



DATE ISSUED: June 4, 2018

ATTENTION: Historic Resources Board  
Meeting of June 28, 2018

SUBJECT: Ezabelle (454 13<sup>th</sup> Street) – Centre City Development Permit/Site  
Development Permit No. 2018-07 – East Village Neighborhood of the  
Downtown Community Plan Area

STAFF CONTACT: James Alexander, Associate Planner

STAFF RECOMMENDATION: That the Historic Resources Board (HRB) receives a presentation on the design proposal and associated permits for the Ezabelle project (“Project”) and recommends that the Planning Commission approves Centre City Development Permit/Site Development Permit (CCDP/SDP) No. 2017-08 for the Project.

This is a Process 4 Site Development Permit (SDP) application (for the substantial alteration of a historically designated structure) that requires a public hearing and decision by the Planning Commission. The Planning Commission decision is appealable to the City Council, which would be the final decision maker on any appeal.

SUMMARY: NDD on 13<sup>th</sup> Street, LLC (“Applicant”) is requesting approval of CCDP/SDP No. 2018-07 for the construction of an seven-story, 77 foot tall mixed-use development comprised of 45 dwelling units (DU), of which six (13%) will be restricted to very low income residents, and approximately 1,722 square feet of ground floor commercial on an approximately 4,200 square-foot (SF) site on the west side of 13<sup>th</sup> Street between Island Avenue and J Street in the East Village neighborhood of the Downtown Community Plan (DCP) area (“Downtown”). The Project is requesting two incentives under the provisions of the City of San Diego’s (“City”) Affordable Housing Regulations to waive the requirement for 11 parking spaces and to reduce the minimum commercial space depth and height requirements.

FISCAL CONSIDERATIONS: Under the Downtown Public Facilities Financing Plan, the Project will pay Development Impact Fees (DIF) to fund its fair share of new park, fire station, and traffic circulation improvements in the DCP area. The DIF for this Project is estimated to be \$372,554.

ECONOMIC IMPACTS: It is estimated that the Project will generate approximately 51 construction jobs and seven permanent jobs. As of December 31, 2017, approximately 84,240 construction jobs and 30,225 permanent jobs have been generated Downtown because of redevelopment activities.

DOWNTOWN COMMUNITY PLANNING COUNCIL: On May 16, 2018, the Downtown Community Planning Council (DCPC) voted 15-1 with two recusals to recommend approval of the Project with the caveat that the Applicant adds an art wall or other upgraded materials on the Project's south and west blank walls, which the Applicant has incorporated into the plans.

BOARD RECOMMENDATION: On May 23, 2018, the CivicSD Board of Directors voted 8-0 to recommend approval of the Project. Board members thought the design issues identified at previous review meetings were successfully addressed and that the Project satisfies the Downtown Design Guidelines and is consistent with the goals, policies, and regulations of the DCP and Centre City Planned District Ordinance (CCPDO).

DEVELOPMENT TEAM

<b>ROLE</b>	<b>FIRM/CONTACT</b>	<b>OWNERSHIP</b>
Applicant/Property Owner	NDD on 13 <sup>th</sup> Street, LLC / Soheil Nakhshab	Soheil Nakhshab, Manager and Sole Owner (Privately Owned)
Architect/Project Manager	Nakhshab Development & Design, Inc. / Soheil Nakhshab	Soheil Nakhshab, Manager and Sole Owner (Privately Owned)

DISCUSSION

Neighborhood Context

The East Village neighborhood is anticipated to be a residential and mixed-use community upon build-out. Ultimately, East Village is projected to contain up to 46,000 residents. The highest residential intensities downtown will be attained in the area, served by the necessary retail, commercial and open space amenities. The Project site is located in the Southeast sub-district of the East Village neighborhood with close proximity to Petco Park, the Central Library, and Fault Line Park. In the southeastern portions of East Village, a mix of residential, office, retail, and convention center growth is envisioned while retaining light industrial uses and support infrastructure.

Applicable DCP Goals and Policies:

The following are some of the key applicable DCP Goals and Policies for the Project:

- 3.1-G-2 Provide for an overall balance of uses – employment, residential, cultural, government, and destination – as well as a full compendium of amenities and services.
- 3.3-G-1 Provide a range of housing opportunities suitable for urban environments and accommodating a diverse population.
- 3.4-G-1 Continue to promote the production of affordable housing in all of Downtown's neighborhoods and districts.
- 3.4-G-1 Increase the supply of rental housing affordable to low income persons.
- 3.5-G-2 Foster a rich mix of uses in all neighborhoods, while allowing differences in emphasis on uses to distinguish between them.



- 6.5-G-3 Foster redevelopment of Southeast with an urban mix of new residents and a variety of housing types, employees, artists, and conventioners, while preserving light industrial commercial service functions that serve downtown.
- 9.1-G-1 Protect historical resources to communicate Downtown's heritage.
- 9.1-G-2 Encourage the rehabilitation and reuse of historical resources.
- 9.2-G-1 Integrate historical resources into the Downtown fabric while achieving policies for significant development and population intensification.
- 9.2-P-1 Incorporate elements of historical buildings in new projects to impart heritage.
- 9.2-P-3 Promote the adaptive reuse of intact buildings (designated or not) and/or significant elements, as a cultural and suitability goal.

#### Site Description

- Project site:
  - Approximately 4,200 SF;
  - Slopes down slightly in elevation from west to east by approximately two feet;
  - Currently occupied by a vacant single-story designated historic resource, a single-story residential building known as the John and Mary Wright House ("historic resource").
- Surrounding land uses include:
  - North – Two-story office within a potentially historic structure;
  - West – Six-story residential (Park Boulevard East);
  - South – Six-story residential (Park Boulevard East);
  - East – Five-story residential (Entrada Apartments).
- Zoning:
  - Residential Emphasis (RE) Land Use District;
  - Park Sun Access Overlay – ensures adequate sunlight to future park sites by controlling the height of new development to the south and west of the future park sites.

#### PROJECT DESCRIPTION

The Project consists of a seven-story, 77 foot tall mixed-use development comprised of 45 DU, of which six are affordable to very low-income residents, with incomes and rents restricted to 30-50% of Area Median Income (AMI). All units are studios that range in size from 211 SF to 392 SF. The ground floor consists of leasable commercial space, 564 SF of which is within portions of a designated historic resource proposed to be retained on-site and incorporated into the Project. The ground floor also includes a partly-covered breezeway leading to a second commercial lease space behind the historic resource, as well as the residential lobby at the rear of the site. The breezeway will also serve as an egress court for the building and an outdoor seating area for at potential future retail/dining operation housed in the commercial space. An approximately 3,000 SF rooftop outdoor common space is proposed that includes lounge areas, a communal barbeque, and a pet relief area. Because the Project contains less than 50 units, it is not required to provide outdoor common space, common indoor space, private open space (balconies), pet open space, or storage. The Project does not include off-street parking as discussed later in the report. The Applicant's project description and architectural narrative are

included in Attachment B.

The following is a summary of the Project based on drawings dated January 23, 2018:

Site Area	4,200 SF
Base Minimum FAR	3.5
Base Maximum FAR	6.0
Maximum FAR with Amenity Bonuses	8.0
Maximum FAR with Affordable Housing Bonus	9.0
Proposed FAR	5.9
Above Grade Gross Floor Area	25,105 SF
FAR Bonuses Proposed	None
Stories / Height	7 stories / 77 feet
Amount of Commercial Space	1,722 SF
Housing Unit Summary	<u>#</u> <u>Size</u> <u>Rents</u>
Total Number of Housing Units	45
Studios	39 211-392 SF \$1,242 - \$1,415
Studios (30-50% AMI)	6 211-320 SF \$776
Number of Buildings over 45 Years Old	1 (designated historic resource)
Inclusionary Affordable Housing Compliance	Inclusionary Affordable Housing will be provided on-site with six affordable units.
Automobile Parking	
Total (Required / Proposed)	11 / 0*
Motorcycle Parking (Required / Proposed)	2 / 0*
Bicycle Parking (Required / Proposed)	9 / 0*
Common Indoor Space	
Required / Proposed	0 SF / 0 SF
Common Outdoor Open Space	
Required / Proposed	0 SF / 3,067 SF
Private Open Space (Balconies and Decks)	
Required / Proposed	0 / 15 balconies
Pet Open Space	
Required / Proposed	0 SF / 185 SF
Residential Storage	N/A
Assessor's Parcel Nos.	535-156-08-00
Sustainability	LEED Platinum

\*Project is utilizing incentive under the Affordable Housing Regulations to waive required parking.

PERMITS REQUIRED:

- CCDP for new construction.
- SDP for the substantial alteration of a historic resource.

## AFFORDABLE HOUSING REGULATIONS

Pursuant to implementing the State of California Density Bonus Law provisions, the San Diego Municipal Code (SDMC) provides for the following when a project includes affordable housing:

1. Floor Area Ratio (FAR) Bonus.
2. Reduced Parking Requirements.
3. Development Incentives.

The Applicant is proposing to restrict six (13%) of the residential units to persons qualifying as very-low income residents, or those earning less than or equal to 50% of the AMI. Based on the provision of affordable housing, the Project is entitled to the following:

1. A 50% FAR Bonus.
2. A reduction in parking requirements from the 1.0 parking space/unit plus guest parking to a rate of 0.5 parking spaces/bedroom.
3. Three incentives from development standards.

The purpose of the Affordable Housing Regulations is to incentivize developers to provide affordable housing and reduce the burden of providing costly parking in areas served by transit.

Per SDMC Section 143.0740, the applicant is requesting that two incentives be used for two deviations requested by the Project. The section states that an incentive can mean a deviation to a development regulation. The Section further states that:

*“Upon an applicant’s request, development that meets the applicable requirements of Section 143.0720 shall be entitled to incentives pursuant to Section 143.0740 unless the City makes a written finding of denial based on substantial evidence, of any of the following:*

- (A) The incentive is not required in order to provide for affordable housing costs, as defined in California Health and Safety Code Sections 50052.5 and 50053;*
- (B) The incentive would have a specific adverse impact upon public health and safety as defined in Government Code section 65589.5, the physical environment, including environmentally sensitive lands, or on any real property that is listed in the California Register of Historical Resources and for which there is no feasible method to satisfactorily mitigate or avoid the specific adverse impact without rendering the development unaffordable to low income and moderate income households;*
- (C) The incentive would be contrary to state or federal law. Requested incentives shall be analyzed in compliance with the California Environmental Quality Act as set forth in Chapter 12, Article 8, and no incentive shall be granted without such compliance.”*

Thus, if the findings for applicable sections A-C above cannot be made, the incentives must be granted. The two incentives are requested for the following deviations:

1. CCPDO Section 156.0310(f)(1) Commercial Space Depth. The Applicant is requesting to reduce the minimum depth of the ground-floor commercial space from 25 to 23.5 feet.
2. CCDPO Section 156.0313(d), SDMC Section 143.0744(b) Parking Requirement. The Applicant is requesting to waive the 11 required parking spaces, which is at a rate of 0.5



spaces per bedroom and further reduced by 50% for lots of 5,000 SF or less under the CCPDO.

#### Incentive #1: Commercial Space Depth

Per the CCPDO, the required minimum depth of commercial, ground-floor spaces shall be no less than 25 feet along at least 75% of the commercial space frontage along a public street. The remaining 25% of commercial frontage shall be no less than 15 feet in depth if needed to accommodate other internal functions of the building. The historic resource, portions of which are to be retained on-site and operated as a retail or dining establishment, occupies approximately 76% (32 feet) of the 42-foot ground floor street frontage. Because the majority of the ground-floor street frontage consists of the facades of an existing historic resource and the Project proposes maintaining the existing characteristics of the building where feasible, the incentive to deviate from the 25-foot minimum commercial space depth by two feet to accommodate the historic resource is reasonable.

#### Incentive #2: Parking Requirement

The Project proposes a total of 45 residential units, six of which (13%) will be restricted to persons qualifying as very-low income residents, or those earning less than or equal to 50% of the AMI. Per the Affordable Housing Regulations of the SDMC, the inclusion of the six affordable units entitles the Project to a reduction in parking from a rate of one space per unit plus guest parking to a rate of 0.5 spaces per bedroom. Since all 45 units in the Project are studio units, this amounts to 22.5 parking spaces. Additionally, the CCPDO includes a parking provision for small lots of 5,000 square feet or less to further reduce the required parking by 50%. Since this lot is 4,200 SF, the 22.5 required parking spaces can be reduced by 50% resulting in a total of 11.25 required parking spaces for the Project. The Applicant is requesting to utilize one of the three incentives earned from the provision of the affordable units to waive the requirement for the 11 parking spaces.

Due to the potential adverse impacts from the elimination of all parking in the Project, the Applicant submitted an economic feasibility report (Attachment C) that evaluated four alternatives to the elimination of parking. The study assessed each alternative's margin on revenue in relation to that of the Base Project, described herein:

- Base Project – No parking is provided in the Project.
- Parking Alternative 1 – Two levels of below-grade parking are constructed.
- Parking Alternative 2 – One level of below-grade parking is constructed that utilizes tandem parking stalls.
- Parking Alternative 3 – On-grade parking is constructed at the expense of all ground floor commercial lease space.
- Parking Alternative 4 – Two levels of above-grade parking is constructed on levels two and three, resulting in less residential units and the elimination of the affordable units.

The Applicant's analysis (Attachments C and D) concluded that due to the small lot size and the retention of the historic resource on-site, the strict application of the provisions of the parking requirement to incorporate the 11 spaces into the design of the Project would result in the loss of the affordable units in order to compensate for the increased construction costs. The provision of both the affordable units and the required parking would make the Project as a whole economically infeasible. Table 1 below summarizes the financial findings of the economic feasibility study and demonstrates the Applicants conclusion that only the Base Project produces a financially feasible project with affordable housing units.

Table 1:

ALTERNATIVE	CAPITALIZED VALUE	DEVELOPMENT COST	DIFFERENTIAL
Base Project	\$9,848,448	\$8,497,894	<b>\$1,350,514</b>
1 – Two below-grade parking levels	\$9,848,448	\$10,737,934	<b>-\$889,486</b>
2 – One below-grade parking level	\$9,848,448	\$10,692,934	<b>-\$844,486</b>
3 – On-grade parking	\$9,848,448	\$10,459,134	<b>-\$610,686</b>
4 – Two above-grade parking levels	\$9,848,448	\$15,134,600	<b>-\$5,286,152</b>

One of the primary concerns expressed by public speakers is the lack of on-site parking in the Project. Residents stated that there already isn't enough parking in the neighborhood and the Project would make the parking conditions worse, especially with the conversion of the former Hotel Metro SRO buildings with 193 SRO units on the block into 86 market-rate studio apartments. The former Hotel Metro project consists of a building on either side of 13<sup>th</sup> Street and is undergoing a conversion of 136 Single Room Occupancy (SRO) units to 84 market-rate studio units. Hotel Metro was originally constructed in 1990 and was not required to provide any on-site parking. The property was fully developed, so when the building was converted to market-rate units, there was no ability to provide the required parking. The developer was therefore granted a Neighborhood Use Permit in 2016 to eliminate the parking requirement.

One Committee member asked about the restrictions of the existing on-street parking surrounding the Project. At this time, the parking on 13<sup>th</sup> Street is open with no restrictions. CivicSD is currently conducting an assessment of on-street parking in East Village, as well as an update to the Downtown Comprehensive Parking Plan, and the block will likely have two-hour, metered parking in the future. Further, due to the elimination of an existing driveway on the subject property, two additional on-street parking spaces would be added as a result of this Project.

The Applicant also referenced his intent to attract residents that seek an urban lifestyle and utilize the transit options available in a dense urban environment. The Downtown area is considered a transit-oriented district with many mobility options, in the immediate area of the Project site. The map shown in Attachment E shows the current and future transit network for the Downtown area, including trolley stations and corridors and bus routes. This network is in

addition to the ride shares, bike shares, and the Free Ride Everywhere Downtown (FRED) service. Also, many services, amenities, and jobs are within walking distance of the Project site. If parking is needed by one of the Project's residents, the 1,000-space Padres Parkade parking garage is located two blocks to the west and a future 180-space public parking garage will be constructed at the East Village Green park site two blocks to the north in the next several years. However, the Project's parking requirement is for 11 parking spaces and the Project is entitled to an incentive to waive the parking requirement under state law and the SDMC in order to make the provision of the six affordable units financially feasible.

Staff did not find any substantial evidence that the incentives would (1) not be required to provide for affordable costs; (2) adversely affect public health or safety; or (3) would be contrary to State or Federal law. Therefore, Staff is recommending approval of the two incentives.

### DESIGN REVIEW

The Project exhibits rectangular massing of Type 1 construction, with uniformly-spaced windows and balconies on the upper floors of the street-fronting façade. The building has a simple, geometrical design that utilizes primarily concrete and stucco, along with black aluminum frames on the windows and rooftop deck shade structure. The property includes an existing historically-designated house known as the John and Mary Wright House. The Applicant proposes to retain portions of the historic resource on the site by removing the south, east, and north facades, rehabilitating them off-site, and installing them onto a new Type 1 ground floor structure to replicate its existing exterior appearance. Above the historic resource are six stories of residential units that are described by the Applicant as "micro units," none exceeding 392 SF in size. The rooftop includes an outdoor deck with a barbeque and lounging space, a pet area, and workout equipment for use by the building's residents.

The ground floor is designed to make use of the historic resource as a lease space for a small café or coffee shop. A partially-covered, double-height breezeway runs along the side of the historic resource, exposing its rehabilitated north façade, and serving to provide access to a second commercial lease space behind the historic resource. The floorplate of the upper floors jogs inward to open the breezeway to the sky and provide sunlight to the courtyard. The breezeway terminates at the rear of the property in the small residential lobby containing the elevator and stairwell.

The upper floors of the Project consist of gray, cast-in-place concrete and black and white smooth stucco. The rooftop deck shade structure frame element, as well as the window frames and balcony railing, are comprised of black aluminum. Balconies are proposed for each of the street-fronting units on floors three through seven. The balconies are proposed to include large, concrete planters which are envisioned to contain plants that drape over the edge and serve to soften the appearance of the building's façade. The second floor street fronting units do not have balconies and the façade is setback approximately 10'-3" from the property line (front of the bay windows) in order to provide a distinction between the existing historic resource and the new construction.



### SDP FOR SUBSTANTIAL ALTERATION OF HISTORIC RESOURCE

The historic resource is currently vacant and consists of a one-story residential building constructed in 1881 in the Italianate architectural style. The building was designated as historic by the City of San Diego Historic Resources Board (HRB) on November 16, 2017 as HRB No. 1278. A primary reason for the designation was the building's conveyance of the historic significance of the Italianate style by embodying the characteristics associated with the style, such as its one-story form sheathed with wood siding, the bay windows with pediments, and the recessed front porch with an entablature. A rear addition to the house was constructed in 1942, which the historic designation excludes.

The Project proposes a substantial alteration of the historic resource by removing three facades, relocating them onto a new concrete structure at the front of the property, and constructing six levels of residential units directly above the building. The Treatment and Monitoring Plan is included as Attachment J. Under the SDMC, a substantial alteration to a designated historical resource requires the approval of an SDP, a Process 4 decision by the Planning Commission after a recommendation by the HRB. Specific findings are required for an SDP, including findings that require analysis of alternatives that could minimize the potential adverse effects on the designated historical resource. The Applicant submitted draft SDP findings which are included as Attachment H.

Chapter 9 of the DCP establishes the strategy for preservation of historical resources as part of Downtown's continued development. Historic buildings are identified under a three-tiered system based on their classification:

1. The National Register of Historic Places (NRHP) – representing the highest level of designation and marking resources contributing to the nation's history – bestows the greatest protection.
2. Listing on the California Register of Historic Resources (CRHR) also establishes substantial protections in recognition of contributions to state heritage.
3. The third tier, the San Diego Register of Historic Resources (SDRHR), includes properties deemed to have contributed significantly to regional history and culture.

The Wright House is a locally listed property as outlined in the third tier above and has not been found to be eligible for either the NRHP or the CRHR. The DCP's strategy for conserving Downtown historic resources relies on the established process through the National, California, and Local Register designations of individual properties and districts. Each designation is associated with preservation goals and development restrictions. Specifically, Table 9-1: *Historical Designations and Preservation Goals*, of the DCP calls for the following preservation goal for buildings listed in the San Diego Register of Historic Resources:

*SDRHR Listed – Whenever possible, retain resource on-site. Partial retention, relocation, or demolition of a resource shall only be permitted through applicable City procedures. Resources contributing to a San Diego Register District have the same protection status as individually-listed resources.*

The Applicant submitted an economic feasibility report (Attachment D) that evaluated potentially less impactful project alternatives, described herein:

- Base Project – The historic resource will be repositioned to the front property line with the new building constructed above and the second level setback 2'-5½" from the front façade of the historic resource's bay windows (Note: the revised Project now provides a 10'-3" setback from the front façade of the historic resource's bay windows).
- Project Alternative 1 – The historic resource would remain in its exact location with a new building constructed in the remaining lot area behind the historic resource.
- Project Alternative 2 – The historic resource would be repositioned to the front property line with a new building constructed in the remaining lot area behind the historic resource.
- Project Alternative 3 – The historic resource would be repositioned to the front property line with a new building constructed behind and cantilevered over the historic resource.
- Project Alternative 4 – The historic resource would be relocated off-site and a new building constructed on the full lot.

The SDP contains further findings that stipulate that the denial of the proposed development would result in economic hardship to the owner. For purposes of this finding, "economic hardship" means there is no reasonable beneficial use of a property and it is not feasible to derive a reasonable economic return from the property.

To address this finding, the Applicant retained the Xpera Group to evaluate the four Project Alternatives against the Base Project. The Xpera Group found that only the Base Project and Alternative 4 were economically feasible. Alternative 4 entails the relocation of the historic resource off-site. After consultation with local preservationists, the Applicant concluded that Alternative 4 is not desirable because it did not keep the historic resource in the Downtown area. There are advantages and disadvantages to both maintaining the historic resource on-site and relocating it off-site, as outlined in Table 2 below:

Table 2:

RETENTION KEEP ON-SITE		RELOCATION OFF-SITE	
ADVANTAGES	DISADVANTAGES	ADVANTAGES	DISADVANTAGES
Will remain in Downtown, preserve character/history	Loss of some historical integrity with new construction directly above	Historic resource is kept fully intact	New site is outside Downtown, loses context
Maintains public views of two facades		Avoids demolition	Location not readily visible from public views on isolated street
Avoids demolition			

In order to retain the historic resource in Downtown, only the Base Project is economically feasible, as the three alternatives exhibit higher costs and lower returns resulting in economically infeasible alternatives according to the Xpera report. CivicSD staff, including financial analysts, did review the Xpera report and found no issues with its analysis and conclusions. Therefore,

denial of the proposed development would result in economic hardship to the owner. Accordingly, the findings provided by the Applicant supporting the SDP request have been included as Attachment H.

### CCDP

The purpose and intent of a CCDP is to administer and ensure compliance with the CCPDO, DCP, Centre City Streetscape Manual, and any policies or guidelines adopted by the City of San Diego to implement the DCP.

### Findings

1. *The proposed development is consistent with the DCP, CCPDO, SDMC, and all other adopted plans and policies of the City of San Diego pertaining to the Centre City Planned District (CCPD).*

The proposed development is consistent with the DCP, CCPDO, Land Development Code (LDC), and all other adopted plans and policies of the City of San Diego pertaining to the CCPD as the development advances the goals and objectives of the DCP and CCPDO by:

- Provide for an overall balance of uses – employment, residential, cultural, government, and destination – as well as a full compendium of amenities and services.
- Provide a range of housing opportunities suitable for urban environments and accommodating a diverse population.
- Continue to promote the production of affordable housing in all of Downtown's neighborhoods and districts.
- Increase the supply of rental housing affordable to low income persons.
- Foster a rich mix of uses in all neighborhoods, while allowing differences in emphasis on uses to distinguish between them.
- Foster redevelopment of Southeast with an urban mix of new residents and a variety of housing types, employees, artists, and conventioners, while preserving light industrial commercial service functions that serve downtown.
- Protect historical resources to communicate Downtown's heritage.
- Encourage the rehabilitation and reuse of historical resources.
- Integrate historical resources into the Downtown fabric while achieving policies for significant development and population intensification.
- Incorporate elements of historical buildings in new projects to impart heritage.

### SDP

The purpose of the SDP procedures is to establish a review process for proposed development that, because of its site, location, size, or some other characteristic, may have significant impacts on resources or on the surrounding areas, even if developed in conformance with all regulations. The intent of these procedures is to apply site-specific conditions as necessary to assure that the development does not adversely affect the applicable land use plan and to help ensure that all regulations are met.



### Findings

The following three General Findings (SDMC Section 126.0504 (a)) are required for all SDPs:

1. *The proposed development will not adversely affect the applicable land use plan;*

Historic Preservation is addressed in Chapter 9 of the DCP and states that locally designated resources are to be retained on-site whenever possible and that “Partial retention, relocation or demolition of a resource shall only be permitted through applicable City procedures,” that are outlined in SDMC Section 143.02 “Historical Resources Regulations.” Substantial alteration of a designated resource means demolition, destruction, relocation, new construction, or alteration activities that would impair the significance of a historical resource. Due to the proposed new construction on and around the historic resource, an SDP is required for the Project. The Planning Commission must make all of the Findings in SDMC Sections 126.0504(a) and 126.0504(i) before construction can occur. Therefore, the processing of this SDP is in compliance with and will not adversely affect the applicable land use plan.

The goals and policies of the DCP generally stipulate that SDRHR Listed buildings should be retained on-site and integrated into the Downtown fabric in a way that contributes to the achievement of the goals for significant development and population intensification. The proposed development will serve to add 45 residential units to a small lot, providing the desired density as well as much needed affordable housing, while at the same time, retaining (while modifying portions of) a historical resource on-site. It will thereby achieve the goals and policies of the DCP and not adversely affect the DCP.

2. *The proposed development will not be detrimental to the public health, safety, and welfare; and,*

The proposed development will not be detrimental to the public health, safety, and welfare through compliance with the applicable Development Regulations of the CCPDO and SDMC as well as the California Building Code.

3. *The proposed development will comply with the applicable provisions of the LDC*

The proposed project will comply with the applicable CCPDO Development Regulations pertaining to building setbacks, building heights, building bulk, building base, ground floor heights, and residential development regulations. It will also comply with the CCPDO’s Urban Design Regulations pertaining to building orientation, façade articulation, street level design, pedestrian entrances, transparency, blank walls, glass and glazing, rooftops, encroachments into public rights-of-way, and regulations pertaining to historical resources requiring an SDP. The two deviations from the development regulations regarding the minimum commercial space depth and required parking are permitted as incentives earned through the provision of affordable housing units per the Affordable Housing Regulations of the SDMC.

In addition to the above findings, the SDMC requires the following Supplemental Findings (SDMC Section 126.0504(i)) for substantial alterations of a designated historical resource:

1. *There are no feasible measures, including a less environmentally damaging alternative that can further minimize the potential adverse effects on the designated historical resource or historical district.*

The following four alternatives were evaluated for their respective margin on revenue (i.e. investment return) versus that of the Base Project:

- Alternative 1: House remains in same position and new building constructed behind the historic resource.
- Alternative 2: Reposition house to the front property line and new building constructed behind the historic resource.
- Alternative 3: Reposition house to the front property line and new building constructed behind and cantilevered over the historic resource.
- Alternative 4: Relocate historic resource off-site out of Downtown and new building constructed on full lot.

The Analysis concluded that the three alternatives that maintain the entire historic resource on-site are not economically viable due to the construction costs exceeding the capitalized value of the Project.

2. *The deviation is the minimum necessary to afford relief and accommodate the development and all feasible measures to mitigate for the loss of any portion of the historical resource have been provided by the applicant; and,*

The three Alternatives that maintain the entire historic resource on-site have been determined to be economically infeasible; therefore, the proposed substantial alteration to the historic resource is the minimum necessary to afford relief and accommodate the development of the site including the provision of affordable housing units. As discussed, the Xpera Group found that only the Base Project and Alternative 4 (relocation off-site) are economically feasible. Therefore, Supplemental Finding #2 can be made.

3. *The denial of the proposed development would result in economic hardship to the owner. For purposes of the finding, "economic hardship" means there is no reasonable beneficial use of a property and it is not feasible to derive a reasonable economic return from the property.*

Per the Xpera Group report, the proposed Base Project will generate a profit of \$993,629. The four Alternatives would result in the profits shown in Table 3 below:

Table 3:

ALTERNATIVE	CAPITALIZED VALUE	DEVELOPMENT COST	DIFFERENTIAL
Base Project	\$9,848,476	8,854,847	<b>\$993,629</b>
1 – House remains, new building behind	\$3,681,216	\$5,049,082	<b>-\$1,367,866</b>
2 – Reposition house, new building behind	\$4,720,008	\$5,816,721	<b>-\$1,096,713</b>
3 – Reposition house, new building above & stepped back	\$4,923,168	\$5,938,286	<b>-\$1,015,118</b>
4 – Relocate house off-site, new building on full lot	\$9,863,980	\$9,614,847	<b>\$249,133</b>

The three Alternatives that maintain the entire historic resource on-site would result in economic hardship to the owner, as they would result in a financial loss or in a project that that would not be able to be financed. The only alternative that is financially feasible is to relocate the historic resource off-site and use the full lot for construction of the new building. However, according to the Applicant, after consulting with community preservation groups, the strong desire is to maintain portions of the historic resource on-site. Therefore, the strict application of the provisions of the historical resources regulations would deprive the developer and property owner reasonable use of the site.

#### PUBLIC CORRESPONDENCE

Staff received public correspondence from community organizations, property owners, and residents who live near the Project site. Circulate San Diego, SOHO, and the East Village Association expressed support, while the East Village Residents Group described some key concerns they have with the Project in their letter. Staff received four letters from nearby property owners expressing support for the Project and 17 letters from individuals, including homeowners in PBE, in opposition of the Project. All public correspondence is included as Attachment I.

#### ENVIRONMENTAL IMPACT

Development within the Downtown Community Planning area is covered under the following documents, all referred to as the “Downtown FEIR”: Final Environmental Impact Report (FEIR) for the San Diego Downtown Community Plan, Centre City Planned District Ordinance, and 10<sup>th</sup> Amendment to the Centre City Redevelopment Plan, certified by the former Redevelopment Agency (“Former Agency”) and the City Council on March 14, 2006 (Resolutions R-04001 and R-301265, respectively); subsequent addenda to the FEIR certified by the Former Agency on August 3, 2007 (Former Agency Resolution R-04193), April 21, 2010 (Former Agency Resolution R-04510), and August 3, 2010 (Former Agency Resolution R-04544), and certified by the City Council on February 12, 2014 (City Council Resolution R-308724) and July 14, 2014 (City Council Resolution R-309115); and, the Final Supplemental Environmental Impact Report for the Downtown San Diego Mobility Plan certified by the City Council on June 21, 2016 (Resolution R-310561). Development within the Downtown Community Planning area is also



covered under the following documents, all referred to as the “CAP FEIR”: FEIR for the City of San Diego Climate Action Plan (CAP), certified by the City Council on December 15, 2015 (City Council Resolution R-310176), and the Addendum to the CAP, certified by the City Council on July 12, 2016 (City Council Resolution R-310595). The Downtown FEIR and CAP FEIR are both “Program EIRs” prepared in compliance with California Environmental Quality Act (CEQA) Guidelines Section 15168. The information contained in the Downtown FEIR and the CAP FEIR reflects the independent judgement of the City of San Diego as the Lead Agency. The Downtown FEIR and CAP FEIR are located on the CivicSD website (<http://civicsd.com/departments/planning/environmental-documents>) and on the City website ([https://www.sandiego.gov/planning/programs/ceqa#Final CEQA Documents](https://www.sandiego.gov/planning/programs/ceqa#Final%20CEQA%20Documents)). Consistent with best practices suggested by CEQA Guidelines Section 15168, a Downtown 15168 Consistency Evaluation (“Evaluation”) has been completed for the project. The Evaluation concluded that the environmental impacts of the project were adequately addressed in the Downtown FEIR and CAP FEIR; that the project is within the scope of the development program described in the Downtown FEIR and CAP FEIR and is adequately described within both documents for the purposes of CEQA; and, that none of the conditions listed in CEQA Guidelines Section 15162 exist. Therefore, no further environmental documentation is required under CEQA. The Evaluation is attached to this staff report for informational purposes only; no action regarding the Evaluation is required by the decision maker.

#### CONCLUSION

Staff recommends that HRB recommends to the Planning Commission approval of CCDP/SDP 2018-07.

Respectfully submitted,

Concurred by:



James Alexander  
Associate Planner



Brad Richter  
Vice President, Planning

Attachments: A – Ownership Disclosure Statement  
B – Architectural Narrative and Project Description (provided by Applicant)  
C – Economic Feasibility Report for Parking Incentive  
D – Economic Feasibility Report for Historic Alternatives  
E – Mobility Options Map  
F – Downtown Design Guidelines  
G – Aerial Views  
H – Applicant’s SDP Findings  
I – Public Correspondence  
J – Treatment & Monitoring Plan  
Basic Concept/Schematic Drawings dated January 23, 2018



## Ownership Disclosure Statement

**Approval Type:** Check appropriate boxes for type of approval(s) requested:

- |  |   |   |
|--|---|---|
| <input type="checkbox"/> Limited Use Approval    | <input type="checkbox"/> Neighborhood Development Permit    | <input type="checkbox"/> Centre City Development Permit     |
| <input type="checkbox"/> Temporary Use Permit    | <input type="checkbox"/> Planned Development Permit         | <input type="checkbox"/> Gaslamp Quarter Development Permit |
| <input type="checkbox"/> Neighborhood Use Permit | <input checked="" type="checkbox"/> Site Development Permit | <input type="checkbox"/> Marina Development Permit          |
| <input type="checkbox"/> Conditional Use Permit  | <input type="checkbox"/> Coastal Development Permit         | <input type="checkbox"/> Other: _____                       |

**Project Title:** ELABELLE

**Project Address:** 454 13<sup>th</sup> ST, SAN DIEGO, CA 92101

**Assessor Parcel Number(s):** 535-156-08-00

**Part 1 – To be completed by property owner when property is held by individual(s)**

By signing this Ownership Disclosure Statement, the property owner(s) acknowledges that an application for a permit, map, or other matter, as identified above, will be filed with Civic San Diego on the premises that is the subject of the application, with the intent to record an encumbrance against the property or properties. List below the owner(s) and tenant(s) (if applicable) of the above referenced property or properties; all subject properties must be included. The list must include the names and addresses of all persons who have an interest in the property or properties, recorded or otherwise, and state the type of property interest (e.g., tenants who will benefit from the permit, all individuals who own the property or properties). Original signatures are required from at least one property owner for each subject property. Attach additional pages if needed. Note: The Applicant is responsible for notifying the Project Planner of any changes in ownership during the time the application is being processed or considered. Changes in ownership are to be given to the Project Planner at least thirty days prior to any public hearing on the subject property or properties. Failure to provide accurate and current ownership information could result in a delay in the hearing process.

Additional pages attached: ☐ Yes ☐ No

**Name of Individual (type or print):**

Assessor Parcel Number(s): \_\_\_\_\_

Street Address: \_\_\_\_\_

City/State/Zip Code: \_\_\_\_\_

Phone Number: \_\_\_\_\_

E-mail: \_\_\_\_\_

Signature: \_\_\_\_\_

Date: \_\_\_\_\_

**Name of Individual (type or print):**

Assessor Parcel Number(s): \_\_\_\_\_

Street Address: \_\_\_\_\_

City/State/Zip Code: \_\_\_\_\_

Phone Number: \_\_\_\_\_

E-mail: \_\_\_\_\_

Signature: \_\_\_\_\_

Date: \_\_\_\_\_

Project Title: EZABELLE**Part 2 – To be completed by property owner when property is held by a corporation or partnership**

By signing this Ownership Disclosure Statement, the property owner(s) acknowledges that an application for a permit, map, or other matter, as identified above, will be filed with Civic San Diego on the premises that is the subject of the application, with the intent to record an encumbrance against the property or properties. List below the names, titles, and addresses of all persons who have an interest in the property or properties, recorded or otherwise, and state the type of property interest (e.g., tenants who will benefit from the permit, all corporate officers, and/or all partners in a partnership who own the property or properties). Original signatures are required from at least one corporate officer or partner who own the property for each subject property. Attach additional pages if needed. Provide the articles of incorporation, articles of organization, or partnership agreement identifying all members of the corporation or partnership. Note: The applicant is responsible for notifying the Project Planner of any changes in ownership during the time the application is being processed or considered. Changes in ownership are to be given to the Project Planner at least thirty days prior to any public hearing on the subject property or properties. Failure to provide accurate and current ownership information could result in a delay in the hearing process.

Additional pages attached: ☐ Yes ☐ No**Corporation/Partnership Name (type or print):**NDD ON 13TH ST LOFTS, LLC☐ Corporation ☒ LLC ☐ Partnership

Assessor Parcel Number(s):

535-156-08-00

Street Address:

340 15TH ST, SUITE #1

City/State/Zip Code:

SAN DIEGO, CA 92101

Name of Corporate Officer/Partner (type or print):

SOHEIL NAKHSHAB

Title:

MANAGER

Phone Number:

(619) 255-7257

E-mail:

SOHEIL@NDDINC.NET

Signature:

Date:

2-21-18**Corporation/Partnership Name (type or print):**NAKSHAB DEVELOPMENT & DESIGN, INC☒ Corporation ☐ LLC ☐ Partnership

Assessor Parcel Number(s):

Street Address:

340 15TH ST, SUITE #1

City/State/Zip Code:

SAN DIEGO, CA 92101

Name of Corporate Officer/Partner (type or print):

SOHEIL NAKHSHAB

Title:

ARCHITECT / PROJECT MANAGER

Phone Number:

(619) 255-7257

E-mail:

SOHEIL@NDDINC.NET

Signature:

Date:

2-21-18

**Project Title:** \_\_\_\_\_

**Part 3 – To be completed by all other financially interested parties**

List below the names, titles, and addresses of all financially interested parties and state the type of financial interest (e.g., applicant, architect, lead design/engineering professional). Original signatures are required from at least one individual, corporate officer, and/or partner with a financial interest in the application for a permit, map, or other matter, as identified above. Attach additional pages if needed. Note: The applicant is responsible for notifying the Project Planner of any changes in ownership during the time the application is being processed or considered. Changes in ownership are to be given to the Project Planner at least thirty days prior to any public hearing on the subject property or properties. Failure to provide accurate and current ownership information could result in a delay in the hearing process.

Additional pages attached: ☐ Yes ☐ No

**Name of Individual (type or print):**

☐ Applicant ☐ Architect ☐ Other \_\_\_\_\_

Street Address: \_\_\_\_\_

City/State/Zip Code: \_\_\_\_\_

Phone Number: \_\_\_\_\_

E-mail: \_\_\_\_\_

Signature: \_\_\_\_\_ Date: \_\_\_\_\_

**Corporation/Partnership Name (type or print):**

☐ Corporation ☐ LLC ☐ Partnership

☐ Applicant ☐ Architect ☐ Other \_\_\_\_\_

Street Address: \_\_\_\_\_

City/State/Zip Code: \_\_\_\_\_

Name of Corporate Officer/Partner (type or print): \_\_\_\_\_

Title: \_\_\_\_\_

Phone Number: \_\_\_\_\_

E-mail: \_\_\_\_\_

Signature: \_\_\_\_\_ Date: \_\_\_\_\_

**Name of Individual (type or print):**

☐ Applicant ☐ Architect ☐ Other \_\_\_\_\_

Street Address: \_\_\_\_\_

City/State/Zip Code: \_\_\_\_\_

Phone Number: \_\_\_\_\_

E-mail: \_\_\_\_\_

Signature: \_\_\_\_\_ Date: \_\_\_\_\_

**Corporation/Partnership Name (type or print):**

☐ Corporation ☐ LLC ☐ Partnership

☐ Applicant ☐ Architect ☐ Other \_\_\_\_\_

Street Address: \_\_\_\_\_

City/State/Zip Code: \_\_\_\_\_

Name of Corporate Officer/Partner (type or print): \_\_\_\_\_

Title: \_\_\_\_\_

Phone Number: \_\_\_\_\_

E-mail: \_\_\_\_\_

Signature: \_\_\_\_\_ Date: \_\_\_\_\_





### **1.3 Ezabelle Project Description**

The Proposed Project, Ezabelle, is a new Affordable mixed-use building consisting of Residential Apartments and Ground Floor Commercial to activate the street. We have designated the existing home as a Historic Resource and will incorporate it into the new development.

The current site, 454 13<sup>th</sup> Street, is a approximately 4,200 sf interior lot located on 13th street, between Island and J Street. Along the South and East Property Line there is an adjacent 6-story apartment building. Along the North Property Line there is an existing 3-story Historic Home currently being used as a law office. The Gross Square Footage of Ezabelle will be approximately 26,491sf within a new 85 ft tall 7-Story Concrete Structure. The project will consist of 48 new residential apartments and 1 large ground floor commercial space. 12% of our units will be dedicated to Very Low Income Tenants. No Parking will be included in the building design as a means of sustainable practices. The new project will capitalize on the public amenities the metro area has to offer: Access to the Trolley Station which is less than a block from the site and the creation of a micro community within the proposed commercial ground floor spaces. The building will create a large courtyard space open to the sky to allow for better quality of life and to create a indoor/outdoor gathering space for the community.

All Residential Units will be fully equipped with modern amenities. The concept for the units is based on a Swiss Army Knife. All fixtures/Appliance/Furniture will be built in to the residential units. High-End Cabinetry will run across the entire wall lengths housing independent Washer-Dryers, Dishwashers, Kitchens, Water Heaters, Beds, Storage, benches, etc.... The building will also be designed and built to meet the standards for LEED Certification. Ezabelle will most likely achieve LEED Platinum status making it sustainable for the future tenants and the city as a whole.



## 1.4 Ezabelle Architectural Narrative

San Diego is experiencing a housing crisis. It is becoming more and more difficult for people to afford homes within our city. Quality Housing at an affordable price point does not exist in our city. The increased cost of living has created a major influx in our city's population growth. A rapidly growing population and a lack of quality, affordable living is the issue that we are addressing with the Ezabelle development.

Many people live in San Diego while attending school or during the early stages of their professional careers. Trying to establish roots in our city is very expensive. The high real estate costs have triggered more demand for rental properties. Unfortunately, the rental market is also very costly for residents. The increase rents force people to live with roommates, or live outside the metro setting. Keeping these factors in mind, Nakhshab Development and Design, Inc. feels there is a need to provide affordable quality modern living for our local residents.

When designing Ezabelle, we decided to create a product that would be considered a design standard in international cities. Our building concept is based on the Swiss army knife as it is comprised of small compact apartments fully outfitted with a variety of modern features. The compact home will be more efficient than a large 2 bedroom apartment. Less will be demanded of our tenants as they will not need to spend as much money on furnishings, fixtures, and appliances.

Our concept for Ezabelle is derived from our value at Nakhshab to elevate quality of life. We design buildings as if we were living in them ourselves. Some elements that we have incorporated into this project that will enhance the quality of life include the following: Our building will be constructed of concrete, creating a more durable structure and helping with sound attenuation for our tenants comfort. Expansive floor to ceiling doors and windows have been incorporated into the design creating an abundance of natural light and ventilation throughout all units.

The current project site contains a historically designated single family home (John and Mary Wright House) built in 1882. We will be leaving the house on the site and building over the profile maintaining the historic integrity of this Italianate Style residence. The building will be built to follow LEED Platinum standards creating a sustainable, healthy, affordable living space for our residents. The ground level communal courtyard adjacent to the commercial space will create a community within our property and the surrounding neighborhood. This space will be open to both our tenants and the public. Other amenities within our building include the common rooftop deck which will provide outdoor dining, lounge, gardening, and potentially a swimming pool.

# Economic Feasibility for Parking Incentive

## Ezabelle Project

APN: 535-156-08-00

The Goal of the following is to provide a thorough written justification for the use of an incentive to reduce the parking requirements from 12 spaces to zero spaces. It will provide an economic analysis which explains why the project would be infeasible if the parking requirement is not eliminated.

Per SDMC Section 143.07, the provision of 12% Very Low Income units allows three incentives to deviate from the development regulations.

### §143.0740 Incentives in Exchange for Affordable Housing Dwelling Units

(d) The number of incentives available are identified in Table 143-07A for very low income households, Table 143-07B for low income households, and Table 143-07C for moderate income households consistent with the percentage of pre-density bonus units identified in the first column of each table.

**Table 143-07A**  
**Very Low Income Density Bonus Households**

Percent Very Low Income Units	Percent Density Bonus	Number of Incentives
12	38.75	3

\*\*\* Applicable portion of official table shown

With 12% Very Low Income, the parking ratio may be reduced from one space per unit to 0.5 spaces per bedroom. With 48 units, at least 6 units must be Very Low Income to take advantage of the Affordable Housing Bonus Parking ratio, which results in 24 required spaces.

**Table 143-07D**  
**Parking Reduction for Proximity to Transit**

Type of Development	Percent Affordable	Transit Requirement	Parking Ratio for Development
<i>Rental or for-sale development containing market rate and low income and/or very low income dwelling units</i> VERY LOW INCOME LOW INCOME	11% 12%	<i>The development is located within a Transit Priority Area</i>	<i>0.5 spaces per bedroom</i>

\*\*\* Applicable portion of official table shown

Additionally, the CCPDO allows for a Small Lot Reduction for lots of 5000 sq. ft. or less to reduce required parking by 50%. The required parking is further reduced from 24 to 12 spaces.

Applying one of the three Incentives, we would like to further reduce our parking to zero spaces.

The justification for this reduction is based on multiple variables which affect the economics of the project in a major way.

Current parking requirements serve to reduce and/or eliminate our ability to provide affordable units, in most cases making it an economically infeasible endeavor altogether.

For the sake of being thorough we have outlined 4 parking alternatives to the base case proposed, however the reality is that including parking would be very challenging to accommodate due to life safety requirements and the small lot size.

However, if we assume somehow that these limitations are surmountable then we must look at the economic picture for each alternative.

#### **Base Case:**

In this case the project will provide no on-site parking, however at least 2 off-site parking spaces plus 2 motorcycle spaces would be established due to the elimination of the existing curb cut and driveway on 13<sup>th</sup> street.

#### **Alternative I:**

To accommodate the required 12 parking spaces and the fact that the project will have a subterranean vault element for the transformer which will take up a considerable footprint of the lot, this alternative explores creating 2 subterranean basement levels. One needs to consider the cost of building these levels, grading and excavation, shoring and an elevator lift plus the extension of both exit stair elements to access both parking levels.

#### **Alternative II:**

This alternative will explore only 1 subterranean level below the vault level. This basement parking level will have to encompass higher floor to ceiling heights to accommodate a stackable tandem parking option. The costs to consider here are the cost of building out the level, grading and excavation, shoring, hydraulic lifts for the vehicles and an elevator lift plus extension of both exit stair elements to access both parking levels.

#### **Alternative III:**

This alternative explores parking on grade which would involve the loss of all commercial elements on the ground floor, hydraulic lifts, loss of amenity value for the tenants, increase in rents to accommodate loss of revenue thereby restricting or eliminating very low income housing.



#### Alternative IV:

In this alternative we explore 2 levels of above grade parking which would save the commercial element on the ground floor but would severely limit the number of residential units and completely eliminate very low income and low income housing altogether. The costs which need to be considered are the building of 2 levels above grade, elevator lift, loss of rental units.

The table below outlines the breakdown of costs for each alternative mentioned above:

### Incremental Costs of Parking Alternatives

		Incremental or Capitalized cost of Parking	Current Project Costs	Total Project Cost Including Parking	Capitalized Value of Project	Value Created
	<b>Base Development</b>					
	2 off site vehicular spaces plus 2 motorcycle	\$0.00				
<b>Base</b>	<b>Total Cost</b>	<b>\$0.00</b>	<b>\$8,497,934.00</b>	<b>\$8,497,934.00</b>	<b>\$9,848,448.00</b>	<b>\$1,350,514.00</b>
		Cost				
	<b>2 Subterranean levels below grade</b>					
	2 basement levels	\$840,000.00				
	Grading & Excavation	\$400,000.00				
	Shoring	\$750,000.00				
	Elevator lift	\$250,000.00				
<b>Alternative I</b>	<b>Total Cost</b>	<b>\$2,240,000.00</b>	<b>\$8,497,934.00</b>	<b>\$10,737,934.00</b>	<b>\$9,848,448.00</b>	<b>-\$889,486.00</b>
	<b>Tandem Parking level below grade</b>					
	1 level with stacked tandem parking	\$735,000.00				
	Grading & Excavation	\$400,000.00				
	Shoring	\$750,000.00				
	Hydrolic lifts (6)	\$60,000.00				
	Elevator lift	\$250,000.00				
<b>Alternative II</b>	<b>Total Cost</b>	<b>\$2,195,000.00</b>	<b>\$8,497,934.00</b>	<b>\$10,692,934.00</b>	<b>\$9,848,448.00</b>	<b>-\$844,486.00</b>
	<b>On Grade Tandem Parking</b>					
	Loss of commercial element (2 units)	\$957,866.67				
	Hydrolic lifts	\$60,000.00				
	Loss of Residential units (2 units)	\$693,333.33				
	Loss of amenity value	\$250,000.00				
	elimination of affordable units					
<b>Alternative III</b>	<b>Total Cost</b>	<b>\$1,961,200.00</b>	<b>\$8,497,934.00</b>	<b>\$10,459,134.00</b>	<b>\$9,848,448.00</b>	<b>-\$610,686.00</b>
	<b>2 Levels above grade parking</b>					
	2 levels of parking	\$840,000.00				
	Elevator lift	\$250,000.00				
	Loss of Residential units (16 units)	\$5,546,666.67				
	Elimination of affordable units					
<b>Alternative IV</b>	<b>Total Cost</b>	<b>\$6,636,666.67</b>	<b>\$8,497,934.00</b>	<b>\$15,134,600.67</b>	<b>\$9,848,448.00</b>	<b>-\$5,286,152.67</b>

Beyond the clear economic challenges which the inclusion of parking brings to the proposed project, the small Lot size creates many ancillary challenges. The lot is only 42ft in width. Given the height of the building we need to setback our proposed structure 1ft on all shared property lines. This reduces our

usable lot width to 40ft. Factoring in the building wall thickness along each property line at 1ft thick concrete walls reduces the useable lot width to 38ft. Now we incorporate the required life safety elements, two exit stairs, gurney compliant elevator, egress court and corridors we are left with very limited space to accommodate vehicular parking on grade. We are forced to go below grade if we are to provide any parking. As demonstrated above, the costs of doing so eliminate affordable units altogether and likely make it impossible even for market rate units in this area since there is only so much one can increase rents on a 300 sq. ft. studio apartment.

Subterranean parking creates its own challenges as well since the proposed project already has a large basement that we need to construct dedicated solely to the utility company for our electric transformer, switchgear and meters.

The footprint of this vault covers nearly 1/3 of the overall site. There is no way of building a driveway or driveway ramp so we are forced to incorporate a parking lift to transport vehicles to the basement levels.

A lift, although would solve this issue, is infeasible as it would be located in the middle of the egress path causing non-compliance issues with the California Building Code Exiting Requirements.

Assuming for a moment that the lift could be installed the Utility Vault forces us to build two stories below grade in order to provide the 12 parking spaces. We will need to extend our 2 exit stairs and elevator to service both of these basement levels as well.

The economic consequences of providing parking on this small lot would be so detrimental that we cannot justify developing the much needed Very Low Income Housing.

The base project as it stands will create 6 Very low-income and 42 Market which are priced to be on the cusp of 80% AMI. Unfortunately to create parking we would undoubtedly need to eliminate all affordable units to be able to shore the losses which stem from any alternatives presented above.

Ezabelle project is a prime example of Transit Oriented Development. We are located adjacent to one of the main trolley stations within metro San Diego. Our goal is to provide a rental option for people without vehicles and to promote the use of the easily accessible public transit. With the expansion of the Trolley line to La Jolla we hope to draw students and professionals into the downtown core.

Finally, as outlined in several studies including a parking study provided to the City of San Diego in 2011 by Wilbur Smith Associated, or by Circulate San Diego in 2016 or by the San Diego Housing commission in 'Addressing the Housing Affordability Crisis', quite often parking requirements can make development unfeasible in San Diego unfortunately.

Parking development costs in San Diego can account for up to 20 percent of a developer's direct costs, which are eventually passed on to final consumers. Given the magnitude of these costs, they have a large impact on overall project feasibility and end-consumer housing prices. The construction cost per parking spot ranges from \$10,000 (ground level) to \$70,000 (below ground). The City of San Diego has particularly conservative parking requirements, setting parking ratios that create an expensive bundling,

whereby residents who do not own cars or use less than the full amount of parking spaces allocated to them are burdened with unnecessary construction costs.

Although this bundling effect generally applies to all types of parking minimums, the level of bundling appears particularly strong in the City of San Diego, where parking ratios are up to two times those of peer cities and where parking minimums are set even around transit-rich areas, such as downtown. The opportunity to reduce parking ratios could be particularly salient to the City of San Diego, whose demographics are younger than most other metro areas in the nation. Changing trends in car ownership among younger age groups that are more likely to utilize alternatives, such as ride-sharing and public transportation, provide an opportunity to relax regulations. Relaxing parking requirements could have a significant impact on the feasibility of housing developments. This study and objectives manual is enclosed for your reference.

Our mission ultimately is to create rich, sprawling, walkable neighborhoods in which green transportation habits are adapted in a departure from the car centric society which has contributed to San Diego's air quality issues as recently reported by the San Diego Tribune.

We firmly believe, as proven by several progressive cities around the country, the provision of adequate resources for young or low income individuals eliminates the need for the expensive proposition of vehicle ownership.

Thank you for your attention to this matter.



Nakhshab Development & Design  
340 15<sup>th</sup> Street – Suite 1  
San Diego, CA 92101  
619.255.7257



May 2, 2018

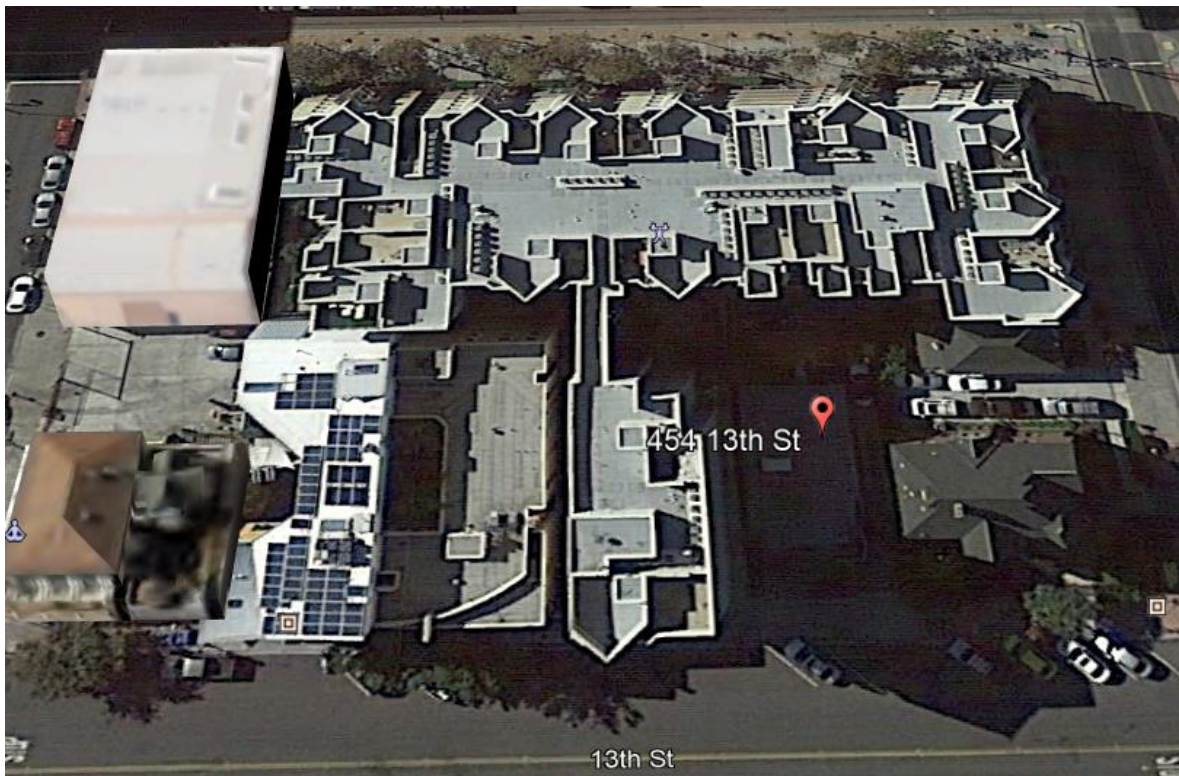
Nakhshab Development & Design  
340 15<sup>th</sup> Street  
Suite 1  
San Diego CA 92101

ATTN: Soheil Nakhshab  
Email: soheil@nddinc.net

**RE: Economic Feasibility of Alternatives for the Property at 454  
13<sup>th</sup> street (Ezabelle) in Downtown San Diego**

Dear Mr. Nakhshab:

This report considers the base case and four alternatives for the development of an affordable rental apartment complex on the 4,200 square foot lot located at 454 13<sup>th</sup> Street between Island Avenue and J Street in downtown San Diego.







As planned, the entire site would be developed with a seven-story concrete structure containing 48 studio apartments with an average size of 300 square feet. There would be no on-site parking.

454 13<sup>th</sup> Street will include 88% market rate units and 12% units to accommodate households that have 50% of Area Median Income.

Market rents would be approximately \$1,300 per month. The market rate units coincide closely with rents at 80% of the Average Median Income in San Diego County.

80% Area Median Income (AMI) Studio Units (1 Occupant) Rent City of San Diego				
80% of AMI Low Income	Monthly Rent (2)	Less Utility Allowance (1)	Cash Rent	
\$ 50,950	\$ 1,274	\$ 45	\$ 1,229	

(1) Based on SD Housing Commission Schedule  
(2) U.S.Dept. of Housing & Urban Development


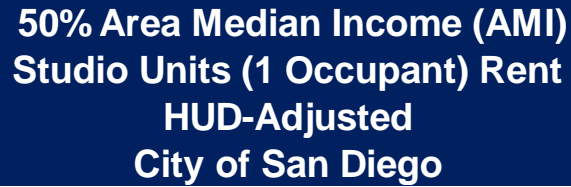


Exhibit 1-A

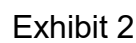
12% of the units would have rents that would be based on the U.S Department of Housing and Urban Development schedule of 50% Average Median Income (AMI). Single occupants could have an AMI of \$31,850.



(1) Based on SD Housing Commission Schedule  
(2) U.S.Dept. of Housing & Urban Development



The site currently contains a one-story historic structure that would be incorporated into the new project, as shown here. The historic structure may be utilized for commercial purposes.







Building will be incorporated into the access doors for the Water Meter and Trash Room.

The building structure will stack from Roof Level down to Grade with this Base Proposal creating an efficient, cost effective structure.

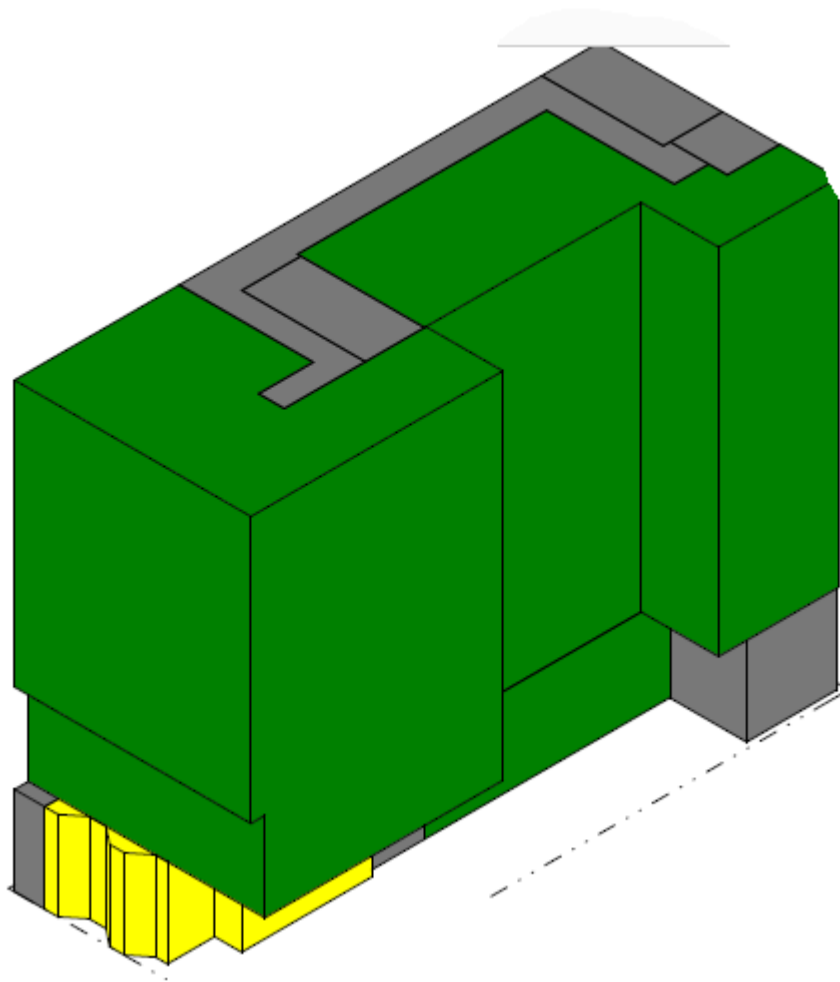


Exhibit 4





### **Alternative 1**

Historic Building will remain in its exact current location. The new development is limited to the area directly to the rear of the Historic Building. The new development will have minimal habitable areas given the need for Life Safety Circulation. The size of the habitable areas will have an economic effect on this project.

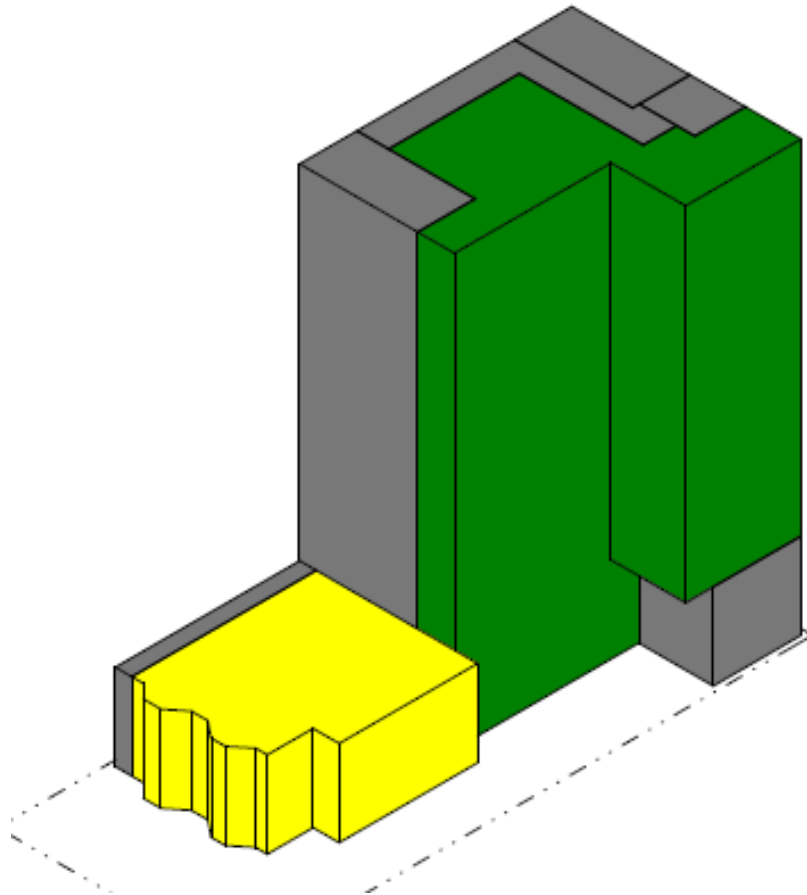


Exhibit 5



## **Alternative 2**

Historic Building will be moved forward to the front property line. The new development will be limited to the area directly to the rear of the Historic Structure. New development has minimal habitable areas given the need for Life Safety Building. The size of the habitable areas will have an economic effect on this project.

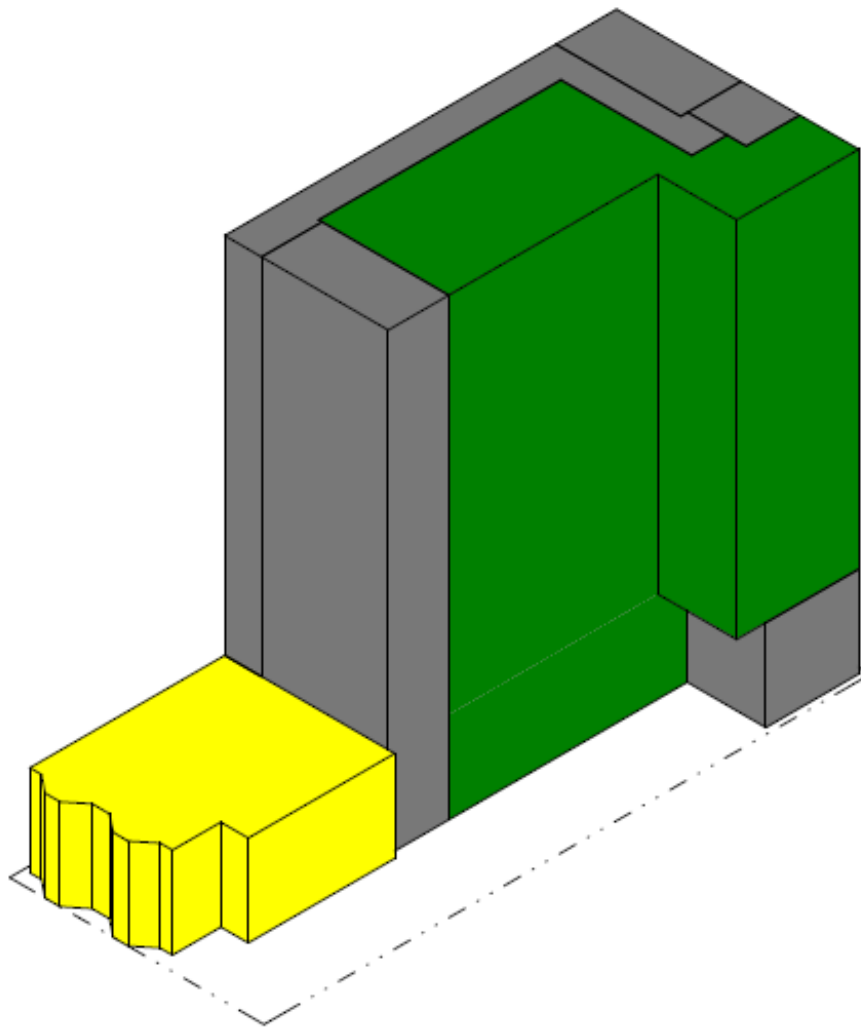


Exhibit 6



### **Alternative 3**

Historic Building will be moved forward to the front property line. The new development will be stepped back at the 2<sup>nd</sup> level 10 feet and levels 3 through 7 will cantilever over the Historic Building.

The biggest challenge with this alternative will be the structural feasibility given the large cantilever. The irregularity of the structure may not meet lateral design requirements per the California Building Code. In addition, the cantilever will require giant concrete beams to be incorporated into the design, which will encroach into the corridors due to the large depths. This could cause the loss of rental units.

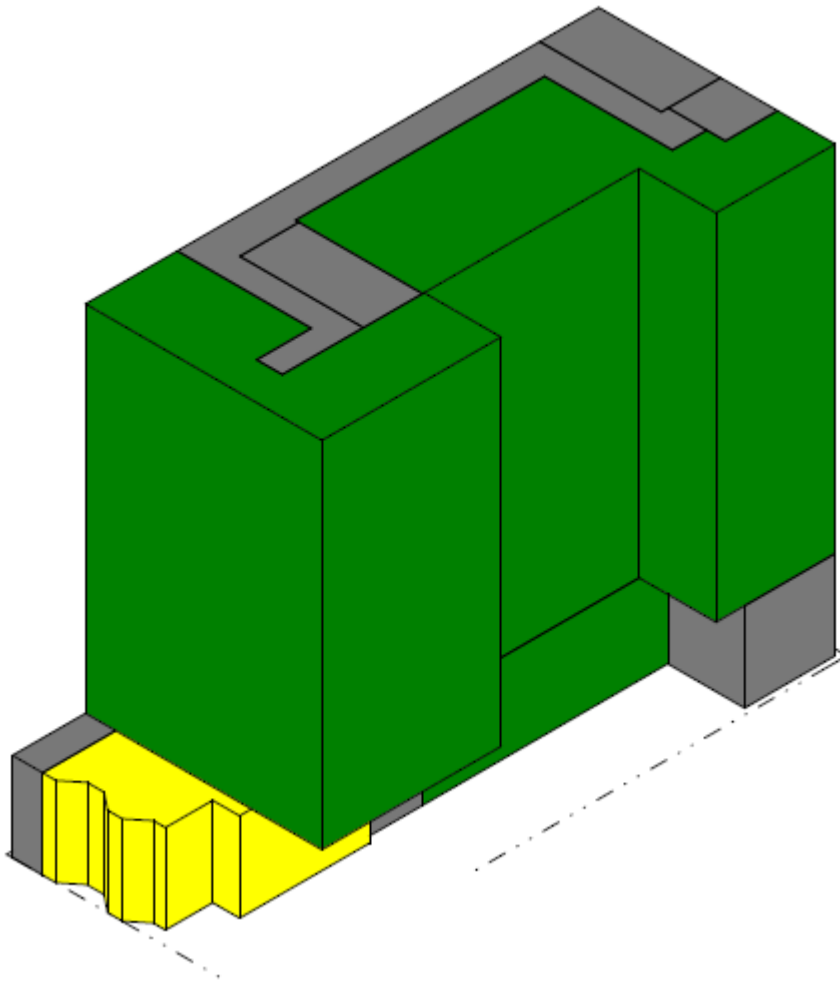


Exhibit 7



#### **Alternative 4**

“This is the alternative that was our former Base Development Proposal where we were planning on relocating the Historic Building from this site for rehabilitation and reuse on another site and building new development on this site. The only reason we are not proceeding with this option is the fact that we want to respond to preservationists’ concerns about removing the Historic Building from this site. Although the relocation alternative will have added expense, it may be the second best option for the Historic Building.”

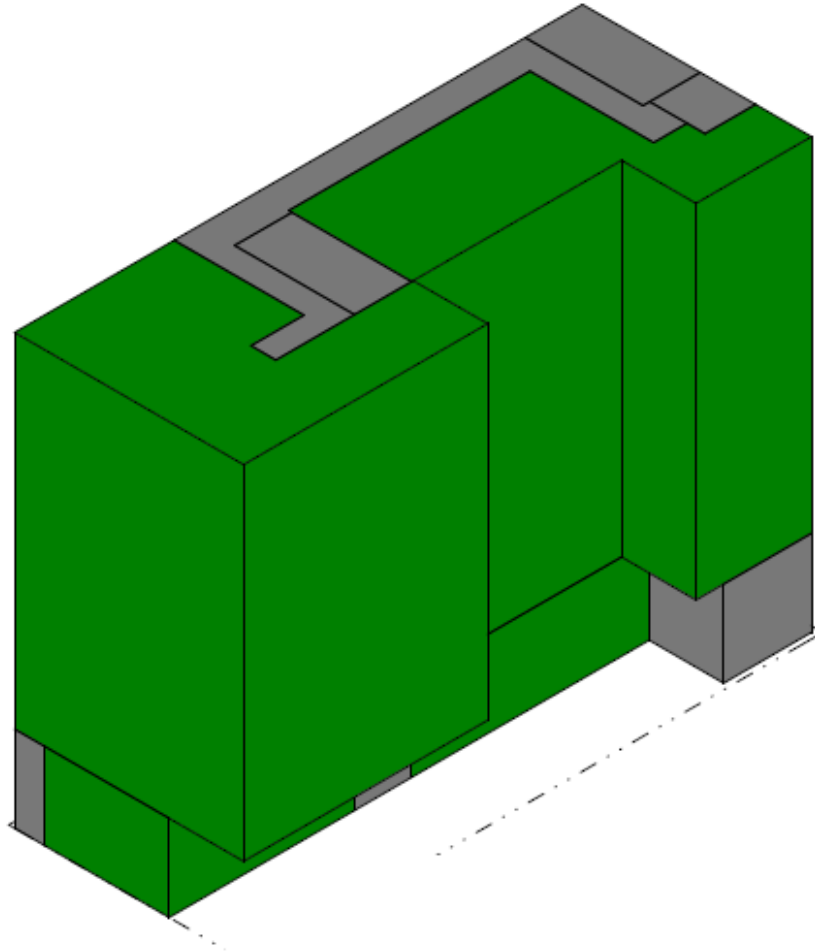


Exhibit 8



The ramifications of the base case and four alternatives are shown in the following exhibit: (Exhibit 9A&B)

Alternative Plans to Determine Economic Consequences 454 13th Street (Ezabelle) Downtown San Diego		
Base Case & Alternates 1 & 2		
Property	Historic Structure	Apartment Project
Base Project: Move historic structure to front property line; build apartments incorporating historic structure		
Construction	Move historic structure to front property line	Build 48 units on site
Construction Implications	Renovation	Cost of units
Parking Implications	No Parking on Site	No Parking on Site
Cost Estimates	Estimate with HVAC, remodeling, seismic & Title 24; exterior to be cosmetically improved	Est. costs prepared for this study
Income potential	Rent out renovated structure	Optimal income with affordable units
Financial implications	Net operating income and return on investment to be calculated	Net operating income and return on investment to be calculated
Alternative 1: Retain existing structure in current location and build new project behind it.		
Construction	Leave historic structure in place and rehab	Build 18 units
Construction Implications	Bring up to seismic stds. and Title 24	Cost per rentable sq.ft. increases dramatically
Parking Implications	No Parking on Site	No Parking on Site
Cost Estimates	Estimate with HVAC, remodeling, seismic & Title 24; exterior to be cosmetically improved	Est. costs prepared for this study
Income potential	Rent out renovated structure	Income would decline dramatically
Financial implications	Net operating income and return on investment to be calculated	Cost would be above value.
Alternative 2: Move historic structure to front property line and build new project behind it.		
Construction	Move historic structure to front property line	Build 24 units
Construction Implications	Bring up to seismic stds. and Title 24	Cost per rentable sq.ft. increases dramatically
Parking Implications	No Parking on Site	No Parking on Site
Cost Estimates	Estimate with HVAC, remodeling, seismic & Title 24; exterior to be cosmetically improved	Est. costs prepared for this study
Income potential	Rent out renovated structure	Income would decline dramatically
Financial implications	Net operating income and return on investment to be calculated	Cost would be above value.

4.2018



Exhibit 9A





**Alternative Plans to Determine Economic Consequences  
454 13th Street (Ezabelle)  
Downtown San Diego**

**Alternates 3 & 4**

Property	Historic Structure	Apartment Project
----------	--------------------	-------------------

**Alternative 3: Move historic structure to front property line; cantilever project over historic structure and build new project over and behind it.**

Construction	Move historic structure to front property line	Build 24 apts.
Construction Implications	Estimate with HVAC, remodeling, seismic & Title 24; exterior to be cosmetically improved	Cost per rentable sq.ft. increases dramatically
Parking Implications	No Parking on Site	No Parking on Site
Cost Estimates	Estimate with HVAC, remodeling, seismic & Title 24; exterior to be cosmetically improved	Est. costs prepared for this study
Income potential	Rent out renovated structure	Income would decline dramatically
Financial implications	Net operating income and return on investment to be calculated	Cost would be above value.

**Alternative 4: Relocate historic structure to another site and built out entire site with apartments**

Construction	Building renovation including new slab and utility hook-ups	Build 48 units on entire site
Construction Implications	Cost of moving structure to another location; renovate building to current code and to Title 24 standards	Cost of units
Parking Implications	No Parking on Site	No Parking on Site
Cost Estimates	Acquire land; relocate building; build new slab; renovate building	Est. costs prepared for this study
Income potential	Rent out renovated structure	Optimal income with affordable units
Financial implications	Net operating income and return on investment to be calculated	Possibility of achieving positive net operating income

4.2018



Exhibit 9B



The base case and alternative unit count, unit square footages and projected rents are shown here (Exhibit 10):

<b>Project Facts</b> <b>No. Units, Square Footage and Projected Revenue</b> <b>454 13th Street (Ezabelle)</b> <b>Downtown San Diego</b>					
	Base Case	Alt 1	Alt 2	Alt 3	Alt 4
<b>Land</b>					
Square footage	4,200				
Cost of Land	\$ 850,000				
Parcel numbers	535-156-08-00				
<b>Unit Mix</b>					
Studios	48	18	24	24	48
Commercial	2	2	2	2	2
<b>Total Units</b>	<b>50</b>	<b>20</b>	<b>26</b>	<b>26</b>	<b>50</b>
<b>Residential Unit Square Footage (Avg.)</b>					
Studios	299	263	284	290	301
<b>Total Square Footage</b>					
Studios	14,352	4,734	6,816	6,960	14,448
Commercial (including historic structure)	1,796	1,559	1,787	1,796	1,830
<b>Total Square Footage</b>	<b>16,148</b>	<b>6,293</b>	<b>8,603</b>	<b>8,756</b>	<b>16,278</b>
<b>Rent Per Residential Unit</b>					
Studios	\$ 1,350	\$ 1,247	\$ 1,279	\$ 1,340	\$ 1,350
<b>Total Projected Monthly Revenue</b>					
Studios	\$ 64,800	\$ 22,446	\$ 30,696	\$ 32,160	\$ 64,800
Commercial	\$ 3,592	\$ 3,118	\$ 3,574	\$ 3,592	\$ 3,660
<b>Total Projected Monthly Revenue</b>	<b>\$ 68,392</b>	<b>\$ 25,564</b>	<b>\$ 34,270</b>	<b>\$ 35,752</b>	<b>\$ 68,460</b>
<b>Total Projected Annual Revenue</b>					
Studios	\$ 777,600	\$ 269,352	\$ 368,352	\$ 385,920	\$ 777,600
Commercial	\$ 43,104	\$ 37,416	\$ 42,888	\$ 43,104	\$ 43,920
<b>Total Projected Annual Revenue</b>	<b>\$ 820,704</b>	<b>\$ 306,768</b>	<b>\$ 411,240</b>	<b>\$ 429,024</b>	<b>\$ 821,520</b>
<b>Differential from Base Case</b>					
% Differential		-37.4%	-50.1%	-52.3%	100.1%
5					
<b>Parking Spaces</b>					
	0	0	0	0	0
<b>Historic Structure (Sq.Ft.):</b>					
	895				

4.2018



Exhibit 10



The base case and alternative plans were developed by NDDInc., including architectural plans and projected costs of development. The report was completed by members of the Xpera Group professional staff, including senior cost estimator James Weber and financial analyst Neal Singer. Mr. Weber reviewed the costs prepared by NDDInc.

John Hansen House Moving provided the cost of moving the historic house. The cost of preparing the new site for the historic home was prepared by NDDinc. The cost of the land for the new site was based on comparable sales in the area.

Preparing the site for the relocated building includes a new slab, utilities hook-ups, grading and (possibly) fencing.



## Conclusions of Economic Alternatives

The four key elements to determine the viability of the base case and alternatives are the rents, expenses, net operating income and development costs. The net operating costs lead to a determination of the value based on a capitalization rate.

### Rents, Expenses & Net Operating Income

#### ➤ Rents for the Base Case and Three Alternatives

The market rate rent levels of the units were determined by the developer:

<b>Market Rate Rents</b> <b>(2018 Basis)</b> <b>Base Case &amp; 4 Alternatives</b> <b>454 13th Street (Ezabelle)</b> <b>Downtown San Diego</b>									
Unit Type	Avg.	Base & Alt 4		Alt 1		Alt 2		Alt 3	
		Units	Annual Revenue	Units	Annual Revenue	Units	Annual Revenue	Units	Annual Revenue
Base & Alt. 4	\$ 1,350	48	\$ 777,600						
Alt 1	\$ 1,247			18	\$ 269,352				
Alt 2	\$ 1,279					24	\$ 368,352		
Alt 3	\$ 1,340							24	\$ 385,920
<b>Total/Avg.</b>		<b>48</b>	<b>777,600</b>	<b>18</b>	<b>269,352</b>	<b>24</b>	<b>368,352</b>	<b>24</b>	<b>385,920</b>

4.2018



Exhibit 11

#### ➤ Operating Expenses

We segmented operating expenses into two categories: fixed and variable. The expenses shown below are at the stabilized level.



- **Fixed expenses** are those that will not vary for the base case and alternatives. They include salaries for the management, maintenance and other salaries and burden. They do vary significantly on a per unit basis.
- **Variable expenses** relate to the number of units. Typically, that category would include insurance, costs of turnover, repairs and maintenance and common area utilities.
- **Property taxes** are calculated at 1.1% of the total costs of the project including land. The actual tax amount will be determined by the County Assessor upon completion of the project. The taxes will change based on the number of units.

Projected Annual Operating Expenses 1st Year of Complete Operation 454 13th Street (Ezabelle) Downtown San Diego						
	%	Base Case & Alt. 4	Alt. 1	Alt. 2	Alt. 3	Alt. 4
Residential Units		48	18	24	24	48
Commercial Units		2	2	2	2	2
<b>Total</b>		<b>50</b>	<b>20</b>	<b>26</b>	<b>26</b>	<b>50</b>
Unit Count Differentia from Best Case Scenario		100.0%	37.5%	50.0%	50.0%	100.0%
<b>Fixed</b>						
Salaries	25.00%	71,811	\$26,842	\$38,669	\$40,354	\$71,883
Landscape	2.50%	7,181	\$2,684	\$3,867	\$4,035	\$7,188
Safety/Security	2.50%	7,181	\$2,684	\$3,867	\$4,035	\$7,188
Advertising/Promotion	3.00%	8,617	\$3,221	\$4,640	\$4,842	\$8,626
Property Taxes	32.50%	93,355	\$34,895	\$50,270	\$52,460	\$93,448
<b>Total Fixed</b>	<b>65.50%</b>	<b>\$ 188,145</b>	<b>\$ 70,327</b>	<b>\$101,314</b>	<b>\$105,726</b>	<b>\$188,333</b>
<b>Per Unit (Res'l &amp; Com'l)</b>		<b>\$ 3,763</b>	<b>\$ 3,516</b>	<b>\$ 3,897</b>	<b>\$ 4,066</b>	<b>\$ 3,767</b>
<b>Variable</b>						
Management Fees	10.00%	\$28,725	\$10,737	\$15,468	\$16,141	\$28,753
Repair & Maintenance	10.00%	\$28,725	\$10,737	\$15,468	\$16,141	\$28,753
Utilities	4.50%	\$12,926	\$4,832	\$6,960	\$7,264	\$12,939
Insurance	10.00%	\$28,725	\$10,737	\$15,468	\$16,141	\$28,753
<b>Total Variable</b>	<b>34.50%</b>	<b>\$99,100</b>	<b>\$37,042</b>	<b>\$53,364</b>	<b>\$55,688</b>	<b>\$99,199</b>
<b>Per Unit - Res'l &amp; Com'l</b>		<b>\$ 1,982</b>	<b>\$ 1,852</b>	<b>\$ 2,052</b>	<b>\$ 2,142</b>	<b>\$ 1,984</b>
<b>Total Project</b>	<b>100.00%</b>	<b>\$ 287,245</b>	<b>\$107,369</b>	<b>\$154,678</b>	<b>\$161,414</b>	<b>\$287,532</b>
<b>Total Per Unit - Res'l &amp; Com'l</b>		<b>\$ 5,745</b>	<b>\$ 5,368</b>	<b>\$ 5,949</b>	<b>\$ 6,208</b>	<b>\$ 5,751</b>
<b>Expenses as % of Gross Revenue</b>		<b>35%</b>	<b>35%</b>	<b>38%</b>	<b>38%</b>	<b>35%</b>

Source: NDDInc.  
4.2018







### ➤ Net Operating Income

The net operating income is computed by subtracting operating expenses from revenue. It is the net operating income that is utilized to calculate the project value, using the capitalization method.

The rents and expenses are calculated for the first full year of operations and which point the building will be completed, rented up and have a stabilized income.

<b>Stabilized Net Operating Income (Projected)</b> Base and Alternative Options 454 13th Street (Ezabelle) Downtown San Diego					
Alternatives:	Base Case	Alt. 1	Alt. 2	Alt. 3	Alt. 4
<b>Residential Units to be Built:</b>	48	18	24	24	48
<b>Commercial Units</b>	2	2	2	2	2
<b>Total Units</b>	50	20	26	26	50
Gross Revenues (Projected)	\$ 820,704	\$ 306,768	\$ 411,240	\$ 429,024	\$ 821,520
Vacancy & Collection %	5.0%	5.0%	5.0%	5.0%	5.0%
\$ Vacancy & Collection	\$ 41,035	\$ 15,338	\$ 20,562	\$ 21,451	\$ 41,076
Net Revenues	\$ 779,669	\$ 291,430	\$ 390,678	\$ 407,573	\$ 780,444
Operating Expenses	\$ 287,245	\$ 107,369	\$ 154,678	\$ 161,414	\$ 287,245
<b>Net Operating Income</b>	<b>\$ 492,424</b>	<b>\$ 184,061</b>	<b>\$ 236,000</b>	<b>\$ 246,158</b>	<b>\$ 493,199</b>

4.2018



Exhibit 13

### ➤ Capitalized Value

A capitalization rate is the rate that the investor marketplace will most often use to determine the value of an investor-grade project. It is the value that relates to the market's determination of the quality of the project, its location and operational history. It is what an investor would expect by way of return on an all-cash basis.

In San Diego today, capitalization rates on investor-grade apartment projects typically range from 4.0% to 6.0% depending on the factors noted above.

We capitalized the project net income from operations at 5.0% because of its highly centralized location in downtown San Diego and quality of architecture.



The capitalized value is greatest with the base case and Alternative 4, with lesser amounts for alternative 1, 2 and 3 because they have fewer units and a higher expense per unit. (Exhibit 14)

<b>Capitalized Value (Year 1: Stabilized Income)</b> <b>454 13th Street (Ezabelle)</b> <b>Downtown San Diego</b>					
Alternatives:	Base Case	Alt. 1	Alt. 2	Alt. 3	Alt. 4
Residential Units to be Built:	48	18	24	24	48
Net Operating Income	\$ 492,424	\$ 184,061	\$ 236,000	\$ 246,158	\$ 493,199
Capitalization Rate	5.0%	5.0%	5.0%	5.0%	5.0%
Capitalized Value of Property	\$9,848,476	\$3,681,216	\$4,720,008	\$4,923,168	\$9,863,980

4.2018




Exhibit 14



## Development Costs

The development costs prepared for this study have four components:

- Cost of Land
- Hard and soft costs of construction;
- Building permits and fees;
- Loan interest; and
- The costs associated with relocation and rehabilitation of the historic building (Alternative 4).

The land cost for the subject property is \$850,000.

<b>Estimated Development Costs 7-Story Concrete Structure 454 13th Street Downtown San Diego</b>					
Alternative	Base	1	2	3	4
<b>Total Sq.Ft.</b>	<b>16,148</b>	<b>6,293</b>	<b>8,603</b>	<b>8,756</b>	<b>16,278</b>
Soft Costs	\$1,808,374	\$842,808	\$954,906	\$1,028,371	\$1,808,374
Hard Costs	\$5,839,560	\$3,152,760	\$3,777,360	\$3,820,560	\$5,839,560
Total	\$ 7,647,934	\$ 3,995,568	\$ 4,732,266	\$ 4,848,931	\$ 7,647,934
Land Costs	\$ 850,000	\$ 850,000	\$ 850,000	\$ 850,000	\$ 850,000
Total Development Costs	\$ 8,497,934	\$ 4,845,568	\$ 5,582,266	\$ 5,698,931	\$ 8,497,934
Cost Per Sq.Ft.	\$ 526	\$ 770	\$ 649	\$ 651	\$ 522



Exhibit 15



Alternative 4 includes the relocation of the historic structure at 454 13<sup>th</sup> Street to a location in Mission Hills. The cost of the relocation is estimated at \$760,000, as shown on Exhibit 16.

Cost of Construction/Moving To Lot in Mission Hills Historic Structure 454 13th Street (Ezabelle) Downtown San Diego	
Sq.Ft.	
Category	Base Case
Site Work Preparation	\$ 35,000
Relocation	\$ 125,000
Historic Rehab	\$ 250,000
Lot Cost	\$ 350,000
<b>Total Structure</b>	<b>\$ 760,000</b>

4.2018

Exhibit 16

There are relatively few lots available for acquisition in the greater Mission Hills area where the developer has designated a lot that could accommodate the historic house. The four lots shown in the exhibit below range in price from \$295,000 to \$520,000. We have used a conservative value of \$350,000.

The comparable lots are shown in Exhibit 17:



**Comparable Properties**  
**Residential Land/Lot Valuation**  
**Mission Hills and Vicinity**  
**City of San Diego**  
**as of April 2018**

APN	Address	ZIP	Lot Size (Sq.Ft.)	Date of Sale	List/Selling Price	\$/Sq.Ft.	Topography	Suitability
443-200-08-00	4285 Randolph St.	92103	15,586	2/2018	\$ 480,000	\$ 30.80	Level	Appropriate
				Sale Pending				
444-272-09-00	Goldfinch St.	92103	6,134	5/2017	\$ 295,000	\$ 48.09	Sloping	Appropriate
436-422-03-00	Lauretta St.	92110	4,994	11/2017	\$ 475,000	\$ 95.11	Sloping	Appropriate
430-382-15-00	Milton Street	92110	4,518	5/2017	\$ 520,000	\$ 115.10	Level	Appropriate

Source: Courthouse Retrieval System  
4.2018

**Exhibit 17**

On Exhibit 18 the total costs of development are shown for the base case and the four alternatives.

**Estimated Total Costs of Development**  
**Base Case and Alternatives**  
**454 13th Street (Ezabelle)**  
**Downtown San Diego**

Category	Units (2)	Land	Building Construction (1)	Soft Costs and Building Permits	Historic Structure Relocation	Interest on Loan	Total	Cost Per Unit
Base Case	50	\$850,000	\$ 5,839,560	\$ 1,808,374		\$ 356,913	<b>\$ 8,854,847</b>	<b>\$177,097</b>
Alt. 1	20	\$850,000	\$ 3,152,760	\$ 842,808		\$ 203,514	<b>\$ 5,049,082</b>	<b>\$252,454</b>
Alt. 2	26	\$850,000	\$ 3,777,360	\$ 954,906		\$ 234,455	<b>\$ 5,816,721</b>	<b>\$223,720</b>
Alt. 3	26	\$850,000	\$ 3,820,560	\$ 1,028,371		\$ 239,355	<b>\$ 5,938,286</b>	<b>\$228,396</b>
Alt. 4	50	\$850,000	\$ 5,839,560	\$ 1,808,374	\$ 760,000	\$ 356,913	<b>\$ 9,614,847</b>	<b>\$192,297</b>

(1) excludes cost of relocating historic structure  
(2) includes residential & commercial



4.2018

**Exhibit 18**





## Final Determination of Economic Value for the Base Case and Alternatives

In the exhibit below, we calculate the differential between the capitalized value of the project and the development costs of the base case and the four alternatives (Exhibit 19):

Summary: Differentials in Value and Net Operating Income Base Case and 4 Alternatives 454 13th Street (Ezabelle)					
Alternative	Base	Alt. 1	Alt. 2	Alt. 3	4
Summary:	Move historic structure to front of property line & build new apartments over it.	Historic structure in its exact current location. The new development is limited to the area directly to the rear of the historic structure.	Move historic structure to front property line. in its exact current location. The new development is limited to the area directly to the rear of the Historic Structure.	Move historic structure to front property line. Cantilever new structure over historic structure.	Move historic structure to new site and build new apts.
No. Residential Units	48	18	24	24	48
% of Base Recommendations		37.5%	50.0%	50.0%	100.0%
Capitalized Value Calculation					
1st full year of operation	\$492,424	\$184,061	\$236,000	\$246,158	\$493,199
Capitalization Rate	5.0%	5.0%	5.0%	5.0%	5.0%
Capitalized Value	\$ 9,848,476	\$ 3,681,216	\$ 4,720,008	\$ 4,923,168	\$ 9,863,980
Decrease in Project Value		\$ 6,167,260	\$ 5,128,468	\$ 4,925,308	\$ (15,504)
% Change in Property Value		-62.6%	-52.1%	-50.0%	0.2%
Loss in Net Operating Income					
Net Operating Income	\$ 492,424	\$ 184,061	\$ 236,000	\$ 246,158	\$ 493,199
Differential		\$ 308,363	\$ 256,423	\$ 246,265	\$ (775)
% Differential		-62.6%	-52.1%	-50.0%	0.2%
Capitalized Value	\$ 9,848,476	\$ 3,681,216	\$ 4,720,008	\$ 4,923,168	\$ 9,863,980
Development Cost	\$ 8,854,847	\$ 5,049,082	\$ 5,816,721	\$ 5,938,286	\$ 9,614,847
Differential	\$ 993,629	\$ (1,367,866)	\$ (1,096,713)	\$ (1,015,118)	\$ 249,133
Capitalized Value Per Unit	\$ 205,177	\$ 204,512	\$ 196,667	\$ 205,132	\$ 205,500
Development Cost Per Unit	\$ 184,476	\$ 280,505	\$ 242,363	\$ 247,429	\$ 200,309
Economic Preference Rating	1	5	4	3	2

4.2018





**Alternative 1** results in a reduction of project value of 62.6% with a similar net operating loss. The development cost would be \$68,393 per unit more than the capitalized value. The significant incremental costs make this an economically infeasible option.

**Alternative 2** moves the historic structure to the front property line and builds the new structure to the rear of the structure. In the process, the number of residential units is reduced to 24, a decline of 50%. Furthermore, the development cost per unit is \$42,181 per unit more than the capitalized value. The significant incremental costs make this an economically infeasible option.

**Alternative 3** moves the existing historic structure to the front property line with the new structure cantilevered over it. This alternative also results in a 50% loss of units and a development cost per unit \$39,043 per unit more than the project's capitalized value. The significant incremental costs make this an economically infeasible option.

**Alternative 4** relocates the historic structure to a new location and builds the base case structure. This alternative is economically feasible; it is however economically inferior to the Base project.

Thus, we rank Base Case and Alternative 4 as the only economically feasible alternatives. Alternatives 1, 2 and 3 are all economically infeasible.

Therefore, it is our professional opinion that the optimal and most economically feasible project is the Base Case, with Alternative 4 a close second.

We stand ready to respond to your questions and comments.

Sincerely yours,

A handwritten signature in black ink, appearing to read 'Alan N. Nevin'.

Alan N. Nevin  
Director of Market Research  
Xpera Group



## ADDENDUM: Economic Feasibility Study for Parking

The goal of the following is to provide analysis of the economic impact due to the addition of parking for the proposed base development.

Current parking requirements indicate the need to accommodate 12 parking spaces. To comply with this requirement the project will need to include 2 subterranean basement levels. The costs to consider are excavation, grading, shoring, build out of 2 subterranean levels and an elevator lift plus the extension of both exit stair elements to access both parking levels.

Below are the respective costs of each and economic impact of the parking requirement (Exhibit 20):

Economic Infeasibility of Parking Ezabelle Project			
2 Subterranean levels below grade	Area	Cost Per sq.ft.	
2 Basement Levels	8400	100	\$840,000
Grading & Excavation			\$400,000
Shoring			\$750,000
Elevator Lift			\$250,000
<b>Total Parking Costs</b>			<b>\$2,240,000</b>

Current Project Costs	Total Project Cost Including Parking	Capitalized Value of Project	Value Created
\$8,497,934	\$10,737,934	\$9,848,448	-\$889,486

Exhibit 20

The economic consequences of providing parking on this small (4,200 sq. ft.) lot would be detrimental to this project in a significant way. The proposed cost of parking would take the total cost of the project to \$10,737,934, with a capitalized value of only \$9,848,448. It is our opinion that this requirement would deem the project economically infeasible as demonstrated by a significant negative value created.

Alan N. Nevin  
Director of Market Research  
Xpera Group



### **Disclaimer**

Although the results, conclusions and recommendations contained within this consultant's report are based upon a thorough review and analysis of current competitive market conditions and the expertise of the author, Consultant does not in any way represent, warrant or guarantee that any reported results will be achieved as a result of various reasons, including but not limited to the sensitivity to ever-fluctuating market conditions and the efficiency of a Client and its representatives, agent, employees, successors and assigns.






Figure 7-5

# 2050 Revenue Constrained Transit Network Based on SANDAG San Diego Forward: The Regional Plan



-  Project Site
-  Cycle Tracks
-  Parking Garages
-  Trolley Station

**Transit Network**

-  Commuter Rail
-  Light Rail
-  Rapid Bus
-  Streetcar
-  Local Bus



# SAN DIEGO DOWNTOWN DESIGN GUIDELINES

## 4.5.10

### Blank Walls

#### Guidelines

- **4.5.10.A** Blank walls on the ground level or on façades of buildings are to be limited to provide a pleasant and rich pedestrian experience. Blank walls include any street wall area that is not transparent, including solid doors and mechanical areas.
- **4.5.10.B** Unavoidable blank walls along public streets or those viewed from public streets, open spaces and thoroughfares should be treated to create an inviting visual experience. All blank wall area should be enhanced with architectural detailing, material texture, ornamentation, landscape treatment and/or artwork.



*Unavoidable blank walls viewed from public streets should be enhanced with architectural detailing, material texture, and other devices. Above, San Diego, CA.*



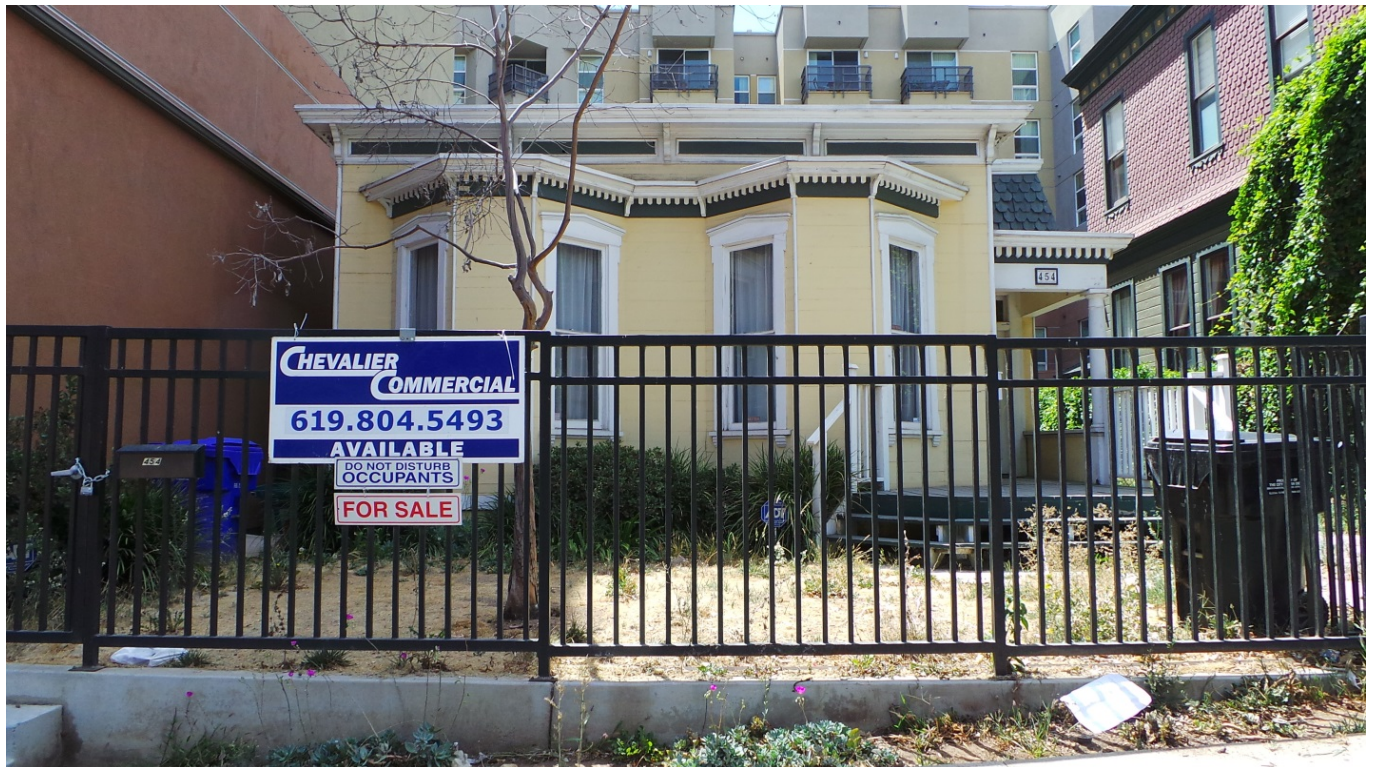
*Blank walls at street-level should be treated through use of rich and textured materials, color, and landscape materials. Top, Portland, OR; bottom, San Diego, CA.*







## **454 13<sup>th</sup> Street Residential Project**



**Prepared by:**

**Marie Burke Lia, Attorney at Law, on behalf of the Project Applicants**

**April 21, 2018**

## **FINDINGS**

### **San Diego Municipal Code**

#### **Article 6: Development Permits**

#### **Division 5: Site Development Permits**

##### **Introduction:**

The proposed development includes the redevelopment of a 4,218 square foot parcel on the west side of 13th Street, between Island and J Streets in the East Village area of the Centre City Planned District. The original structure on this parcel was the John and Mary Wright House, constructed in 1881, which consisted of 864 square feet and was described as a 5-room cottage. In 1942, a 486-square foot addition was added to the rear this cottage. On November 16, 2017, the original 864 square foot cottage was designated a San Diego Historical Landmark #1278. The 1942 addition was excluded from that designation. The proposed redevelopment of this site will incorporate the historical resource into the new development on the site.

The existing 4,218 square foot parcel will be redeveloped in accordance with the requirements of the Centre City Planned District Ordinance and all other applicable sections of the San Diego Municipal Code. The proposed development will contain 25,853 square feet of residential rental use in 48 units on 7 stories, including 12% very low income affordable units, with no off-street parking spaces. The proposed development is consistent with the Centre City Planned District, which is subject to the Downtown Community Plan.

#### **§126.0504 Findings for Site Development Permit Approval**

##### **(a) Findings for all Site Development Permits**

##### **(1) The proposed development will not adversely affect the applicable land use plan**

Land Use and housing issues are addressed in Chapter 3 of the Downtown Community Plan. As shown in the Plan's Land Use Map, Figure 3-4, attached as *Exhibit 1*, the development on this block is designated for Residential Emphasis. Residential Emphasis areas will accommodate primarily residential development. Small-scale businesses, offices, services and ground floor commercial uses are also allowed provided they do not exceed 20% of the overall building area.<sup>1</sup>

Figure 3-4 of the Downtown Community Plan, attached as *Exhibit 2*, shows the allowable minimum and maximum FARs (Floor Area Ratios) for various sites. The incorporation of the

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<sup>1</sup> Chapter 3, page 3-12.

historic resource into the proposed new development will allow the development of the subject 13<sup>th</sup> Street property in accordance with the minimum 3.5 FAR and the maximum 6.0 FAR for the site as established by this Plan. The maximum 6.0 FAR can be increased by an Affordable Housing Density Bonus. The proposed project will be entitled to a 6.2 FAR because of this Bonus. The proposed project will, therefore, comply with Chapter 3 of the Downtown Community Plan and will be consistent with the Residential Emphasis area.

Historic Conservation is addressed in Chapter 9 of the Downtown Community Plan. The existing 1881 cottage on the project site is a locally designated historical resource, HRB #1278. As indicated in Table 9-1 of the Plan, attached as *Exhibit 3*, San Diego Register listed properties should, whenever possible, be retained on site. “Partial retention, relocation or demolition of a resource shall only be permitted through applicable City procedures.”<sup>2</sup>

The applicable City procedures are established in San Diego Municipal Code Chapter 14, Article 3, Division 2, entitled “Historical Resources Regulations.” Municipal Code §143.0210 (2) (C) requires a Site Development Permit in accordance with Process Four for any development that proposes to deviate from the development regulations for historical resources described in this division. Substantial alteration of a designated resource by relocation or other means is a deviation from the historical resources regulations and, therefore, a Site Development Permit, as authorized by Chapter 12, Article 6, Division 5, entitled “Site Development Permit Procedures,” is required. The Planning Commission as decision maker must make all of the Findings in §126.0505(a) and §126.0505(i) before the substantial alteration or incorporation of a locally designated resource can occur.

Municipal Code §126.0505(a) contains the three basic Findings required for all Site Development Permits, which are: (1) The proposed development will not adversely affect the applicable land use plan; (2) the proposed development will not be detrimental to the public health, safety and welfare; and (3) the proposed development will comply with the regulations of the Land Development Code. As discussed above, the proposed development will comply with the provisions of Municipal Code §126.0505(a).

Municipal Code §126.0505(i) contains the three supplemental Findings required for all Site Development Permits for Substantial Alteration, which are: (1) There are no feasible measures, including a less environmentally damaging alternative, that can further minimize the potential adverse effects on the historical resource; (2) the deviation is the minimum necessary to afford relief and accommodate the development and all feasible measures to mitigate for the loss of any portion of the historical resource have been provided by the applicant, and (3) the denial of the proposed development would result in economic hardship to the owner. For purposes of

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<sup>2</sup> Chapter 9, page 9-3.

this finding, “economic hardship” means there is no reasonable beneficial use of the property and it is not feasible to derive a reasonable economic return from the property.

The Mitigation Monitoring and Reporting Program (MMRP) for the Downtown Community Plan requires the implementation of Mitigation Measure HIST – A. 1-2-1 if a locally designated historical resource would be partially retained on site, relocated or demolished. That Mitigation Measure requires the submission of a Documentation Program with Photo Documentation and Measured Drawings of the resource to the Historical Resources Board Staff for review and approval.

Mitigation Measure HIST – A. 1-2 requires the development and submission of a Treatment Plan to ensure the protection of the resource during its incorporation and subsequent rehabilitation on the site to the Historical Resources Board Staff for review and approval. This Mitigation Measure also requires that a Monitoring Plan, developed and conducted by the Qualified Historical Monitor during the incorporation and the post-incorporation rehabilitation of the resource on the site, be submitted to the Historical Resources Board Staff for review and approval. And, lastly, the final Draft Monitoring Report must be submitted for review and approval by the Historical Resources Board Staff at the completion of the project, after which a Certificate of Occupancy may be issued for the incorporated resource. The implementation of these Mitigation Measures will be required as a Condition of this Site Development Permit.

Therefore, the processing of this Site Development Permit is in compliance with and will not adversely affect this aspect of the applicable land use plan.

(2) The proposed development will not be detrimental to the public health, safety, and welfare.

The proposed development will consist of a seven-story residential and commercial rental building on the west side of 13<sup>th</sup> Street between Island and J Streets. The project will consist of 48 residential rental units, including 12% very low income affordable units, 1 large ground floor commercial space, and no off-street parking spaces. The project site is small, only 4,218 square feet, and the proposed development provides the largest number of units possible at this site.

The proposed development complies with the San Diego Municipal Code and Uniform Building Code provisions intended to ensure the proposed development will not be detrimental to the public health, safety, and welfare.

(3) The proposed development will comply with the applicable regulations of the Land Development Code.

The proposed project will consist of a transit and pedestrian oriented, high density, mid-rise, mixed income residential rental apartment and commercial development within a Residential Emphasis Area as called for in the Downtown Community Plan.

The proposed development will comply with the applicable provisions of the Centre City Planned District Ordinance in the following manner. It is located within the Residential Emphasis Area of the Plan that specifically calls for this type of property use. The development will comply with the Plan's FAR regulations in that its FAR of 6.2 is within the minimum FAR of 3.5 and the maximum FAR of 6.0 when the Affordable Housing Density Bonus is applied to the calculations.

The development complies with the Centre City Planned District Ordinance's Development Regulations pertaining to lot size, minimum building set-backs, building heights and residential development regulations with some minor deviations such as the FAR. It will comply with the PDO's Urban Design Regulations pertaining to building orientation, façade articulation, street level design, pedestrian entrances, transparency, blank walls, exterior projecting balconies, rooftops, encroachments into public rights-of-way, regulations pertaining to historical resources requiring a Site Development Permit, additional standards for main streets, and off-street parking.

The relevant Land Development Code's Planning and Development Regulations for topics not addressed in the Centre City Planned District Ordinance are contained in that Code's Chapter 14 and include: Grading Regulations, Drainage Regulations, Landscape Regulations, Parking Regulations, Refuse and Recyclable Materials Storage, Mechanical and Utility Equipment Storage Regulations, Building Regulations, Electrical Regulations, and Plumbing Regulations. The project will comply with these regulations, as will be required by the building permit to be issued for this project. Therefore, the proposed development will comply with all applicable regulations of the Land Development Code.

**§126.0505 (i) Supplemental Findings – Historical Resources Deviation for Substantial Alteration of a Historical Resource**

1. There are no feasible measures, including retaining the resource on the site, that can further minimize the potential adverse effects on the historical resource;

The existing one-story house on this site was originally constructed of wood as an 864-square foot single family residence in 1881 and was enlarged with a 486-square foot addition in 1942. The addition was excluded from the local historical designation action of November 16, 2017. It is assumed that this 864-square foot house will require some repair and rehabilitation, which is typical of buildings of similar vintage. In addition, the building is expected to require electrical



and plumbing upgrades. The building occupies the 4,218 square foot lot at the on the west side of 13<sup>th</sup> Street between Island and J Streets in the East Village area of Centre City.

Base Development Proposal - It has been determined that it would be feasible to retain the resource on the site, although this course of action would incorporate this small one story building into the proposed new seven story building that will be constructed on this site. While this course of action is a less environmentally damaging alternative than other options, it would still require a Site Development Permit for the Substantial Alteration of the resource because it will result in an actual “substantial alteration” of the resource, which will be used for a commercial space on the ground floor of the new development. This option moves the historic resource forward to the front property line and incorporates the resource completely into the new development. The second floor and above wall planes will be setback approximately 3 feet from the outermost face of the historic building. The historic resource will be incorporated into the new development as a ground floor commercial space.

This option is the preferred Base Development Proposal and it is the only economically feasible option for retaining the historic resource on this site. A graphic illustration of this Base Development Proposal is attached as *Exhibit 4, Sheet ST-1*.

Alternative I would retain the resource in its exact current location and limit the proposed new development on the site to the open land area at the rear of the lot. This Alternative development would have minimal habitable areas given the need for Life Safety Circulation. The limited size of the habitable areas would have an economic effect on the project, as will be discussed below. A graphic illustration of Alternative I is also attached as *Exhibit 4, Sheet ST-1*.

Alternative II would move the resource forward to the front property line. The new development would be limited to the open land area at the rear of the lot. The limited size of the habitable areas would have an economic effect on the project, as will be discussed below. A graphic illustration of Alternative II is attached as *Exhibit 5, Sheet ST-2*.

Alternative III would move the resource forward to the front property line. The new development would be stepped back at the 2<sup>nd</sup> level 10 feet and levels 3 through 7 would cantilever over the historic resource. The biggest issue with this Alternative is the infeasibility of the building structure given the large cantilever. Stepping back the second level 10 feet will create an irregular structure. The irregularity of the structure may not meet lateral design requirements of the California Building Code. In addition the cantilever will require giant cantilevered concrete beams to be incorporated into the design which will encroach into the corridors due to the large depths. This would cause the loss of 20 units. A graphic illustration of Alternative III is also attached as *Exhibit 5, Sheet ST-3*.

The limited size of the habitable areas would have an economic effect on the project, as will be discussed below.

Alternative IV would relocate the historic resource from this site to the site of a new single family residence in the Mission Hills neighborhood of the City to serve as a guest house for that new residence, which is located on a wooded hillside overlooking the Presidio Park area of the city. That Alternative was the Base Development Proposal until local preservationists requested that the current Base Development Proposal be considered. Alternative IV would permit the development of the subject property as initially designed, as a seven story apartment complex. This Alternative is the second best option for this development although there will additional costs associated with the relocation of the resource. A graphic illustration of Alternative IV is also attached as *Exhibit 6, Sheet ST-3*.

2. The deviation is the minimum necessary to afford relief and accommodate the development and all feasible measures to mitigate the loss of any portion of the historical resource have been provided by the applicant, and

The historic resource is an 864 square foot wood frame cottage that was constructed on this site in 1881 and altered thereafter by a 486 square foot addition. The cottage was designated as a local historical resource under Criterion C for embodying the distinctive characteristics of a style, type, period or method of 1880s Italianate construction. The designation excluded the 1942 rear addition. The south, east and north façades of the house are intact although they have been altered somewhat, the west façade was altered by the rear addition which was found not to be a part of the historic resource. The three remaining facades will be incorporated into the new development and they will continue to serve as exterior façades as illustrated by Sheets A4.0, A4.1, A4.4 and A5.0 of the new development plans, attached as *Exhibit 7* to these Findings. The Mitigation, Monitoring and Reporting Program, discussed above, will ensure that the surviving exterior facades, doors, openings and trim will be rehabilitated in accordance with the Secretary of the Interior's Standards for Rehabilitation. The supervision of their rehabilitation by the Historical Resources Board Staff and Civic San Diego Staff will ensure that the MMRP is complied with and that the required documentation of this property is archived with the appropriate City of San Diego departments and the San Diego History Center. The planned incorporation of this resource into a new development on the original site will enable it to survive for another one hundred years close to where it was constructed in 1881.

3. The denial of the proposed development would result in economic hardship to the owner. For purposes of this finding, "economic hardship" means there is no reasonable beneficial use of a property and it is not feasible to derive a reasonable economic return for the property.

As discussed above, land use and housing issues are addressed in Chapter 3 of the Downtown Community Plan. The block containing the subject property is designated for residential emphasis under that Plan and the minimum and maximum floor area ratios (FARs), which determine the amount of development in the area, are established by that Plan. The minimum FAR for the subject property is 3.5 FAR and maximum FAR is 6.0. The maximum figure can be increased by bonus programs, such as those to encourage affordable housing, which, in this case will increase the FAR for this site to 6.2, the amount of the proposed Base Development Project.

Historic Conservation is addressed in Chapter 9 of the Downtown Community Plan, which, with reference to local designated historical resources, requires that “Partial retention, relocation of demolition of a resource shall only be permitted through applicable City procedures.” The partial retention and incorporation of the John and Mary Wright House, HRB #1278, can only be permitted through the application of the “Site Development Procedures” in Chapter 12, Article 6, Division 5 of the Municipal Code.

The proposed Site Development Permit for Substantial Alteration of the John and Mary Wright House, by its incorporation into the new seven story residential and commercial development on the subject property, can only be permitted if the “denial of the proposed development would result in economic hardship to the owner. For purposes of this finding, ‘economic hardship’ means there is no reasonable beneficial use of a property and it is not feasible to derive a reasonable economic return for the property.”

In order to make these economic hardship findings, an Independent Economic Feasibility analysis must be conducted that compares the Base Development Proposal, which would incorporate the surviving elements of the 1881 cottage into the new residential and commercial rental building proposed for the site, with various project Alternatives that would incorporate and retain the cottage on the site in different configurations and one project Alternative that would relocate the cottage to another location for rehabilitation.

As discussed above, the Base Development Proposal would move the cottage to the front of the lot and incorporate it into the new development at the ground floor.

The Economic Feasibility Analysis of this project and the Alternatives has been conducted by Alan Nevin of the Xpera Group for the property owner. Mr. Nevin has had extensive experience in conducting such analyses in and for the City of San Diego. A copy of this Analysis will be submitted with these Findings.

In the Xpera Report’s “Final Determination of Economic Value for the Base Case Alternatives,” the following information is presented on page 21 of 23.

The Base Development Project will retain the historic resource and build 48 new apartments. The Capitalized Value of the project will be \$9,848,476. The annual net operating income will be \$492,424. The Capitalized Value per unit will be \$205,177 and the Development Cost per unit will be \$184,476 per unit. The Economic Preference Rating for this project is #1.

Alternative 1 will build 18 new apartments. Its Capitalized Value will be \$3,681,216. Its Annual Net Operating Income will be \$184,061. The Capitalized Value per unit will be \$204,512 and its Development Cost per unit will be \$280,505. The Economic Preference Rating for this project is #5.

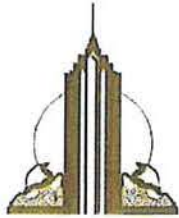
Alternative 2 will build 24 new apartments. Its Capitalized Value will be \$4,720,008. Its Annual Net Operating Income will be \$236,000. The Capitalized Value per unit will be \$196,667 and its Development Cost per unit will be \$242,363. The Economic Preference Rating for this project is #4.

Alternative 3 will build 24 new apartments. Its Capitalized Value will be \$4,923,168. Its Annual Net Operating Income will be \$246,158. The Capitalized Value per unit will be \$205,132 and its Development Cost per unit will be \$247,429. The Economic Preference Rating for this project is #3.

Alternative 4 will build 48 units. Its Capitalized Value will be \$9,863,980. Its Annual Net Operating Income will be \$493,199. The Capitalized Value per unit will be \$205,500 and its Development Cost per unit will be \$200,309. The Economic Preference Rating for this project is #2

When compared with Base Development Proposal, Alternative 1 will result in a 62.6% reduction in value, Alternative 2 will result in a 52.1% reduction in value, Alternative 3 will result in a 50.0% reduction in value and Alternative 4 will result in a 0.2% reduction in value.

The Xpera Report concludes that it is “our professional opinion that the optimal project is the Base Case, with Alternative 4 a close second.”



# PINNACLE

International Development, Inc.

April 13, 2018

RE: EZABELLE PROJECT – 454 13<sup>th</sup> street

To whom it may concern,

I, Dennis La Salle, representative of property owner of address 424 15<sup>th</sup> St., San Diego, CA 92101, would like to express my full support for the proposed Nakhshab Development & Design project in the East Village Neighborhood known as EZABELLE. I have carefully reviewed all design aspects of this project and am pleased to support the design as proposed.

Thank you for including our support in your considerations and anticipate that this is going to be a positive addition to our neighborhood.

Kind Regards,

Dennis La Salle

619-231-7072

[dlasalle@pinnacleinternational.ca](mailto:dlasalle@pinnacleinternational.ca)

# HP INVESTORS

April 16, 2018

RE: EZABELLE PROJECT – 454 13<sup>th</sup> street

To Whom It May Concern,

HPI-ROSARIO, LLC, property owner of 1220-1225 J Street, San Diego, CA would like to express our full support for the proposed Nakhshab Development & Design project in the East Village Neighborhood known as EZABELLE. We have discussed in concept all design aspects of this project and are pleased to support the design as proposed.

Thank you for including our support in your considerations and anticipate that this is going to be a positive addition to our neighborhood. If you have any questions, please feel free to contact me via phone or email.

Kind Regards,



Sumeet Parekh  
Managing Partner

HP Investors  
335 15<sup>th</sup> Street  
San Diego, CA 92101

**RE: EZABELLE PROJECT – 454 13<sup>th</sup> street**

**To whom it may concern,**

**I, David Allen, property owner of address 1492 K Street, San Diego CA 92101, would like to express my full support for the proposed Nakhshab Development & Design project in the East Village Neighborhood known as EZABELLE. I have carefully reviewed all design aspects of this project and am pleased to support the design as proposed.**

**Thank you for including our support in your considerations and anticipate that this is going to be a positive addition to our neighborhood.**

**Kind Regards,**

**David Allen**

**Name**

A handwritten signature in black ink, appearing to read 'DA', followed by a horizontal line.

**Signature**

**509-280-5469**

**Contact Number/Email**



RE: EZABELLE PROJECT – 454 13<sup>th</sup> street

To whom it may concern,

I, Brendan Foote, Managing Member of Fabric 16<sup>th</sup> & K, LLC and property owner of 301 16<sup>th</sup> Street in East Village, would like to express my full support for the proposed Nakhshab Development & Design project in the East Village Neighborhood known as EZABELLE. I have carefully reviewed all design aspects of this project and am pleased to support the design as proposed.

Thank you for including our support in your considerations and anticipate that this is going to be a positive addition to our neighborhood.

Kind Regards,

*Brendan Foote*

Brendan Foote  
[Brendan@fabricinvestments.com](mailto:Brendan@fabricinvestments.com)  
619.840.7721



**Circulate San Diego**  
1111 6th Avenue, Suite 402  
San Diego, CA 92101  
Tel: 619-544-9255  
Fax: 619-531-9255  
[www.circulatesd.org](http://www.circulatesd.org)

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April 24, 2018

Soheil Nakhshab, PE  
Principal, CEO  
Nakhshab Development & Design Inc.  
340 15th Street, Suite 1  
San Diego, CA 92101

**Subject: MOVE Alliance Certification for Ezabelle Project**

Dear Mr. Nakhshab,

On behalf of Circulate San Diego, whose mission is to create excellent mobility choices and vibrant, healthy neighborhoods, we are pleased to award a MOVE Alliance certification for the proposed Ezabelle project, an innovative and sustainable infill transit-oriented development project.

Ezabelle proposes a mixed use building with 48 micro-apartments, a bazaar courtyard that will feature small businesses, and will preserve a historic home built in 1881 on-site. The micro-apartments, which include bicycle racks in each unit, will be built with 12% of the units dedicated to Very Low Income affordable housing. The bazaar will provide a public space for the community on the ground floor courtyard with outdoor benches and greenery. The historic home will be incorporated into the ground floor of the building, providing human-scaled architecture and a unique design to complement the combination of historical and modern architecture on the project's block.

Located in East Village on 13<sup>th</sup> Street between Island Avenue and J Street, Ezabelle is one block from the Park and Market Trolley station, which will provide convenient transit access to the border, Mission Valley, and soon UCSD, along with many other destinations. The project is also located directly adjacent to cycle tracks that are planned for the Park Boulevard and J Street corridors, a major public investment in safe bicycle facilities that will connect residents throughout Downtown and to surrounding communities. While the project has optimal proximity to high quality transit and bicycle facilities, it is walkable to every day amenities like grocery stores and restaurants in addition to cultural venues such as the Downtown library and Petco Park.

For all of the above reasons and more, Ezabelle is the ideal project to be built without on-site parking. This game changing project will provide car-free renters in the market with the choice to live Downtown without having to shoulder the high cost of parking. Currently, the supply of this type of home does not meet the growing demand and Ezabelle can become a positive example for future development. Ezabelle not only provides a vibrant mixed-use infill project that adds to the area's housing stock, but it will also help the City of San Diego reach its Climate Action Plan goals by encouraging walking, bicycling, and taking transit for commutes.

Our certification is based on the current vision for the project as presented to the MOVE Alliance. We understand that changes are to be expected throughout the entitlement process; however if there are

significant modifications to the project which reduce its commitment to sustainable and compact transit-oriented development, we reserve the right to re-evaluate significant changes to the project to maintain our certification.

Congratulations on earning the MOVE Alliance certification for Ezabelle, an innovative development which fosters sustainable modes of transportation.

Please be encouraged to use the MOVE Alliance certification and logo as you move through entitlement and marketing for your project.



Sincerely,

A handwritten signature in blue ink that reads "Colin Parent". The signature is fluid and cursive.

Colin Parent  
Executive Director and General Counsel

*THE MOVE ALLIANCE was formed in 2012 to provide certification for transit-oriented, smart growth projects in the San Diego region. MOVE Alliance members consist of local experts in smart growth planning and sustainable transit oriented development disciplines, including planners, developers, urban designers, and transportation engineers. By recognizing and supporting projects which meet the MOVE Alliance criteria, we can help to create complete communities, one project at a time.*

<http://www.circulatesd.org/move>



May 07, 2018

Reese Jarrett, President  
c/o Brad Richter, Vice President Planning  
Civic San Diego  
401 B St. 4<sup>th</sup> Floor – sent electronically  
San Diego, CA 92101

Re: Nakhshab Development & Design: Ezabelle - 454 13<sup>th</sup> Street

Dear Mr. Jarrett:

East Village Association, Inc. (EVA) represents San Diego's largest downtown neighborhood encompassing 130 blocks between Seventh Avenue and 17<sup>th</sup> Street. An arts and industrial neighborhood in transition, EVA is the non-profit organization that manages the East Village Business Improvement District, which currently represents 700 members and 13,000 residents. EVA's mission is to support and promote neighborhood businesses and residents by establishing the community as San Diego's livable urban village where people and the entrepreneurial spirit thrive through the distinct mix of arts, culture, education, and entertainment.

On May 03, 2018, the EVA Executive Board met for a review of the Ezabelle, 454 13<sup>th</sup> Street project. Typically, EVA meets in coordination with the East Village Residents Group (EVRG) for the voluntary joint Pre-Design Committee review; however due the timing and the Civic Ezabelle project review schedule, Ezabelle presented exclusively in front of EVA. After careful discussion, and consideration of EVRG letters entered into the record, EVA supports this project, with one objection, contingent upon:

- a) Should the project proceed as a "pet-friendly" property, property management needs to find a pet relief area, and maintain a level of cleanliness; and
- b) There is recognition that the project does not offer any parking, and will be the third apartment building on the 13<sup>th</sup> Street footprint with no parking. One suggestion presented, would be that if the project gets rid of the curb cut, 2 spaces could be created for short-term retail parking.

If you have questions, please contact Dora McCann Guerreiro, EVA Executive Director via email at [dora@EastVillageSanDiego.com](mailto:dora@EastVillageSanDiego.com) or call 619.546.5636.

Sincerely,

A handwritten signature in black ink, appearing to read 'D. McCann'.

Dora McCann Guerreiro, Executive Director  
East Village Association, Inc.

May 8th, 2018

Dear CivicSD,

I am writing you this letter to express my concerns regarding the proposed "Ezabelle" Project (File Number: CCDP/SDP 2018-07). As a 6yr home owner in East Village, (Park Boulevard East 1225 Island Ave) my family and I have supported and seen the benefit of the current "boom" in residential real-estate development in East Village. However, we cannot in good conscious support this project.

The first and biggest problem we have is that this would be the **third** apartment building on the same street with zero parking. This is a safety & security issue. Not having any parking for the residents, will demand they park on an overly saturated street of vehicles. This part of East Village is already rampant with crime, and homeless. San Diego cannot in good conscience allow this to proceed. The first two (434 and 435 13<sup>th</sup> street "The Studios") are currently under construction, and are adding 86 new units to the street with zero parking. Ezabelle would add an additional 48 units to the same block again with no parking. If this project is approved it would total **134 residential units on one block with zero parking.** I understand that "The Studios" were already built but vacant, which is why we as residents supported the reconstruction. Yet, I cannot see how the city could approve a **third apartment building with zero parking on the same street.** Please note: the police records for the disturbance calls in that area are high in volume. This isn't providing a safe place for those residents, and the residents of the existing building, like Park Blvd East.

Secondly, this building is going to have a direct adverse effect on the equity of Park Blvd East, and other condo, & independently owned units. The current proposed plans of an additional apartment building doesn't ensure more homeowners in the area, only renters. We've invested our hard earned money in this area for hopes of appreciation, and establishing a good financial decision. Renters do not add that security, especially when so many are focused to be located on such a dense area. Park Boulevard East is a great investment for the owners of those condos, and allows the few renters we have, to be inspired to own their one home one day. I can confidently say that "Ezabelle" would be an unacceptable over-development of a site, which already contains the historic Wright House. The attractiveness of the area, investment potential for families and future homeowners would be gone if this building was approved.

We understand East Village is rapidly growing, and believe that expanding development projects are good for certain parts of the community, but this project is simply too much. They are utilizing the City's Affordable Housing Density Bonus to expedite this project and not require parking, but only **SIX units will be for low-income residents.** We have concluded that only having 6 low income units is

not a justifiable use of the Density bonus, and is a gross misrepresentation of that program. Local businesses & residents rely on the already limited street parking, and adding 48 more units will not help anyone. I urge EVA to not support a third apartment building on one city block with zero parking. There is so much potential to restore the Wright House, and not build simply on top of it.

Respectfully,

Eric & Meghan Atilano

Park Blvd East Residents & Homeowners.

James,

Attached are the three letters I have received already regarding this project. There will be many more to come but we were under the impression it wouldn't be needed to be submitted to you until Friday. After our phone call, I understand you need them by noon on Thursday to be included in your report. I will try to get them to you as soon as I can.

Some highlights:

If this gets approved it will be over 130 units on one block with ZERO parking. While we do understand that there is a trolley station nearby, people in this city still need cars. Especially low income residents. There is already limited parking on our block. Business needs parking as well.

While we understand the EVA approved this project, we were notified last minute of the meeting and were not able to voice our concerns so it was a one-sided conversation.

East Village houses almost 90% of the low income housing already, adding six more units does nothing.

No attention has been paid to the South and West-facing walls both of which face over 40 residential units.

The entrance to Ezabelle is in the back, which greatly concerns the residents of Park Boulevard East for security.

The developer seems to have reached out to everyone in East Village except the people who are most affected, the neighboring residents.

The building does no justice to the original design of the Wright house, and as you mentioned is going to now have one room as a trash receptacle.

If there is an emergency like a fire, is part of their plan to utilize Park Boulevard East? How does the fire department get in from the back without us?

He refers to this as a "Middle-Income" housing but he is receiving the Low Income exceptions for only 6 units.

There is no close rentable street parking, and the closest ones during parking season are over \$250 dollars a month and are not guaranteed to find spots.

There are more that other residents have brought up, but that is all I can think of at this time.

Thanks for taking the time to talk to me today and I will see you tomorrow.



# Letter to CivicSD

May 3rd, 2018

Dear Civic,

I am writing to express my concerns regarding the proposed "Ezabelle" Project (File Number: CCDP/SDP 2018-07). As a resident and home owner in East Village, (Park Boulevard East 1225 Island Ave) my family and I have supported and seen the benefit of the current "boom" in residential real-estate development in East Village. However, we cannot in good conscious support this project.

The first and biggest problem we have is that this would be the third apartment building on the same street with zero parking. The first two (434 and 435 13<sup>th</sup> street "The Studios") are currently under construction, and are adding 86 new units to the street with zero parking. Ezabelle would add an additional 48 units to the same block again with no parking. If this project is approved it would total **134 residential units on one block with zero parking.** I understand that "The Studios" were already built but vacant, which is why we as residents supported the reconstruction. Yet, I cannot see how the city could approve a **third apartment building with zero parking on the same street.**

Secondly, this building is going to have a direct adverse effect on the residential privacy and sunlight access of the residents in Park Boulevard East. The current proposed plans only show the street side view of the building, and does show how much this building would overshadow Park Boulevard East. I can confidently say that "Ezabelle" would be an unacceptable over-development of a site, which already contains the historic Wright House. The minimal sunlight that our gardens do get would be gone if this building was approved. The developer has paid little attention to the South and West facing walls which will directly impact over 50 units in Park Boulevard East.

I understand that East Village is rapidly growing, and believe that expanding development projects are good for the community, but this project is simply too much. They are utilizing the City's Affordable Housing Density Bonus to expedite this project and not require parking, but only **SIX units will be for low-income residents.** Local businesses rely on the already limited street parking, and adding 48 more units will not help anyone. I urge CivicSD to not support a third apartment building on one city block with zero parking. There is so much potential to restore the Wright House, and not build simply on top of it.

Respectfully,

Benjamin and Samantha Bowen

Dear CivrSD,

I am writing you regarding the Ezabelle project on 13th and Island. I am NOT in favor of this project. I am an East Village resident and business owner (Realtor) in East Village. The fact that this project offer absolutely no parking on a street where there already are two buildings with no parking on 13th is pretty upsetting. I get that this project offers low income housing, but only 6 out of the 48 units are low income. It would be nice to think that noone owns a car and would walk the neighborhood, but that is really not realistic. I rarely drive but still need somewhere to park my vehicle.

Also, squeezing a project above and next to two historical projects in SD is upsetting. These buildings have been here for decades and should be celebrated, not built around. I do have a lot to say on this project, (and frankly all of my neighbors do as well) but am in between appointments and need to head out to another one now.

Being a Realtor I am all for building in San Diego, but it should make sense.

I hope this letter and other letters let you know how we as residents feel about this. Thank you for your time and considerations!

**David Spiewak**

REALTOR®

(858) 527.2269

Coldwell Banker Residential Brokerage

## James Alexander

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**From:** Nadia Bruno <nadiasbruno8@gmail.com>  
**Sent:** Wednesday, May 09, 2018 5:56 PM  
**To:** christopherward@sandiego.gov; mbchase@sandiego.gov; belliot@sandiego.gov; fpstark@gmail.com; cce@carrierjohnson.com; Brad Richter; James Alexander  
**Subject:** \*\*\*STOP\*\*\* "Ezabelle" File Number: CCDP/SDP 2018-07

Dear San Diego Local Officials and Residents of this Finest City,

I'm writing as a concerned resident of the Park Boulevard East building located between Park and 13th streets on Island Avenue.

The proposed "Ezabelle" project would greatly constrict the amount of street parking available within the radius of this block. "The Studios" currently under construction at 434 and 435 13th Street 92101 also do not contain designated parking per unit within an 86 unit building. During Padre's games many of the streets are re-directed or closed and this just adds to the congestion.

Another concern is that the project encroaches onto the residents balconies within the already tight space. Once construction is underway, there would be dust, debris and massive amount of noise. We're already living through the many buildings being built and that noise for years, to have it at each level as this is being built upwards is unacceptable. There is also the fact that each resident will lose privacy, any plants or foliage and sunlight on said balconies.

As for the safety of the entrance to the "Ezabelle" building facing our courtyard, is there a plan if there is an emergency like a fire, is part of their plan to utilize Park Boulevard East? How does the fire department get in from the back without access from our building?

I understand that the growth of the East Village is vital to the City of San Diego but this project goes too far. The Wright House and the historical home/law firm on the corner are valuable additions to the character of the neighborhood. It is also my understanding that the developer seems to have reached out to everyone in East Village except the people who will be most affected, Park Boulevard East.

I urge you to not support a third apartment building on one city block with zero parking. There is so much potential to restore the Wright House, and not build simply on top of it.

Respectfully,

Nadia Bruno

May 9, 2018

Re: Proposed Ezabelle Mixed-Use Project  
Development Permit No. 2018-07

Civic San Diego,

The East Village Residents Group (EVRG) represents over thirteen thousand residents who currently live in the East Village District of Downtown San Diego. EVRG's mission is to promote a better quality of life and family environment for every resident in our District.

Concerning the proposed Ezabelle mixed-use project, the EVRG feels that they cannot support this project until the following concerns are addressed:

Considerations of the Design Elements:

Relationship of the new structure to the historic structure is unfortunate. The problem lies in the massing of the new architecture: it sits directly on top of the little yellow historic house. The views shown on pages A5.2, A5.3 and A5.4 of the presentation drawings clearly illustrate this point.

Although the architect stated that the owner of the property to the north will obtain permission to move the Victorian home, that seems unlikely since the structure is a valuable asset to East Village and the downtown area. Therefore, the north façade of the Ezabelle project should be revisited, recognizing its permanent relationship to the beautiful Victorian.

The west façade makes no attempt to acknowledge that this wall will be *the view* for all the neighbors in the building to the west. It is accepted, and not disputed, that the loss of the existing long vistas is not relevant. That said, there is no need to create intentional blight: some effective architectural gesture or articulation should be employed.

Urban Planning Issues:

The city block where the proposed project is to be located will already be heavily impacted by 86 new studio units, with no parking. The cumulative effect of un-parked 134 new market rate units in one block will be significant. The availability of utilizing our parking structure is seasonal and sporadic due to events at Petco Park.

The low cost housing apartment units in the East Village neighborhood presently make up over 34% of all apartments. If affordable housing units are to be incorporated into new market rate developments, two and three bedroom apartments for families should be encouraged.

As frequently happens, individual projects evoke discussions on larger issues. Our hope is that all issues noted will be addressed in this project, and all projects moving forward by the developers, and by Civic San Diego.

Thank you for your time in considering these comments.

Kathleen Hallahan  
President, East Village Residents Group

## James Alexander

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**From:** Eric Sal <eric.salavat@gmail.com>  
**Sent:** Wednesday, May 09, 2018 5:33 PM  
**Cc:** christopherward@sandiego.gov; mbchase@sandiego.gov; belliot@sandiego.gov; fpstark@gmail.com; cce@carrierjohnson.com; Brad Richter; James Alexander  
**Subject:** Stop Project Ezabelle

May 9, 2018

To whom it may concern,

I am writing to express my concerns regarding the proposed “Ezabelle” Project (File Number: CCDP/SDP 2018-07). I am an owner in Park Blvd East which is directly adjacent to the new proposed building. Myself, and many of my fellow residents do not support this project.

The biggest impact this will have on residents is parking. There is already too little parking on this block. There are currently two other apartments under construction on the same block with zero parking. This project would add an additional 48 units with zero parking.

The developers are using the Affordable Housing Density Bonus to not require parking, however only 6 of the 48 units will be low income housing. This is just a technicality they are employing to get their building approved with zero parking. This would be devastating to the current residents of the area.

Sincerely,

Eric Salavatcioglu

## James Alexander

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**From:** Steve S <gospectrum@gmail.com>  
**Sent:** Wednesday, May 09, 2018 7:31 PM  
**Subject:** Stop project Ezabelle (File number: CCDP/SDP 2018-07)

To whom it may concern,

I'm a resident and homeowner in East Village (Park Boulevard East 1225 Island Ave) and I'm writing to express my concern with the proposed project Ezabelle (File number: CCDP/SDP 2018-07). My major concern is the number of residential units on this block with zero parking spaces. The neighborhood cannot support the number of vehicles this type of development requires. Please consider the impact of the number of residential units on this block without off street parking. There are already 86 new units "the studios" under construction on the same block with no off street parking. There cannot be justification for an additional 48 units without off street parking.

I urge you to **not** support the proposed Ezabelle project as it is a clear example of over-development. Thank you for your consideration.

Sincerely,  
Steve Sjoberg

## James Alexander

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**From:** Cari Melton <carolann.melton@gmail.com>  
**Sent:** Thursday, May 10, 2018 9:51 AM  
**To:** James Alexander  
**Subject:** Ezabelle Project Proposal--Please Vote To Stop This Development

Dear Mr. Alexander,

I am writing to you to share my concerns in regards to the Ezabelle Project, a proposed nine-story building in the small lot of 454 13th Avenue, in the East Village. My family and I are homeowners at 1225 Island Avenue. Our building has entrance points and units around the block on 13th Ave; Park Blvd; and Island Ave. While I understand that more housing is needed in San Diego, the impact of the project is detrimental to our neighborhood, our block and our building. These challenges may not be immediately obvious when looking at the proposal so please consider the following and **vote to eliminate this project** or to find a more suitable location.

Overall, I support the idea of more housing options in San Diego but there are currently two apartment housing projects underway on the small block of 13th Avenue, between Island and J streets. The addition of the Ezabelle Project would make that 3 new apartment buildings on the same street with NO designated parking for more than 140 new units, further impacting the limited resources of parking for residents, guests, businesses, service vehicles and deliveries. Despite the close proximity to the Trolley, many people rely on their cars for employment and family needs that cannot be addressed by public transportation in a realistic way. The Developer was able to present the proposal without parking by offering 6/48 units for residents with very low incomes. The East Village already has 90% of the housing options for neighbors with low incomes and six more units is an insignificant addition--not enough to begin to justify the negative impact of this excessive project on the current residents/neighborhood.

Despite contacting others in East Village, **the Developer did not reach out to the HOA of Park Blvd East**, which is troublesome as we are the residents who will be most impacted if this project is approved. The proposal shows a street view of the project from 13th Avenue but does not address the extreme proximity and impact on Park Blvd East. The plans do not give attention to the South and West facing walls that will be facing and practically touching our building, directly impacting more than 50 residential units...our HOMES! The project would essentially block the view of the sky, all sunlight and fresh breezes to most of the units of Park Blvd East Condos--our home for the past 9 years. We purchased our home because of the location and view. I understand that we do not "own" the view but should the residents of our building suffer loss of value and quality of life so that a new, excessive project can happen? How is that equitable? The value of our homes will decrease significantly if this project happens and we will be forced to leave or to have the Ezabelle building pressed up against our patio, within close view of all of our windows. Especially with a small daughter, I am uncomfortable with that possibility. Furthermore, the entrance to the Ezabelle building is designed to be in the "back" of the lot, essentially next to our small, common-area courtyard. Security has been an ongoing issue that we have recently managed to control with expensive modifications and now we could have constant foot traffic and noise in our vulnerable area once again? Unacceptable!

The historic yellow Wright house that is currently on the lot is a welcome change to the increasing number of mid and high rise buildings in the immediate area. Additionally, the plan to incorporate the home into the design does not preserve the beauty or charm of the building with its sloped roof, skylights and porches. We understand that one room in the home would actually be used as the trash receptacle! Additionally, Ezabelle



Project would overshadow the historic Sheldon Home on the corner of 13th and Island, further reducing the charm of the entire area.

Beyond the impact on the beauty of the neighborhood, I have concerns in regards to the safety plan for this building. Adding 48 units to an area that was designed for one, single-family home seems to be a further stretch on resources, especially with an entrance located at the back of the lot, not directly accessible from 13th Avenue by emergency crews.

Families, homeowners and voting citizens help to stabilize a neighborhood. We want East Village to be the interesting, inviting, Arts District it was meant to be! Please do not allow it to become a generic, over-crowded portion of the city that people pass through in order to get to Petco Park or to The Gaslamp District. Please help to retain some of East Village's charm and appeal so that it is pleasant for all residents and visitors, now and in the future. As a teacher and a Veteran, my husband and I struggled 9 years ago to purchase our home and obviously, we want our home to remain pleasant and to retain its value but we also recognize that the Ezabelle Project is an excessive addition to this portion East Village due to traffic concerns, parking issues and over-crowding!

In closing, our 3 year old daughter summed it up for me when I told her that I was finishing a letter to our city leaders to ask for help to stop the possibility of a building in our "backyard" (patio). Grace said, "Building go away!!! No more sun for Jude-Jude!" (our dog Jude loves the direct sun that streams through our bedroom window during the spring and summer! A seasonal treat!)

I welcome any questions or comments from you and I truly appreciate your time.

Carol Ann Melton  
619-993-6156

## James Alexander

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**From:** Vickie Monegan <vickie1.04@verizon.net>  
**Sent:** Thursday, May 10, 2018 10:08 AM  
**To:** christopherward@sandiego.gov; mbchase@sandiego.gov; belliot@sandiego.gov; fpstark@gmail.gov; cce@carrierjohnson.com; Brad Richter; James Alexander  
**Subject:** Ezabelle Project (File #CCDP/SDP 2018-07)

Good morning.

My name is Vickie Monegan. I am a resident of Park Boulevard East. My husband and I are retired and purchased our condo in May of 2017.

In just 12 months, we are amazed at the building in East Village. One morning we woke up and discovered our view of the Coronado Bridge is going to disappear. I do not know the quantity of apartments being built down the street by Petco, but walking by and seeing how huge this project is tells me traffic is going to be a nightmare. I've been told that this project was approved based on the builder/owner providing low income housing. We get it – but what an opener that building everywhere seems to be a priority of the City regardless of how the San Diego downtown residents feel about this. One morning during this time we learned about the 34 story building going up on the corner of Market and Park. Both of these projects will have “low income residences”. It's apparent that “low income housing” guarantee is how these builders are approved to build.

I am against this project for numerous reasons.

- Parking is already a major issue downtown – there simply isn't enough parking for the 8<sup>th</sup> largest city in the country. East Village parking is impossible. We have actually had to have our family park in a hotel in Cortez Hill for the weekend and pick them up to bring to our home. Construction on 13<sup>th</sup> (between Island and J) has been going on since I have lived there. Between Island and J and 13<sup>th</sup> and 14<sup>th</sup>, construction on both sides to renovate condos “for low income again”, has been going on since I moved in. Both of the condos DO NOT PROVIDE PARKING. Now you are approving another large condo complex in the exact same area that DOES NOT PROVIDE PARKING. What are you thinking??? What about handicap parking which I highly support since I am handicapped. There are a few handicap parking spaces on 13<sup>th</sup> by this project that HAS NOT BEEN AVAILABLE SINCE I MOVED IN BECAUSE OF CONSTRUCTION AND THE CITY HASN'T PROVIDED TEMPORARY HANDICAP PARKING IN THIS AREA DURING THIS CONSTRUCTION! Since the existing two buildings under renovation do not have parking, where are those handicap individuals supposed to park. Where are the handicap people who will occupy the Ezabelle building supposed to park? Furthermore, with the Ezabelle project and the two renovation projects (in the same location) which provide no parking adds up to 134 units. That's 134 parking spaces. If there are two people per unit, that's 268 parking spaces needed. Have you even considered this?
- The fact that the builder didn't inform our building about this project is UNACCEPTABLE. I feel like they are sneaking in another project just to make the almighty dollar. I know how this works and I am sure you DO TOO.
- My husband and I have reviewed the tentative plans. My husband is a retired construction project manager where he supervised tilt up buildings. We walked down to our courtyard and found that the one renovation building completely blocks any sun light from about a 1/3 of the courtyard. Looks like a cave and no vegetation will grow. When we walked to the other side and my husband mentally surveyed the boundary lines, we could see that this new project will block all sunlight from the balance of our courtyard. The courtyard is small. This project will be extremely close to our building and will definitely destroy any peace those residents have of feeling like they have a yard. Downtown is a cement city – our residents need sunlight – our residents need some privacy – our residents need peace. Have any of you even taken the time to come and inspect the issues

this is going to create for our building or the streets? I welcome each of you to join us and see what we are already experiencing and what is to come of this should this building go up.

- I saw on the project that the bottom floor of this building is supposed to be designated as business. Seriously, have you even seen what is happening in East Village. You want low income housing – what about low income rates for businesses. I cannot believe the amount of businesses I have seen close down because they cannot afford the rent. Having this builder provide “business space” on the bottom floor is a joke if the rent isn’t low enough for them to operate. Salvucci’s closed down because of the rent. The Bottega Americano closed down because of rent. I heard this week that the Thomas Jefferson School of Law is going to move because of rent – seriously?
- Do any of you live downtown? Do any of you walk the streets of East Village? I feel like so much interest is given to the Marina District and the Gas Lamp area. We were told by friends that East Village is going to be an upcoming and awesome community and that was the place to buy. So we did. We live with low housing rent and half way houses all around us. I don’t feel safe at all. You just keep adding to the problem and I am requesting that you please stop. I am sure that you are under pressure by groups to provide low rent housing but with this comes problems for our community and there is already enough.
- In my opinion, there is already limited to no parking available for businesses in East Village. With the already two renovation buildings and now with this projected new building providing no parking, how are the current businesses supposed to provide services. People will stay away and go somewhere else that is more convenient.
- The entrance to this new project is in the back which concerns our residents. I feel this is a major security issue.
- In my opinion I feel it would be extremely unsafe to build this project. What if there is a fire – it is so close to us that our building could catch on fire. Since the main entrance is in the back – would the fire department need to come through our courtyard to get to the emergency.
- What about street lights. If I recall, there aren’t any on 13<sup>th</sup> between Island and J. WHY???? Most of the time a new builder is required to pay for street improvement – street lights. Is this builder being required to install street lights for the safety of our community?

Thank you for the time you have spent reading my concerns. I do hope you all would consider meeting with the residents of Park Boulevard East so you can see for yourselves the impact this project would have not only for Park Boulevard East, but the surrounding residents as well.

Vickie Monegan  
1225 Island Avenue, #404  
San Diego, CA 92101

## James Alexander

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**From:** Brandon Montgomery <brandon.montgomery@gmail.com>  
**Sent:** Thursday, May 10, 2018 8:43 AM  
**To:** christopherward@sandiego.gov  
**Cc:** mbchase@sandiego.gov; bellriott@sandiego.com; fpstark@gmail.com; cce@carrierjohnson.com; Brad Richter; James Alexander  
**Subject:** Project Ezabelle

To Whom it may concern,

Since I've purchased into and resided at Park Boulevard East, 1225 Island Ave., my experience in East Village has been continuously improving. I am normally very welcoming to all the new development because it's the best for the East Village as a whole, but I don't see the need for or want for Project Ezabelle to proceed.

As a resident on the east facing side of Park Boulevard East, my view will be directly impacted with the new building. When I decided to acquire my unit, I did so partly because of the calming views of the historic Wright House and morning sunshine. I would be very disappointed to have this aspect negatively impacted.

Another issue I have with building is the additional parking congestion that will be created with so many more additional units located on a small block. Finding open and available parking is already an issue with during peak periods. With the activation of Ezabelle and the reactivation of the 434 and 435 Studios, I see the parking for guests and customers of local business to become like parking in Ocean Beach, North Park and Mission beach.

Please consider not continuing the development of Project Ezabelle as it stands.

Regards

Brandon Montgomery

## James Alexander

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**From:** Adam Moss <ammsd6@gmail.com>  
**Sent:** Thursday, May 10, 2018 9:23 AM  
**To:** christopherward@sandiego.gov  
**Cc:** mbchase@sandiego.gov; James Alexander; Brad Richter; cce@carrierjohnson.com; fpstark@gmail.com; bellriott@sandiego.gov  
**Subject:** Ezabelle Project

To Whom It May Concern,

I am a home owner in East Village and live at Park Blvd East. I have many concerns with the proposed Ezabelle project (File Number CCDP/SDP 2018-17).

Parking is already a massive issue with family, guests, service contractors who come to visit my home. Typically, a guest of mine is forced to park 4-5 blocks away. This means they are parking on 17-18th st which most guests do not feel safe leaving their car in that area and fear for their personal safety. Ezabelle adds an additional 48 units to my block and provides no parking at all?!? Combined with the 2 other complexes being built on my block, this is a total of 134 residential units with no parking available. I anticipate this issue will force guests to park east of the 5 freeway. This means guest will no longer even be parking in downtown, they will be parking in Sherman Heights. East village is home to many events, most prominent is the Padres. Padres games immediately raises all parking spots to \$25. Those prices are not reasonable for a guests who simply coming to visit my home.

The other main issue for parking is businesses. East Village (between Park-16th st) is doing a great job of adding residential units but businesses such as restaurants, gyms, dry cleaners, etc.. will have no parking available. Local businesses need parking available or it will never be feasible for consumers who do not live downtown to visit them.

I have looked at the building plan for Ezabelle and it will cast a shadow on Park Blvd east. This will affect approximately 50% of building. These east facing units will now receive very minimal sunlight making them less desirable.

It seems to me that this project is taking advantage of affordable housing density bonus but this complex is only providing 6 low income residents. I would love to see the Wright House restored and not demolished. I urge you to not support Project Ezabelle.

Thank you for your Time,

Adam Moss

**James Alexander**

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**From:** Woody <woodyaal@aol.com>  
**Sent:** Friday, May 11, 2018 3:35 PM  
**To:** christopherward@sandiego.gov  
**Subject:** Project Ezabelle

Hello,

I live in the East Village at Park Blvd. and Island Ave. I am writing to object to the Ezabelle project proposed for 13th Street between Island Ave. and J Street.

This proposed project is on a street with two other buildings with about 90 units that do not have parking. They are proposing 48 more units on this street with no parking. There is already extremely limited parking on 13th street to begin with between Market and where it ends at K St. On top of this the project is trying to "work the system" and get an exemption for lack of parking because there will be 6 units that are "proposed" low income.

From the plans the building will be built almost to the fence of Park Blvd. East, where I live. With a planned back entrance and a project taking up almost the entire lot there will be not ingress or egress to speak of for fire services. This hidden entrance is also a security risk for our building. The way the building is proposed will remove a lot of privacy from our building. They had to jack knife the project onto the property.

They have made up a design for the historic house where the building sits right on top of it making the house look like a pancake. It completely takes over and distracts from the original house. One room will be a trash room making taking away completely from the historic nature of the house.

There are not any close parking garages for rent. The garage the Padres owns rents during the off season for \$250 a month.

The East Village has almost 90% of the low income housing already. The 6 units they would add are unnecessary and would be only "proposed". It is time to distribute low income housing throughout San Diego and share the obligation to build low cost housing in an area.

Please reject this project as it is not an addition to the neighborhood and will subtract from the quality of life in this area.

Regards,

Sarah Woodruff Watkins (Woody)



## James Alexander

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**From:** Vickie Monegan <vickie1.04@verizon.net>  
**Sent:** Saturday, May 12, 2018 1:12 PM  
**To:** christopherward@sandiego.gov; mbchase@sandiego.gov; belliot@sandiego.gov; fpstark@gmail.gov; cce@carrierjohnson.com; Brad Richter; James Alexander  
**Subject:** Ezabelle Project (File #CCDP/SDP2018-07) 2nd Letter

Hello again everyone:

This is my second email to all of you. This morning I experienced a conversation with a young lady who is presently renting a unit on the 4<sup>th</sup> floor of our building at 1225 Island Avenue (Park Boulevard East). We were waiting on the elevator and I asked, "how are you", she replied the following:

"I'm not doing well. The owner of my rental condo has informed me that they are going to sell the condo and I need to find a new place to live. I would love to buy the condo but with that new building going in, I don't want to look in someone else's condo or want someone looking into my condo. This project is too close to our building. I don't blame the owner for wanting to sell. I am meeting a real estate agent right now to look at condos for sell in the area".

When I existed the building with her, the agent was waiting for her.

ALARM PEOPLE!!!!!!!!!!!!!!!!!!!!!! This is not good for our community or the residents at Park Boulevard East. I realize this project isn't exactly approved by you yet but the panic is already starting because of it.

Please reconsider any thought of approving this project.

Vickie Monegan  
1225 Island Avenue, #404  
San Diego, CA 92101  
(909) 374-3216

May 14, 2018

Dear CivicSD Board,

A request is being made to have the Board ensure that sufficient community review time is provided prior to the full CivicSD Board vote on the Ezabelle project, CivicSD Project No. 2018-07.

Based on the results of the May 16 DRC meeting, a CivicSD staff recommendation will be made to the Board to recommend Design Review Approval and Planning Commission approval of CivicSD No. 2018-07 CCDP/SDP permits. This will follow the DCPC review scheduled to be conducted shortly.

The public notice associated with the Ezabelle project indicated that a full Board review would follow one week from the DCPC meeting, if so, that would place Ezabelle on the May 23 Board agenda. It is assumed this would be conditioned on completion of the City engineering preliminary review (IB630) of the project. That being said, as a community partner, it is requested that the Board address the Ezabelle project no earlier than the regularly scheduled June 27 Board meeting. The reason for the request is two-fold.

First an *independent* community based working group assessment of the projects compliance with the CCPDO and DCP is underway; preliminary results indicate areas of non-compliance and inconsistency; however more time is needed to fully flesh out these concerns. The group may seek consultation of certified design professionals and/or civil engineers in which case they will provide their independent assessments to the Board. A core value of CivicSD as the politically entrepreneurial development partner of the City of San Diego is to respect the community as partners. It is hoped the Board will adhere to this in its decision to ensure sufficient community review time.

The second reason for the request to push out the Board review of the Ezabelle design is it is believed that the Board, per corporate bylaws, should formally elect an interim CivicSD President prior to considering any planning related matters involving the CCPDO. CCPDO 156.0304(a) states the President *shall* administer the CCPDO and ensure compliance with the procedures and regulations of the CCPDO, as well as the DCP and other applicable documents.

It is understood that the CFO has informally assumed the duties of the interim President and has publicly used a dual hat title of COO. The capabilities of the CFO to perform *operationally* as the President are not being contested. However, as the Board is undoubtedly aware, a basic tenant of good management practice is to document and formalize responsibility and accountability within the organization. If the Board seeks the CFO to assume interim Presidential responsibility and accountability, it should be formalized through a Board election as specified in the corporate bylaws. If the Board seeks to have a COO assume the responsibility and accountability duties in lieu of a President, the corporate bylaws and CCPDO should be addressed.

Thank you for your consideration.

Very Respectfully,

John Smith, Member

Ezabelle Community Partner Working Group

## James Alexander

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**From:** Jesse Tenenbaum <jtenenbaum@guildmortgage.net>  
**Sent:** Monday, May 14, 2018 1:58 PM  
**To:** christopherward@sandiego.gov; mbchase@sandiego.gov; belliot@sandiego.gov; fpstark@gmail.com; cce@carrierjohnson.com; Brad Richter; James Alexander  
**Subject:** "Ezabelle" Project

To Whom it May Concern

I am writing to express my concerns regarding the proposed "Ezabelle" Project (File Number: CCDP/SDP 2018-07). As a resident and home owner in East Village, (Park Boulevard East 1225 Island Ave) my family and I have supported and seen the benefit of the current "boom" in residential real-estate development in East Village. However, we cannot in good conscious support this project.

The first and biggest problem we have is that this would be the third apartment building on the same street with zero parking. The first two (434 and 435 13th street "The Studios") are currently under construction, and are adding 86 new units to the street with zero parking. Ezabelle would add an additional 48 units to the same block again with no parking. If this project is approved it would total 134 residential units on one block with zero parking. I understand that "The Studios" were already built but vacant, which is why we as residents supported the reconstruction. Yet, I cannot see how the city could approve a third apartment building with zero parking on the same street.

Secondly, this building is going to have a direct adverse effect on the residential privacy and sunlight access of the residents in Park Boulevard East. The current proposed plans only show the street side view of the building, and does show how much this building would overshadow Park Boulevard East. I can confidently say that "Ezabelle" would be an unacceptable over-development of a site, which already contains the historic Wright House. The minimal sunlight that our gardens do get would be gone if this building was approved. The developer has paid little attention to the South and West facing walls which directly impact over 50 units in Park Boulevard East.

I understand that East Village is rapidly growing, and believe that expanding development projects are good for the community, but this project is simply too much. They are utilizing the City's Affordable Housing Density Bonus to expedite this project and not require parking, but only SIX units will be for low-income residents. Local businesses rely on the already limited street parking, and adding 48 more units will not help anyone. I urge you to not support a third apartment building on one city block with zero parking. There is so much potential to restore the Wright House, and not build simply on top of it.

Respectfully,

Jesse Tenenbaum

## James Alexander

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**From:** George Aban <g\_aban@hotmail.com>  
**Sent:** Wednesday, May 16, 2018 1:47 PM  
**To:** christopherward@sandiego.gov; mbchase@sandiego.gov; bellriott@sandiego.gov; fpstark@fmail.com; cce@carrierjonson.com; Brad Richter; James Alexander  
**Subject:** Ezabelle Project number CCPD/SDP 2018-07

To whom it may concern:

I am want to express my concerns regarding the new Ezabelle Project. As a resident and homeowner in East village (park blvd east, 1225 Islan Ave, San Diego, CA 92101), I don't support the Ezabelle Project. There are two studio apartment buildings on 13th that are under construction with no available parking. A third apartment building (Ezabelle) is under review with no plans of parking for there tenants. This is a big issue. Parking on 13th street and other local streets are already congested. I cant see as to why the city would approve a third apartment building with zero parking. With the East Village rapidly growing, parking for business and its residents are rapidly diminishing.

I urge you to not support the third apartment with zero parking.

Respectfully,

George Aban  
Home Owner at Park Blvd East

## James Alexander

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**From:** Michelle Brower <brower.michelle@yahoo.com>  
**Sent:** Wednesday, May 16, 2018 4:54 PM  
**To:** chistopherward@sandiego.gov  
**Cc:** mbchase@sandiego.gov; bellott@sandiego.gov; fpstark@gmail.com; cce@carrierjohnson.com; Brad Richter; James Alexander  
**Subject:** Ezabelle project

Michelle Brower

1225 Island Ave, Unit 217  
San Diego, CA 92101  
May 16, 2018

To whom it may concern,

My name is Michelle Brower, I'm a registered nurse at a local hospital and have been a resident of East Village for over 3 years. I am now a home owner in East Village. Coming from a big city, Washington D.C., I was enticed to live in downtown San Diego with the hopes to have a similar city life. I have supported the growth and progress that the city is making, and I am quite aware of the housing difficulties in San Diego. I am writing to you today with regards to my concern of the new "Ezabelle" development (file number CCDP/SDP 2018-07).

I supported "The Studios" (434 and 435 13th street) transformation, and as they are existing building being revamped I understand why they are not able or required to have parking for their 86 studio units. I cannot in good conscious support the "Ezabelle" project as it would be the third building on this block, adding 48 units with ZERO parking. This would total over **130 units** on this one block with absolutely **ZERO parking**. Since this project is being built from scratch the developer is capable of including parking in the design.

I personally tried to live without a car in San Diego, as I did in D.C., but quickly discovered how difficult it is to get many places solely using public transportation. Street parking in East Village is not abundant either. There is weekly street sweeping which limits the amount of overnight parking on any given night. During the day most parking spots are either 2 hour metered or 30 minute zones. There is very limited parking in the garages downtown as well; and garage spots average between \$180- \$250 a month if you are able to acquire one. These spots may not be guaranteed, especially during events. This doesn't even begin to take into account the street closings when there are Padres games. Business and guests rely on the scarce street parking already.

The 8-story proposed design of the project would oppress the historical landmark that was well-maintained before purchase by the current developer. There is no mention for designs, other than plain concrete walls, of the south and west facing walls which would directly impact the current neighborhood residents of Park Blvd East. Not to mention this proposed building would tower more than 30 ft above any other structure on this block and eradicate sunlight to existing gardens. "Ezabelle" would not embrace the historic site, more so it would crush and hide the historic structure. We have had several historical homes turned into thriving businesses such as "The Mission" on 13<sup>th</sup> street and Half Door Brewing on 9<sup>th</sup> street; without losing the essence and integrity of these beautiful historic buildings.

San Diego has been labeled one of the top five most unaffordable cities to live in. The U.S. Department of Housing and Urban Development (HUD) defines "affordable" as housing that costs no more than 30 percent of

a household's monthly income. The average salary for a family of 4 in San Diego is just around \$64,000 gross. That equates to an affordable rent of no more than \$1,500.00 for a family. Who is going to keep these buildings accountable to ensure the rent stays affordable after they take advantage of these affordable housing incentives and are built? **Only 6 units out of 48 have been designated for low-income single occupancy.**

I am hoping that as public officials representing our community's best interest you will consider the impact of this proposed "Ezabelle" project will have. There is a need for housing in San Diego, especially affordable housing but "Ezabelle" micro-luxury studio project on this small plot that will desecrate the historic property is not the way to go about it. Most jobs in San Diego are not located downtown and people must commute to their employment by car. I hope that you will take into account the issues and the negative impact this project will have on our community.

Respectfully,

Michelle Brower, RN

## **To Appointed Officials reviewing the Ezebelle Project,**

My fiancé and I live in East Village and are proud owners at Park Boulevard East. We see great potential the East Village community but this community still has a long way to go. I currently work as a Project Engineer for one San Diego's largest aerospace companies, and my fiancé works at a well know medical center in La Jolla. We spend a good portion of our income in the East Village area and hope to see it prosper. What I see with the development of the Ezebelle Project is a rushed idea, pitched for approval, and is geared for maximizing initial return, but lacks long term sustainment, and ignores the overall benefit to the community. The developers have provided no constructive communication with East Village community and its residence. The developer seems to have reached out to approving officials in East Village except the people who will be most affected, Park Boulevard East which houses over 40 residential units and surrounding blocks. The entrance to Ezabelle is in the back, basically in our courtyard, which greatly concerns the residents of Park Boulevard East for security as well as proper emergency personnel access.

The 864 square-foot house on the Ezabelle site was built in 1881 and qualifies as a local landmark that is an example of Italianate style from early development in East Village. Such landmark should be preserved and cherished as one of the few San Diego icons in the community. Instead the developer is looking to pancake stack an 8 story development on top of a historical building, which is in no way is tasteful. Currently this historic wright house is dilapidating further with development plans to use one room as a trash receptacle. An alternative solution for this site would be to restore the historical building into a profitable business. Example Queenstown Public house which is a super successful restaurant in Little Italy.

There is an importance in balancing the community, neighborhoods flourish when there's a healthy mix of life, businesses, work, and play development. The addition of Ezabelle is not contributing to the healthy balance, instead is tipping the balance further to an already dense area of low income housing and lack of parking. Micro-apartments for this development are sized at 211 to 348 square feet, which equivalent to living in your 14x15 ft bedroom. Hardly livable space for one individual with lack of full appliances, and basic needs such as in unit showers and bathrooms. Ezebelle Micro-apartment sizes exuberate temporary housing, high turnover rates, lack of community pride and ownership. The Studios 435 across the street are completing a "Luxory" micro apartment upgrade with 202 sq-ft going for \$1,975 per month and 355 sq-ft going for \$2,175 per month. I don't see Ezebelle providing sustainable low income housing if that's what is advertised, but instead adopting a similar more profitable model similar to Studios 435.

Low Income Housing, Ezebelle is not an ideal location:

- Ezebelle Project is 48 unit complex but only 6 units will be low income, which is less than <13% geared towards affordable living!!! Hardly seems like a significant positive impact to affordable housing in San Diego. I do not think this is a good sales pitch for affordable housing in East Village.
- East village already contains almost 90% of the low income housing in downtown, so adding six more units is unnecessary. I support affordable housing and I strongly recommend developers to seek other areas in San Diego to balance the affordable housing development.
- Cost of groceries, and other basic needs in downtown are less affordable than many other places in San Diego. Makes little sense to provide low income housing in an area where whole sale goods and services are significantly higher. Logically this seems like argument to not build further low income housing in a not so affordable area.

Parking Considerations to the Community:

- The city block where project is proposed is to be located is already heavily impacted by 86 new micro-apartment units located on 13<sup>th</sup> St, The Studios 435 will begin renting to the public in the next few months with NO PARKING. The cumulative effect of un-parked 134 new market rate units in a single block will be significant.



- The availability of utilizing our parking structure is seasonal and sporadic due to events at Petco Park. Monthly parking in the area is on the order of \$250 per month and not guaranteed. The low cost housing apartment units in the East Village neighborhood presently make up over 34% of all apartments.
- Statistically speaking <6% of households in San Diego are actually operate without car ownership ([governing.com/gov-data](http://governing.com/gov-data)). Statistical certainty that 94% of Ezebelle residence would add additional vehicles with no resident or guest parking.
- Current approved high rise apartments in East Village the future for affordable parking is going to become a rising issue. San Francisco has a parking issues with cost ranging from \$300 to \$500 monthly, I don't think we want to see this level of impact to our city.

#### Top Concerns from East Village Community

- There are a number of issues that concern the East Village community, and I challenge you our elected officials, project planners, developers, local authorities, to evaluate the location of proposed by Ezebelle and ask yourself how does approving this project provide support improving current issues that are ongoing and unresolved in East Village?
- Absolutely #1 the Homeless issue. This is a huge issue affecting community safety, health with the Hepatitis outbreak, tourism, real estate valuation, and can be contributed to the cause of failing businesses.
- Crime, out of 125 San Diego communities, East Village ranks number one for incidences of violent crimes (according to SDNew.com), within the past year totaling accounts of; 506 Arrest, 409 Assaults, 396 Thefts, 143 Burglary, and 110 Vandalisms.
- Failing businesses (Bottega Americano, Halycon, Stella House, Tilted Kilt, Jefferson School of Law, Primos, several more.....)

#### Real Estate Impact

- East Village comprises 40 percent of Downtown real estate. People view real estate in East Village as potential financial growth and prospect for a thriving community. Ezebelle is NOT contributing to a sustainable solution creating more micro-apartments with no parking.
- This project has the potential to impact real estate negatively in the area, and drive overall valuation down, which directly impacts state property taxes assessment.
- In the event real estate drops due to poor project planning there are a number ways residents can address the tax code including seeking/ordering appeals to the current property tax assessment, seeking appraisals, filing review requests, etc.

As part of the growing East Village community I strongly recommend further review of the Ezebelle Project at a more community involved level and consider alternate solutions for the historic house. While I support further development in East Village, after evaluating the developers proposal I strongly believe that the Ezebelle Project is less than ideal development for this location and would better suited in another location.

Respectfully,



Jeremiah Farinella  
16 May 2018

[JPFarinella@gmail.com](mailto:JPFarinella@gmail.com)  
760.845.5594

Liz Foster  
1225 Island Ave, Unit 208  
San Diego, CA 92101  
May 15<sup>th</sup>, 2018

To Appointed Officials Reviewing the Ezabelle Project,

My name is Liz Foster, I'm a civil engineer, construction project manager and proud community member of East Village. What initially drew me to the East Village community was the revitalization of a community in the heart of beautiful San Diego that was rising in a promising direction. In speaking with neighbors and community members, I am astounded at the history of our neighborhood and how far it has come over the past decade. The growth and turnaround has been the product of informed and thoughtful decisions that align with the collective goals of East Village and the City of San Diego. I write you today out of concern for the new proposed Ezabelle development located at 454 13<sup>th</sup> Street.

As a commercial construction project manager for over 10 years, it is easy to say I have deep seeded passion for buildings and the beauty and prosperity they can bring to a city and community. On my block alone, there are multiple buildings that I adore: The home and office of notable San Diego architect Rob Quigley (416 13<sup>th</sup> St), Sheldon Residence (1245 Island Ave) and the Wright House (454 13<sup>th</sup> Street). Each building is unique in it's own way and brings culture and character to the block and to East Village. These housing icons range from modern industrial to Queen Anne Victorian to Italianate architecture. When I first saw the renderings for the proposed Ezabelle project, my heart sank and a shook my head in disbelief. My gut interpretation was the overwhelming feeling of a historical landmark with great potential being devastated, dominated and crushed. This proposed 8-story monstrosity of a building does not celebrate or embrace the home's historical past and architectural beauty, but makes it a sad pedestal for the foot of the building above. A well-thought-out development that genuinely wants to improve the visibility and livelihood of a historical home can be achieved. There are numerous alternatives that can achieve preserving the building's character and making it a successful impact to the surrounding community. For example, the Mission (on our same block) and Queenstown Public (Little Italy) are successful local restaurants that embrace their building's original structure and design. Even the next block over on J Street, the Alexan project is another example of a project that has tastefully restored the small building on the adjacent property. The design of the Ezabelle project reflects the rushed design to help push a ROI without consideration of the 1881 landmark in its path.

In addition to the design and character concerns, the neighborhood continues to live in the struggle for parking from the ever-growing number of new apartment buildings and lack of sufficient parking structures and designated spots. This concern only intensifies during year-round Petco events nearby. With the Ezabelle development, there is NO PARKING provided for the 48 new units created. In addition, the newly renovated Studios 435 project is nearing completion and this same block is already preparing to absorb the influx of new tenants from 86 units that also have NO PARKING. Combined, 134 units will be added to the block with no provisions for parking other than straining the already stressed parking availability.

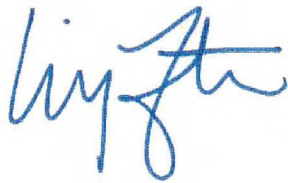
I also understand Ezabelle is being labeled as a positive for the community for providing 6 affordable housing units. In the growing concern for the homeless and low-income families in our city, I do not agree that 6 single occupant 211sf studio apartments will effectively aid or impact the

community needs. A development, whether on this block or elsewhere, can have a greater influence with 2-3 bedroom floorplans to accommodate families in need of stabilization in a low-income home. A single person dwelling will only increase the likelihood of quick turnover tenants that are not invested in the culture or prosperity of the East Village community. In addition, with the recent additions to the neighborhood (Pinnacle 1, Pinnacle 2, Alexan, Shift, etc.) the cost of rent and the cost of the surrounding services are increasing on an already higher-than-average San Diego cost of living. I am a supporter of affordable housing, but this does not seem to be the ideal placement for low-income tenants to optimally thrive.

As a member of the East Village community, myself and my neighbors are always aware of the alarming levels of homelessness, drugs and crime in the community. Just last Monday night, there was a multi-victim shooting at the corner of 15<sup>th</sup> St and Island (less than 2 blocks from the proposed development) that was reported to be gang related. This was one of the most eye-opening and impactful crimes that has happened so closely to my home in recent years. The stretch of 16<sup>th</sup> and 17<sup>th</sup> streets, along with the temporary camps on the sidewalks and parks, is a sad and disturbing reality of our community that each citizen should strive to guide in a positive direction.

As trusted appointed officials, planners and local authorities representing and shepherding our community towards our collective goals, I sincerely ask you if this development design intent and location is truly for the sustainable growth, benefit and support of our neighborhood and neighbors. The Ezabelle project is not our only option. Your decision to not approve this development will send a strong message that future proposed buildings and/or businesses will need to consider the wellbeing of our community. Furthermore, only solutions and ideas that can not only align with the goals of East Village, but also help it thrive, will be approved and implemented. Thank you for your valuable time and commitment to our community.

Respectfully,

A handwritten signature in blue ink, appearing to read "Liz Foster", with a stylized, cursive script.

Liz Foster



# Save Our Heritage Organisation

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*Saving San Diego's Past for the Future*

Friday, May 18, 2018

CIVIC San Diego  
Board of Directors  
401 B Street, Suite 400  
San Diego, CA 92101

Re: 454 13<sup>th</sup> Street, East Village

CIVIC Board Members,

The Board of Directors' for Save Our Heritage Organisation (SOHO) voted to support the project at 454 Thirteenth Street, which includes the historic Wright House, presented as Option 1 (see below) at the May 11, 2018 meeting. This alternative provides approximately 10 feet between the roof of the historic resource and the new development, recedes the second floor behind the historic resource, and utilizes glass on the second floor front and side facades to set the resource apart. This option does not have beams that protrude from the side of the structure.

The SOHO Board appreciates that Nakhshab Development & Design recognized the historical significance of this resource and first sought historical designation for the Wright House. The SOHO Board also acknowledges that this solution retains the historic resource on site, which conveys the fabric and grain of the original built environment, and maintains the resource as a separate building from the new development.

Thank you for the opportunity to comment,

Bruce Coons  
Executive Director  
Save Our Heritage Organisation



**REHABILITATION TREATMENT PLAN**

**DATE:** May 23, 2018

**PROJECT:** Move off site:  
454 13<sup>th</sup> Street  
City historic resource # 1278  
San Diego, Ca. 92101  
Assessors Parcel# 535-156-08-00

Temp. Storage facility / move-on/off site:  
1141 E Street  
San Diego, CA 92101

Move on site:  
454 13<sup>th</sup> Street  
San Diego, Ca. 92101  
Assessors Parcel# 535-156-08-00

**PROJECT TEAM:**

D: Developer: NDD (Nakshab Development & Design)  
PA: Project Architect: NDD  
HA: Historic Architect: NDD  
HAM: Historic Architect Monitor: John Eisenhart, Union Architecture  
PI: Principal Investigator: Law Office of Marie Burke Lia. Marie Burke Lia Attorney  
CM: Construction Manager: NDD  
HM: House Mover: Joe Hansen, John T. Hansen Enterprises  
BI: Building Inspector: City of San Diego Development Services: Environmental and Historical staff.  
RE: Resident Engineer: NDD

**PROPERTY DESCRIPTION:**

The structure at 454 13<sup>th</sup> Street (John and Mary Wright House) is a historically designated single story example of an Italianate Style residence. Building date is 1882. The main structure is of rectangular form 32'-3" x 24'-2" and has a flat roof with projecting cornice (containing wood gutter), wood brackets, cornice, frieze on perimeter at roof wall junction. The building has two prominent bays on the east façade, the main entry porch on the northeast corner, has a wood shingled mansard roof (with cornice and wood dentils), supported by a round wood column (tuscan style). Exterior materials consist of 1x redwood "flush joint" tongue and groove siding (4 1/8" exposure, painted), one over one wood double hung windows, wood casing and trim.

An addition on the west side of the historic building, containing a kitchen, bathrooms and bedroom, is non-historic (1942).

A wood deck and railing extending the original entry area and steps on east side and a wood deck and steps off of the non-historic addition on the west side are non-original.

A 7 story apartment building with penthouse and basement will be constructed at the site at 454 13<sup>th</sup> Street. To facilitate this development the resource has to be temporarily removed from the site. Once the shell of the new development is completed the resource is to be returned to the site. The new location of the resource will be approximately 4'-0" south and 17'-0" east from it's original location on the same lot. It will be incorporated into the street level commercial space of the new development and rehabilitated at this new location in accordance with the U.S. Secretary of Interior's Standards Treatment Plan.

## **INTRODUCTION:**

This Treatment Plan is being prepared to dismantle and temporarily remove the historic portion of the historic building from its current location at 454 13<sup>th</sup> Street (Assessors Parcel# 535-156-08-00). The resource is to be moved and stored safely at a warehouse at 1141 E Street, San Diego, CA 92101 while construction for a new development on the original site is undertaken. Approximate moving distance is 1 mile.

The implementation of the Treatment Plan for the relocation and transportation of 454 13<sup>th</sup> Street structure will be facilitated by a qualified historic House Mover under the observation of the Project Architect (PA) and Historic Architect Monitor (HAM) in a manner consistent with the mitigation, monitoring, and reporting program for this project.

The drawings outline proposed general stabilization and preparation of the structure for relocation. Including demolition of the westerly non-historic addition, and aforementioned non-original decks / steps.

The House Mover (HM) is responsible for detailing exact stabilization, disassembly, bracing and stabilization of pieces etc. to assure safe move of resource. Project Architect (PA) and Resident Engineer (RE) to be responsible for detailing exact stabilization, bracing, disassembly etc. to assure safety of resource. HAM to review.

This Treatment Plan is accompanied by a copy of HABS drawings of the property prepared by the (HAM).

This Treatment Plan and its related drawings will be included in all subsequent plans for the discretionary permit processing and construction documents.

## **PREPARATION / RELOCATION OF STRUCTURE:**

### 1. Preparation of the resource prior to move:

The 1882 original structure is to be partially disassembled to be transported and stored in sections. The entire structure is to be stabilized, braced, and secured, individual building sections are to be stabilized, braced and secured. Structural framing members at non-visible areas may be braced with sheathing / blocking, additional framing etc. as required.

Fenestration (doors and window sashes) to be removed, frames and casings to remain in place. Fenestration openings to be secured. Use the City of San Diego standards for securing fenestration openings. Exterior plumbing vent, supply and waste pipes, electrical boxes and conduits shall be removed. All utilities to be disconnected.

Specific procedures to be determined by qualified historic House Mover, Project Architect and Resident Engineer and to be reviewed by HAM. Monitor to be notified prior to modification of structure not outlined in Treatment Plan. Consistent with Standards # 6,7, 9 and 10.

The 4x4 beams and wood columns below the existing 2x6 floor joists maybe removed. The perimeter foundation is not a significant feature and may be removed as required also. Any redwood exterior siding found on the west façade during demolition of the 1942 appendix should be left in salvaged.

Brace entire structure. Exterior siding or trim pieces affected by this shall be removed prior to damage. These pieces are to be stored and refastened during rehabilitation.

## 2. Demolition / removal of non-historic additions:

Prior to the start of the demolition / removal process. The Contractor and Monitor will meet on site to review the scope of demolition / removal work.

Non-historic addition and decks shall be clearly marked, and adjacent historic areas shall be protected from accidental demolition impact damage approved by monitor prior to demolition. Provide protective barriers (plywood, wood studs, red tape etc. – any attachment to historic fabric with as minimal nailing as possible) at corners of the resource, at any detailing and over windows. Contractor and Monitor to review on site.

The Treatment plan drawings will show location of the areas to be demolished and location of historic structure to remain.

## 3. Partial disassembly of historic structure:

Prior to the start of any disassembly work. The Contractor and Monitor will meet on site to review the scope of work. All parts of the resource to be disassembled shall be clearly marked and recorded prior and during disassembly; by PA, RE, HA and Historic House Mover to be reviewed by HAM.

Dismantling should be minimized as much as possible to preserve integrity of the resource: Per Treatment Plan the historic structure shall be dismantled into approximately five pieces. Mansard roof and porch shall be braced and transported as an assembly. The eastern façade panel containing the (2) bays will be divided in (2) wall segments. Each bay will be braced in place and remain part of their pertaining wall panel.

At the eastern and western façade the ceiling and floor joists (running north – south direction) will be cut at the sill / top plate. Along the southern and northern façade the floor and ceiling joists abutting the façade will remain in place and help stabilize. Each wall segment will be detached at each corner.

Damage to the flush wood siding shall be minimized by using a thin kerf blade for the vertical saw cuts. Removal of flush wood siding is not recommended. Damage is expected due to age of wood, specific type of siding (flush) and type of nails.

- a. Exterior wall assemblies north and south façade: Wood siding, interior and exterior window casing and trim, wood studs to remain in place. Interior lath and plaster on walls and ceilings shall be removed. The perimeter walls are to be secured (from wall sill plate up to wall top plate at parapet) from the interior side with ½” plywood sheathing across



the faces of the existing exposed studs. 4x8 Wood strong-backs 4'-0" o.c. are to be secured horizontally to the interior face of each wall segment attached to the existing stud framing with ¼" diameter, 8" length lag bolts (to be reviewed by RE). Existing cornices are to be braced and protected in place as required in order to remain intact / connected with their wall segment. Each segment is to be labeled.

The ceiling and floor joists (running north/south direction) will be cut at the sill / top plate of each wall segment. Additionally, each wall segment will have to be detached at each corner (nails pulled from stud connection and round corner trim should be left attached to one of the two segments this edge should be protected in place). PA, RE, HA and Historic House Mover, to be reviewed by HAM.

- b. Exterior wall assembly of east façade: Bays, Wood siding, interior and exterior window casing and trim, wood studs to remain in place. The perimeter walls are to be secured (from wall sill plate up to wall top plate at parapet) from the interior side with ½" plywood sheathing across the faces of the existing exposed studs. 4x8 Wood strong-backs 4'-0" o.c. are to be secured horizontally to the interior face of each wall segment attached to the existing stud framing with ¼" diameter, 8" length lag bolts (to be reviewed by RE). Existing cornices and existing roofing at each bay are to be braced and protected in place as required in order to remain intact / connected with perimeter walls of the bay. Each segment is to be labeled. The ceiling and floor joists (running north / south direction) abutting the wall segment shall be left in place, they will aid in stabilization of the wall segment. Additionally, each wall segment will have to be detached at each corner (nails pulled from stud connection and round corner trim should be left attached to one of the two segments this edge should be protected in place).
- c. Parapet assemblies on north, east, south and west: Frieze board / cornice, parapet bracing / brackets, wood gutter shall remain whole with their pertaining wall panel. Verify existing connections. Parapet pieces shall be stabilized and braced with additional ½" plywood and wood studs as necessary. PA, RE, HA and Historic House Mover, to be reviewed by HAM.
- d. East façade bays: Each bay should be stabilized and braced, as part of their pertaining wall segment (see 3b.). PA, RE, HA and Historic House Mover, to be reviewed by HAM.
- e. Entry porch accessory structure on north-east side of main structure to be stabilized and braced, to be moved as one whole assembly including roof. Vertical cut line to be determined by PA, RE, HA and Historic House Mover, to be reviewed by HAM.
- f. Round corner trims at siding corners: Care should be taken as to not damage the round corner trims at building corners. They should either remain in place attached to one of the two façade panels, or if not feasible carefully removed, catalogued. PA, RE, HA and Historic House Mover, to be reviewed by HAM.
- g. Floor: The existing floor and framing will be demolished during dissassembly of the resource. Old growth wood members should be salvaged. Methods of separating horizontal framing members from walls (cut locations) to be determined by PA, RE, HA and Historic House Mover, to be reviewed by HAM. Floor framing members to stay intact and help with bracing at eastern bays and at entry porch.
- h. Roof: Roof and ceiling at eastern bays and at entry porch to remain intact. The existing main ceiling / roof and framing will be demolished during dissassembly of the resource. Old growth wood framing members shall be salvaged. Projecting parapet to stay intact. PA, RE, HA and Historic House Mover, to be reviewed by HAM.

Dissassembly work and subsequent transport shall occur without any time delay.

At end of each work day, or as required, all parts to be protected from weather and vandalism.

During demolition / removal and disassembly work, Construction Manager to inform Monitor of discovery of any architectural elements on site (these may include brackets, posts, casings, doors, leaded windows, exterior siding on interior walls (ie. west wall) etc.. Monitor to evaluate relevance of such materials and discuss any change to treatment plan and construction documents that might better interpret the historical significance of the residence. Consistent with Standards # 2, 6, 7, and 9.

#### 4. Movement and storage of resource:

The resource will be moved to a warehouse located at 1141 E Street San Diego, CA 92101. Each section of the resource will be protected and transported to this secure, climate controlled warehouse. Each individual piece shall be catalogued / labeled. Once the shell construction of the new development is completed each section of the resource will return to the site for reassembly and rehabilitation.

The location of the residence at the move-on site will be approximately 4'-0" south and 17'-0" east of the original location. The present height finish floor to grade is approximately 18". The new height of the finish first floor will be at approximately 12" above grade, resource will integrate with new development at the first floor.

Reassembly of the resource will occur at the new site, any temporary bracing will be removed and any required rehabilitation of the structure will commence. Since the original entry porch deck and steps are non-surviving a new entry porch wood deck and steps, sympathetic to Italianate era, will be constructed at the new location per Secretary of the Interior's Standards of Rehabilitation, to be reviewed by HAM. Consistent with Standards # 1, 2, 9, 10.

#### 5. House Mover / disassembly and reassembly of resource:

1. Prior to cutting or dismantling panels a structural engineer needs to provide a letter of acceptance for the method employed by the contractor. A letter of acceptance from the engineer should review and accept the method for cutting, moving the panels will work in accordance with the Treatment Plan (historic resource not to be damaged). Monitor (HAM) to observe method employed by contractor on site with structural engineer and project architect (PA) present.
2. Each panel is to be marked on the plywood side with the number provided on Treatment Plan, if any field changes occur, additional numbers need to be added and shall be noted on the Treatment Plan. In addition, each panel shall have an envelope securely taped, containing the follow: A. historic name and resource #., B. the elevation and floor plan of the panel in relation to the entire building, C. the name, address, phone number, email address of the project architect, general contractor who dismantled the panels, historic monitor, city historic planner, and owner and finally D. the date it was disassembled from the original location.

House Mover to outline path of move, sequence of move, and means in which disassembled pieces are to be secured for the move. Monitor and City Staff to approve plan prior to moving date.

Generally, the movement of the historic resource shall be done slowly and on a path that is smooth and graded. If damage occurs to the resource during the move the monitor will be notified immediately. Consistent with Standards # 1, 2.

**EXISTING FOUNDATION:**

The residence consists of areas with brick perimeter wall foundation (1882, brick size 8 ¼"x 3 7/8" x 2 5/8"), isolated concrete and wood pier footings with 4x4 or 2x4 columns supporting 4x4 beam (1882). Floor joists (1882) are 2x6's @ 24" o.c. and rest on top of 4x4 beams and 2x sill plate at perimeter wall. Floor sheathing is original 1x4 wood plank in western portion of the residence (not verified under non-original oak floor in eastern part of residence). The existing 2x sill plate at the perimeter wall shall remain as part of each individual wall panel. Existing foundation, floor framing, flooring will be removed in their entirety. Consistent with Standards #9 and 10.

**NEW FOUNDATION:**

The new development will include a basement, the resource is to be located on the ground floor concrete slab – elevation +/- 12" above grade (original finish floor level is approx. 18" above grade). Work at move-on site new development shall be sufficiently completed to prior to move-off of resource. Consistent with Standards # 9 and 10.

**EXISTING FRAMING:**

Horizontal members:

First floor framing is non-original 2 ¼" width oak finish flooring over 2x6 floor joists (1882). It could not be verified if original subfloor wood plank is still existing in this area. Original 1x4 subfloor on 2x6 floor joists at 24" o.c. in carpeted areas.

Roof framing consists of 2x4 roof rafters at 24" o.c. with 1x10 wood roof sheathing board, ceiling framing consists of 2x4 ceiling joists at 16" o.c..

This will be removed and cut as required to detach wall panels.

Vertical members:

Exterior wall framing is 2x4 wood studs at 32" o.c. with intermediate 1x4 members. 1x horizontal redwood "flush joint" tongue and groove siding (4 1/8" exposure) with wood round corner trims. All existing perimeter wood stud framing, siding and trim is to remain intact. Each individual wall panel will be rehabilitated and reinstalled, plywood backing will be carefully removed and new interior wall finish will be installed. Any additional structural work necessary at walls to be concealed from exterior view.

Interior wall framing consists of 2x4 wood studs at 32" o.c. with intermediate 1x4 members. This will be demolished.

Consistent with Standards # 2, 9 and 10.

**ROOF:**

Main roof: The main roof (1882 structure) is a flat roof, roofing material is sheet roll roofing. This will be removed, roof framing members will be cut from walls once walls are braced and ready for transport (see existing roof framing above). Repair and restore cornice, bracing and frieze: The approx. 1'-4" horizontal roof projections (cornice, brackets) on the north, east and south shall remain intact, roof projection / cornice and brackets along west façade shall be salvaged to be used as needed during rehabilitation work.

Porch roof: The porch north and west side has a wood shingle mansard roof. No evidence of other roofing material was discovered. Entire porch roof / wall assembly to remain as one piece. Mansard wood roof shingles shall be restored, if any new wood shingles are installed, the exist. shingles will serve as a template. Consistent with Standards # 6, 9 and 10.

**EXTERIOR WALL FINISHES:**

The existing 1x horizontal redwood “flush joint” tongue and groove siding (4 1/8” exposure) to remain in place on exterior walls.

If siding board or trim has minor cracks, repair with wood epoxy filler. If the entire board or trim is damaged or missing, an existing board taken from a non-visible area shall replace it in likeness and kind. If this is not possible, a new board or trim of the same profile and species may be used as a replacement (west façade). If a section of siding or trim is damaged or missing a “Dutchman” type repair should be performed. Wood fenestration casings (head, jamb, sill and apron) are wood and will remain and be repaired as necessary. Missing pieces shall be replicated existing profile serves as template, match species.

Existing will serve as a template for the restoration at the in-filled window opening on the south façade.

Consistent with Standards # 2, 6, 7, 9, and 10.

### **EXTERIOR DOORS AND WINDOWS:**

Windows are wood, double hung type. The frames are wood and should be kept in place.

Window sashes to be removed along with hardware and safely stored (remove, mark and store dead weights for each window, mark and store any hardware that needs to be dismantled).

Repair, clean and paint. Reinstall for smooth operation.

If a feature of the sash or frame is missing or deteriorated beyond repair (Monitor to determine condition) a replicated profile of the same wood species as the missing element shall be used.

Repair of window units shall use epoxy resin type putty to infill missing profiles. If the sash and fixed units are a new unit, they should match the profile of the existing original rail and style.

Both options (repair or replication) would be consistent with the standards, repair is generally the preferred option.

An original window opening has been in-filled on the south façade, this shall be restored, the original windows will serve as a template. Original door openings on the interior appear to have been modified in height. No original doors have been found to remain. Transom window at main entrance to be restored in likeness and kind, original door is missing a new wood door sympathetic to italianate style should be installed, HAM to review proposed. The existing original window sashes shall be marked for location taken from on plan by PA. The fenestration will be reinstalled and be repaired for smooth operation. If any new wood windows, the original windows will serve as a template.

Refer to Preservation Brief # 9 for repair of windows. Consistent with Standards # 2, 6, 7, 9, 10.

### **MAIN ENTRY PORCH:**

The main entry porch is to remain, it is preferred it is used as the main entry area in its new location. The porch deck material has been altered, the original steps are missing. The original porch area is about 18” above grade. At move-on site it will be at approx. 12” above grade. The original porch bead board ceiling, column and low wall along porch north side should remain in place and be repaired. Since the original porch floor material and steps no longer exist a new a new porch floor and steps should be constructed consistent with the Secretary of the Interior’s Standards for rehabilitation. Historic Monitor to review Project / Historic Architect design.

Consistent with Standards # 6, 9 and 10.

### **FIREPLACE:**

The fireplace was installed at a later date. An original window opening was in-filled on the south façade. The fireplace and interior chimney shall be removed and the window opening restored, see *Exterior Windows and Doors*. Consistent with Standards # 6.

### **ELECTRICAL & LIGHTING:**

The existing electrical and lighting system will be upgraded to conform to current code. Electrical meter shall be located discretely away from view. Exterior lighting fixtures to be surface mounted or pendant type sympathetic to Italianate Style. Per Historic Building Code, lighting fixtures that replicate the Italianate Style may be incandescent. Incandescent lighting should be used throughout the historic residence. Consistent with Standards # 9 and 10.

#### **PLUMBING:**

All exterior plumbing and vent pipe to be dismantled. New interior plumbing and vents to be installed as required. Vent pipes to avoid being visible from principal elevation or near the edge of roof. Areas in exterior siding where old pipes have been removed to be repaired with salvaged siding from building. The plumbing system should be upgraded to conform to current code. Consistent with Standards # 9 and 10.

#### **PAINTING:**

Remove existing paint, dirt, mildew from exterior wall, fenestration and fenestration casing. Lead abatement should be acknowledged with these finishes. Existing materials to be tested for lead paint and if detected, follow current laws for careful removal and disposal. Proper sand, repair, finish shall be enforced (Contractor to be CDPH LRC-certified, lead paint may be sanded with EPA approved sanders with full unit HEPA certified vacuums, by Festool brand or equal: <http://hepa.festool.us/hepa-vacuums/EPA-RRP-Certified-HEPA-Vacuum/>). Paint scheme on the exterior of the building, at fenestration, fenestration casing should be in period colors / color scheme (2-4 colors recommended). PA to select, HAM to review. Monitor and City Staff to approve final paint scheme. Refer to Preservation Brief #10. Consistent with Standards #6 and 7.

#### **REPAIR:**

The cleaning of all historic material/fabric shall occur through using the gentlest means possible. An appropriate means of control and disposal of lead, asbestos or other chemicals shall be provided. Historic fabric shall be retained as much as possible. Do not sandblast or water power wash materials. Minor wood repair shall use Abatron Epox fill. If wood is damaged to a greater extent, a dutchman type repair shall be performed. Refer to Preservation Brief #1.

#### **REHABILITATION CHARACTER DEFINING FEATURES:**

The overall character defining features of the resource are two prominent bay forms on the east elevation, flat roof with projecting cornice at the roof wall junction, dental frieze and brackets, a mansard roof over the entry porch area supported by wood column.

The character defining material elements are: Painted redwood horizontal "flat joint" tongue and groove siding (4 1/8" exposure) with round wood trim finished corners, wood one over one double hung windows, wood casing and trim, tuscan type wood column.

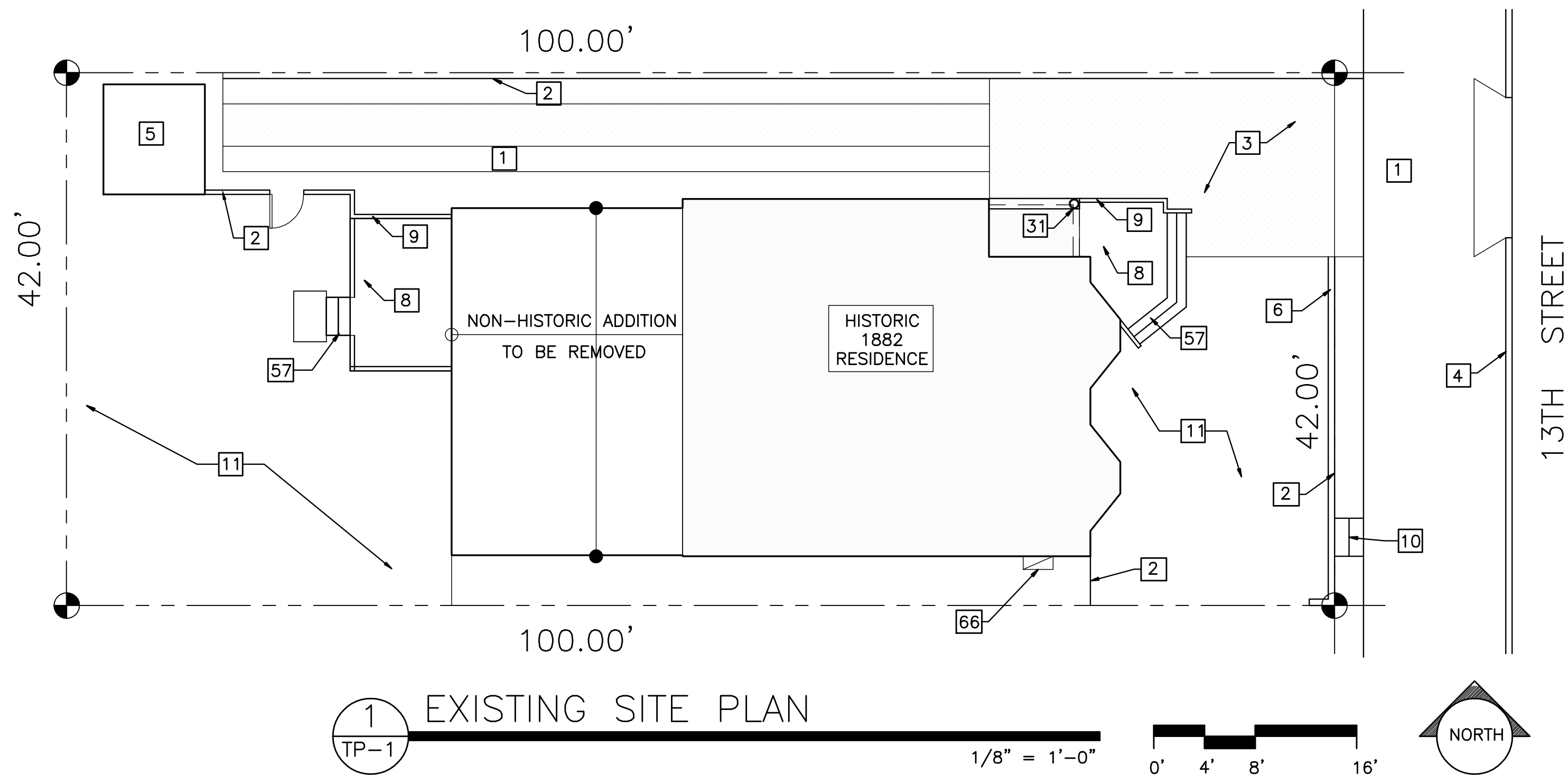
Should damage occur to the resource, it shall be repaired in conformance with the Secretary of the Interior's Standards for Rehabilitation or Reconstruction. Consistent with Standards #2, 6, 9, and 10.

#### **ATTACHMENTS: T**

reatment Drawings, HABS Documents.

TREATMENT PLAN FOR JOHN AND MARY WRIGHT RESIDENCE – CITY OF SAN DIEGO HISTORIC RESOURCE #1278

ERRECTED IN 1882, PERIOD OF SIGNIFICANCE 1882



SHEET INDEX:

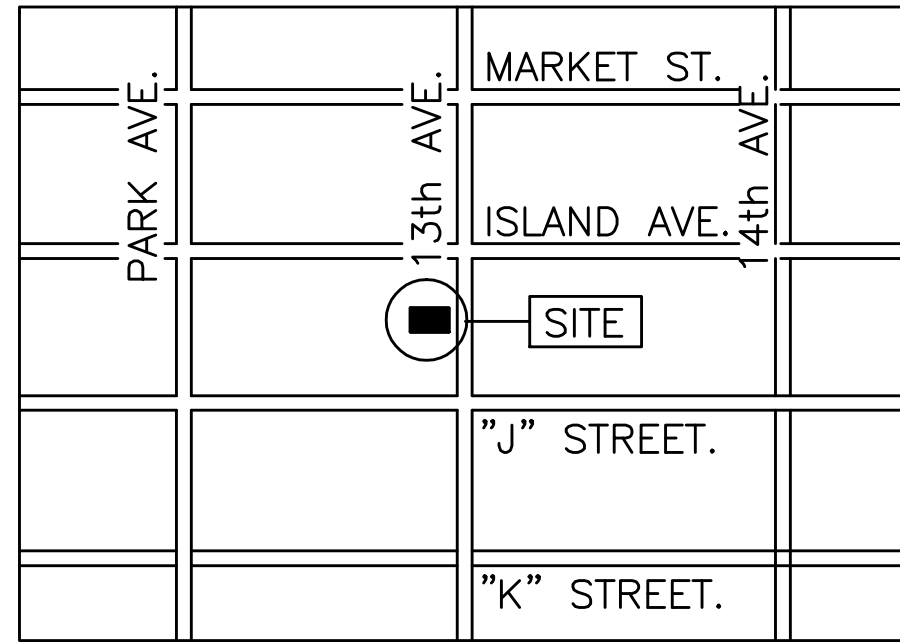
- TP-1 EXIST. SITEPLAN, NOTES & PHOTOS  
TP-1.1 COPY OF TREATMENT PLAN  
TP-1.2 COPY OF MONITORING PLAN  
TP-2 FLOOR PLAN, ROOF PLAN & DETAILS  
TP-3 ELEVATIONS & DETAILS  
TP-4 SECTIONS & DETAILS



2 PHOTO OF NORTH ELEVATION 2017

DESCRIPTION OF RESOURCE.

THE JOHN AND MARY WRIGHT RESIDENCE WAS BUILT IN 1882. IT IS A HISTORICAL DESIGNATED RESOURCE UNDER CRITERION "C" AS A GOOD REPRESENTATION OF ITALIANATE STYLE ARCHITECTURE. THE CHARACTER DEFINING FEATURES OF THE RESIDENCE INCLUDE TWO PROMINENT BAY FORMS AT EAST ELEVATION, TYPICAL BRACKETS, DENTIL FRIEZE AND CORNICE AT ROOF / WALL JUNCTION, MANSARD ROOF OVER PORCH ENTRY AREA, ONE OVER ONE DOUBLE HUNG WINDOWS AND FLUSH WOOD TONGUE AND GROOVE SIDING.



VICINITY MAP

NO SCALE



4 PHOTO OF EAST ELEVATION BAYS 2017



3 PHOTO OF PORCH ELEVATION 2017

LEGEND

NOTES

- A. MATERIALS OR CONSTRUCTION FROM PERIOD OF SIGNIFICANCE LABELLED "1882" U.N.O.  
B. NON-PERIOD OF SIGNIFICANCE MATERIALS ARE NOT LABELLED

- |                           |                                 |                           |  |                              |  |                          |                          |   |
|---------------------------|---------------------------------|---------------------------|--|------------------------------|--|--------------------------|--------------------------|---|
| 1 CONCRETE DRIVE          | 8 WOOD DECK                     | 16 WOOD D.H. WINDOW       | 24 FRIEZE PANEL 1882                       | 32 WOOD CAPITAL 1882         | 40 2X6 FLOOR JOIST 1882                                | 48 WOOD DOOR             | 56 ATTIC VENT            | 65 ROOF VENT  |
| 2 FENCE                   | 9 WOOD RAILING                  | 17 WOOD CASING /TRIM 1882 | 25 WOOD CORNICE 1882                       | 33 WOOD DENTIL 1882          | 41 2X6 FLOOR JOIST                                     | 49 CARPET                | 57 WOOD STEPS            | 66 ELECTRIC METER   |
| 3 CONCRETE STREET CURB    | 10 CONCRETE STEPS               | 18 WOOD CASEMENT          | 26 WOOD GUTTER 1882                        | 34 WOOD DENTIL               | 42 1X4 WOOD PLANK 1882                                 | 50 TILE                  | 58 4'X8' SKYLIGHT        | 67 WOOD FRAMED UTIL. CHASE                                |
| 4 BRICK PAVER             | 11 LANDSCAPE                    | 19 VINYL SLIDER WINDOW    | 27 WOOD BRACKET 1882                       | 35 WOOD BASEBOARD 1882       | 43 4X4 POST 1882                                       | 51 FIREPLACE             | 59 2'X2' SKYLIGHT        | 68 WOOD BEAM  |
| 5 WOOD SHED               | 12 1X VERTICAL WOOD BOARD 1882  | 20 PANEL DOOR             | 28 WOOD ROUND CORNER TRIM 1882             | 36 OAK FLOORING 2 1/4" WIDTH | 44 4X4 BEAM 1882                                       | 52 BUILT IN CABINET 1882 | 60 VINYL FLOORING        | 69 1X4 WOOD CORNER TRIM                                   |
| 6 CONCRETE RETAINING WALL | 13 1X SIDING "FLUSH JOINT" 1882 | 21 TRANSOM WINDOW 1882    | SEE DETAIL 5 / TP-3                        | 37 2X4 CEILING JOIST 1882    | 45 1X10 WOOD SHEATHING BOARD 1882                      | 53 BOOKSHELVES           | 61 ENGINEERED WOOD FLOOR | 70 1X6 BEAD BOARD CEILING WITH MIDPOINT BEAD. SEE 9 /TP-3 |
| 7 BRICK FOUNDATION 1882   | 14 WOOD SHINGLE ROOF 1882       | 22 1X SKIRT BOARD 1882    | 29 WOOD ROUND CAP 1882 SEE DETAIL 5 / TP-3 | 38 2X4 ROOF RAFTER 1882      | 46 TYP. WALL 2X4 @32"O.C. WITH INTERMEDIATE 1X4 MEMBER | 54 WOOD SHIPLAP SIDING   | 62 WOOD FASCIA RAFTER    | 71 2X4 STUD WALL 16" O.C.                                 |
| 8 1/4" X 3 7/8" X 2 5/8"  | 15 WOOD D.H. WINDOW 1882        | 23 1X SKIRT BOARD         | 30 WOOD PLINTH 1882                        | 39 WOOD LATH / PLASTER 1882  | 47 WOOD FRENCH DOOR                                    | 55 WOOD SIDING           | 63 SHEET ROLL ROOFING    |   |
|                           | SEE DETAIL 3 /TP-2              |                           | 31 WOOD COLUMN 1882. SEE DETAIL 9/TP-3     |                              |  |                          | 64 ROOF SCUPPER          |   |

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ARCHITECT:  
**UNION ARCHITECTURE, INC.**  
JOHN H. EISENHART, ARCHITECT  
EVA L.J. THORN, INTERIORS

1530 BROOKES AVENUE, SAN DIEGO, CALIFORNIA 92103  
TELEPHONE: 619.269.4941 / WWW.UNIONARCH.COM

PROJECT:  
JOHN AND MARY WRIGHT HOUSE, 454 13TH AVE.  
CITY OF SAN DIEGO HISTORIC RESOURCE SITE #1278,  
BUILT 1882.

CLIENT:  
NAKSHAB DESIGN DEVELOPMENT  
340 15TH STREET, SAN DIEGO, CA 92101

DATE: 01.19.2018  
REV: 04.20.2018

TP-1

SHEET 1 OF 6



TREATMENT PLAN FOR JOHN AND MARY WRIGHT RESIDENCE – CITY OF SAN DIEGO HISTORIC RESOURCE #1278

ERRECTED IN 1882, PERIOD OF SIGNIFICANCE 1882

dead weights for each window, mark and store any hardware that needs to be dismantled). Repair, clean and paint. Reinstall for smooth operation. An original window opening has been in-filled on the south façade, this shall be restored, the original windows will serve as a template. Original door openings on the interior appear to have been modified in height. No original doors have been found to remain. Transom window at main entrance to be restored in likeness and kind, original door is missing a new wood door sympathetic to Italianate style should be installed, HAM to review proposed. The existing original window sashes shall be marked for location taken from on plan by PA. The fenestration will be reinstalled and be repaired for smooth operation. If any new wood windows, the original windows will serve as a template. Refer to Preservation Brief # 9 for repair of windows. Consistent with Standards # 2, 6, 7, 9, 10.

MAIN ENTRY PORCH:

The main entry porch is to remain, it is preferred it is used as the main entry area in its new location. The porch deck material has been altered, the original steps are missing. The original porch area is about 18" above grade. At move-on site it will be at grade level. The original porch bead board ceiling, column and low wall along porch north side should remain in place and be repaired. Consistent with Standards # 6, 9 and 10.

FIREPLACE:

The fireplace was installed at a later date. An original window opening was in-filled on the south façade. The fireplace and interior chimney shall be removed and the window opening restored, see *Exterior Windows and Doors*. Consistent with Standards # 6.

ELECTRICAL & LIGHTING:

The existing electrical and lighting system will be upgraded to conform to current code. Electrical meter shall be located discretely away from view. Exterior lighting fixtures to be surface mounted or pendant type sympathetic to Italianate Style. Per Historic Building Code, lighting fixtures that replicate the Italianate Style may be incandescent. Incandescent lighting should be used throughout the historic residence. Consistent with Standards # 9 and 10.

PLUMBING:

All exterior plumbing and vent pipe to be dismantled. New interior plumbing and vents to be installed as required. Vent pipes to avoid being visible from principal elevation or near the edge of roof. Areas in exterior siding where old pipes have been removed to be repaired with salvaged siding from building. The plumbing system should be upgraded to conform to current code. Consistent with Standards # 9 and 10.

PAINTING:

Remove existing paint, dirt, mildew from fenestration and fenestration casing. Paint scheme on the exterior of the building, at fenestration, fenestration casing should be in period colors / color scheme (2-4 colors recommended). PA to select, HAM to review. Existing materials to be tested for lead paint and if detected, follow current laws for careful removal and disposal. Monitor and City Staff to approve final paint scheme. Refer to Preservation Brief # 10. Consistent with Standards # 6.and 7.

REPAIR:

The cleaning of all historic material/fabric shall occur through using the gentlest means possible. An appropriate means of control and disposal of lead, asbestos or other chemicals shall be provided. Historic fabric shall be retained as much as possible. Do not sandblast or water

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power wash materials. Minor wood repair shall use **Abatron Epox fill**. If wood is damaged to a greater extent, a dutchman type repiar shall be performed. Refer to Preservation Brief # 1.

REHABILITATION CHARACTER DEFINING FEATURES:

The overall character defining features of the resource are two prominent bay forms on the east elevation, flat roof with projecting cornice at the roof wall junction, dental frieze and brackets, a mansard roof over the entry porch area supported by wood column.

The character defining material elements are: Painted redwood horizontal "flat joint" tongue and groove siding (4 1/8" exposure) with round wood trim finished corners, wood one over one double hung windows, wood casing and trim, tuscan type wood column.

Should damage occur to the resource, it shall be repaired in conformance with the Secretary of the Interior's Standards for Rehabilitation or Reconstruction. Consistent with Standards # 2, 6, 9, and 10.

ATTACHMENTS:

Treatment Drawings, HABS Documents.

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The location of the residence at the move-on site will be approximately 4'-0" south and 17'-0" east of the original location. The present height finish floor to grade is approximately 18". The new height of the finish first floor will be at grade level. resource will integrate with new development at the first floor slab on grade. Reassembly will occur at the new site, any temporary bracing will be removed and any required rehabilitation of the structure will commence. Consistent with Standards # 1, 2, 9, 10.

5. House Mover / disassembly and reassembly of resource:

- Prior to cutting or dismantling panels a structural engineer needs to provide a letter of acceptance for the method employed by the contractor. A letter of acceptance from the engineer should review and accept the method for cutting, moving the panels will work in accordance with the Treatment Plan (historic resource not to be damaged). Monitor (HAM) to observe method employed by contractor on site with structural engineer and project architect (PA) present.
- Each panel is to be marked on the plywood side with the number provided on Treatment Plan, if any field changes occur, additional numbers need to be added and shall be noted on the Treatment Plan. In addition, each panel shall have an envelope securely taped, containing the follow: A. historic name and resource #., B. the elevation and floor plan of the panel in relation to the entire building, C. the name, address, phone number, email address of the project architect, general contractor who dismantled the panels, historic monitor, city historic planner, and owner and finally D. the date it was disassembled from the original location.

House Mover to outline path of move, sequence of move, and means in which disassembled pieces are to be secured for the move. Monitor and City Staff to approve plan prior to moving date. Generally, the movement of the historic resource shall be done slowly and on a path that is smooth and graded. If damage occurs to the resource during the move the monitor will be notified immediately. Consistent with Standards # 1, 2.

EXISTING FOUNDATION:

The residence consists of areas with brick perimeter wall foundation (1882, brick size 8 1/4"x 3 7/8" x 2 5/8"), isolated concrete and wood pier footings with 4x4 or 2x4 columns supporting 4x4 beam (1882). Floor joists (1882) are 2x6's @ 24"o.c. and rest on top of 4x4 beams and 2x sill plate at perimeter wall. Floor sheathing is original 1x4 wood plank in western portion of the residence (not verified under non-original oak floor in eastern part of residence). The existing 2x sill plate at the perimeter wall shall remain as part of each individual wall panel. Existing foundation, floor framing, flooring will be removed in their entirety. Consistent with Standards #9 and 10.

NEW FOUNDATION:

The new development will include a basement, the resource is to be located on the ground floor concrete slab – elevation 0'-0" (original finish floor level is approx. 18" above grade). Work at move-on site new development shall be sufficiently completed to prior to move-off of resource. Consistent with Standards # 9 and 10.

EXISTING FRAMING:

Horizontal members: First floor framing is non-original 2 1/4" width oak finish flooring over 2x6 floor joists (1882). It could not be verified if original subfloor wood plank is still existing in this area. Original 1x4 subfloor on 2x6 floor joists at 24" o.c. in carpeted areas. Roof framing consists of 2x4 roof rafters at 24" o.c. with 1x10 wood roof sheathing board, ceiling framing consists of 2x4 ceiling joists at 16" o.c. This will be removed and cut as required to detach wall panels.

Vertical members:

Exterior wall framing is 2x4 wood studs at 32" o.c. with intermediate 1x4 members. 1x horizontal redwood "flush joint" tongue and groove siding (4 1/8" exposure) with wood round corner trims. All existing perimeter wood stud framing, siding and trim is to remain intact. Each individual wall panel will be rehabilitated and reinstalled, plywood backing will be carefully removed and new interior wall finish will be installed. Any additional structural work necessary at walls to be concealed from exterior view.

Interior wall framing consists of 2x4 wood studs at 32" o.c. with intermediate 1x4 members. This will be demolished.

Consistent with Standards # 2, 9 and 10.

ROOF:

Main roof: The main roof (1882 strucutre) is a flat roof, roofing material is sheet roll roofing. This will be removed, roof framing members will be cut from walls once walls are braced and ready for transport (see existing roof framing above). Repair and restore cornice, bracing and frieze: The approx. 1'-4" horizontal roof projections (cornice, brackets) on the north, east and south shall remain intact, roof projection / cornice and brackets along west façade shall be salvaged to be used as needed during rehabilitation work.

Porch roof: The porch north and west side has a wood shingle mansard roof. No evidence of other roofing material was discovered. Entire porch roof / wall assembly to remain as one piece. Mansard wood roof shingles shall be restored, if any new wood shingles are installed, the exist. shingles will serve as a template.

Consistent with Standards # 6, 9 and 10.

EXTERIOR WALL FINISHES:

The existing 1x horizontal redwood "flush joint" tngue and groove siding (4 1/8" exposure) to remain in place on exterior walls. If siding board has minor cracks, repair with wood epoxy filler. If the entire board is damaged or missing, an existing board taken from a non-visible area shall replace it likeness in kind. If this is not possible, a new board of the same profile and species may be used as a replacement (west façade). Wood fenestration casings (head, jamb, sill and apron) are wood and to remain in place and be repaired. Existing will serve as a template for the restoration at the in-filled window opening on the south façade. Consistent with Standards # 2, 6, 7, 9, and 10.

EXTERIOR DOORS AND WINDOWS:

Windows are wood, double hung type. The frames are wood and should be kept in place. Window sashes to be removed along with hardware and safely stored (remove, mark and store

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Specific procedures to be determined by qualified historic House Mover, Project Architect and Resident Engineer and to be reviewed by HAM. Monitor to be notified prior to modification of structure not outlined in Treatment Plan. Consistent with Standards # 6, 7, 9 and 10.

The 4x4 beams and wood columns below the existing 2x6 floor joists maybe removed. The perimeter foundation is not a significant feature and may be removed as required also. Any redwood exterior siding found on the west façade during demolition of the 1942 appendix should be left in salvaged.

Brace entire structure. Exterior siding or trim pieces affected by this shall be removed prior to damage. These pieces are to be stored and refastened during rehabilitation.

2. Demolition / removal of non-historic additions:

Prior to the start of the demolition / removal process. The Contractor and Monitor will meet on site to review the scope of demolition / removal work.

Non-historic addition and decks shall be clearly marked and approved by monitor prior to demolition. The Treatment plan drawings will show location of the areas to be demolished and loaction of historic structure to remain.

The historic resource shall be protected as the work on the areas to be removed is undertaken.

3. Partial disassembly of historic structure:

Prior to the start of any disassembly work. The Contractor and Monitor will meet on site to review the scope of work. All parts of the resource to be disassembled shall be clearly marked and recorded prior and during disassembly and shall be reviewed by HAM.

Dismantling should be minimized as much as possible to preserve integrity of the resource: Per Treatment Plan the historic structure shall be dismantled into approximately five pieces. Mansard roof and porch as well as each bay shall be braced and tranported as an assembly.

Largest workable pieces and major components to be determined by PA, RE, HA and Historic House Mover to be reviewed by HAM.

- Exterior wall assemblies north and south façade:** Wood siding, interior and exterior window casing and trim, wood studs to remain in place. Interior lath and plaster shall be removed. The perimeter walls are to be secured from the interior side with 1/2" plywood sheathing across the faces of the existing exposed studs. Two steel strong-backs are to be secured horizontally to the interior face of each wall segment attached to the existing stud framing, one at the top and one at the bottom of the wall segment (to be reviewed by RE). Existing cornices are to be braced and protected in place as required in order to remain intact / connected with their wall segment. Each segment is to be labeled. If entire length of one façade is too large for transport, methods of stabilization, bracing and vertical cut line to be determined by PA, RE, HA and Historic House Mover, to be reviewed by HAM.
- Exterior wall assembly of east façade:** Bays, Wood siding, interior and exterior window casing and trim, wood studs to remain in place. The perimeter walls are to be secured from the interior side with 1/2" plywood sheathing across the faces of the existing exposed studs. Two steel strong-backs are to be secured horizontally to the interior face of each

3 OF 8

- wall segment attached to the existing stud framing, one at the top and one at the bottom of the wall segment (to be reviewed by RE). Existing cornices and existing roofing at each bay are to be braced and protected in place as required in order to remain intact / connected with perimeter walls of the bay. Each segment is to be labeled. If entire length of one façade is too large for transport, methods of stabilization, bracing and vertical cut line to be determined by PA, RE, HA and Historic House Mover, to be reviewed by HAM. Parapet assemblies on north, east, south and west: Frieze board / cornice, parapet bracing / brackets, wood gutter shall remain whole. Parapet pieces shall be stabilized and braced. If not feasible to retain connection / assembly with vertical wall assembly below they may be removed in one piece from each façade (north, east, south, west). Methods of stabilisation and bracing and horizontal cut lines to be determined by PA, RE, HA and Historic House Mover, to be reviewed by HAM.
- East façade bays:** Each bay should be stabilized and braced, to be moved as one whole assembly including roof. Vertical cut line to be determined by PA, RE, HA and Historic House Mover, to be reviewed by HAM.
  - Entry porch accessory structure** on north-east side of main structure to be stabilized and braced, to be moved as one whole assembly including roof. Vertical cut line to be determined by PA, RE, HA and Historic House Mover, to be reviewed by HAM.
  - Round corner trims at siding corners:** Care should be taken as to not damage the round corner trims at building corners. They should either remain in place attached to one of the two façade panels, or if not feasible carefully removed, catalogued. To be determined by PA, RE, HA and Historic House Mover, to be reviewed by HAM.
  - Floor:** The existing floor and framing will be demolished during disassembly of the resource. Old growth wood members should be salvaged. Methods of separating horizontal framing members from walls (cut locations) to be determined by PA, RE, HA and Historic House Mover, to be reviewed by HAM. Floor framing members to stay intact and help with bracing at eastern bays and at entry porch.
  - Roof:** Roof and ceiling at eastern bays and at entry porch to remain intact. The existing main ceiling / roof and framing will be demolished during disassembly of the resource. Old growth wood framing members shall be salvaged. Methods of separating horizontal framing members from walls (cut locations) to be determined by PA, RE, HA and Historic House Mover, to be reviewed by HAM.

Disassmably work and subsequent transport shall occur without any time delay. At end of each work day, or as required, all parts to be protected from weather and vandalism.

During demolition / removal and disassembly work, Construction Manager to inform Monitor of discovery of any architectural elements on site (these may include brackets, posts, casings, doors, leaded windows, exterior siding on interior walls (ie. west wall) etc.. Monitor to evaluate relevance of such materials and discuss any change to treatment plan and construction documents that might better interpret the historical significance of the residence. Consistent with Standards # 2, 6, 7, and 9.

4. Movement and storage of resource:

The resource will be moved to a warehouse located at 1141 E Street San Diego, CA 92101. Each section of the resource will be protected and transported to this secure, climate controlled warehouse. Each individual piece shall be catalogued / labeled. Once the shell construction of the new development is completed each section of the resource will return to the site for reassembly and rehabilitation.

4 OF 8

UNION ARCHITECTURE INC. 1530 BROOKES AVE. SAN DIEGO, CA. 92103 619-269-4941

REHABILITATION TREATMENT PLAN

DATE: April 20, 2018

PROJECT: Move off site: 454 13<sup>th</sup> Street City historic resource # 1278 San Diego, Ca. 92101 Assessors Parcel# 535-156-08-00

Temp. Storage facility / move-on/off site: 1141 E Street San Diego, CA 92101

Move on site: 454 13<sup>th</sup> Street San Diego, Ca. 92101 Assessors Parcel# 535-156-08-00

PROJECT TEAM:

D: Developer: NDD (Nakshab Development & Design)  
PA: Project Architect: NDD  
HA: Historic Architect: NDD  
HAM: Historic Architect Monitor: John Eisenhart, Union Architecture  
PI: Principal Investigator: Law Office of Marie Burke Lia. Marie Burke Lia Attorney  
CM: Construction Manager: TBD.  
HM: House Mover: TBD.  
BI: Building Inspector: City of San Diego Development Services: Environmental and Historical staff.  
RE: Resident Engineer: NDD

PROPERTY DESCRIPTION:

The structure at 454 13<sup>th</sup> Street (John and Mary Wright House) is a historically designated single story example of an Italianate Style residence. Building date is 1882. The main structure is of rectangular form 32'-3" x 24'-2" and has a flat roof roof with projecting cornice (containing wood gutter), wood brackets, cornice, frieze on perimeter at roof wall junction. The building has two prominent bays on the east façade, the main entry porch on the northeast corner, has a wood shingled mansard roof (with cornice and wood dentils), supported by a round wood column (tuscan style). Exterior materials consist of 1x redwood "flush joint" tongue and groove siding (4 1/8" exposure, painted), one over one wood double hung windows, wood casing and trim.

An addition on the west side of the historic building, containing a kitchen, bathrooms and bedroom, is non-historic (1942).

A wood deck and railing extending the original entry area and steps on east side and a wood deck and steps off of the non-historic addition on the west side are non-original.

1 OF 8

A 7 story apartment building with penthouse and basement will be constructed at the site at 454 13<sup>th</sup> Street. To facilitate this development the resource has to be temporarily removed from the site. Once the shell of the new development is completed the resource is to be returned to the site. The new location of the resource will be approximately 4'-0" south and 17'-0" east from it's original location on the same ot. It will be incorporated into the street level commercial space of the new development and rehabilitated at this new location in accordance with the U.S. Secretary of Interior's Standards Treatment Plan.

INTRODUCTION:

This Treatment Plan is being prepared to dismantle and temporarily remove the historic portion of the historic building from its current location at 454 13<sup>th</sup> Street (Assessors Parcel# 535-156-08-00). The resource is to be moved and stored safely at a warehouse at 1141 E Street, San Diego, CA 92101 while construction for a new development on the original site is undertaken. Approximate moving distance is 1 mile. The implementation of the Treatment Plan for the relocation and transportation of 454 13<sup>th</sup> Street structure will be facilitated by a qualified historic House Mover under the observation of the Project Architect (PA) and Historic Architect Monitor (HAM) in a manner consistent with the mitigation, monitoring, and reporting program for this project.

The drawings outline proposed general stabilization and preparation of the structure for relocation. Including demolition of the westerly non-historic addition, and aforementioned non-original decks / steps.

The House Mover (HM) is responsible for detailing exact stabilization, disassembly, bracing and stabilization of pieces etc. to assure safe move of resource. Project Architect (PA) and Resident Engineer (RE) to be responsible for detailing exact stabilization, bracing, disassembly etc. to assure safety of resource. HAM to review.

This Treatment Plan is accompanied by a copy of HABS drawings of the property prepared by the (HAM).

This Treatment Plan and its related drawings will be included in all subsequent plans for the discretionary permit processing and construction documents.

PREPARATION / RELOCATION OF STRUCTURE:

1. Preparation of the resource prior to move:

The 1882 original structure is to be partially disassembled to be transported and stored in sections. The entire structure is to be stabilized, braced, and secured, individual building sections are to be stabilized, braced and secured. Structural framing members at non-visible areas may be braced with sheathing / blocking, additional framing etc. as required.

Fenestration (doors and window sashes) to be removed, frames and casings to remain in place. Fenestration openings to be secured. Use the City of San Diego standards for securing fenestration openings. Exterior plumbing vent, supply and waste pipes, electrical boxes and conduits shall be removed. All utilities to be disconnected.

2 OF 8

PROJECT:

JOHN AND MARY WRIGHT HOUSE, 454 13TH AVE.  
CITY OF SAN DIEGO HISTORIC RESOURCE SITE #1278,  
BUILT 1882.

CLIENT:

NAKSHAB DESIGN DEVELOPMENT  
340 15TH STREET, SAN DIEGO, CA 92101

ARCHITECT:

UNION ARCHITECTURE ,INC.

JOHN H. EISENHART, ARCHITECT  
EVA L.J. THORN, INTERIORS

1530 BROOKES AVENUE, SAN DIEGO, CALIFORNIA 92103  
TELEPHONE: 619.269.4941 / WWW.UNIONARCH.COM

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DATE: 01.19.2018  
REV: 04.20.2018

TP—1.1

SHEET 2 OF 6



TREATMENT PLAN FOR JOHN AND MARY WRIGHT RESIDENCE – CITY OF SAN DIEGO HISTORIC RESOURCE #1278

ERRECTED IN 1882, PERIOD OF SIGNIFICANCE 1882

6. Pre-construction meeting move-on site. (HAM, HA, PI, CM, BI, D)

Issue: Overview of Treatment Plan, Architectural, Landscaping and Engineering Documents as related to move-on site. Review work involved by CM to prepare site for arrival of structure.

7. New foundation, utilities, site preparation for move on (HAM, HA, CM, HM)

Issue: Review of work on site to assure work will properly receive move-on of resource.

**Monitoring at Move-Off Site “B” Storage Facility at 1141 “E” Street to Move-On Site “A” at 454 13<sup>th</sup> Street.**

8. Move-on site (HAM, HA, CM, BI).

Issue: Review move-on site with resource present. Overview of Treatment Plan for rehabilitation of resource, Architectural, Landscaping and Engineering Documents.

9. Move-on site as required by construction activity (HAM, HA, CM).

Issue: Review rehabilitation of resource in accordance with Treatment Plan and Architectural, Landscaping and Engineering Documents.

10. Final Monitoring (HAM, HA, CM, D).

Issue: Final punch list of items to complete according Treatment Plan and Architectural, Landscaping and Engineering Documents.

11. Draft Report (HAM, BI).

Issue: Draft report of monitor process to be submitted to BI for review.

12. Final Report (HAM, BI, PI, D).

Issue: Final report of monitoring process, submit to PI for distribution to City of San Diego Developmental Services Department, San Diego History Center for archiving.

3 OF 6

UNION ARCHITECTURE INC. 1530 BROOKES AVE. SAN DIEGO, CA. 92103 619-269-4941

**MONITORING PLAN**

DATE: April 20, 2018

PROJECT: Move off site:  
454 13<sup>th</sup> Street  
City historic resource # 1278  
San Diego, Ca. 92101  
Assessors Parcel# 535-156-08-00

Temp. Storage facility / move-on/off site:  
1141 E Street  
San Diego, CA 92101

Move on site:  
454 13<sup>th</sup> Street  
San Diego, Ca. 92101  
Assessors Parcel# 535-156-08-00

**PROJECT TEAM:**

D: Developer: NDD (Nakshab Development & Design)  
PA: Project Architect: NDD  
HA: Historic Architect: NDD  
HAM: Historic Architect Monitor: John Eisenhart, Union Architecture  
PI: Principal Investigator: Law Office of Marie Burke Lia. Marie Burke Lia Attorney  
CM: Construction Manager: TBD  
HM: House Mover:TBD  
BI: Building Inspector: City of San Diego Development Services: Environmental and Historical staff.  
RE: Resident Engineer: NDD

**PROPERTY DESCRIPTION:**

The structure at 454 13<sup>th</sup> Street (John and Mary Wright House) is a historically designated single story example of an Italianate Style residence. Building date is 1882. The main structure is of rectangular form 32'-3" x 24'-2" and has a flat roof roof with projecting cornice (containing wood gutter), wood brackets, cornice, frieze on perimeter at roof wall junction. The building has two prominent bays on the east façade, the main entry porch on the northeast corner, has a wood shingled mansard roof (with cornice and wood dentils), supported by a round wood column (tuscan style). Exterior materials consist of 1x redwood "flush joint" tongue and groove siding (4 1/8" exposure, painted), one over one wood double hung windows, wood casing and trim.

An addition on the west side of the historic building, containing a kitchen, bathrooms and bedroom, is non-historic (1942).

A wood deck and railing extending the original entry area and steps on east side and a wood deck and steps off of the non-historic addition on the west side are non-original.

1 OF 6

A 7 story apartment building with penthouse and basement will be constructed at the site at 454 13<sup>th</sup> Street. To facilitate this development the resource has to be temporarily removed from the site. Once the shell of the new development is completed the resource is to be returned to the site. The new location of the resource will be approximately 4'-0" south and 17'-0" east from it's original location on the same lot, it will be incorporated into the street level commercial space of the new development.

This Treatment Plan is being prepared to dismantle and temporarily remove the historic portion of the building from its current location at 454 13<sup>th</sup> Street (Assessors Parcel# 535-156-08-00). The resource is to be moved to and stored safely at a warehouse at 1141 E Street, San Diego, CA 92101. Approximate moving distance is 1 mile each way.

**Monitoring at Move-Off Site“A” : 454 13<sup>th</sup> Street City, San Diego, Ca. 92101, Assessors Parcel# 535-156-08-00, See area to be monitored figure 1 , 2 and 3 below.**

1. Overview of Treatment Plan and Monitoring Plan (HAM, HA, PI, PA, CM, BI, D, HM).

Issue: Pre-construction meeting as related to historic resource on site. Discuss sequence and type of work to be done prior to move. General methods of protection of structure during demolition work of non-historic additions to be discussed.

2. Preparation of resource for moving (HAM, HA, CM).

Issue: Monitor to be present prior to any disassembly of structure. Location marks of cuts by HM to be determined, general method of disassembly and support to be discussed and approved by HAM per Treatment Plan.

3. Final review of preparation of resource for moving (HAM, HA, CM, HM).

Issue: Monitor to review after completion of the following work: Removal of exterior plumbing, electrical lines. Monitor to take inventory of wood shingles, braces, windows and doors to be salvaged per Treatment Plan. Bracing and protection of structure prior to move off date.

Per Treatment Plan the historic structure shall be dismantled into approximately five pieces. Mansard roof and porch as well as bays shall be braced as one piece. All pieces shall be labeled / catalogued. To be reviewed by HAM.

The historic resources will be moved to site "B" Storage Facility at 1141 "E" Street.

**Monitoring at Move-Off Site “B” Storage Facility at 1141 “E” Street. See area to be monitored figure 4 below.**

4. Move-off storage site (HAM,CM).

Issue: Review work involved by CM to brace and protect structure for storage duration.

5. Continuing monitoring of structures at storage facility (HAM).

Issue: Review protection of resource every 3 months or as required if damage occurs, issue report documenting the damage.

2 OF 6

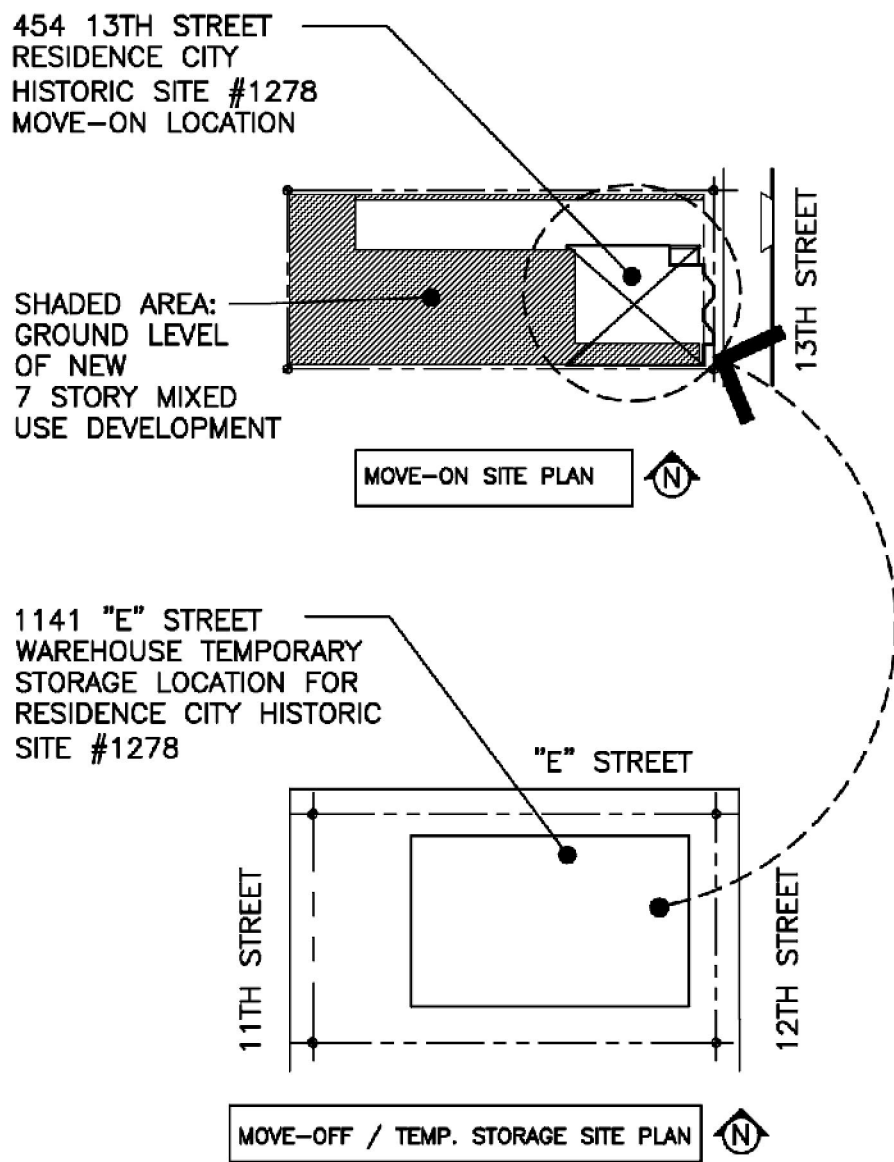


FIG 4.: Move-off site "B" at 1141 E Street, resource to be moved-off from temporary stoarge at warehouse. Final move-on site plan at 454 13th Street. Area shaded is new 7 story development.

6 OF 6

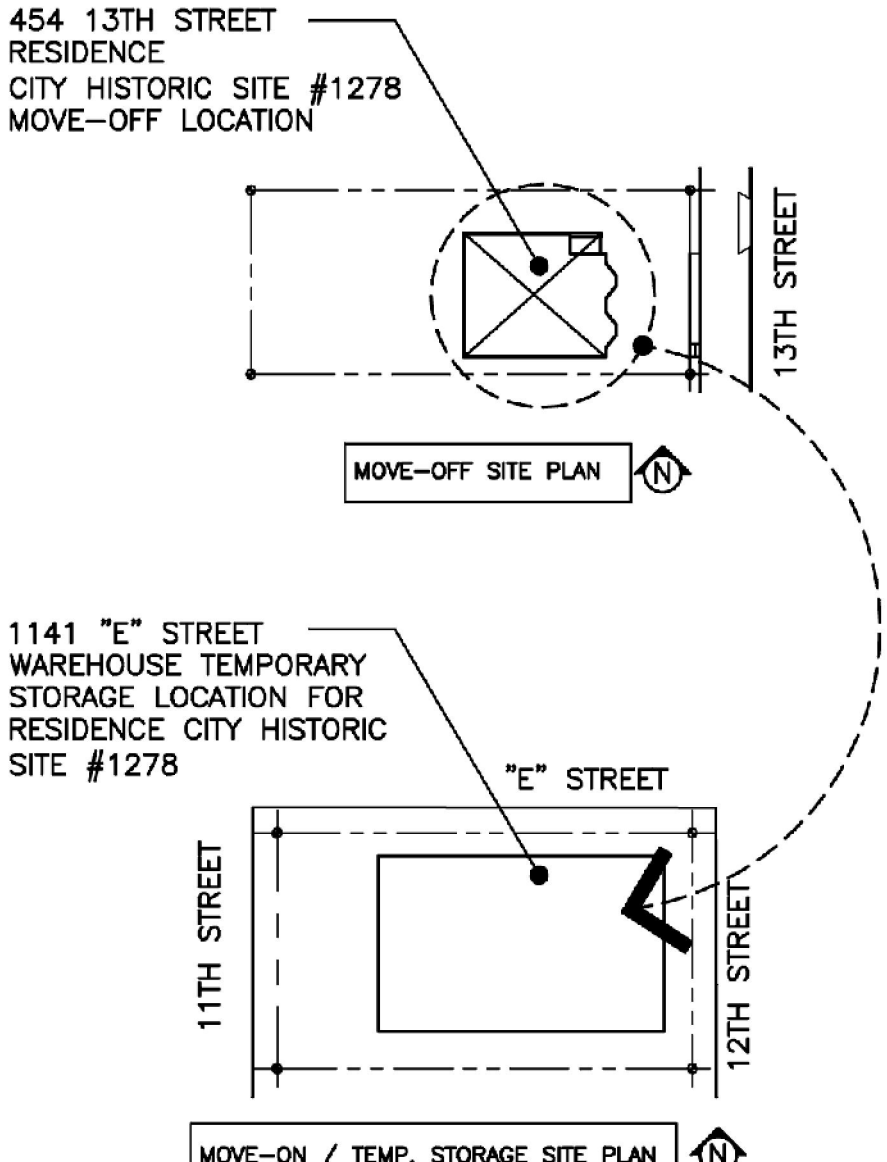


FIG 3.: Move-off site "A" at 454 13th Street. Resource to be moved-off from original site to tempo rary storage at warehouse. Temporary move-on site plan at 1141 E Street.

5 OF 6

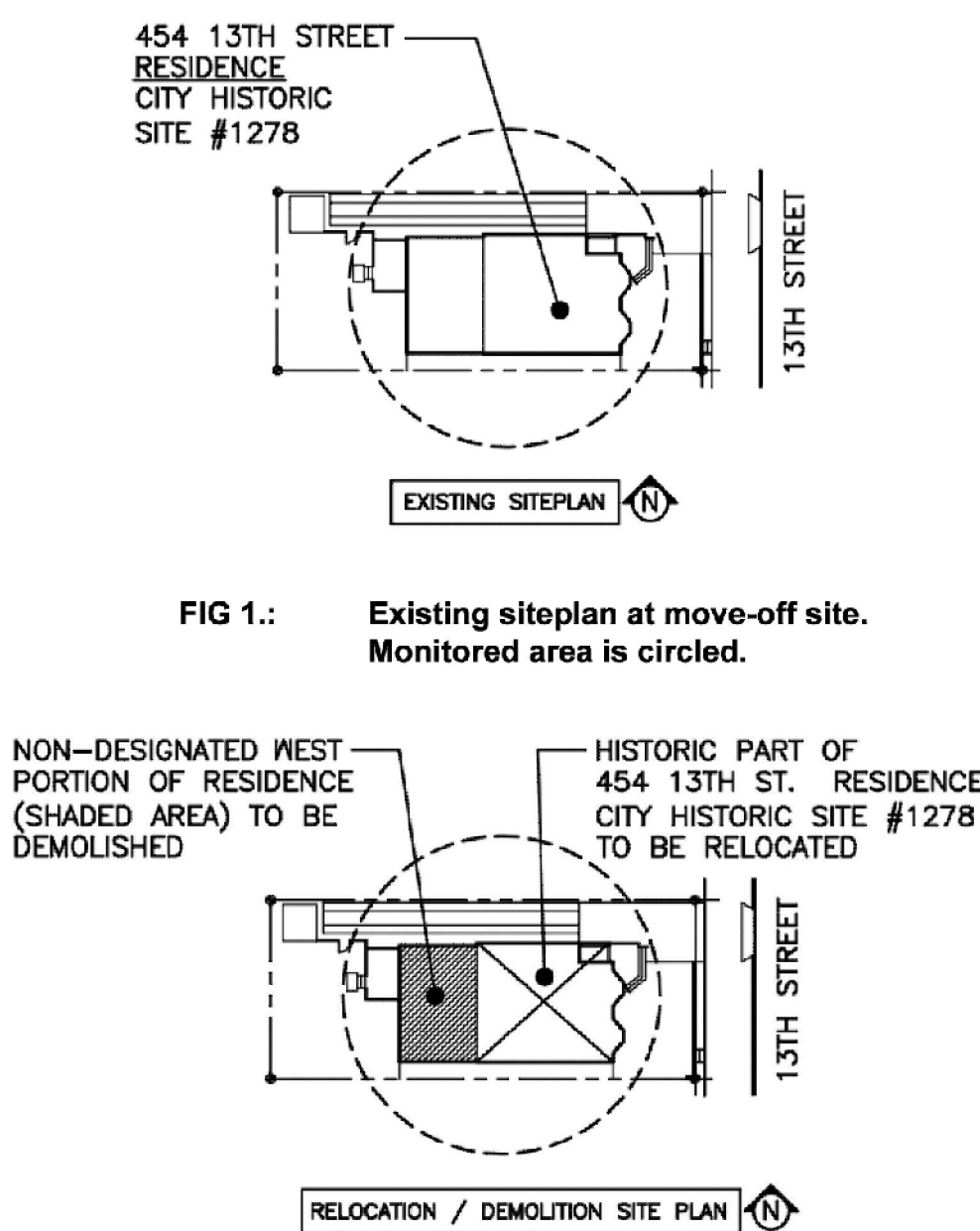


FIG 2.: Relocation / Demolition siteplan. Shaded area to be demolished.

4 OF 6

PROJECT:

JOHN AND MARY WRIGHT HOUSE, 454 13TH AVE.  
CITY OF SAN DIEGO HISTORIC RESOURCE SITE #1278,  
BUILT 1882.

CLIENT: NAKHSHAB DESIGN DEVELOPMENT

340 15TH STREET, SAN DIEGO, CA 92101

ARCHITECT:

UNION ARCHITECTURE , INC.

JOHN H. EISENHART, ARCHITECT  
EVA L.J. THORN, INTERIORS

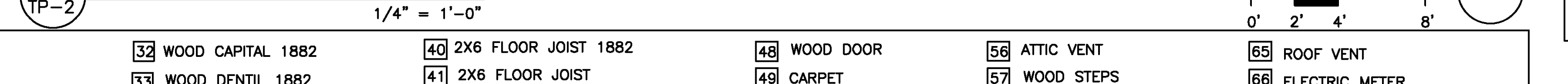
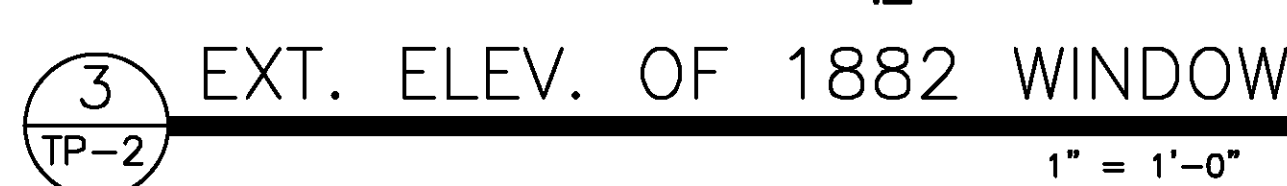
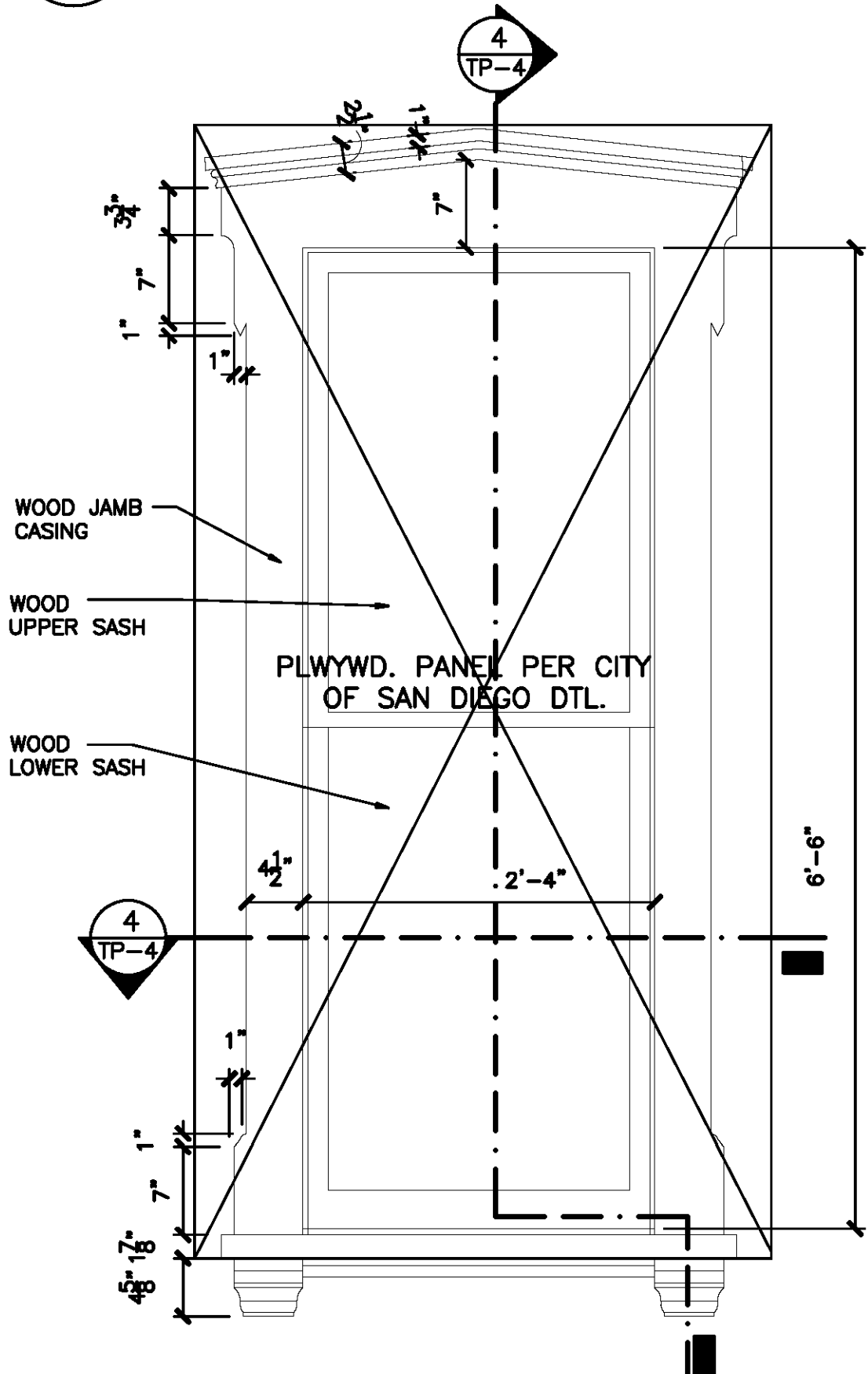
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DATE: 01.19.2018  
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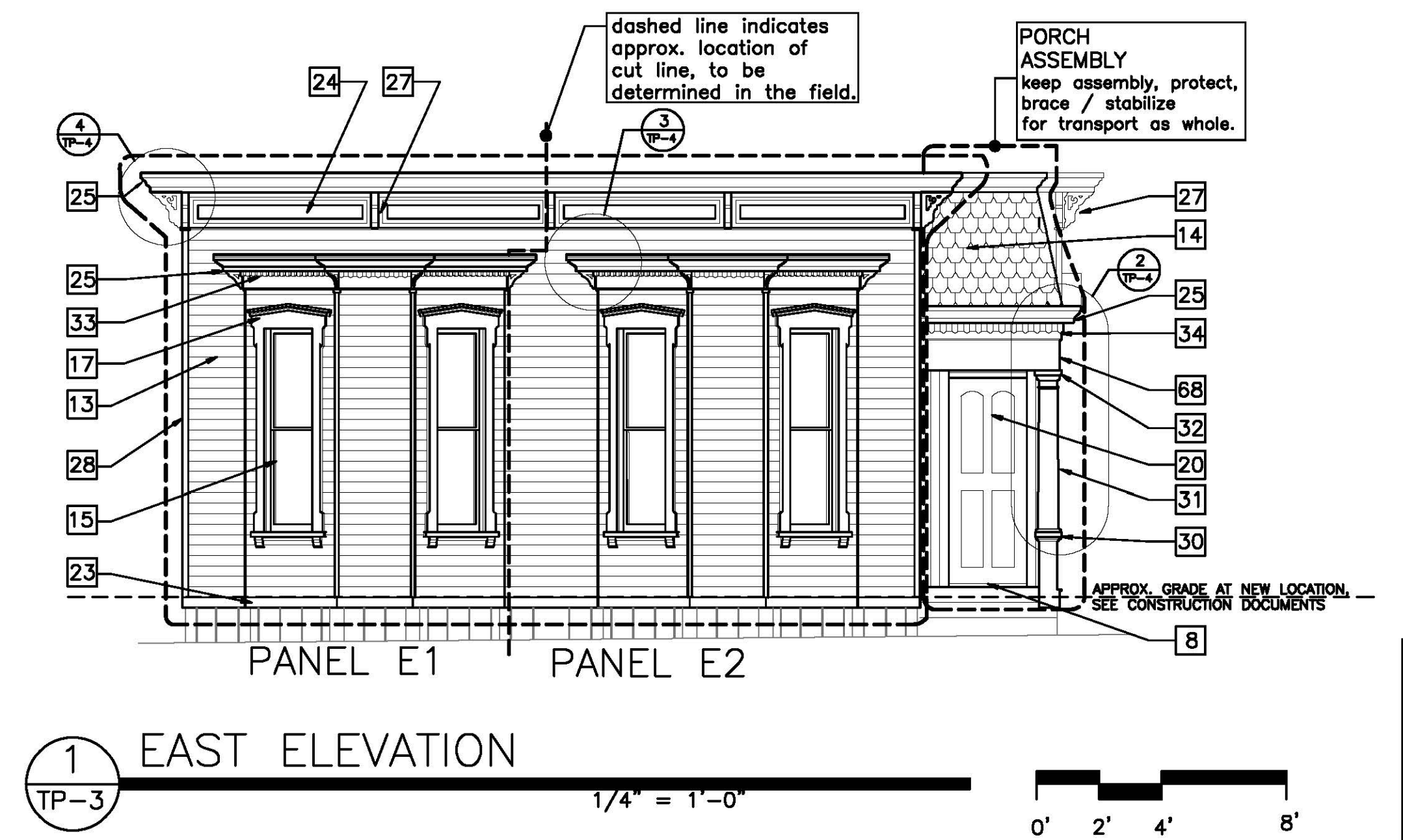
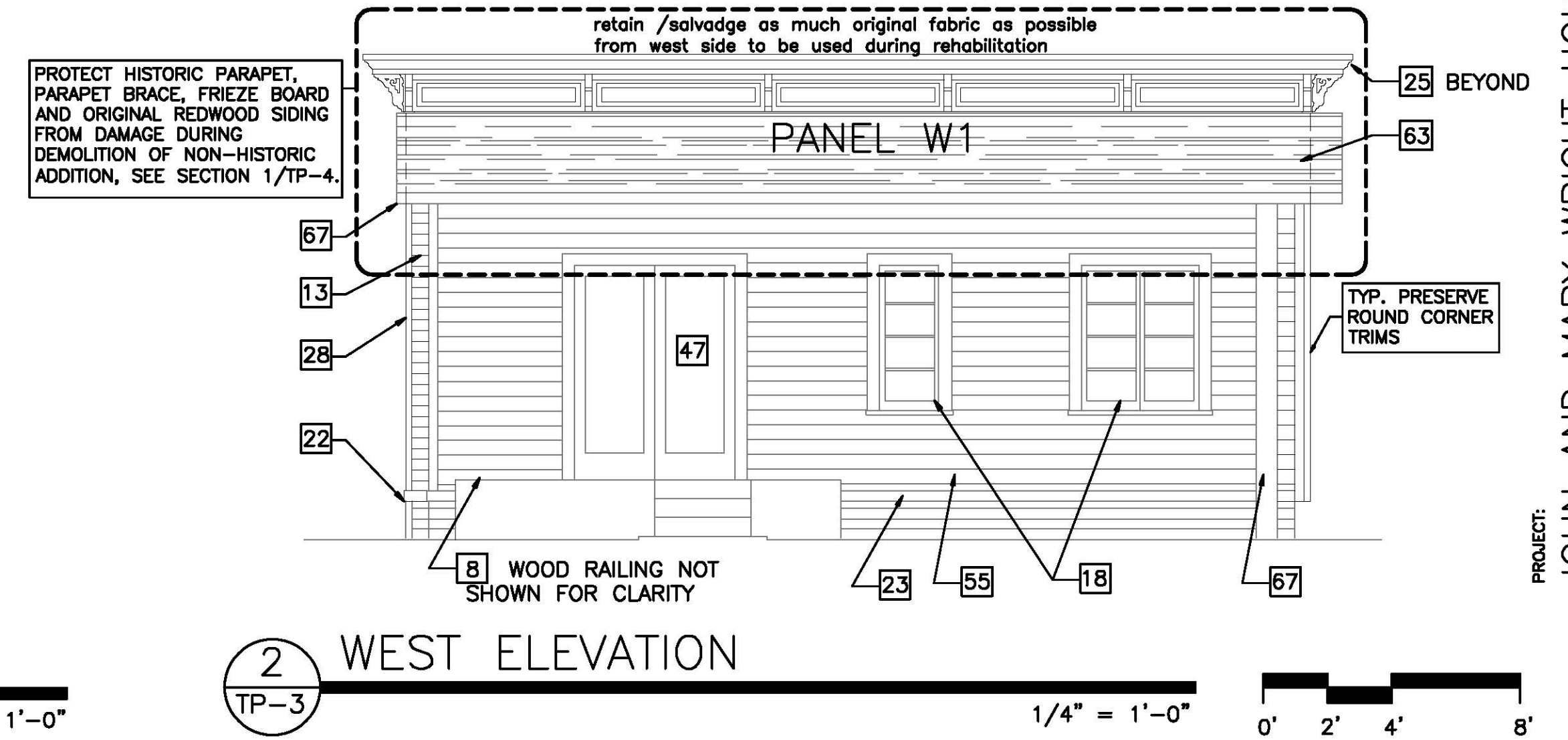
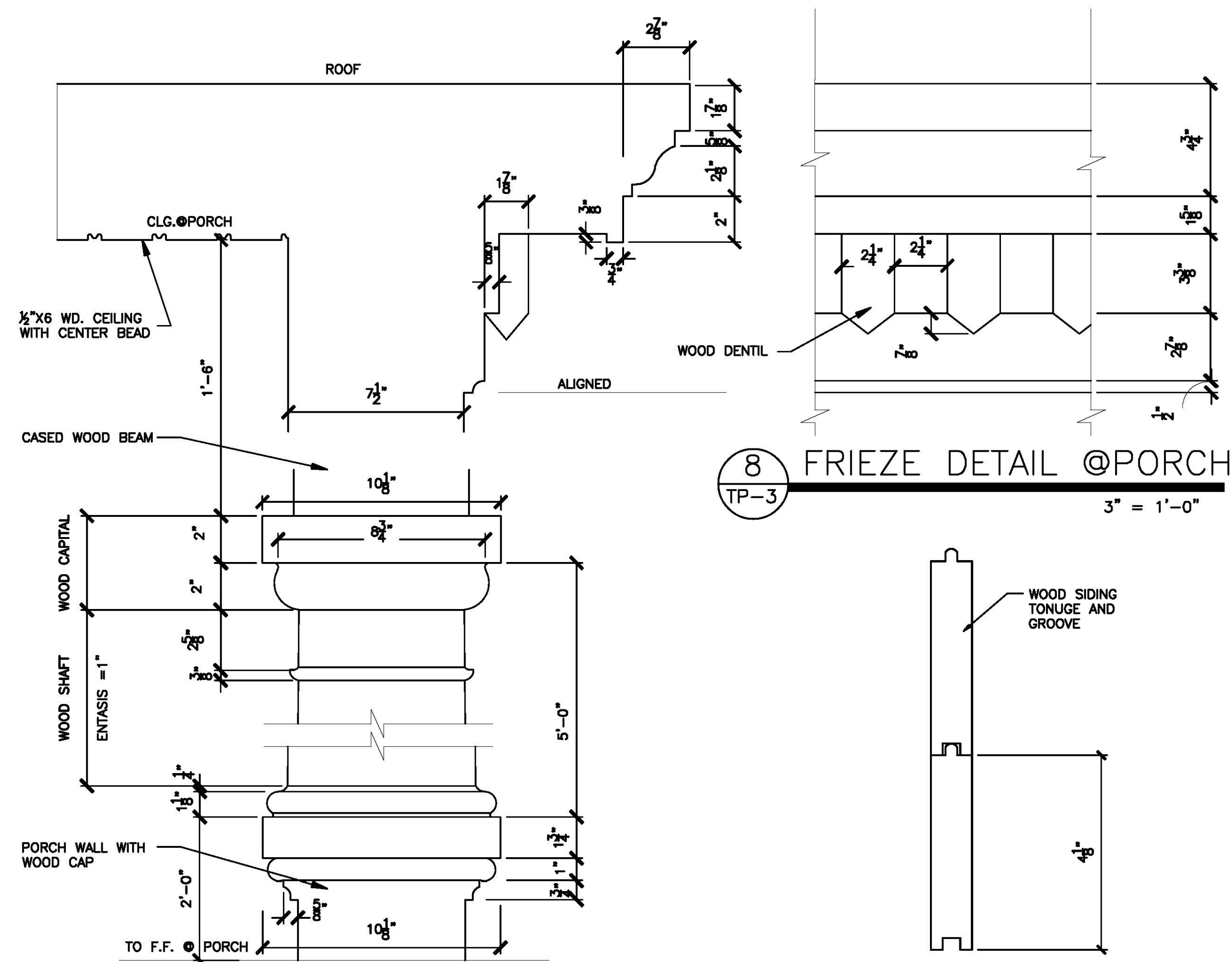
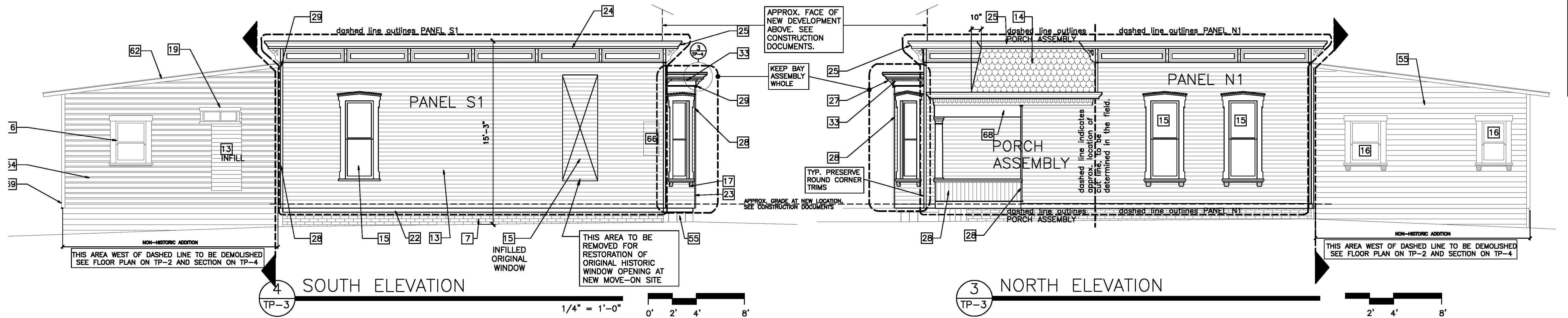
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SHEET 3 OF 6



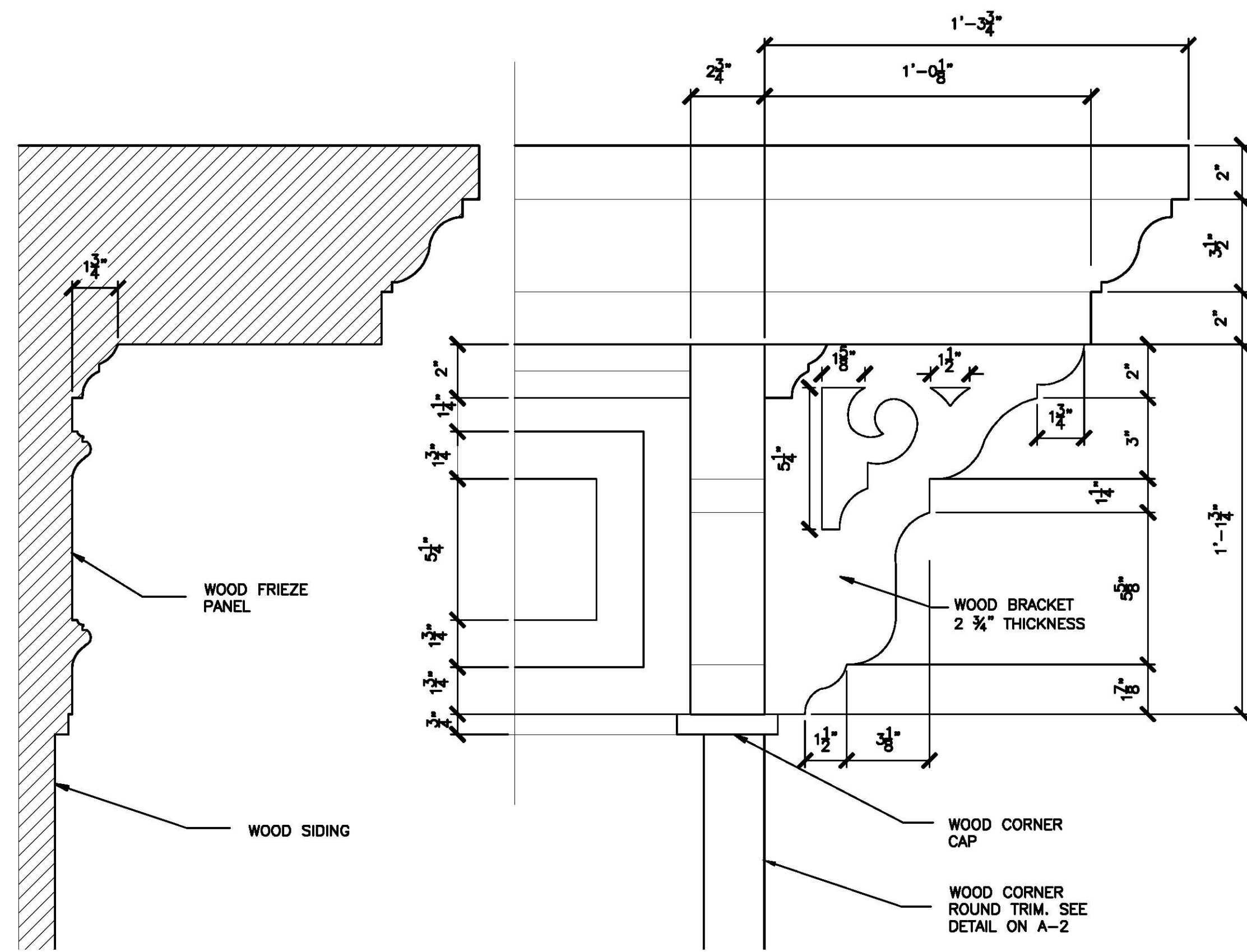
All rights reserved. No part of this drawing may be reproduced in any form without the written permission of Architect.	<b>ARCHITECT:</b>  <b>UNION ARCHITECTURE, INC.</b>  JOHN H. EISENHART, ARCHITECT EVA L.J. THORN, INTERIORS  1530 BROOKES AVENUE, SAN DIEGO, CALIFORNIA 92103 TELEPHONE: 619.269.4841 / WWW.UNIONARCH.COM	<b>PROJECT:</b>  JOHN AND MARY WRIGHT HOUSE, 454 13TH AVE.  CITY OF SAN DIEGO HISTORIC RESOURCE SITE #1278,  BUILT 1882.
		<b>CLIENT:</b>  NAKHSHAB DESIGN DEVELOPMENT 340 13TH STREET, SAN DIEGO, CA 92101
DATE: 01.19.2018 REV: 04.20.2018		
TP-2		
SHEET 4 OF 6		



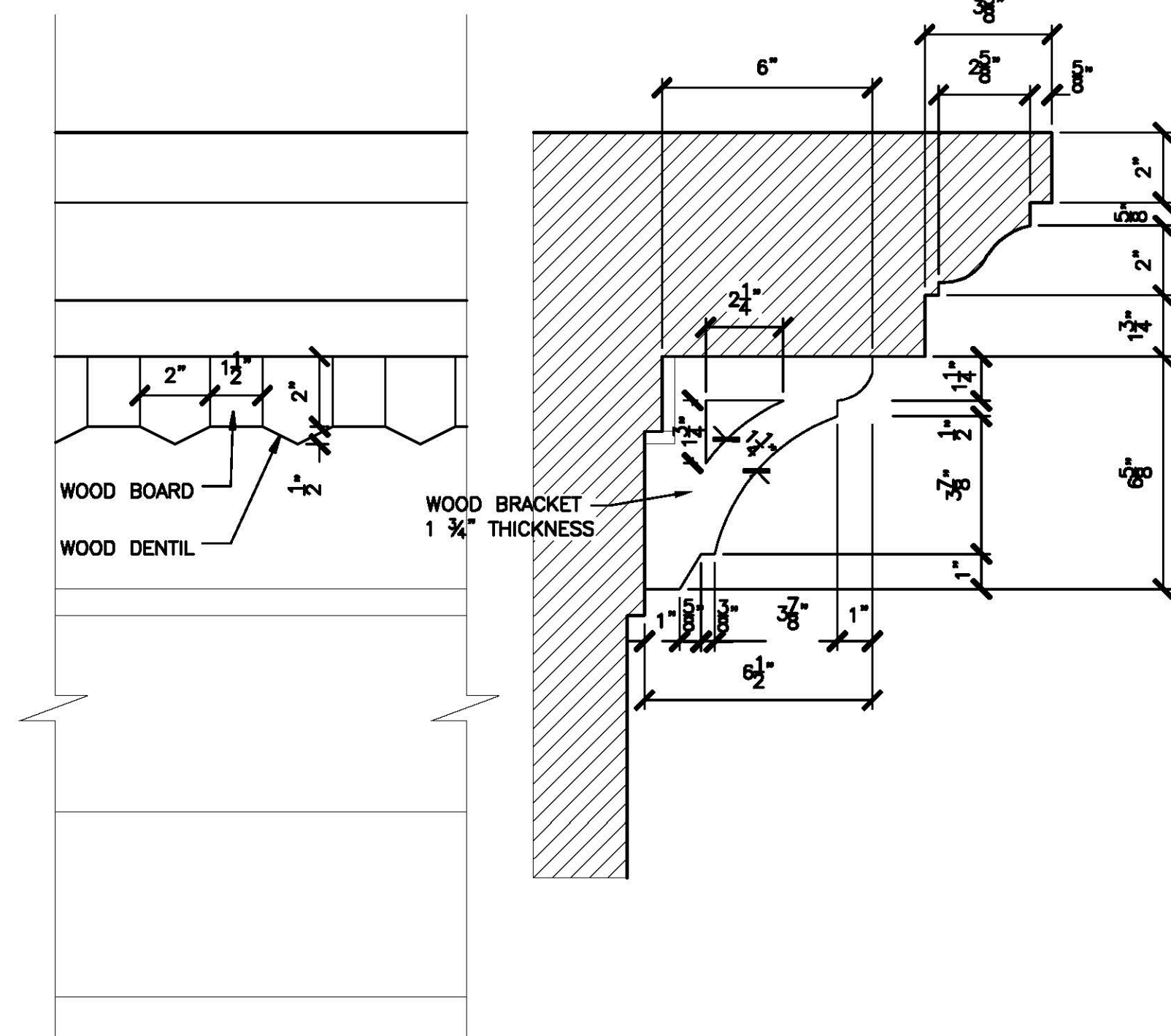


LEGEND			
NOTES			
A. MATERIALS OR CONSTRUCTION FROM PERIOD OF SIGNIFICANCE LABELLED "1882" U.N.O.			
B. NON-PERIOD OF SIGNIFICANCE MATERIALS ARE NOT LABELLED			
1 CONCRETE DRIVE	8 WOOD DECK	16 WOOD D.H. WINDOW	24 FRIEZE PANEL 1882
2 FENCE	9 WOOD RAILING	17 WOOD CASING /TRIM 1882	25 WOOD CORNICE 1882
3 CONCRETE STREET CURB	10 CONCRETE STEPS	18 WOOD CASEMENT	26 WOOD GUTTER 1882
4 BRICK PAVER	11 LANDSCAPE	19 VINYL SLIDER WINDOW	27 WOOD BRACKET 1882
5 WOOD SHED	12 1X VERTICAL WOOD BOARD 1882	20 PANEL DOOR	28 WOOD ROUND CORNER TRIM 1882
6 CONCRETE RETAINING WALL	13 1X SIDING "FLUSH JOINT" 1882	21 TRANSOM WINDOW 1882	29 WOOD ROUND CAP 1882 SEE DETAIL 5 / TP-3
	14 WOOD SHINGLE ROOF 1882	22 1X SKIRT BOARD 1882	30 WOOD PLINTH 1882
	15 WOOD D.H. WINDOW 1882	23 1X SKIRT BOARD	31 WOOD COLUMN 1882. SEE DETAIL 9/TP-3
			32 WOOD CAPITAL 1882
			33 WOOD DENTIL 1882
			34 WOOD DENTIL
			35 WOOD BASEBOARD 1882
			36 OAK FLOORING 2 1/4" WIDTH
			37 2X4 CEILING JOIST 1882
			38 2X4 ROOF RAFTER 1882
			39 WOOD LATH / PLASTER 1882
			40 2X6 FLOOR JOIST 1882
			41 2X6 FLOOR JOIST
			42 1X4 WOOD PLANK 1882
			43 4X4 POST 1882
			44 4X4 BEAM 1882
			45 1X10 WOOD SHEATHING BOARD 1882
			46 TYP. WALL 2X4 @32"O.C. WITH INTERMEDIATE 1X4 MEMBER
			47 WOOD FRENCH DOOR
			48 WOOD DOOR
			49 CARPET
			50 TILE
			51 FIREPLACE
			52 BUILT IN CABINET 1882
			53 BOOKSHELVES
			54 WOOD SHIPLAP SIDING
			55 WOOD SIDING
			56 ATTIC VENT
			57 WOOD STEPS
			58 4'X8' SKYLIGHT
			59 2'X2' SKYLIGHT
			60 VINYL FLOORING
			61 ENGINEERED WOOD FLOOR
			62 WOOD FASCIA RAFTER
			63 SHEET ROLL ROOFING
			64 ROOF SCUPPER
			65 ROOF VENT
			66 ELECTRIC METER
			67 WOOD FRAMED UTIL. CHASE
			68 WOOD BEAM
			69 1X4 WOOD CORNER TRIM
			70 1X6 BEAD BOARD CEILING WITH MIDPOINT BEAD. SEE 9 /TP-3
			71 2X4 STUD WALL 16" O.C.

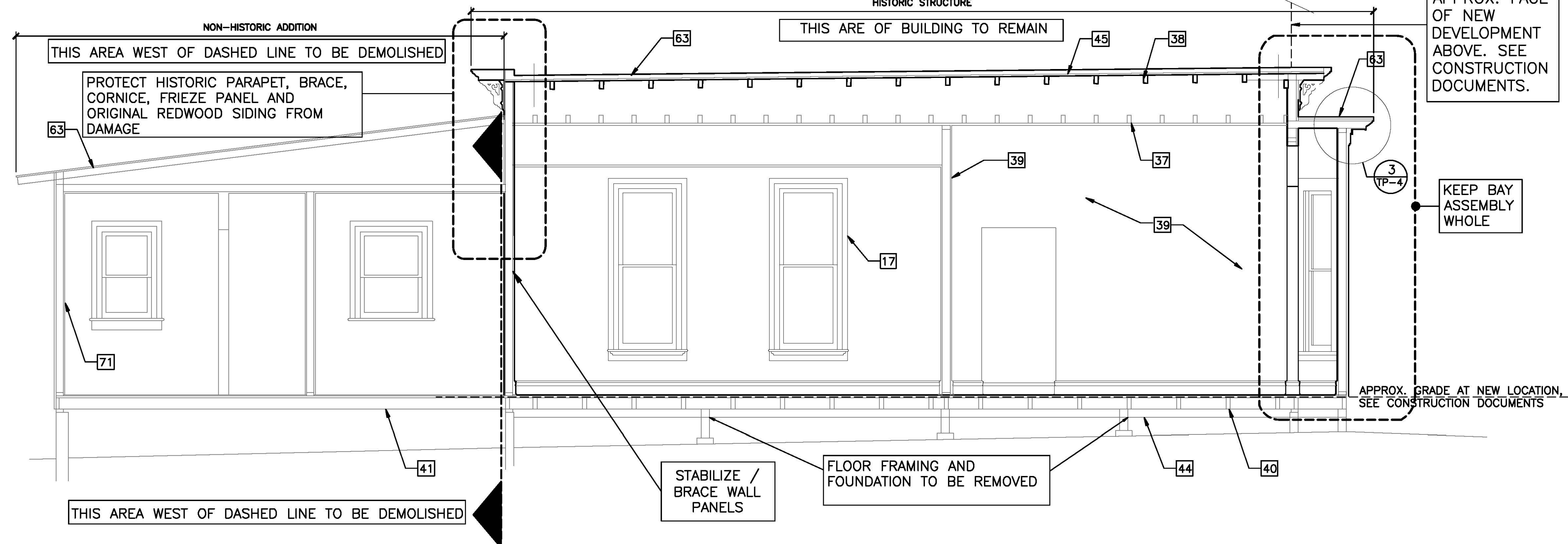




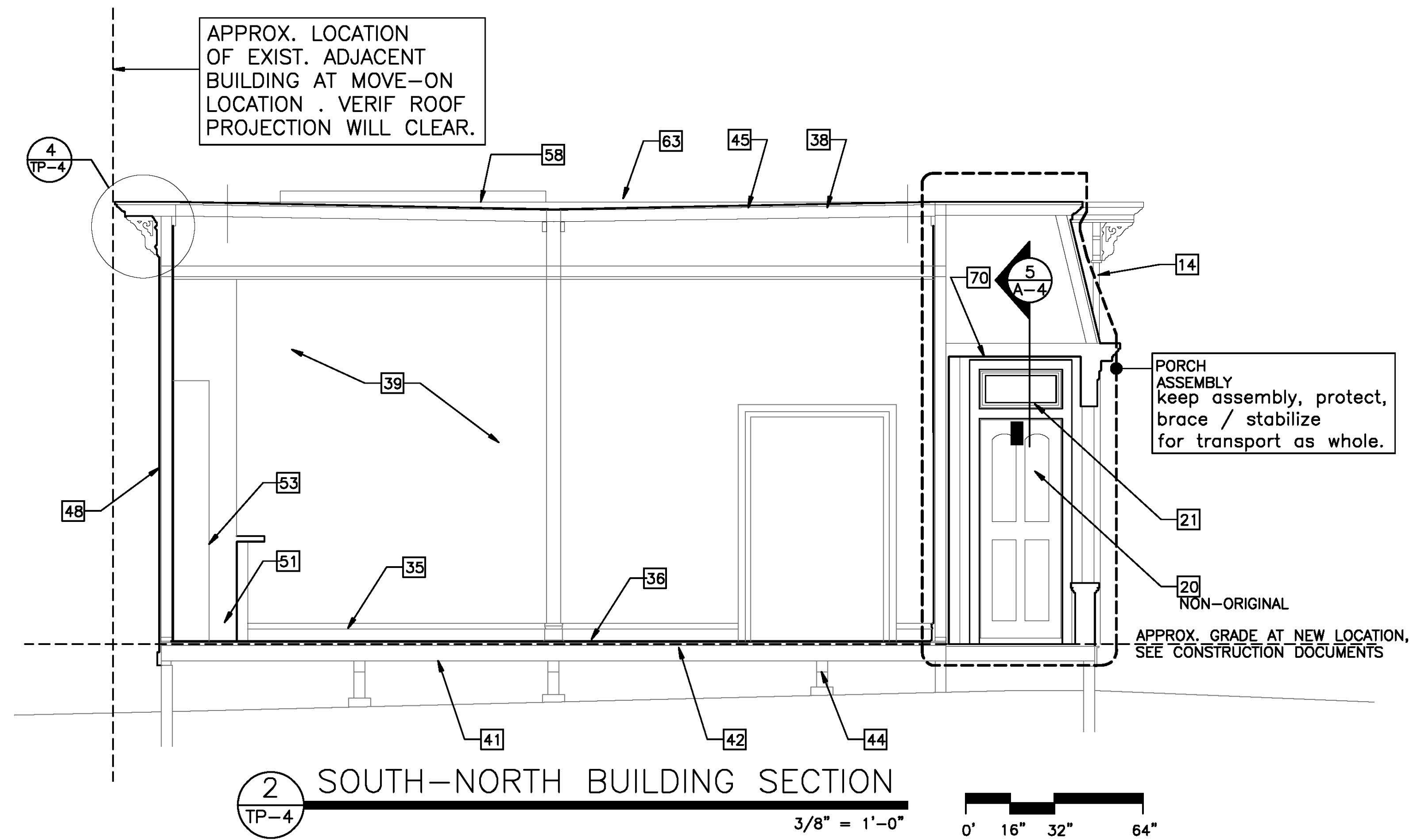
4 TYP. DETAIL. FRIEZE, BRACKET, CORNICE  
3" = 1'-0"



3 DETAIL OF FRIEZE, BRACKET, CORNICE @ BAY  
3" = 1'-0"



1 WEST-EAST BUILDING SECTION  
3/8" = 1'-0"



2 SOUTH-NORTH BUILDING SECTION  
3/8" = 1'-0"

LEGEND									
NOTES									
A. MATERIALS OR CONSTRUCTION FROM PERIOD OF SIGNIFICANCE LABELLED "1882" U.N.O.									
B. NON-PERIOD OF SIGNIFICANCE MATERIALS ARE NOT LABELLED									
1	CONCRETE DRIVE	8	WOOD DECK	16	WOOD D.H. WINDOW	32	WOOD CAPITAL 1882	48	WOOD DOOR
2	FENCE	9	WOOD RAILING	17	WOOD CASING /TRIM 1882	33	WOOD DENTIL 1882	49	CARPET
3	CONCRETE STREET CURB	10	CONCRETE STEPS	18	WOOD CASEMENT	34	WOOD DENTIL	50	TILE
4	BRICK PAVER	11	LANDSCAPE	19	VINYL SLIDER WINDOW	35	WOOD BASEBOARD 1882	51	FIREPLACE
5	WOOD SHED	12	1X VERTICAL WOOD BOARD 1882	20	PANEL DOOR	36	OAK FLOORING 2 1/4" WIDTH	52	BUILT IN CABINET 1882
6	CONCRETE RETAINING WALL	13	1X SIDING "FLUSH JOINT" 1882 SEE DETAIL 7 / TP-3	21	TRANSOM WINDOW 1882 SEE DETAIL 5 /TP-2	37	2X4 CEILING JOIST 1882	53	BOOKSHELVES
7	BRICK FOUNDATION 1882 8 1/4" X 3 7/8" X 2 5/8"	14	WOOD SHINGLE ROOF 1882 SEE 6 /TP-3	22	1X SKIRT BOARD 1882	38	2X4 ROOF RAFTER 1882	54	WOOD SHIPLAP SIDING
		15	WOOD D.H. WINDOW 1882 SEE DETAIL 3 /TP-2	23	1X SKIRT BOARD	39	WOOD LATH / PLASTER 1882	55	WOOD SIDING
				24	FRIEZE PANEL 1882	40	2X6 FLOOR JOIST 1882	56	ATTIC VENT
				25	WOOD CORNICE 1882	41	2X6 FLOOR JOIST	57	WOOD STEPS
				26	WOOD GUTTER 1882	42	1X4 WOOD PLANK 1882	58	4'X8' SKYLIGHT
				27	WOOD BRACKET 1882	43	4X4 POST 1882	59	2'X2' SKYLIGHT
				28	WOOD ROUND CORNER TRIM 1882 SEE DETAIL 5/ TP-3	44	4X4 BEAM 1882	60	VINYL FLOORING
				29	WOOD ROUND CAP 1882 SEE DETAIL 5 / TP-3	45	1X10 WOOD SHEATHING BOARD 1882 TYP. WALL 2X4 @32"O.C. WITH INTERMEDIATE 1X4 MEMBER	61	ENGINEERED WOOD FLOOR
				30	WOOD PLINTH 1882	46		62	WOOD FASCIA RAFTER
				31	WOOD COLUMN 1882. SEE DETAIL 9/TP-3	47	WOOD FRENCH DOOR	63	SHEET ROLL ROOFING
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								70	1X6 BEAD BOARD CEILING WITH MIDPOINT BEAD. SEE 9 /TP-3
								71	2X4 STUD WALL 16" O.C.

PROJECT: JOHN AND MARY WRIGHT HOUSE, 454 13TH AVE. CITY OF SAN DIEGO HISTORIC RESOURCE SITE #1278, BUILT 1882.

CLIENT: NAKHSHAB DESIGN DEVELOPMENT 340 15TH STREET, SAN DIEGO, CA 92101

ARCHITECT: UNION ARCHITECTURE, INC. JOHN H. EISENHART, ARCHITECT EVA L.J. THORN, INTERIORS 1530 BROOKES AVENUE, SAN DIEGO, CALIFORNIA 92103 TELEPHONE: 619.269.4841 / WWW.UNIONARCH.COM

DATE: 01.19.2018 REV: 04.20.2018

TP-4

SHEET 6 OF 6

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**MONITORING PLAN**

**DATE:** May 23, 2018

**PROJECT:** Move off site:  
454 13<sup>th</sup> Street  
City historic resource # 1278  
San Diego, Ca. 92101  
Assessors Parcel# 535-156-08-00

Temp. Storage facility / move-on/off site:  
1141 E Street  
San Diego, CA 92101

Move on site:  
454 13<sup>th</sup> Street  
San Diego, Ca. 92101  
Assessors Parcel# 535-156-08-00

**PROJECT TEAM:**

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PA: Project Architect: NDD  
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HAM: Historic Architect Monitor: John Eisenhart, Union Architecture  
PI: Principal Investigator: Law Office of Marie Burke Lia. Marie Burke Lia Attorney  
CM: Construction Manager: NDD  
HM: House Mover: Joe Hansen, John T. Hansen Enterprises  
BI: Building Inspector: City of San Diego Development Services: Environmental and Historical staff.  
RE: Resident Engineer: NDD

**PROPERTY DESCRIPTION:**

The structure at 454 13<sup>th</sup> Street (John and Mary Wright House) is a historically designated single story example of an Italianate Style residence. Building date is 1882. The main structure is of rectangular form 32'-3" x 24'-2" and has a flat roof with projecting cornice (containing wood gutter), wood brackets, cornice, frieze on perimeter at roof wall junction. The building has two prominent bays on the east façade, the main entry porch on the northeast corner, has a wood shingled mansard roof (with cornice and wood dentils), supported by a round wood column (tuscan style). Exterior materials consist of 1x redwood "flush joint" tongue and groove siding (4 1/8" exposure, painted), one over one wood double hung windows, wood casing and trim.

An addition on the west side of the historic building, containing a kitchen, bathrooms and bedroom, is non-historic (1942).

A wood deck and railing extending the original entry area and steps on east side and a wood deck and steps off of the non-historic addition on the west side are non-original.

A 7 story apartment building with penthouse and basement will be constructed at the site at 454 13<sup>th</sup> Street. To facilitate this development the resource has to be temporarily removed from the site. Once the shell of the new development is completed the resource is to be returned to the site. The new location of the resource will be approximately 4'-0" south and 17'-0" east from it's original location on the same lot, it will be incorporated into the street level commercial space of the new development.

This Treatment Plan is being prepared to dismantle and temporarily remove the historic portion of the building from its current location at 454 13<sup>th</sup> Street (Assessors Parcel# 535-156-08-00). The resource is to be moved to and stored safely at a warehouse at 1141 E Street, San Diego, CA 92101. Approximate moving distance is 1 mile each way.

**Monitoring at Move-Off Site "A" : 454 13<sup>th</sup> Street City, San Diego, Ca. 92101, Assessors Parcel# 535-156-08-00, See area to be monitored figure 1 , 2 and 3 below.**

1. Overview of Treatment Plan and Monitoring Plan (HAM, HA, PI, PA, CM, BI, D, HM).

Issue: Pre-construction meeting as related to historic resource on site. Discuss sequence and type of work to be done prior to move. General methods of protection of structure during demolition work of non-historic additions to be discussed.

2. Preparation of resource for moving (HAM, HA, CM).

Issue: Monitor to be present prior to any disassembly of structure. Location marks of cuts by HM to be determined, general method of disassembly and support to be discussed and approved by HAM per Treatment Plan.

3. Final review of preparation of resource for moving (HAM, HA, CM, HM).

Issue: Monitor to review after completion of the following work: Removal of exterior plumbing, electrical lines. Monitor to take inventory of wood shingles, braces, windows and doors to be salvaged per Treatment Plan. Bracing and protection of structure prior to move off date.

Per Treatment Plan the historic structure shall be dismantled into approximately five pieces. Mansard roof and porch as well as bays shall be braced as one piece. All pieces shall be labeled / catalogued. To be reviewed by HAM.

The historic resources will be moved to site "B" Storage Facility at 1141 "E" Street.

**Monitoring at Move-Off Site "B" Storage Facility at 1141 "E" Street. See area to be monitored figure 4 below.**

4. Move-off storage site (HAM,CM).

Issue: Review work involved by CM to brace and protect structure for storage duration.

5. Continuing monitoring of structures at storage facility (HAM).

Issue: Review protection of resource every 3 months or as required if damage occurs, issue report documenting the damage.

6. Pre-construction meeting move-on site. (HAM, HA, PI, CM, BI, D)

Issue: Overview of Treatment Plan, Architectural, Landscaping and Engineering Documents as related to move-on site. Review work involved by CM to prepare site for arrival of structure.

7. New foundation, utilities, site preparation for move on (HAM, HA, CM, HM)

Issue: Review of work on site to assure work will properly receive move-on of resource.

**Monitoring at Move-Off Site “B” Storage Facility at 1141 “E” Street to Move-On Site “A” at 454 13<sup>th</sup> Street.**

8. Move-on site (HAM, HA, CM, BI).

Issue: Review move-on site with resource present. Overview of Treatment Plan for rehabilitation of resource, Architectural, Landscaping and Engineering Documents.

9. Move-on site as required by construction activity (HAM, HA, CM).

Issue: Review rehabilitation of resource in accordance with Treatment Plan and Architectural, Landscaping and Engineering Documents.

10. Final Monitoring (HAM, HA, CM, D).

Issue: Final punch list of items to complete according Treatment Plan and Architectural, Landscaping and Engineering Documents.

11. Draft Report (HAM, BI).

Issue: Draft report of monitor process to be submitted to BI for review.

12. Final Report (HAM, BI, PI, D).

Issue: Final report of monitoring process, submit to PI for distribution to City of San Diego Developmental Services Department, San Diego History Center for archiving.