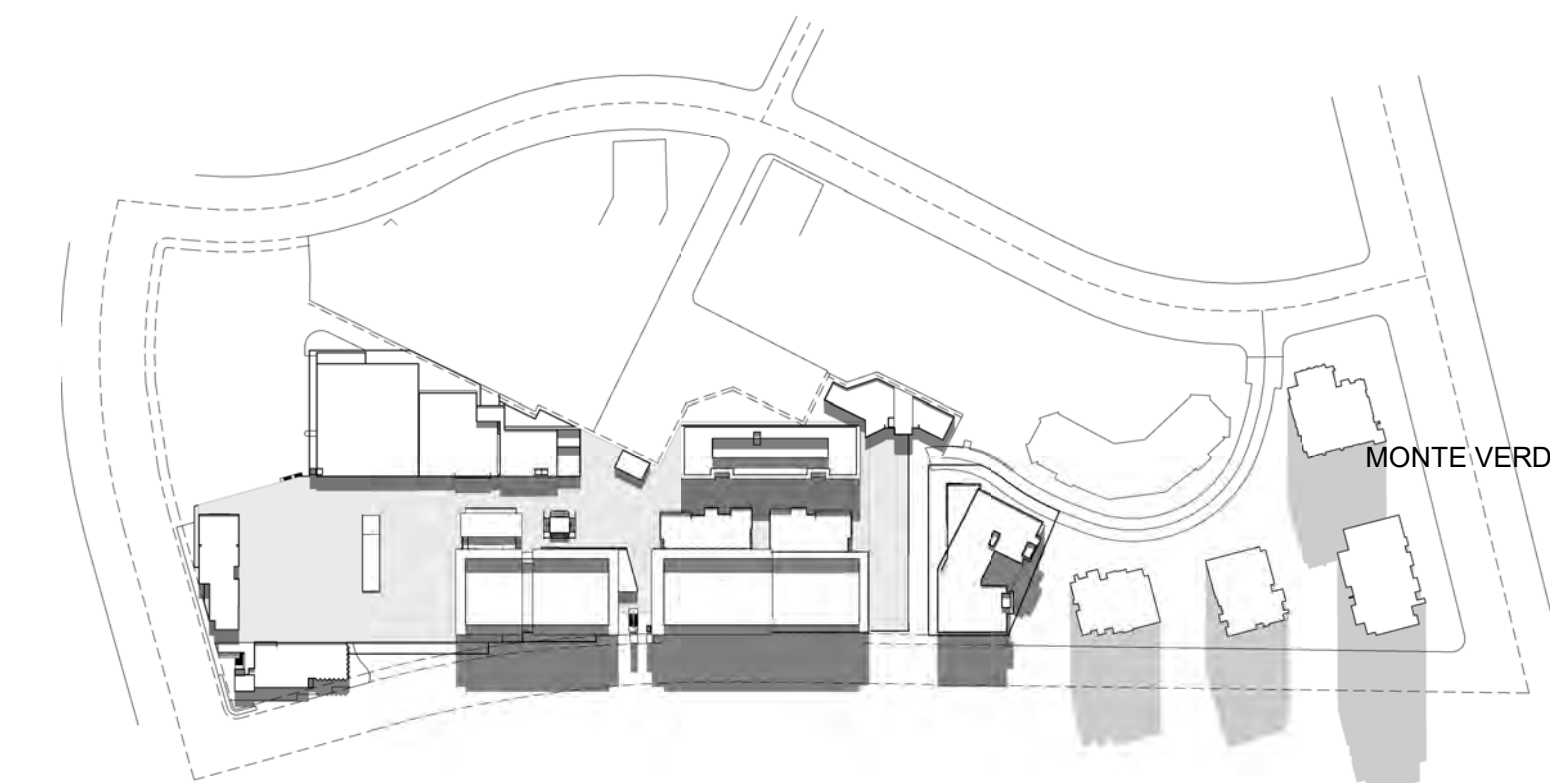
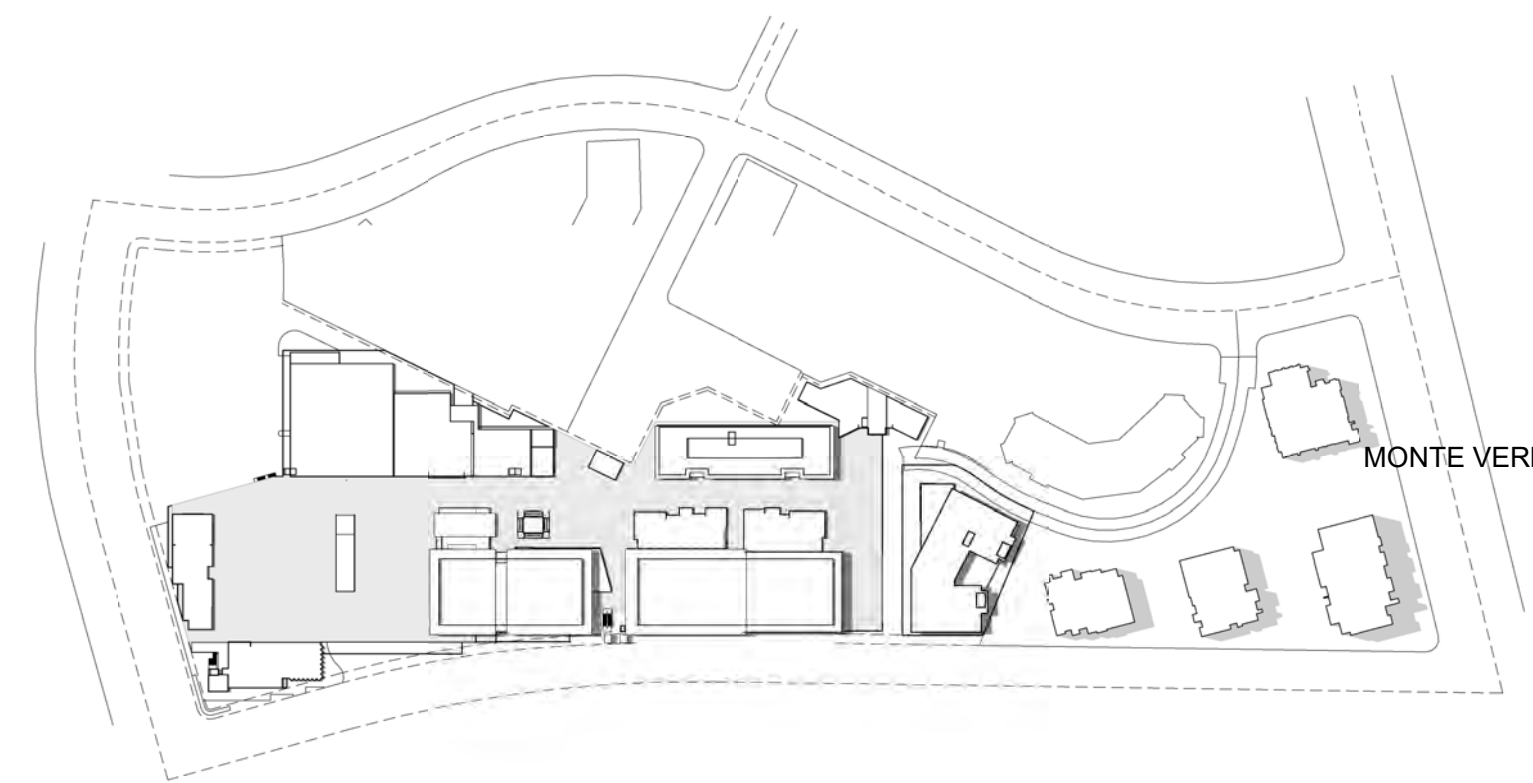
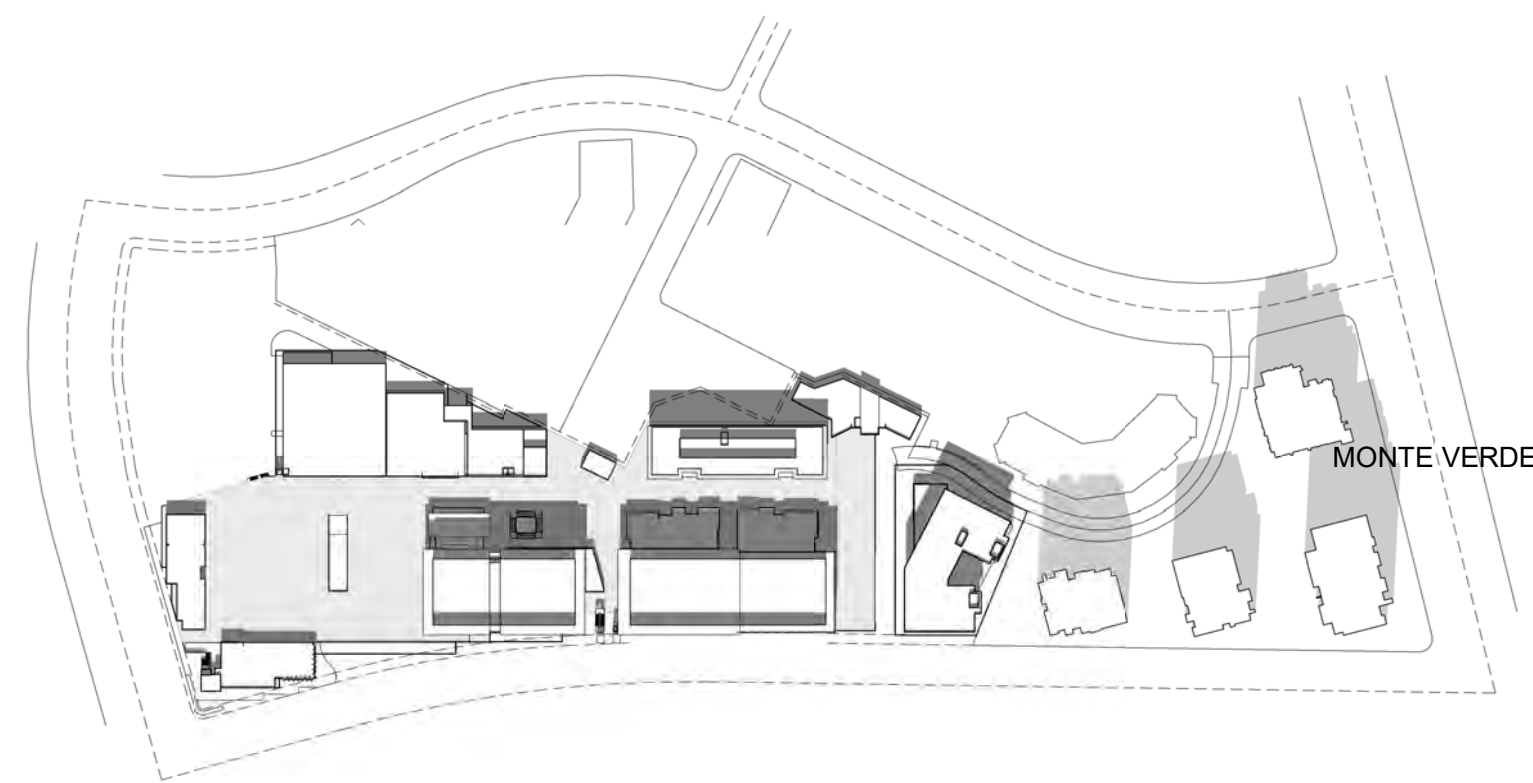
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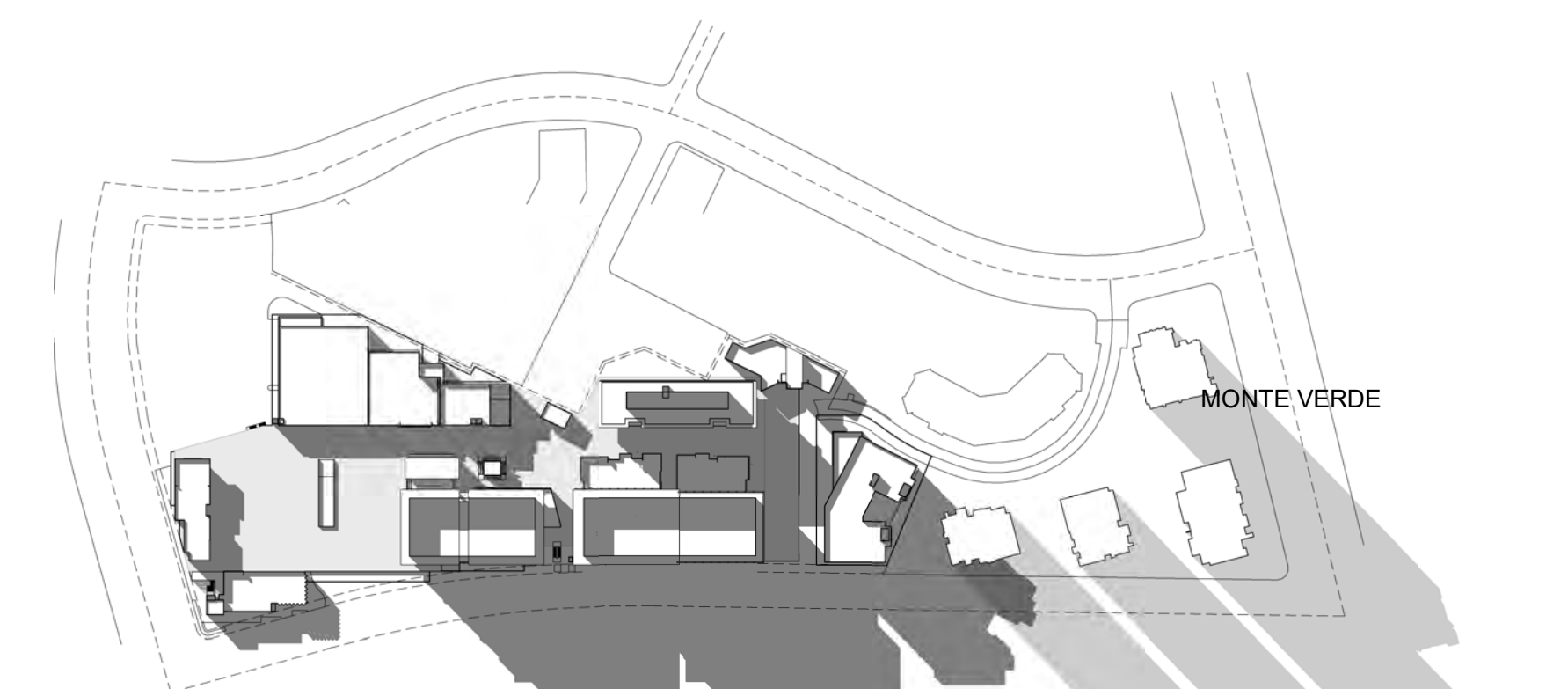
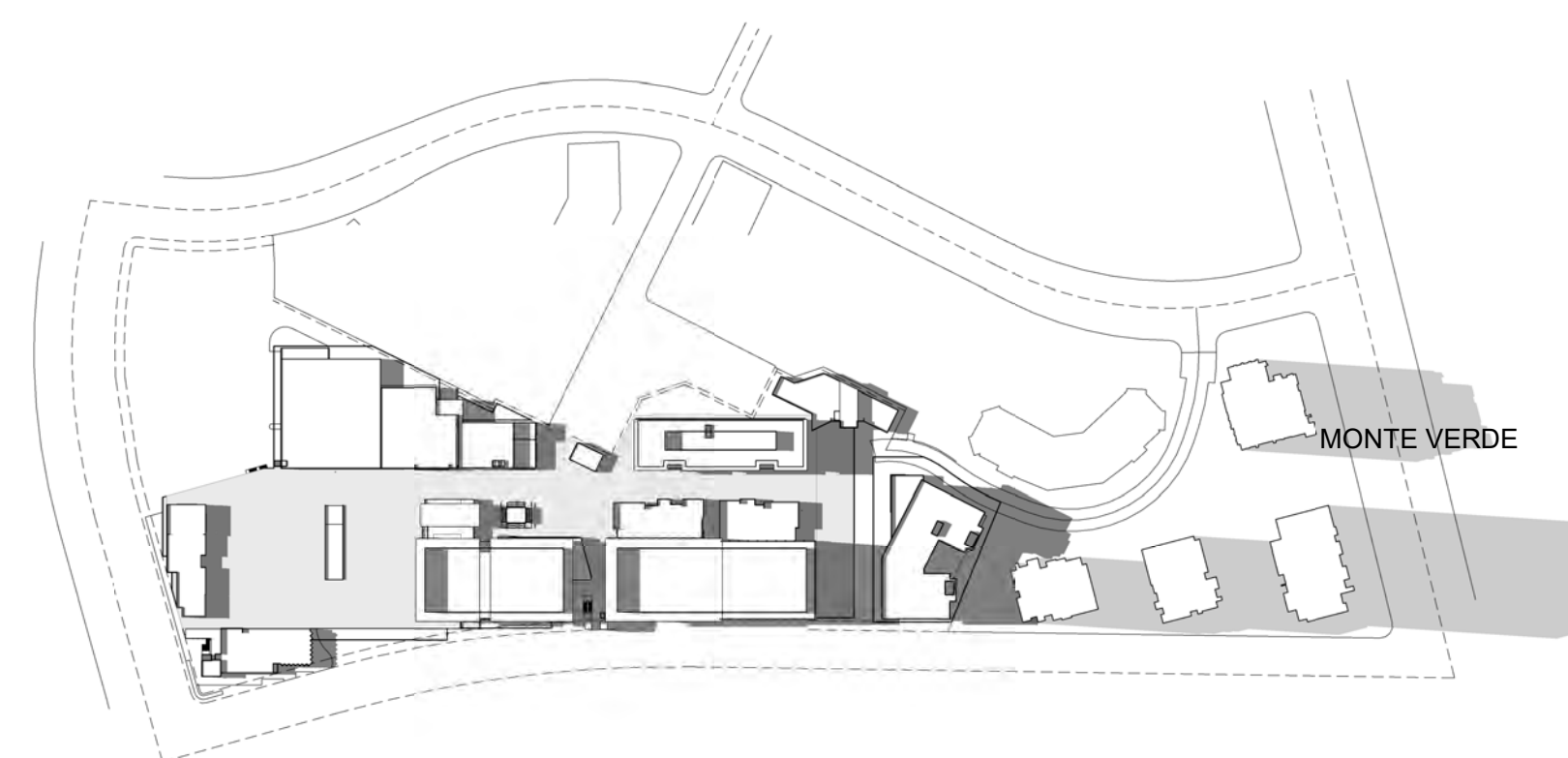
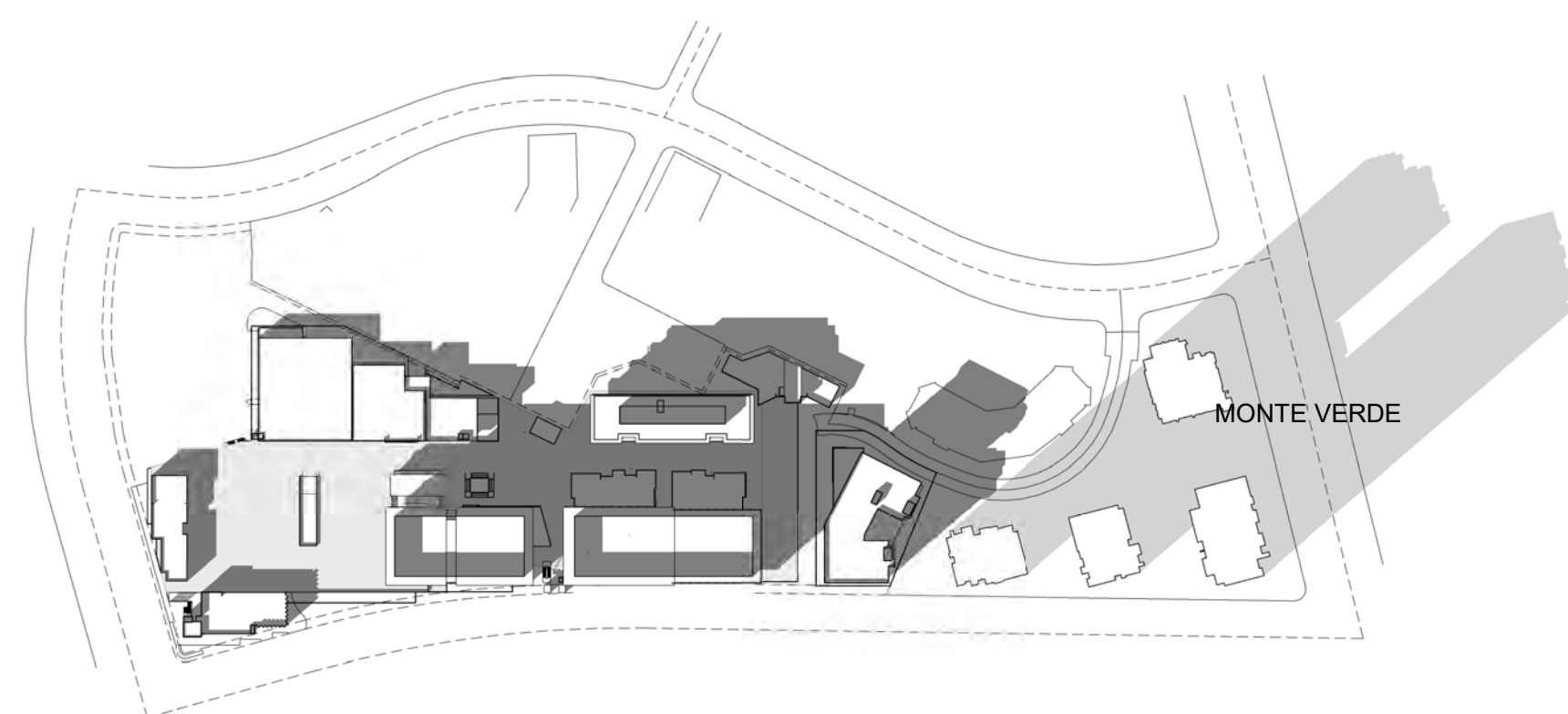
SPRING or FALL
EQUINOX
±March 20 or ±September 23



SUMMER SOLSTICE
±June 21



WINTER SOLSTICE
±December 21

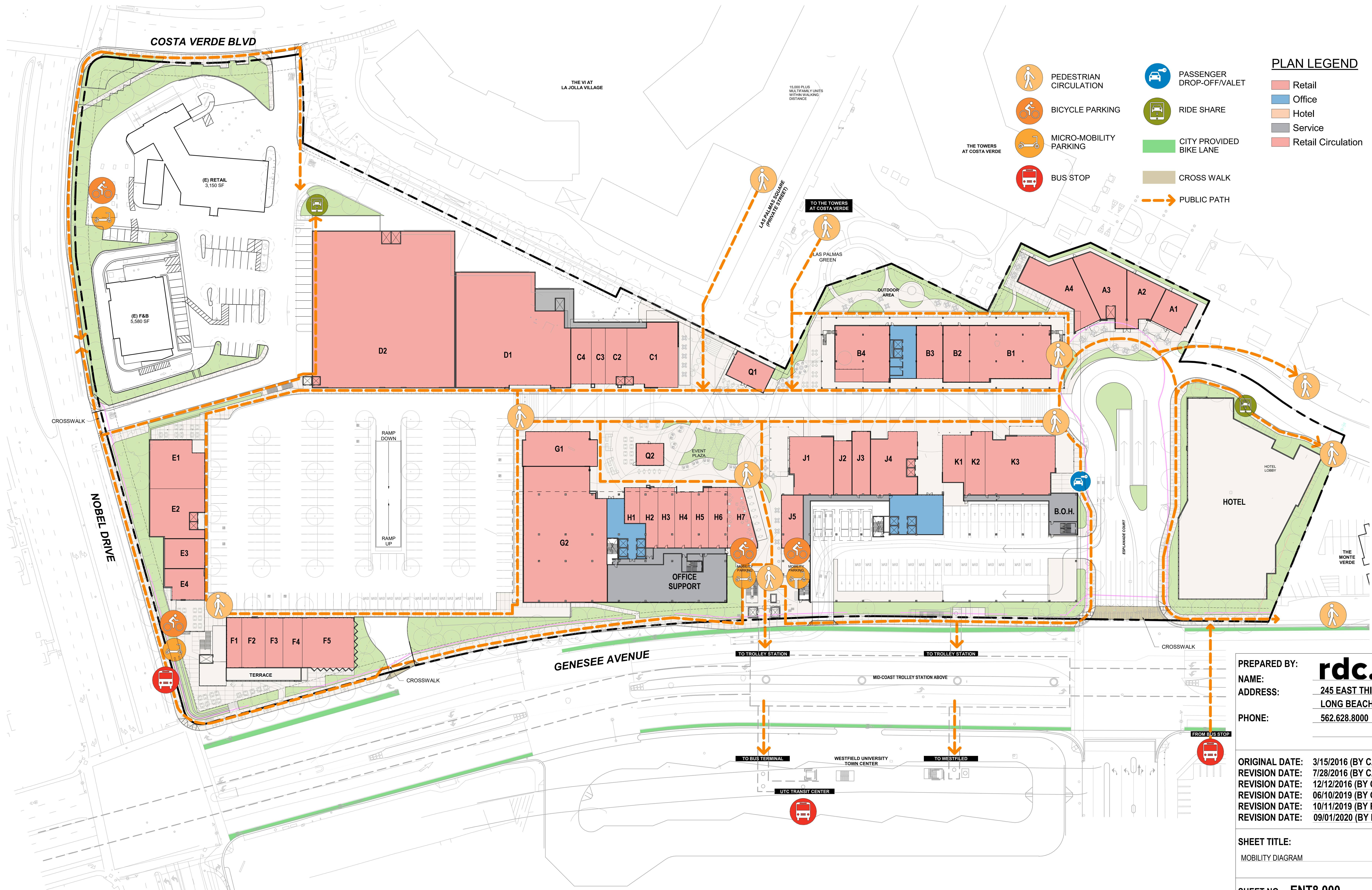


PREPARED BY:	rdc.
NAME:	
ADDRESS:	245 EAST THIRD STREET LONG BEACH, CA 90802
PHONE:	562.628.8000
ORIGINAL DATE:	3/15/2016 (BY CALLISONRTKL)
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REVISION DATE:	06/10/2019 (BY GENSLER)
REVISION DATE:	10/11/2019 (BY RDC)
SHEET TITLE:	SOLAR STUDIES
SHEET NO:	ENT6.000 52 OF 65
PTS #:	477943
PROJECT #:	477943
Regency Centers.	REGENCY CENTERS 420 STEVENS AVE SUITE 320 SOLANA BEACH, CA 92075

















• These perspectives have been included for illustrative purposes only. The intent of this exhibit is to convey community-level design concepts rather than specific architectural or landscape information. For project details, please refer to the other sheets in this plan set.

PREPARED BY:	rdc.
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REVISION DATE:	06/10/2019 (BY GENSLER)
REVISION DATE:	10/11/2019 (BY RDC)
SHEET TITLE:	PERSPECTIVES BETWEEN T1 & T2
SHEET NO:	ENT7.000 53 OF 65
PTS #:	477943
PROJECT #:	477943
Regency Centers.	REGENCY CENTERS 420 STEVENS AVE SUITE 320 SOLANA BEACH, CA 92075



PLAN LEGEND

-  PEDESTRIAN CIRCULATION
-  BICYCLE PARKING
-  MICRO-MOBILITY PARKING
-  BUS STOP
-  PASSENGER DROP-OFF/VALET
-  RIDE SHARE
-  CITY PROVIDED BIKE LANE
-  CROSS WALK
-  PUBLIC PATH
-  Retail
-  Office
-  Hotel
-  Service
-  Retail Circulation

1 MOBILITY DIAGRAM
SCALE: 1" = 40'-0"

NOTE: PROJECT PROVIDE CONNECTIVITY TO CITY PROVIDED BIKE LANE, TYP.

PREPARED BY: **rdc.**
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PHONE: 562.628.8000

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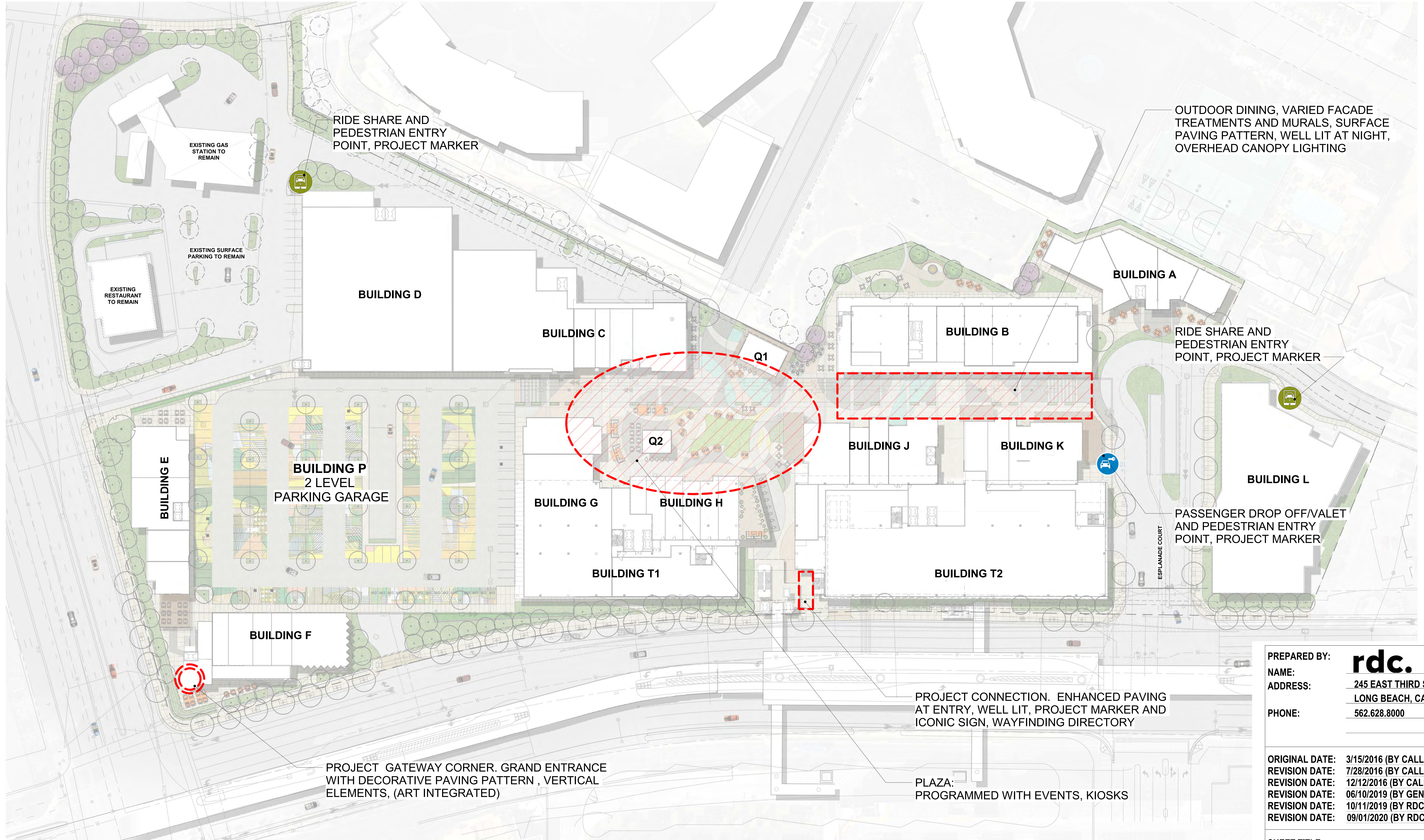
SHEET TITLE:
MOBILITY DIAGRAM

SHEET NO: **ENT8.000** 54 OF 65

PTS #: 477943
PROJECT #: 477943

Regency Centers. REGENCY CENTERS
420 STEVENS AVE SUITE 320
SOLANA BEACH, CA 92075

8/28/2020 5:33:28 PM C:\Local\Rev\ARCH\19187\19-Costa Verde Center Entitlement_RDC_sing_long.rvt



PREPARED BY: **rdc.**
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 REVISION DATE: 10/11/2019 (BY RDC)
 REVISION DATE: 09/01/2020 (BY RDC)

SHEET TITLE:
 URBAN DESIGN STRATEGY DIAGRAM

SHEET NO: **ENT8.001** 55 OF 65

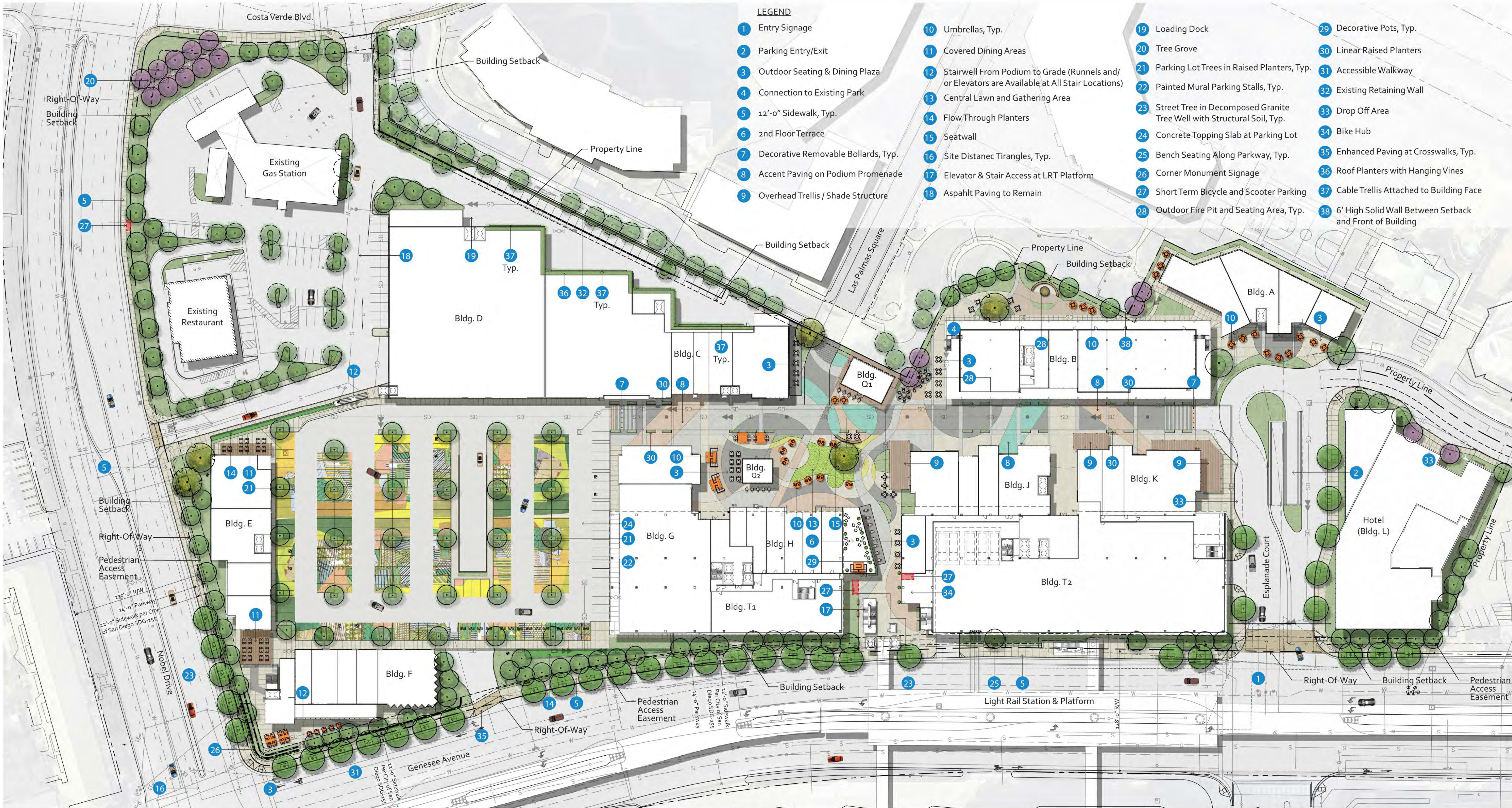
PTS #: 477943
 PROJECT #: 477943

Regency Centers REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187

1 UD STRATEGY DIAGRAM
 SCALE: 1" = 40'-0"

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LEGEND

- 1 Entry Signage
- 2 Parking Entry/Exit
- 3 Outdoor Seating & Dining Plaza
- 4 Connection to Existing Park
- 5 12'-0" Sidewalk, Typ.
- 6 2nd Floor Terrace
- 7 Decorative Removable Bollards, Typ.
- 8 Accent Paving on Podium Promenade
- 9 Overhead Trellis / Shade Structure
- 10 Umbrellas, Typ.
- 11 Covered Dining Areas
- 12 Stairwell From Podium to Grade (Runnels and/or Elevators are Available at All Stair Locations)
- 13 Central Lawn and Gathering Area
- 14 Flow Through Planters
- 15 Seatwall
- 16 Site Distanc Tirangles, Typ.
- 17 Elevator & Stair Access at LRT Platform
- 18 Asphalt Paving to Remain
- 19 Loading Dock
- 20 Tree Grove
- 21 Parking Lot Trees in Raised Planters, Typ.
- 22 Painted Mural Parking Stalls, Typ.
- 23 Street Tree in Decomposed Granite Tree Well with Structural Soil, Typ.
- 24 Concrete Topping Slab at Parking Lot
- 25 Bench Seating Along Parkway, Typ.
- 26 Corner Monument Signage
- 27 Short Term Bicycle and Scooter Parking
- 28 Outdoor Fire Pit and Seating Area, Typ.
- 29 Decorative Pots, Typ.
- 30 Linear Raised Planters
- 31 Accessible Walkway
- 32 Existing Retaining Wall
- 33 Drop Off Area
- 34 Bike Hub
- 35 Enhanced Paving at Crosswalks, Typ.
- 36 Roof Planters with Hanging Vines
- 37 Cable Trellis Attached to Building Face
- 38 6' High Solid Wall Between Setback and Front of Building

PLANTING LEGEND:

QTY:	Botanical Name	Common Name	Mature Height/Spread	Function
5	<i>Platanus racemosa</i> <i>Quercus Agrifolia</i>	California Sycamore Coast Live Oak	60' x 40' 40' x 40'	Canopy Tree Canopy Tree
116	<i>Metrosideros excelsa</i> <i>Quercus ilex</i> <i>Rhus lancea</i> <i>Tipuana tipu</i>	New Zealand Christmas Tree Holly Oak African Sumac Tipu Tree	25' x 25' 20' x 25' 25' x 25' 50' x 45'	Shade Tree Shade Tree Shade Tree Shade Tree
16	<i>Agonis flexuosa 'Jervis Bay'</i> <i>Aloe tongaensis 'Medusa'</i> <i>Arbutus 'Marini'</i> <i>Jacaranda mimosifolia</i> <i>Cercis 'Forest Pansy'</i>	Red Peppermint Tree Medusa Aloe Marina Strawberry Tree Jacaranda Forest Pansy Redbud	18' x 15' 10' x 8' 15' x 15' 25' x 25' 15' x 18'	Accent Tree Accent Tree Accent Tree Accent Tree Accent Tree
38	<i>Jacaranda mimosifolia</i> <i>Platanus racemosa</i> <i>Podocarpus gracilior</i> <i>Quercus ilex</i>	Jacaranda California Sycamore Fern Pine Holly Oak	25' x 25' 60' x 40' 25' x 20' 20' x 15'	Accent Tree Accent Tree Evergreen Tree Evergreen Tree
20	<i>Bermuda</i>	Hybrid Bermuda	N/A	N/A

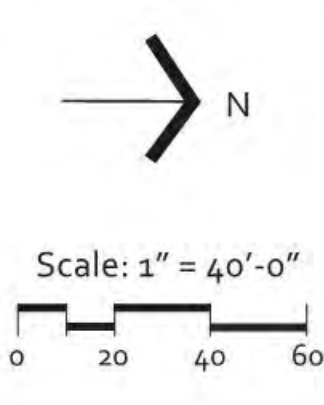
DESIGN STATEMENT

The design for Costa Verde Center creates a unique destination for the residents of the La Jolla and the University Towne Centre Community. The design is inspired by the network of local canyons, like the site adjacent Rose Canyon, along with coastal influences and features such as tide pools and kelp forests.

Esplanade Court invites visitors into the site with a grand entrance and locally inspired signage. The rhythm of the project architecture creates a series of pocket spaces which soften and accent the facade of the building, while establishing niches for people to gather and linger. A plaza, set within the retail experience, includes dining opportunities and a large outdoor room for visitors to enjoy the San Diego sun. This space is enclosed by the architecture along with a series of shade structures creating a sequence of enclosure. The streetscape includes winding patterns of paving color, raised decorative planters & pots, and smaller seating nodes. The plaza space can be used for year round events, and caters to the needs of working professionals, along with the community to create a fun and engaging experience. The podium level parking lot serves the adjacent retail establishments and provides art spaces in the stalls for community building opportunities.

Costa Verde Center can also be experienced on the rooftops. Visitors can access the second story of the retail spaces to experience local art, and murals, which can also be viewed from the office spaces rising above.

Whether visiting to shop, people-watch, or meet a neighbor; the re-visioning of Costa Verde Center creates an authentic town center to serve the residents of the surrounding communities.



PREPARED BY: SCHMIDT DESIGN GROUP

NAME: 1310 ROSECRANS ST. SUITE G
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PHONE: 619.236.1462

ORIGINAL DATE: 3/15/2016 (BY GENSLER)
REVISION DATE: 7/28/2016 (BY GENSLER)
REVISION DATE: 12/12/2016 (BY GENSLER)
REVISION DATE: 06/10/2019 (BY GENSLER)
REVISION DATE: 10/11/2019 (BY RDC)
REVISION DATE: 09/01/2020 (BY RDC)

SHEET TITLE: LANDSCAPE PLAN

SHEET NO: ENT- L0.100 56 OF 65

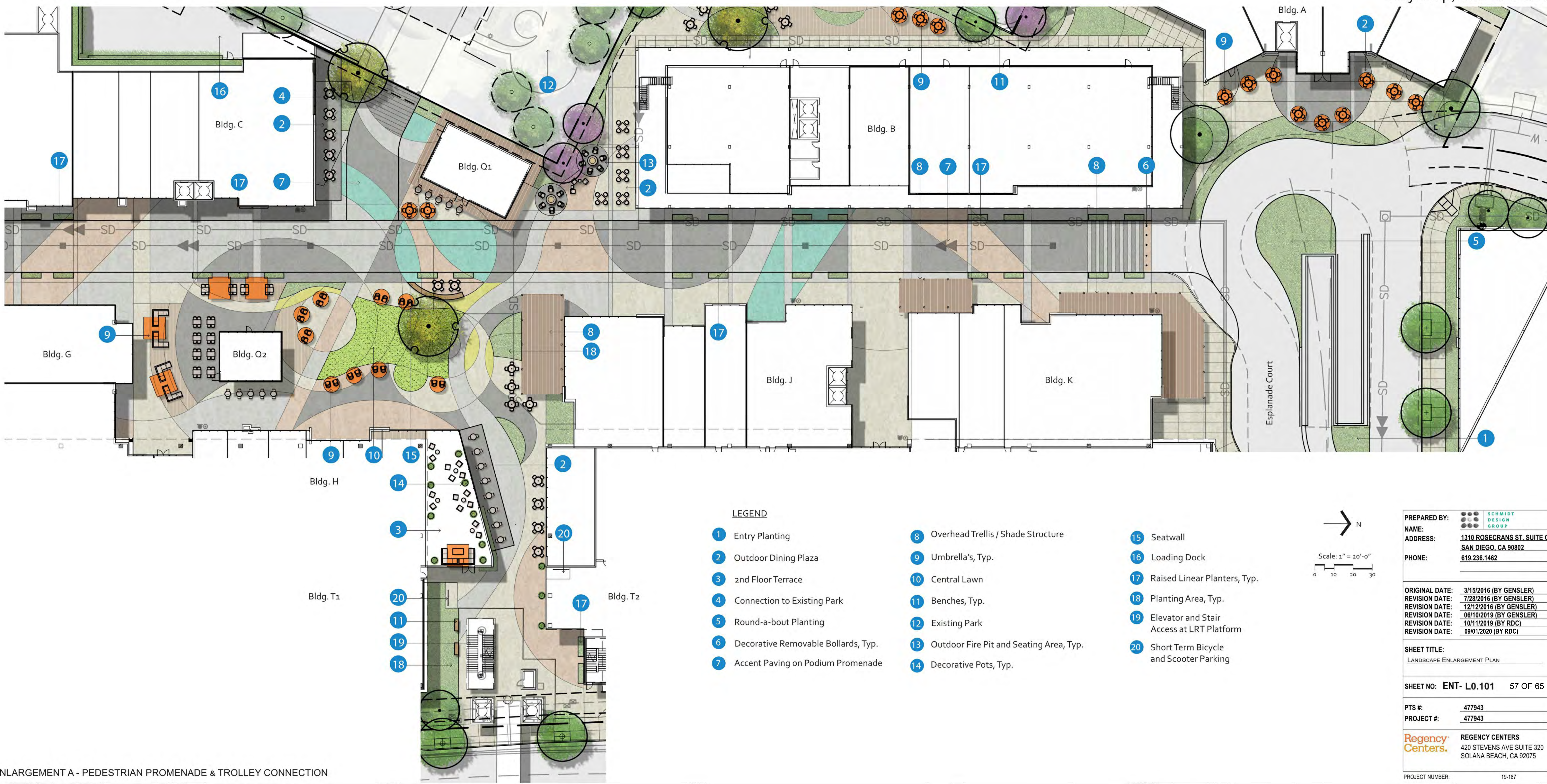
PTS #: 477943
PROJECT #: 477943

Regency Centers
420 STEVENS AVE SUITE 320
SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187

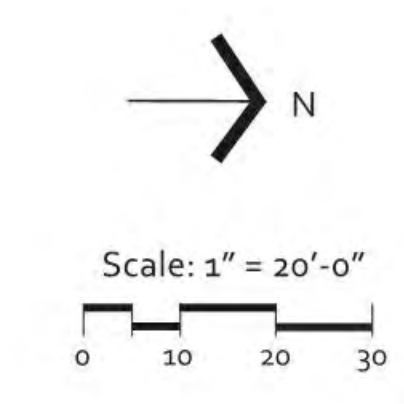


Key Map, Not To Scale



LEGEND

- 1 Entry Planting
- 2 Outdoor Dining Plaza
- 3 2nd Floor Terrace
- 4 Connection to Existing Park
- 5 Round-a-bout Planting
- 6 Decorative Removable Bollards, Typ.
- 7 Accent Paving on Podium Promenade
- 8 Overhead Trellis / Shade Structure
- 9 Umbrella's, Typ.
- 10 Central Lawn
- 11 Benches, Typ.
- 12 Existing Park
- 13 Outdoor Fire Pit and Seating Area, Typ.
- 14 Decorative Pots, Typ.
- 15 Seatwall
- 16 Loading Dock
- 17 Raised Linear Planters, Typ.
- 18 Planting Area, Typ.
- 19 Elevator and Stair Access at LRT Platform
- 20 Short Term Bicycle and Scooter Parking



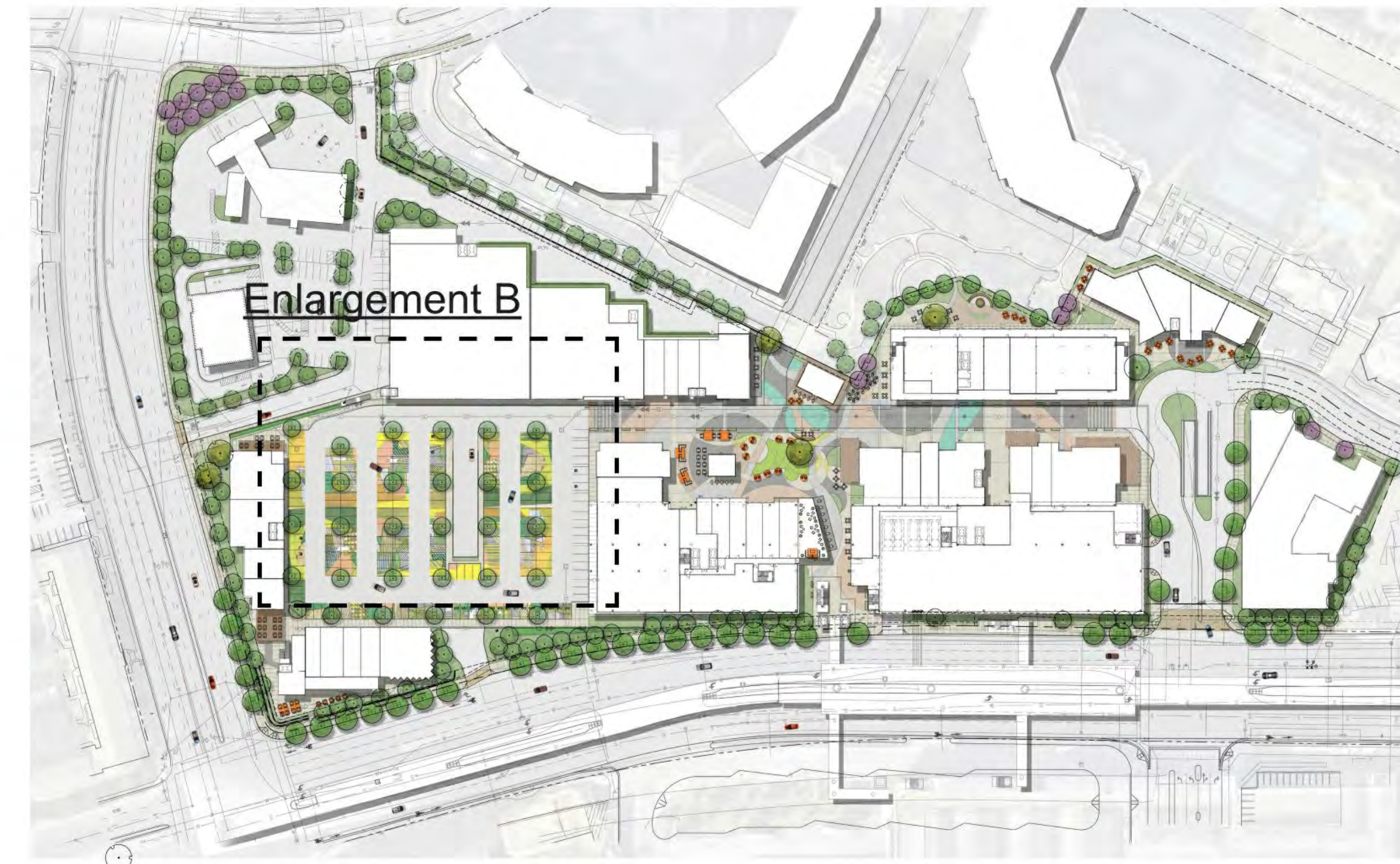
PREPARED BY:	SCHMIDT DESIGN GROUP
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REVISION DATE:	06/10/2019 (BY GENSLER)
REVISION DATE:	10/11/2019 (BY RDC)
REVISION DATE:	09/01/2020 (BY RDC)
SHEET TITLE:	LANDSCAPE ENLARGEMENT PLAN
SHEET NO:	ENT- L0.101 57 OF 65
PTS #:	477943
PROJECT #:	477943
Regency Centers.	REGENCY CENTERS 420 STEVENS AVE SUITE 320 SOLANA BEACH, CA 92075
PROJECT NUMBER:	19-187



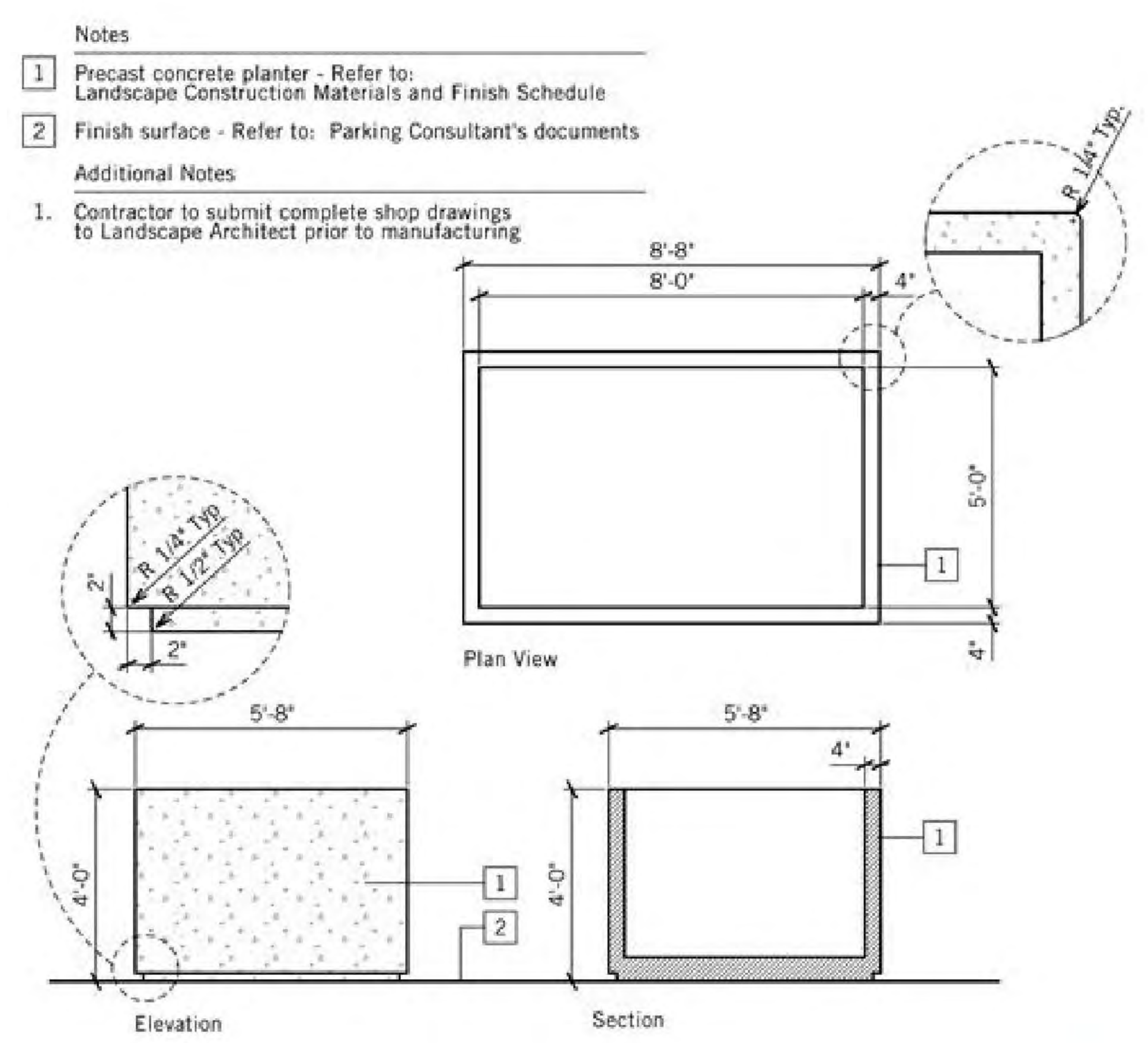
ENLARGEMENT B - PARKING LOT

LEGEND

- | | | |
|---------------------------------------|----------------------------------|---|
| 1 Outdoor Seating & Dining Plaza | 7 Umbrella's, Typ. | 13 Parking Lot Trees in Raised Planters, Typ. |
| 2 Decorative Removable Bollards, Typ. | 8 Accessible Ramp | 14 Painted Mural Parking Stalls, Typ. |
| 3 Sidewalk, Typ. | 9 Stairwell From Podium to Grade | 15 Raised Linear Planters |
| 4 2nd Floor Dining Terrace | 10 Flow Through Planters | 16 Concrete Topping Slab at Parking Lot |
| 5 Accent Paving on Podium Promenade | 11 Corner Monument Signage | 17 Planting Area, Typ. |
| 6 Overhead Trellis / Shade Structure | 12 AC Paving to Remain | |



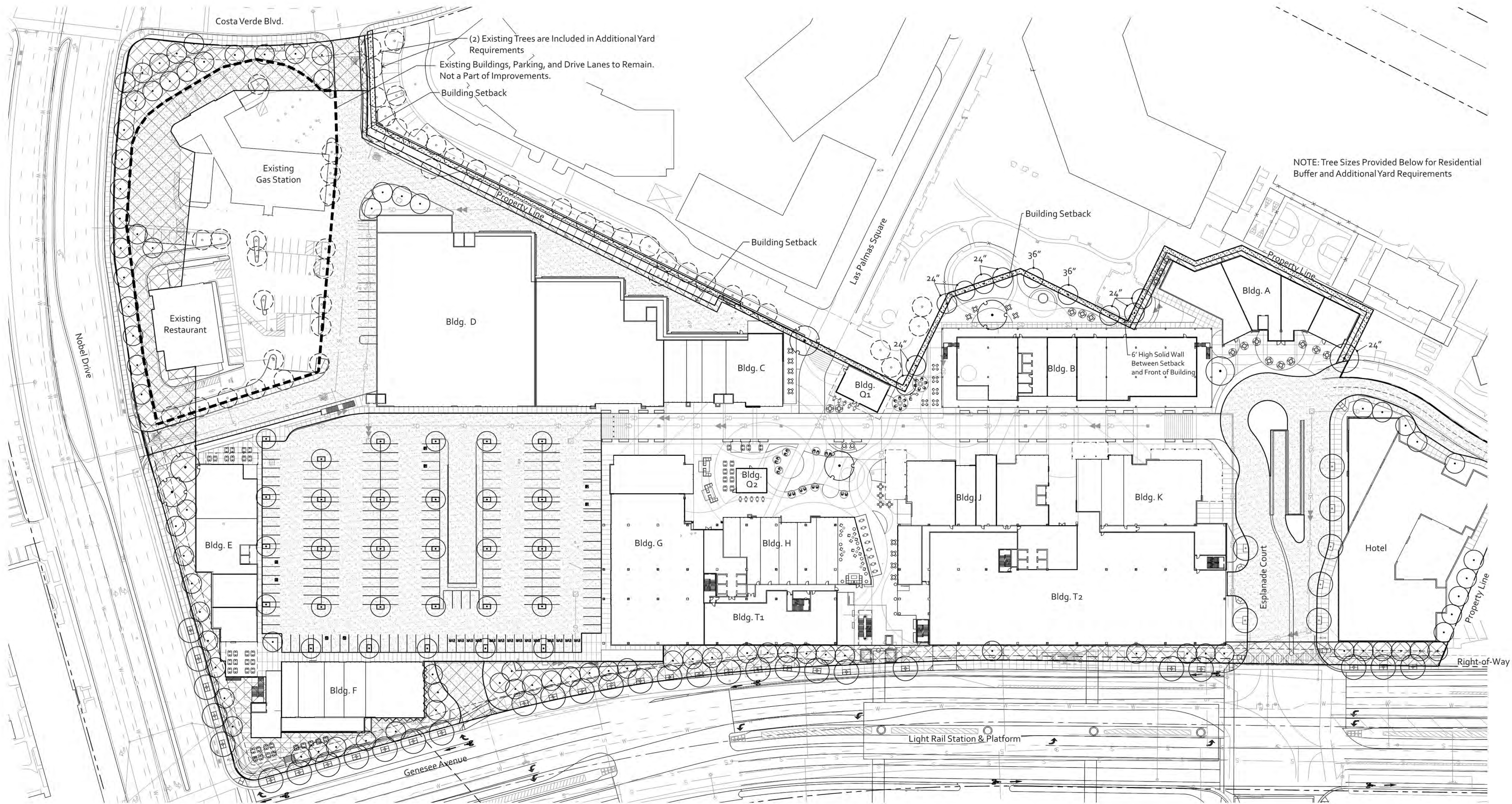
Key Map, Not To Scale



Precast Concrete Planter

13

PREPARED BY:	SCHMIDT DESIGN GROUP
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REVISION DATE:	10/11/2019 (BY RDC)
REVISION DATE:	09/01/2020 (BY RDC)
SHEET TITLE:	LANDSCAPE ENLARGEMENT PLAN
SHEET NO:	ENT- L0.105 58 OF 65
PTS #:	477943
PROJECT #:	477943
Regency Centers.	REGENCY CENTERS 420 STEVENS AVE SUITE 320 SOLANA BEACH, CA 92075
PROJECT NUMBER:	19-187



NOTE: Tree Sizes Provided Below for Residential Buffer and Additional Yard Requirements

LANDSCAPE & IRRIGATION MAINTENANCE NOTES:

- A. This site will be utilizing reclaimed water. An automatic, water-efficient irrigation system shall be provided to establish and maintain landscaping. Irrigation systems shall meet the following design requirements:
 1. Include and install a city-approved electronically controlled automatic rain shut-off device. Approved devices include: W.C.S. 'Rainsguard', Toro 'Rainswitch', and Hilton 'Mini-Click'.
 2. Include and install a city-approved moisture sensing device. Approved devices include: W.C.S. 'Hydrovisor' & Rainbird 'Aqua Mister'.
 3. Include and install low precipitation rate sprinkler nozzles. Heads shall be placed to minimize over spray and to reduce run-off of water from over watering.
 4. Include and install an automatic electric controller that shall be seasonally adjusted to operate the irrigation system with the least practical amount of water applied.
- B. All landscape and irrigation shall conform to the standards of the city-wide landscape regulations and the city of San Diego land development manual landscape standards and all other landscape related city and regional standards.
- C. All required landscape areas shall be maintained by the owner (Regency Centers).
- D. Landscape areas shall be free of debris and litter and all plant material shall be maintained in a healthy growing condition. Diseased or dead plant material shall be satisfactorily treated or replaced per the condition of the permit.
- E. The owner (Regency Centers) shall also be responsible for maintaining plant and irrigation material within the right-of-way.
- F. Required per city, root barriers are required for all street trees within 6' of any hardscape, curbs or walls. They shall be 24" deep, 0.8" thick, and be placed flush with the top of any adjacent hardscape or curb and be centered 10' on either side of the tree (20' long total). See city detail #726.
- G. Minimum tree/improvement separation distance:
 - Traffic signals/stop sign - 30 ft
 - Underground utility lines - 5 ft
 - Above ground utility structures - 10 ft
 - Driveways - 10 ft
- H. Protect roots from flooding, erosion, chemical spills, and excessive wetting & drying during re-watering. Maintain existing grade within the drip line. Cut roots approximately 6" from new construction & seal cut with wood paint made by flintkote or approved equal.
- I. Maintain & document a tree-watering schedule during construction.
- J. Replace damaged trees with one of equal size.

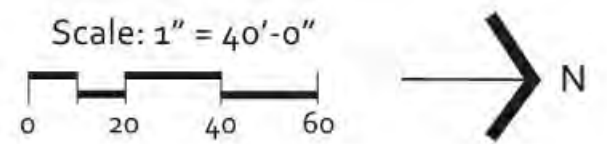
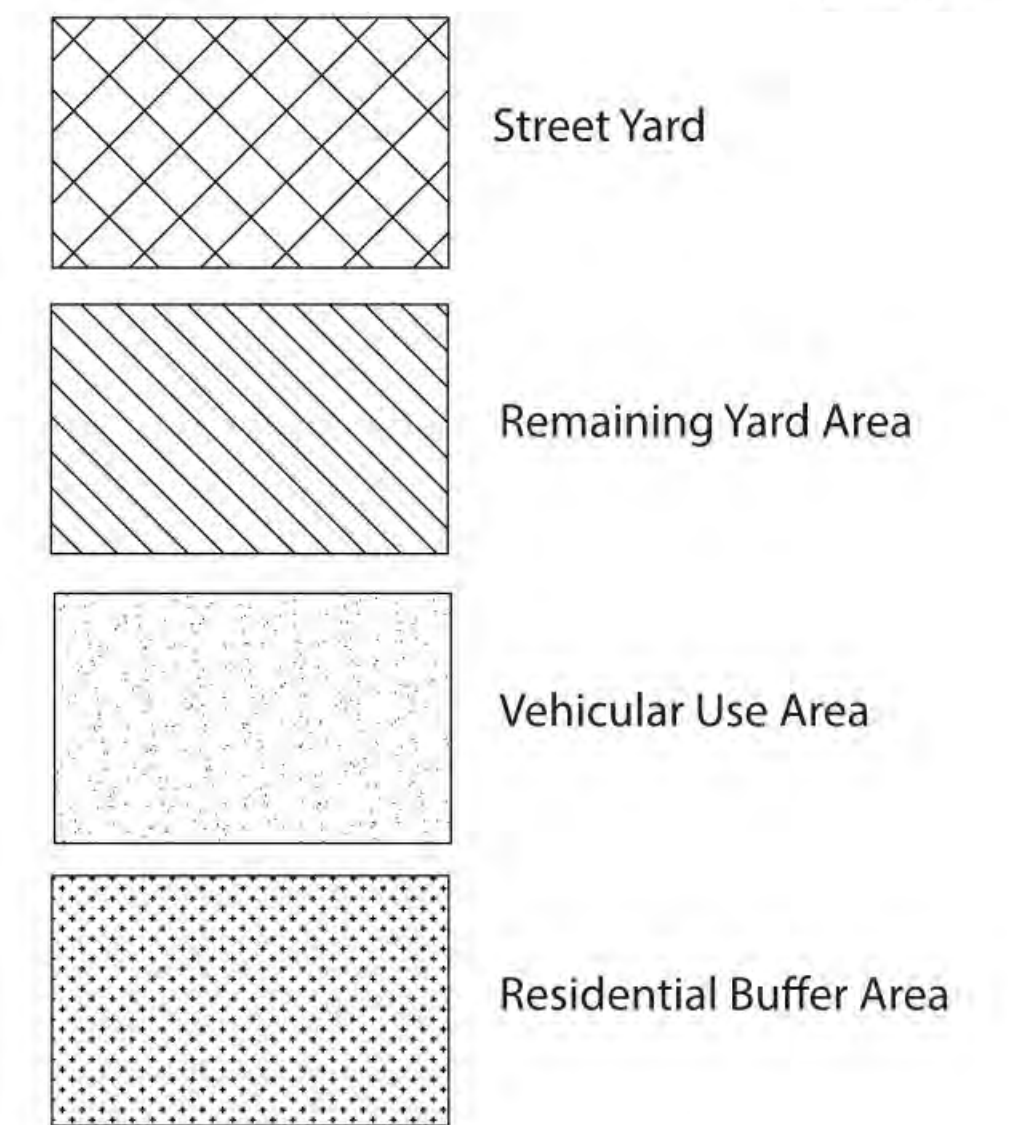
EROSION AND SEDIMENT CONTROL NOTES:

- A. Prior to construction the contractor shall prepare and submit for approval an Erosion control plan identifying:
 - Adequate perimeter bmp location
 - Adequate sediment control bmp location
 - Adequate tracking control bmp location
 - Adequate material storage control bmp location
 - Adequate control for graded areas
- B. The contractor shall maintain all erosion control devices in working order to the satisfaction of the owner's representative throughout the construction phase of the project and until permanent ground cover and landscaping is established.
- C. The contractor shall install additional erosion control measures as may be required by the owner's representative due to completed grading operations or unforeseen circumstances which may arise.
- D. The contractor shall be responsible for and shall take necessary precautions to prevent public trespass onto areas where impounded waters create a hazardous condition.
- E. Graded areas around the project perimeter must drain away from the face of slope at the conclusion of each working day.
- F. All gravel bags shall have 3/4 inch minimum aggregate.
- G. All exposed disturbed areas must have erosion prevention controls properly installed including building pads, unfinished roads and slopes. Slopes less than 3:3 or 1:3 (vertical vs. horizontal) may use properly designed and installed de-silting basins at all discharge points in lieu of this requirement.
- H. Adequate perimeter protection bmps must be installed and maintained.
- I. Adequate sediment control bmps must be installed and maintained.
- J. Adequate bmps designed to control off-site sediment tracking must be installed and maintained.
- K. At a minimum, 25% of the materials needed to install standby bmps necessary to completely protect exposed portions of the site from erosion and prevent sediment discharges must be stored on site or at an approved off-site staging area.
- L. An approved "weather triggered" response plan is mandated for implementation in the event that a predicted storm event has a 50% chance of rain. The proponent must have the capacity to deploy the standby bmp's a minimum of 24 hours prior to the predicted storm event.
- M. The weather triggered response plan shall include, but not be limited to, covering all exposed soil and areas impacted by the project with visqueen sheeting.
- N. Cleared or graded areas shall not be left exposed at any given time during a storm event and shall be adequately covered prior to a storm event to prevent erosion.

GENERAL NOTES:

- A. An automatic, electrically controlled irrigation system shall be provided as required by LDC §142.0403(c) for proper irrigation, development, and maintenance of the vegetation in a healthy, disease-resistant condition. The design of the system shall provide adequate support for the vegetation selected. Irrigation system to be a combination of spray, drip & high efficiency rotators.
- B. Maintenance: All required landscape areas shall be maintained by the owner. Landscape and irrigation areas in the public right-of-way shall be maintained by the owner. The landscape areas shall be maintained free of debris and litter, and all plant material shall be maintained in a healthy growing condition. Diseased or dead plant material shall be satisfactorily treated or replaced per the conditions of the permit.
- C. A minimum root zone of 40sf in area shall be provided for all trees. The minimum dimension for this area shall be 5 feet, per SDMC §142.0403(b)(5).
- D. All required planting areas and all exposed soil areas without vegetation shall be covered with mulch to a minimum depth of 3 inches, excluding slopes requiring revegetation per SDMC §142.0421.
- E. Tree root barriers shall be installed where trees are placed within 5 feet of public improvements including walks, curbs, or street pavements or where new public improvements are placed adjacent to existing trees. The root barrier will not wrap around the root ball.
- F. If any required landscape indicated on the approved construction document plans is damaged or removed during demolition or construction, it shall be repaired and/or replaced in kind and equivalent size per the approved documents to the satisfaction of the Development Services Department within 30 days of damage.
- G. Within site visibility triangles, no vegetation, other than trees, in these areas will exceed 24" in height.

Legend



PREPARED BY: SCHMIDT DESIGN GROUP

NAME: 1310 ROSECRANS ST, SUITE G

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PHONE: 619.236.1462

ORIGINAL DATE: 3/15/2016 (BY GENSLER)

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REVISION DATE: 10/11/2019 (BY RDC)

REVISION DATE: 09/01/2020 (BY RDC)

SHEET TITLE: LANDSCAPE AREAS

SHEET NO: ENT- L0.106 59 OF 65

PTS #: 477943

PROJECT #: 477943

Regency Centers. REGENCY CENTERS
420 STEVENS AVE SUITE 320
SOLANA BEACH, CA 92075

PROJECT NUMBER: 19-187

Landscape Calculations Worksheet
Industrial Development in RM and C Zones
Commercial Development in All Zones

Provide the following information on the Landscape Plans. The Landscape Calculations determine the planting area and points required by the Landscape Regulations, Chapter 14, Article 2, Division 4 of the Land Development Code.

STREET YARD

Industrial and Commercial Development (except Auto Service Stations; see below):

Planting Area Required [142.0404]	Planting Area Provided	Excess Area Provided
Total Area 75,327 sq. ft. x 25%= 18,832 sq. ft.	34,671 sq. ft.	15,839 sq. ft.

Planting Points Required [142.0404]	Plant Points Provided - To be achieved with trees only	Excess Points Provided
Total Area 75,327 sq. ft. x 0.05= 3,766 points	3,780 points	14 points

Auto Service Stations Only

Planting Area Required [142.0405(c)(2)]	Planting Area Provided	Excess Area Provided
Total Area sq. ft. x 15%= sq. ft.	sq. ft.	sq. ft.

Plant Points Required [142.0405(c)(2)]	Plant Points Provided	Excess Points Provided
Total Area sq. ft. x 0.03= points	points	points

Note: All required Street Yard planting areas located outside the Vehicular Use Area (VUA) except for auto service stations may consist of hardscape or unattached unit pavers, excluding the minimum area required for trees. [142.0405(C)(1)]

REMAINING YARD

Planting Area Required [142.0404]	Planting Area Provided	Excess Area Provided
Total Area 8,450 sq. ft. x 30%= 2,535 sq. ft.	3,768 sq. ft.	1,233 sq. ft.

Plant Points Required [142.0404]	Plant Points Provided	Excess Points Provided
Total Area 8,450 sq. ft. x 0.05= 423 points	1,111 points	688 points

Note: Plant points achieved with shrubs

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Upon request, this information is available in alternative formats for persons with disabilities.

DS-4 (03-06)

ADDITIONAL YARD REQUIREMENTS FOR PROJECTS ABUTTING RESIDENTIAL ZONES

Planting Area Buffer Between Zones	Buffer Area Provided	Excess Area Provided
Length of Property line(s) abutting residential zone 1,372 ft. x 5 = 6,860 sq. ft. buffer required	8,713 sq. ft.	1,853 sq. ft.

Plant Points Required within buffer	Plant Points Provided	Excess Points Provided
Total Area 8,713 sq. ft. x 0.05= 435 points	450 points	15 points

Note: Plant points achieved with trees only

Note: Plant Points required are in addition to those required in the remaining yard. [142.0405(c)(3)].

VEHICULAR USE AREA (VUA) - See separate worksheet (DS-5)

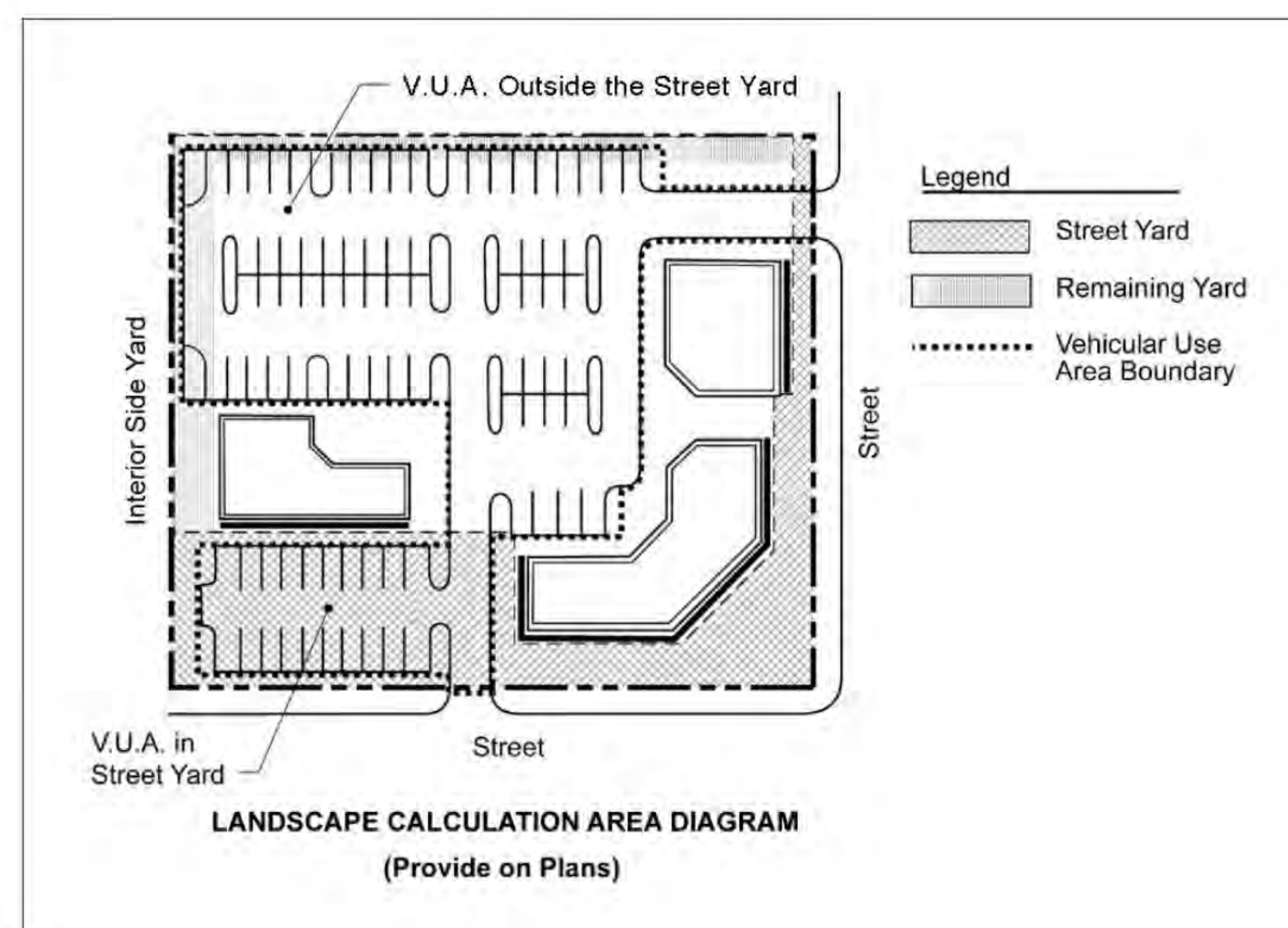
ADDITIONAL YARD REQUIREMENT CALCULATIONS

TREES:

2 Existing	=100 pts
10 @ 24" box	=200 pts
3 @ 36" box	=150 pts
Total:	=450 pts

ADDITIONAL YARD PLANTING AREA AND POINT REQUIREMENTS

If any of the requirements of Landscape Regulations, Section 142.0405 (a) 1, 2, or 3 apply to your project, provide a written summary explaining how requirements are being met.



Landscape Calculations Worksheet
Vehicular Use Areas (VUA)

Provide the following information on the Landscape Plans. The Landscape Calculations determine the planting area and points required by the Landscape Regulations, Chapter 14, Article 2, Division 4 of the Land Development Code.

One tree (minimum 24-inch box size) is required within 30 ft. of each parking space. (If palm trees are used, one palm (minimum 8 ft. brown trunk height) is required within 15 ft. of each parking space.)

VEHICULAR USE AREA (<6,000 sf) [142.0406 - 142.0407]

Planting Area Required: provide 40 sq. ft. per tree (with no dimension less than 5')

Plant Points Required	Plant Points Provided	Excess Points Provided
Total VUA: sq. ft. x 0.05 = points	points	points

Points achieved through trees (at least half): points

VEHICULAR USE AREA (≥6,000 sf) [142.0406 - 142.0407]

Required Planting Area	Planting Area Provided	Excess Area Provided
VUA inside Street Yard: 20,162 sq. ft. x 0.05 = 1,008 sq. ft.	3,721 sq. ft.	2,713 sq. ft.
VUA outside Street Yard: 139,293 sq. ft. x 0.03 = 4,179 sq. ft.	5,370 sq. ft.	1,191 sq. ft.

Required Plant Points	Plant Points Provided	Points Provided with Trees (at least half)
VUA inside Street Yard: 20,162 sq. ft. x 0.05 = 1,008 points	1,101 points	140 points
VUA outside Street Yard: 139,293 sq. ft. x 0.03 = 4,179 points	3,634 points	2,400 points

* Plant sizes have been increased in the planting areas adjacent to the VUA in street yard with 75% 15 gal. and 25% 5 gal. (a) Proposed 24" box trees located just outside VUA along entry to the parking structure at Genesee. All other VUA areas within street yard are bound by sidewalks. Please provide an exception here

TEMPORARY VEHICULAR USE AREA [142.0408]

Required Planting Area	Planting Area Provided
Length of Public Right-of-Way adjacent to VUA: ft. x 3 ft. = sq. ft.	sq. ft.

- Provide planting area between Public Right-of-Way and VUA.
- Plant with evergreen shrubs.
- Shrubs must achieve a minimum height of 30" within 2 years of installation over at least 50% of the required planting area.

ADDITIONAL YARD PLANTING AREA AND POINT REQUIREMENTS

If any of the requirements of Landscape Regulations, Section 142.0405 (a) 1, 2, or 3 apply to your project, provide a written summary explaining how requirements are being met.

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DS-5 (03-06)

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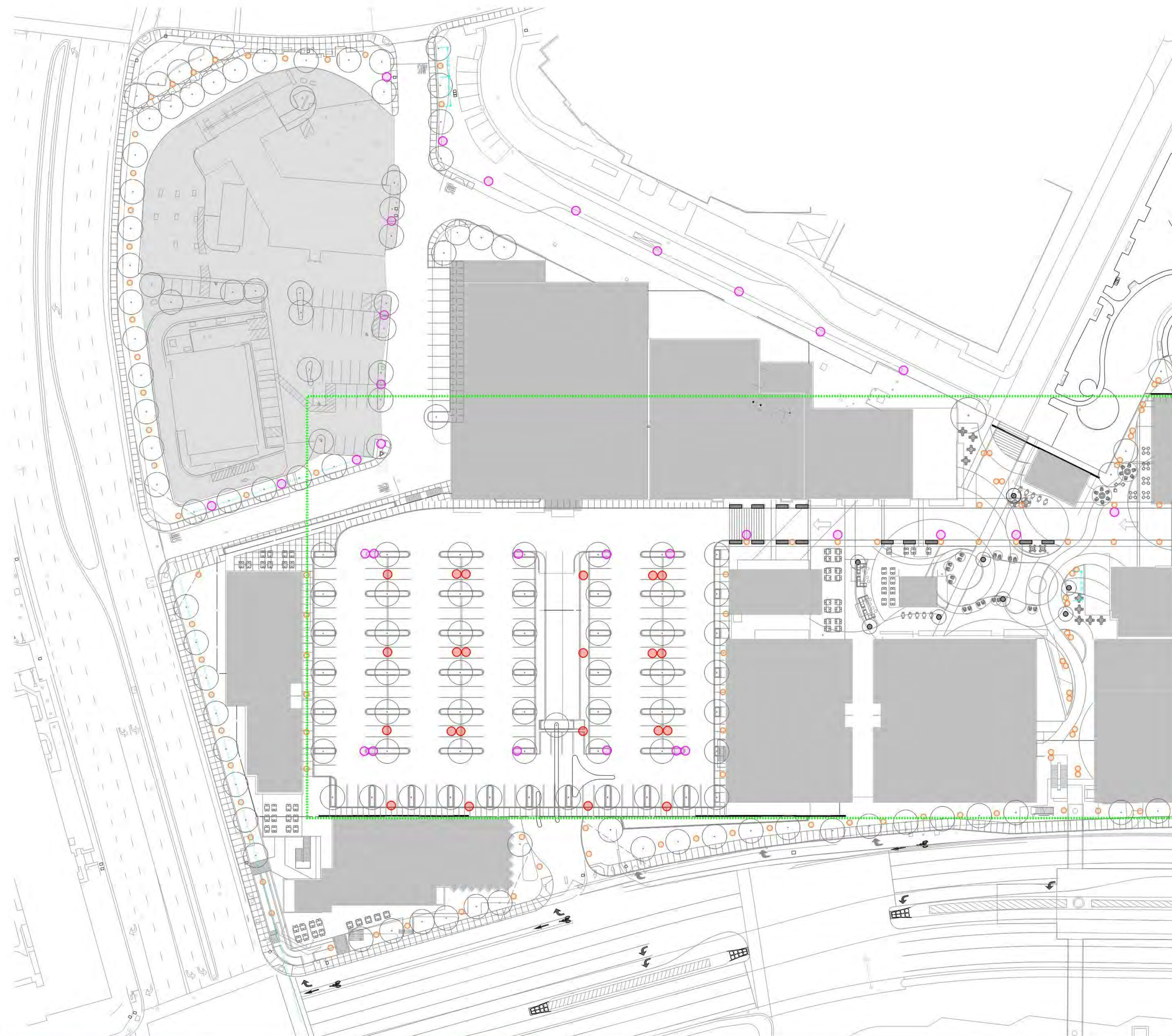
ORIGINAL DATE: 3/15/2016 (BY GENSLER)
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REVISION DATE: 06/10/2019 (BY GENSLER)
REVISION DATE: 10/11/2019 (BY RDC)
REVISION DATE: 09/01/2020 (BY RDC)

SHEET TITLE:
LANDSCAPE CALCULATIONS

SHEET NO: ENT- L0.107 60 OF 65

PTS #: 477943
PROJECT #: 477943

Regency Centers
REGENCY CENTERS
420 STEVENS AVE SUITE 320
SOLANA BEACH, CA 92075



San Diego Municipal Code




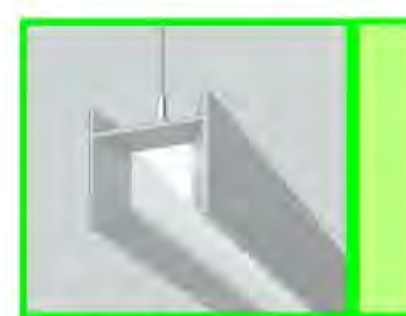
Chapter 14: Outdoor Lighting Regulations

- Project site is located outside the 30-mile radius of Palomar and Mount Laguna observatories.
- Shields and flat lenses shall be required to control and direct the light below an imaginary horizontal plane passing through the lowest point of the fixture, except for outdoor lighting fixtures less than 4,050 lumens including landscape lighting, decorative lighting and outdoor illuminated signs.
- New outdoor lighting fixtures shall minimize light trespass in accordance with the Green Building Regulations where applicable, or otherwise shall direct, shield, and control light to keep it from falling onto surrounding properties. Zero direct-beam illumination shall leave the premises.
- Outdoor lighting shall not exceed nominal 4000 Kelvin Color Correlated Temperature (CCT).
- Outdoor lighting may remain lighted for commercial and industrial uses that continue to be fully operational after 11:00 P.M. such as sales, assembly, and repair; and for security purposes or to illuminate walkways, roadways, equipment yards, and parking lots subject to the following:
 - Adequate lighting for public safety shall be maintained.

Outdoor lighting shall otherwise be reduced after 11:00 P.M. where practicable.

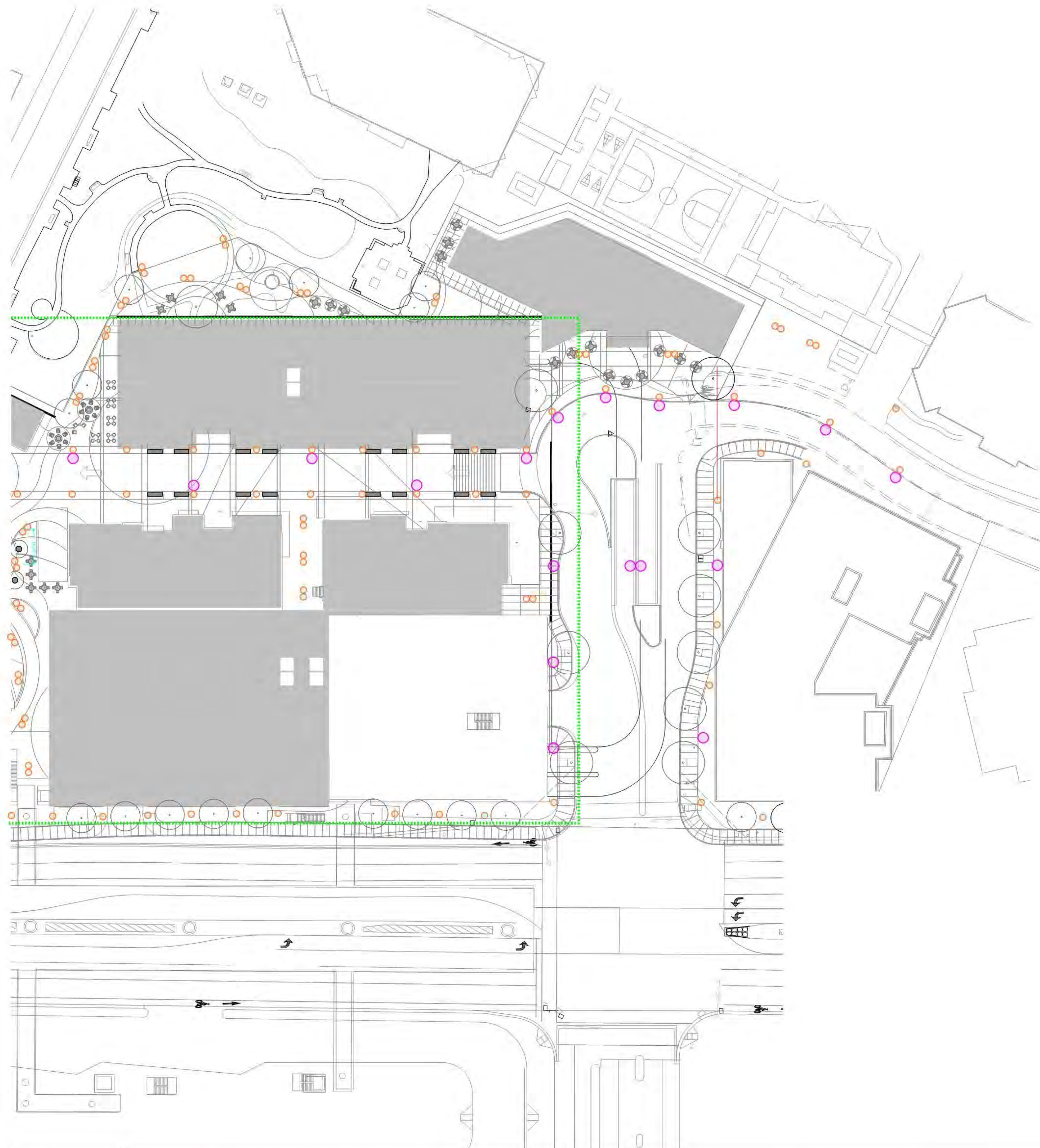
- Outdoor lighting for the following is permitted to remain lighted after 11:00 P.M. and is exempt from the maximum Kelvin CCT and maximum lumen requirements specified in Section 142.0740(c)(4) and (c)(5)(A):
 - Outdoor lighting used to illuminate recreational activities that are not in a residential zone may continue after 11:00 P.M. only when equipped with automatic timing devices and shielded to minimize light pollution.
 - Illuminated on-premises signs for businesses that are open to the public after 11:00 P.M. may remain lighted during business operating hours only. Illuminated off-premises advertising display signs shall not be lighted after 11:00 P.M. Signs located both on-and off-premises shall be equipped with automatic timing devices.
 - Outdoor lighting for automated teller machines and associated parking lot facilities and access areas shall be provided during hours of darkness in accordance with California Financial Code Sections 13040-13041.

PREPARED BY:	
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PHONE:	949-681-0500
FAX:	949-681-0501
ORIGINAL DATE:	6/10/2019
REVISION DATE:	
REVISION DATE:	
REVISION DATE:	
REVISION DATE:	
SHEET TITLE:	LIGHTING
SHEET NO:	ENT-LT0.100 61 OF 65
PTS #:	477943
PROJECT #:	477943
REGENCY CENTERS 420 STEVENS AVE SUITE 320 SOLANA BEACH, CA 92075	
GENSLER PROJECT NUMBER:	05.2283.000

 Parking lot pole <small>(single and double head configurations)</small>	<p>Luminaire description: Single or Double head pole</p> <p>Kelvin CCT: 3500K</p> <p>Mounting height: 24ft</p> <p>BUG rating: B0-U0-G2</p> <p>Light Spill control: Spill control optics Full Cut-off Field installed house-side shield Capable of photocell and occupancy sensor control</p>	 Roadway pole <small>(single and double head configurations)</small>	<p>Luminaire description: Single or Double head pole</p> <p>Kelvin CCT: 3500K</p> <p>Mounting height: 24ft</p> <p>BUG rating: B2-U0-G2</p> <p>Light Spill control: Spill control optics Full Cut-off Field installed house-side shield Capable of photocell and occupancy sensor control</p>	 Pedestrian pole <small>(single and double head configurations)</small>	<p>Luminaire description: Single or Double head pole</p> <p>Kelvin CCT: 3500K</p> <p>Mounting height: 15ft</p> <p>BUG rating: B2-U0-G1</p> <p>Light Spill control: Spill control optics Full Cut-off Field installed house-side shield Capable of photocell and occupancy sensor control</p>	 Sub-level parking <small>(surface and pendant mounting)</small>	<p>Luminaire description: Sub-level parking garage</p> <p>Kelvin CCT: 3500K</p> <p>Mounting height: 9ft</p> <p>BUG rating: B1-UX-G1</p> <p>Light Spill control: Capable of photocell and occupancy sensor control Direct illumination</p>
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NOTES:
 * Fixture specification and pictures indicate above are intended to represent fixture performance, ambience, and overall light quality. The final fixture specification and style may vary per design development. Pictures and fixture locations on plans are placeholders for representation purpose only.
 - Fixtures exempted from municipality code regulations are not indicated on drawings. This includes low wattage fixtures below 4050 lumens, landscape lighting, seasonal lighting, illuminated signs, facade lighting, etc. with controlled optics and glare control accessories.
 - Lighting shown for concept only

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San Diego Municipal Code





Chapter 14: Outdoor Lighting Regulations

- Project site is located outside the 30-mile radius of Palomar and Mount Laguna observatories.
- Shields and flat lenses shall be required to control and direct the light below an imaginary horizontal plane passing through the lowest point of the fixture, except for outdoor lighting fixtures less than 4,050 lumens including landscape lighting, decorative lighting and outdoor illuminated signs.
- New outdoor lighting fixtures shall minimize light trespass in accordance with the Green Building Regulations where applicable, or otherwise shall direct, shield, and control light to keep it from falling onto surrounding properties. Zero direct-beam illumination shall leave the premises.
- Outdoor lighting shall not exceed nominal 4000 Kelvin Color Correlated Temperature (CCT).
- Outdoor lighting may remain lighted for commercial and industrial uses that continue to be fully operational after 11:00 P.M. such as sales, assembly, and repair; and for security purposes or to illuminate walkways, roadways, equipment yards, and parking lots subject to the following:
 - Adequate lighting for public safety shall be maintained.

Outdoor lighting shall otherwise be reduced after 11:00 P.M. where practicable.

- Outdoor lighting for the following is permitted to remain lighted after 11:00 P.M. and is exempt from the maximum Kelvin CCT and maximum lumen requirements specified in Section 142.0740(c)(4) and (c)(5)(A):
 - Outdoor lighting used to illuminate recreational activities that are not in a residential zone may continue after 11:00 P.M. only when equipped with automatic timing devices and shielded to minimize light pollution.
 - Illuminated on-premises signs for businesses that are open to the public after 11:00 P.M. may remain lighted during business operating hours only. Illuminated off-premises advertising display signs shall not be lighted after 11:00 P.M. Signs located both on-and off-premises shall be equipped with automatic timing devices.
 - Outdoor lighting for automated teller machines and associated parking lot facilities and access areas shall be provided during hours of darkness in accordance with California Financial Code Sections 13040-13041.

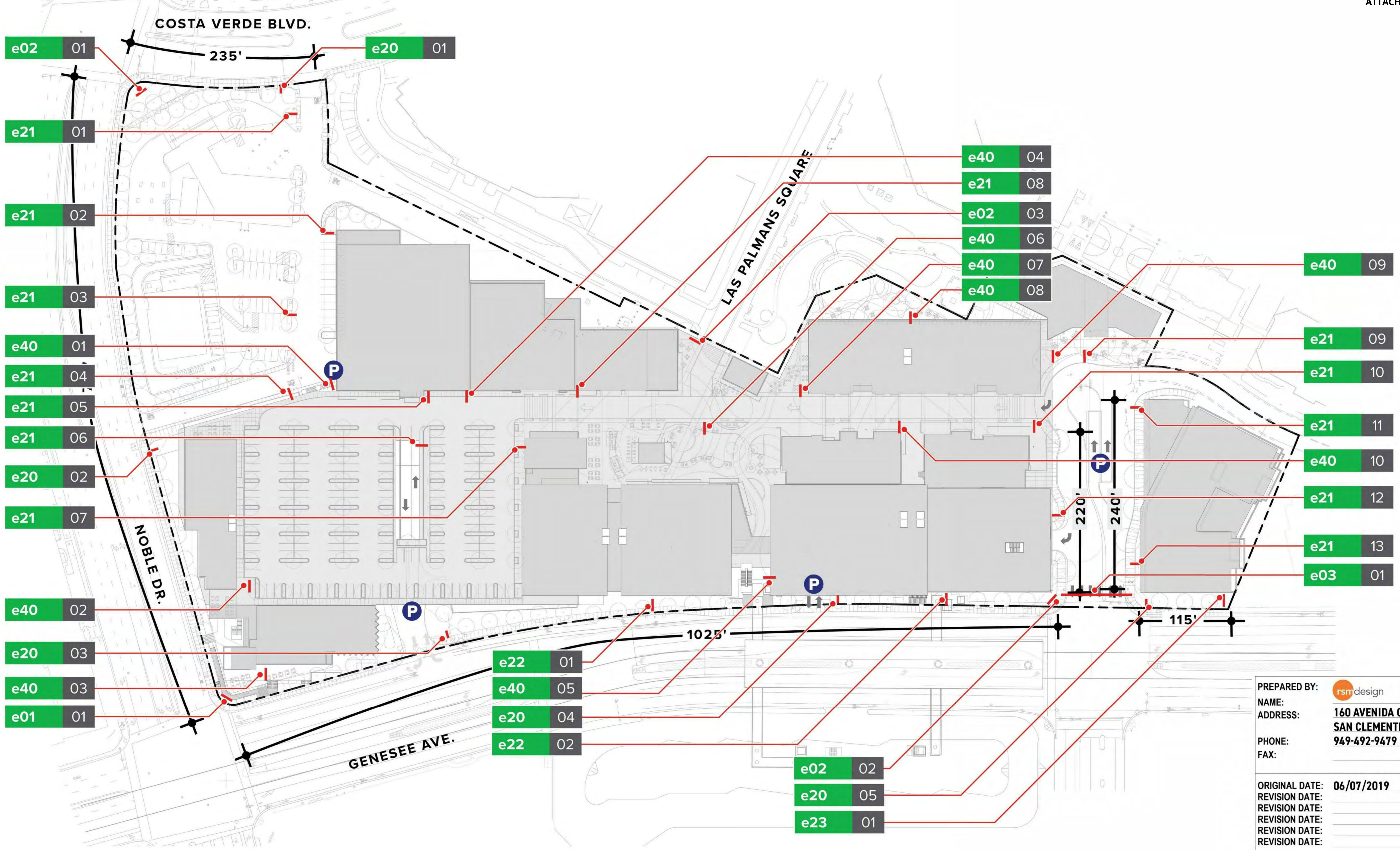
PREPARED BY:	
NAME:	3187-C AIRWAY AVE
ADDRESS:	COSTA MESA, CA 92626
PHONE:	949-681-0500
FAX:	949-681-0501
ORIGINAL DATE:	6/10/2019
REVISION DATE:	
REVISION DATE:	
REVISION DATE:	
REVISION DATE:	
SHEET TITLE:	LIGHTING
SHEET NO:	ENT-LT0.101 62 OF 65
PTS #:	477943
PROJECT #:	477943
REGENCY CENTERS 420 STEVENS AVE SUITE 320 SOLANA BEACH, CA 92075	
GENSLER PROJECT NUMBER:	05.2283.000

 Parking lot pole <small>(single and double head configurations)</small>	<p>Luminaire description: Single or Double head pole</p> <p>Kelvin CCT: 3500K</p> <p>Mounting height: 24ft</p> <p>BUG rating: B0-U0-G2</p> <p>Light Spill control: Spill control optics Full Cut-off Field installed house-side shield Capable of photocell and occupancy sensor control</p>	 Roadway pole <small>(single and double head configurations)</small>	<p>Luminaire description: Single or Double head pole</p> <p>Kelvin CCT: 3500K</p> <p>Mounting height: 24ft</p> <p>BUG rating: B2-U0-G2</p> <p>Light Spill control: Spill control optics Full Cut-off Field installed house-side shield Capable of photocell and occupancy sensor control</p>	 Pedestrian pole <small>(single and double head configurations)</small>	<p>Luminaire description: Single or Double head pole</p> <p>Kelvin CCT: 3500K</p> <p>Mounting height: 15ft</p> <p>BUG rating: B2-U0-G1</p> <p>Light Spill control: Spill control optics Full Cut-off Field installed house-side shield Capable of photocell and occupancy sensor control</p>	 Sub-level parking <small>(surface and pendant mounting)</small>	<p>Luminaire description: Sub-level parking garage</p> <p>Kelvin CCT: 3500K</p> <p>Mounting height: 9ft</p> <p>BUG rating: B1-UX-G1</p> <p>Light Spill control: Capable of photocell and occupancy sensor control Direct illumination</p>
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NOTES:

- * Fixture specification and pictures indicate above are intended to represent fixture performance, ambience, and overall light quality. The final fixture specification and style may vary per design development. Pictures and fixture locations on plans are placeholders for representation purpose only.
- Fixtures exempted from municipality code regulations are not indicated on drawings. This includes low wattage fixtures below 4050 lumens, landscape lighting, seasonal lighting, illuminated signs, facade lighting, etc. with controlled optics and glare control accessories.
- Lighting shown for concept only

5/22/2019 5:55:14 PM \\gen\skir\ad\Projects\Rev\Use\m\051827616.2283.000_COSTA_MESA_CENTRAL_ARCH_2019_marcos_kfesseo@gen\skir.com.vnt



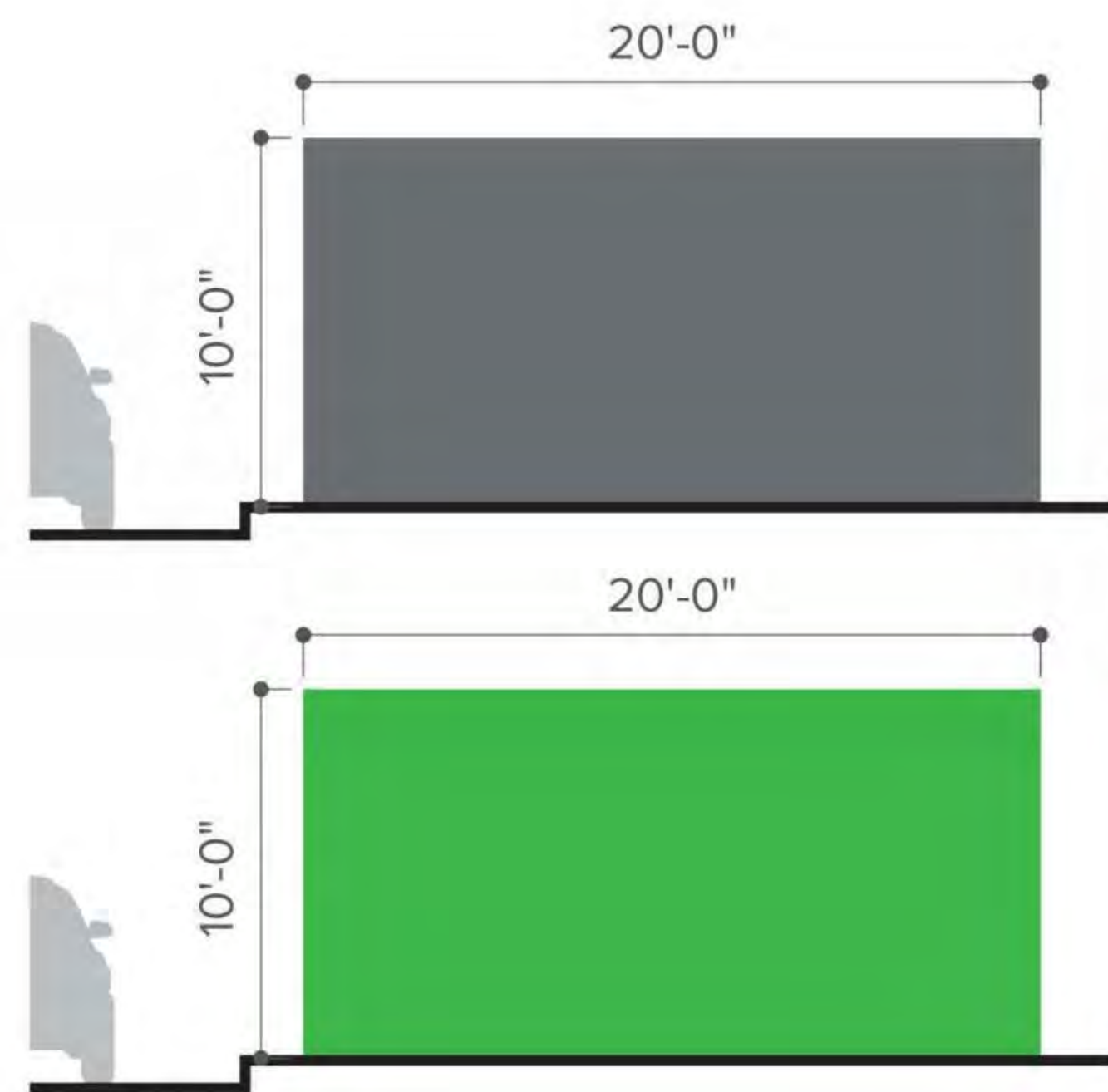
NOTE:
SIGNAGE FOR CONCEPT ONLY

PREPARED BY:	
NAME:	160 AVENIDA CABRILLO
ADDRESS:	SAN CLEMENTE, CA.
PHONE:	949-492-9479
FAX:	
ORIGINAL DATE:	06/07/2019
REVISION DATE:	
REVISION DATE:	
REVISION DATE:	
REVISION DATE:	
SHEET TITLE:	SIGNAGE & WAYFINDING
SHEET NO:	ENT-SG1.201 63 OF 65
PTS #:	477943
PROJECT #:	477943
	REGENCY CENTERS 420 STEVENS AVE SUITE 320 SOLANA BEACH, CA 92075



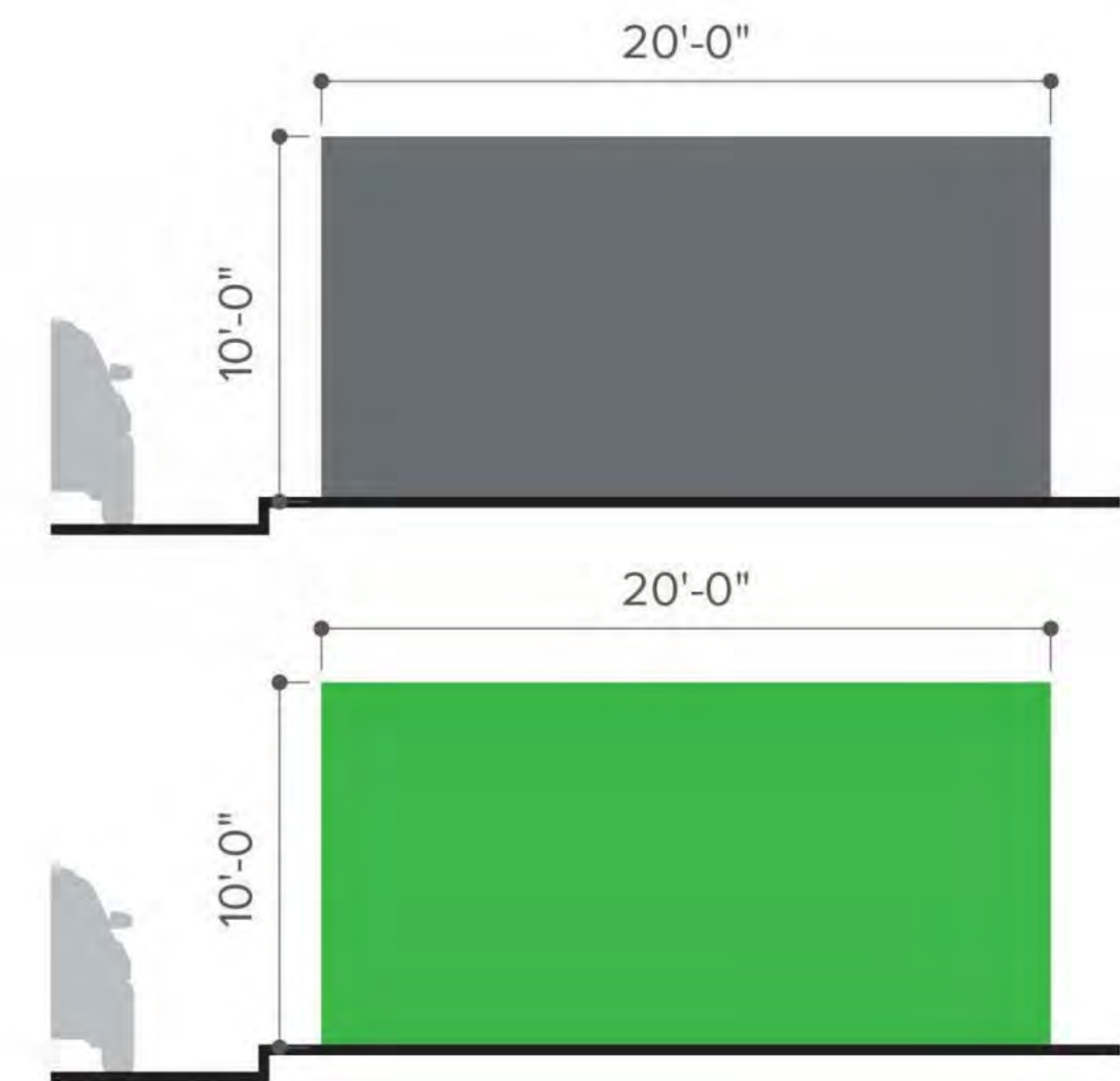
e01 Primary Property Identity

MAX. ALLOWABLE SQUARE FOOTAGE	200 SQ. FT.	-
MAX. HEIGHT	30'-0"	-
ALLOWABLE QUANTITY	1	
EXISTING QUANTITY	1	
PROPOSED QUANTITY	1	



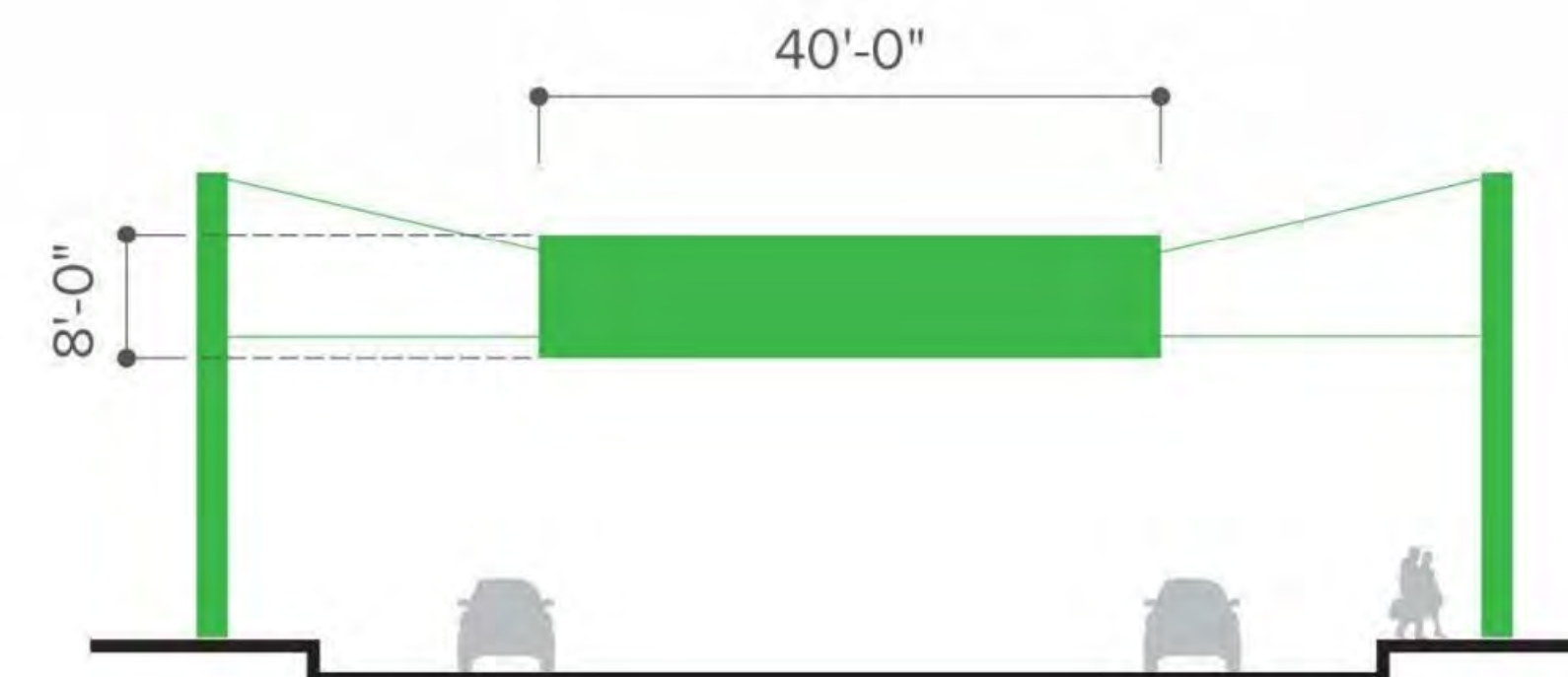
e02 Secondary Property Identity

MAX. ALLOWABLE SQUARE FOOTAGE	200 SQ. FT.	-
MAX. HEIGHT	30'-0"	-
ALLOWABLE QUANTITY	Genesee Ave	4
	Nobel Dr.	3
	Costa Verde Bl.	1
EXISTING QUANTITY	1	
PROPOSED QUANTITY	3	



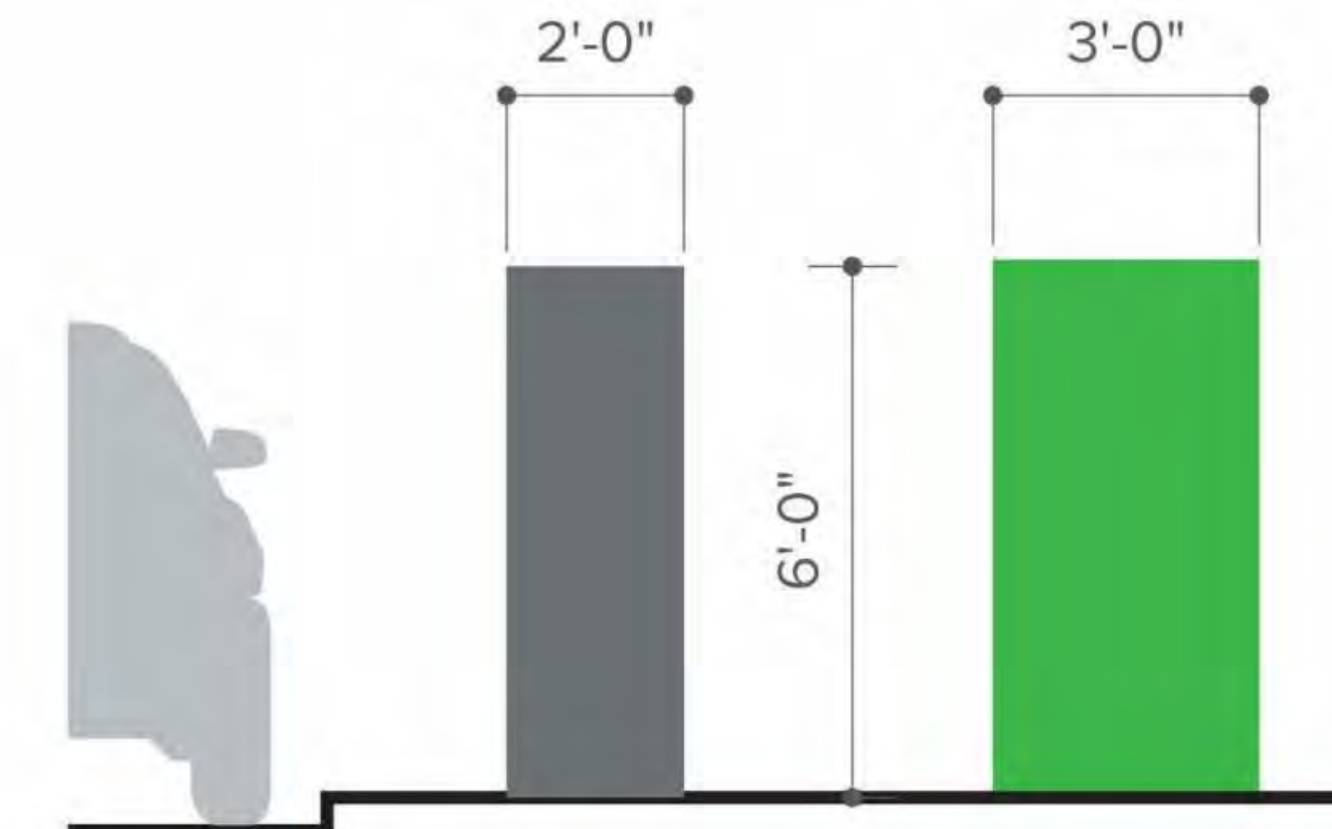
e03 Gateway Project Identity

MAX. ALLOWABLE SQUARE FOOTAGE	200 SQ. FT.	+
MAX. HEIGHT	30'-0"	-
ALLOWABLE QUANTITY	N/A	+
EXISTING QUANTITY	0	
PROPOSED QUANTITY	1	



e20 Vehicular Entry Identity

MAX. ALLOWABLE SQUARE FOOTAGE	12 SQ. FT.	+
MAX. HEIGHT	8'-0"	-
ALLOWABLE QUANTITY	AT EACH DRIVEWAY	5 (AT ENTRY)
EXISTING QUANTITY	4	
PROPOSED QUANTITY	5	



Grey area indicates maximum allowable sign areas per San Diego Municipal Code.
 *Building Mounted Signage, Tenant Signage, and Parking related signage are excluded from submittal and will be a deferred submittal.

Green area indicates proposed sign area (final proportions to be determined)

+ Exceeds Allowance - Does NOT Exceed Allowance

NOTE:
SIGNAGE FOR CONCEPT ONLY

PREPARED BY: rsm design
 NAME: **160 AVENIDA CABRILLO**
 ADDRESS: **SAN CLEMENTE, CA.**
 PHONE: **949-492-9479**
 FAX:

ORIGINAL DATE: **06/07/2019**
 REVISION DATE:
 REVISION DATE:
 REVISION DATE:
 REVISION DATE:

SHEET TITLE:
SIGNAGE & WAYFINDING

SHEET NO: ENT-SG1.202 64 OF 65

PTS #: **477943**
 PROJECT #: **477943**

Regency Centers. REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075



e21 Vehicular Directional

MAX. ALLOWABLE SQUARE FOOTAGE	12 SQ. FT.	+
MAX. HEIGHT	8'-0"	-
ALLOWABLE QUANTITY	AT EACH DRIVEWAY	
	9 (ON-SITE)	
EXISTING QUANTITY	2	
PROPOSED QUANTITY	13	



e22 Office Identity

MAX. ALLOWABLE SQUARE FOOTAGE	24 SQ. FT.	+
MAX. HEIGHT	8'-0"	-
ALLOWABLE QUANTITY	2	
EXISTING QUANTITY	0	
PROPOSED QUANTITY	2	



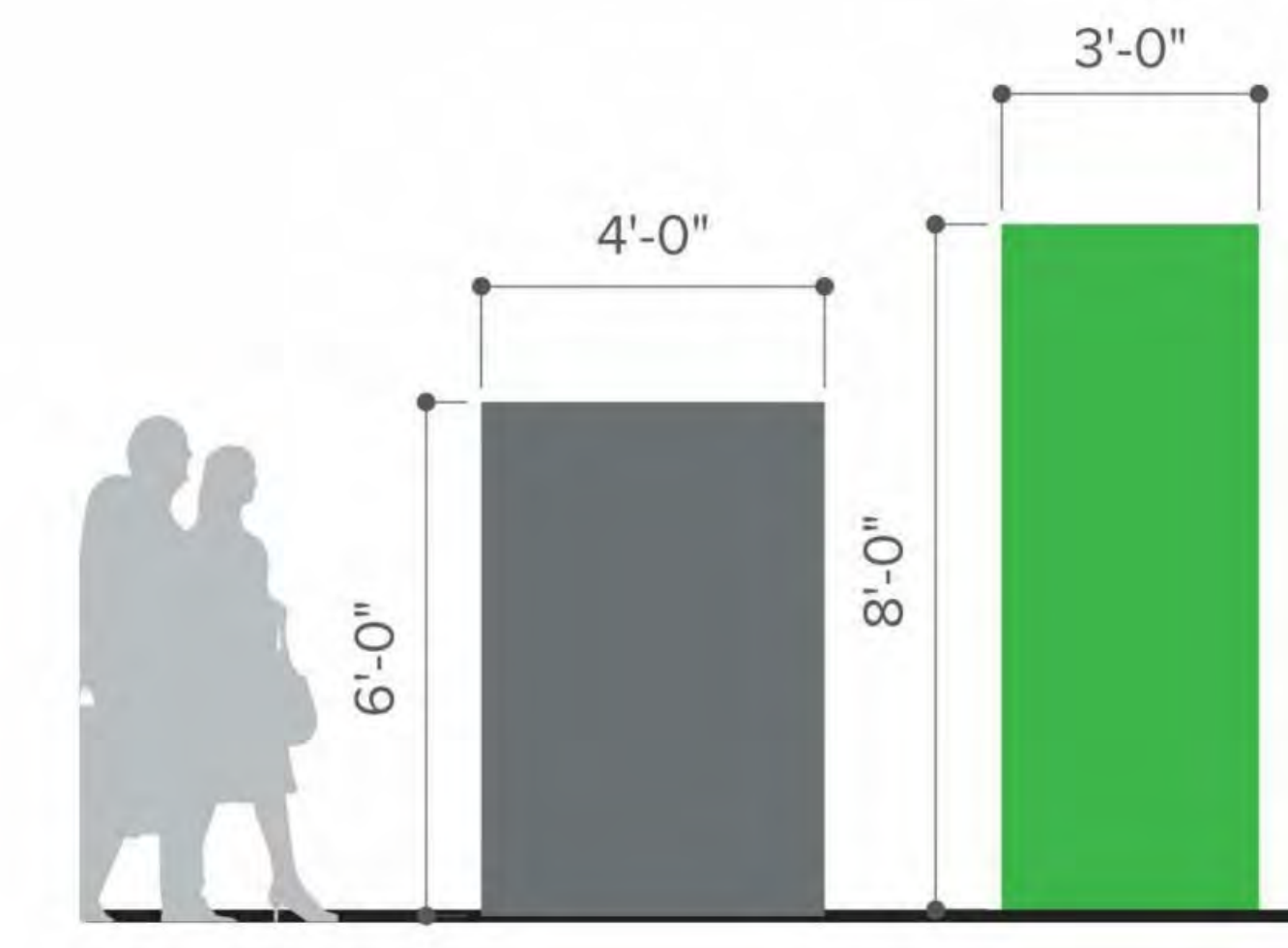
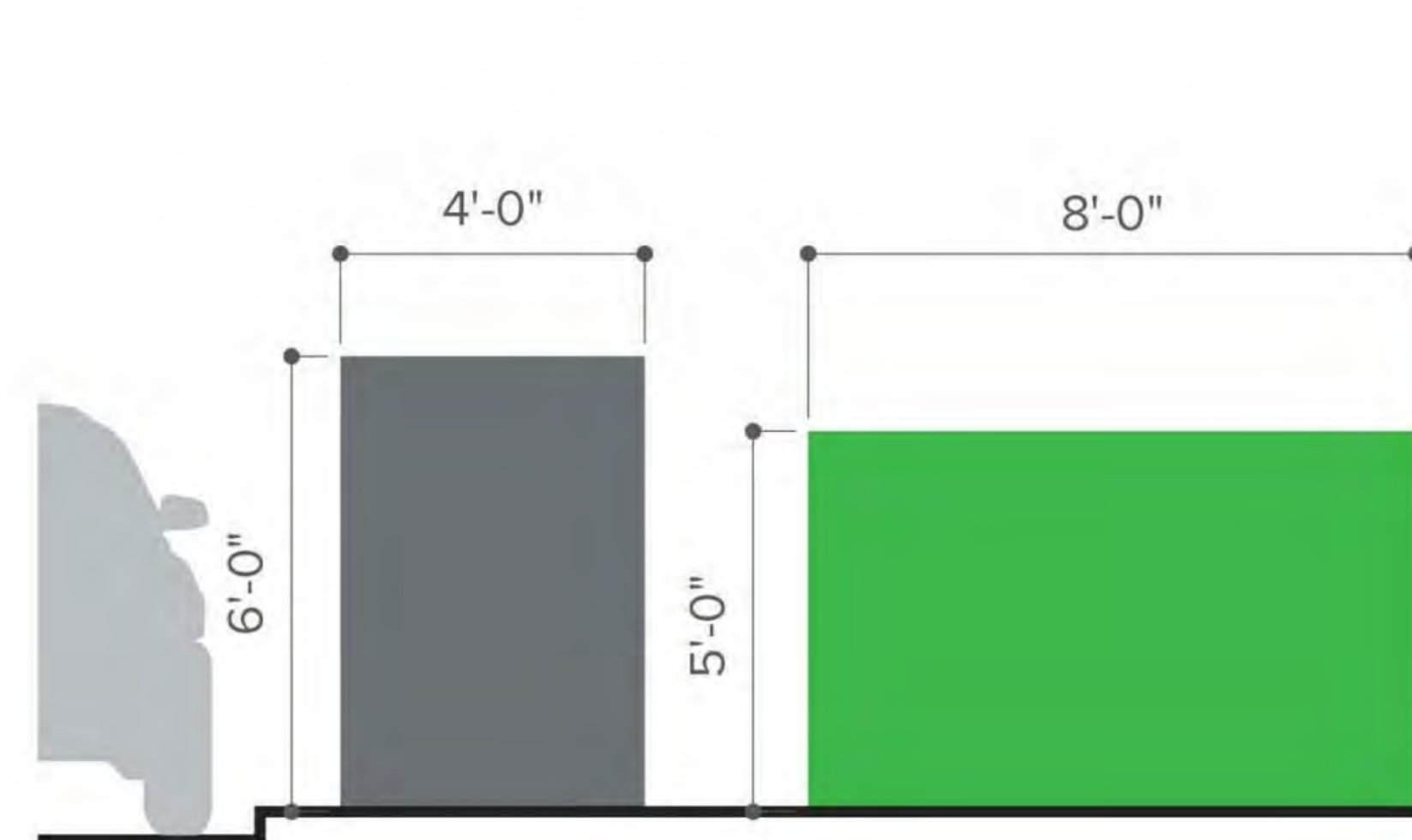
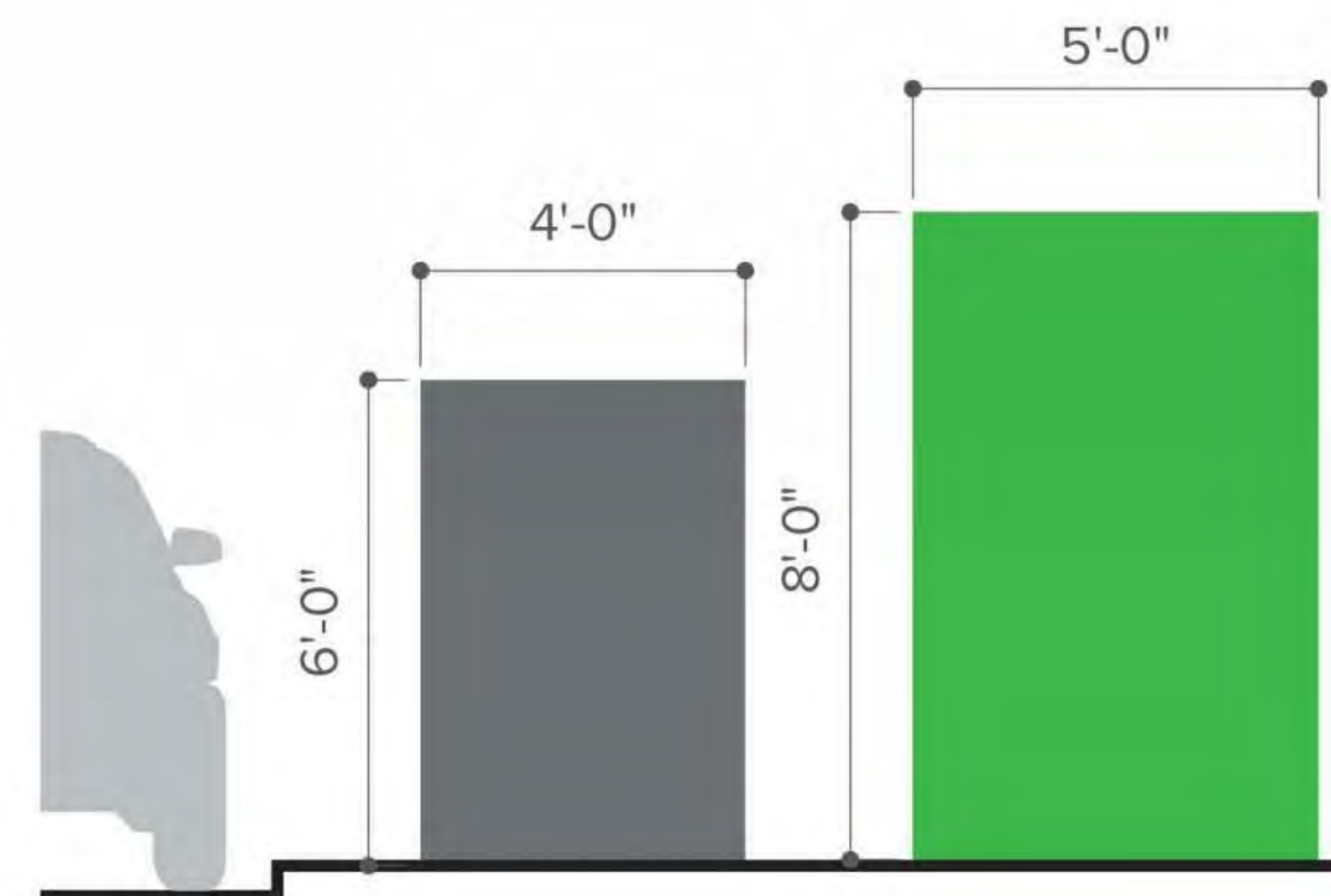
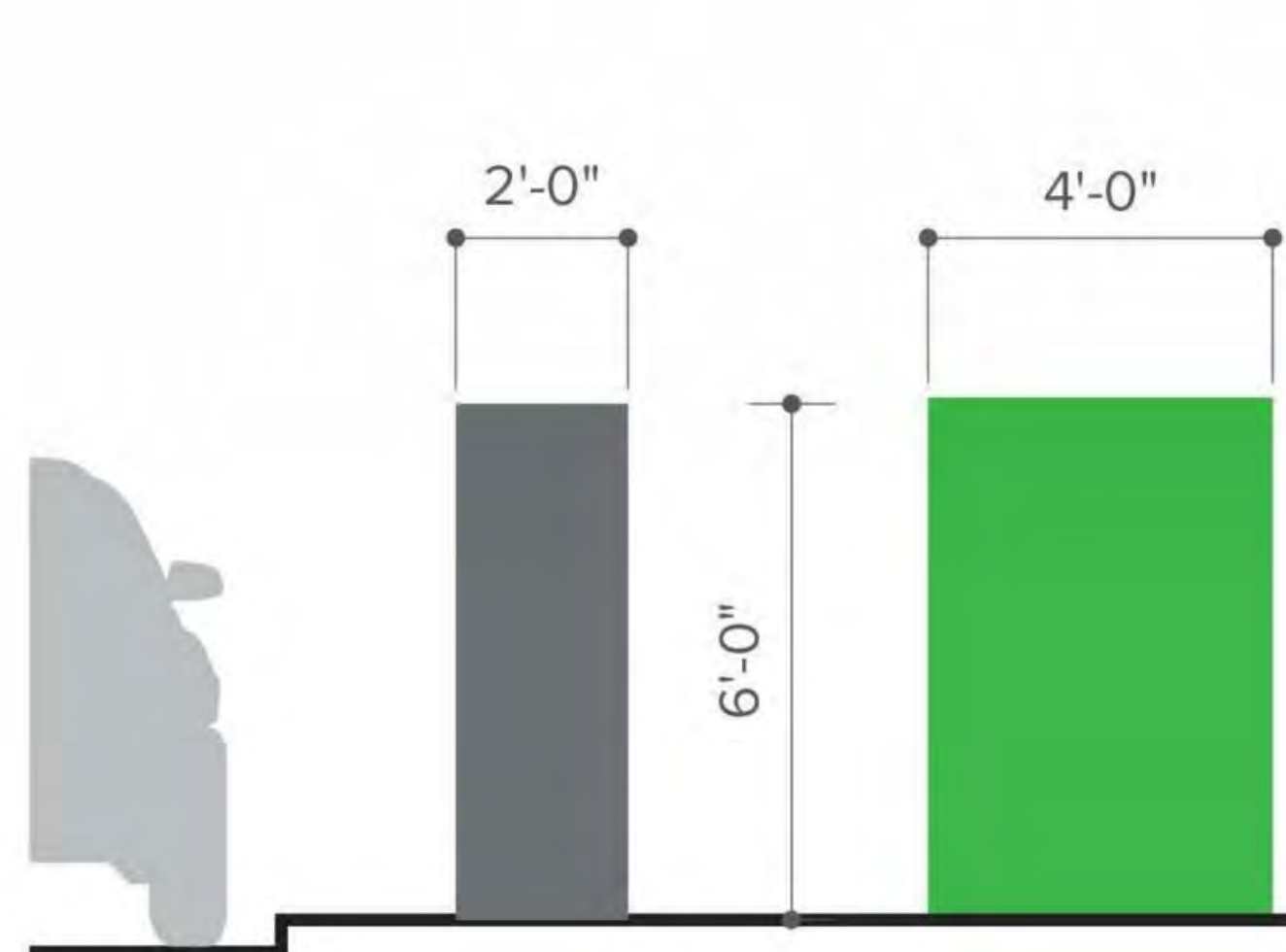
e23 Hotel Identity

MAX. ALLOWABLE SQUARE FOOTAGE	24 SQ. FT.	+
MAX. HEIGHT	8'-0"	-
ALLOWABLE QUANTITY	2	
EXISTING QUANTITY	0	
PROPOSED QUANTITY	1	



e40 Pedestrian Directory

MAX. ALLOWABLE SQUARE FOOTAGE	24 SQ. FT.	-
MAX. HEIGHT	8'-0"	-
ALLOWABLE QUANTITY	AT EACH ENTRY	
	7	
EXISTING QUANTITY	1	
PROPOSED QUANTITY	10	



Grey area indicates maximum allowable sign areas per San Diego Municipal Code.

*Building Mounted Signage, Tenant Signage, and Parking related signage are excluded from submittal and will be a deferred submittal.

Green area indicates proposed sign area (final proportions to be determined)

+ Exceeds Allowance

- Does NOT Exceed Allowance

NOTE:
SIGNAGE FOR CONCEPT ONLY

PREPARED BY: **rsm**design
 NAME:
 ADDRESS: **160 AVENIDA CABRILLO
 SAN CLEMENTE, CA.
 949-492-9479**
 PHONE:
 FAX:

ORIGINAL DATE: **06/07/2019**
 REVISION DATE:
 REVISION DATE:
 REVISION DATE:
 REVISION DATE:


SHEET TITLE:
SIGNAGE & WAYFINDING

SHEET NO: ENT-SG1.203 65 OF 65

PTS #: **477943**
 PROJECT #: **477943**

Regency Centers. REGENCY CENTERS
 420 STEVENS AVE SUITE 320
 SOLANA BEACH, CA 92075

The University Community Planning Group recommendation will be provided after they vote on the project, which is anticipated to happen on September 15, 2020, after printing of this report.

	City of San Diego Development Services 1222 First Ave., MS 302 San Diego, CA 92101 (619) 446-5000	<h1 style="margin:0;">Ownership Disclosure Statement</h1>	FORM DS-318 October 2017
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Approval Type: Check appropriate box for type of approval(s) requested: Neighborhood Use Permit Coastal Development Permit
 Neighborhood Development Permit Site Development Permit Planned Development Permit Conditional Use Permit Variance
 Tentative Map Vesting Tentative Map Map Waiver Land Use Plan Amendment • Other Easement Vacation

Project Title: Costa Verde Revitalization **Project No. For City Use Only:** _____
Project Address: 8410-8750 Genesee Avenue, San Diego, CA 92122

Specify Form of Ownership/Legal Status (please check):
 Corporation Limited Liability -or- General - What State? FL Corporate Identification No. 593429602
 Partnership Individual

By signing the Ownership Disclosure Statement, the owner(s) acknowledge that an application for a permit, map or other matter will be filed with the City of San Diego on the subject property with the intent to record an encumbrance against the property. Please list below the owner(s), applicant(s), and other financially interested persons of the above referenced property. A financially interested party includes any individual, firm, co-partnership, joint venture, association, social club, fraternal organization, corporation, estate, trust, receiver or syndicate with a financial interest in the application. If the applicant includes a corporation or partnership, include the names, titles, addresses of all individuals owning more than 10% of the shares. If a publicly-owned corporation, include the names, titles, and addresses of the corporate officers. (A separate page may be attached if necessary.) If any person is a nonprofit organization or a trust, list the names and addresses of **ANY** person serving as an officer or director of the nonprofit organization or as trustee or beneficiary of the nonprofit organization. A signature is required of at least one of the property owners. Attach additional pages if needed. Note: The applicant is responsible for notifying the Project Manager of any changes in ownership during the time the application is being processed or considered. Changes in ownership are to be given to the Project Manager at least thirty days prior to any public hearing on the subject property. Failure to provide accurate and current ownership information could result in a delay in the hearing process.

Property Owner
 Name of Individual: Regency Centers L.P. Owner Tenant/Lessee Successor Agency
 Street Address: 1 Independent Drive Suite 114
 City: Jacksonville State: FL Zip: 32202
 Phone No.: 858.847.4660 Fax No.: 858.350.1669 Email: johnmurphy@regencycenters.com
 Signature: _____ Date: 8.27.2020
 Additional pages Attached: Yes No

Applicant
 Name of Individual: John W. Murphy Owner Tenant/Lessee Successor Agency
 Street Address: 420 Stevens Avenue #320
 City: Solana Beach State: CA Zip: 92075
 Phone No.: 858.847.4660 Fax No.: 858.350.1669 Email: johnmurphy@regencycenters.com
 Signature: _____ Date: 8.27.2020
 Additional pages Attached: Yes No

Other Financially Interested Persons
 Name of Individual: N/A Owner Tenant/Lessee Successor Agency
 Street Address: _____
 City: _____ State: _____ Zip: _____
 Phone No.: _____ Fax No.: _____ Email: _____
 Signature: _____ Date: _____
 Additional pages Attached: Yes No