



THE CITY OF SAN DIEGO

## Report to the Historical Resources Board

DATE ISSUED: July 9, 2020 REPORT NO. HRB-20-033

HEARING DATE: July 23, 2020

SUBJECT: **ITEM #11 – 9G Site Development Permit/Neighborhood Development Permit/Conditional Use Permit/Tentative Map No. 649856**

RESOURCE INFO: [California Historical Resources Inventory Database \(CHRID\) link](#)

APPLICANT: Cisterra Partners, LLC, represented by Paul Thometz

LOCATION: 659 Ninth avenue (south side of G Street between Ninth and Tenth avenues), Downtown Community Plan Area, Council District 3, APN 535-136-01-00

DESCRIPTION: Recommend to the Planning Commission adoption of the mitigation measures and findings associated with the Site Development Permit (SDP) as presented or recommend inclusion of additional permit conditions related to a designated historical resource.

### STAFF RECOMMENDATION

Recommend to the Planning Commission approval of the findings and mitigation measures associated with the SDP related to the designated resource located at 659 Ninth Avenue (HRB Site No. 1355, the Pacific Telephone and Telegraph Company Garage) as presented.

### BACKGROUND

San Diego Municipal Code (SDMC) Section 126.0503(b)(2) requires a recommendation from the Historical Resources Board (HRB) prior to the Planning Commission decision on a SDP when a historical district or designated historical resource is present. The HRB has adopted the following procedure for making recommendations to decision-makers (Historical Resources Board Procedures, Section II.B):

When the HRB is taking action on a recommendation to a decision- maker, the Board shall make a recommendation on only those aspects of the matter that relate to the historical aspects of the project. The Board's recommendation action(s) shall relate to the cultural resources section, recommendations, findings and mitigation measures of the final environmental document, the SDP findings for historical purposes, and/or the project's compliance with the Secretary of the Interior's Standards for Treatment of Historic Properties. If the Board desires to recommend the inclusion of additional conditions, the motion should

include a request for staff to incorporate permit conditions to capture the Board's recommendations when the project moves forward to the decision maker.

The Project proposes a substantial alteration of the Pacific Telephone and Telegraph Company Garage (HRB Site No. 1355) ("Resource"), constructed in 1932 in the Art Deco style. The concrete building spans the length of G Street between 9<sup>th</sup> and 10<sup>th</sup> Avenues. A small, one-story addition constructed in 1946 is located on 9<sup>th</sup> Avenue and exhibits similar architectural characteristics as the main building. Although simplistic in design, the building is a good example of the Art Deco style for downtown San Diego.

The site was reviewed by the HRB and designated as HRB Site #1355 on November 21, 2019. The property was designated under Criterion A with a period of significance of 1932-1956 as a special element of the City's economic development, revealing the increasing importance of the automobile as America's favored method of private transportation from 1900-1956. Specifically, the Resource which was originally constructed for use as a garage, continues to convey the significant character defining features of automobile-related development as identified in the East Village Historical Context Statement, including a single story, warehouse-like massing, limited ornamentation, raised vertical projections and numerous garage doors. The Resource was also designated under Criterion C with a period of significance of 1932 as a good example of the Art Deco style. Specifically, the Resource exhibits smooth wall surfaces, a flat roof, vertical projections above the roofline, fluted pilasters, geometric detailing and metal windows. The designation includes the 1946 addition constructed within the period of significance.

The Applicant proposes to demolish the interior of the Resource, retain the existing historic façade, and construct a 21-story, 253-foot tall mixed-use development above it, comprised of 241 dwelling units. The interior of the historic structure on the ground floor, as well as one new basement level, will contain a combined approximately 34,000 square feet of commercial retail space. Directly above the Resource is proposed to be five parking garage levels. The full development plans are included as Attachment 2.

The Project is located within the East Village neighborhood of the Downtown Community Plan (DCP) area. The East Village neighborhood is anticipated to be a residential and mixed-use community upon build-out. Currently, the neighborhood consists of a mix of commercial, warehouse, light industrial, educational, and residential uses; mostly at low intensities and densities. Ultimately, East Village is projected to grow to 46,000 residents. Various portions of East Village will have substantially different character, contributing to the eclecticism and interest of this neighborhood. The Northwest sub-district of this neighborhood is envisioned as Downtown's residential core that will yield Downtown's highest-intensity residential-emphasis district. Northwest's many opportunity sites, location at the heart of Downtown, accessibility to transit, and distance from the airport overflight zone make it ideal for high-intensity building.

The DCP establishes the strategy for preservation of historical resources as part of Downtown's continued development. Historic buildings are identified under a three-tiered system based on their classification. The Pacific Telephone and Telegraph Company Garage is a locally listed property. The following are some key applicable DCP Goals and Policies for the Project:

- 6.5-G-6: Develop Northwest as the most intensive residential area in concert with its



- central location, transit access, and available redevelopment sites.
- 6.5-G-8: Reinforce Northwest's proximity to Downtown destinations as an essential component of its character.
- 9.1-G-1: Protect historical resources to communicate Downtown's heritage.
- 9.1-G-2: Encourage the rehabilitation and reuse of historical resources.
- 9.2-G-1: Integrate historical resources into the Downtown fabric while achieving policies for significant development and population intensification.
- 9.2-P-1: Incorporate elements of historical buildings in new projects to impart heritage.
- 9.2-P-3: Promote the adaptive reuse of intact buildings (designated or not) and/or significant elements, as a cultural and suitability goal.

## ANALYSIS

The redevelopment of the site cannot be determined to be consistent with the Secretary of the Interior's Standards (Standards) due to the loss of original materials on the historic structure, including the removal of the 1946 addition, and the massing, size, scale and proportion of the proposed tower. Therefore, the proposed redevelopment and reuse of the Resource is, by definition, a substantial alteration requiring an SDP, consistent with SDMC Section 143.0251. Specific SDP Supplemental Findings are required for projects proposing substantial alterations to a designated historical resource or within a historical district, including findings that require analysis of alternatives that could minimize the potential adverse effects on the Resource.

The required SDP Supplemental Findings regarding the Project's proposed substantial alteration to the Pacific Telephone and Telegraph Company Garage and supporting information are below. The Applicant-submitted Draft SDP findings are included as Attachment 5.

### **1. There are no feasible measures, including a less environmentally damaging alternative, that can further minimize the potential adverse effects on the designated historical resource or historical district;**

The Resource was designated because it was deemed to be a special element of the City's economic development as an example of America's emerging automobile industry in the first half of the 20<sup>th</sup> century. The Resource was also designated for its retention of character defining features of the Art Deco style, including its smooth wall surfaces, flat roof, vertical projections above the roofline, fluted pilasters, geometric detailing, and metal windows.

The proposed Project (Base Project) retains the existing façade of the Resource, adds a basement level below and 21 levels above the Resource, including five levels of parking, with no setback from the existing facade. Modifications to the existing façade that cannot be determined to be consistent with the Standards include removal of the existing roof, the slab on grade, the 1946 addition, and a portion of the façade on Tenth Avenue for a driveway. In addition, doors will be added on two facades and storefront infill will be added at existing garage openings (Attachment 2, Sheets 26-28). The construction of the 21 levels above the Resource also cannot be determined to be consistent with the Standards due to the proposed massing, size, scale and proportion of the tower. The Applicant retained the London Moeder Advisors (LMA) to conduct an economic analysis (Attachment 4) to evaluate five alternatives for potential feasible measures to lessen the impact of the Project on the

Resource. The most important variables studied in the alternative analysis were setback of the tower from the façade of the Resource and height; however, all alternatives retained either the whole Resource or its original facades. The following five alternatives were evaluated for their respective Internal Rate of Return (IRR) and Yield of Cost (YOC) versus that of the Base Project, which is summarized in the table below:

Alternative	Historic Structure	Additions	Setbacks from Existing Facade	Dwelling Units	Retail Square Footage	Parking Spaces
<b>Base Project</b>	<b>Retain existing facade</b>	<b>1 basement level, 5 parking levels, 16 residential levels</b>	<b>None</b>	<b>241</b>	<b>34,000</b>	<b>276</b>
1	Rehabilitate existing structure	None	N/A	0	22,000	0
2	Retain existing facade	1 basement level, 1 parking level (open air)	N/A	0	34,000	34
3	Retain existing facade	1 basement level, 5 parking levels, 10 residential levels	15' on each street	155	34,000	189
3B	Retain existing facade	1 basement level, 5 parking levels, 16 residential levels	15' on each street	227	34,000	189
4	Retain existing facade	5 subterranean parking levels, 1 extra retail level, 11 residential levels	5' on level above existing façade, 15' for tower	168	34,000	202

The LMA analysis concluded that the three alternatives that both retain the existing historic façade and include construction above the resource with setbacks are not economically viable due to their failure to meet the minimum IRR and YOC needed to achieve project financing (see #3 below). Therefore, it was determined that the base project, which includes no setbacks and a twenty-one story tower, was the project that best balanced economic feasibility with adverse impacts to the Resource.

**2. The deviation is the minimum necessary to afford relief and accommodate the development and all feasible measures to mitigate for the loss of any portion of the historical resource have been provided by the applicant; and**

While the use of this site for any other purpose other than its historic use (warehousing, which is not permitted by current zoning) will likely result in substantial alterations to the historical resource, the proposed Project will take steps to mitigate this impact. Historical resource mitigation measures have been developed and adopted within the Downtown Final Environmental Impact Report (Downtown FEIR), with which the Project has been evaluated against and deemed consistent (Attachment 3). The Mitigation Monitoring and Reporting Program (MMRP) for the Downtown FEIR requires the implementation of a documentation program submitted to City Historic Resources Division staff for review and approval, a pre-construction meeting, implementation of the Treatment Plan and monitoring to ensure appropriate execution of the plan. The Project's Treatment Plan is included as Attachment 6 and the Monitoring Plan is included as Attachment 7. Additionally, the Resource was documented through the measured drawings and photography in Attachment 9 consistent with the Historical American Building Survey (HABS). A copy of this documentation will be archived with the City and other depositories as outlined in the MMRP.

The proposed Project rehabilitates the existing façade and minimizes the need for modifications to the historic façade, utilizing existing openings and maintaining the character-defining architectural features called out in the designation. The full scope of the modifications made to the existing façade can be seen in the Treatment Plan (Attachment 6). The most significant alteration to the existing façade itself is the removal of the 1946 addition Ninth Avenue (Attachment 2, Sheet 26) and the alteration to the 1932 façade on 10<sup>th</sup> Avenue (Attachment 2, Sheet 27). These modifications are a result of the need for vehicular access for the site and the constraints of the presence of the original 1932 structure and the relatively small lot size, limiting the available locations for driveways. Each driveway is one-way and the minimum width allowed per City standards—the minimum necessary deviation to accommodate access for the development.

The construction of the 21 levels above the Resource contributes to the loss of historic context; however, the economic analysis concluded that the Base Project was the only economically feasible alternative. In the Base Project, while no setbacks are provided between the existing façade and the new construction above, the design of the new building incorporates a horizontal distinction between the historic façade and new construction through the use of materials and color. The first 4'-10" to 7'-11" (depending on height of parapet) of new wall above the Resource is behind the face of the existing façade (one foot) (Attachment 2, Sheet 30) and is comprised of dark gray metal panels intended to create negative space to help the Resource appear as an independent component that is separate from the new construction above comprised of angled metal panels and spandrel glazing (Attachment 2, Sheets 40-41). The lack of setbacks allows for the maximization of buildable floor area on the relatively small lot, which allows for the number of dwelling units and parking spaces to make the Project economically feasible project, as demonstrated in the LMA analysis, while at the same time retaining the Resource on-site consistent with the DCP Goals and Policies. Therefore, the Project is designed with the minimum necessary deviation to afford relief to and accommodate the project and reasonable, feasible measures to mitigate the impact to the Resource are proposed.

**3. The denial of the proposed development would result in economic hardship to the owner. For purposes of this finding, “economic hardship” means there is no reasonable beneficial use of the property and it is not feasible to derive a reasonable economic return from the property.**

The LMA analysis used the Internal Rate of Return (IRR) and Yield of Cost (YOC) as measures to determine the economic feasibility of each alternative. As stated in the report (Attachment 4, page 5), the typical IRR for rental housing project ranges from 13-15%; anything less would struggle to attract investors and achieve project financing. The YOC spread over existing cap rates is required to be 1.5% for redevelopment; therefore, if cap rates are approximately 4% for residential projects and 6% for retail projects, the targeted YOC is 5.5% and 7.5%, respectively, for a project to be economically feasible and to qualify for project financing. The table below summarizes the conclusions of the LMA analysis for each alternative:

Alternative	IRR Min: 13-15%	YOC Max: 5.5-7.5%
<b>Base</b>	<b>16.5%</b>	<b>5.6%</b>
1	9.2%	3.9%
2	-4.2%	3.4%
3	7.2%	4.7%
3B	10.2%	4.7%
4	-6%	3.9%

The LMA Analysis concluded that, in order to retain the Resource, only the Base Project is economically feasible, as the five alternatives exhibit fail to meet the minimum IRRs and YOCs to successfully attract investors and achieve project financing. Therefore, denial of the Project would result in economic hardship to the owner because there is no reasonable beneficial use of the property that does not require complete redevelopment in order to derive a reasonable economic return from the property.

The Project was submitted to the City’s Economic Development Department (EDD) for a requested peer review as has been done on some previous SDPs; however, due to current workloads and programs associated with the COVID-19 pandemic, EDD was unable to perform the review. Urban Division staff compared the LMA analysis to previous economic alternatives analysis for Downtown projects and found the assumptions consistent with those analyses.

City Staff from the Urban Division and Historic Resources Division believe that there is sufficient evidence to support the SDP Supplemental Findings related to the designated historic resource. In addition, Staff believes that the proposed mitigation measures of the MMRP (Attachment 3) and draft permit conditions (Attachment 1) are sufficient to reduce the identified impacts to the Pacific Telephone and Telegraph Company Garage.

## CONCLUSION

Staff recommends that the HRB recommend to the Planning Commission adoption of the mitigation measures and findings associated with the SDP related to the designated historical resource.



James Alexander  
Program Manager  
Urban Division



Suzanne Segur  
Senior Planner  
Development Services Department

Attachment(s):

1. Draft Permit
2. Development Plans
3. Downtown FEIR Consistency Evaluation & MMRP
4. Applicant-submitted LMA Economic Analysis
5. Applicant-submitted Draft SDP Findings
6. Historic Treatment Plan
7. Historic Monitoring Plan
8. Historic Technical Report
9. Historic American Building Survey (HABS) documents

**RECORDING REQUESTED BY**  
CITY OF SAN DIEGO  
URBAN DIVISION  
MAIL STATION 501

**WHEN RECORDED MAIL TO**  
PROJECT MANAGEMENT  
PERMIT CLERK  
MAIL STATION 501

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INTERNAL ORDER NUMBER: 24008439 SPACE ABOVE THIS LINE FOR RECORDER'S USE

SITE DEVELOPMENT PERMIT/NEIGHBORHOOD DEVELOPMENT PERMIT/  
TENTATIVE MAP/CONDITIONAL USE PERMIT

**9G - PROJECT NO. 649856**

PLANNING COMMISSION

This Site Development Permit/Neighborhood Development Permit/Tentative Map/Conditional Use Permit is granted by the Planning Commission of the City of San Diego ("City") to 4 Lee Tenth, LLC, Owner, and Cisterra Partners, LLC, Permittee, pursuant to San Diego Municipal Code (SDMC) Section 126.0505, 126.0404, 125.0440, and 126.0305, to allow 1) the substantial alteration of a historical resource, 2) five deviations from the development regulations of the SDMC, 3) the creation of 241 residential condominium dwelling units (DU), and off-site alcoholic beverage sales for 9G ("Project"). The approximately 25,000 square-foot (SF) site is located at 659 Ninth Avenue (south side of G street between Ninth and Tenth avenues) in the East Village neighborhood of the Downtown Community Plan (DCP) area and within the Centre City Planned District. The project site is legally described as Lots A, B, C, K, and L in Block 83 of Horton's Addition in the City of San Diego, County of San Diego, State of California, according to map thereof on file in the Office of the County Recorder of San Diego County.

Subject to the terms and conditions set forth in this Permit, permission is granted to the Owner and Permittee to construct and operate a development and uses as described and identified by size, dimension, quantity, type, and location on the approved exhibits (Exhibit "A") dated [INSERT Approval Date] , on file in the Development Services Department (DSD).

The Project shall include:

- a. Construction of a 22-story, 253-foot tall mixed-use development, totaling approximately 432,175 SF and comprised of 241 dwelling units, approximately 35,800 SF of commercial retail space on the ground floor and in one basement level, and five levels of above-grade parking containing 273 parking spaces.
- b. **Site Development Permit (SDP):** Substantial alterations to Historical Resources Board (HRB) Site No. 1355, the Pacific Telephone and Telegraph Company Garage, including demolition of the interior and maintenance of the existing façade.

- c. **Neighborhood Development Permit (NDP):** Five deviations from the SDMC as followings:
1. Sec. 131.0454 – Personal Storage: Reduce the number of personal storage areas from 100% of DU (241 DU) to 21% of DU (51 DU).
  2. Sec. 142.0560(j) – Driveway & Access Regulations: Reduce the minimum width for the one-way driveway on Ninth Avenue width from 14 feet to 12 feet.
  3. Sec. 156.0310(d)(3)(A) – Tower Lot Coverage: Increase the maximum tower lot coverage from 50% to 66%.
  4. Sec. 156.0310(d)(3)(B) – Tower Floor Plate: Increase the maximum east-west tower floor plate dimension from 130 feet to 150 feet.
  5. Sec. 156.0311(d)(1) – Transparency: Reduce the minimum ground level transparency of the Tenth avenue building façade from 60% to 40%.
- d. **Tentative Map (TM):** The creation of 241 residential condominium DU in the Project.
- e. **Conditional Use Permit (CUP):** Off-site alcoholic beverage sales in the ground floor and basement level commercial retail space.
- f. Public and private accessory improvements determined by DSD to be consistent with the land use and development standards for this site in accordance with the adopted community plan, the California Environmental Quality Act [CEQA] and the CEQA Guidelines, the City Engineer's requirements, zoning regulations, conditions of this Permit, and any other applicable regulations of the SDMC.

**STANDARD REQUIREMENTS:**

1. This permit must be utilized within thirty-six (36) months after the date on which all rights of appeal have expired. If this permit is not utilized in accordance with Chapter 12, Article 6, Division 1 of the SDMC within the 36-month period, this permit shall be void unless an Extension of Time has been granted. Any such Extension of Time must meet all SDMC requirements and applicable guidelines in effect at the time the extension is considered by the appropriate decision maker. This permit must be utilized by [INSERT DATE HERE].
2. The utilization of this CUP is contingent upon the approval of a license to sell alcohol at this location by the California Department of Alcoholic Beverage Control (ABC). The issuance of this CUP does not guarantee that the ABC will grant an alcoholic beverage license for this location.
3. No permit for the construction, occupancy, or operation of any facility or improvement described herein shall be granted, nor shall any activity authorized by this Permit be conducted on the premises until:
  - a. The Owner/Permittee signs and returns the Permit to DSD; and
  - b. The Permit is recorded in the Office of the San Diego County Recorder.

4. While this Permit is in effect, the subject property shall be used only for the purposes and under the terms and conditions set forth in this Permit unless otherwise authorized by the appropriate City decision maker.
5. This Permit is a covenant running with the subject property and all of the requirements and conditions of this Permit and related documents shall be binding upon the Owner/Permittee and any successor(s) in interest.
6. The continued use of this Permit shall be subject to the regulations of this and any other applicable governmental agency.
7. Issuance of this Permit by the City does not authorize the Owner/Permittee for this Permit to violate any Federal, State or City laws, ordinances, regulations or policies including, but not limited to, the Endangered Species Act of 1973 [ESA] and any amendments thereto (16 U.S.C. § 1531 et seq.).
8. The Owner/Permittee shall secure all necessary building permits. The Owner/Permittee is informed that to secure these permits, substantial building modifications and site improvements may be required to comply with applicable building, fire, mechanical, and plumbing codes, and State and Federal disability access laws.
9. Construction plans shall be in substantial conformity to Exhibit "A." Changes, modifications, or alterations to the construction plans are prohibited unless appropriate application(s) or amendment(s) to this Permit have been granted.
10. All of the conditions contained in this Permit have been considered and were determined necessary to make the findings required for approval of this Permit. The Permit holder is required to comply with each and every condition in order to maintain the entitlements that are granted by this Permit.
11. If any condition of this Permit, on a legal challenge by the Owner/Permittee of this Permit, is found or held by a court of competent jurisdiction to be invalid, unenforceable, or unreasonable, this Permit shall be void. However, in such an event, the Owner/Permittee shall have the right, by paying applicable processing fees, to bring a request for a new permit without the "invalid" condition(s) back to the discretionary body which approved the Permit for a determination by that body as to whether all of the findings necessary for the issuance of the proposed permit can still be made in the absence of the "invalid" condition(s). Such hearing shall be a hearing de novo, and the discretionary body shall have the absolute right to approve, disapprove, or modify the proposed permit and the condition(s) contained therein.
12. The Owner/Permittee shall defend, indemnify, and hold harmless the City, its agents, officers, and employees from any and all claims, actions, proceedings, damages, judgments, or costs, including attorney's fees, against the City or its agents, officers, or employees, relating to the issuance of this permit including, but not limited to, any action to attack, set aside, void, challenge, or annul this development approval and any environmental document or decision. The City will promptly notify Owner/Permittee of any claim, action, or proceeding and, if the City should fail to



cooperate fully in the defense, the Owner/Permittee shall not thereafter be responsible to defend, indemnify, and hold harmless the City or its agents, officers, and employees. The City may elect to conduct its own defense, participate in its own defense, or obtain independent legal counsel in defense of any claim related to this indemnification. In the event of such election, Owner/Permittee shall pay all of the costs related thereto, including without limitation reasonable attorney's fees and costs. In the event of a disagreement between the City and Owner/Permittee regarding litigation issues, the City shall have the authority to control the litigation and make litigation related decisions, including, but not limited to, settlement or other disposition of the matter. However, the Owner/Permittee shall not be required to pay or perform any settlement unless such settlement is approved by Owner/Permittee.

13. Development Impact Fees: The development will be subject to Centre City Development Impact Fees. The fee shall be determined in accordance with the fee schedule in effect at the time of building permit issuance. The Owner/Permittee shall provide to the City's Facilities Financing Department the following information at the time of application for building permit plan check: 1) total square footage for commercial lease spaces and all areas within the building dedicated to support those commercial spaces including, but not limited to: loading areas, service areas and corridors, utility rooms, and commercial parking areas; and 2) applicable floor plans showing those areas outlined for verification. In addition, it shall be responsibility of the Owner/Permittee to provide all necessary documentation for receiving any "credit" for existing buildings to be removed. Development Impact Fees shall be calculated in accordance with the fee schedule in effect at the time of building permit issuance and with the SDMC.

14. While this Permit is in effect, the subject property shall be used only for the purposes and under the terms and conditions set forth in this Permit unless otherwise authorized by the appropriate City decision maker.

15. The continued use of this Permit shall be subject to the regulations of this and any other applicable governmental agency.

16. Issuance of this Permit by City does not authorize the Owner/Permittee for this Permit to violate any Federal, State or City laws, ordinances, regulations or policies, including, but not limited to, the Endangered Species Act of 1973 [ESA] and any amendments thereto (16 U.S.C. §1531 et seq.).

17. This Permit is a covenant running with the subject property and all of the requirements and conditions of this Permit and related documents shall be binding upon the Owner/Permittee and any successor(s) in interest.

18. This development shall comply with the standards, policies, and requirements in effect at the time of approval of this development, including any successor(s) or new policies, financing mechanisms, phasing schedules, plans and ordinances adopted by the City.

19. No permit for construction, operation, or occupancy of any facility or improvement described herein shall be granted, nor shall any activity authorized by this Permit be conducted on the premises until this Permit is recorded in the Office of the San Diego County Recorder.

20. Owner/Permittee shall secure all necessary building permits. The Owner/Permittee is informed that to secure these permits, substantial building modifications and site improvements may be required to comply with applicable building, fire, mechanical, and plumbing codes, and State and Federal disability access laws.

21. Construction plans shall be in substantial conformity to the approved Basic Concept/Schematic Drawings on file at the City. Changes, modifications, or alterations to the construction plans are prohibited unless appropriate application(s)/amendment(s) to the Project have been granted.

**ENVIRONMENTAL/MITIGATION REQUIREMENTS:**

22. As required by SDMC Sec. 156.03049h), the development shall comply with all applicable MMRP measures from the 2006 Downtown Final Environmental Impact Report (Downtown FEIR) for the DCP to the satisfaction of DSD and the City Engineer. Prior to issuance of any construction permit, all conditions of the MMRP shall be adhered to, to the satisfaction of the City Engineer. All mitigation measures described in the MMRP shall be implemented for the following issue areas:

AQ-B.1-1; HIST-A.1-1; HIST-A.1-2; HIST-B.1-1; PAL-A.1-1; NOI-B.1-1; NOI-C.1-1; TRF-A.1.1-2

**CLIMATE ACTION PLAN REQUIREMENTS:**

23. Owner/Permittee shall comply with the Climate Action Plan (CAP) Consistency Checklist stamped as Exhibit "A." Prior to issuance of any construction permit, all CAP strategies shall be noted within the first three (3) sheets of the construction plans under the heading "Climate Action Plan Requirements" and shall be enforced and implemented to the satisfaction of DSD, including:

- a. Cool/Green Roofs: Roofing materials with a minimum three-year aged solar reflection and thermal emittance or solar reflection index equal to or greater than the values specified in the voluntary measures under California Green Building Standards Code (CAL Green).
- b. Plumbing Fixtures & Fittings:
  - i. Residential:
    - 1. Kitchen faucets: Maximum flow rate not to exceed 1.5 gallons per minute at 60 PSI;
    - 2. Standard dishwashers: 4.25 gallons per cycle;
    - 3. Compact dishwashers: 3.5 gallons per cycle;
    - 4. Clothes washers: Water factor of six gallons per cubic feet of drum capacity.
  - ii. Nonresidential:
    - 1. Plumbing fixtures and fittings that do not exceed the maximum flow rate specified in Table A5.303.2.3.1 (voluntary measures) of CAL Green.
    - 2. Appliances and fixtures for commercial applications that meet the provisions of Section A5.303.3 (voluntary measures) of CAL Green.
- c. Electric Vehicle Charging: Of the total required listed cabinets, boxes, or enclosures, 50% have the necessary electric vehicle supply equipment installed to provide active electric vehicle charging stations ready for use.

- d. Bicycle Parking Spaces: Project provides more short- and long-term bicycle parking spaces than required in the SDMC.
- e. Shower Facilities: The Project includes changing/shower facilities in accordance with the voluntary measures under CAL Green for the nonresidential component.
- f. Designated Parking Spaces: The Project provides designated parking for a combination of low-emitting, fuel-efficient, and carpool/vanpool vehicles.

**AFFORDABLE HOUSING REQUIREMENTS:**

24. Prior to receiving the first building permit, Owner/Permittee shall comply with the provisions of Chapter 14, Article 2, Division 13 of the SDMC (Including Affordable Housing Regulations) by performing one of the following, at the Owner/Permittee's sole election, and to the extent permitted by applicable law:

- a. Payment to the City of the full Inclusionary Affordable Housing Fee based upon the aggregate square footage of all residential units in the project, on terms set forth within the Inclusionary Affordable Housing Regulations; or
- b. Provide evidence to the San Diego Housing Commission, in the form of executed Exemption Agreements and Public Entity Agreements, as referenced with Part IV of the Inclusionary Affordable Housing Implementation and Monitoring Procedures Manual ("Procedures Manual") approved by the City Council, demonstrating that the Owner/Permittee is exempt from the payment of the Inclusionary Affordable Housing Fee based upon SDMC Section 142.1303(f) because the Owner/Permittee is receiving specific regulatory incentives and/or concessions from a public agency that result in identifiable, financially sufficient, and actual project cost reductions, including, but not limited to, expedited permit processing provided through the Affordable/In-Fill Housing and Sustainable Building Expedite Program and/or the approval of a deviation, waiver, or reduction of development standards or regulations (development incentives) and because the Owner/Permittee is voluntarily restricting rental units in exchange for such development incentives, as provided for within Part IV of the Procedures Manual; or
- c. Provide evidence to the San Diego Housing Commission, in the form of executed Exemption Agreements and Public Entity Agreements, as referenced with Part IV of the Inclusionary Affordable Housing Implementation and Monitoring Procedures Manual ("Procedures Manual") approved by the City Council, demonstrating that the Owner/Permittee is exempt from the payment of the Inclusionary Affordable Housing Fee based upon SDMC Section 142.1303(g) because the Owner/Permittee is voluntarily pursuing and receiving tax credits, multi-family housing bonds, below market interest rate government agency loans, and/or grants to facilitate the construction of the development.

**AIRPORT REQUIREMENTS:**

25. The Owner/Permittee shall comply with conditions established by the City Airport Approach Overlay Zone (and any successor or amendment thereto) which were approved at the Airport Land Use Commission (ALUC) meeting on March 5, 2020. The ALUC Board made the determination that the project is conditionally consistent with the San Diego International Airport Land Use Compatibility Plan (ALUCP). The Applicant shall comply with the following ALUC conditions:

- a. The structure and construction crane shall be marked and lighted in accordance Federal Aviation Administration (FAA) procedures.
- b. An aviation easement for the building height shall be recorded with the County Recorder.
- c. The ALUCP requires that a means of overflight notification be provided for new residential land uses. In instances when an aviation easement is required, the overflight notification requirement is satisfied.

**ENGINEERING REQUIREMENTS:**

26. The Site Development Permit shall comply with all Conditions of the Final Map for the Tentative Map No. 2365584.

27. The Tentative Map shall comply with the conditions of Site Development Permit No. 2358005.

28. The following will be conditions of the Tentative Map Resolution that the subdivider will need to satisfy/assure before the Final Map is recorded.

29. The Subdivider shall assure, by permit and bond, the removal of existing driveways, and replace it with curb, gutter and sidewalk per City Standard, adjacent to the site on 9th Avenue, G street and 10th Avenue, satisfactory to the City Engineer.

30. The Subdivider shall assure, by permit and bond, the reconstruction of the existing curb with curb and gutter per current City Standards, adjacent to the site on 9th Avenue, G street and 10th Avenue, satisfactory to the City Engineer.

31. The Subdivider shall assure, by permit and bond, to reconstruct the sidewalk with current City Standard sidewalk, maintaining the existing sidewalk scoring pattern and preserving the contractor's stamp, adjacent to the site on 9th Avenue, G street and 10th Avenue, satisfactory to the City Engineer.

32. The Subdivider shall assure, by permit and bond, to reconstruct the two existing curb ramps at the corner of 9th Avenue/G Street and 10th Avenue/G Street, with current City Standard curb ramp Standard Drawing SDG-130 and SDG-132 with Detectable/Tactile Warning Tile, satisfactory to the City Engineer.

33. The Subdivider shall obtain an Encroachment Maintenance Removal Agreement, from the City Engineer, for the sidewalk underdrain/curb outlet and proposed private improvements of any kind, including enhanced pavers, street trees and landscaping to be installed within the in the public right-of-way (ROW).
34. The Subdivider shall obtain a Grading Permit for the shoring proposed for this project, satisfactory to the City Engineer.
35. The Subdivider shall incorporate any construction Best Management Practices necessary to comply with Chapter 14, Article 2, Division 1 (Grading Regulations) of the SDMC, into the construction plans or specifications.
36. The Subdivider shall submit a Water Pollution Control Plan (WPCP). The WPCP shall be prepared in accordance with the guidelines in Part 2 Construction BMP Standards Chapter 4 of the City's Storm Water Standards.
37. The drainage system proposed for this development, as shown on the site plan, is subject to approval by the City Engineer.
38. The Subdivider shall enter into a Maintenance Agreement for the ongoing permanent BMP maintenance, satisfactory to the City Engineer.
39. The Subdivider shall submit a Technical Report that will be subject to final review and approval by the City Engineer, based on the Storm Water Standards in effect at the time of the construction permit issuance.

**GEOLOGY REQUIREMENTS:**

40. The Owner/Permittee shall submit a geotechnical investigation report or update letter that specifically addresses the proposed construction plans. The geotechnical investigation report or update letter shall be reviewed for adequacy by the Geology Section of DSD prior to issuance of any construction permits.
41. The Owner/Permittee shall submit an as-graded geotechnical report prepared in accordance with the City's "Guidelines for Geotechnical Reports" following completion of the grading. The as-graded geotechnical report shall be reviewed for adequacy by the Geology section of DSD prior to exoneration of the bond and grading permit close-out.

**HISTORICAL RESOURCES REQUIREMENTS:**

42. The Project shall comply with all applicable Historical Resources MMRP measures from the 2006 Downtown FEIR for the DCP as applicable, including HIST-A.1-2
43. Prior to the issuance of any construction permits, the Owner/Permittee shall submit drawings that incorporate the Treatment Plan as approved by HRB and City Historical Resources Staff on July 23, 2020.

44. Prior to issuance of any construction permits, the Historic American Building Survey (HABS) documentation as approved by HRB and City Historical Resources Staff on July 23, 2020 shall be submitted for archival storage with the City of San Diego HRB, South Coastal Information Center, the California Room of the City of San Diego Public Library, the San Diego Historical Society, and/or other historical society or group(s).

45. During construction of the Project, the Owner/Permittee shall implement the Monitoring Plan as approved by HRB and City Historical Resources staff on July 23, 2020. The Project's Principal Investigator shall send monitoring reports as described in the Monitoring Plan to the City's Mitigation Monitoring staff and Historical Resources staff. The Principal Investigator may submit a detailed letter to City staff prior to the start of work or during construction requesting a modification to the Monitoring Plan. This request shall be based on relevant information and site conditions.

**LANDSCAPE REQUIREMENTS:**

46. Prior to issuance of any grading permit, the Owner/Permittee shall submit complete construction documents for the revegetation and hydro-seeding of all disturbed land in accordance with the City Landscape Standards, Storm Water Design Manual, and to the satisfaction of DSD. All plans shall be in substantial conformance to this permit (including Environmental conditions) and Exhibit "A," on file in DSD.

47. Prior to issuance of any public improvement permit, the Owner/Permittee shall submit complete landscape construction documents for ROW improvements to DSD for approval. Improvement plans shall show, label, and dimension a 40-square-foot area around each tree which is unencumbered by utilities. Driveways, utilities, drains, water and sewer laterals shall be designed so as not to prohibit the placement of street trees.

48. Prior to issuance of any building permit (including shell), the Owner/Permittee shall submit complete landscape and irrigation construction documents, which are consistent with the Landscape Standards, to DSD for approval. The construction documents shall be in substantial conformance with Exhibit "A," Landscape Development Plan, on file in DSD. Construction plans shall provide a 40-square-foot area around each tree that is unencumbered by hardscape and utilities unless otherwise approved per §142.0403(b)5.

49. The Owner/Permittee shall be responsible for the maintenance of all landscape improvements shown on the approved plans, including in the ROW, unless long-term maintenance of said landscaping will be the responsibility of another entity approved by DSD. All required landscape shall be maintained consistent with the Landscape Standards in a disease, weed, and litter free condition at all times. Severe pruning or "topping" of trees is not permitted.

50. If any required landscape (including existing or new plantings, hardscape, landscape features, etc.) indicated on the approved construction documents is damaged or removed, the Owner/Permittee shall repair and/or replace in kind and equivalent size per the approved documents to the satisfaction of DSD within 30 days of damage or Certificate of Occupancy.

**MAP CHECK REQUIREMENTS:**

51. Prior to the expiration of the tentative map (TM), if approved, a Parcel or Final map to consolidate and subdivide the properties into 241 shall be recorded at the Office of the San Diego County Recorder.
52. Prior to the recordation of the map, taxes must be paid or bonded for the property pursuant to Section 66492 of the Subdivision map Act. A current original tax certificate, recorded in the Office of the San Diego County Recorder must be provided to satisfy this condition. If a tax bond is required as indicated in the tax certificate, ensure that it is paid or posted at the County Clerk of the Board of Supervisors Office and supply proof prior to the recordation of the map.
53. The map shall be based on field survey and all lot corners must be marked with durable survey monuments pursuant to SDMC Section 144.0311(d) and Subdivision Map Action Section 66495. All survey monuments shall be set prior to the recordation of the map, unless the setting of monuments is deemed impractical due to the proposed improvements and/or grading associated with the Project, in which case, delayed monumentation may be applied on the map in accordance with SDMC Section 144.0130.
54. All subdivision maps in the City are required to be tied to the California Coordinate system of 1983 (CCS83), Zone 6 pursuant to Section 8801-8819 of the California Public Resources Code.
55. The map shall:
- a. Use the California Coordinate system for its "Basis of Bearings" and express all measured and calculated bearing values in terms of said system. The angle of grid divergence from a true meridian (theta or mapping angle) and the north point of said map shall appear on each sheet thereof. Establishment of said Basis of Bearings may be by use of existing Horizontal Control stations or astronomic observations.
  - b. Show two measured ties from the boundary of the map to existing Horizontal Control stations having California Coordinate values of First Order accuracy. These tie lines to the existing control shall be shown in relation to the California Coordinate System (i.e. grid bearings and grid distances). All other distances shown on the map are to be shown as ground distances. A combined factor for the conversion of grid-to-ground shall be shown on the map.

**PLANNING/DESIGN REQUIREMENTS:**

56. Floor Area Ratio (FAR): The Project achieves a FAR of 11.14 through the following FAR bonuses to increase the Project FAR above the Base Maximum of 6.0 pursuant to SDMC: 1) three-bedroom units, 2) LEED silver certification, 3) FAR Payment Program.
57. Parking: 36 parking spaces shall be provided in the Project for the commercial space and no on-site parking is required for the residential DU. The Project proposes 243 parking spaces. The parking spaces shall be designed to City standards, except as permitted in SDMC Sec. 156.0313(k):

- a. The off-street parking spaces shall consist only of unbundled parking.
- b. The number of accessible off-street parking spaces shall be provided in accordance with Title 24 of the California Code of Regulations (California Building Standards Code).
- c. The number of off-street electric vehicle charging spaces shall be provided in accordance with the California Green Building Standards Code.
- d. One motorcycle parking space shall be provided for every 20 required vehicle spaces for the commercial space and for every ten parking spaces provided for the residential DU, or 22 spaces with the 243 parking spaces as proposed.

58. Bicycle Parking: Secured bicycle storage shall be provided to accommodate a minimum of 49 bicycles (one bicycle for every five DU and for every 20 required vehicle spaces for the commercial space). Bicycle storage areas shall be within a secured enclosure with access restricted to authorized persons and provide devices for the locking of individual bicycles.

59. Urban Design Standards: The Project, including its architectural design concepts and off-site improvements, shall be consistent with the CCPDO and Centre City Streetscape Manual (CCSM). These standards, together with the following specific conditions, will be used as a basis for evaluating the development through all stages of the development process.

60. Architectural Standards: The architecture of the development shall establish a high quality of design and complement the design and character of the East Village neighborhood as shown in the approved Basic Concept/Schematic Drawings on file with the City. The development shall utilize a coordinated color scheme consistent with the approved Basic Concept Drawings.

61. Form and Scale: The development shall consist of a 22-story mixed-use development (approximately 253 feet tall) measured to the top of the roofline, with roof equipment enclosures, elevator penthouses, and mechanical screening above this height permitted per the CCPDO and the FAA. All building elements shall be complementary in form, scale, and architectural style.

62. Building Materials: All building materials shall be of a high quality as shown in the Basic Concept/Schematic Drawings and approved materials board. All materials and installation shall exhibit high-quality design, detailing, and construction execution to create a durable and high-quality finish. The base of the buildings shall be clad in upgraded materials and carry down to within one inch of finish sidewalk grade, as illustrated in the approved Basic Concept/Schematic Drawings. Any graffiti coatings shall be extended the full height of the upgraded base materials or up to a natural design break such a cornice line. All downspouts, exhaust caps, and other additive elements shall be superior grade for urban locations, carefully composed to reinforce the architectural design. Reflectivity of the glass shall be the minimum reflectivity required by Title 24 of the California Code of Regulations (Title 24). All construction details shall be of the highest standard, as shown in the approved Basic Concept/Schematic Drawings on file with the City, and executed to minimize weathering, eliminate staining, and not cause deterioration of materials on adjacent properties or the ROW. No materials/colors substitutions shall be permitted without prior written City consent.



63. **Street Level Design:** Street level windows shall be clear glass and may be lightly tinted. Architectural features such as awnings and other design features which add human scale to the streetscape are encouraged where they are consistent with the design theme of the structure. Exit corridors including garage entrances shall provide a finished appearance to the street with street level exterior finishes wrapping into the openings a minimum of ten feet, or the garage door, whichever is deeper. All exhaust caps, lighting, sprinkler heads, and other elements on the undersides of all balconies and surfaces shall be logically composed and placed to minimize their visibility, while meeting code requirements. All soffit materials shall be high quality and consistent with adjacent elevation materials, and incorporate drip edges and other details to minimize staining and ensure long-term durability.
64. **Utilitarian Areas:** Areas housing trash, storage, or other utility services shall be completely concealed from view of the ROW and adjoining developments, except for utilities required to be exposed by the City or utility company. The development shall provide trash and recyclable material storage areas per SDMC Sections 142.0810 and 142.0820. Such areas shall be provided within an enclosed building area and kept clean and orderly at all times.
65. **Mail and Delivery Locations:** It is the Owner/Permittee's responsibility to coordinate mail service and mailbox locations with the United States Postal Service and to minimize curb spaces devoted to postal and loading use. The Owner/Permittee shall locate all mailboxes and parcel lockers outside of the ROW either within the building or recessed into a building wall.
66. **Circulation and Parking:** Owner/Permittee shall prepare a plan which identifies the location of curbside parking control zones, parking meters, fire hydrants, valet services if any, trees, street lights to the satisfaction of the City, and consistent with the performance standards in the CCPDO and CCSM. Such plan shall be submitted in conjunction with Construction Permits. All subterranean parking shall meet the requirements of the Building Department, Fire Department and City Engineer. All parking shall be mechanically ventilated. The exhaust system for mechanically ventilated structures shall be located to mitigate noise and exhaust impacts on the public ROW. The garage doors shall be a minimum 80% opaque to prevent views into the garage areas.
67. **Open Space and Development Amenities:** A landscape plan that illustrates the relationship of the proposed on and off-site improvements and the location of water, and electrical hookups to the satisfaction of the City and consistent with the performance standards in the CCPDO, shall be submitted with construction drawings.
68. **Roof Tops:** A rooftop equipment and appurtenance location and screening plan and consistent with the performance standards in the CCPDO shall be prepared and submitted to the satisfaction of the City with construction drawings. Any roof-top mechanical equipment shall be grouped, enclosed, and screened from surrounding views.
69. **Signage:** All signs shall comply with the City Sign Regulations and the CCPDO.
70. **Lighting:** A lighting plan which highlights the architectural qualities of the proposed development and also enhances the lighting of the public ROW shall be submitted with construction

drawings. All lighting shall be designed to avoid illumination of, or glare to, adjoining properties, including those across any street.

71. Noise Control: All mechanical equipment, including but not limited to, air conditioning, heating and exhaust systems, shall comply with the City Noise Ordinance and California Noise Insulation Standards as set forth in Title 24. The Owner/Permittee shall provide evidence of compliance with construction drawings.

72. Street Address: Building address numbers shall be provided that are visible and legible from the ROW.

73. On-Site Improvements: All on-site improvements shall be designed as part of an integral site development. An on-site improvement plan shall be submitted to the satisfaction of the City with construction drawings.

74. Off-Site Improvements: Public improvements shall be installed in accordance with the Centre City Streetscape Manual (CCSM). The Manual is currently being updated and the Owner/Permittee shall install the appropriate improvements according to the latest requirements at the time of Building Permit issuance.

75. Street Trees: Street trees shall be Chinese Flame on G Street, Chinese Evergreen Elm on Ninth Avenue, and Jacaranda on Tenth Avenue, per the CCSM. All trees shall be planted at a minimum 36-inch box size with tree grates provided as specified in the CCSM, and shall meet the requirements of Title 24. Tree spacing shall be accommodated after street lights have been sited, and generally spaced 20 to 25 feet on center. All landscaping shall be irrigated with private water service from the subject development. Associated tree grates shall be Gateway on G Street and Tenth Avenue and CCDC Standard on Ninth Avenue, per the CCSM.

76. Street Lights: Street lights shall be Gateway on G Street and Tenth Avenue and Standard on Ninth Avenue, per the CCSM. All existing lights shall be evaluated to determine if they meet current City requirements, and shall be modified or replaced if necessary.

77. Sidewalk Paving: Paving shall be Gateway on G Street and Tenth Avenue and CCDC Standard on Ninth Avenue, per the CCSM. Any specialized paving materials shall be approved through the execution of an EMRA with the City.

78. Litter Containers: The development shall provide trash receptacles, one at each intersection.

79. Landscaping: All required landscaping shall be maintained in a disease, weed, and litter free condition at all times. If any required landscaping (including existing or new plantings, hardscape, landscape features, etc.) indicated on the approved construction documents is damaged or removed during demolition or construction, it shall be repaired and/or replaced in kind and equivalent in size per the approved documents and to the satisfaction of the City within 30 days of damage or Certificate of Occupancy, whichever occurs first.

80. Planters: Planters shall be permitted to encroach into the ROW a maximum of two feet. The planter encroachment shall be measured from the property line to the face of the curb/wall surrounding the planter. A minimum five-foot clear path shall be maintained between the face of the planter and the edge of any tree grate or other obstruction in the ROW.

81. On-Street Parking: Owner/Permittee shall maximize the on-street parking wherever feasible.

82. Franchise Public Utilities: The Owner/Permittee shall be responsible for the installation or relocation of franchise utility connections including, but not limited to, gas, electric, telephone and cable, to the development and all extensions of those utilities in public streets. Existing franchised utilities located above grade serving the property and in the sidewalk ROW shall be removed and incorporated into the adjoining development. All franchise utilities shall be installed as identified in the Basic Concept Drawings. Any above grade devices shall be screened from view from the ROW.

83. Construction Fence: Owner/Permittee shall install a construction fence pursuant to specifications of, and a permit from, the City Engineer. The fence shall be solid plywood with wood framing, painted a consistent color with the development's design, and shall contain a pedestrian passageway, signs, and lighting as required by the City Engineer. The fencing shall be maintained in good condition and free of graffiti at all times.

84. Development Identification Signs: Prior to commencement of construction on the site, the Owner and/or Permittee shall prepare and install, at its cost and expense, one sign on the barricade around the site which identifies the development. The sign shall be at least four feet by six feet and be visible to passing pedestrian and vehicular traffic. The signs shall at a minimum include: 1) Color rendering of the development, 2) Development name, 3) Developer, 4) Completion Date, 5) For information call \_\_\_\_\_. Additional development signs may be provided around the perimeter of the site. All signs shall be limited to a maximum of 160 sq. ft. per street frontage. Graphics may also be painted on any barricades surrounding the site. All signs and graphics shall be submitted to the City for approval prior to installation.

**POLICE DEPARTMENT REQUIREMENTS:**

85. No off-sale of distilled spirits.

86. No alcoholic beverages shall be sold except between the hours of 10:00 a.m. and 10:00 p.m.

87. No malt liquor sales.

88. Beer or wine cooler products, regardless of container size, must be sold in manufacturer pre-packaged multi-unit quantities totaling a minimum of 64 ounces.

89. No wine shall be sold in containers of less than 750 milliliters.

90. Loitering is prohibited.

91. No public pay phones are permitted on the premises or adjacent and under the control of the Owner/Permittee.
92. No arcade games of any type will be allowed/maintained inside the premises.
93. Video surveillance shall be recorded and available to law enforcement upon request covering the interior and public access points of the premises. Upon request of law enforcement, video surveillance shall not be deleted, voided, or destroyed. Recordings shall be maintained for a minimum of 30 days absent a request of law enforcement.
94. Litter and any unauthorized graffiti shall be removed promptly.
95. All retail employees shall be trained in State of California Alcoholic Beverage Control (ABC) regulations (LEAD), theft deterrence, and policies against the sales of alcoholic beverages to those under 21 years of age, transient, and/or intoxicated.
96. There shall be no exterior alcoholic advertising or signage of any kind or type, including advertising directed to the exterior from within, promoting or indicating the availability of alcoholic beverages. Interior displays of alcoholic beverages or signs which are clearly visible to the exterior shall constitute a violation of this condition.

**TRANSPORTATION REQUIREMENTS**

97. All automobile, motorcycle and bicycle parking spaces must be constructed in accordance with the requirements of the SDMC. All on-site parking stalls and aisle widths shall be in compliance with requirements of the City's Land Development Code and shall not be converted and/or utilized for any other purpose, unless otherwise authorized in writing by the appropriate City decision maker in accordance with the SDMC.
98. Prior to the issuance of any building permits, the Owner/Permittee shall assure by permit and bond, the construction of the northerly driveway to a 16-foot driveway per SDG-163 and the southerly driveway to a 12-foot one-way exit only driveway along 9th Avenue consistent with City standards, satisfactory to the City Engineer. Along with the construction of the driveways, the Owner/Permittee shall install red curb at each - at the southerly driveway, 29 feet of red curb to the south, and at the northern driveway, 21 feet of red curb to the north. All improvements shall be completed and accepted by the City Engineer prior to first occupancy.

**PUBLIC UTILITIES DEPARTMENT REQUIREMENTS:**

99. Prior to the issuance of any building permits, the Owner/Permittee shall finalize water and sewer capacity charges. Capacity charges, as well as service and meter size, are determined by water meter data card which is completed during the building plan review process.
100. Prior to the issuance of any building permits, the Owner/Permittee shall assure, by permit and bond, the design and construction of new water and sewer service(s) outside of any driveway or

drive aisle and the abandonment of any existing unused water and sewer services within the ROW adjacent to the Project site, in a manner satisfactory to the PUD Director and City Engineer.

101. The Owner/Permittee shall apply for a plumbing permit for the installation of appropriate private backflow prevention device(s) (BFPD) on each water service (domestic, fire, and irrigation) in a manner satisfactory to the PUD Director and the City Engineer. BFPDs shall be located above ground on private property, in line with the service and immediately adjacent to the ROW.

102. No trees or shrubs exceeding three feet in height at maturity shall be installed within ten feet of any sewer facilities and five feet of any water facilities.

DRAFT

APPROVED by the Planning Commission of the City of San Diego on [INSERT Approval Date] and [Approved Resolution Number].

Permit Type/PTS Approval No.: SDP/NDP/TM/CUP No. 649856

Date of Approval: XX

AUTHENTICATED BY THE CITY OF SAN DIEGO URBAN DIVISION

---

James Alexander  
Program Manager  
Urban Division

**NOTE: Notary acknowledgment  
must be attached per Civil Code  
section 1189 et seq.**

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**The undersigned Owner/Permittee**, by execution hereof, agrees to each and every condition of this Permit and promises to perform each and every obligation of Owner/Permittee hereunder.

**4 Lee Tenth, LLC**  
Owner

By \_\_\_\_\_  
Elizabeth Lee  
Manager

**Cisterra Partners, LLC**  
Permittee

By \_\_\_\_\_  
Jason R. Wood  
Principal

**NOTE: Notary acknowledgments  
must be attached per Civil Code  
section 1189 et seq.**



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# 9G PLANNED DEVELOPMENT PERMIT PACKAGE

659 9th Avenue, California 92101  
DATE 6.9.2020  
THIRD SUBMITTAL



## 9G PLANNED DEVELOPMENT PERMIT PACKAGE

3rd SUBMITTAL  
6.9.20

ISSUES:

PROJECT NO:  
5937.00  
FILE NAME:  
BTM 300/5937 00-Cisterra  
-ARCH - Cisterra 6/9/2018.3.rvt  
DRAWN BY: CHECKED BY:  
Author Checker  
PLOT DATE:  
6/9/2020 2:02:26 PM  
TITLE:

Cover





## PROJECT ADDRESS

669 9TH AVE SAN DIEGO, CA, 92101-6409  
APN: 535-136-01-00

## OWNER'S NAME AND ADDRESS

4 Lee Tenth, LLC, a California limited liability company.

## PROJECT DESCRIPTION

1. NEW CONSTRUCTION, PARTIAL BLOCK MIXED-USE DEVELOPMENT.
2. 22 STORY RESIDENTIAL TOWER WITH GROUND FLOOR AND (1) LEVEL SUBGRADE RETAIL/OFFICE SPACE.
3. 3 LEVELS OF ABOVE GROUND PARKING
3. RENOVATION OF EXISTING HISTORIC BUILDING, FARKAS STORE FIXTURE BUILDING, BUILT 1932.
4. SITE WORK TO STREETSCAPE SURROUND PROPERTY.

## LEGAL DESCRIPTION

THE LAND REFERRED TO HEREIN BELOW IS SITUATED IN THE CITY OF SAN DIEGO, IN THE COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOW:

LOTS A, B, C, K AND L IN BLOCK 83 OF HORTON'S ADDITION, IN THE CITY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO THE MAP THEREOF ON FILE IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY.

## PROJECT TEAM

### APPLICANT/DEVELOPER

Cisterra Development  
3580 Carmel Mountain Road, Suite 450  
San Diego, Ca 92130  
T: 619.615.0200  
E: jwood@cisterra.com  
Contact: Jason Wood

### ARCHITECT / LANDSCAPE ARCHITECT

Carrier Johnson + Culture  
185 W F Street, suite 500  
San Diego, Ca. 92101  
T: 619.239.2353  
E: dlh@carrierjohnson.com  
Contact: Duane Hagedwood

### HISTORIC ARCHITECT

Union Architecture, Inc.  
344 22ND STREET  
SAN DIEGO, CA 92102  
T: 619.269.4941

### CIVIL ENGINEER

Fuscoe Engineering, Inc.  
6390 Greenwich Drive, Suite 170  
San Diego, Ca. 92122  
T: 858.554.1500  
E: bsmith@fuscoe.com  
Contact: Bryan Smith

## DRAWING INDEX:

- 1 Cover
- 2 Program Sheet Index
- 3 Program Summary
- 4 Vicinity Map
- 5 Photographic Survey
- 6 Site Plan
- 7 Fire Access Plan
- 8 Site Accessibility
- 9 Level B1 / Basement
- 10 Level 1 / Ground Floor
- 11 Level 2 / Parking
- 12 Level 3 / Parking
- 13 Level 4 / Parking
- 13a Level 5 / Parking
- 14 Level 6 / Parking
- 15 Level 7 / Residential
- 16 Level 8-10 / Residential
- 17 Level 20 / Residential
- 18 Level 21 / Residential
- 19 Level 22 / Amenities
- 20 Roof Level
- 21 North Elevation
- 22 West Elevation
- 23 South Elevation
- 24 East Elevation
- 25 Enlarged Elevation North
- 26 Enlarged Elevation West
- 27 Enlarged Elevation East
- 28 Section A
- 29 Section B
- 30 Detail Section
- 31 Enlarged Units
- 32-40 Perspectives
- 41 Ground Level Hardscape Plan
- 42 Street Tree Plan
- 43 Level 7 Hardscape Plan
- 44 Level 7 Illustrative
- 45 Level 22 Hardscape Plan
- 46 Level 22 Illustrative
- 47-58 Treatment Plan
- 59 FAA
- 60 Civil - Conceptual Grading & Utility Plan
- 60a Civil - Conceptual Exist. Curb Usage Plan
- 60b Civil - Conceptual Proposed Curb Usage Plan
- 61 FAR Bonus Incentives
- 62 FAR Bonus Incentives
- 63 FAR Bonus Incentives

## CONSTRUCTION TYPE

TYPE 1A FULLY SPRINKLERED

## FLOOR AREA RATIO

BASE FAR	6.0
FAR BONUSES:	
3 BEDROOM UNITS	2.0
LEED SILVER	1.0
PURCHASED FAR	2.14
ALLOWED FAR	11.14
PROPOSED FAR	11.14

## OCCUPANCY CLASSIFICATION

R-2	RESIDENTIAL
A-3	RESIDENTIAL AMENITIES
B	OFFICE
M	RETAIL
S-2	PARKING
S-1	STORAGE

## APPLICABLE BUILDING CODE

CBC 2019  
CITY OF SAN DIEGO MUNICIPAL CODE

## PROPOSED BUILDING HEIGHT

22	STORY HIGHRISE
50.75	FEET SITE ELEVATION (SE)
253	FEET ABOVE GROUND LEVEL (AGL)
304	FEET ABOVE MEAN SEA LEVEL (AMSL)

## REQUIRED DISCRETIONARY PERMITS/APPROVALS

1. Site Development Permit
2. Tentative Map
3. Conditional Use Permit for sale of alcohol

## REQUESTED DEVIATIONS

Relevant CCPDO (9-2019) sections for which the project is seeking deviations

1. Personal Storage	131.0454
2. Driveway Width	142.0560 (l), & Table 142-05m
3. Max. Lot Coverage Tower	156.0310 (d)(3)(A)
4. Floor Plate Dimensions Tower	156.0310 (d)(3)(B), & Table 156-0310-A
5. Transparency of Facade	156.0311(d)(1)

### ISSUES:

3rd SUBMITTAL  
6.9.20

## 9G PLANNED DEVELOPMENT PERMIT PACKAGE



**carrierjohnson + CULTURE**  
architecture + environments + brand strategy + graphics

185 W F St suite 500, San Diego, CA 92101  
phone 619.239.2353



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Cisterra 9th & G

BUILDING AREA SUMMARY (refer to "General Notes" below for area calculation definitions)					
LEVEL	PROGRAM	GROSS AREA*	AREA BREAKDOWNS		FAR ONLY
			CORE	RESIDENTIAL	
B1	RETAIN/OFFICE	24,928 sf	852 sf		
	Garage/Utilities	2,872 sf			
	Retail/Office	21,204 sf			
1	LEVEL 1 TOTAL	24,725 sf	844 sf		20,230 sf
	Garage	4,435 sf			
	Utilities	1,962 sf			
	Retail/Office	14,464 sf			
	Residential Lobby	3,804 sf			
2	PARKING / GARAGE	24,729 sf			
	Retail/Office Parking	23,643 sf			
	Building Core/Utilities	1,046 sf			
3	PARKING / GARAGE	24,708 sf			
4	PARKING / GARAGE	24,708 sf			
5	PARKING / GARAGE	24,708 sf			
6	PARKING / GARAGE	24,708 sf			
7	LEVEL 7 TOTAL	16,465 sf	2,200 sf	12,160 sf	73.9%
	Residential	14,360 sf			
	Amenities	2,105 sf			
8	RESIDENTIAL	16,473 sf	2,102 sf	14,371 sf	87.2%
9	RESIDENTIAL	16,473 sf	2,102 sf	14,371 sf	87.2%
10	RESIDENTIAL	16,473 sf	2,102 sf	14,371 sf	87.2%
11	RESIDENTIAL	16,473 sf	2,102 sf	14,371 sf	87.2%
12	RESIDENTIAL	16,473 sf	2,102 sf	14,371 sf	87.2%
13	RESIDENTIAL	16,473 sf	2,102 sf	14,371 sf	87.2%
14	RESIDENTIAL	16,473 sf	2,102 sf	14,371 sf	87.2%
15	RESIDENTIAL	16,473 sf	2,102 sf	14,371 sf	87.2%
16	RESIDENTIAL	16,473 sf	2,102 sf	14,371 sf	87.2%
17	RESIDENTIAL	16,473 sf	2,102 sf	14,371 sf	87.2%
18	RESIDENTIAL	16,473 sf	2,102 sf	14,371 sf	87.2%
19	RESIDENTIAL	16,473 sf	2,102 sf	14,371 sf	87.2%
20	RESIDENTIAL	16,423 sf	2,102 sf	14,321 sf	87.2%
21	RESIDENTIAL	16,308 sf	1,992 sf	14,316 sf	87.8%
	Residential Balcony (>100sf)				41 sf
22	LEVEL 22 TOTAL	12,086 sf	1,760 sf	6,687 sf	55.3%
	Residential	8,447 sf			
	Amenities	3,639 sf			
TOTALS		432,175 sf	34,974 sf	219,939 sf	
SUMMARIES:					
Total Parking/UHL		130,928 sf			
Total Retail/Office		59,351 sf			
Total Leasable Residential				219,939 sf	
Total Residential Amenities, Lobby & Leasing		9,548 sf			
Total Gross Building Area		432,175 sf			
Total FAR					279,232
BUILDING AREA NOTES:					
* GROSS AREA - measured to exterior face of wall and does not include open to below areas or open exterior occupied space.					
† RESIDENTIAL - Leasable area is gross residential / unit area measured to the exterior face of wall, exterior face of corridor or shaft, and centerline of demising walls. Balcony area is not included.					
SITE SUMMARY					
SITE					
Lot Area		0.5753214 acres	equals	25,061 sf	
Buildable Area*				25,061 sf	
Max FAR		6.0 (Base) + 2.0 (3 bedrooms) + 1.0 (LEED) +2.22 (Purchased)		11.14	
FAR				11.14	
Community and Specific Plans:					CCPD - ER
Zoning:					Transit overlay Transit Priority Resid Tandem
Fire District:					
SITE NOTES:					
43,560 sf per acre					
* Lot area minus setback area					

UNIT MIX SUMMARY (linked to sheet "Unit Mix")							
Level	JR 1 BR	1BR	2BR	3BR	PH		Total
7	4	7	2	1	0		14
8	4	8	2	2	0		16
9	4	8	2	2	0		16
10	4	8	2	2	0		16
11	4	8	2	2	0		16
12	4	8	2	2	0		16
13	4	8	2	2	0		16
14	4	8	2	2	0		16
15	4	8	2	2	0		16
16	4	8	2	2	0		16
17	4	8	2	2	0		16
18	4	8	2	2	0		16
19	4	8	2	2	0		16
20	4	8	2	2	0		16
21	2	5	1	0	4		12
22	2	3	1	0	1		7
Total	60	119	30	27	5		241
% of Total	24.90%	49.38%	12.45%	11.20%	2.07%		100.00%
Average s.f.	645 sf	833 sf	1,207 sf	1,360 sf	1,828 sf		913 sf

STORAGE REQUIREMENTS (municipal code, Mission Valley PD)		
	*REQUIRED	PROVIDED
Level 2		0
Level 3		23
Level 4		23
Level 5		23
Level 6		28
TOTALS		241
Accessible storage	5%	12.05
STORAGE NOTES:		4.55
240 sf per (1) dwelling unit		
*Requested Deviation for (0) storage requirements		
(1) per unit, 7'-0" min horizontal dimension on (1) side		
57,840 sf		

BICYCLE PARKING SUMMARY (SDMC 156.0313)		
	REQUIRED	PROVIDED
Bicycles		
0.2 stalls per dwelling unit	48.20	
TOTAL BICYCLE PARKING	48.20	50
PROVIDED BICYCLE DETAILS		
Level 2		50
Level 3		0
Level 4		0
Level 5		0
Level 6		0
Total		50
Bicycle Notes:		
*Check Calgreen / LEED for more stringent requirements		
*LEED requirements: (looks like not a pre-requisite)(check for distance requirements from main entry)		
COMMERCIAL Locker room with shower, (Short Term): 2.5% of peak visitors, 4 minimum (Long Term): 5% bldg occupants		
RESIDENTIAL (Short Term): 2.5% of peak visitors, 4 minimum (Long Term): 30% bldg occupants, not less than 1 per unit + short term		

OPEN SPACE SUMMARY (MC 156.0310, (g)(1))				
OPEN SPACE	REQUIRED		PROVIDED	
	exterior	interior	exterior	interior balcony count
15% of lot size (exterior)	3,759 sf			
500 sf per development		500 sf		
50% balconies per dwelling units	121			
Level 1			3,804 sf	
Level 7			1,884 sf	9
Level 8				9
Level 9				9
Level 10				9
Level 11				9
Level 12				9
Level 13				9
Level 14				9
Level 15				9
Level 16				9
Level 17				9
Level 18				9
Level 19				9
Level 20				9
Level 21				8
Level 22			4,250 sf	3,639 sf 2
TOTALS			6,134 sf	9,548 sf 136
REQUIRED			3,759 sf	500 sf 121
OPEN SPACE NOTES:				
Open Space must be a minimum of 30' in each direction, or 40' when bordered by (3) building walls over 15'-0'				
*Exterior Requirement = 15% of lot size				
*Interior Requirement = 500sf min, adjacent to common outdoor open space				
*Private open space = 50% of all dwelling units				
*minimum 10% of common outdoor space must be planting				

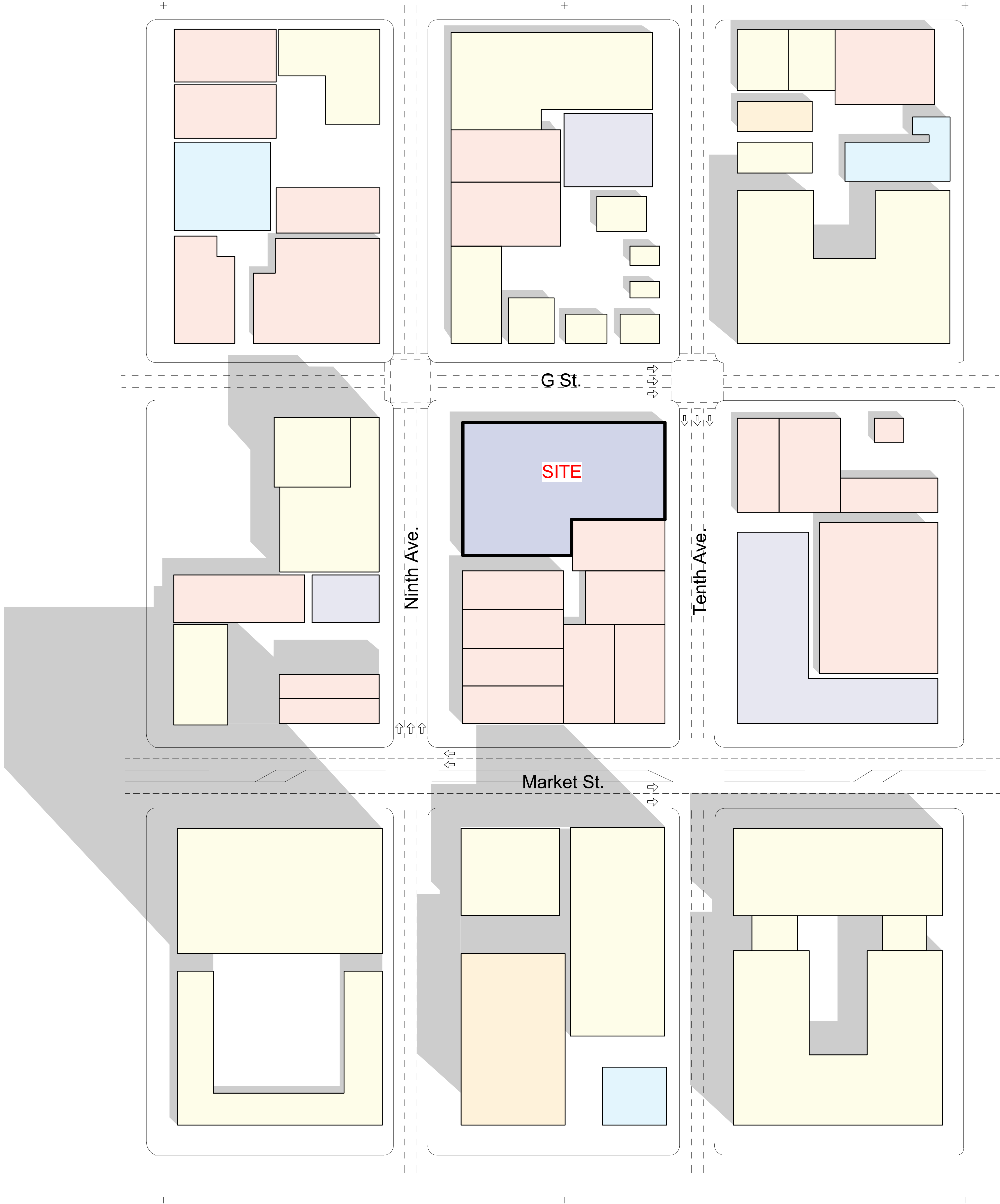
VEHICULAR PARKING SUMMARY (SDMC 156.0313)				
VEHICULAR PARKING TOTALS				
VEHICULAR PARKING	Required		Provided	
	minimum	maximum	minimum	maximum (see below)
Residential	0	1	0	241.00
USPS included				243
Retail 1.0 stalls per 1,000 sf; under 30,000sf exempt				
1 stalls per 1,000 sf		35,668 sf	35,668	35,668
TOTAL VEHICULAR PARKING	35.67	276.67	243	
MOTORCYCLE PARKING				
RESIDENTIAL	required		provided	
			(see below)	
0.1 stalls per parking space			24.30	28
TOTAL MOTORCYCLE PARKING			24.30	28
ACCESSIBLE PARKING CALCULATIONS				
Total Residential HC Req'd. (2%) (included in the parking counts above)				
Total Retail HC Req'd. (Table 11B-208.2) (included in the parking counts above)				
PROVIDED VEHICULAR PARKING DETAILS				
Level 2	standard	tandem	accessible	EVCS future EVCS
Level 3	38	0	2	1 1
Level 4	37	8	2	2 1
Level 5	38	8	2	1 2
Level 6	41	8	1	1 0
Level 7	41	8	0	0 0
Total	195	32	7	5 4
PROVIDED MOTORCYCLE PARKING DETAILS				
Level 2	required com.	required res.		provided
Level 3	2	0		4
Level 4	0	6		6
Level 5	0	6		6
Level 6	0	6		6
Level 7	0	5		6
Total	2	23		28
PARKING NOTES:				

Cisterra 9th & G

UNIT SUMMARY		LEVELS																				COUNT	% of TOTAL	RESIDENTIAL UNITS																
		7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22			AVE. GSF	TOTAL GSF																			
JR 1 BEDROOM																																								
JA		1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	16	6.6%	628 sf	10,043 sf																			
JB		1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	16	6.6%	645 sf	10,320 sf																			
JC		1	1	1	1	1	1	1	1	1	1	1	1	1	1	0	0	14	5.8%	623 sf	8,722 sf																			
JD		1	1	1	1	1	1	1	1	1	1	1	1	1	1	0	0	14	5.8%	667 sf	9,618 sf																			
STUDIO totals																		4	4	4	4	4	4	4	4	4	4	4	4	2	2	60	25%	645 sf	38,703 sf					
1 BEDROOM																																								
1A		1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	16	6.6%	815 sf	13,043 sf																			
1B		1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	0	15	6.2%	801 sf	12,017 sf																			
1C		1	1	1	1	1	1	1	1	1	1	1	1	1	1	0	0	14	5.8%	830 sf	11,620 sf																			
1D		0	1	1	1	1	1	1	1	1	1	1	1	1	1	0	0	13	5.4%	788 sf	10,244 sf																			
1D.1		1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0.4%	808 sf	808 sf																			
1E		0	1	1	1	1	1	1	1	1	1	1	1	1	1	0	0	13	5.4%	776 sf	10,088 sf																			
1F		1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	0	15	6.2%	806 sf	12,090 sf																			
1G		1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	0	15	6.2%	799 sf	11,985 sf																			
1H		0	1	1	1	1	1	1	1	1	1	1	1	1	1	1	0	15	6.2%	1,037 sf	15,555 sf																			
1I		1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0.4%	914 sf	914 sf																			
1K		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0.4%	801 sf	801 sf																			
1 BEDROOM totals																		7	8	8	8	8	8	8	8	8	8	8	8	5	3	119	49%	833 sf	99,165 sf					
2 BEDROOM																																								
2A		1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	16	6.6%	1,203 sf	19,251 sf																			
2B		0	1	1	1	1	1	1	1	1	1	1	1	1	1	1	0	13	5.4%	1,206 sf	15,678 sf																			
2C		1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0.4%	1,279 sf	1,279 sf																			
2 BEDROOM totals																		2	2	2	2	2	2	2	2	2	2	2	2	2	1	30	12%	1,207 sf	36,208 sf					
3 BEDROOM																																								
3A		0	1	1	1	1	1	1	1	1	1	1	1	1	1	0	0	12	5.0%	1,372 sf	16,464 sf																			
3A.1		1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	2	0.8%	1,322 sf	2,644 sf																			
3B		0	1	1	1	1	1	1	1	1	1	1	1	1	1	1	0	13	5.4%	1,355 sf	17,615 sf																			
3 BEDROOM totals																		1	2	2	2	2	2	2	2	2	2	2	2	2	0	27	11%	1,369 sf	36,723 sf					
PENTHOUSES																																								
PH1		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0.4%	1,902 sf	1,902 sf																		
PH2		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0.4%	1,914 sf	1,914 sf																		
PH3		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0.4%	1,815 sf	1,815 sf																		
PH4		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0.4%	1,951 sf	1,951 sf																		
PH5		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	1	0.4%	1,558 sf	1,558 sf																		
3 BEDROOM totals																		0	0	0	0	0	0	0	0	0	0	0	0	0	4	1	5	2%	1,828 sf	9,140 sf				
TOTAL UNITS		14																16	16	16	16	16	16	16	16	16	16	16	12	7	241	ave. unit size	913 sf	219,939 sf						
design totals		14																16	16	16	16	16	16	16	16	16	16	16	12	7	241	100%								



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- LEGEND:
- SITE
  - COMMERCIAL
  - RESIDENTIAL
  - PARKING
  - HOSPITALITY
  - OFFICE



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# PHOTO KEY MAP



1. VIEW SOUTHWEST ALONG 9TH AVE. - BUSINESS PREMISES



2. VIEW WEST ALONG 9TH AVE. - RESIDENTIAL BUILDING



3. VIEW NORTHWEST ALONG 9TH & G ST. - CROSSFIT GYM



4. VIEW NORTHEAST ALONG 10TH & G ST. - RESIDENTIAL BUILDING



5. VIEW SOUTHEAST ALONG 10TH AVE. - PUBLIC PARKING



6. VIEW SOUTHEAST ALONG 9TH AVE. - FARKAS STORE BUILDING



7. VIEW EAST ALONG G ST. - RESIDENTIAL BUILDING AND FARKAS STORE



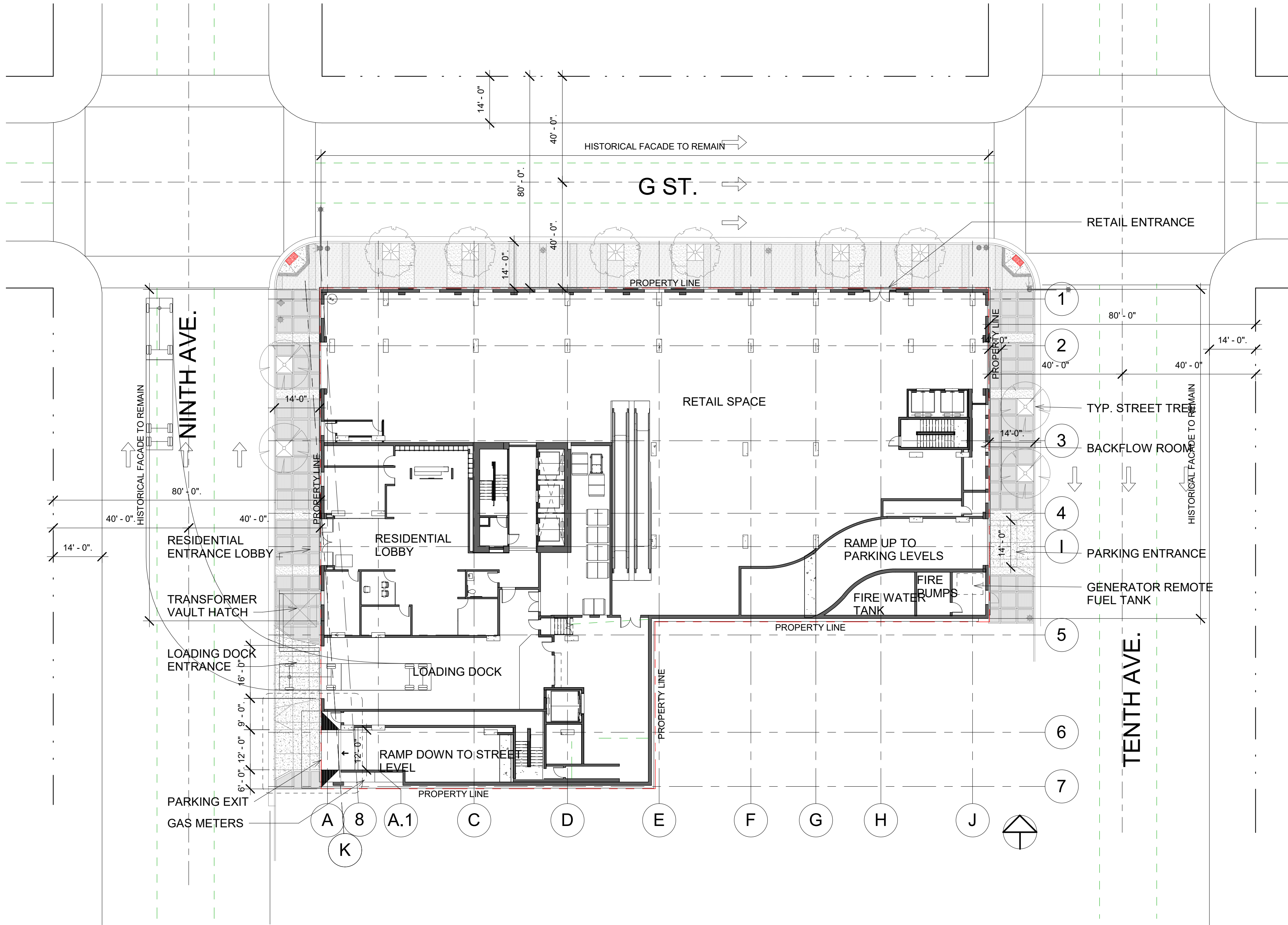
8. VIEW SOUTHWEST CORNER ALONG 9TH & G ST. - FARKAS STORE BUILDING



9. VIEW SOUTH ALONG 10TH ST. - BUSINESS PREMISES AND FARKAS STORE BUILDING



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SITE PLAN

1" = 20'-0"

9G  
PLANNED DEVELOPMENT PERMIT  
PACKAGE

3rd SUBMITTAL  
6.9.20

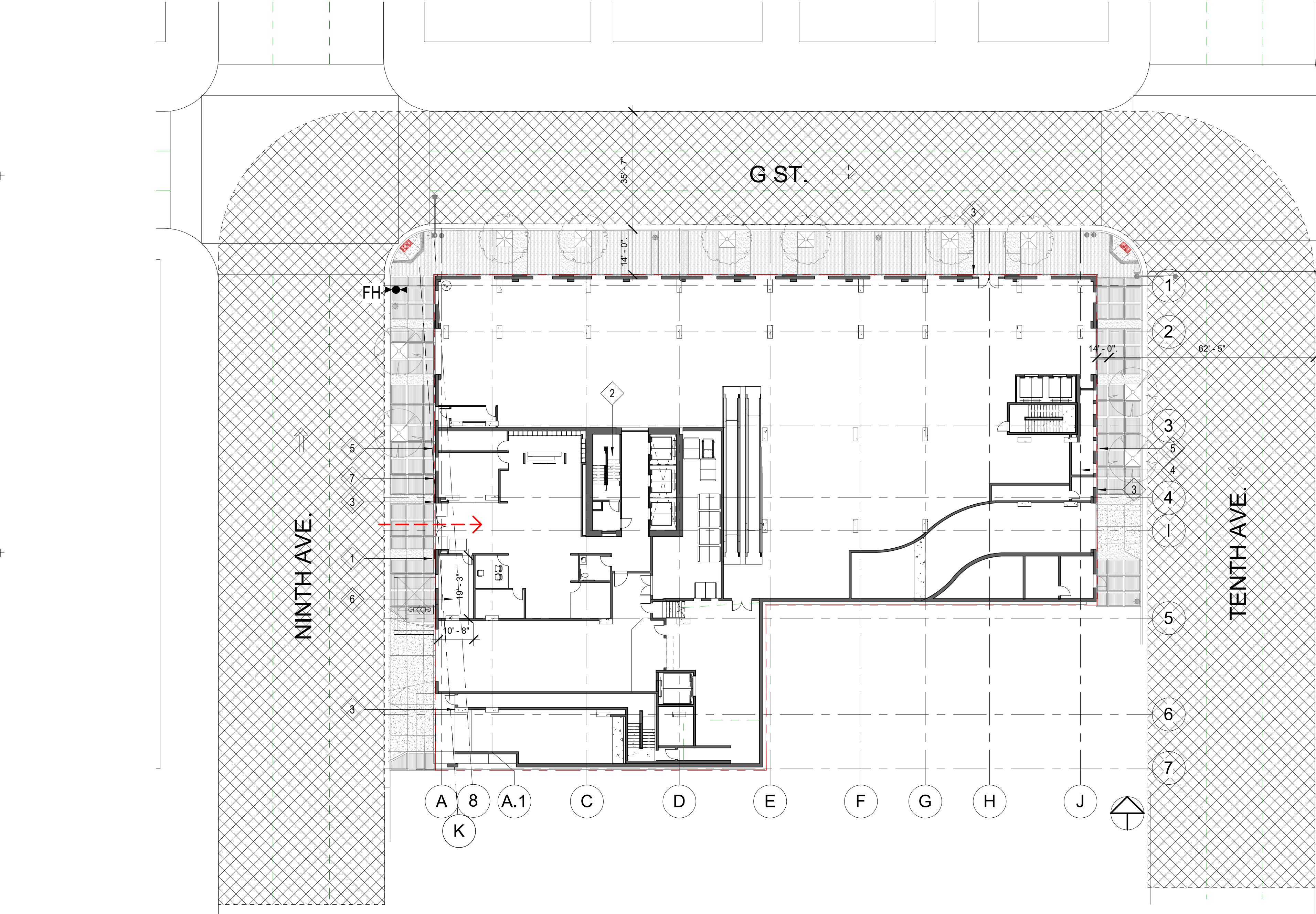
ISSUES:

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Author Checker  
PLOT DATE:  
6/9/2020 2:02:33 PM  
TITLE:

Site Plan



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1. PROVIDE BUILDING ADDRESS NUMBERS, VISIBLE AND LEGIBLE FROM THE STREET OR ROAD FRONTING THE PROPERTY PER SAN DIEGO MUNICIPAL CODE SECTION 95.0209.
2. POST INDICATOR VALVES, FIRE DEPARTMENT CONNECTIONS, AND ALARM BELL ARE TO BE LOCATED ON THE ADDRESS/ACCESS SIDE OF THE STRUCTURE.
3. CBC SEC. 3002.4A – GENERAL STRETCHER REQUIREMENTS – ALL BUILDINGS AND STRUCTURES WITH ONE OR MORE PASSENGER SERVICE ELEVATORS SHALL BE PROVIDED WITH NOT LESS THAN ONE MEDICAL EMERGENCY SERVICE ELEVATOR TO ALL LANDINGS MEETING THE PROVISIONS OF SECTION 3002.4A
4. THE REQUIRED WIDTH OF ACCESS ROADWAYS SHALL NOT BE OBSTRUCTED IN ANY MANNER, INCLUDING THE PARKING OF VEHICLES. WHERE NO SPACE IS PROVIDED FOR PARKING ALONG ACCESS ROADWAYS, THEY SHALL BE KEPT CLEAR BY THE POSTING OF SIGNS OR THE PAINTING OF CURBS PER POLICY A-14-1
5. STAIRWAYS EXITING DIRECTLY TO THE EXTERIOR OF A BUILDING FOUR OR MORE STORIES IN HEIGHT SHALL BE PROVIDED WITH A MEANS FOR EMERGENCY ENTRY FOR FIRE DEPARTMENT ACCESS. DOORS AT THESE LOCATIONS SHALL ALSO COMPLY WITH THIS REQUIREMENT.
6. CFC 504.3 – NEW BUILDINGS FOUR OR MORE STORIES ABOVE GRADE PLANE, EXCEPT THOSE WITH A ROOF SLOPE GREATER THAN FOUR UNITS VERTICAL IN 12 UNITS HORIZONTAL SHALL BE PROVIDED WITH A STAIRWAY TO THE ROOF.
7. EVERY BUILDING FOUR STORIES OR MORE IN HEIGHT SHALL BE PROVIDED WITH NOT LESS THAN ONE STANDPIPE FOR USE DURING CONSTRUCTION INSTALLED IN ACCORDANCE WITH CFC 3313.1. STANDPIPE SHALL BE INSTALLED WHEN PROGRESS OF CONSTRUCTION IS NOT MORE THAN 40 FEET IN HEIGHT ABOVE THE LOWEST LEVEL OF FIRE DEPARTMENT ACCESS. CFC 3313.1
8. VEGETATION SHALL BE SELECTED AND MAINTAINED IN SUCH MANNER AS TO ALLOW IMMEDIATE ACCESS TO ALL HYDRANTS, VALVES, FIRE DEPARTMENT CONNECTIONS, PULL STATIONS, EXTINGUISHERS, SPRINKLER RISERS, ALARM CONTROL PANELS, RESCUE WINDOWS, AND OTHER DEVICES OR AREAS USED FOR FIRE FIGHTING PURPOSES. VEGETATION OF BUILDING FEATURES SHALL NOT OBSTRUCT ADDRESS NUMBERS OR INHIBIT THE FUNCTIONING OF ALARM BELLS, HORNS OR STROBES.
9. DECORATIVE MATERIALS SHALL BE MAINTAINED IN A FLAME RETARDANT CONDITION. CFC SEC. 804
10. ALL BUILDINGS AND SITES UNDERGOING CONSTRUCTION, ALTERATION, OR DEMOLITION SHALL COMPLY WITH THE REQUIREMENTS OF CHAPTER 33 OF THE CFC
11. FIRE APPARATUS ACCESS ROADS AND WATER SUPPLIES FOR FIRE PROTECTION, SHALL BE INSTALLED AND MADE SERVICEABLE PRIOR TO AND DURING TIME OF CONSTRUCTION CFC 501.4
12. FIRE COMMAND CENTER SHALL COMPLY WITH NFPA 72 AND SHALL CONTAIN THE FEATURES LISTED IN CFC SECTION 508
13. POST INDICATOR VALVES, FIRE DEPARTMENT CONNECTIONS, AND ALARM BELL ARE TO BE LOCATED ON THE ADDRESS/ACCESS SIDE OF THE STRUCTURE. 912.2.1
14. SAN DIEGO MUNICIPAL CODE SECTION 55.507 ITEM (C) HYDRANT LOCATIONS SHALL BE IDENTIFIED BY THE INSTALLATION OF REFLECTIVE BLUE COLORED MARKERS. SUCH MARKERS SHALL BE AFFIXED TO THE ROADWAY SURFACE, APPROXIMATELY CENTERED BETWEEN CURBS, AND AT A RIGHT ANGLE TO THE HYDRANT.
15. EXTERIOR DOORS AND OPENINGS REQUIRED BY THIS CODE OR THE CBC SHALL BE MAINTAINED READILY ACCESSIBLE FOR EMERGENCY ACCESS BY THE FIRE DEPARTMENT. AN APPROVED ACCESS WALKWAY LEADING FROM FIRE APPARATUS ACCESS ROADS TO EXTERIOR OPENINGS SHALL BE PROVIDED WHEN REQUIRED BY THE FIRE CODE OFFICIAL.
16. EMERGENCY SYSTEMS CONFORMING WITH CFC SECTION 604 AND THE CALIFORNIA ELECTRICAL CODE SHALL BE PROVIDED.
17. CFC 105.4.4 CONSTRUCTION DOCUMENTS APPROVED BY THE FIRE CODE OFFICIAL ARE APPROVED WITH THE INTENT THAT SUCH CONSTRUCTION DOCUMENTS COMPLY IN ALL RESPECTS WITH THE CFC. REVIEW AND APPROVAL BY THE FIRE CODE OFFICIAL SHALL NOT RELIEVE THE APPLICANT OF THE RESPONSIBILITY OF COMPLIANCE WITH THIS CODE.
18. ROOMS CONTAINING CONTROLS FOR A/C SYSTEMS, SPRINKLER RISERS AND VALVES, OR OTHER FIRE DETECTION, SUPPRESSION OR CONTROL ELEMENTS SHALL BE IDENTIFIED FOR THE USE OF THE FIRE DEPARTMENT. APPROVED SIGNS REQUIRED TO IDENTIFY FIRE PROTECTION EQUIPMENT AND EQUIPMENT LOCATION SHALL BE CONSTRUCTED OF DURABLE MATERIALS, PERMANENTLY INSTALLED AND READILY VISIBLE.
19. FD POLICY 10-09 - HIGH RISE BUILDINGS FDC' SHALL HAVE FOUR 2 1/2 INCH INLETS. HIGH RISE BUILDINGS SHALL HAVE TWO REMOTELY LOCATED FDC'S FOR EACH ZONE. HIGH RISE AND OTHER BUILDINGS EQUIPPED WITH HOSE VALVES OF THE PRESSURE REGULATING TYPE (PRV'S) SHALL PROVIDE A SIGN INDICATING THE MINIMUM PRESSURE THE FIRE APPARATUS REQUIRED TO PUMP TO PUMP INTO THE FDC. (EXAMPLE: FD MINIMUM PUMP PRESSURE 225 PSI). A WEATHER-RESISTANT SIGN SECURED WITH A CORROSION RESISTANT CHAIN OR FASTENER SHALL INDICATE THE ADDRESS, PORTION OF THE BUILDING SERVED. SEE NFPA 14 - 6.4.6 FOR HEIGHT REQUIREMENTS.
20. A "DISCRETIONARY" PLAN REVIEW IS "CONCEPTUAL" BY DEFINITION, AND AS SUCH DOES NOT CONSTITUTE AN APPROVAL FOR FIRE ACCESS. IT SHALL THEREFORE BE INCUMBENT OF THE APPLICANT TO ENSURE THAT A FIRE PLAN REVIEW CYCLE IS PROVIDED DURING THE "MINISTERIAL" REVIEW. ALSO, AN "EXHIBIT A" PACKAGE WITH OR WITHOUT A "FIRE ACCESS" PLAN DOES NOT CONSTITUTE AN APPROVED FAP FOR ISSUANCE OF CONSTRUCTION/BUILDING PERMIT.
21. FIRE APPARATUS ACCESS ROADS SHALL BE DESIGNED AND MAINTAINED TO SUPPORT THE IMPOSED LOADS OF FIRE APPARATUS AND SHALL BE SURFACED SO AS TO PROVIDE ALL WEATHER DRIVING CAPABILITIES.

22. AERIAL FIRE ACCESS ROAD(S) ADJACENT TO BUILDINGS THAT ARE GREATER THAN 30 FEET IN HEIGHT FROM GRADE PLANE, SHALL HAVE A MINIMUM WIDTH OF 26 FEET. THE PROXIMAL EDGE OF AERIAL FIRE ACCESS SHALL BE A MINIMUM OF 15-30 FEET FROM THE BUILDING FACADE(S) AND/OR PLUMB LINE OF EAVE(S). AERIAL ACCESS SHALL BE PROVIDED ALONG ONE ENTIRE LONG SIDE(S) OF THE BUILDING(S).
23. ALL REQUIRED HOSE PULLS ARE SHOWN TO REACH ALL PORTIONS OF THE EXTERIOR OF THE BUILDING(S) PER POLICY A-14-1. HOSE PULL IS MEASURED FROM THE FIRE APPARATUS (ENGINE) WHEN THE FIRE ENGINE IS IN A FIRE ACCESS ROAD/LANE. HOSE PULL CAN BE MEASURED FROM MULTIPLE LOCATIONS WITHIN THE ACCESS ROAD/LANE. THE HOSE PULLS MUST CONNECT OR OVERLAP TO SHOW COMPLETE COVERAGE FOR SPRINKLERED BUILDING(S); THE MAXIMUM HOSE PULL IS 200'. FOR NON-SPRINKLERED BUILDING(S); THE MAXIMUM HOSE PULL IS 150'. CHANGE IN VERTICAL ELEVATION MUST ALSO BE ACCOUNTED FOR.
24. ALL EXISTING AND/OR PROPOSED FIRE HYDRANTS WITHIN 600 FEET OF THE PROJECT SITE AND A 300 FEET RADIUS OVERLAY SHALL BE SHOWN TO ENCOMPASS ALL PORTIONS OF ALL STRUCTURES AS PART OF SUBMITTED PROJECT.
25. ALL RED CURB/NO PARKING SIGN AREAS HAVE BEEN SHOWN WITH A KEY INDICATOR. ALL REQUIRED ACCESS ROADWAYS SHALL NOT PROVIDE LESS THAN THE REQUIRED/APPROVED WIDTH AND/OR BE OBSTRUCTED IN ANY MANNER, INCLUDING THE PARKING OF VEHICLES. WHERE INADEQUATE WIDTH HAS NOT BEEN PROVIDED FOR PARKING ALONG ACCESS ROADWAYS, THEN SUCH ACCESS SHALL BE KEPT CLEAR BY THE POSTING OF SIGNS OR THE PAINTING OF CURBS PER POLICY A-14-1.
26. THE LOCATION(S) OF AN APPROVED "KNOX" KEY BOX ARE SHOWN ON THE FAP AND FOLLOW THE SAN DIEGO FIRE DEPARTMENT FPB POLICY K-15-2. (SANDIEGO.GOV/FIRE SERVICES & PROGRAMS, POLICIES, KNOX BOX REQUIREMENTS).
27. MINIMUM ROOM SIZE FOR FIRE COMMAND CENTER IS 200 SQUARE FEET IN AREA WITH A MINIMUM DIMENSION OF 10 FEET.
28. AT LEAST ONE FIRE EXTINGUISHER WITH A MINIMUM RATING OF 2-A-10-BC SHALL BE PROVIDED WITHIN 75 FEET MAXIMUM TRAVEL DISTANCE FOR EACH 6,000 SQUARE FEET OR PORTION THEREOF ON EACH FLOOR CFC SEC 906.
29. STRUCTURES UNDER CONSTRUCTION, ALTERATION OR DEMOLITION SHALL BE PROVIDED WITH NOT LESS THAN ONE APPROVED PORTABLE FIRE EXTINGUISHER IN ACCORDANCE WITH SECTION 906 AND SIZED FOR NOT LESS THAN ORDINARY HAZARD (2A10BC) AS FOLLOWS:
  1. AT EACH STAIRWAY ON ALL FLOOR LEVELS WHERE COMBUSTIBLE MATERIALS HAVE ACCUMULATED.
  2. IN EVERY STORAGE AND CONSTRUCTION SHED
  3. ADDITIONAL PORTABLE FIRE EXTINGUISHERS SHALL BE PROVIDED WHERE SPECIAL HAZARDS EXIST, INCLUDING, BUT NOT LIMITED TO, THE STORAGE AND USE OF FLAMMABLE AND COMBUSTIBLE LIQUIDS. CFC 3315.
30. PROVIDE STAIRWAY IDENTIFICATION SIGNS PER CFC 3315.
31. A CLASS I (OR I AND I OR II) STANDPIPE OUTLET CONNECTION IS REQUIRED IN OCC. OF 4 OR MORE STORIES AT EVERY FLOOR LEVEL CONNECTION OF EVERY REQUIRED STAIRWAY ABOVE OR BELOW GRADE. OUTLETS AT STAIRWAYS SHALL BE LOCATED WITHIN THE EXIT ENCLOSURE OR, IN THE CASE OF PRESSURIZED ENCLOSURES, WITHIN THE VESTIBULE OR EXTERIOR BALCONY, GIVING ACCESS TO THE STAIRWAY. THERE SHALL BE AT LEAST ONE OULET ABOVE THE ROOF LINE WHEN THE ROOF HAS A SLOPE OF LESS THAN 4/12 UNITS HORIZONTAL. IN BUILDINGS WHERE MORE THAN ONE STANDPIPE IS PROVIDED, THE STANDPIPES SHALL BE INTERCONNECTED CFC 905.
32. FIRE PROTECTION EQUIPMENT SHALL BE IDENTIFIED IN AN APPROVED MANNER. ROOMS CONTAINING CONTROLS FOR A/C SYSTEMS, SPRINKLER RISERS AND VALVES, OR OTHER FIRE DETECTION, SUPPRESSION OR CONTROL ELEMENTS SHALL BE IDENTIFIED FOR THE USE OF THE FIRE DEPARTMENT. APPROVED SIGNS REQUIRED TO IDENTIFY FIRE
33. FUEL TANKS (TO INCLUDE BELLY TANKS OF GENERATOR EQUIPMENT) REQUIRE PLAN CHECK AND APPROVAL FROM THE TECHNICAL SERVICES SECTION OF THE FIRE DEPARTMENT. TO OBTAIN A BUILDING PERMIT / TA NUMBER OR REQUEST A REVIEW FOR PERMIT, CALL (619) 533-4477 TO SCHEDULE AN APPOINTMENT. BUILDING FINAL WILL NOT BE APPROVED UNTIL THE TANK PERMIT HAS BEEN APPROVED.

3rd SUBMITTAL  
6.9.20

ISSUES:

PROJECT NO:  
5937.00  
FILE NAME:  
B:\M 300\5937.00-Culture  
RESEARCH - Culture\05\_2018.3.rvt  
DRAWN BY: CHECKED BY:  
Author Checker  
PLOT DATE:  
6/9/2020 2:02:36 PM  
TITLE:

Fire Access Plan



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SITE ACCESSIBILITY PLAN LEGEND



ACCESSIBLE PATH OF TRAVEL PER CBC 11B-403, 405  
PATH OF TRAVEL (POT) AS INDICATED IS A BARRIER  
FREE ACCESS ROUTE WITHOUT ANY ABRUPT  
VERTICAL CHANGES EXCEEDING 1/2" BEVELED AT  
1:2 MAXIMUM SLOPE, EXCEPT THAT VERTICAL  
CHANGES DO NOT EXCEED 1/4" VERTICAL AND IS  
AT LEAST 48" WIDE. SURFACE IS SLIP RESISTANT  
(MEDIUM BROOM FINISH WITH AMPLITUDE OF 1/16"  
TO 1/32", STABLE, FIRM, AND SMOOTH. CROSS  
SLOPE DOES NOT EXCEED 2% AND SLOPE IN THE  
DIRECTION OF TRAVEL IS NO MORE THAN 5%  
UNLESS OTHERWISE INDICATED, POT SHALL BE  
MAINTAINED FREE OF OVERHANGING  
OBSTRUCTIONS TO 80" MINIMUM AND PROTRUDING  
OBJECTS GREATER THAN 4" PROJECTION FROM A  
WALL AND ABOVE 27" AND LESS THAN 80",  
CONTRACTOR TO VERIFY THAT THERE ARE NO  
BARRIERS IN THE POT, AND THE POT COMPLIES  
WITH CBC 11B-403, 405.

3rd SUBMITTAL  
6.9.20

ISSUES:

PROJECT NO:  
5937.00  
FILE NAME:  
BM 360-16207-00-Cisterra  
-SGARCH\_Cisterra9G\_2018.3.rvt  
DRAWN BY: Author  
CHECKED BY: Checker  
PLOT DATE:  
6/9/2020 2:02:38 PM  
TITLE:

Site Accessibility

9G  
PLANNED DEVELOPMENT PERMIT  
PACKAGE



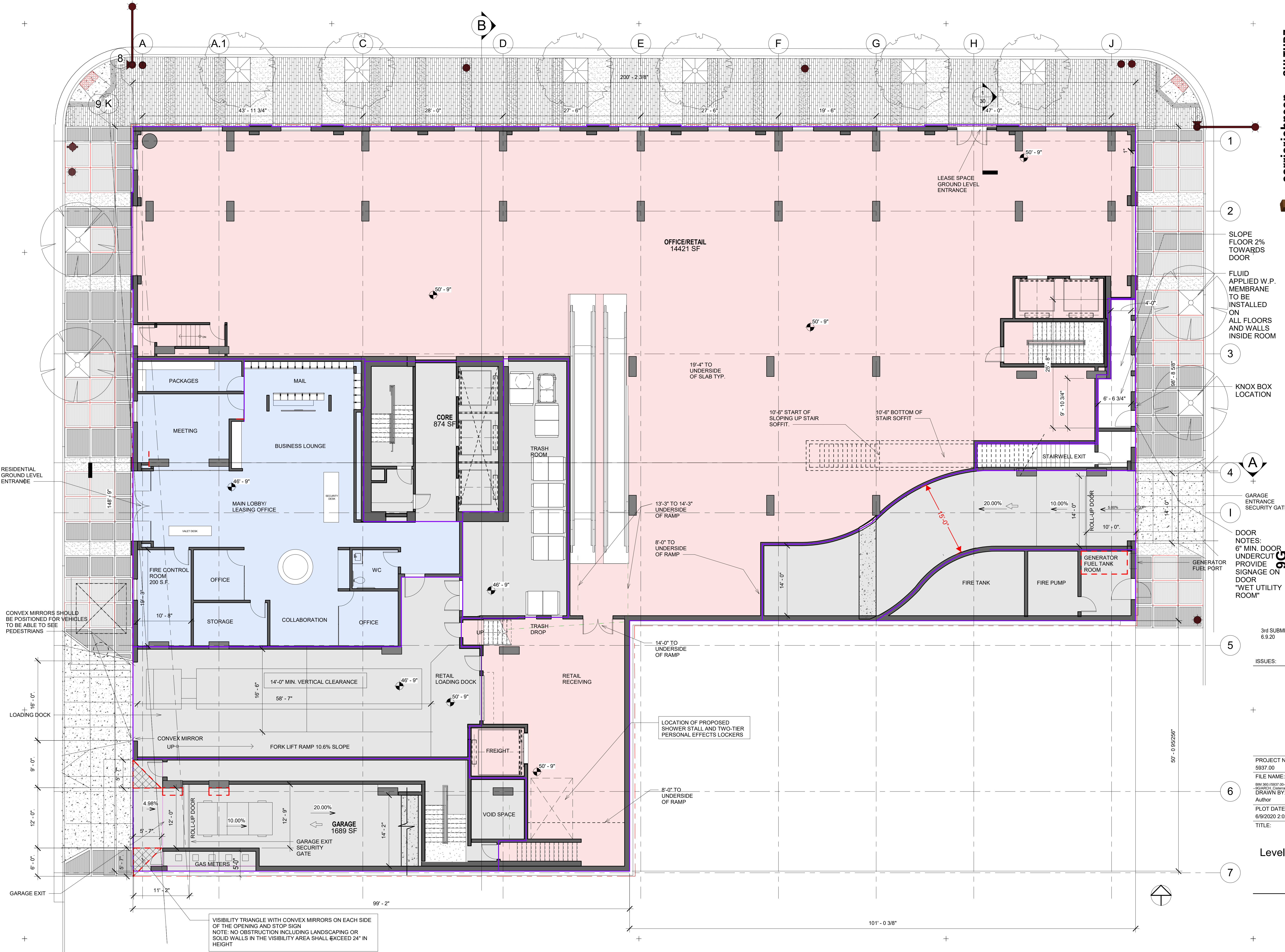
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phone 619.239.2353

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**PLANNED DEVELOPMENT PERMIT  
PACKAGE**

99

DOOR  
NOTES:  
6" MIN. DOOR  
UNDERCUT  
PROVIDE  
SIGNAGE ON  
DOOR  
"WET UTILITY  
ROOM"

3rd SUBMITTAL  
6.9.20

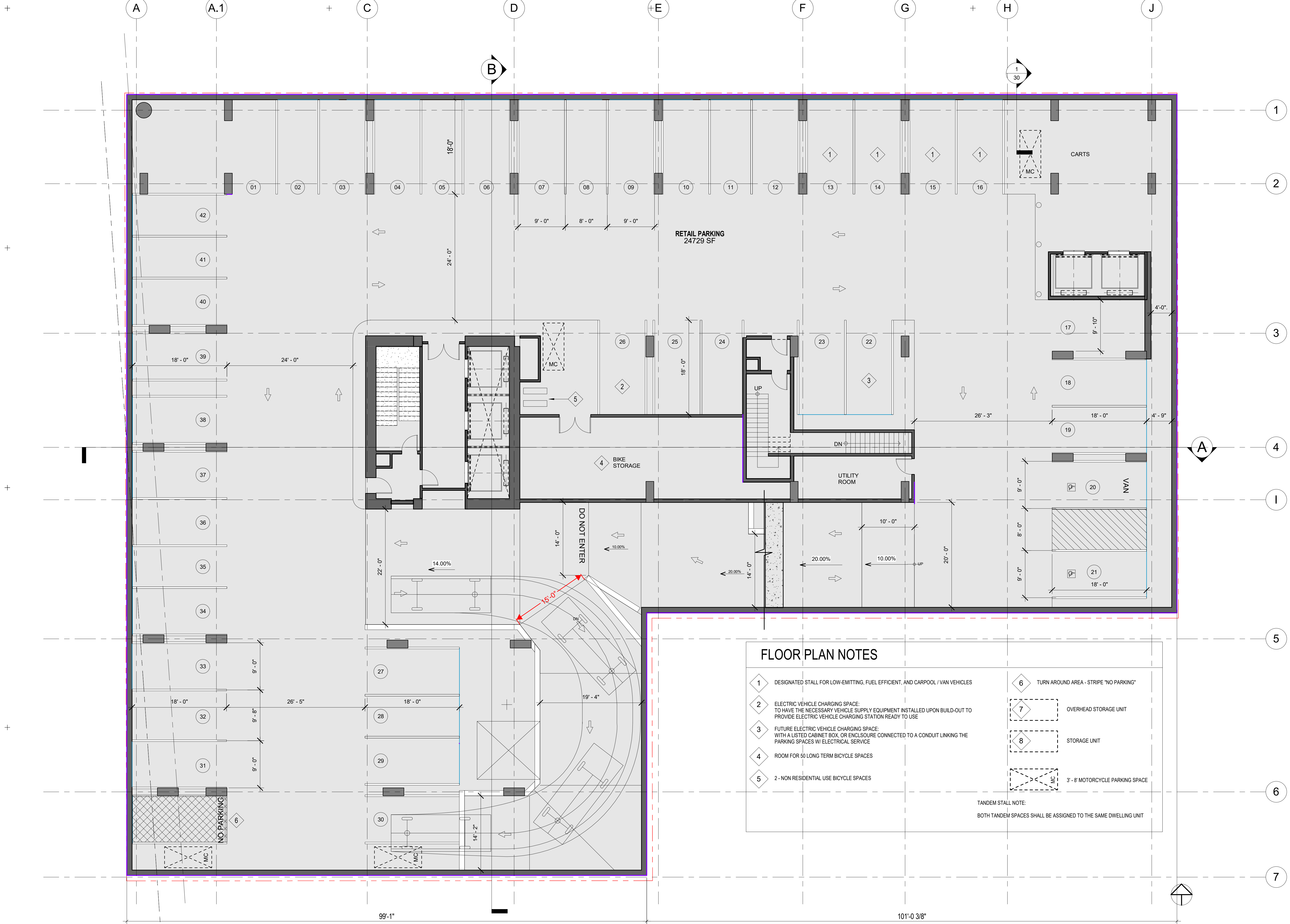
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-202003-01-Carter-2018.3.rvt  
DRAWN BY: Author  
CHECKED BY: Checker  
PLOT DATE:  
6/9/2020 2:02:41 PM  
TITLE:

Level 1 - Ground  
Floor



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Cisterra

DEVELOPMENT

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9G  
PLANNED DEVELOPMENT PERMIT  
PACKAGE

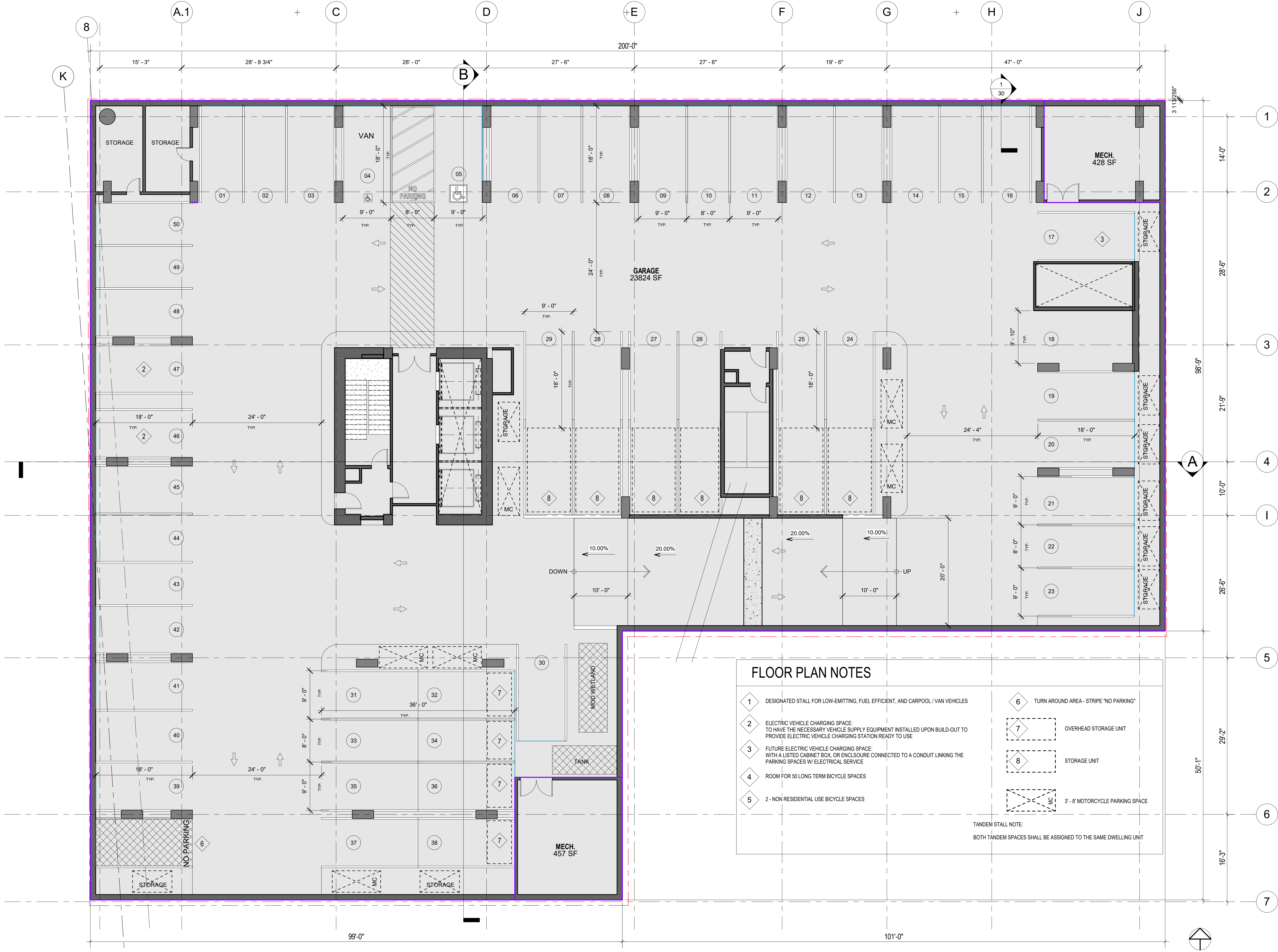
3rd SUBMITTAL  
6.9.20

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DRAWN BY: Author  
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PLOT DATE:  
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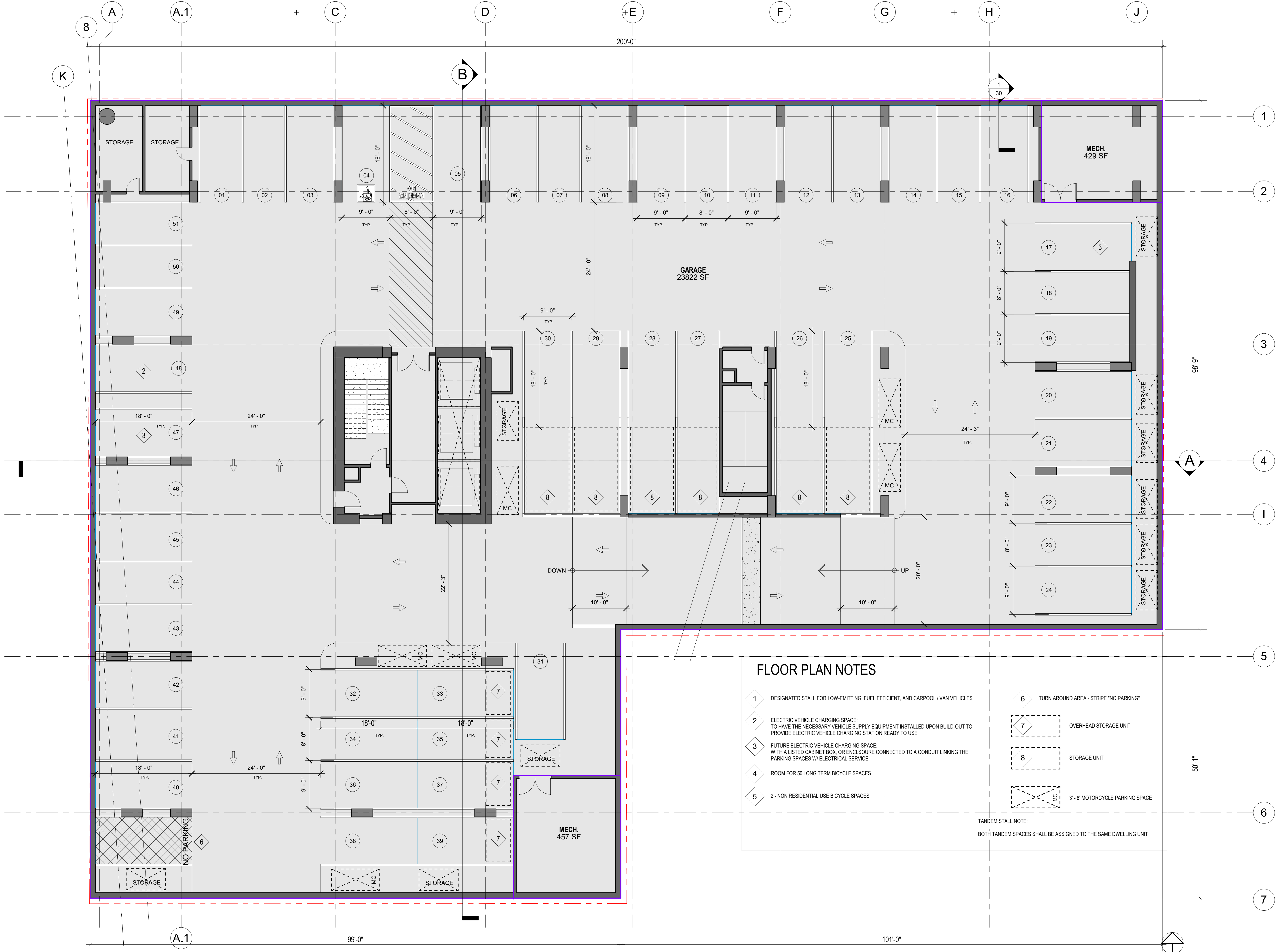
Level 2 - Parking

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9G  
PLANNED DEVELOPMENT PERMIT  
PACKAGE

3rd SUBMITTAL 6.9.20	ISSUES:
PROJECT NO: 5937.00	FILE NAME: BIM 300/5537 00-Culture -SEARCH - Culture95_2018.3.rvt
DRAWN BY: Author	CHECKED BY: Checker
PLOT DATE: 6/9/2020 2:02:46 PM	TITLE:

Level 4 - Parking

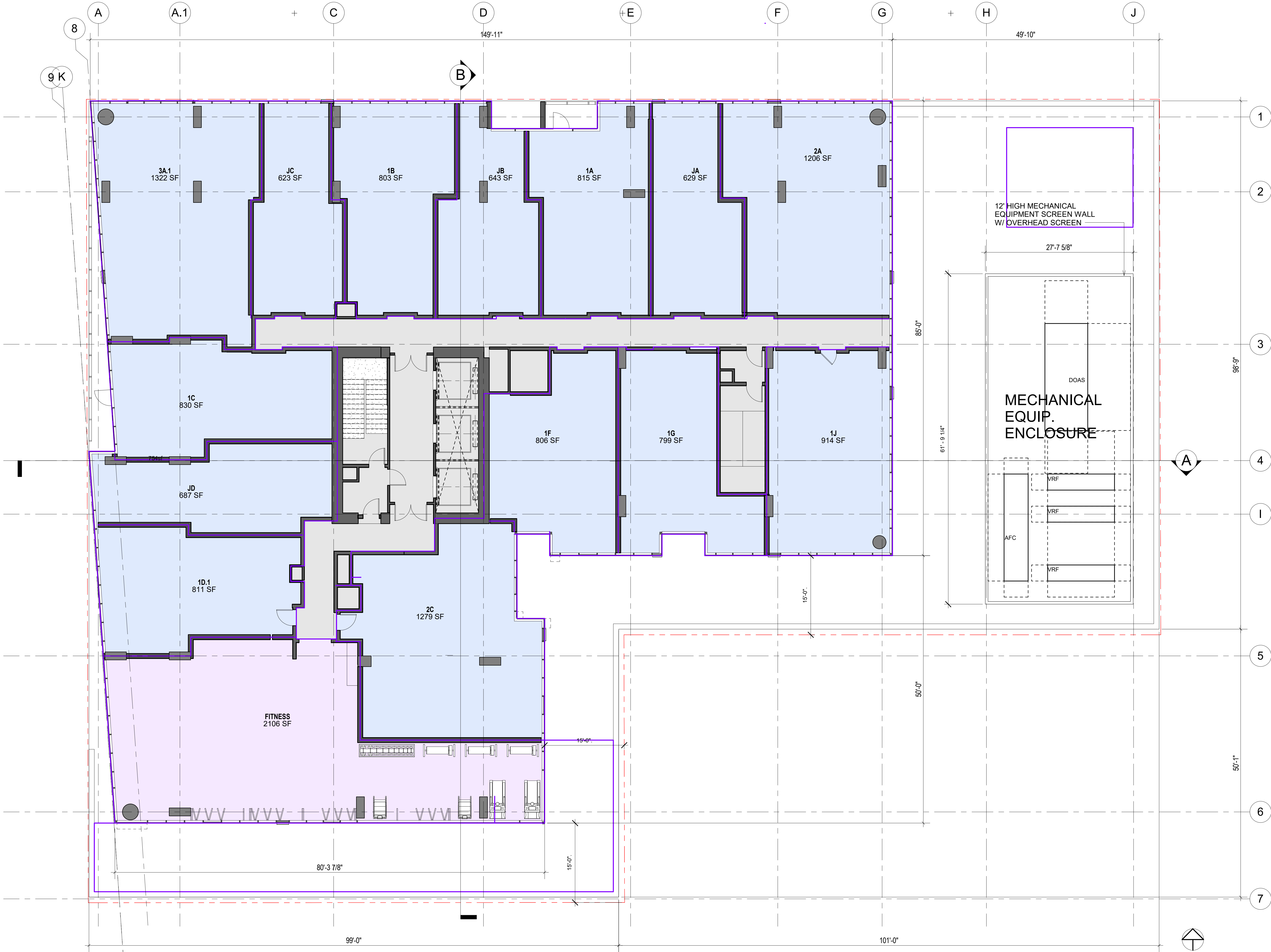
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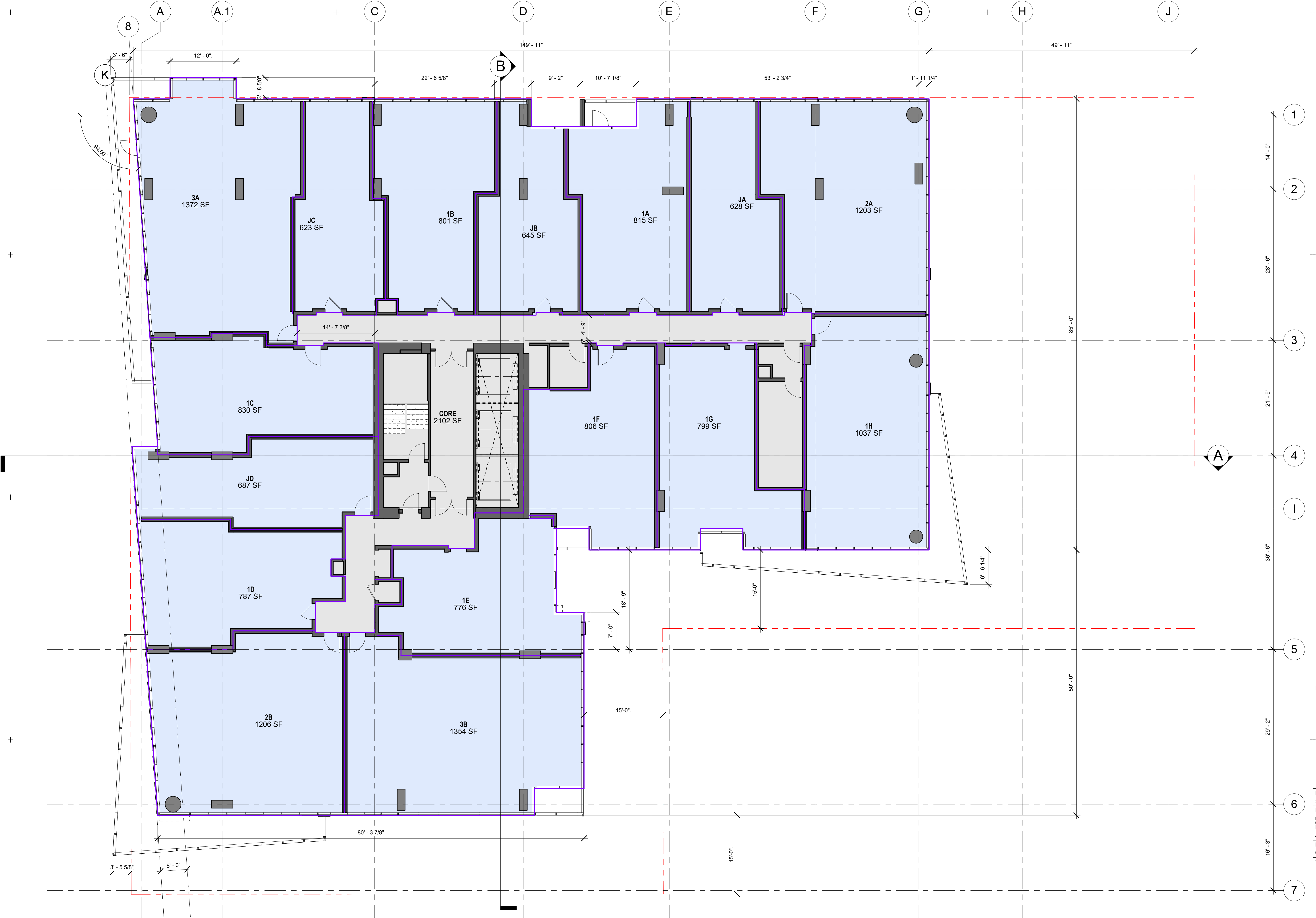


**9G PLANNED DEVELOPMENT PERMIT PACKAGE**

3rd SUBMITTAL 6.9.20	
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DRAWN BY: Author	CHECKED BY: Checker
PLOT DATE: 6/9/2020 2:02:53 PM	
TITLE:	

Level 7 -  
Residential

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Cisterra

DEVELOPMENT

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9G

PLANNED DEVELOPMENT PERMIT  
PACKAGE

3rd SUBMITTAL  
6.9.20

ISSUES:

PROJECT NO:  
5937.00

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Author Checker

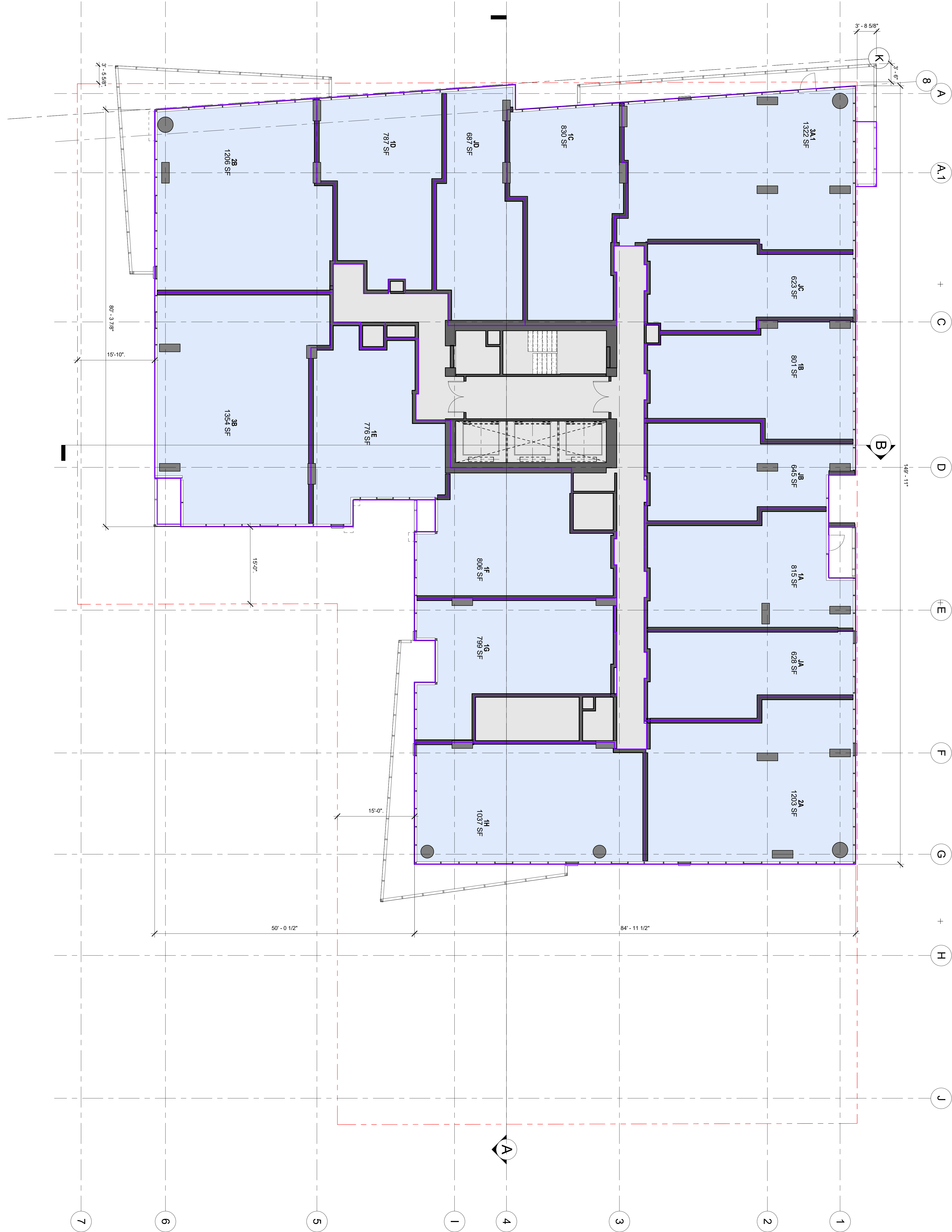
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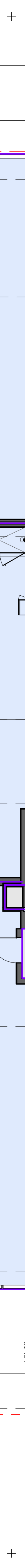
Levels 8-19 -  
Residential

16





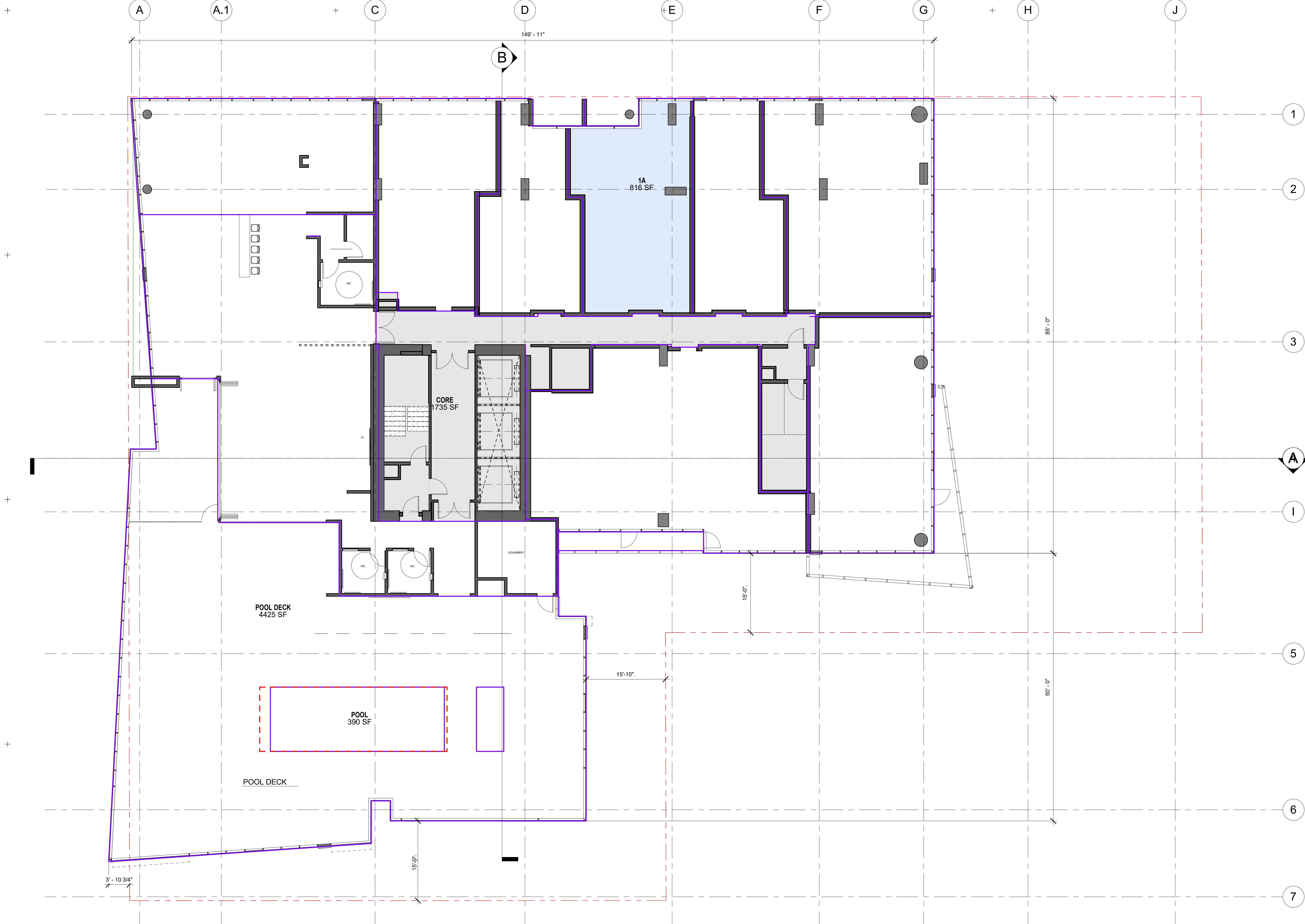


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PLANNED DEVELOPMENT PERMIT  
PACKAGE

3rd SUBMITTAL  
6.9.20

ISSUES:

PROJECT NO:  
5937.00

FILE NAME:  
9GARCH\_Culture95\_2018.3.rvt

DRAWN BY: Author  
CHECKED BY: Checker

PLOT DATE:  
6/9/2020 2:03:03 PM

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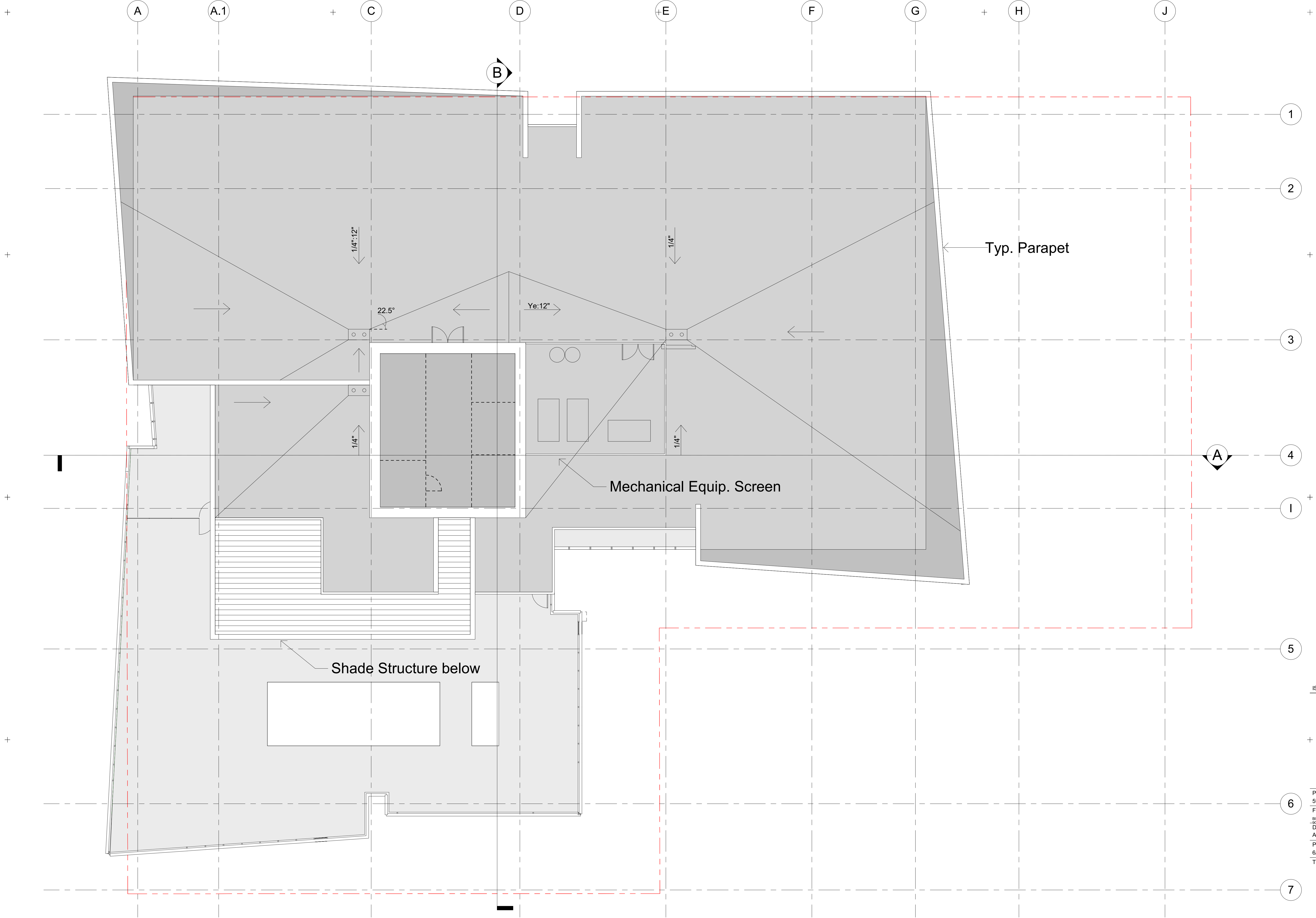
Level 22 -  
Amenities



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9G  
PLANNED DEVELOPMENT PERMIT  
PACKAGE

3rd SUBMITTAL  
6.9.20

ISSUES:

PROJECT NO:  
5937.00

FILE NAME:  
B:\M 360\5937.00-Carterra  
-2020\ARCH - Carterra\9G\_2018.3.rvt

DRAWN BY: Author  
CHECKED BY: Checker

PLOT DATE:  
6/9/2020 2:03:05 PM

TITLE:

Roof Level



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9G PLANNED DEVELOPMENT PERMIT PACKAGE

3rd SUBMITTAL  
6.9.20

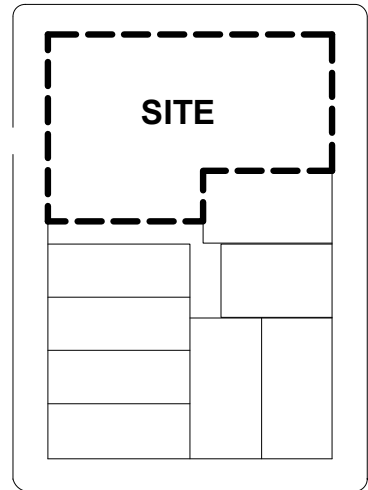
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DRAWN BY: Author  
CHECKED BY: Checker  
PLOT DATE:  
6/9/2020 2:03:08 PM  
TITLE:

Elevation - North



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MATERIAL SELECTION

- 1 Window Wall Glazing System
- 2 Concrete Panel
- 3 Glass Railing
- 4 Angled/Flat Metal Panel
- 5 Metal Louver mechanical
- 6 Steel Canopy
- 7 Spandrel Window Wall
- 8 Steel Canopy
- 9 Lighting Signage Element

3rd SUBMITTAL  
6.9.20

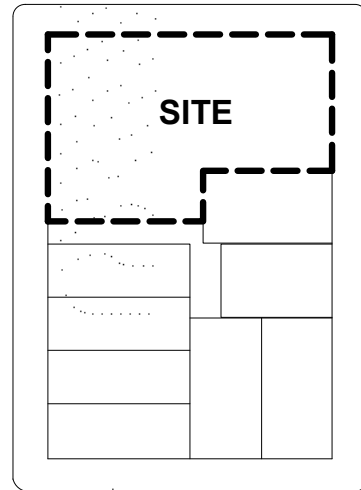
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BIM 360/5537 00-Culture  
-ARCH - Culture05\_2018.3.rvt  
DRAWN BY: Author  
CHECKED BY: Checker  
PLOT DATE:  
6/9/2020 2:03:10 PM  
TITLE:

Elevation - West



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**MATERIAL SELECTION**

- 1 Window Wall Glazing System
- 2 Concrete Panel
- 3 Glass Railing
- 4 Angled/Flat Metal Panel
- 5 Metal Louver mechanical
- 6 Steel Canopy
- 7 Spandrel Window Wall
- 8 Steel Canopy
- 9 Lighting Signage Element

**9G PLANNED DEVELOPMENT PERMIT PACKAGE**

3rd SUBMITTAL  
6.9.20

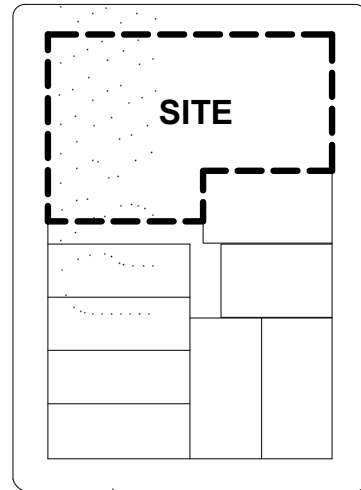
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DRAWN BY: CHECKED BY:  
Author Checker  
PLOT DATE:  
6/9/2020 2:03:14 PM  
TITLE:

Elevation - South



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**MATERIAL SELECTION**

- Window Wall Glazing System
- Concrete Panel
- Glass Railing
- Angled/flat Metal Panel
- Metal Louver mechanical
- Steel Canopy
- Spandrel Window Wall
- Steel Canopy
- Lighting Signage Element

3rd SUBMITTAL  
6.9.20

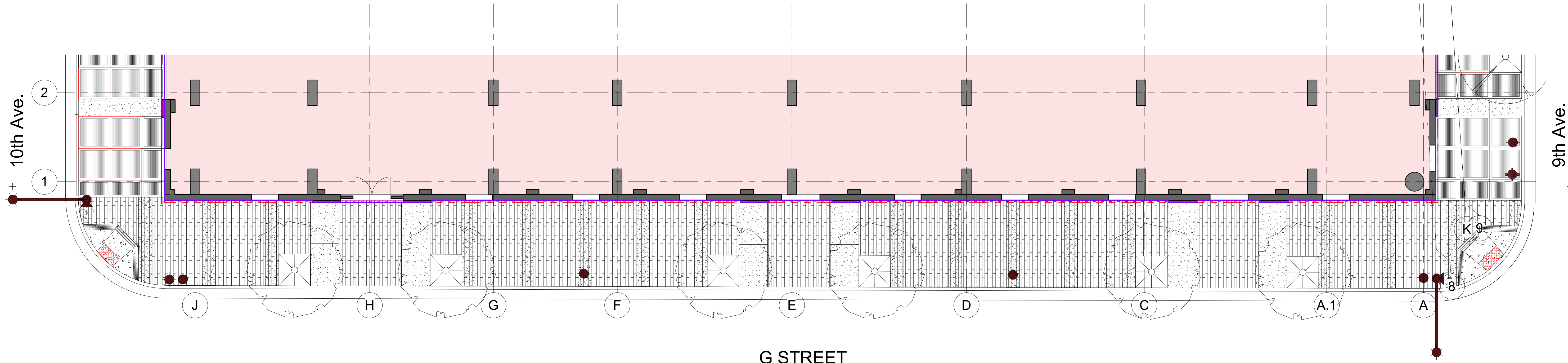
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DRAWN BY: CHECKED BY:  
Author Checker  
PLOT DATE:  
6/9/2020 2:03:17 PM  
TITLE:

Elevation - East



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**MATERIAL SELECTION**

- 1 Lighting Signage Element
- 2 Angled/Flat Metal Panel
- 3 Concrete Panel
- 4 Spandrel Window Wall
- 5 Precast Concrete

3rd SUBMITTAL  
6.9.20

ISSUES:

PROJECT NO:  
5837.00  
FILE NAME:  
BIM 360/5837.00-Cisterra  
-03/ARCH - Cisterra03\_2018.3.rvt  
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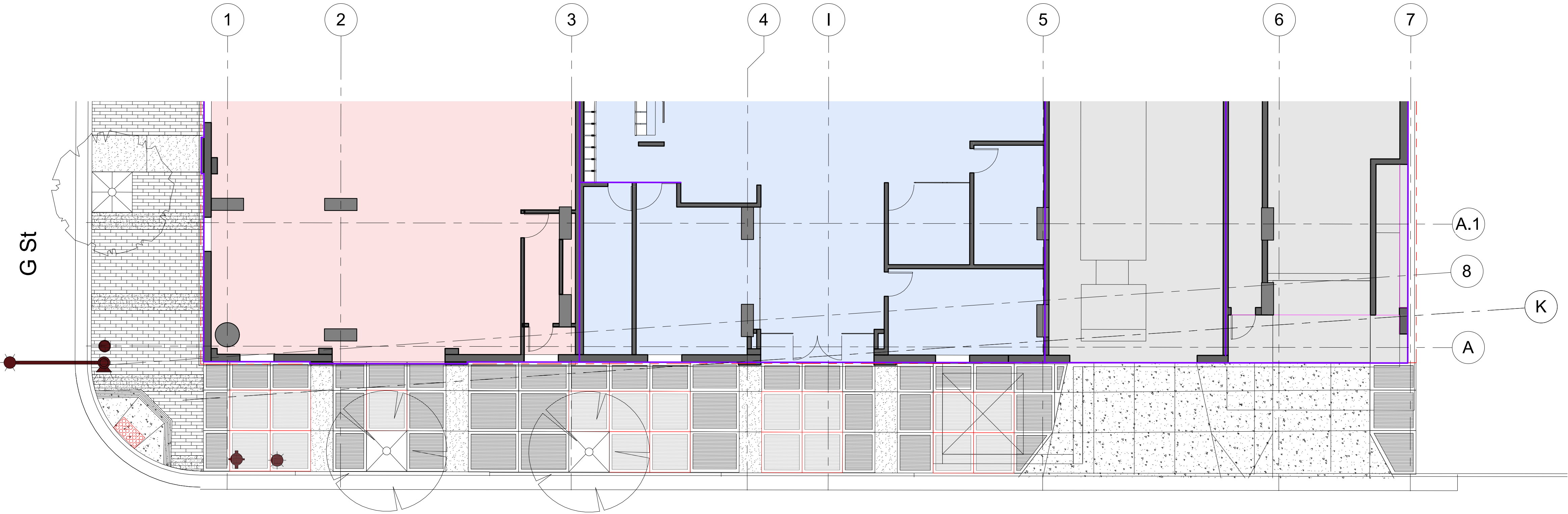
Enlarged North  
Elevation



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MATERIAL SELECTION

- 1 Lighting Signage Element
- 2 Angled/Flat Metal Panel
- 3 Concrete Panel
- 4 Spandrel Window Wall
- 5 Precast Concrete



3rd SUBMITTAL  
6.9.20

ISSUES:

PROJECT NO:  
5937.00  
FILE NAME:  
BIM 360/5937.00-Cisterra  
-931ARCH-Cisterra90\_2018.3.rvt  
DRAWN BY: Author  
CHECKED BY: Checker  
PLOT DATE:  
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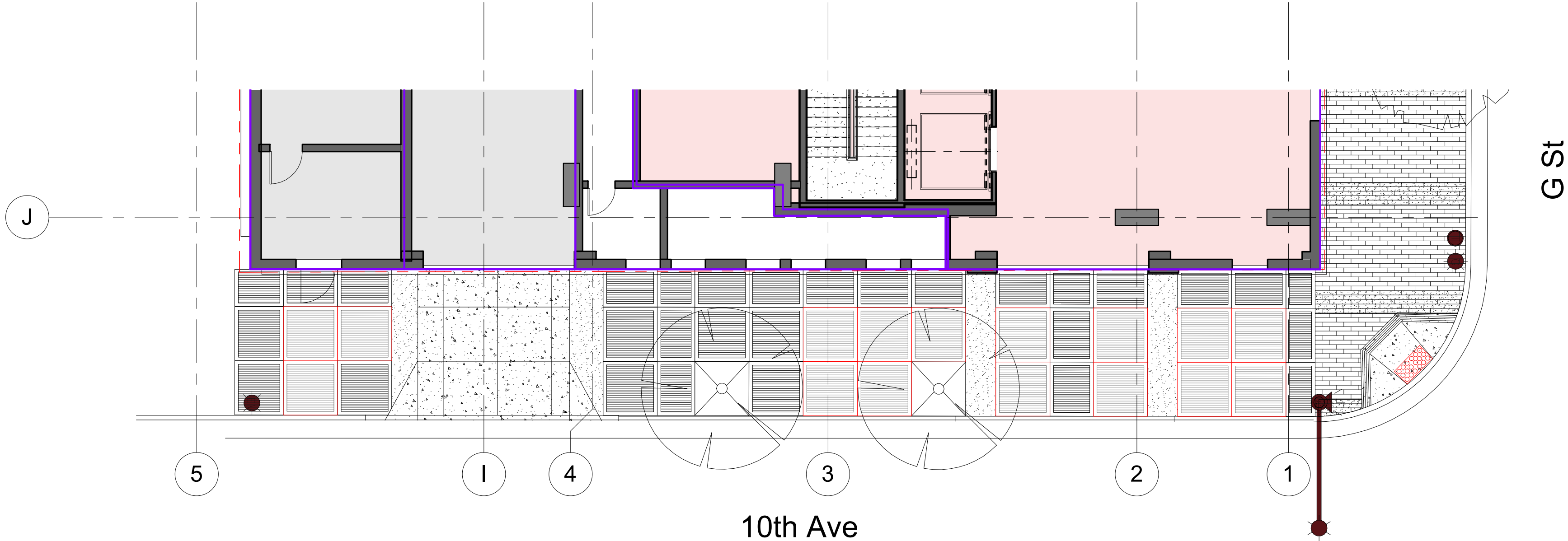
Enlarged West  
Elevation



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MATERIAL SELECTION

- 1 Lighting Signage Element
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3rd SUBMITTAL  
6.9.20

ISSUES:

PROJECT NO:  
5837.00

FILE NAME:

BIM 360://5837.00-Cisterra

-03/ARCH - Cisterra#00\_2018.3.rvt

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Author

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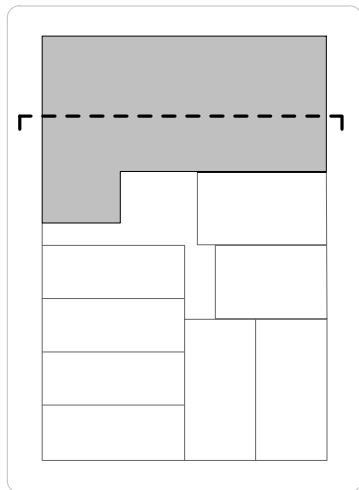
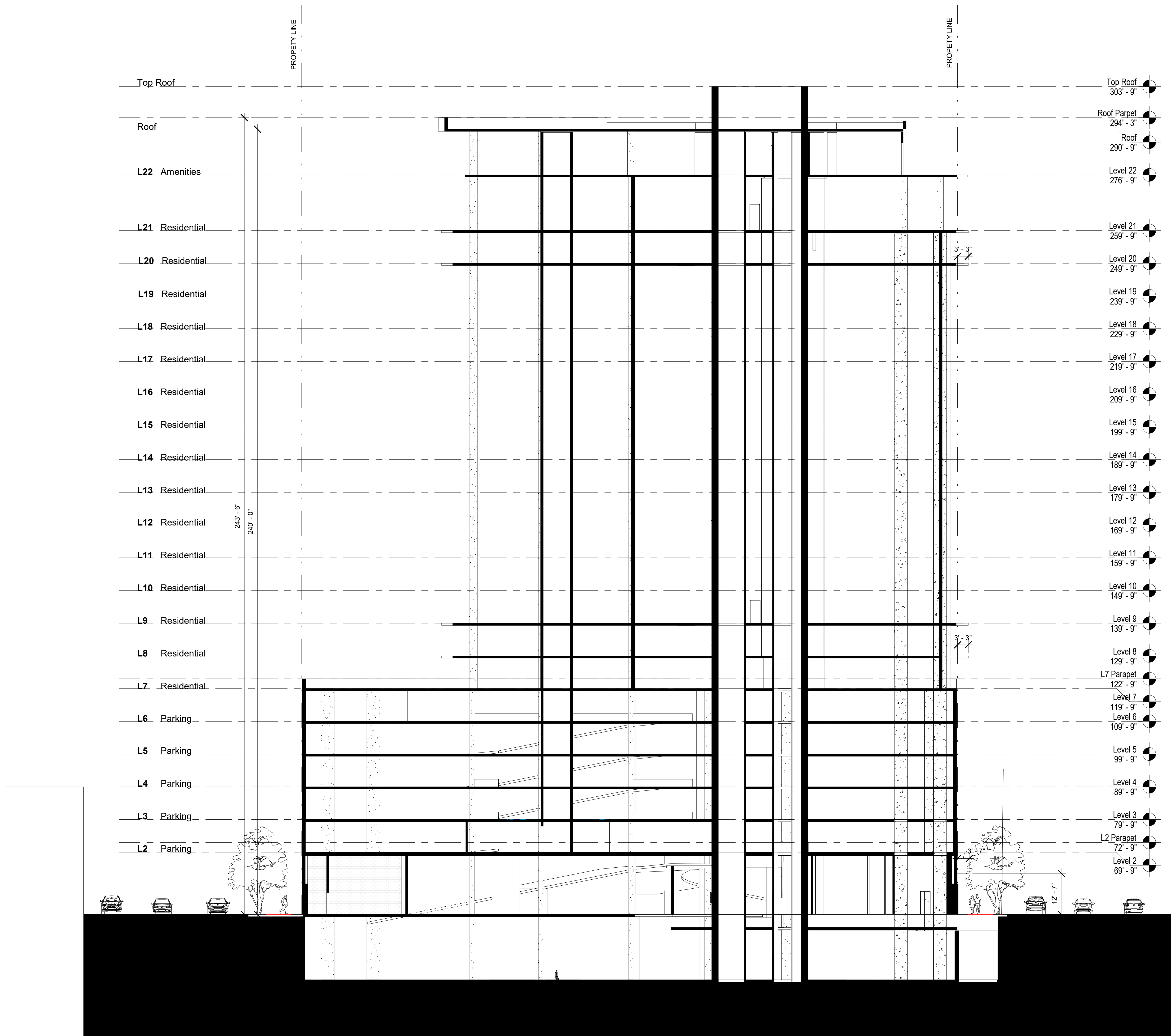
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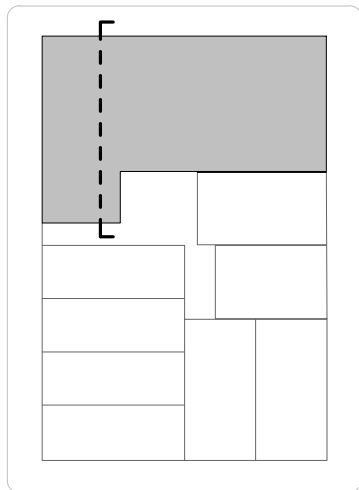
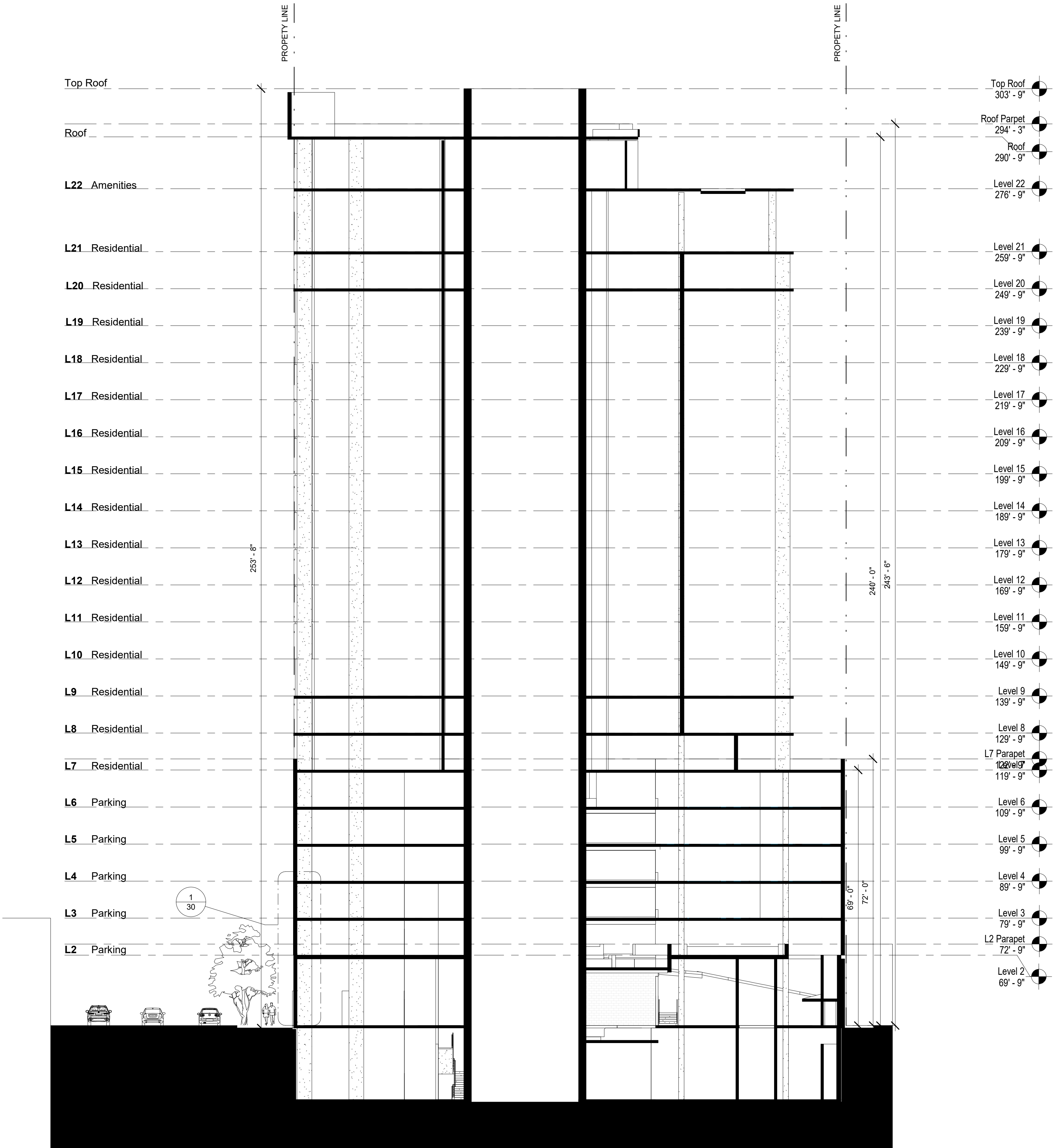
Enlarged East  
Elevation

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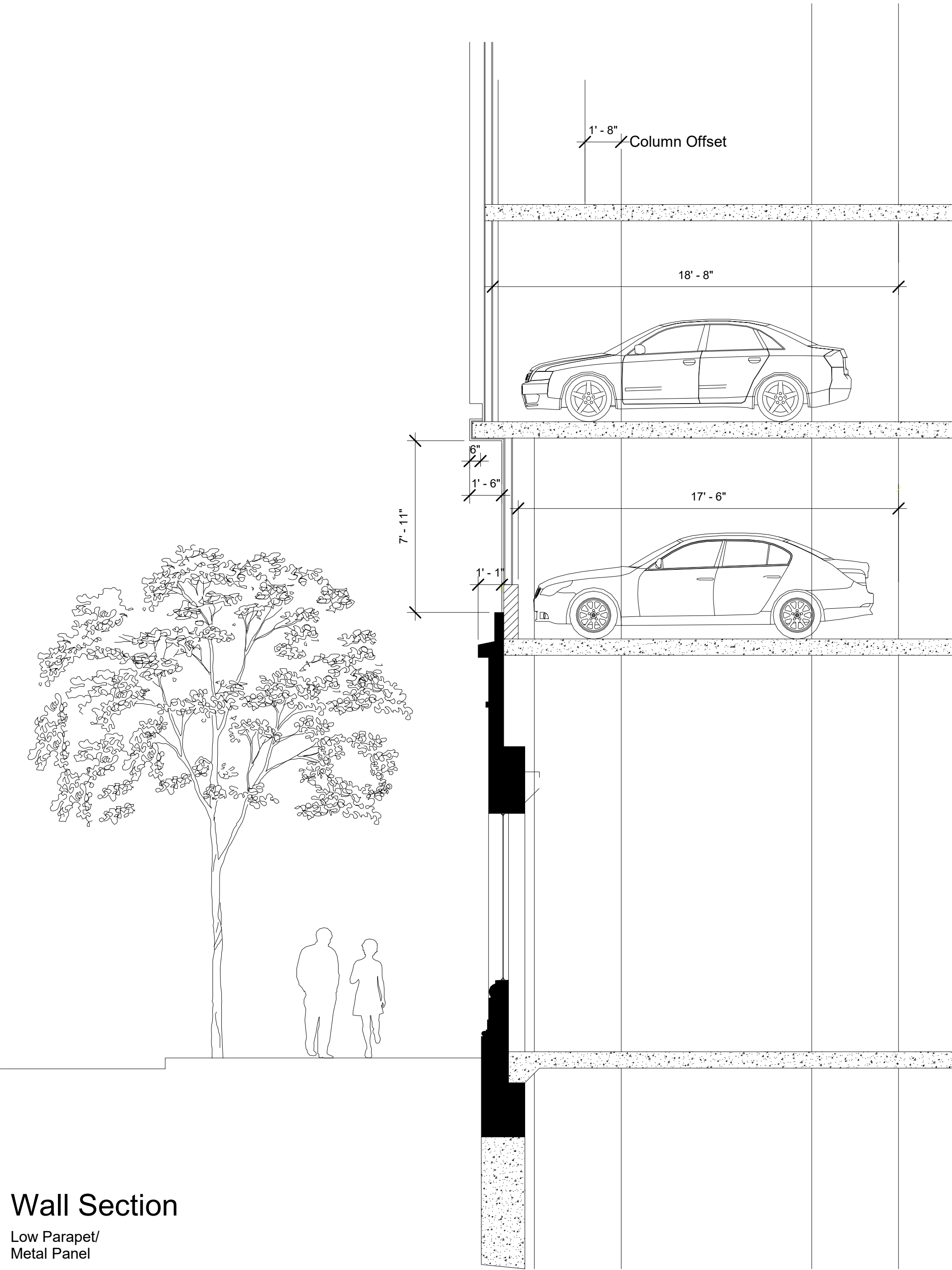




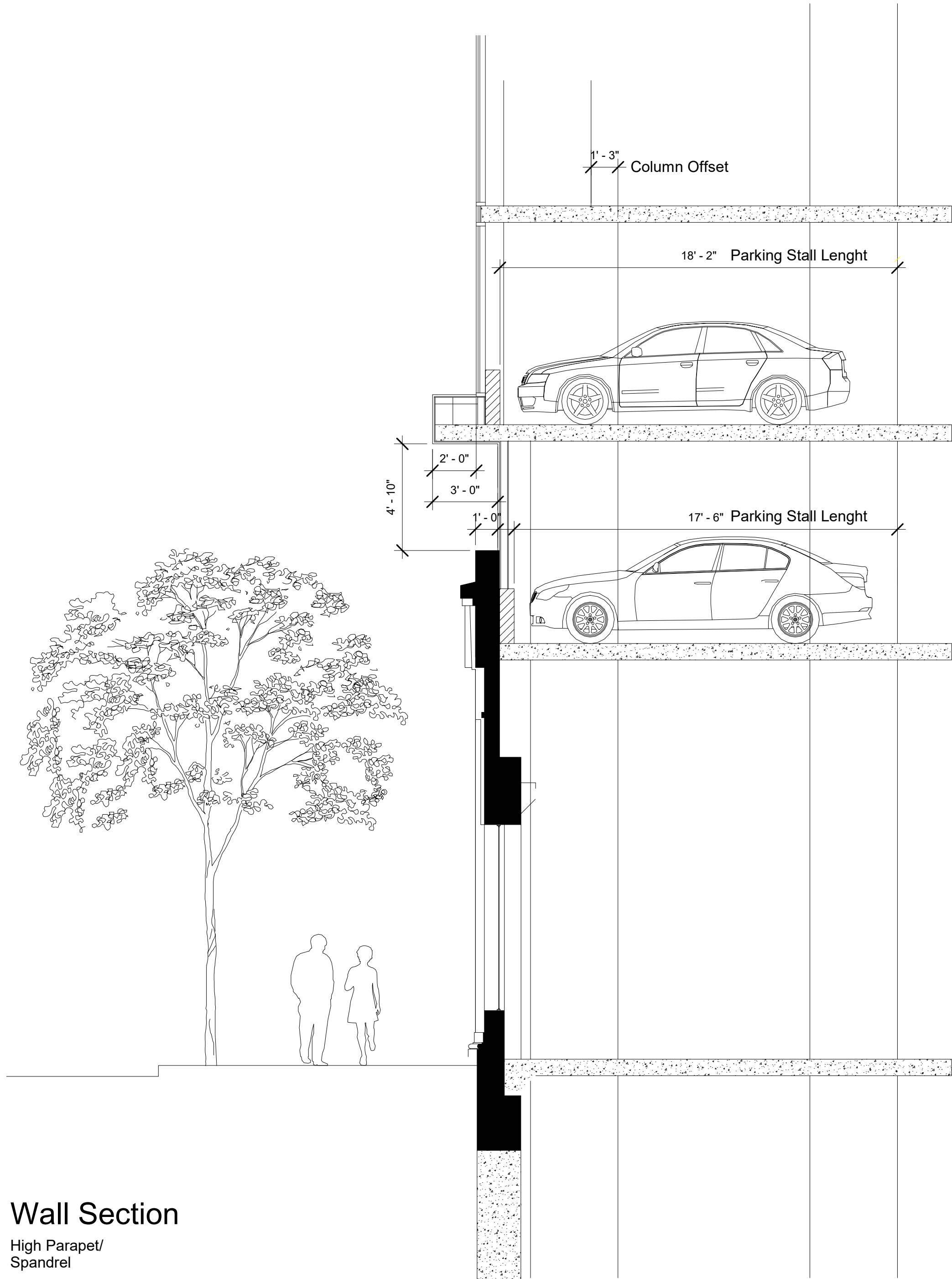
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Wall Section  
Low Parapet/  
Metal Panel



Wall Section  
High Parapet/  
Spandrel

9G  
PLANNED DEVELOPMENT PERMIT  
PACKAGE

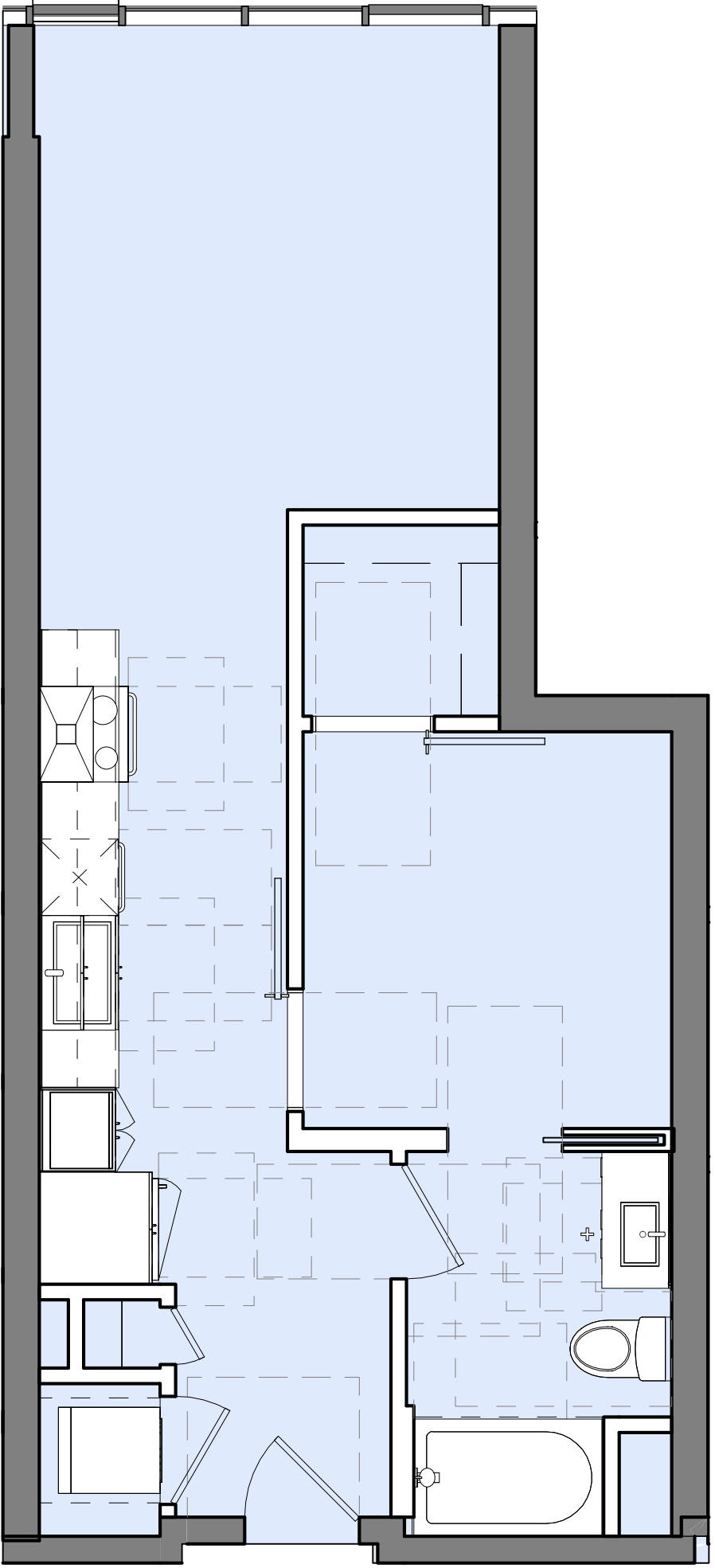
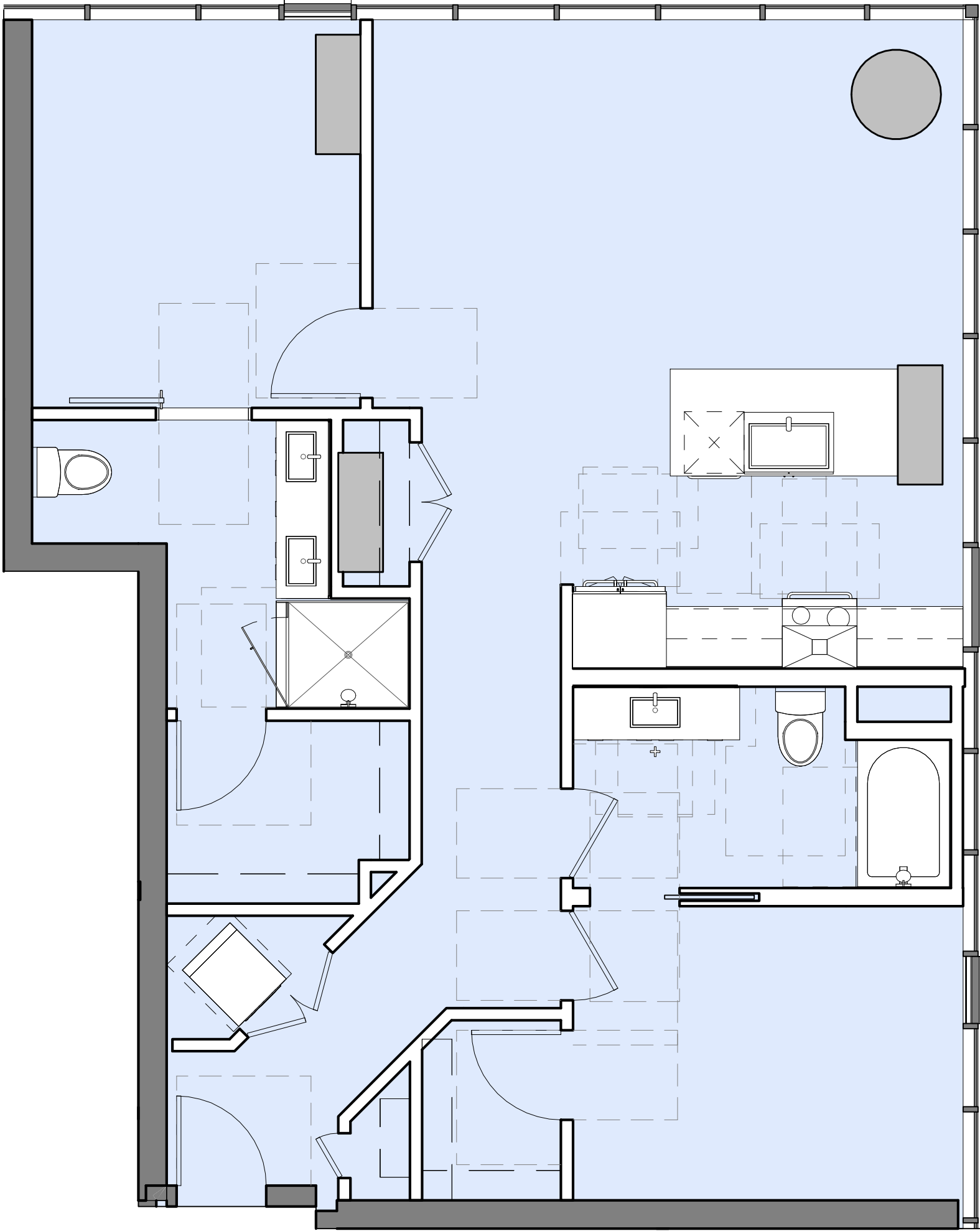
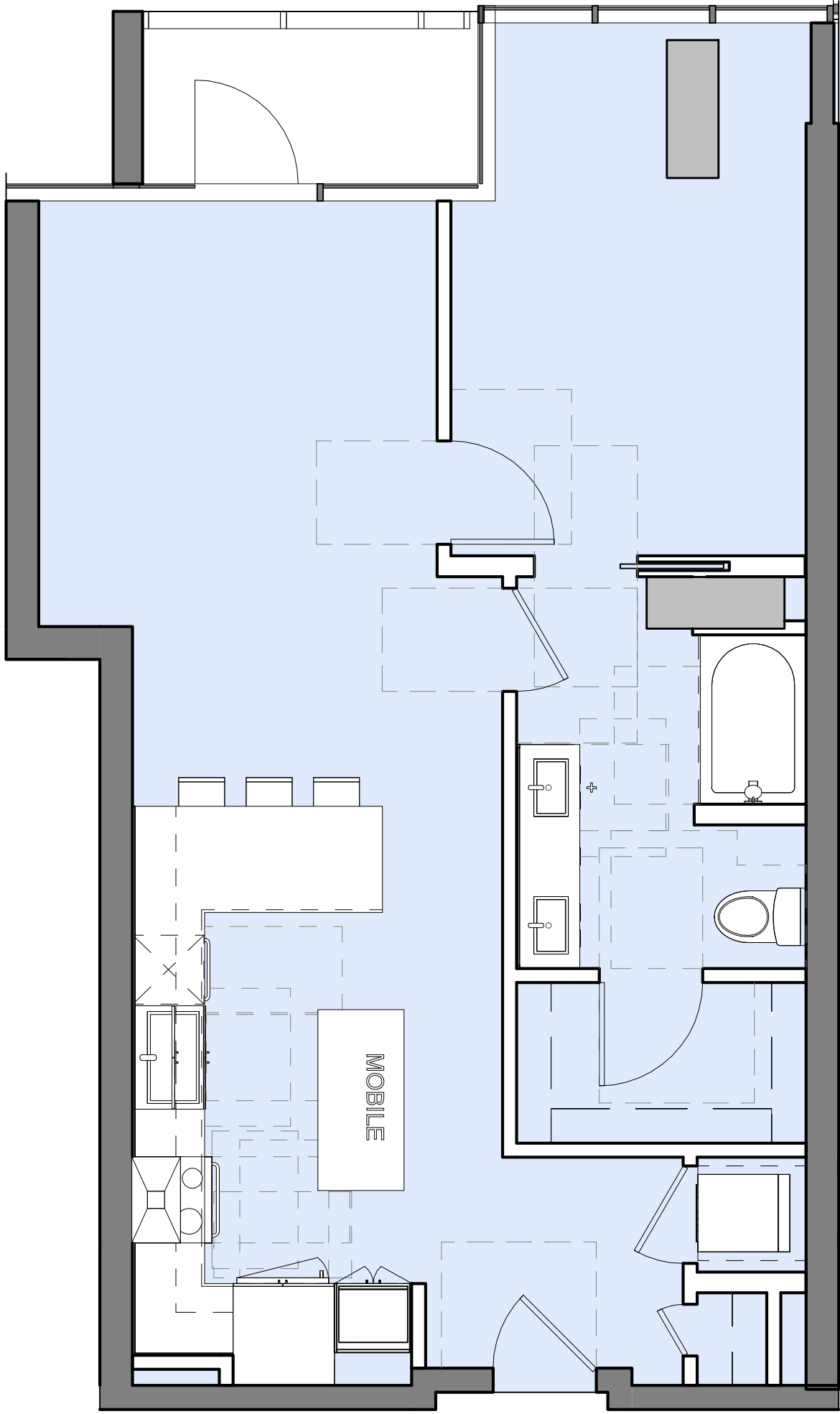
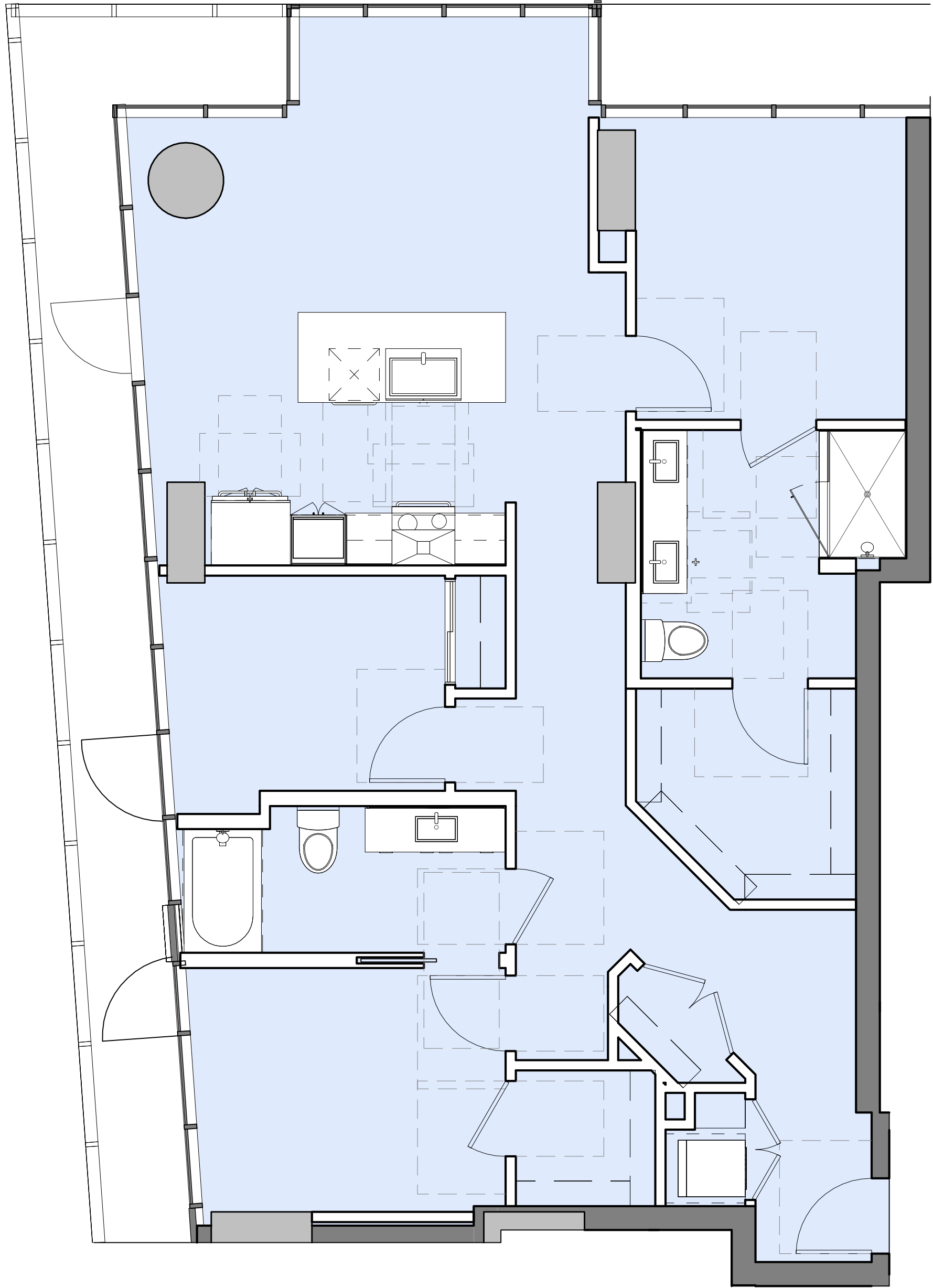
3rd SUBMITTAL  
6.9.20

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5937.00  
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-2020-06-09-09-2018.3.rvt  
DRAWN BY: CHECKED BY:  
Author Checker  
PLOT DATE:  
6/9/2020 2:03:27 PM  
TITLE:

Detail Section

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PLANNED DEVELOPMENT PERMIT  
PACKAGE

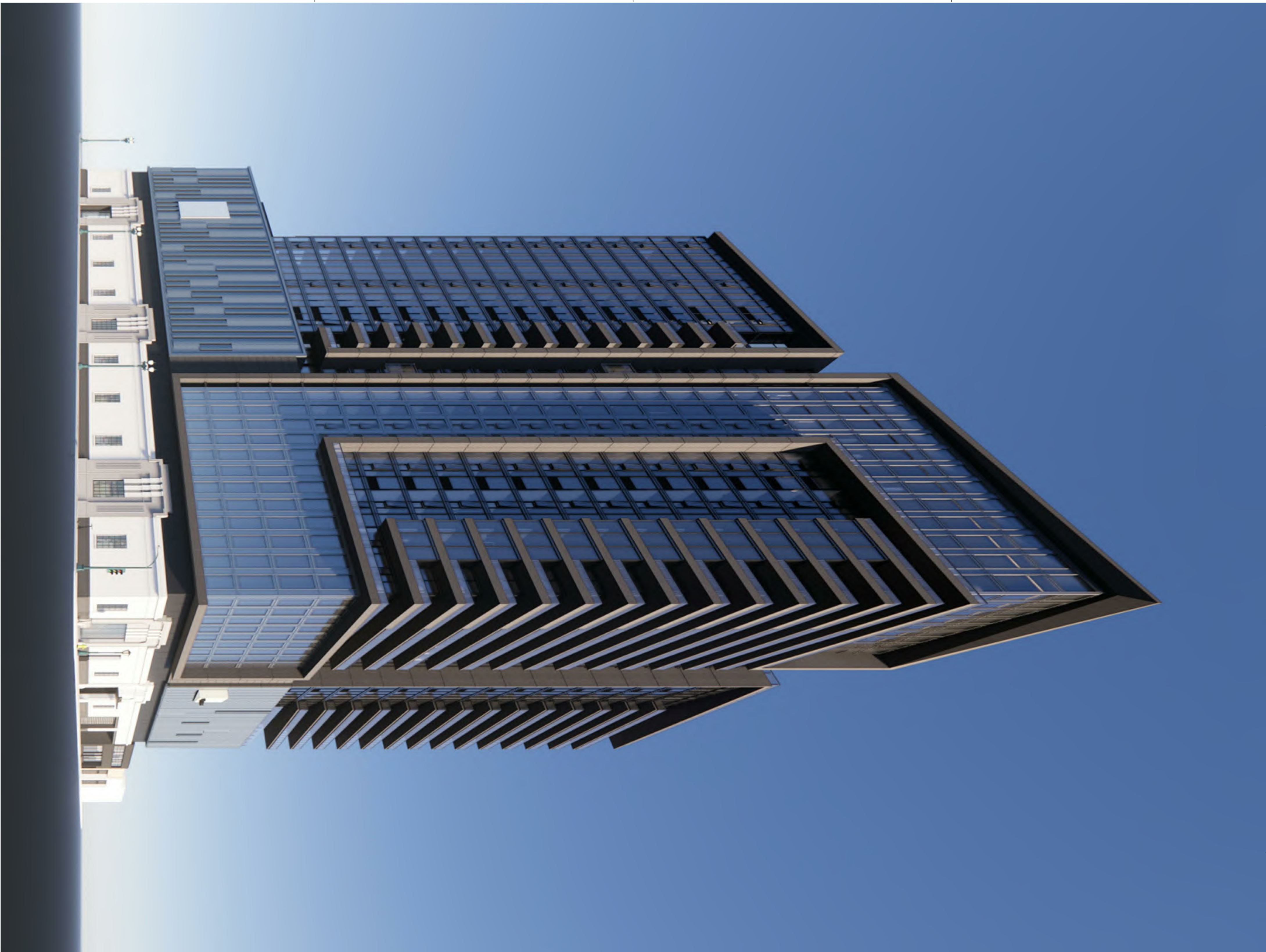
3rd SUBMITTAL  
6.9.20

ISSUES:

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5937.00  
FILE NAME:  
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DRAWN BY: CHECKED BY:  
Author Checker  
PLOT DATE:  
6/9/2020 2:03:30 PM  
TITLE:

Enlarged Units





3rd SUBMITTAL  
6.9.20

ISSUES:

9G  
PLANNED DEVELOPMENT PERMIT  
PACKAGE



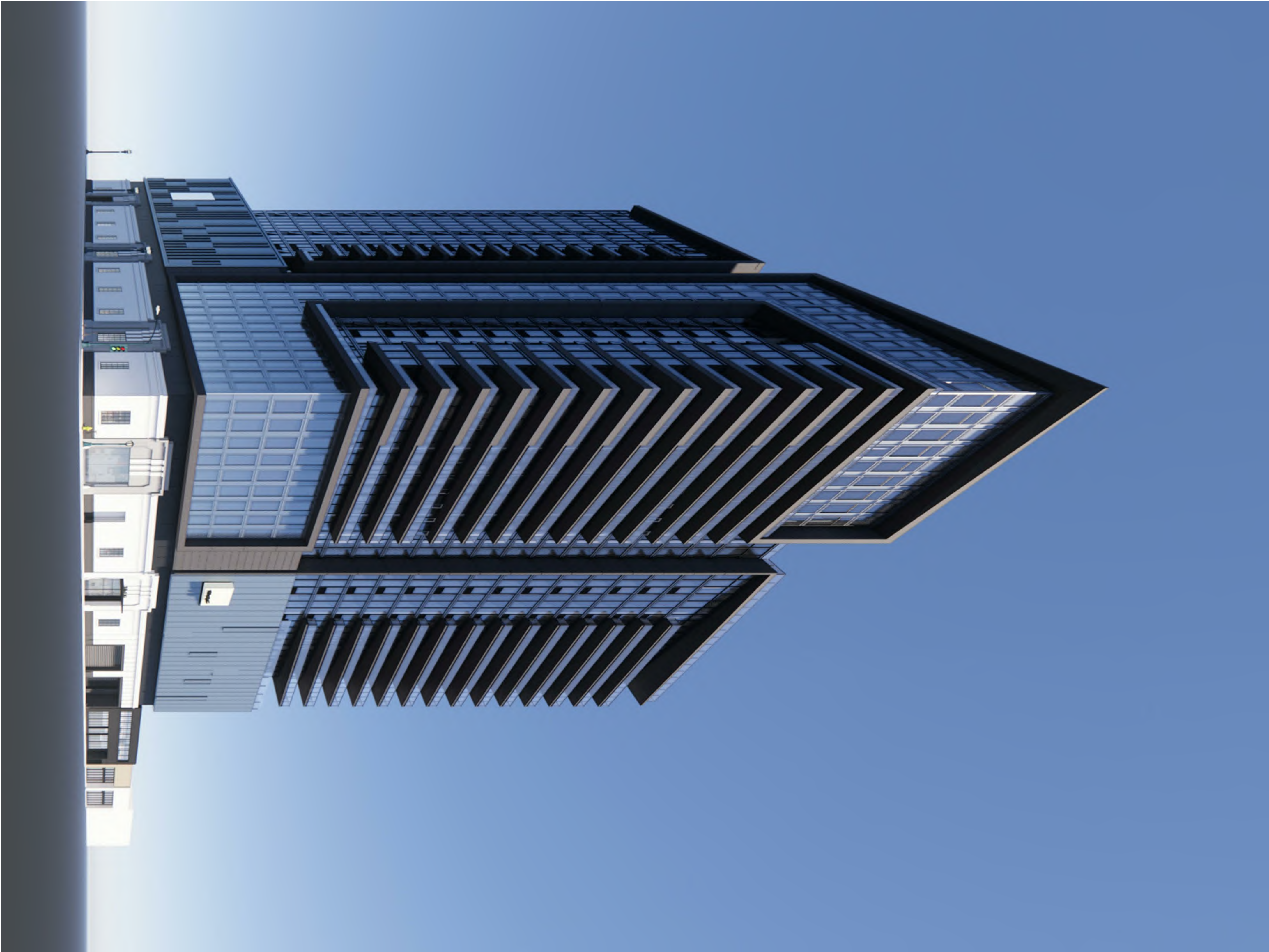
**carrierjohnson + CULTURE**  
architecture + environments + brand strategy + graphics

185 W F St suite 500, San Diego, CA 92101  
phone 619.239.2353

PROJECT NO:	5937.00
FILE NAME:	BPA 9007097.00-CARRIER
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Perspective





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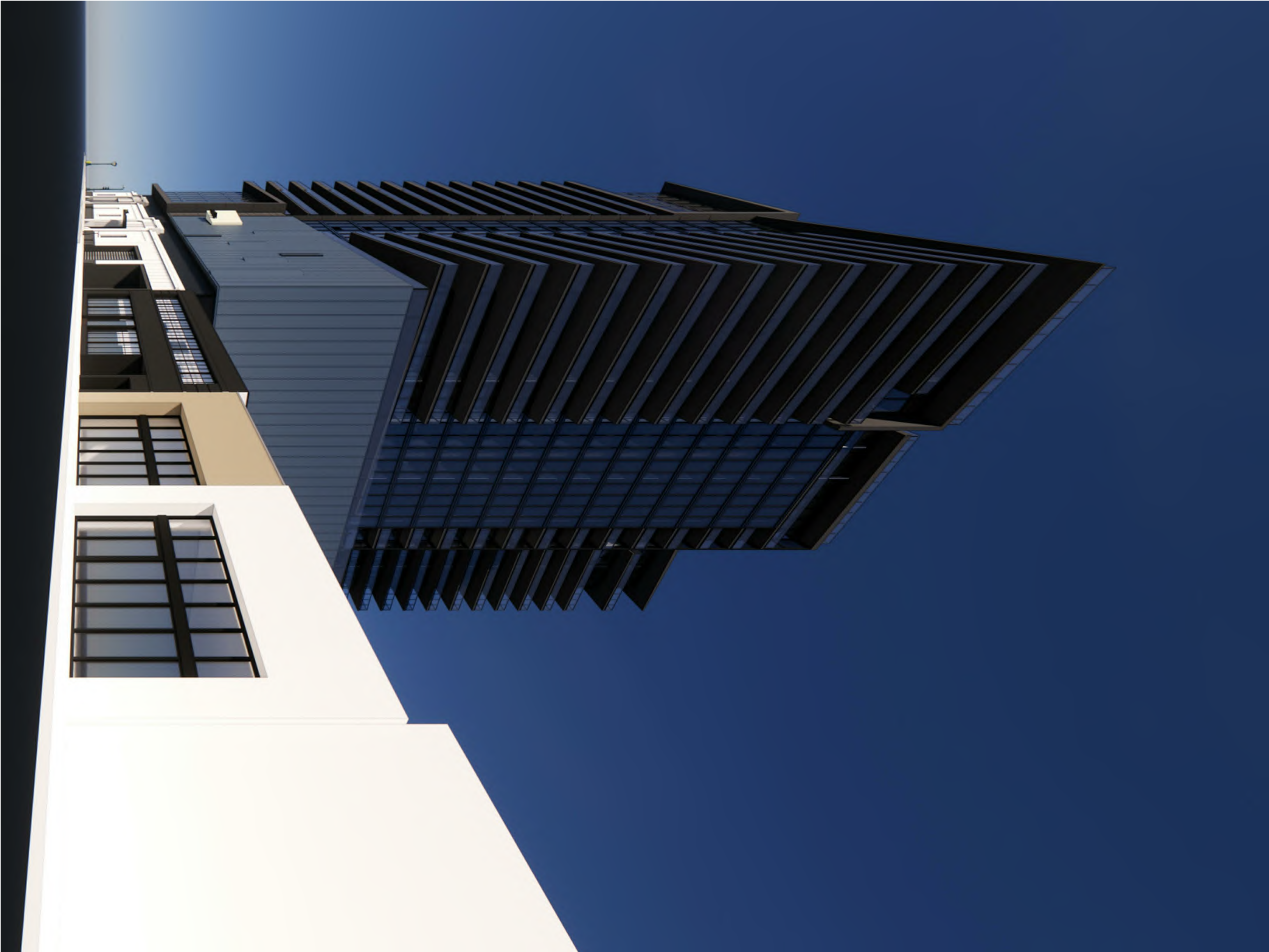
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Perspective









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## 9G PLANNED DEVELOPMENT PERMIT PACKAGE



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Perspective



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ISSUES:

PROJECT NO:  
5937.00  
FILE NAME:  
BM 300/5937 00-Cisterra  
-ARCH - Cisterra\_2018.3.rvt  
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9G  
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PACKAGE

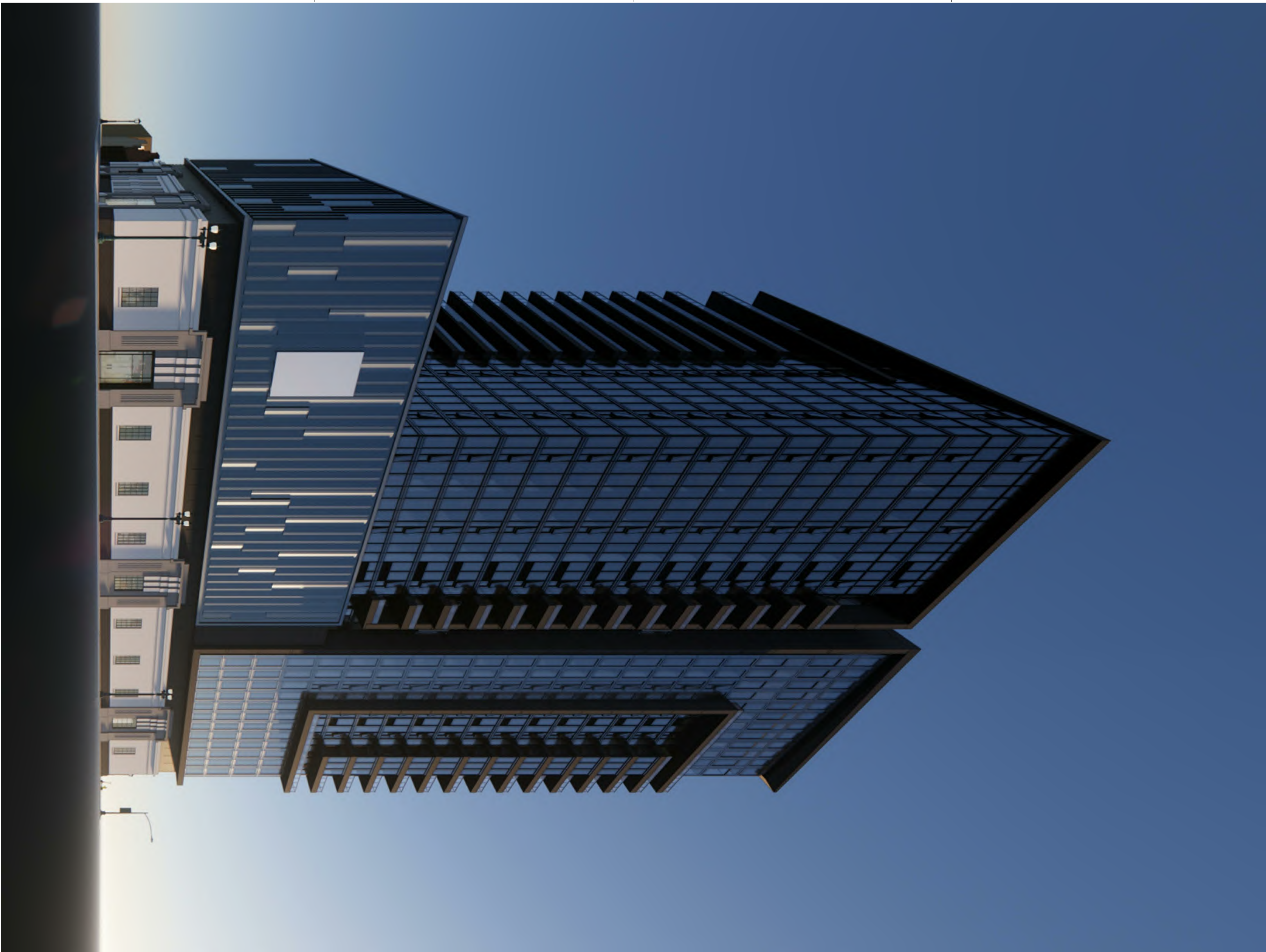
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6/9/2020 2:03:35 PM  
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Perspective





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ISSUES:

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PROJECT NO:	5937.00
FILE NAME:	BPA 9007097.00-CARRIER
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Perspective



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ISSUES:

PROJECT NO.

5937:00

FILE NAME

BIM 360://5937.00-Cisterra  
-9GJARCH Cisterra9G\_2018.3.rvt

Author: \_\_\_\_\_  
 Drawn by: \_\_\_\_\_  
 Checked by: \_\_\_\_\_

Adults	Children
<hr/>	
PLOT DATE	

6/9/2020 2:03:37 PM

TITLE:

## Perspective

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architecture + environments + brand strategy + graphics

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## ISSUES:

PROJECT NO:

5937.00

FILE NAME:

BIM 360.M5937.00-Cierra  
AG/ARCH Cierra9G 2018.3.rvt

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Author	Checker

PLOT DATE: 2-03-20 PM

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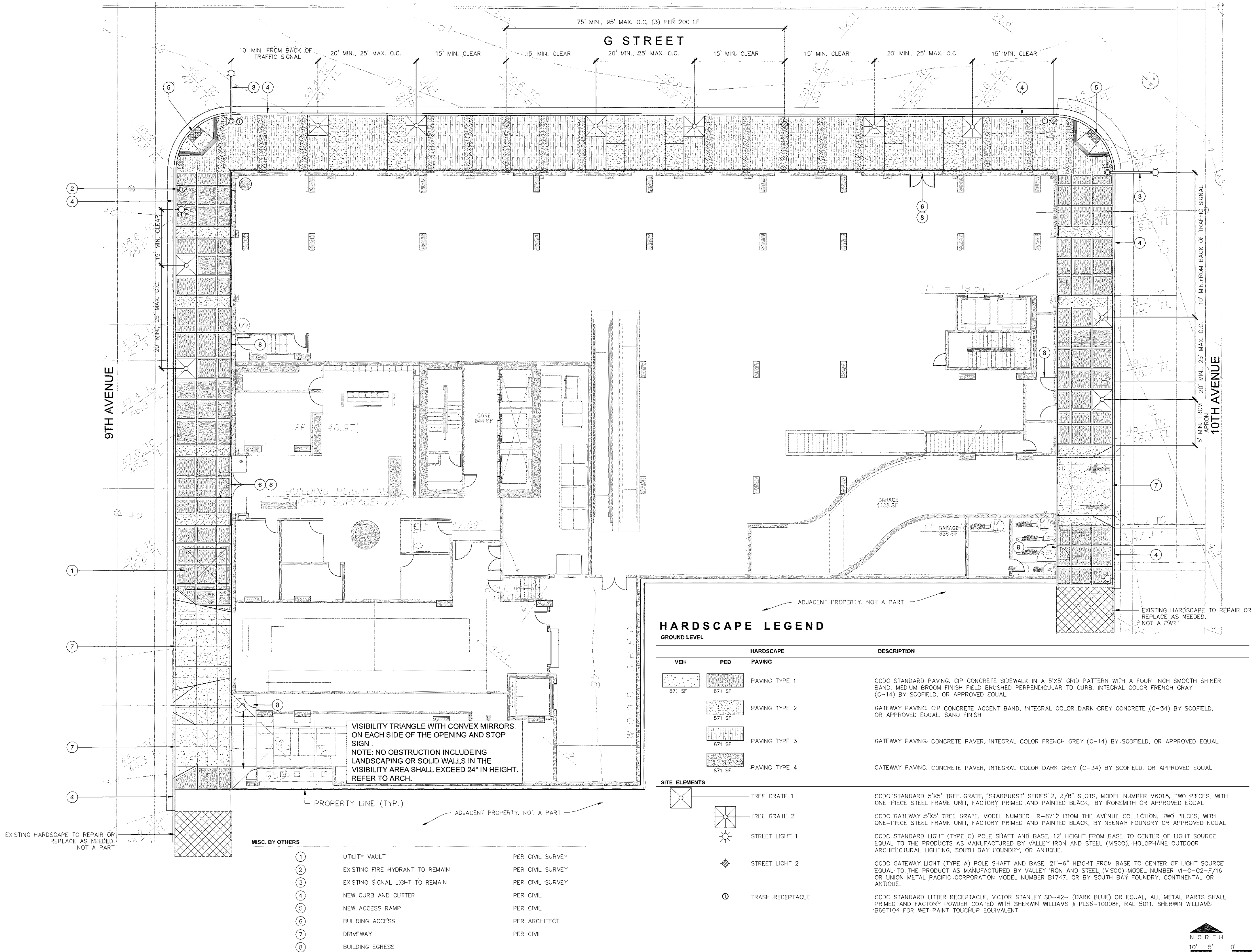
## Perspective

**carrierjohnson + CULTURE**  
architecture + environments + brand strategy + graphics

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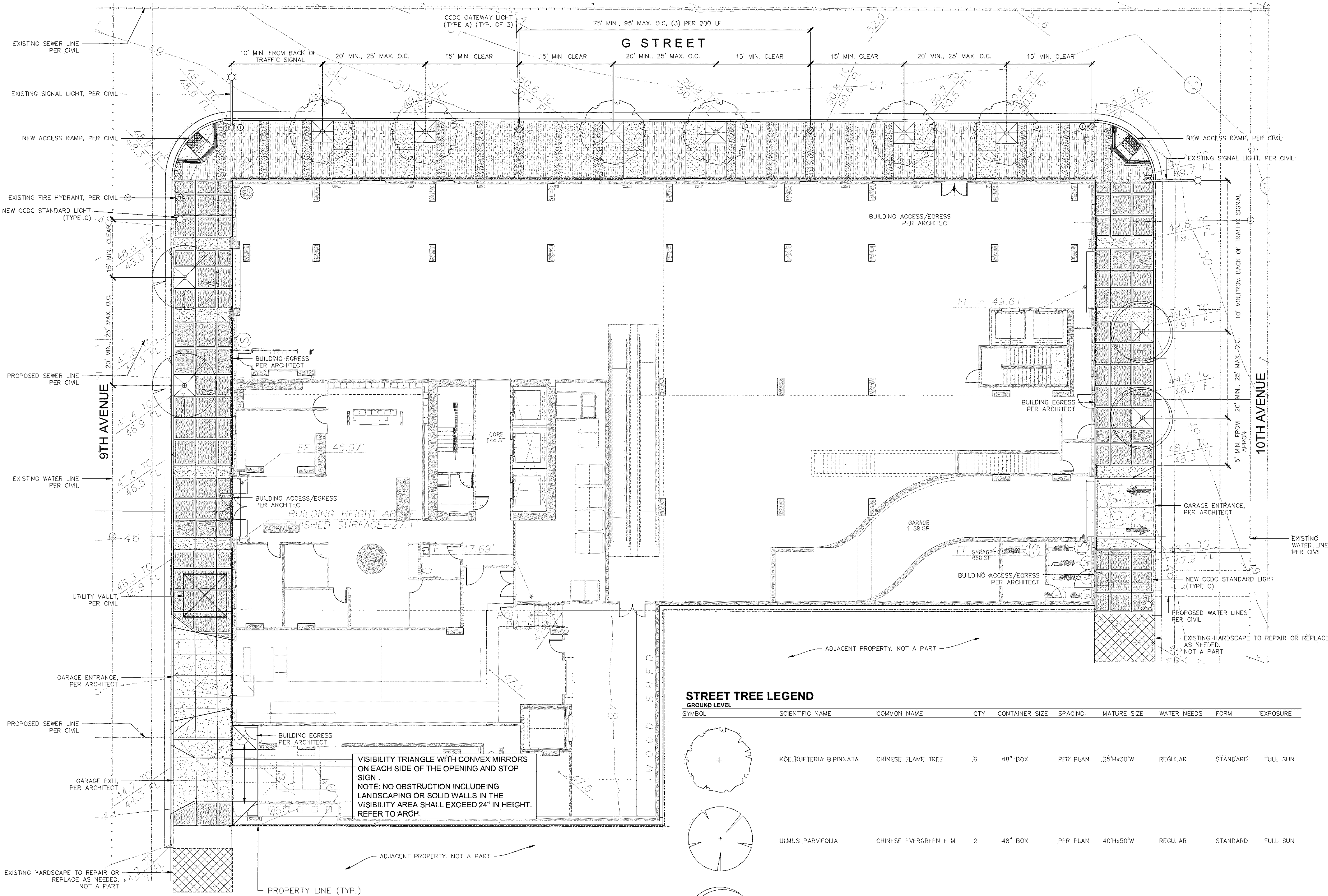


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**LONG-TERM MAINTENANCE / CONFORMANCE**

ALL RIGHT-OF-WAY LANDSCAPE AS SHOWN ON THESE PLANS SHALL BE MAINTAINED BY OWNER. THE RIGHT-OF-WAY SHALL BE MAINTAINED FREE OF DEBRIS AND LITTER AND ALL PLANT MATERIAL SHALL BE MAINTAINED IN A HEALTHY GROWING CONDITION CONSISTENT WITH CENTRE CITY STREET DESIGN MANUAL AND THE CITY OF SAN DIEGO LANDSCAPE REGULATIONS AND STANDARDS.

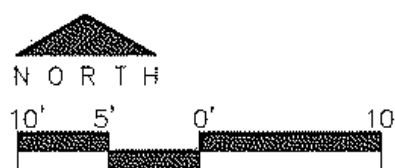
**MINIMUM TREE SEPARATION DISTANCE**

FIFTEEN (15) FEET MINIMUM FROM STREET LIGHTS.  
FIVE (5) FEET MINIMUM FROM DRIVEWAY APRON FLARES.  
TWENTY (20) FEET MINIMUM FROM THE FRONT OF ANY TRAFFIC SIGNAL OR STOP SIGN.  
TEN (10) FEET MINIMUM FROM THE BACK OF ANY TRAFFIC SIGNAL OR STOP SIGN.  
FIVE (5) FEET MINIMUM FROM UNDERGROUND UTILITIES.  
TEN (10) FEET MINIMUM FROM SEWER LINES.

**STREET TREE LEGEND**

SYMBOL	SCIENTIFIC NAME	COMMON NAME	QTY	CONTAINER SIZE	SPACING	MATURE SIZE	WATER NEEDS	FORM	EXPOSURE
	KOELRUTERIA BIPINNATA	CHINESE FLAME TREE	6	48" BOX	PER PLAN	25'Hx30'W	REGULAR	STANDARD	FULL SUN
	ULMUS PARVIFOLIA	CHINESE EVERGREEN ELM	2	48" BOX	PER PLAN	40'Hx50'W	REGULAR	STANDARD	FULL SUN
	JACARANDA MIMOSIFOLIA	JACARANDA	2	48" BOX	PER PLAN	30'Hx25'W	REGULAR	STANDARD	FULL SUN

TREES PLANTED IN URBAN SPACE AREAS SHOULD HAVE A MINIMUM PLANTING AREA OF 100 SQUARE FEET, WITH A MINIMUM SOIL DEPTH OF 5 FEET



ISSUES:

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5937.00  
FILE NAME:  
BIM 300/5937-00-Calendar  
-06-ARCH - Calendar-05\_2018.3.rvt  
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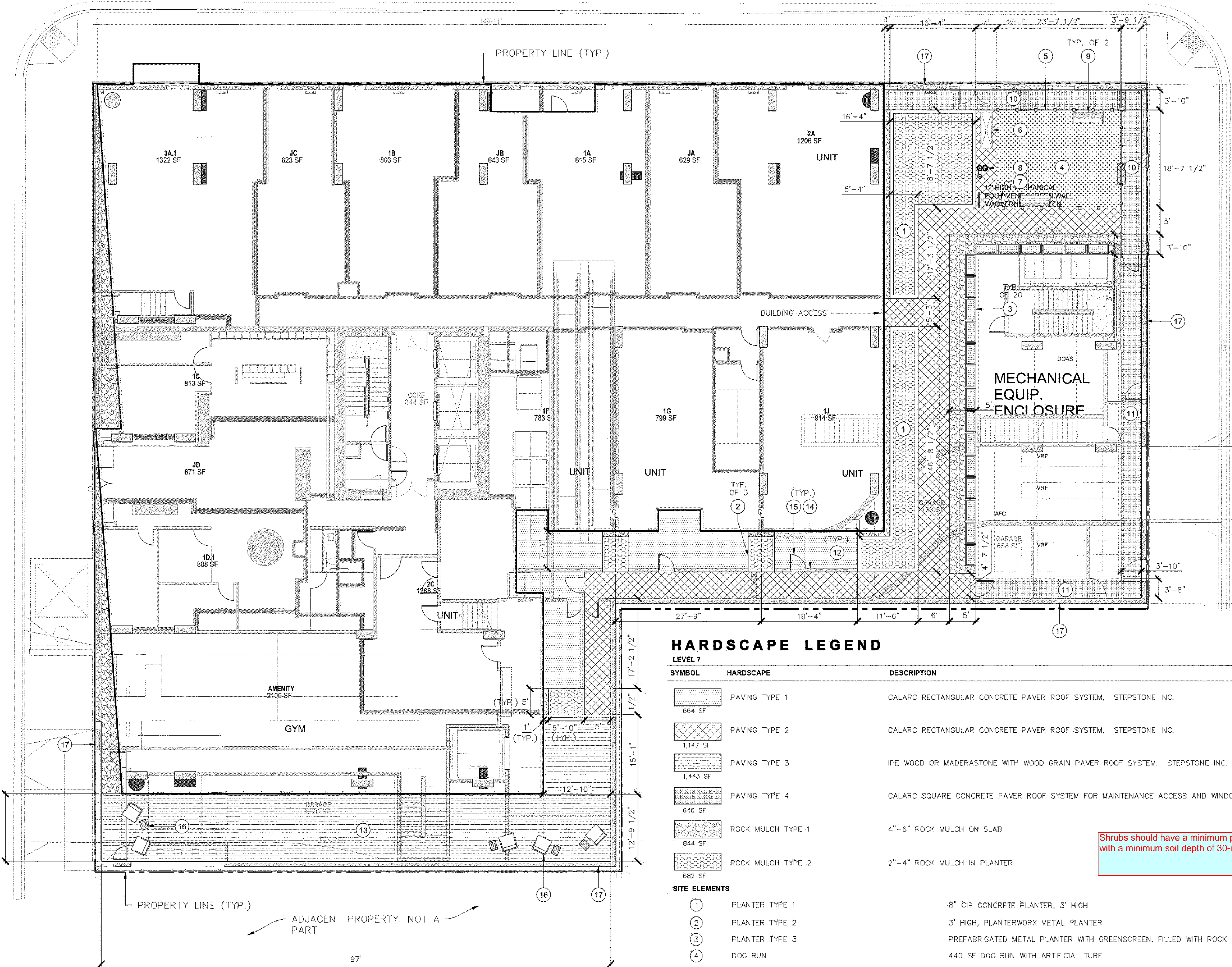
Street Tree Plan



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9TH AVENUE

12'-10"



### HARDSCAPE LEGEND

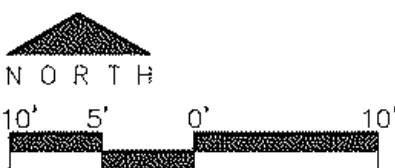
LEVEL 7

SYMBOL	HARDSCAPE	DESCRIPTION
	PAVING TYPE 1	CALARC RECTANGULAR CONCRETE PAVER ROOF SYSTEM, STEPSTONE INC.
	PAVING TYPE 2	CALARC RECTANGULAR CONCRETE PAVER ROOF SYSTEM, STEPSTONE INC.
	PAVING TYPE 3	IPE WOOD OR MADERASTONE WITH WOOD GRAIN PAVER ROOF SYSTEM, STEPSTONE INC.
	PAVING TYPE 4	CALARC SQUARE CONCRETE PAVER ROOF SYSTEM FOR MAINTENANCE ACCESS AND WINDOW WASHING. STEPSTONE INC.
	ROCK MULCH TYPE 1	4"-6" ROCK MULCH ON SLAB
	ROCK MULCH TYPE 2	2"-4" ROCK MULCH IN PLANTER

### SITE ELEMENTS

1	PLANTER TYPE 1	8" CIP CONCRETE PLANTER, 3' HIGH
2	PLANTER TYPE 2	3' HIGH, PLANTERWORK METAL PLANTER
3	PLANTER TYPE 3	PREFABRICATED METAL PLANTER WITH GREENSCREEN, FILLED WITH ROCK MULCH
4	DOG RUN	440 SF DOG RUN WITH ARTIFICIAL TURF
5	DOG RUN FENCE	5' HIGH, DOG RUN ENCLOSURE FENCE, WITH SELF-CLOSING SWING GATE
6	DOG WASH	DOG WASH TUB, PER MANUFACTURER
7	DOG WASTE STATION	DOG WASTE STATION, PER MANUFACTURER
8	DOG WATER FOUNTAIN	DOG WATER FOUNTAIN, PER MANUFACTURER
9	BENCH SEATING	BENCH SEATING FOR DOG RUN AREA
10	WINDOW WASH ACCESS	5' WIDE AREA FOR WINDOW WASHING EQUIPMENT MOUNTING
11	MAINTENANCE ACCESS	5' WIDE AREA FOR MAINTENANCE ACCESS AND WINDOW WASHING EQUIPMENT MOUNTING
12	PATIO	PRIVATE PATIO, AREA PER PLAN
13	WOOD DECK AREA	FLEXIBLE LOUNGE AREA
14	WALL	8" THICK, 3' HIGH, FREESTANDING CIP CONCRETE WALL, WITH 3' WIDE SELF-CLOSING SWING GATE
15	GATE	3' WIDE SELF CLOSING SWING GATE FOR MAINTENANCE ACCESS
16	FLEXIBLE SEATING	MOVABLE LOUNGE SEATING WITH SIDETABLES
17	PARAPET	PARAPET PER ARCHITECTURAL PLANS

Shrubs should have a minimum planting area of 24 square feet, with a minimum soil depth of 30-inches.



## 9G PLANNED DEVELOPMENT PERMIT PACKAGE

3rd SUBMITTAL  
6.9.20

ISSUES:

PROJECT NO:  
5937.00  
FILE NAME:  
BIM 300/5537 00-Client  
-SEARCH - Client 6/20/2018.3.rvt  
DRAWN BY: CHECKED BY:  
Author Checker  
PLOT DATE:  
6/9/2020 2:03:41 PM  
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Level 7  
Hardscape Plan



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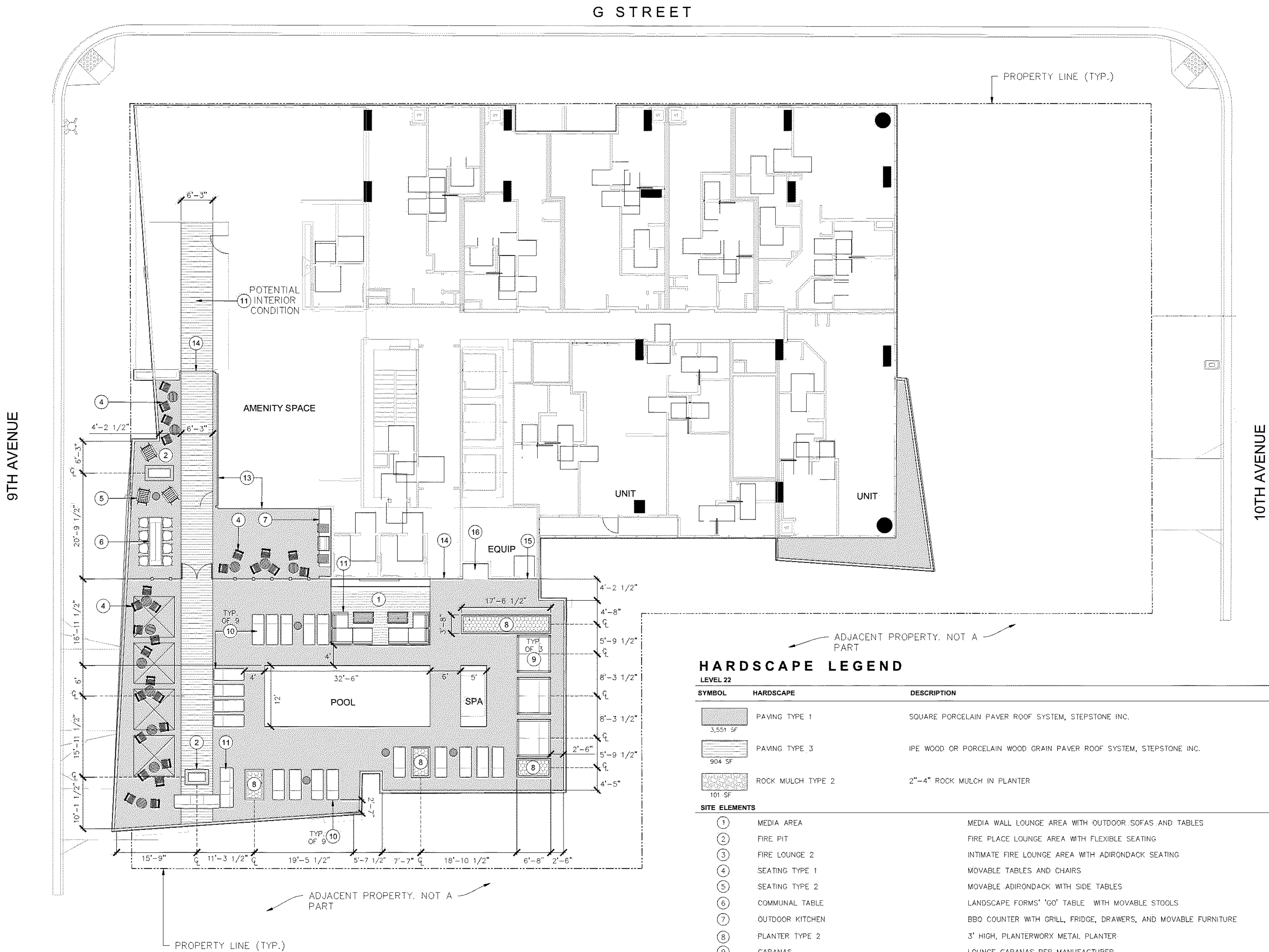
Shrubs should have a minimum planting area of 24 square feet, with a minimum soil depth of 30-inches.

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5	DOG RUN FENCE	5" HIGH, DOG RUN ENCLOSURE FENCE, WITH SELF-CLOSING SWING GATE
6	DOG WASH	DOG WASH TUB, PER MANUFACTURER
7	DOG WASTE STATION	DOG WASTE STATION, PER MANUFACTURER
8	DOG WATER FOUNTAIN	DOG WATER FOUNTAIN, PER MANUFACTURER
9	BENCH SEATING	BENCH SEATING FOR DOG RUN AREA
10	WINDOW WASH ACCESS	5' WIDE AREA FOR WINDOW WASHING EQUIPMENT MOUNTING
11	MAINTENANCE ACCESS	5' WIDE AREA FOR MAINTENANCE ACCESS AND WINDOW WASHING EQUIPMENT MOUNTING
12	PATIO	PRIVATE PATIO, AREA PER PLAN
13	WOOD DECK AREA	FLEXIBLE LOUNGE AREA
14	WALL	8" THICK, 3" HIGH, FREESTANDING CIP CONCRETE WALL, WITH 3' WIDE SELF-CLOSING SWING GATE
15	GATE	3' WIDE SELF CLOSING SWING GATE FOR MAINTENANCE ACCESS
16	FLEXIBLE SEATING	MOVABLE LOUNGE SEATING WITH SIDETABLES
17	PARAPET	PARAPET PER ARCHITECTURAL PLANS





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Shrubs should have a minimum planting area of 24 square feet, with a minimum soil depth of 30-inches.

### HARDSCAPE LEGEND

LEVEL 22

SYMBOL	HARDSCAPE	DESCRIPTION
	PAVING TYPE 1	SQUARE PORCELAIN PAVER ROOF SYSTEM, STEPSTONE INC.
	PAVING TYPE 3	IPE WOOD OR PORCELAIN WOOD GRAIN PAVER ROOF SYSTEM, STEPSTONE INC.
	ROCK MULCH TYPE 2	2"-4" ROCK MULCH IN PLANTER

### SITE ELEMENTS

1	MEDIA AREA	MEDIA WALL LOUNGE AREA WITH OUTDOOR SOFAS AND TABLES
2	FIRE PIT	FIRE PLACE LOUNGE AREA WITH FLEXIBLE SEATING
3	FIRE LOUNGE 2	INTIMATE FIRE LOUNGE AREA WITH ADIRONDACK SEATING
4	SEATING TYPE 1	MOVABLE TABLES AND CHAIRS
5	SEATING TYPE 2	MOVABLE ADIRONDACK WITH SIDE TABLES
6	COMMUNAL TABLE	LANDSCAPE FORMS 'GO' TABLE WITH MOVABLE STOOLS
7	OUTDOOR KITCHEN	BBO COUNTER WITH GRILL, FRIDGE, DRAWERS, AND MOVABLE FURNITURE
8	PLANTER TYPE 2	3' HIGH, PLANTERWORX METAL PLANTER
9	CABANAS	LOUNGE CABANAS PER MANUFACTURER
10	CHAISE LOUNGES	OUTDOOR CHAISE LOUNGE CHAIRS AND SIDE TABLES
11	SEATING TYPE 3	SOFA SEATING WITH COFFEE TABLE
12	POOL FENCE AND GATE	5' HIGH GLASS POOL FENCE WITH GLASS SWING GATE AND PUSH HARDWARE
13	AMENITY SPACE	PER ARCHITECT
14	BUILDING ENTRY	PER ARCHITECT
15	MAINTENANCE ACCESS	PER ARCHITECT
16	POOL SHOWER AND WATER FOUNTAINS	PER ARCHITECT



## 9G PLANNED DEVELOPMENT PERMIT PACKAGE

3rd SUBMITTAL  
6.9.20

ISSUES:

PROJECT NO:  
5937.00  
FILE NAME:  
BIM 300/5537 00-Culture  
-ARCH- Culture05\_2018.3.rvt  
DRAWN BY: CHECKED BY:  
Author Checker  
PLOT DATE:  
6/9/2020 2:03:43 PM  
TITLE:

Level 22  
Hardscape Plan

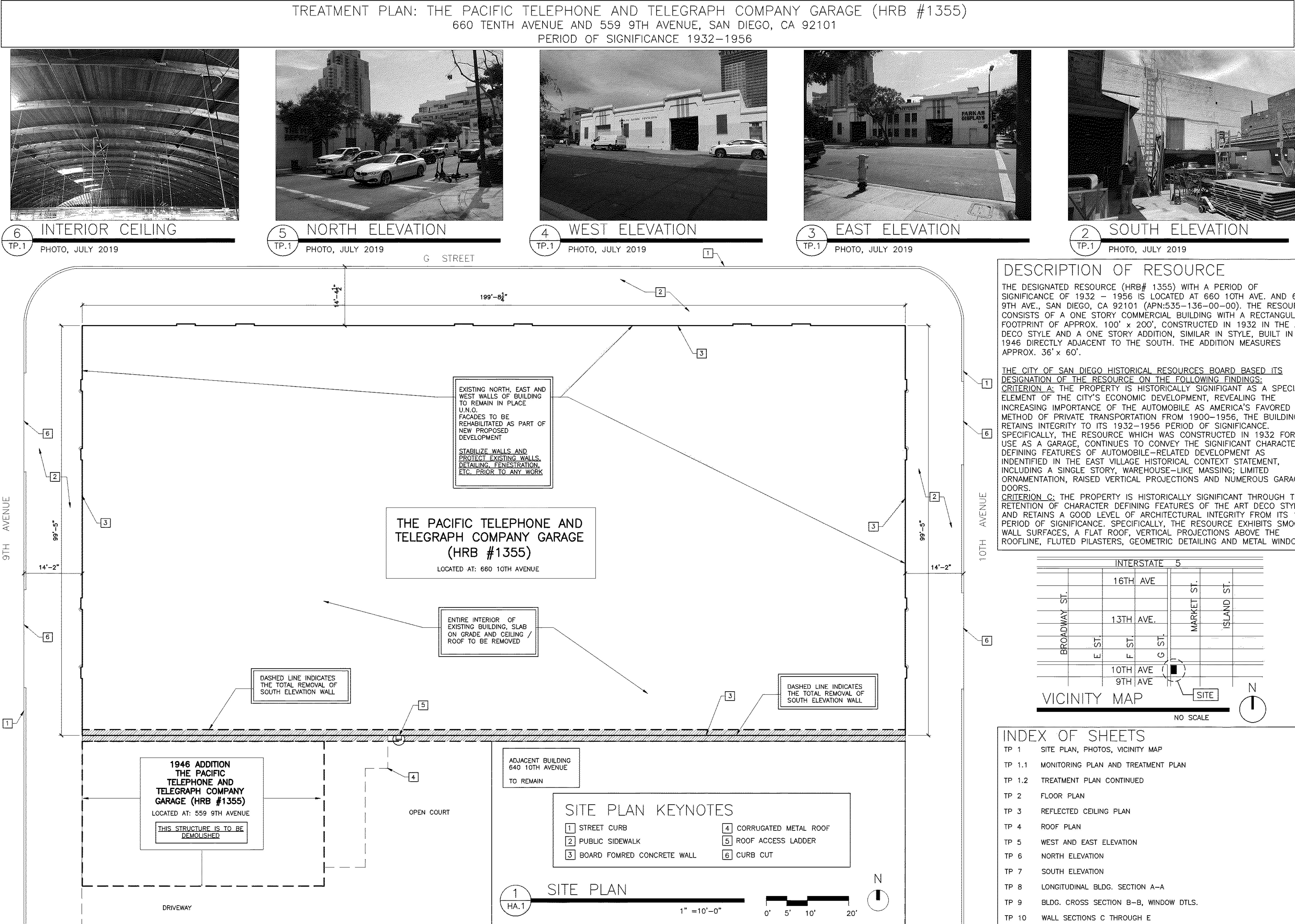


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TREATMENT PLAN

PROJECT :  
THE PACIFIC TELEPHONE & TELEGRAPH CO. GARAGE  
HRB #1355 TREATMENT PLAN  
660 10TH AVE. AND 559 9TH AVE., SAN DIEGO, CA 92101  
CLIENT :  
CISTERRA PARTNERS, LLC  
3580 CARMEL MOUNTAIN ROAD, STE. # 460, SAN DIEGO, CA 92130

ARCHITECT:  
UNION ARCHITECTURE, INC.  
JOHN H. EISENHART, ARCHITECT  
EVA THORN, INTERIORS  
344 22ND STREET, SAN DIEGO, CA 92102  
TELEPHONE: 619.786.2882 / WWW.UNIONARCH.COM

DATE: 09.13.2019  
REV: 10.10.2019  
REV: 03.20.2020

TP.1  
SHEET 1 OF 12

PROJECT NO:  
5937.00  
FILE NAME:  
BIM 360/5537-00-Cisterra-  
-SEARCH\_Collabor8-2018.3.rvt  
DRAWN BY: CHECKED BY:  
Author Checker  
PLOT DATE:  
6/9/2020 2:03:45 PM  
TITLE:



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TREATMENT PLAN: THE PACIFIC TELEPHONE AND TELEGRAPH COMPANY GARAGE (HRB #1355)  
660 TENTH AVENUE AND 559 9TH AVENUE, SAN DIEGO, CA 92101  
PERIOD OF SIGNIFICANCE 1932–1956

<p><u>3. Select demolition of structure (HAM, PA, CM).</u></p> <p>Issue:</p> <p>a. Demolition of interior walls, windows, doors, channels at overhead doors, wooden roof, skylights and south elevation are completed. Monitor to review completed work and any items that have been removed and labeled for storage</p> <p>b. Discuss post-demo stabilization and protection of historic structure.</p> <p>c. Overview of Treatment Plan, Architectural, Landscaping and Engineering documents as related to rehabilitation on site.</p> <p><u>4. Review of stabilization and protection of historic structure during rehabilitation on site (new footings, foundation, shotcrete, utilities, site preparation) (D, HAM, PA, CM, SE).</u></p> <p>Issue:</p> <p>a. Review of preparation work for rehabilitation on site.</p> <p><u>5. Continued monitoring of rehabilitation of structure. Monthly or as required by construction activity (HAM, PA, CM).</u></p> <p>Issue:</p> <p>a. Review rehabilitation of resource in accordance with Treatment Plan and Architectural, Landscaping and Engineering Documents.</p> <p>b. Review of rehabilitation of windows, doors, concrete repair, concrete details.</p> <p><u>6. Final Monitoring (HAM, PA, CM, D).</u></p> <p>Issue:</p> <p>a. Final punch list of items to complete rehabilitation work according to Treatment Plan and Architectural, Landscaping and Engineering Documents.</p> <p><u>7. Draft Report (HAM, PI).</u></p> <p>Issue:</p> <p>a. Draft report of monitoring process to be submitted to PI for review.</p> <p>Page 3 of 4</p>	<p>its Architecture is a good example of the Art Deco Style. The period of significance is 1932-1956.</p> <p>This Monitoring Plan will follow the Treatment Plan and supporting architectural documents prepared to rehabilitate this historic structure at its current location at 660 10<sup>th</sup> Avenue.</p> <p>Treatment Plan documents shall be used by Monitor as guidelines. <u>Monitoring actions are as follows:</u></p> <p><u>1. Pre-construction meeting (D, PA, HAM, GC, PI, CM, City of SD: BI, EM &amp; HM)</u></p> <p>Issue:</p> <p>a. Overview of Treatment Plan and Monitoring Plan as related to historic resource rehabilitation.</p> <p><u>2. Preparation of structure for demolition (HAM, CM, PA, SE).</u></p> <p>Issue:</p> <p>a. Monitor to be present <u>prior</u> to removal / demolition of any interior walls, any windows, doors, wooden roof, skylights and south elevation.</p> <p>b. Discuss pre-demo stabilization and protection of historic structure.</p> <p>c. Review work involved by CM to protect-in-place / remove / take-off typical template samples for rehabilitation. Windows, doors, any items that may be removed and taken from the structure to be rehabilitated / reinstalled shall be labeled, photographed, removed and protected for transport and storage.</p> <p><i>Any labels should include the following information:</i></p> <ul style="list-style-type: none"><li>• Property name and address item was taken from.</li><li>• Photo of item (applicable for typical windows, typ. concrete profile etc.).</li><li>• Information where item was taken (location for window)</li><li>• Contact information of dismantling contractor.</li><li>• Property Owner contact.</li><li>• Project Architect contact / Historic Architect contact.</li><li>• Historic Monitor contact.</li><li>• City Historic Planner contact.</li><li>• Dismantling date.</li></ul> <p>d. Monitor to approve structure is ready for select demolition.</p> <p>Page 2 of 4</p>	<p><b>UNION ARCHITECTURE INC.</b></p> <p>344 22<sup>ND</sup> STREET, SAN DIEGO, CA. 92102 T:619-788-2862</p> <p><u>MONITORING PLAN</u></p> <p>DATE: March 20, 2020</p> <p>PROJECT: The Pacific Telephone and Telegraph Co. Garage Located at: 660 10<sup>th</sup> Ave. and 559 9<sup>th</sup> Ave. San Diego, CA 92101 APN: # 535-136-01-00 HRB Site #1355 Year built: 1932 and 1946 Period of Significance: 1932-1956</p> <p>SUBJECT: Monitoring Plan for rehabilitation.</p> <p>PROJECT TEAM:</p> <p>Developer (D): Paul Thometz, Cisterra Partners. Project Architect (PA): Frank A. Landry, Carrier Johnson + Culture. Historic Architect &amp; Monitor (HAM): John Eisenhart, Union Architecture Inc. Principal Investigator (PI): Marie Burke Lia, Attorney at Law General Contractor (GC): T.B.D. Structural Engineer (SE): T.B.D. Construction Manager (CM): T.B.D. Building Inspector (BI): T.B.D. City of San Diego, Environmental (EM): Jennifer Wakem City of San Diego, Planning Historical (HM): Suzanne Segur</p> <p>LOCATIONS: Monitoring will take place on the site at 660 10<sup>th</sup> Avenue.</p> <p>PROPERTY DESCRIPTION:</p> <p>The Pacific Telephone and Telegraph Co. Garage building, a one story commercial building, is located at 660 10<sup>th</sup> Ave and 559 9<sup>th</sup> Ave. It was initially constructed in 1932 in the Art Deco Style, an addition to the south, similar in style was built in 1946. The parking garage, later warehouse building, is historically designated and significant under City of San Diego Historic guidelines (HRB #1355) per Criteria "A" (special element of the City's Development) and "C" (Architecture). The typology of the resource as an auto garage is a significant character defining feature of the automobile-related development of the City and</p> <p>Page 1 of 4</p>
<p>Historical Context Statement, including a single story, warehouse-like massing; limited ornamentation, raised vertical projections and numerous garage doors. Criterion C: The property is historically significant through retention of character defining features of the Art Deco Style and retains a good level of architectural integrity from its 1932 period if significance. Specifically, the resource exhibits smooth wall surfaces, a flat roof, vertical projections above the roofline, fluted pilasters, geometric detailing and metal windows.</p> <p>The footprint of the 1932 building is rectangular, it measures approximately 100' x 200'. The later addition to the south measures 36' x 60'. Exterior walls are mostly constructed of board formed concrete, with the western portion of the southern elevation of the 1932 building consisting of concrete post and beam construction with brick in-fill.</p> <p>The 3 main elevations (the street elevations) are laid out symmetrical. They are structured horizontally into a base zone, main wall field and a continuous decorative cornice / parapet banding at the top. All decorative detailing is cast in place concrete as part of the wall.</p> <p>All door openings have a decorative motif at the top. Windows are commercial steel sash type with wire mesh glazing. They are covered with protective metal woven mesh on the exterior. The roof and ceiling are comprised of a wooden "bow" truss system. There are six skylights on the roof of the 1932 structure. All roofing is built-up roofing.</p> <p>The building will be rehabilitated as part of a new development on the site. The 1932 building will be integrated with a new multi-story mixed-use building, the 1946 addition will be demolished. Modifications to the 1932 building include demolition of the ceiling / roof and demolition of the south elevation. Existing historic openings (doors and windows) on the east, north and west elevations will be modified in select locations to allow for the new use. This Treatment Plan is being prepared to rehabilitate the historic structure at the existing location.</p> <p><b>INTRODUCTION:</b></p> <p>The implementation of the Treatment Plan for rehabilitation is under the supervision of the Project Architect and Historic Architect / Monitor in a manner consistent with the mitigation, monitoring, and reporting program for this project. This Treatment Plan is accompanied by a copy of HABS drawings of the property prepared by the Historic Architect / Monitor, drawings that outline the proposed stabilization and preparation of the structure for rehabilitation. This Treatment Plan and its related drawings will be included in all subsequent plans for the discretionary permit processing, construction documents, building permit, and observational process.</p> <p>TREATMENT PLAN CONTINUED ON SHEET TP 1.2</p> <p>Page 2 of 7</p>	<p><b>UNION ARCHITECTURE INC.</b></p> <p>344 22<sup>ND</sup> STREET, SAN DIEGO, CA. 92102 619-269-4941</p> <p><u>TREATMENT PLAN</u></p> <p>DATE: March 20, 2020</p> <p>PROJECT: The Pacific Telephone and Telegraph Co. Garage Located at: 660 10<sup>th</sup> Ave. and 559 9<sup>th</sup> Ave. San Diego, CA 92101 APN: # 535-136-01-00 HRB Site #1355 Year built: 1932 and 1946 Period of Significance: 1932-1956</p> <p>SUBJECT: Treatment Plan for rehabilitation.</p> <p>PROJECT TEAM:</p> <p>Developer: Paul Thometz, Cisterra Partners, LLC Project Architect: Frank A. Landry, Carrier Johnson + Culture. Historic Architect &amp; Monitor: John H. Eisenhart, Union Architecture Inc. Principal Investigator: Marie Burke Lia, Attorney at Law General Contractor: T.B.D. Structural Engineer: T.B.D.</p> <p>PROPERTY DESCRIPTION:</p> <p>The Pacific Telephone and Telegraph Co. Garage building, a one story commercial building, is located at 660 10<sup>th</sup> Ave and 559 9<sup>th</sup> Ave. It was initially constructed in 1932 in the Art Deco Style, an addition to the south, similar in style was built in 1946 (period of significance of resource is 1932 to 1956). The parking garage, later warehouse building, is historically designated under City of San Diego Historic guidelines with the HRB #1355.</p> <p>The City of San Diego Historical Resources Board based its designation of the resource on the following findings:</p> <p>Criterion A: The property is historically significant as a special element of the City's economic development, revealing the increasing importance of the automobile as America's favored method of private transportation from 1900-1956, the building retains integrity to its 1932 – 1956 period of significance, specifically, the resource which was constructed in 1932 for use as a garage, continues to convey the significant character defining features of the automobile-related development as identified in the east Village</p> <p>Page 1 of 7</p>	<p><u>8. Final Report (HAM, PI, D).</u></p> <p>Issue:</p> <p>a. Final report of monitoring process, review updating of HABS and Treatment Plan documents to be submitted to PI for distribution to Developmental Services and San Diego History Center for archives.</p> <p>End of Monitoring</p> <p>Page 4 of 4</p>

TREATMENT PLAN

PROJECT : THE PACIFIC TELEPHONE & TELEGRAPH CO. GARAGE  
HRB #1355 TREATMENT PLAN  
660 10TH AVE. AND 559 9TH AVE., SAN DIEGO, CA 92101  
CLIENT : CISTERRA PARTNERS, LLC  
3580 CARMEL MOUNTAIN ROAD, STE. # 460, SAN DIEGO, CA 92130

ARCHITECT: UNION ARCHITECTURE, INC.  
JOHN H. EISENHART, ARCHITECT  
EVA THORN, INTERIORS  
344 22ND STREET, SAN DIEGO, CA 92102  
TELEPHONE: 619.788.2862 / WWW.UNIONARCH.COM

PROJECT NO: 5937.00  
FILE NAME:  
DRAWN BY: CHECKED BY:  
Author Checker  
PLOT DATE: 6/9/2020 2:03:46 PM  
TITLE:

DATE: 09.13.2019  
REV: 03.20.2020  
TP. 1.1  
SHEET 2 OF 12

9G  
PLANNED DEVELOPMENT PERMIT  
PACKAGE

3rd SUBMITTAL  
6.9.20

ISSUES:

PROJECT NO: 5937.00  
FILE NAME:  
DRAWN BY: CHECKED BY:  
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PLOT DATE: 6/9/2020 2:03:46 PM  
TITLE:

TP.1.1



TREATMENT PLAN: THE PACIFIC TELEPHONE AND TELEGRAPH COMPANY GARAGE (HRB #1355)  
660 TENTH AVENUE AND 559 9TH AVENUE, SAN DIEGO, CA 92101  
PERIOD OF SIGNIFICANCE 1932–1956

Department of the Interior, <https://www.nps.gov/tps/how-to-preserve/briefs/15-concrete.htm#preservation>)  
Consistent with Standards #2, 6, 7, 9, and 10.

**EXTERIOR DOORS AND WINDOWS:**

The steel doors and windows are generally in good shape. The contractor should repair as needed for smooth operation. If a feature of the sash or frame is missing or deteriorated beyond repair (monitor to determine condition) a replicated profile of the missing element shall be used. If the sash and fixed units are a new unit, they should match the profile of the existing original rail and style (ie. Hope's steel windows, Torrance Co. or equal, shop drawings to be submitted for approval). Follow Preservation Brief #13, of the National Park Service of the US Department of the Interior, <https://www.nps.gov/tps/how-to-preserve/briefs/13-steel-windows.htm#repair>  
All of these options would be consistent with the standards, repair is generally the preferred option. The single leaf entry door on the east elevation is non-original. Consistent with Standards #2, 6, 7, 9 and 10.

**ELECTRICAL & LIGHTING:**

Existing electrical and lighting system will be upgraded to conform to current code. All conduit and non-historic electrical and lighting on the exterior shall be removed. Electrical meters shall be located discretely away from public view. Exterior lighting fixtures to be surface mounted and to be sympathetic to Art Deco style influence. Consistent with Standards #9 and 10.

**PLUMBING:**

All exterior plumbing and vent pipes to be dismantled. New interior plumbing and vents to be installed as required. The plumbing system will be upgraded to conform to current code. Consistent with Standards #9 and 10.

**HEATING:**

New HVAC to conform with current code, any equipment is to be screened from public view. Consistent with Standards #9 and 10.

**PAINTING:**

If lead paint is detected, follow current laws for careful removal or apply removal per EPA lead paint regulations.

New paint scheme on the exterior of the building shall be in Art Deco style colors. Historic monitor and City staff to approve final painting scheme.  
The colors can be existing scheme or a painting scheme from Dunn Edwards Company: Trade Winds. Det647 - Field

Page 5 of 7

**EXISTING FOUNDATION:**

The Pacific Telephone and Telegraph Co. Garage building has concrete foundation walls and footings. The depth of the foundation and footing cannot be determined prior to excavation. Consistent with Standards #9 and 10.

**NEW FOUNDATION:**

The east, north and west elevations are to remain in place. They will be stabilized following the structural engineer / general contractor's recommendations. This may involve new shotcrete on the interior side of existing concrete exterior walls, new foundation walls, steel reinforcement on the interior side (new openings) and new footings. Consistent with Standards #9 and 10

**EXISTING WALLS / EXISTING FRAMING:**

The existing walls consist of board formed concrete, painted at exterior and interior. The south elevation consists of brick-in fill and boardformed concrete, this exterior wall will be demolished entirely. Consistent with Standards #2, 9 and 10.

**NEW WALLS / NEW FRAMING:**

The existing board formed concrete walls will remain in place at the east, north and west elevations. These elevations will be affected by modifications to allow for the new use. Select existing window and door openings will be modified.  
The existing building will be incorporated with a new development. New construction will be differentiated from existing historic. Consistent with Standards #2, 9 and 10.

**ROOF:**

The roof and ceiling will be demolished. Any new roofing, gutters and downspouts shall be located as not to detract / conflict with the existing facades. Consistent with Standards #6, 9 and 10.

**EXTERIOR FINISHES:**

The existing board formed concrete walls will remain in place in the east, north and west elevations. The concrete finish is painted. Any cracks and exposed steel reinforcement shall be repaired during the rehabilitation process to protect the existing concrete structure from progressing damage due to water intrusion. Repairs shall be done in a manner chemically compatible with the composition of the existing historic concrete mixture (follow Preservation Brief #15 of the National Park Service of the US

Page 4 of 7

TREATMENT PLAN CONTINUED  
FROM SHEET TP 1.1

**PREPARATION OF STRUCTURE:**

1. Preparation of the structure prior to any rehabilitation / demolition work:

The entire structure at 660 10<sup>th</sup> Ave. is to be stabilized, braced, and secured. Specific procedures to be determined by qualified structural engineers and general contractor.

The implementation of these procedures will occur only after review from Historic Architect / Monitor. Exterior concrete trim pieces, windows and doors affected by this shall be protected from damage.

Windows and doors to remain should be protected in place, optionally, they may be removed, labeled and stored to be reinstalled during rehabilitation and fenestration openings to be covered with ¾" plywood nailed to wood bracing. This option may be acceptable per monitor's approval.

Any existing exterior plumbing pipes, wiring etc. shall be removed along with site utilities to be disconnected. Monitor to be notified prior to modification of structure required for rehabilitation. Consistent with Standards #6, 7, 9 and 10.

2. Removal of interior of structure / select demolition at exterior:

Prior to the start of any demolition / removal process the Contractor and Monitor will meet on site to review the scope of demolition / removal work.  
The entire 1946 structure, located at 559 9<sup>th</sup> Ave. will be demolished. The 1932 structure located at 660 10<sup>th</sup> Ave. will only be partially demolished as described in the Treatment Plan drawings and as follows: The wood stud walls, plaster, windows and doors on the first and second floor will be demolished. The roof / ceiling will be demolished. The concrete floor will be demolished. The southern concrete / brick-in-fill exterior wall will be demolished. Select existing openings will be enlarged on the street facades.

Contractor shall inform Monitor of discovery of items during demolition and removal. The Monitor will evaluate relevance of such materials on site. Consistent with Standards #6, 7, and 9.

3. Post-demo stabilization and protection of structure:

The entire remaining structure is to be stabilized, braced, and secured during the rehabilitation process. Specific procedures to be determined by qualified structural engineers and general contractor.

The implementation of these procedures will occur only after review from Historic Architect / Monitor. Exterior concrete trim pieces, windows and doors shall be protected from potential damage during construction.

Page 3 of 7

are to be applied to specific rehabilitation projects in a reasonable manner, taking into consideration economic and technical feasibility.

1. A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment.

2. The historic character of a property shall be retained and preserved. The removal of historic materials or alteration of features and spaces that characterize a property shall be avoided.

3. Each property shall be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or architectural elements from other buildings, shall not be undertaken.

4. Most properties change over time; those changes that have acquired historic significance in their own right shall be retained and preserved.

5. Distinctive features, finishes, and construction techniques or examples of craftsmanship that characterize a property shall be preserved.

6. Deteriorated historic features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature shall match the old in design, color, texture, and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical, or pictorial evidence.

7. Chemical or physical treatments, such as sandblasting, that cause damage to historic materials shall not be used. The surface cleaning of structures, if appropriate, shall be undertaken using the gentlest means possible.

8. Significant archeological resources affected by a project shall be protected and preserved. If such resources must be disturbed, mitigation measures shall be undertaken.

9. New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment.

10. New additions and adjacent or related new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

Page 6 of 7

So Chic. Det614 - Trim, cornice, fluting.  
Red Clay. Det447 - Windows  
Consistent with Standards #6.

**LANDSCAPING:**

The new site will be landscaped and hardscaped in accordance with all relevant regulations of the Land Development Code for the relocation, rehabilitation, and reuse of historic resources. Consistent with Standards #9 and 10.

**REPAIR WORK:**

Repair work is preferred over use of replicated historic new materials. Retention of as much historic fabric as possible is paramount during rehabilitation. If a historic element is damaged but not in jeopardy of creating a performance issue with regard to the envelope of the building or building system, the feature should remain. Consistent with Standards #2, 5, 6 and 7.

**RECONSTRUCTION / REHABILITATION:**

The cleaning of all historic material / fabric shall occur by using the gentlest means possible. An appropriate means of control and disposal of lead or other chemicals shall be provided. Historic fabric shall be retained as much as possible. Do not sandblast or water power wash materials.  
The character defining massing / form of the structure is a one story commercial building, 100 ft. x 200 ft. with wood "bow" trusses spanning across at 15'-0" o.c.. The character defining material elements are: Board formed concrete walls with concrete detailing at base, cornice and parapet, large steel windows with wire-glass panes and overhead roll-up doors.  
Should damage occur to the resource, it shall be repaired in conformance with the Secretary of the Interior's Standards for Rehabilitation or Reconstruction. Attachment of materials shall be similar to the original, historic method. Consistent with Standards #2, 6, 9, and 10.

**ATTACHMENTS:**

Treatment Plan Drawings.

**The Secretary of the Interior's Standards for Rehabilitation**

The Standards (Department of Interior regulations, 36 CFR 67) pertain to historic buildings of all materials, construction types, sizes, and occupancy and encompass the exterior and the interior, related landscape features and the building's site and environment as well as attached, adjacent, or related new construction. The Standards

Page 6 of 7

TREATMENT PLAN

PROJECT :  
THE PACIFIC TELEPHONE & TELEGRAPH CO. GARAGE  
HRB #1355 TREATMENT PLAN  
660 10TH AVE. AND 559 9TH AVE., SAN DIEGO, CA 92101  
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CISTERRA PARTNERS, LLC  
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TP. 1.2

SHEET 3 OF 12

3rd SUBMITTAL  
6.9.20

ISSUES:

PROJECT NO:  
5937.00  
FILE NAME:  
BIM 360/5537-00-Client-2020-03-20-2020-2018.3.rvt  
DRAWN BY: CHECKED BY:  
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PLOT DATE:  
6/9/2020 2:03:47 PM  
TITLE:

9G  
PLANNED DEVELOPMENT PERMIT  
PACKAGE







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NOTE:

A. THE 1932 BUILDING IS ORIGINAL EXCEPT FOR:  
1. THE METAL OVERHEAD ROLL-UP DOORS.  
2. THE WIRE MESH WINDOW GUARDS

B. CIP : ABBR. CAST-IN-PLACE CONCRETE

KEYNOTES

1 CONCRETE SIDEWALK / GRADE  
2 KEYSTONE TYPE DECORATIVE CAST-IN PLACE CONCRETE MOULDING  
3 CONCRETE FLOOR  
4 BOARD FORMED (CAST-IN-PLACE) CONCRETE WALL, PAINTED  
5 CONCRETE CAST-IN-PLACE (CIP) BASE/TORUS MOULDING DETAIL, SEE 5/TP.10

6 CONCRETE (CIP) MOULDED CORNICE BAND  
7 PARAPET (CIP CONCRETE)  
8 "FLUTING" DETAIL AT PIER (CIP CONCRETE)  
9 STEEL SASH WINDOWS W/ AWNING FUNCTION (SEE DTLS. 1 + 2/TP.9)  
10 BEAD MOULDING CONCRETE WINDOW SILL (CIP)  
11 RECTANGULAR BLOCK CIP CONCRETE  
12 CIP VERTICAL CONCRETE STRIP DECORATIVE RELIEF (PROTRUDING FROM WALL PLANE)

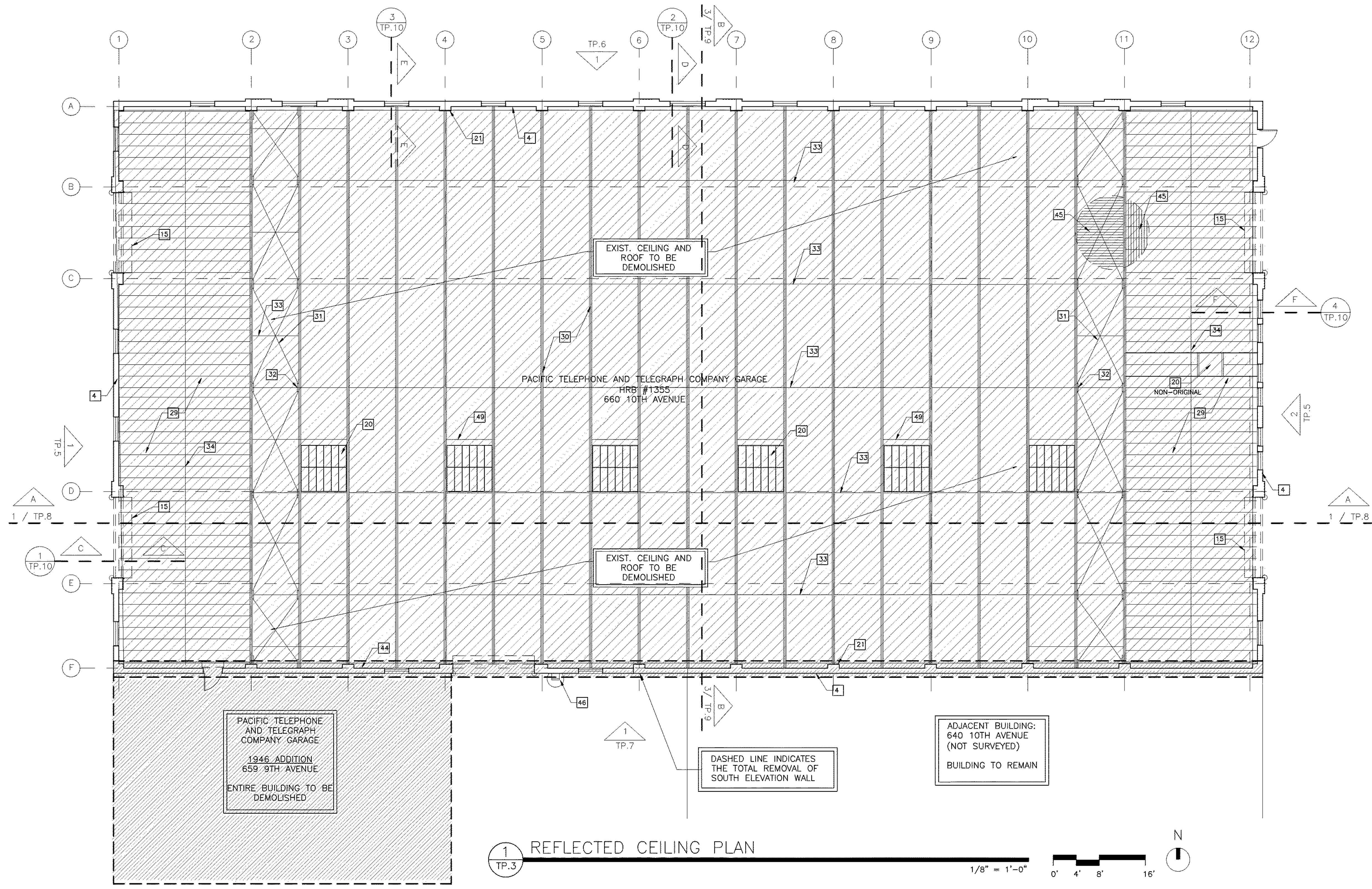
13 DECORATIVE METAL VENT  
14 CONCRETE CIP DOOR TRIM (JAMB & HEAD)  
15 OVERHEAD STEEL ROLL-UP DOOR W/ METAL WALL CHANNEL  
16 "BULLET" BOLLARD  
17 BUILT-UP ROOFING  
18 ROOF OVERFLOW DRAIN  
19 ROOF DRAIN / DOWNSPOUT  
20 METAL SASH SKYLIGHT

21 CONCRETE CIP PIER  
22 CONCRETE CIP BEAM / CORBEL  
23 METAL HANDRAIL  
24 2X4 STUD WALL @ 16" O.C.  
25 2x CEILING JOIST @ 24" O.C.  
26 2X10 FLOOR JOIST @ 16" O.C.  
27 PLASTER FINISH WALL  
28 7/8" WOOD SHEATHING

29 2x14 ROOF RAFTERS @ 2'-0" O.C.  
30 (2) 3X14 WOODEN "BOW" TRUSSES  
31 3/4" STEEL TIE RODS  
32 STEEL ANGLE  
33 3X8 ROOF RAFTER  
34 2X6 WOOD CROSS BRIDGING  
35 WOOD DOOR  
36 2x4 ACOUSTIC LAY IN CEILING (WITH PLASTER CEILING ABOVE)

37 1X WOODEN TREAD AT STAIR  
38 WROUGHT IRON RAILING  
39 STEEL "C" CHANNEL  
40 CORRUGATED METAL CANOPY (OVERHANG)  
41 VINYL FLOORING  
42 STEEL WINDOWS  
43 METAL MESH WINDOW GUARDS  
44 BRICK

45 1X6 DOUGLAS FIR SHEATHING  
46 METAL ROOF ACCESS LADDER  
47 TILE FLOORING  
48 STEEL DOOR  
49 2X8 ROOF RAFTER  
50 CIP CONCRETE HORIZ. BANDING  
51 RECESSED WALL CAVITY W/ UTILITY CONNECTION (APPROX. 7.5" DEEP)



TREATMENT PLAN

PROJECT :  
THE PACIFIC TELEPHONE & TELEGRAPH CO. GARAGE  
HRB #1355 TREATMENT PLAN  
660 10TH AVE. AND 559 9TH AVE., SAN DIEGO, CA 92101  
CLIENT :  
CISTERRA PARTNERS, LLC  
3560 CARMEL MOUNTAIN ROAD, STE. # 460, SAN DIEGO, CA 92130

ARCHITECT:  
UNION ARCHITECTURE, INC.  
JOHN H. EISENHART, ARCHITECT  
EVA THORN, INTERIORS  
3444 22ND STREET, SAN DIEGO, CA 92102  
TELEPHONE: 619.788.2852 / WWW.UNIONARCH.COM

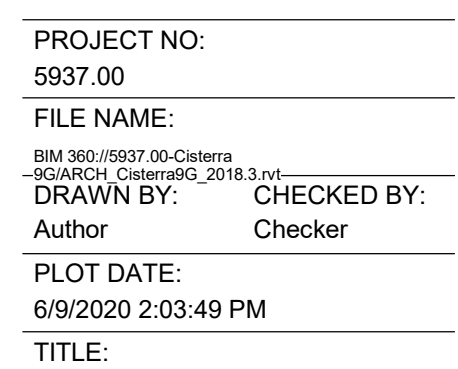
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5937.00  
FILE NAME:  
BIM 360/5537-00-Cisterra-  
-SEARCH - Columns05\_2018.3.rvt  
DRAWN BY: CHECKED BY:  
Author Checker  
PLOT DATE:  
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TITLE:

DATE: 09.13.2019  
REV: 03.20.2020  
TP. 3  
SHEET 5 OF 12



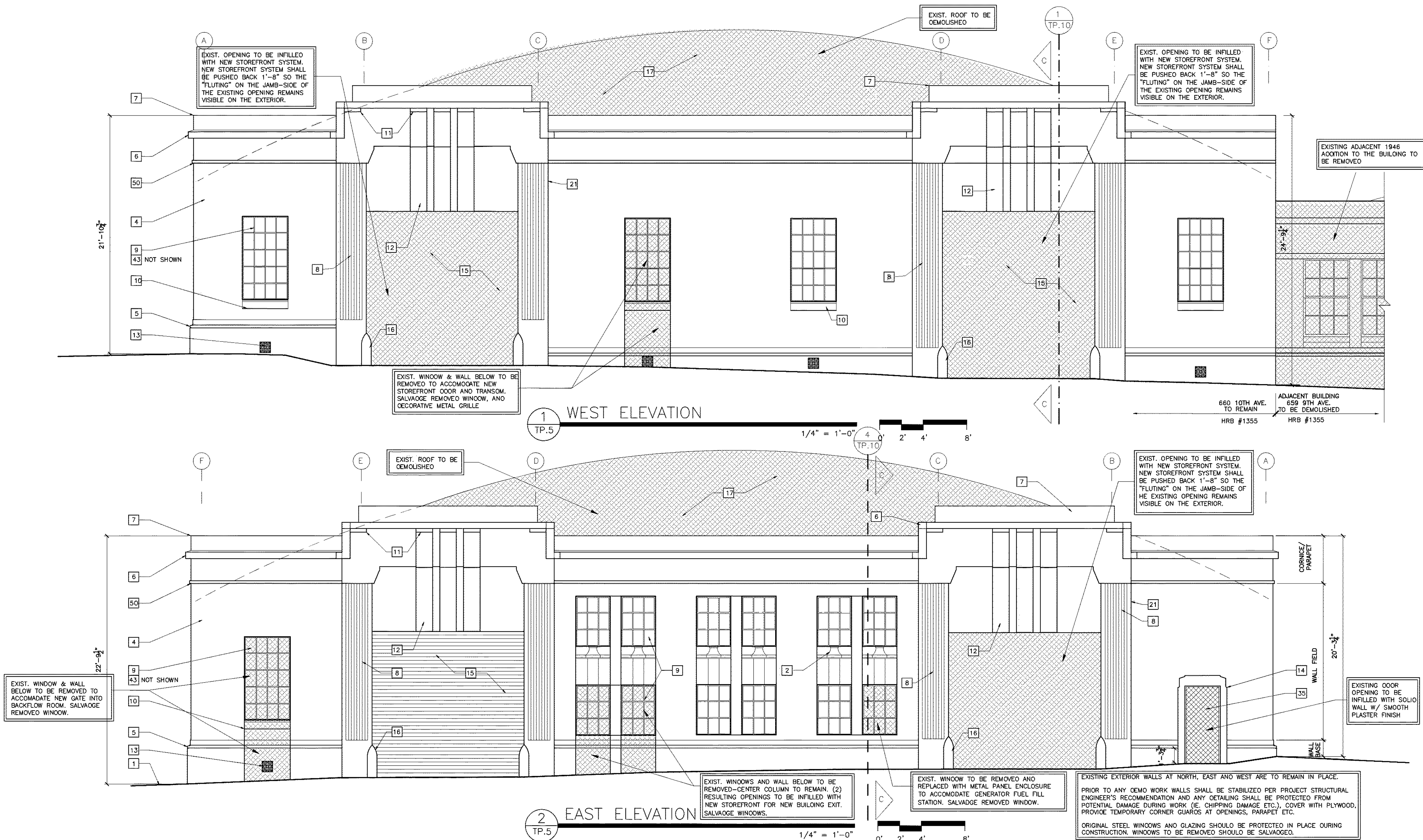
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TP.4





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NOTE:	KEYNOTES				
	1 THE 1932 BUILDING IS ORIGINAL EXCEPT FOR: THE METAL OVERHEAD ROLL-UP DOORS. THE WIRE MESH WINDOW GUARDS	2 KEYSTONE TYPE DECORATIVE CAST-IN PLACE CONCRETE MOULDING	3 CONCRETE FLOOR	4 BOARD FORMED (CAST-IN-PLACE) CONCRETE WALL, PAINTED	5 CONCRETE CAST-IN-PLACE (CIP) BASE/TORUS MOULDING DETAIL, SEE 5/TP.10
B. CIP : ABBR. CAST-IN PLACE CONCRETE	6 CONCRETE (CIP) MOULDED CORNICE BAND	7 PARAPET (CIP CONCRETE)	8 "FLUTING" DETAIL AT PIER (CIP CONCRETE)	9 STEEL SASH WINDOWS W/ AWNING FUNCTION (SEE DTLS. 1 + 2/TP.9)	10 BEAD MOULDING CONCRETE WINDOW SILL (CIP)
	11 RECTANGULAR BLOCK CIP CONCRETE	12 CIP VERTICAL CONCRETE STRIP DECORATIVE RELIEF (PROTRUDING FROM WALL PLANE)	13 DECORATIVE METAL VENT	14 CONCRETE CIP DOOR TRIM (JAMB & HEAD)	15 OVERHEAD STEEL ROLL-UP DOOR W/ METAL WALL CHANNEL
	16 "BULLET" BOLLARD	17 BUILT-UP ROOFING	18 ROOF OVERFLOW DRAIN	19 ROOF DRAIN / DOWNSPOUT	20 METAL SASH SKYLIGHT
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	26 2X10 FLOOR JOIST @ 16" O.C.	27 PLASTER FINISH WALL	28 7/8" WOOD SHEATHING	29 2x14 ROOF RAFTERS @2'-0" O.C.	30 (2) 3X14 WOODEN "BOW" TRUSSES
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TREATMENT PLAN

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HRB #1355 TREATMENT PLAN  
660 10TH AVE. AND 559 9TH AVE., SAN DIEGO, CA 92101  
CLIENT : CISTERRA PARTNERS, LLC  
3590 CARMEL MOUNTAIN ROAD, STE. # 460, SAN DIEGO, CA 92130

ARCHITECT: UNION ARCHITECTURE, INC.  
JOHN H. EISENHART, ARCHITECT  
EVA THORN, INTERIORS  
344 22ND STREET, SAN DIEGO, CA 92102  
TELEPHONE: 619.788.2882 / WWW.UNIONARCH.COM

PROJECT NO: 5937.00  
FILE NAME: BHM 300/5537-00-Cisterra-2018.3.rvt  
DRAWN BY: JGARCH/ Cisterra/2018.3.rvt  
CHECKED BY: Author  
PLOT DATE: 6/9/2020 2:03:50 PM  
TITLE:

DATE: 09.13.2019  
REV: 10.10.2019  
REV: 03.20.2020  
TP. 5  
SHEET 7 OF 12

9G PLANNED DEVELOPMENT PERMIT PACKAGE

3rd SUBMITTAL  
6.9.20

ISSUES:

TP.5



[illegible]

- A. THE 1932 BUILDING IS ORIGINAL EXCEPT FOR:
  1. THE METAL OVERHEAD ROLL-UP DOORS.
  2. THE WIRE MESH WINDOW GUARDS
- B. CIP : ABBR. CAST-IN PLACE CONCRETE

- 1 CONCRETE SIDEWALK / GRADE
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- 5 CONCRETE CAST-IN-PLACE (CIP)  
BASE/TORUS MOULDING DETAIL, SEE  
5/TP.10

- |    |   |
|----|---|
| 6  | CONCRETE (CIP) MOULDED CORNICE BAND                                     |
| 7  | PARAPET (CIP CONCRETE)  |
| 8  | "FLUTING" DETAIL AT PIER (CIP CONCRETE)                                 |
| 9  | STEEL SASH WINDOWS w/ AWNING FUNCTION (SEE DTLS. 1 + 2/TP.9)            |
| 10 | BEAD MOULDING CONCRETE WINDOW SILL (CIP)                                |
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- |    |                             |
|----|-----------------------------|
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| 23 | METAL HANDRAIL              |
| 24 | 2X4 STUD WALL @ 16" O.C.    |
| 25 | 2x CEILING JOIST @ 24" O.C. |
| 26 | 2X10 FLOOR JOIST @ 16" O.C. |
| 27 | PLASTER FINISH WALL         |
| 28 | ¾" WOOD SHEATHING           |

- |    |  |
|----|--|
| 29 | 2x14 ROOF RAFTERS @2'-0" O.C.                            |
| 30 | (2) 3X14 WOODEN "BOW" TRUSSES                            |
| 31 | ¾" STEEL TIE RODS  |
| 32 | STEEL ANGLE  |
| 33 | 3X8 ROOF RAFTER  |
| 34 | 2X6 WOOD CROSS BRIDGING                                  |
| 35 | WOOD DOOR  |
| 36 | 2x4 ACOUSTIC LAY IN CEILING (WITH PLASTER CEILING ABOVE) |

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TP. 6  
SHEET 8 OF 10

PROJECT : THE PACIFIC TELEPHONE & TELEGRAPH CO. GARAGE  
 TRHB #1355 TREATMENT PLAN  
 6660 10TH AVE. AND 559 9TH AVE., SAN DIEGO, CA 92101  
 CLIENT : CISTERRA PARTNERS, LLC  
 3580 CARMEL MOUNTAIN ROAD, STE. # 460, SAN DIEGO, CA 92130

ARCHITECT:  
UNION ARCHITECTURE, INC.

JOHN H. EISENHART, ARCHITECT  
EVA THORN, INTERIORS  
344 22ND STREET, SAN DIEGO, CA 92102  
TELEPHONE: 619.788.2862 / [WWW.UNIONARCH.COM](http://WWW.UNIONARCH.COM)

3rd SUBMITTAL  
6.9.20

ISSUES:

PROJECT NO:  
5937.00

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FILE NAME:  
BIM 360://5937.00-Cisterra  
-9G/ARCH\_Cisterra9G\_2018.3.rvt

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DRAWN BY: CHECKED BY:  
Author Checker


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PLOT DATE:  
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TITLE:

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TP. 6  
SHEET 8 OF 10

**Cisterra**  
DEVELOPMENT

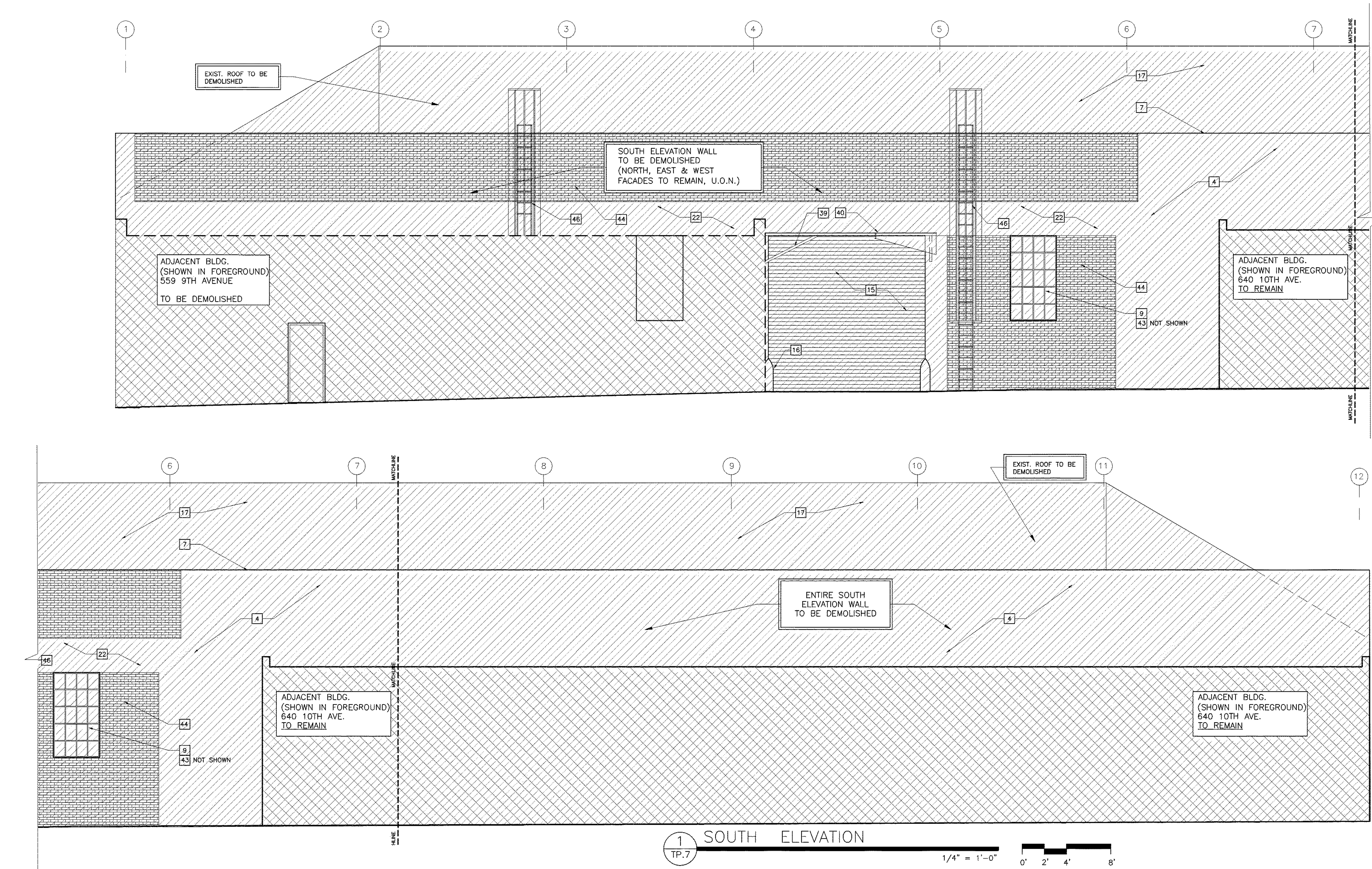
**carrierjohnson + CULTURE**  
architecture + environments + brand strategy + graphics

185 W F St suite 500, San Diego, CA 92101  
phone 619.239.2353

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TREATMENT PLAN

PROJECT :  
THE PACIFIC TELEPHONE & TELEGRAPH CO. GARAGE  
HRB #1355 TREATMENT PLAN  
660 10TH AVE. AND 559 9TH AVE., SAN DIEGO, CA 92101  
CLIENT :  
CISTERRA PARTNERS, LLC  
3580 CARVEL MOUNTAIN ROAD, STE. # 460, SAN DIEGO, CA 92130

ARCHITECT:  
UNION ARCHITECTURE, INC.  
JOHN H. EISENHART, ARCHITECT  
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3444 22ND STREET, SAN DIEGO, CA 92102  
TELEPHONE: 619.788.2862 / WWW.UNIONARCH.COM

PROJECT NO:  
5937.00  
FILE NAME:  
BIM 360/5537-00-Cisterra-2018.3.rvt  
DRAWN BY: CHECKED BY:  
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6/9/2020 2:03:52 PM  
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**TP. 7**  
SHEET 9 OF 12







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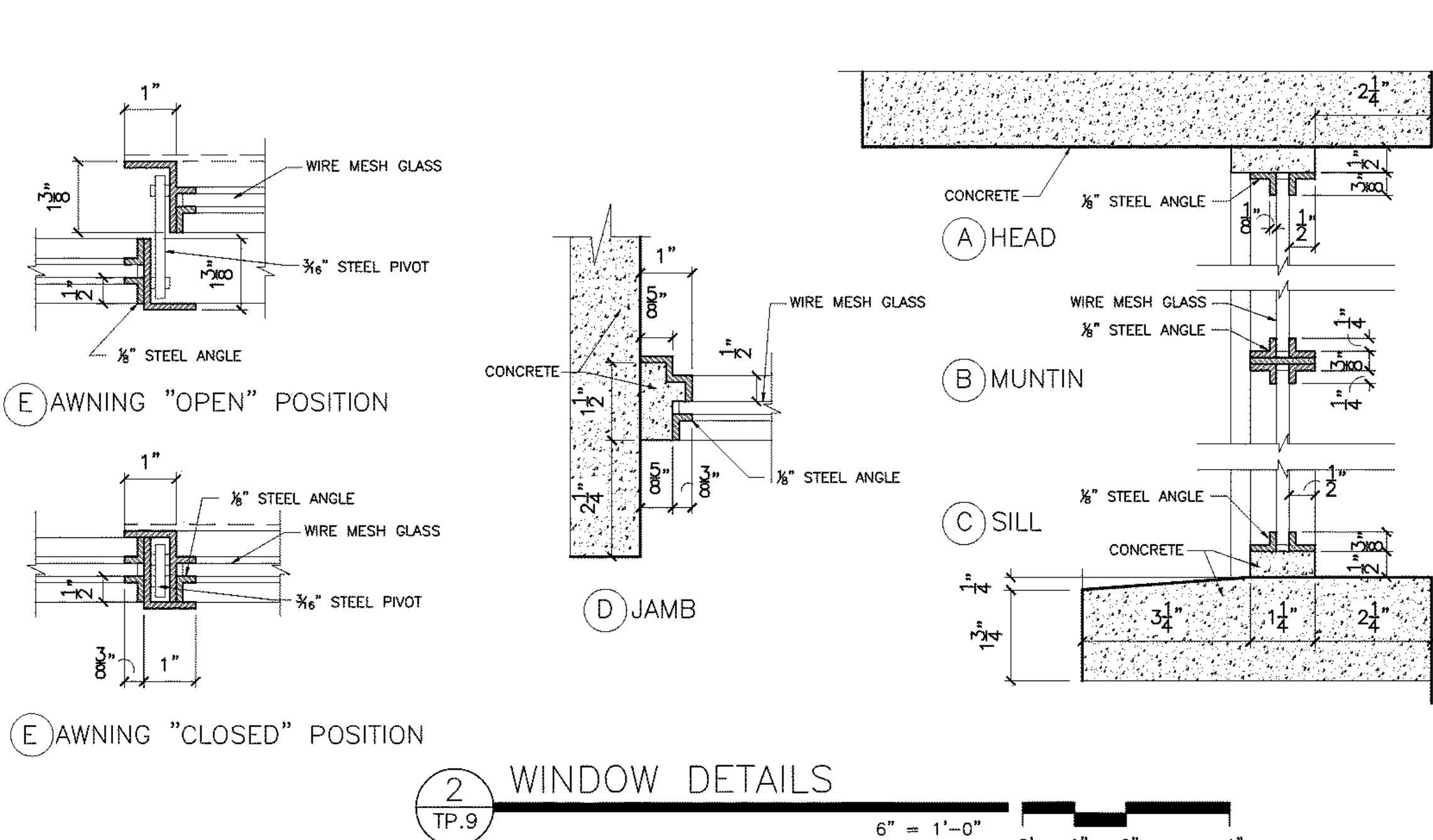
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48 STEEL DOOR

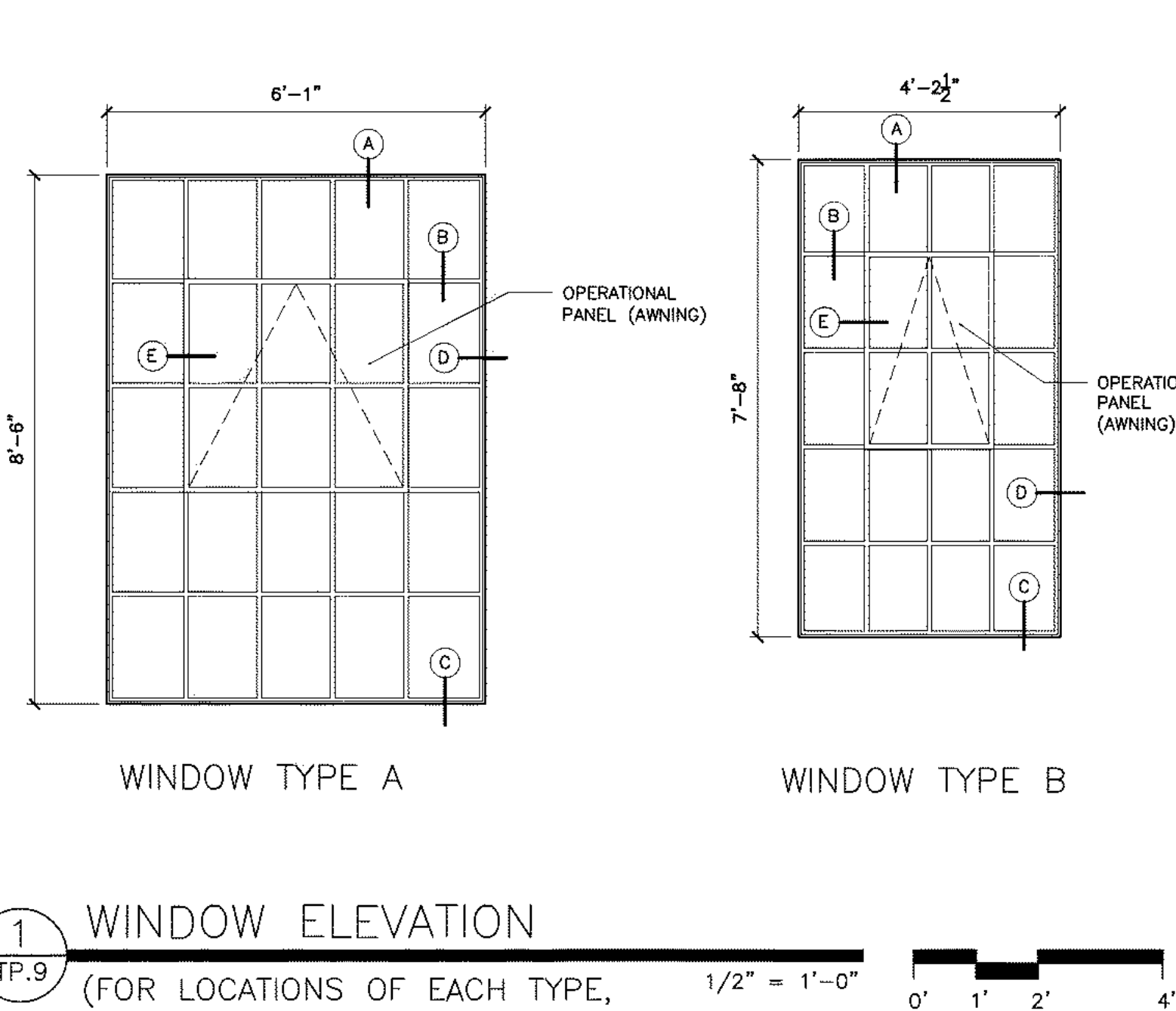
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50 CIP CONCRETE HORIZ. BANDING

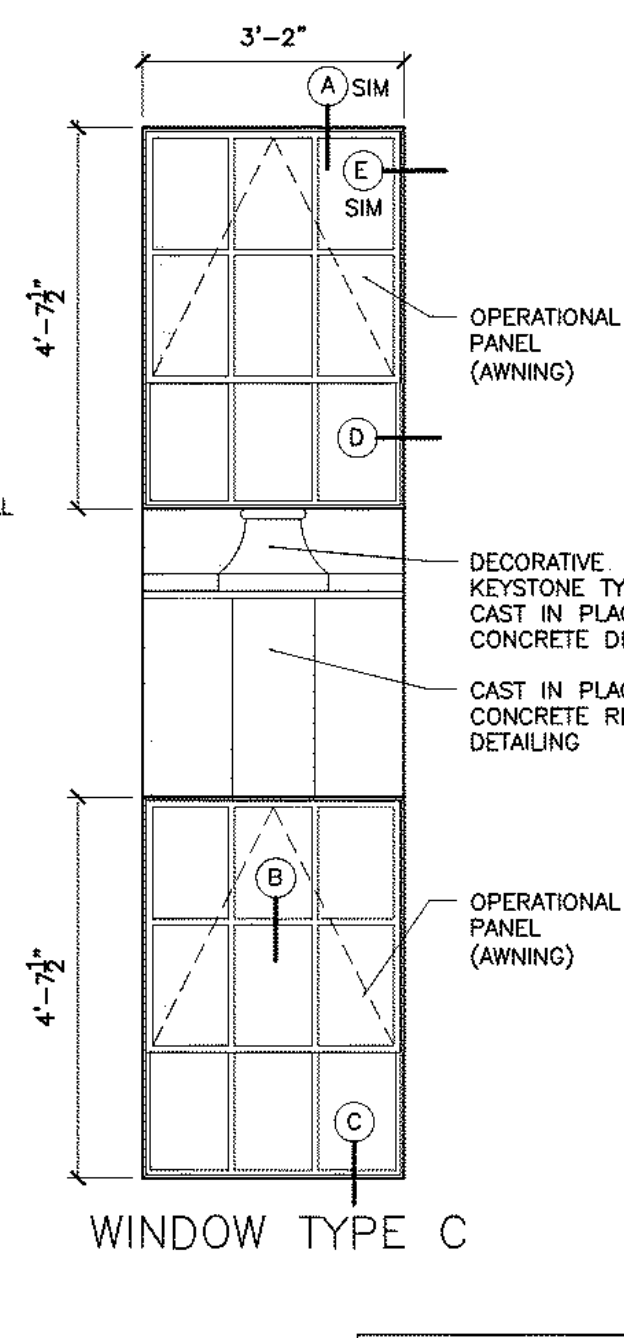
51 RECESSED WALL CAVITY W/ UTILITY CONNECTION (APPROX. 7.5" DEEP)



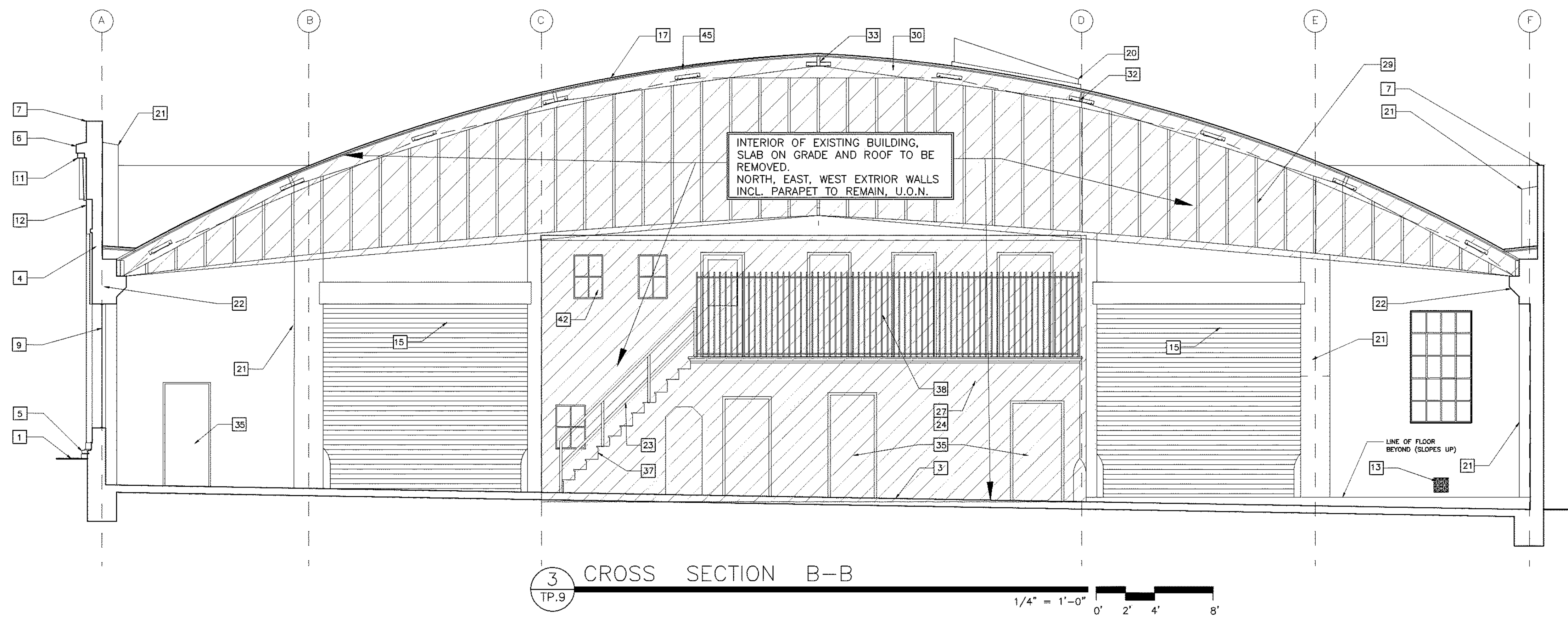
2 WINDOW DETAILS  
TP.9  
6" = 1'-0"  
0' 1" 2" 4"



1 WINDOW ELEVATION  
TP.9  
1/2" = 1'-0"  
0' 1" 2" 4"  
(FOR LOCATIONS OF EACH TYPE, SEE NOTES ON FLOOR PLAN: 1/TP.2)



WINDOW TYPE C  
TP.9  
1/2" = 1'-0"  
0' 1" 2" 4"



3 CROSS SECTION B-B  
TP.9  
1/4" = 1'-0"  
0' 2' 4' 8'

EXISTING EXTERIOR WALLS AT NORTH, EAST AND WEST ARE TO REMAIN IN PLACE.

PRIOR TO ANY DEMO WORK WALLS SHALL BE STABILIZED PER PROJECT STRUCTURAL ENGINEER'S RECOMMENDATION AND ANY DETAILING SHALL BE PROTECTED FROM POTENTIAL DAMAGE DURING WORK (IE. CHIPPING DAMAGE ETC.), COVER WITH PLYWOOD, PROVIDE TEMPORARY CORNER GUARDS AT OPENINGS, PARAPET ETC.

ORIGINAL STEEL WINDOWS AND GLAZING SHOULD BE PROTECTED IN PLACE DURING CONSTRUCTION.

FOR CONCRETE REHABILITATION WORK SEE NATIONAL PARK SERVICE PRESERVATION BRIEF #15.

FOR STEEL WINDOW REHABILITATION WORK SEE NATIONAL PARK SERVICE PRESERVATION BRIEF #13.

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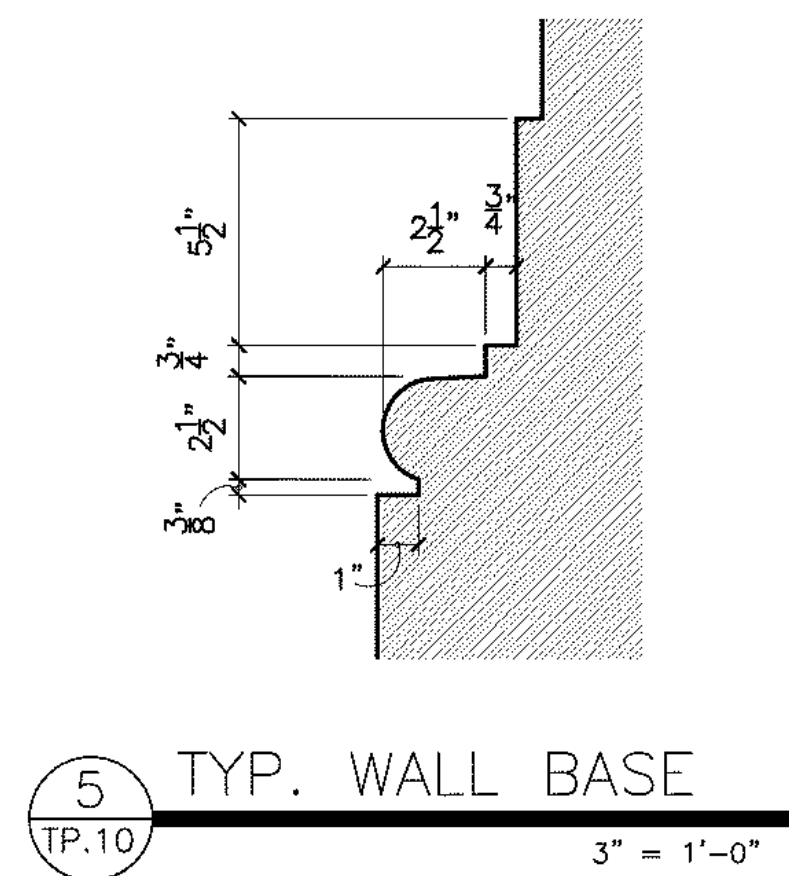
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REV: 03.20.2020  
TP.9  
SHEET 11 OF 12



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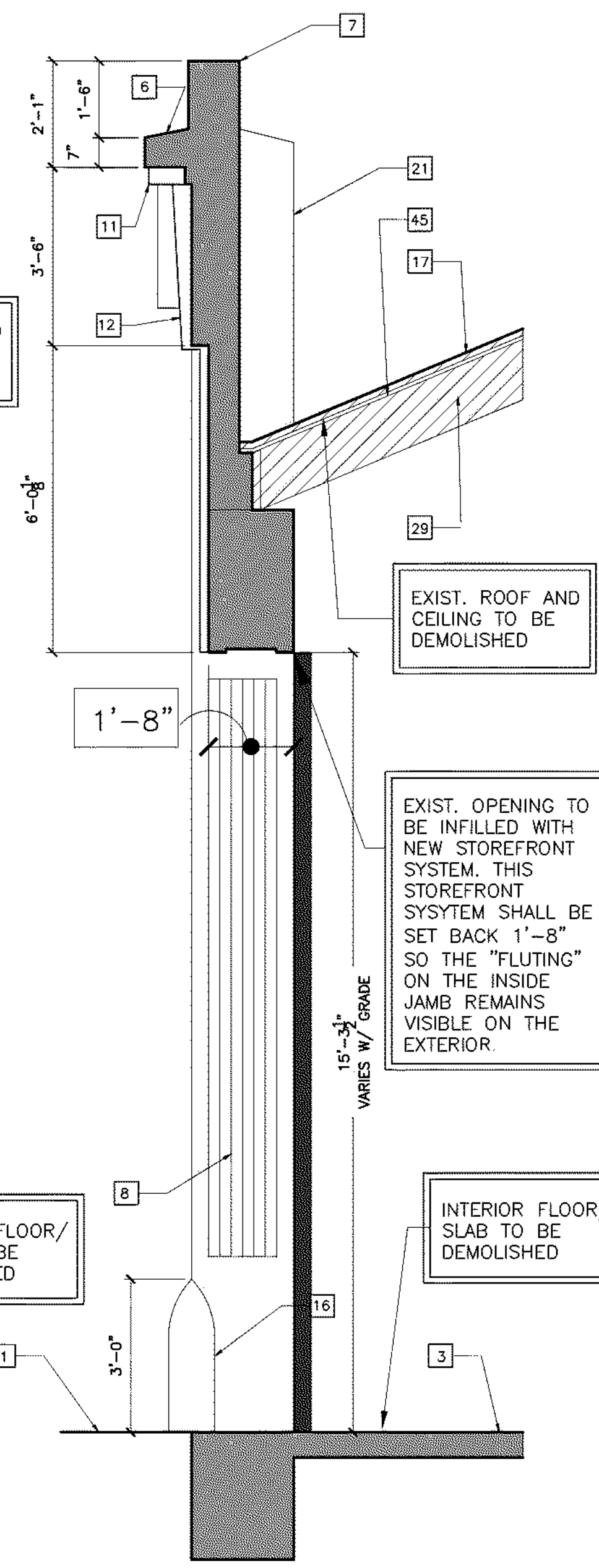
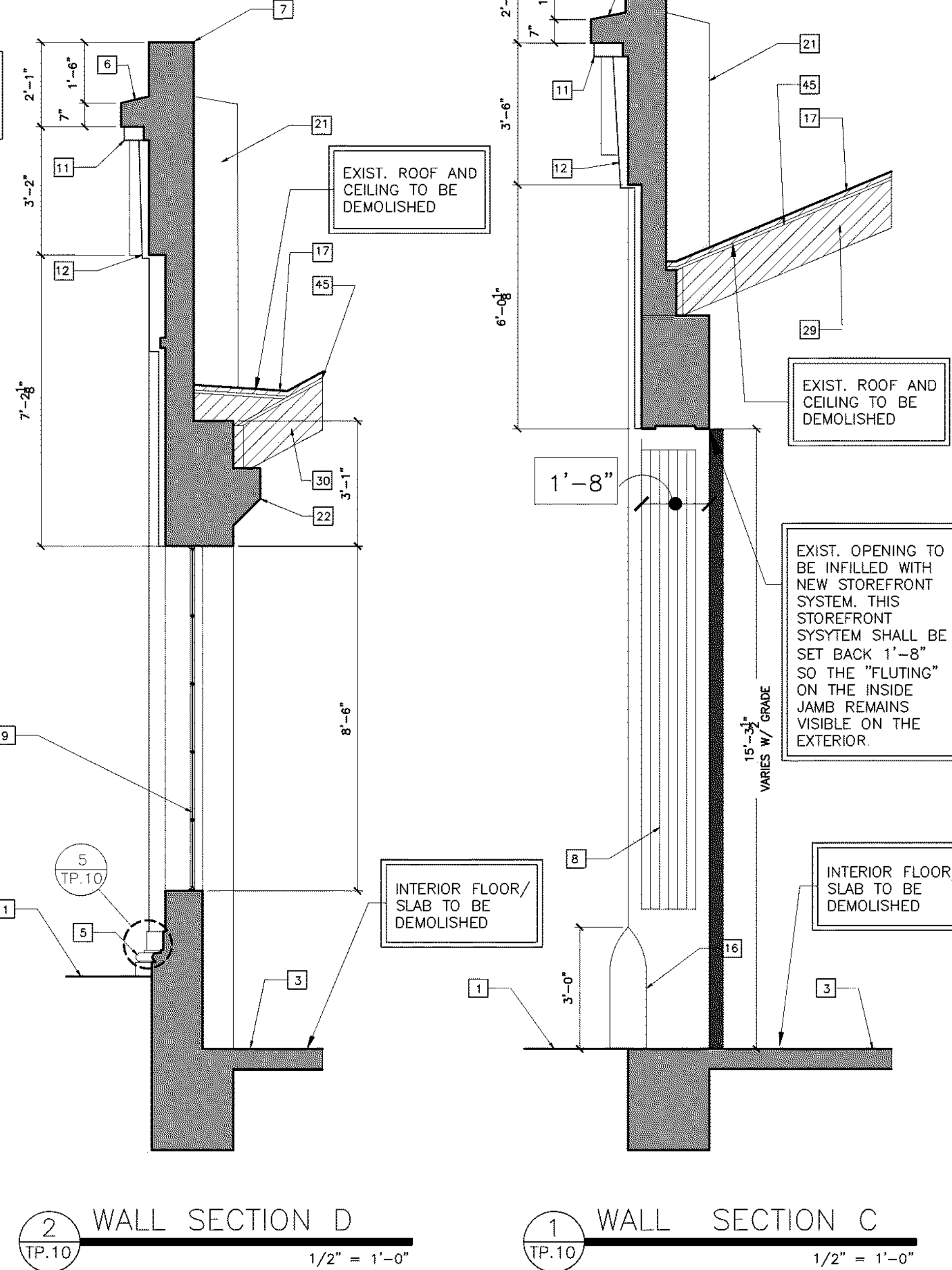
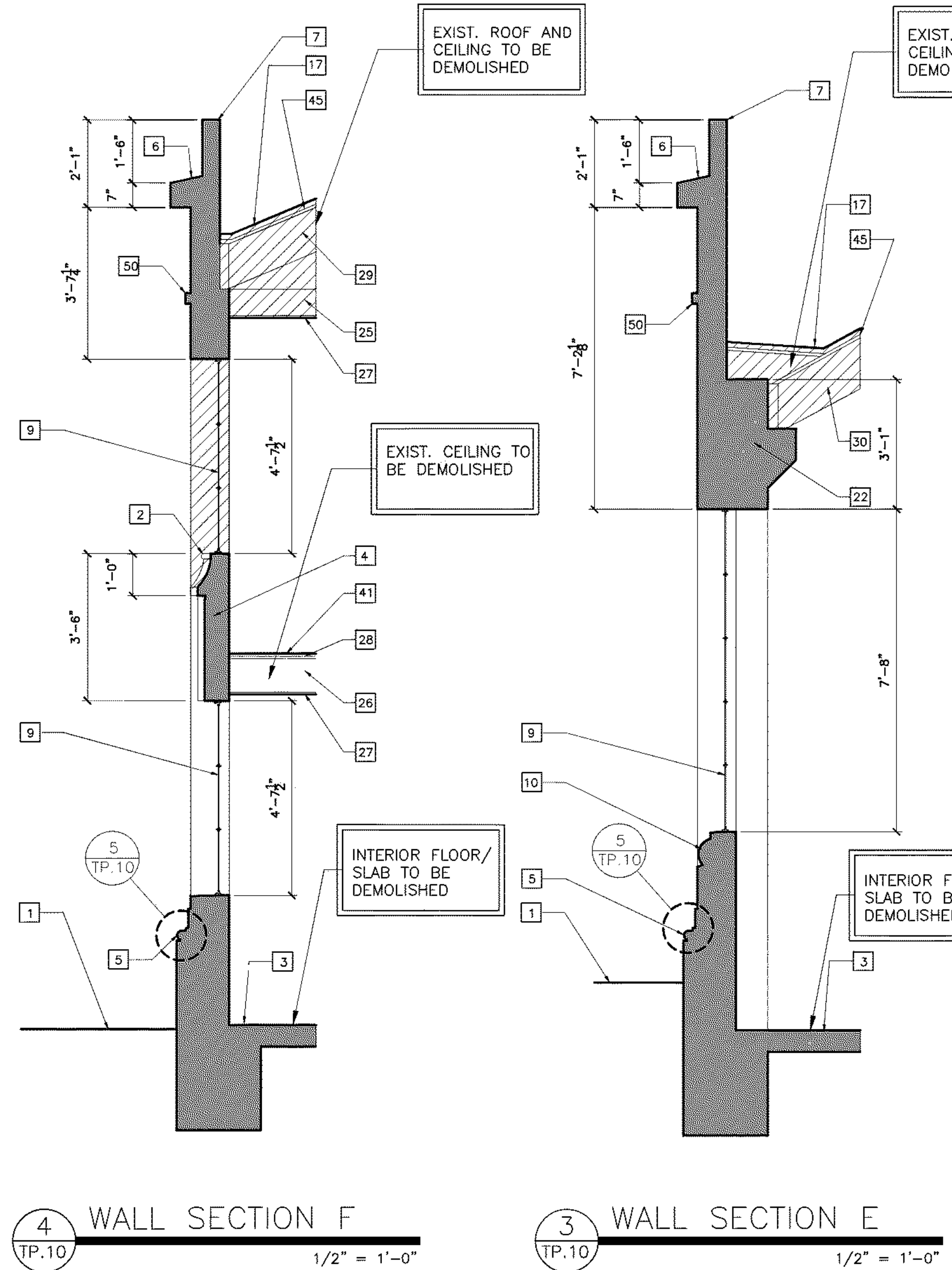
EXISTING EXTERIOR WALLS AT NORTH, EAST AND WEST ARE TO REMAIN IN PLACE.

PRIOR TO ANY DEMO WORK WALLS SHALL BE STABILIZED PER PROJECT STRUCTURAL ENGINEER'S RECOMMENDATION AND ANY DETAILING SHALL BE PROTECTED FROM POTENTIAL DAMAGE DURING WORK (IE. CHIPPING DAMAGE ETC.). COVER WITH PLYWOOD, PROVIDE TEMPORARY CORNER GUARDS AT OPENINGS, PARAPET ETC.

ORIGINAL STEEL WINDOWS AND GLAZING SHOULD BE PROTECTED IN PLACE DURING CONSTRUCTION.

FOR CONCRETE REHABILITATION WORK SEE NATIONAL PARK SERVICE PRESERVATION BRIEF #15.

FOR STEEL WINDOW REHABILITATION WORK SEE NATIONAL PARK SERVICE PRESERVATION BRIEF #13.



**TREATMENT  
PLAN**

PROJECT : THE PACIFIC TELEPHONE & TELEGRAPH CO. GARAGE  
HRS. #1355 TREATMENT PLAN  
660 10TH AVE. AND 559 9TH AVE., SAN DIEGO, CA 92101  
CLIENT : CISTERRA PARTNERS, LLC  
3580 CARMEL MOUNTAIN ROAD, STE. # 460, SAN DIEGO, CA 92130

ARCHITECT: UNION ARCHITECTURE, INC.  
JOHN H. EISENHART, ARCHITECT  
EVA THORN, INTERIORS  
3434 22ND STREET, SAN DIEGO, CA 92102  
TELEPHONE: 619 788 2862 / WWW.UNIONARCH.COM

3rd SUBMITTAL  
6.9.20

ISSUES:

PROJECT NO:  
5937.00

FILE NAME:  
BIM\_360//5937.00-Cisterra  
-9QUARCH\_Cisterra9G\_2018.3.rvt

DRAWN BY: \_\_\_\_\_ CHECKED BY: \_\_\_\_\_  
Author Checker

PLOT DATE:  
6/9/2020 2:03:55 PM

TITLE:


TP.10

DATE: 09.13.2019  
REV: 03.20.2020

**TP.10**

SHEET 12 OF 12

58

 **carrierjohnson + CULTURE**  
architecture + environments + brand strategy + graphics

**Cisterra**  
DEVELOPMENT

185 W F St suite 500, San Diego, CA 92101  
phone 619.239.2353

**9G  
PLANNED DEVELOPMENT PERMIT  
PACKAGE**

185 W F St suite 500, San Diego, CA 92101  
phone 619.239.2353



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Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2019-AWP-9554-OE

Issued Date: 09/12/2019

Paul Thometz  
Cisterra  
3580 Carmel Mountain road  
San Diego, CA 92130

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building 9G Tower  
Location: San Diego, CA  
Latitude: 32-42-44.80N NAD 83  
Longitude: 117-09-22.85W  
Heights: 47 feet site elevation (SE)  
321 feet above ground level (AGL)  
368 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does exceed obstruction standards but would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, red lights - Chapters 4,5(Red),&12.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

☐ At least 10 days prior to start of construction (7460-2, Part 1)  
☒ Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

This determination expires on 03/12/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, with



Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2019-AWP-9907-OE

Issued Date: 09/25/2019

Paul Thometz  
Cisterra  
3580 Carmel Mountain road  
San Diego, CA 92130

**\*\*DETERMINATION OF NO HAZARD TO AIR NAVIGATION FOR TEMPORARY STRUCTURE\*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Crane 9G Tower  
Location: san diego, CA  
Latitude: 32-42-43.57N NAD 83  
Longitude: 117-09-21.89W  
Heights: 45 feet site elevation (SE)  
365 feet above ground level (AGL)  
410 feet above mean sea level (AMSL)

This aeronautical study revealed that the temporary structure does exceed obstruction standards but would not be a hazard to air navigation provided the condition(s), if any, in this letter is (are) met:

**\*\*SEE ATTACHMENT FOR ADDITIONAL CONDITION(S) OR INFORMATION\*\***

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of a structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this temporary structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

A copy of this determination will be forwarded to the Federal Aviation Administration Flight Procedures Office if the structure is subject to the issuance of a Notice To Airman (NOTAM).

6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (424) 405-7643, or karen.mcdonald@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AWP-9554-OE.

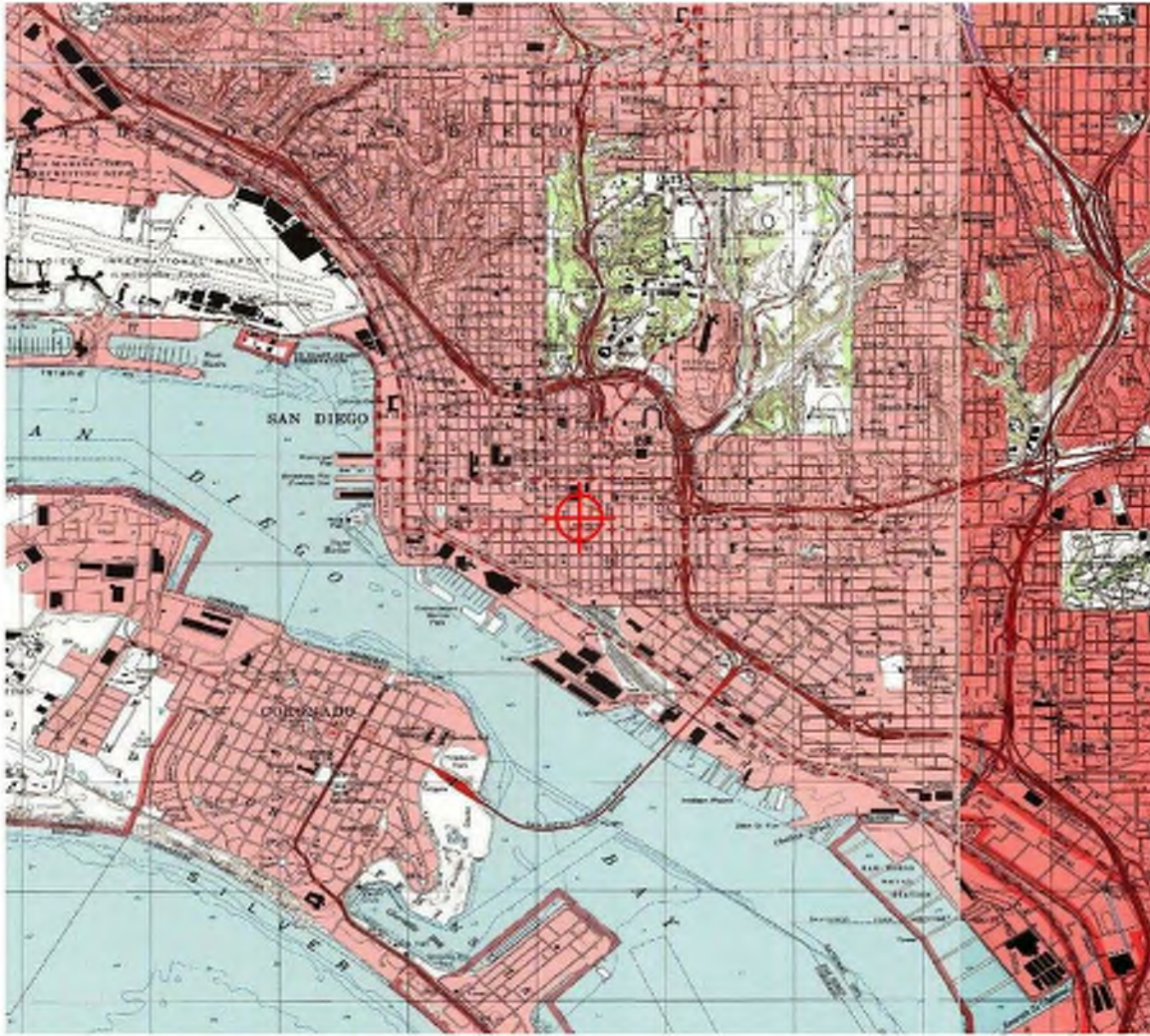
Signature Control No: 414607185-417125244

Karen McDonald  
Specialist

Attachment(s)  
Map(s)

( EBO )

TOPO Map for ASN 2019-AWP-9554-OE



Additional Condition(s) or Information for ASN 2019-AWP-9907-OE

**Proposal:** To construct and/or operate a(n) Crane to a height of 365 feet above ground level, 410 feet above mean sea level.

**Location:** The structure will be located 2.13 nautical miles southeast of SAN Airport reference point.

**Part 77 Obstruction Standard(s) Exceeded and Aeronautical Impacts, if any:**

Section 77.17 (a) (2) by 165 feet - a height that exceeds 245 feet above mean sea level within 2.13 nautical miles of SAN.

Section 77.17 (a) (5) a height that affects an Airport Surface by penetrating:

Section 77.19 (a) Horizontal Surface by 244 feet as applied to SAN.

**Preliminary FAA study indicates that the above mentioned structure would:**

have no effect on any existing or proposed arrival, departure, or en route instrument/visual flight rules (IFR/ VFR) minimum flight altitudes.  
not exceed traffic pattern airspace  
have no physical or electromagnetic effect on the operation of air navigation and communications facilities.  
have no effect on any airspace and routes used by the military.

Based on this aeronautical study, the structure would not constitute a substantial adverse effect on aeronautical operations or procedures because it will be temporary. The temporary structure would not be considered a hazard to air navigation provided all of the conditions specified in this determination are strictly met.

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 2, Obstruction Marking and Lighting, flags/red lights - Chapters 3(Marked),4,5(Red),&12.

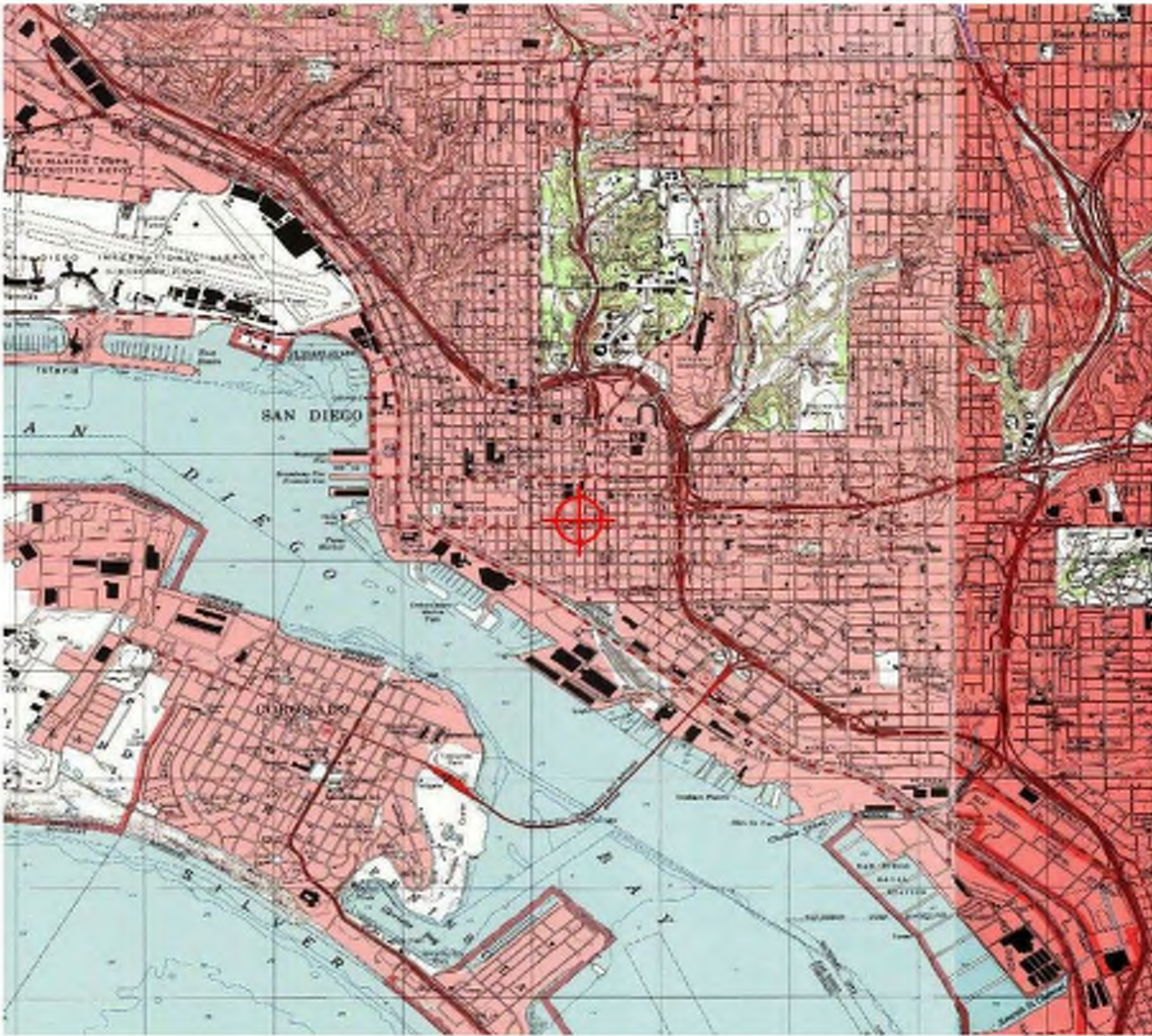
Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that the manager of SAN DIEGO INTL @ (619) 400-2761 be notified at least 3 business days prior to the temporary structure being erected and again when the structure is removed from the site.

This determination expires on 03/25/2021 unless extended, revised, or terminated by the issuing office.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

TOPO Map for ASN 2019-AWP-9907-OE



3rd SUBMITTAL  
6.9.20

ISSUES:

PROJECT NO:  
5937.00  
FILE NAME:  
B:\M 5937\5937-00-Cisterra  
-95047CH - Cisterra\05\_2018.3.rvt  
DRAWN BY: CHECKED BY:  
Author Checker  
PLOT DATE:  
6/9/2020 2:03:56 PM  
TITLE:

FAA

59

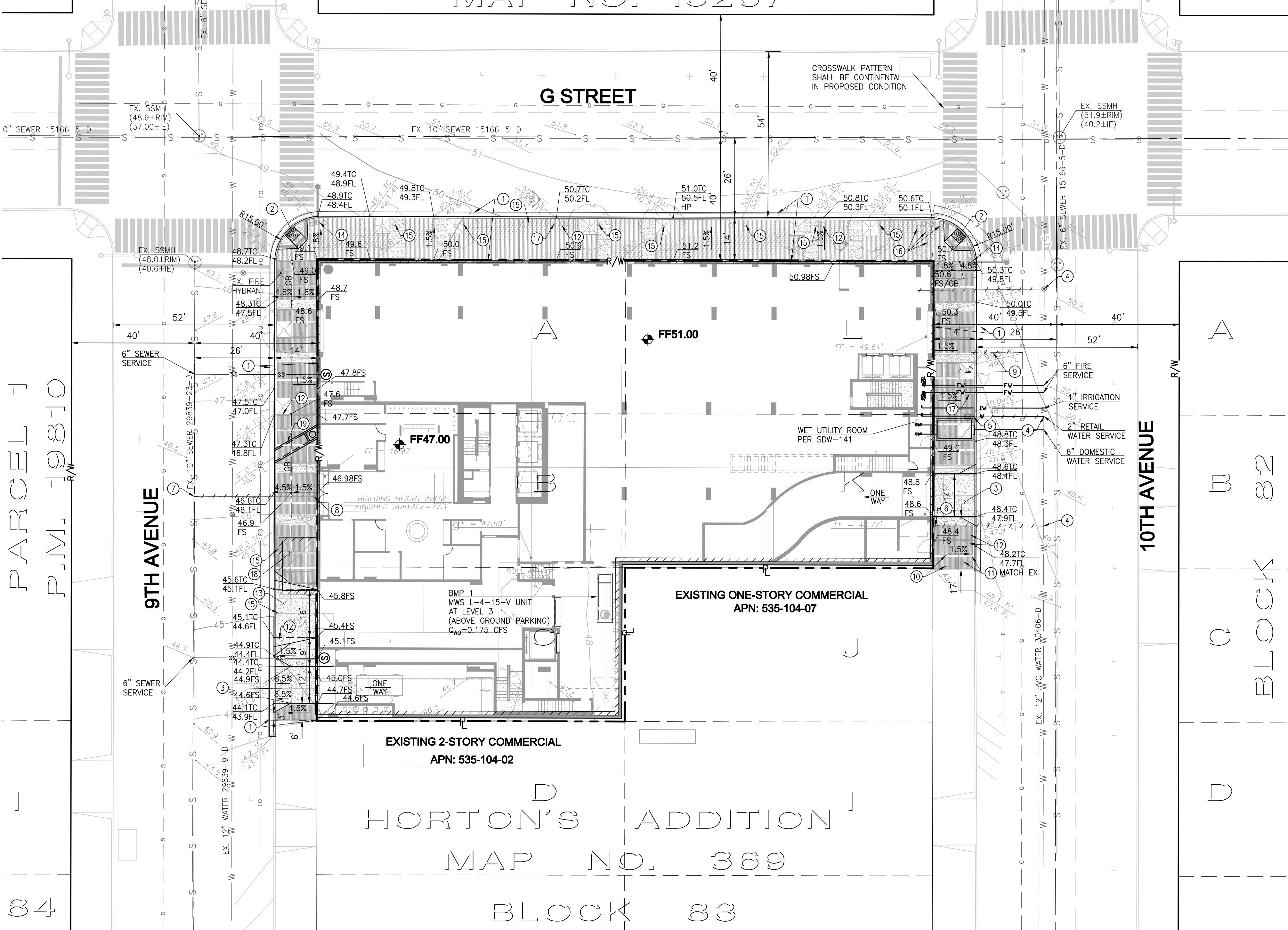
9G  
PLANNED DEVELOPMENT PERMIT  
PACKAGE

carrierjohnson + CULTURE  
architecture + environments + brand strategy + graphics  
Cisterra  
DEVELOPMENT

185 W F St suite 500, San Diego, CA 92101  
phone 619.239.2353



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## LEGAL DESCRIPTION

THE LAND REFERRED TO HEREIN BELOW IS SITUATED IN THE CITY OF SAN DIEGO, IN THE COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOW:

LOTS A, B, C, K AND L IN BLOCK 83 OF HORTON'S ADDITION, IN THE CITY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO MAP THEREOF ON FILE IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY.

APN: 535-136-01-00

## GRADING QUANTITIES

TOTAL SITE AREA: 0.575 AC  
DISTURBED AREA: 0.575 AC  
AMOUNT OF CUT: 21,500 CY  
MAX. DEPTH OF CUT: 23 FT  
AMOUNT OF FILL: 0 CY  
MAX. DEPTH OF FILL: 0 FT  
AMOUNT OF EXPORT: 21,500 CY

GRADING QUANTITIES ARE APPROXIMATE AND SUBJECT TO CHANGE BASED ON FINAL DESIGN. QUANTITIES SHALL NOT BE USED FOR BIDDING PURPOSES.

## THE BASIS OF BEARINGS

THE BASIS OF BEARINGS OF THIS SURVEY IS THE CENTERLINE OF "G" STREET AS SHOWN ON MAP 15267, I.E. N 89°51'44"W

## TOPOGRAPHY SOURCE

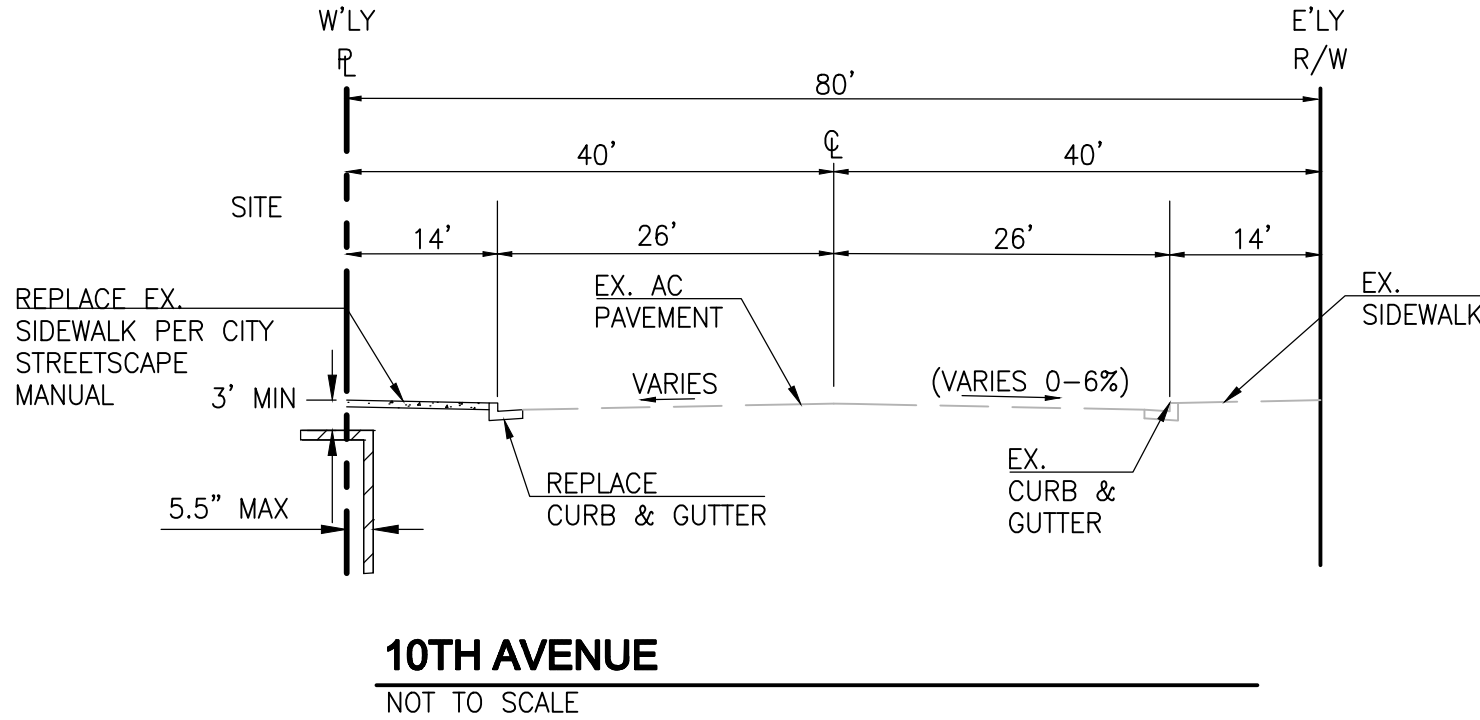
SAN DIEGO LAND SURVEYING & ENGINEERING, INC  
9665 CHESAPEAKE DRIVE, SUITE 445  
SAN DIEGO, CA 92123  
ALTA/NSPS LAND TITLE SURVEY DATED MAY 23, 2019

## LEGEND

PROPERTY LINE/ TM BOUNDARY	---
RIGHT OF WAY	---
EXISTING LOT LINE	---
CENTERLINE	---
EXISTING IMPROVEMENTS	
WATER	---
SEWER	---
GAS	---
FIBER OPTICS	---
ELECTRICAL	---
TRAFFIC SIGNAL	---

## PROPOSED IMPROVEMENTS

WATER SERVICE	---
IRRIGATION SERVICE	---
SEWER LATERAL	---
STORM DRAIN	---
FIRE SERVICE	---
CURB AND GUTTER	---
BACKFLOW PREVENTER	---
WATER METER VAULT WITH DUAL 4" METERS	---
TREE GRATE	---
STREET LIGHT	---

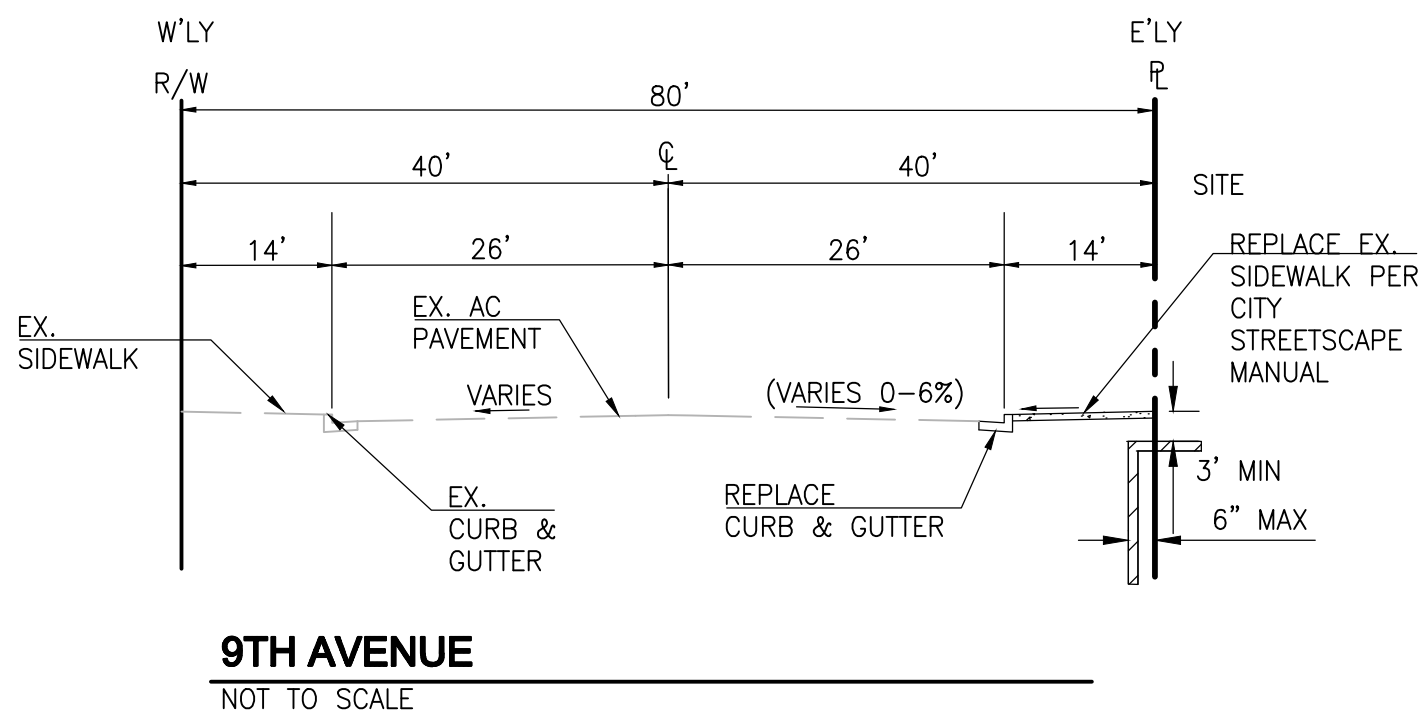
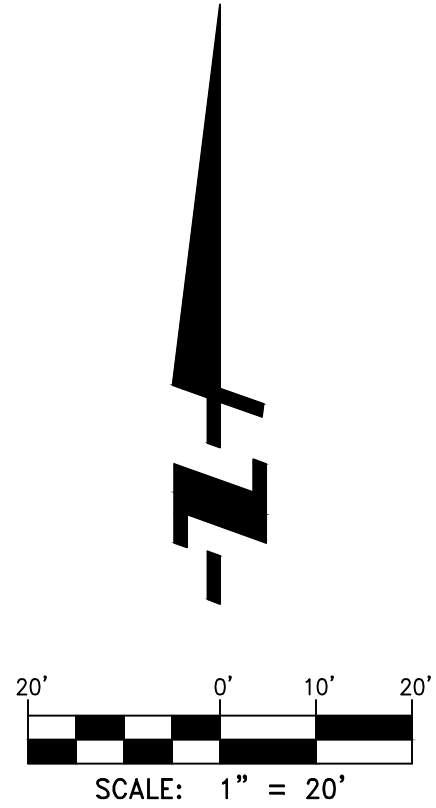
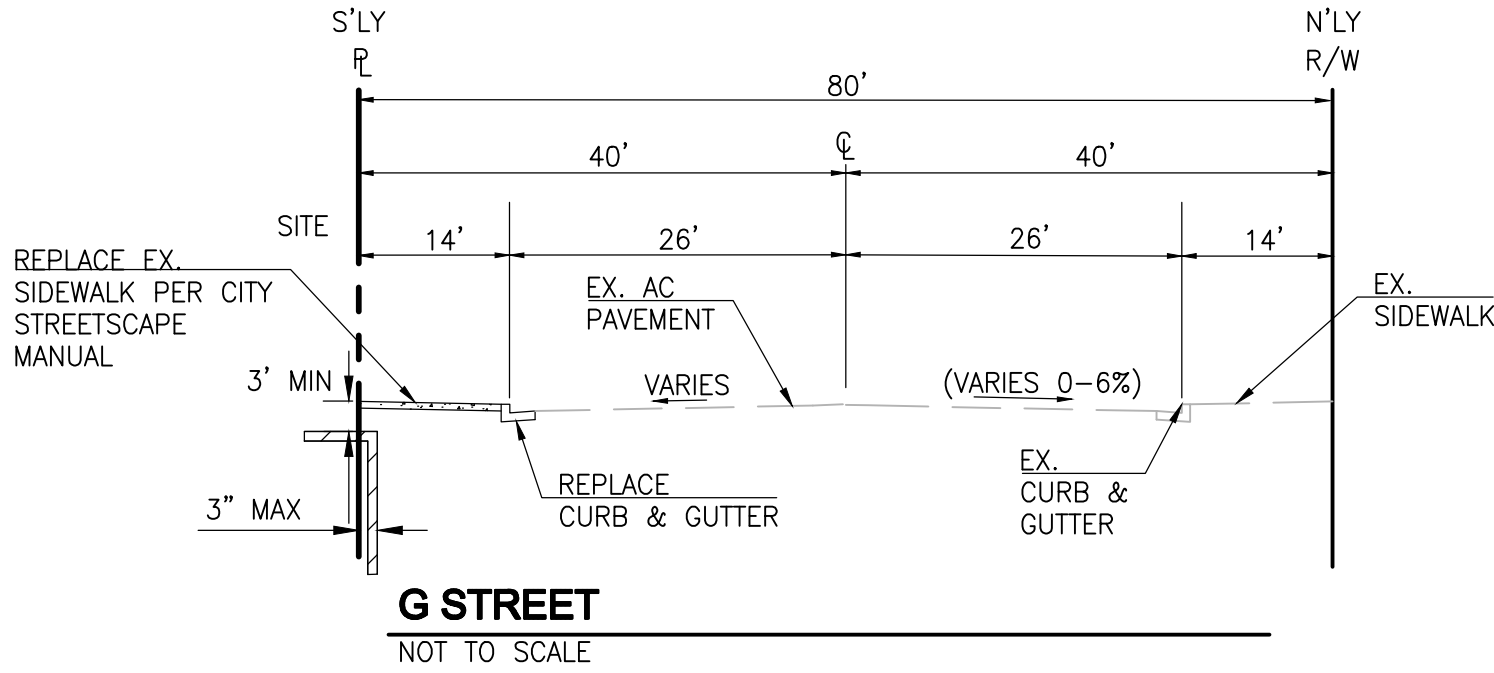


## NOTES

- FOR STREETSCAPE, STREET LIGHTS, AND STREET TREES REFER TO ARCHITECT DRAWINGS
- IF A 3" OR LARGER METER IS REQUIRED FOR THIS PROJECT, THE OWNER/PERMITEE SHALL CONSTRUCT A NEW METER VAULT BELOW GROUND WITHIN THE PUBLIC ROW IN A MANNER SATISFACTORY TO THE PUBLIC UTILITIES DIRECTOR AND CITY ENGINEER
- ALL PROPOSED WATER AND SEWER FACILITIES (PUBLIC AND PRIVATE) WITHIN THE PUBLIC ROW OR PUBLIC EASEMENT MUST BE DESIGNED, CONSTRUCTED, OR ABANDONED IN ACCORDANCE WITH THE CRITERIA ESTABLISHED WITHIN THE CITY OF SAN DIEGO'S CURRENT WATER AND SEWER FACILITY DESIGN GUIDELINES, REGULATIONS, STANDARDS, AND PRACTICES PERTAINING THERETO.
- ALL WATER LINES SERVICING THIS DEVELOPMENT MUST PASS THROUGH A PERMITTED, PRIVATE, ABOVE GROUND, BACKFLOW PREVENTION DEVICE (BFPD)
- THE OWNER/PERMITEE SHALL BE RESPONSIBLE FOR ANY DAMAGE CAUSED TO CITY OF SAN DIEGO WATER AND SEWER FACILITIES IN THE VICINITY OF THE PROJECT SITE, DUE TO THE CONSTRUCTION ACTIVITIES ASSOCIATED WITH THIS PROJECT, IN ACCORDANCE WITH MUNICIPAL CODE SECTION 142.0607 IN THE EVENT THAT ANY SUCH FACILITY LOSES INTEGRITY THEN, THE OWNER/PERMITEE SHALL REPAIR OR RECONSTRUCT ANY DAMAGED PUBLIC WATER AND SEWER FACILITY IN A MANNER SATISFACTORY TO THE PUBLIC UTILITIES DIRECTOR AND THE CITY ENGINEER
- THE PROPOSED PROJECT WILL COMPLY WITH ALL THE REQUIREMENTS OF THE CURRENT CITY OF SAN DIEGO STORM WATER STANDARDS MANUAL BEFORE A GRADING OR BUILDING PERMIT IS ISSUED. IT IS THE RESPONSIBILITY OF THE OWNER/DESIGNER/APPLICANT TO ENSURE THAT THE CURRENT STORM WATER PERMANENT BMP DESIGN STANDARDS ARE INCORPORATED INTO THE PROJECT.
- PRIOR TO THE ISSUANCE OF ANY CONSTRUCTION PERMIT, THE OWNER/PERMITEE SHALL ENTER INTO A MAINTENANCE AGREEMENT FOR THE ONGOING PERMANENT BMP MAINTENANCE, SATISFACTORY TO THE CITY ENGINEER.
- PRIOR TO THE ISSUANCE OF ANY CONSTRUCTION PERMIT, THE OWNER/PERMITEE SHALL INCORPORATE ANY CONSTRUCTION BEST MANAGEMENT PRACTICES NECESSARY TO COMPLY WITH CHAPTER 14, ARTICLE 2, DIVISION 1 (GRADING REGULATIONS) OF THE SAN DIEGO MUNICIPAL CODE, INTO THE CONSTRUCTION PLANS OR SPECIFICATIONS.
- PRIOR TO THE ISSUANCE OF ANY CONSTRUCTION PERMIT, THE OWNER/PERMITEE SHALL SUBMIT A WATER POLLUTION CONTROL PLAN (WPCP). THE WPCP SHALL BE PREPARED IN ACCORDANCE WITH THE GUIDELINES IN PART 2 CONSTRUCTION BMP STANDARDS CHAPTER 4 OF THE CITY'S STORM WATER STANDARDS.

## KEYNOTES

- REPLACE EXISTING CURB, GUTTER, AND SIDEWALK PER CITY STREETSCAPE MANUAL
- REPLACE EXISTING CURB RAMP PER SDG-132
- DRIVEWAY PER SDG-159, WIDTH PER PLAN
- EXISTING WATER SERVICE TO BE KILLED AT MAIN
- EXISTING WATER METER TO BE REMOVED
- EXISTING FDC TO BE REMOVED
- EXISTING SEWER LATERAL, TO BE ABANDONED AT THE PL
- EXISTING SEWER CLEANOUT TO BE REMOVED
- EXISTING GAS SERVICE AND METER TO BE REMOVED
- EXISTING SDG&E TRANSFORMERS TO BE REMOVED/RELOCATED
- EXISTING COMMUNICATION AND ELECTRICAL BOX TO REMOVED/RELOCATED
- EXISTING STREETLIGHT AND BOX TO BE RELOCATED
- DRIVEWAY PER SDG-163, WIDTH PER PLAN
- EXISTING TRAFFIC SIGNAL TO REMAIN, PROTECT IN PLACE
- EXISTING PARKING METER TO BE RELOCATED
- EXISTING ELECTRICAL AND COMMUNICATION VAULTS AND ELECTRICAL RISER TO REMAIN, PROTECT IN PLACE
- EXISTING SIGN TO BE RELOCATED
- UNDERGROUND SDG&E VAULT
- CURB OUTLET PER SDSRD D-25A



ISSUES:  
PRELIMINARY NOT FOR CONSTRUCTION

PROJECT NO:  
1459-002  
FILE NAME:  
DRAWN BY:  
BC  
CHECKED BY:  
BS  
PLOT DATE:  
06/09/2020  
TITLE:

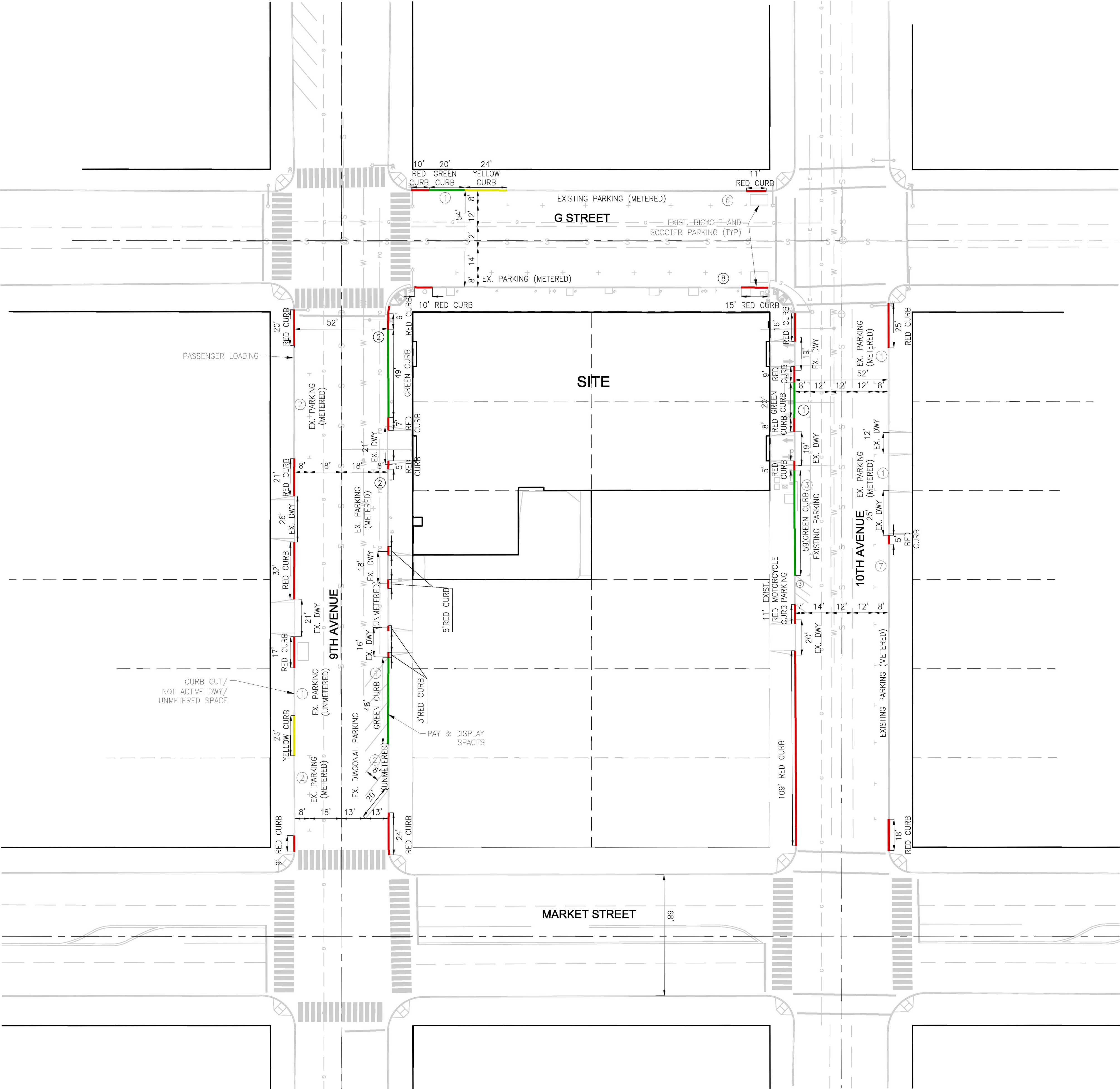
CONCEPTUAL  
GRADING &  
UTILITY  
PLAN

Sheet C1 of C3

60 C-1



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CONCEPTUAL EXISTING CURB USAGE PLAN

LEGEND

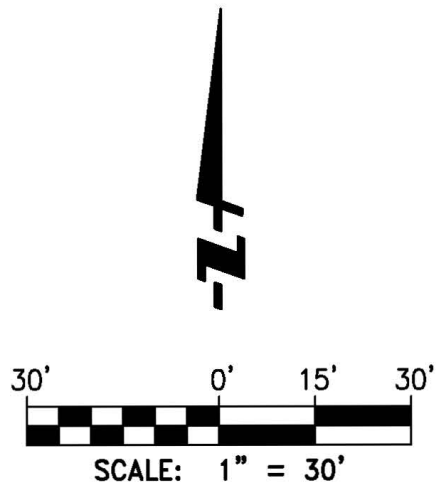
- PROPERTY LINE
- - - STREET CENTERLINE
- Ⓢ PARKING COUNT
- ◻ EXISTING DRIVEWAY

PARKING SUMMARY TABLE

STREET	EXISTING PARKING SUMMARY			
	PARKING TYPE			
	METERED	UNMETERED	TIME RESTRICTED (GREEN CURB)	TOTAL
G STREET	14	0	1	15
9TH AVENUE	9	0	6	15
10TH AVENUE	9	0	4	13
TOTAL PARKING COUNT	29	0	11	40

CURB DESIGNATIONS

- NO PARKING
- COMMERCIAL LOADING  
20 MIN MAX
- TIME RESTRICTED (30 MIN, UNLESS OTHERWISE NOTED)



ISSUES:  
PRELIMINARY NOT FOR CONSTRUCTION

PROJECT NO:  
1459-002  
FILE NAME:  
DRAWN BY:  
DM  
CHECKED BY:  
BS  
PLOT DATE:  
06/09/2020  
TITLE:

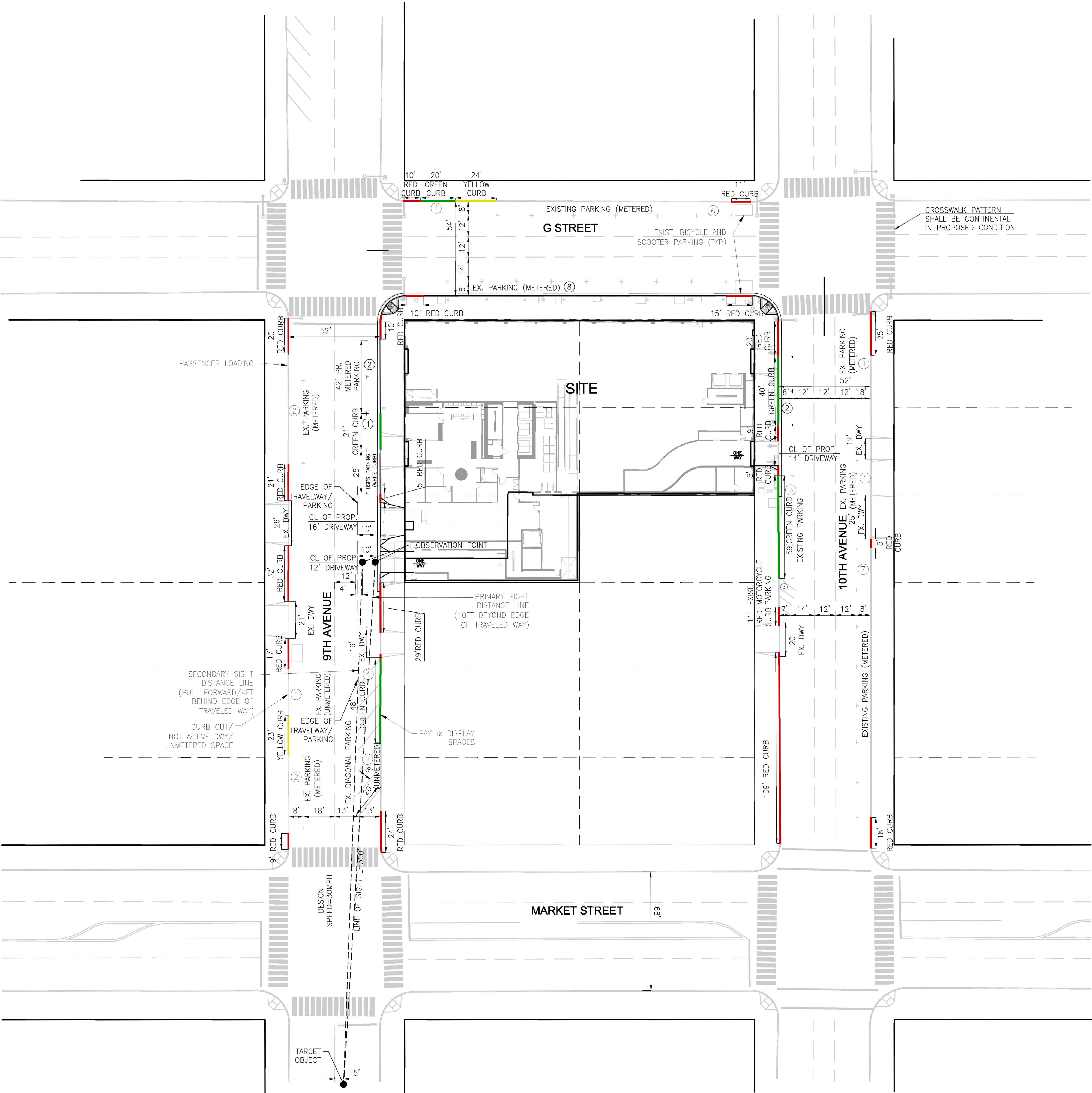
CONCEPTUAL  
EXISTING  
CURB USAGE  
PLAN

Sheet C2 of C3

C-2  
60a



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CONCEPTUAL PROPOSED CURB USAGE PLAN

LEGEND

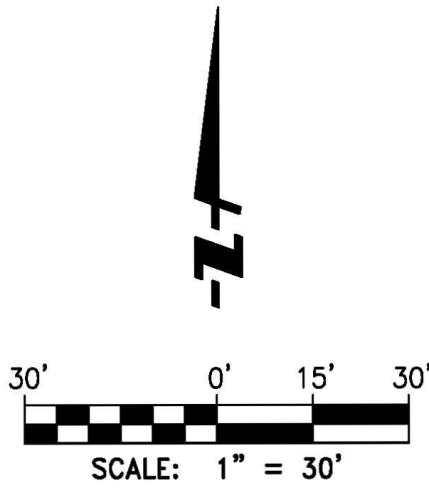
- PROPERTY LINE
- STREET CENTERLINE
- LINE OF SIGHT
- Ⓢ PARKING COUNT
- EXISTING DRIVEWAY

PARKING SUMMARY TABLE

STREET	PROPOSED PARKING SUMMARY			
	PARKING TYPE			
	METERED	UNMETERED	TIME RESTRICTED (GREEN CURB)	TOTAL
G STREET	14	0	1	15
9TH AVENUE	9	3	5	14
10TH AVENUE	9	0	5	14
TOTAL PARKING COUNT	29	3	11	34

CURB DESIGNATIONS

- NO PARKING
- COMMERCIAL LOADING  
20 MIN MAX
- TIME RESTRICTED (30 MIN, UNLESS OTHERWISE NOTED)



**FUSCOE**  
ENGINEERING  
6390 Greenwich Drive, Suite 170  
San Diego, California 92122  
tel 858.554.1500 • fax 858.597.0335  
www.fuscoe.com



ISSUES:  
PRELIMINARY NOT FOR CONSTRUCTION

PROJECT NO:  
1459-002  
FILE NAME:  
DRAWN BY: DM  
CHECKED BY: BS  
PLOT DATE:  
08/09/2020  
TITLE:

CONCEPTUAL  
PROPOSED  
CURB USAGE  
PLAN

Sheet C3 of C3

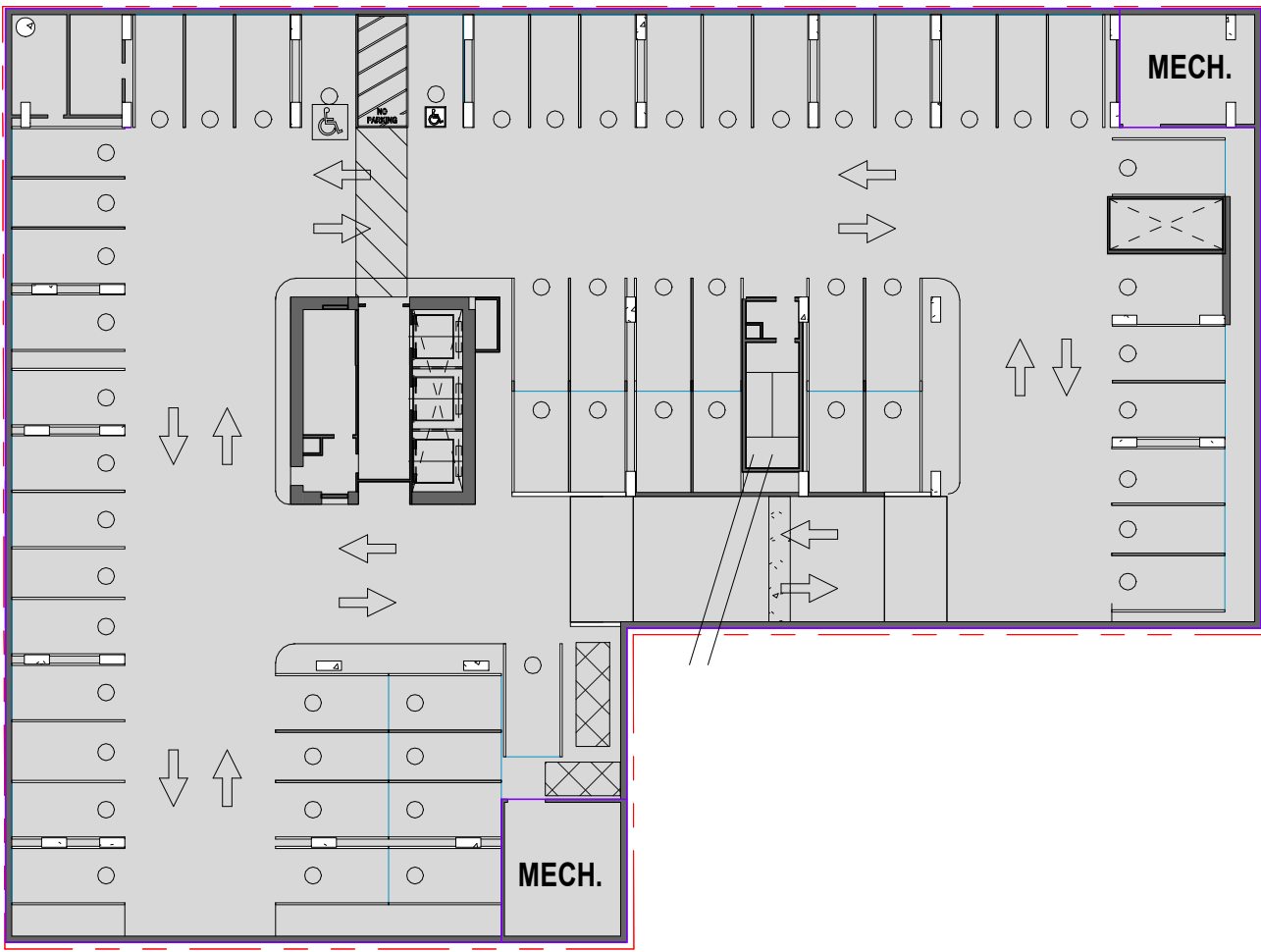
C-3  
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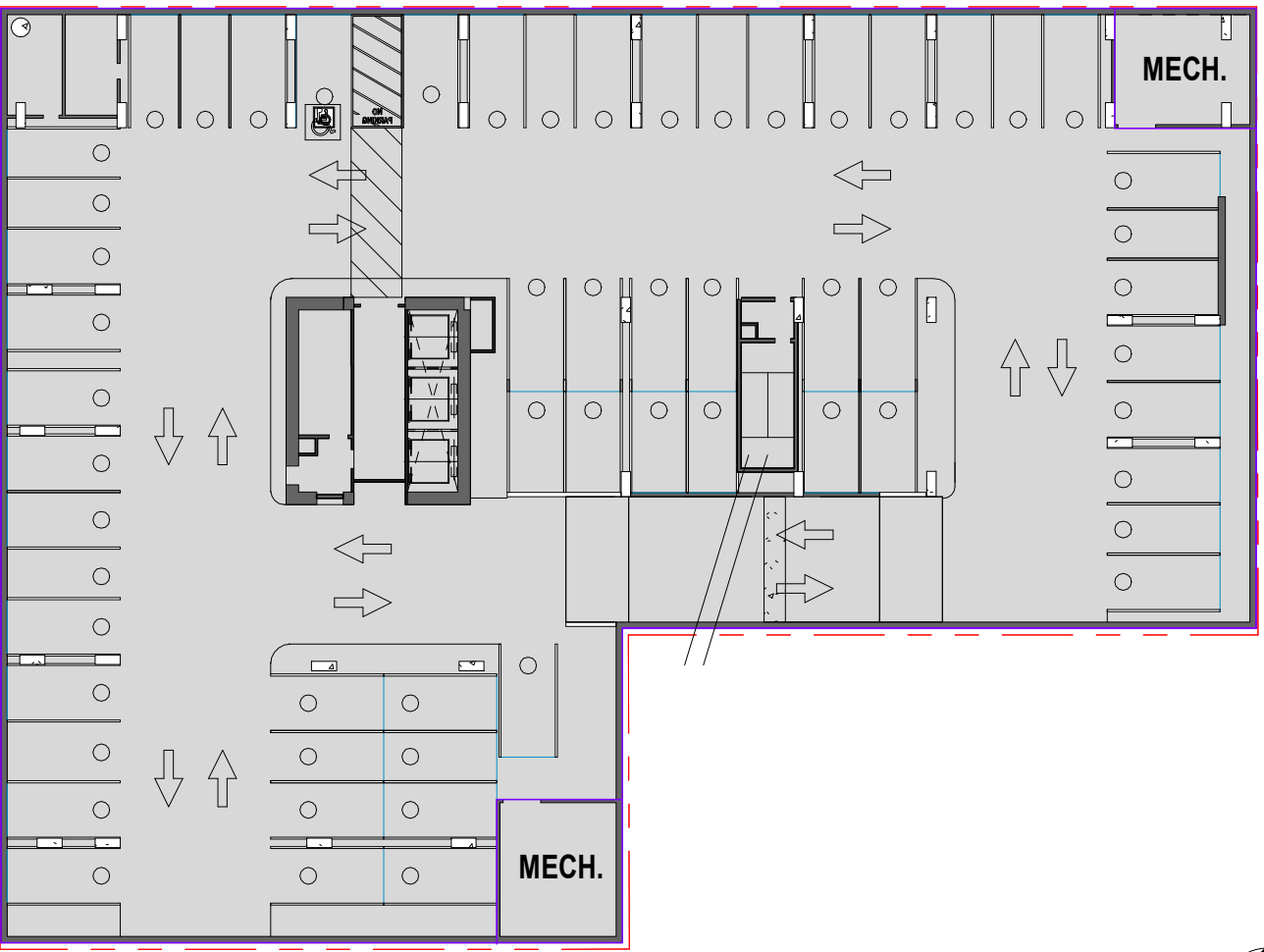
Level Breakdown  
Level 3

Parking/Garage - EXEMPT



Level Breakdown  
Level 4-5

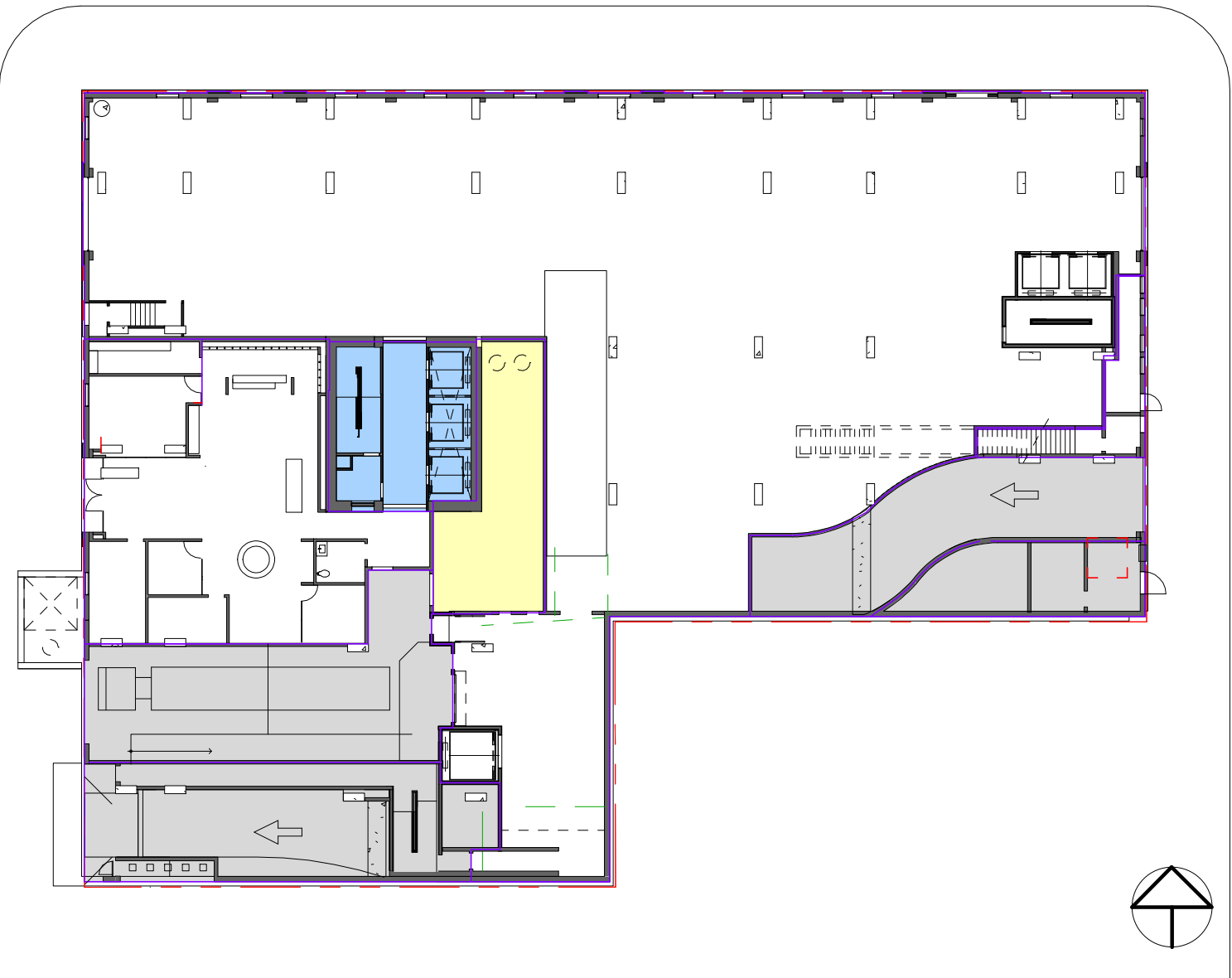
Parking/Garage - EXEMPT



Level Breakdown  
Level 1

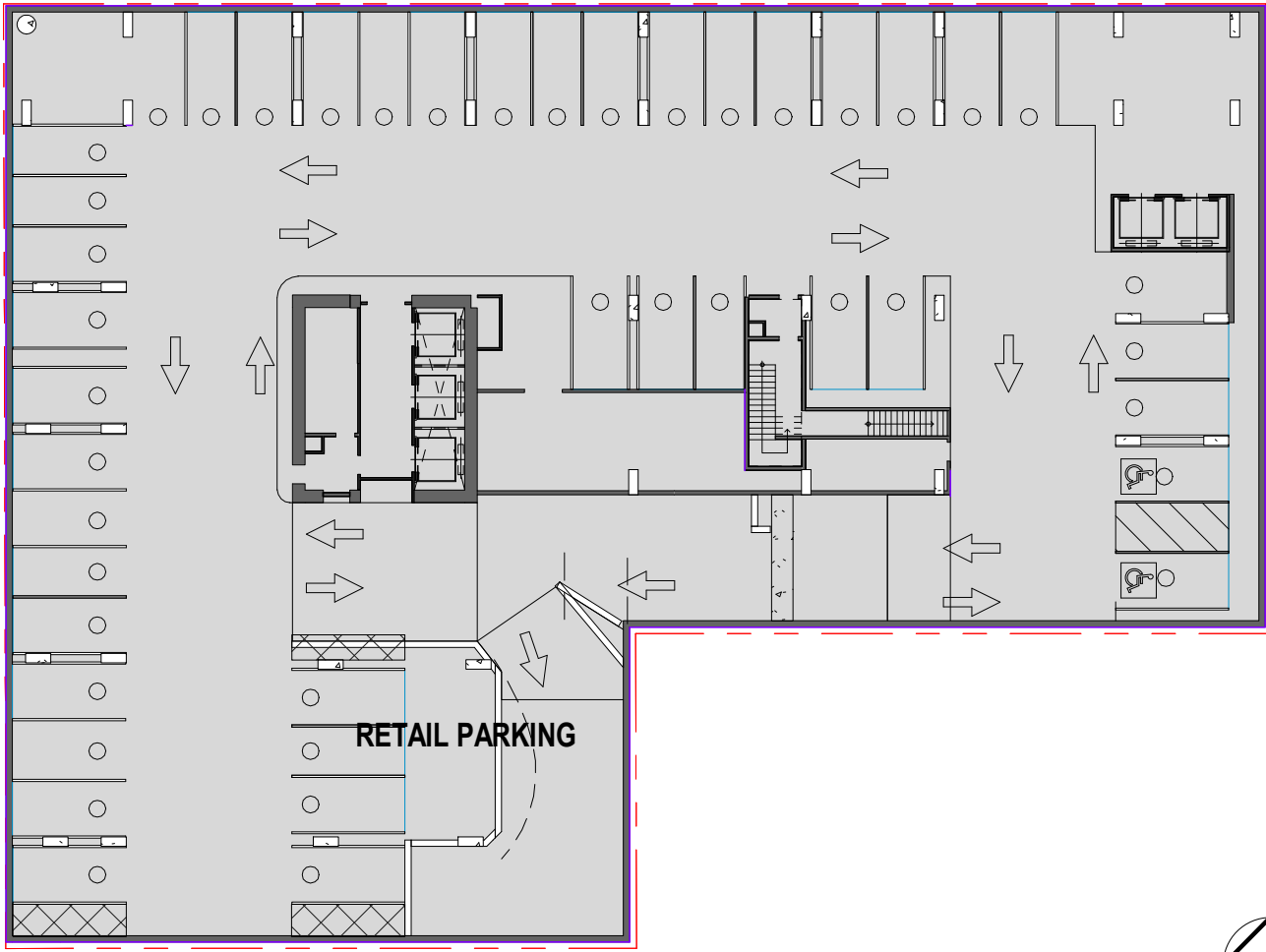
Retail/Office - 14,554 sf  
Residential Lobby - 3,804 sf  
Utilities - 1,872 sf  
Parking/Garage - EXEMPT

Total - 22,112 sf



Level Breakdown  
Level 2

Parking/Garage - EXEMPT



3rd SUBMITTAL  
6.9.20

ISSUES:

PROJECT NO:  
5937.00  
FILE NAME:  
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-ARCH-Calendar\_2018.3.rvt  
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Author Checker  
PLOT DATE:  
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TITLE:

FAR Bonus  
Incentives

9G  
PLANNED DEVELOPMENT PERMIT  
PACKAGE



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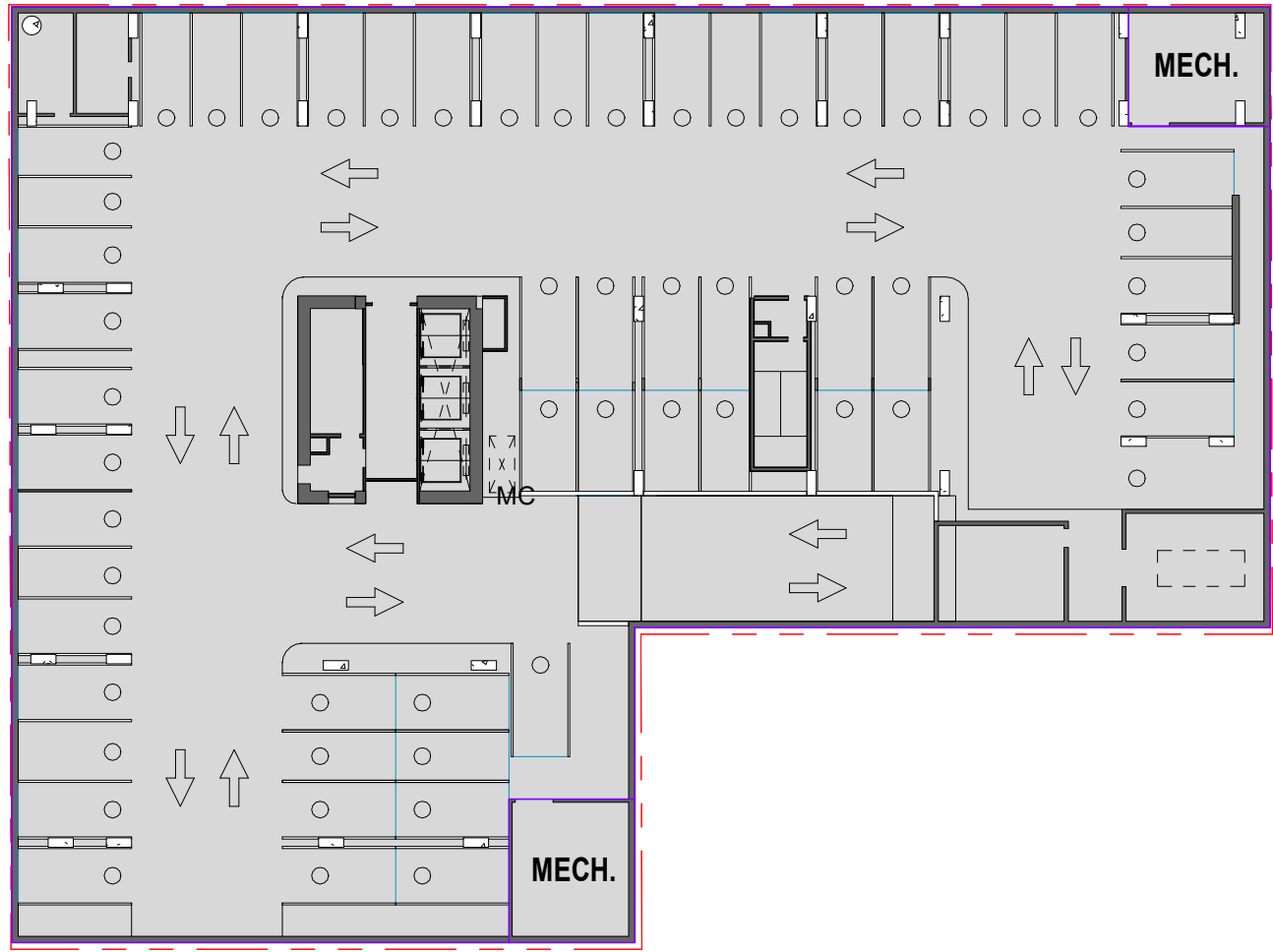
185 W F St suite 500, San Diego, CA 92101  
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Level Breakdown  
Level 6

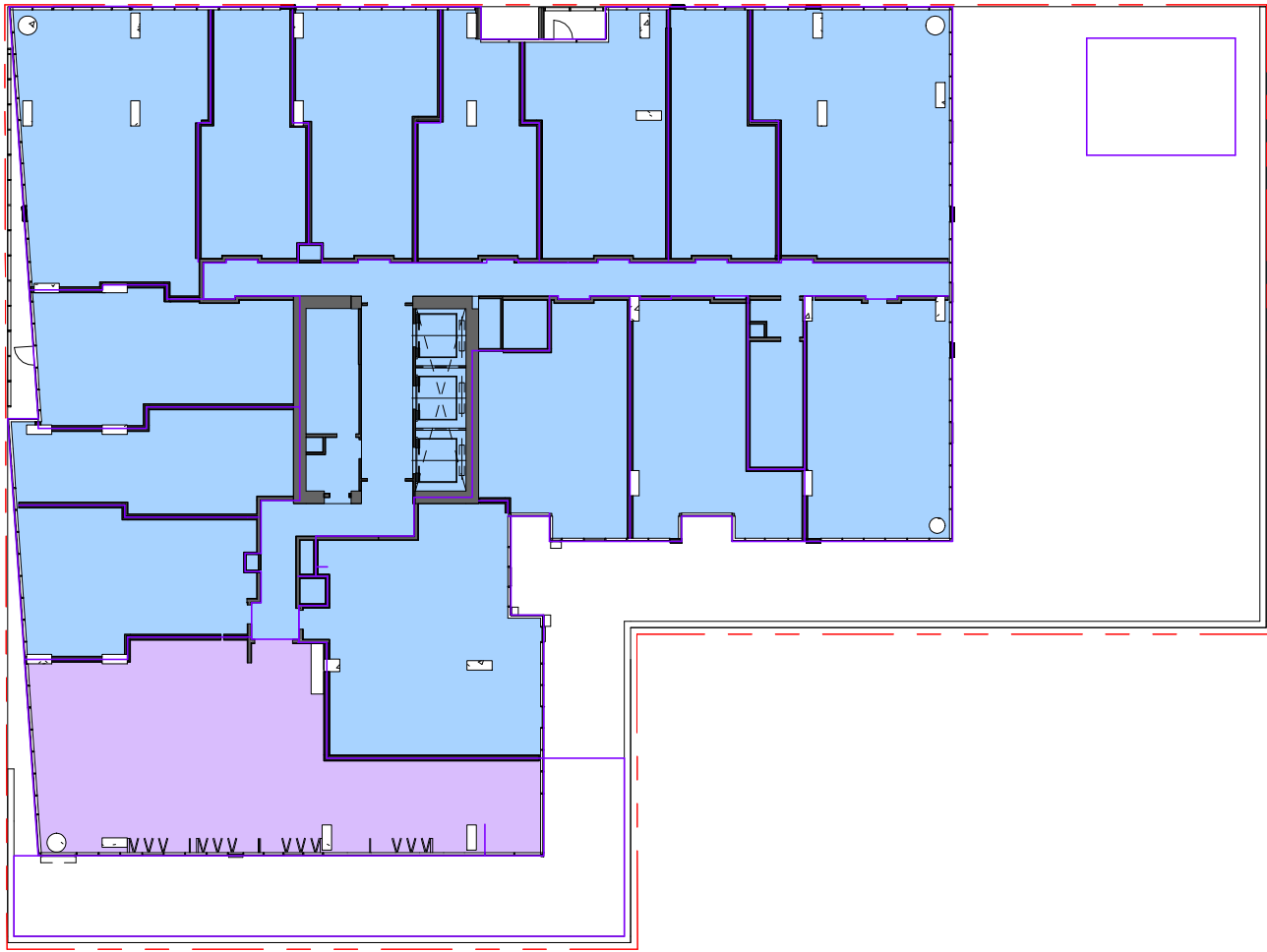
Parking/Garage - EXEMPT



Level Breakdown  
Level 7

Residential - 14,360 sf  
Amenities - 2,108 sf

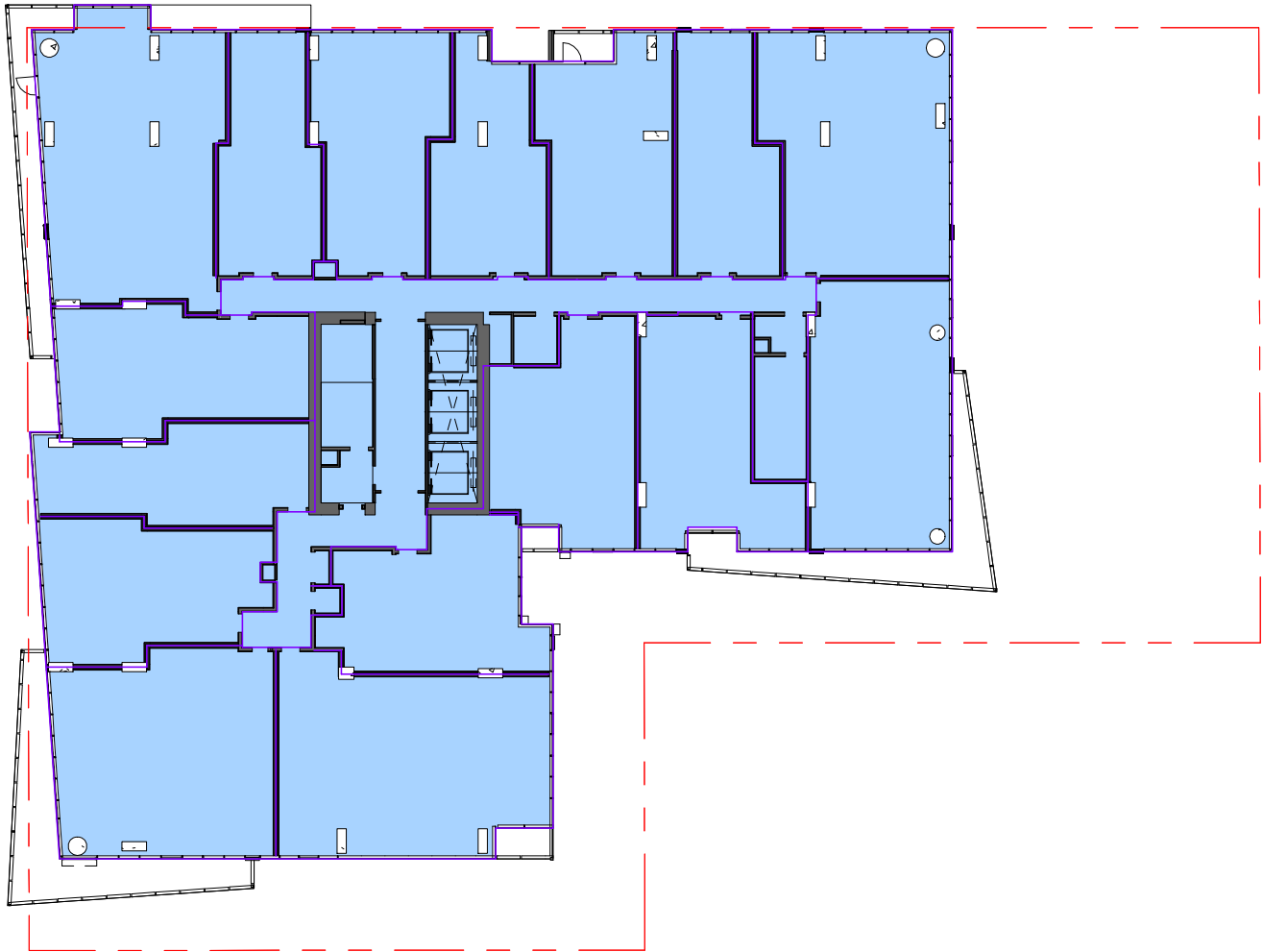
Total - 16,468 sf



Level Breakdown  
Level 8-19

Residential - 16,473 sf

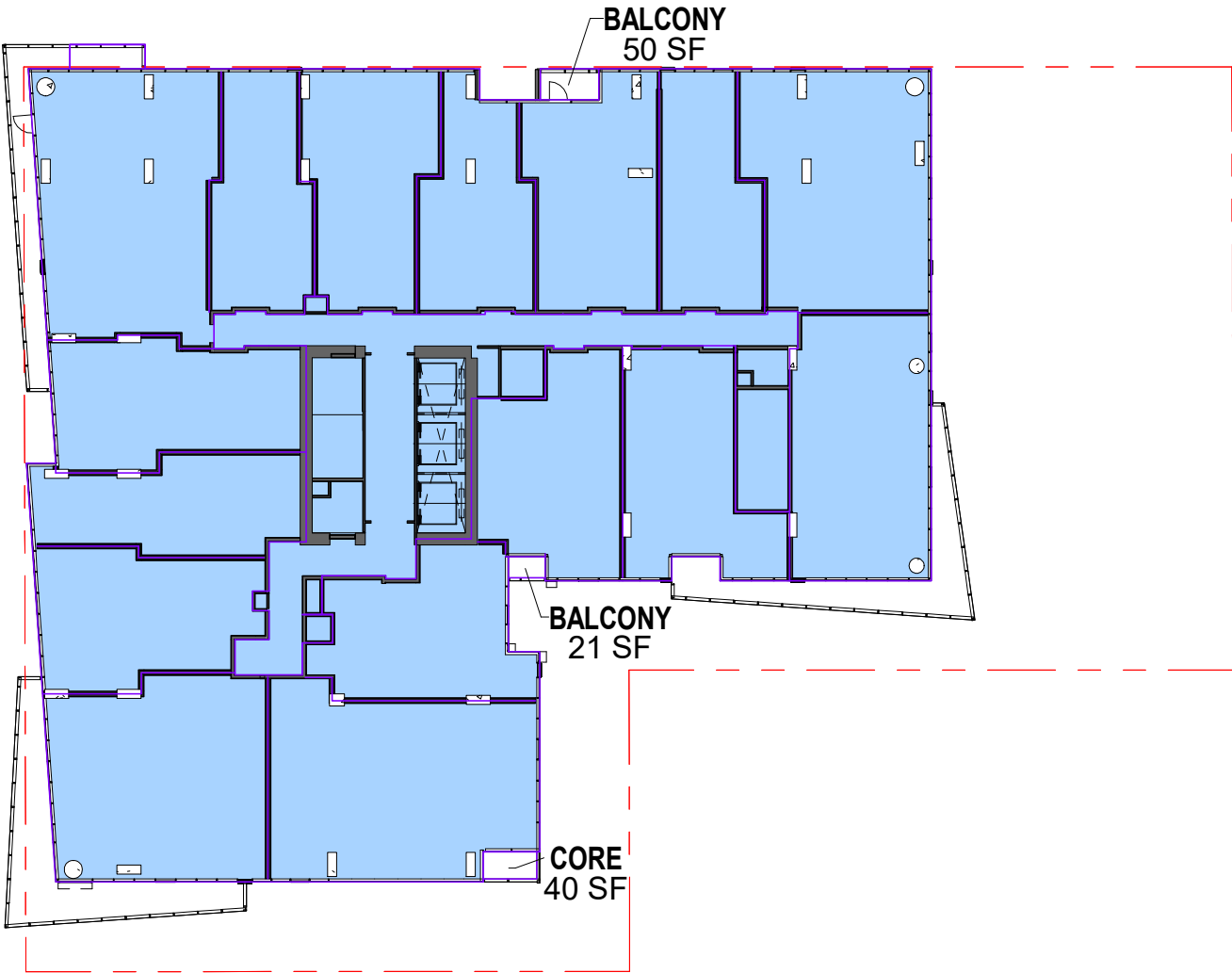
Total - 16,473 sf



Level Breakdown  
Level 20

Residential - 16,423 sf

Total - 16,423 sf



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PLANNED DEVELOPMENT PERMIT  
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6.9.20

ISSUES:

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5937.00  
FILE NAME:  
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FAR Bonus  
Incentives



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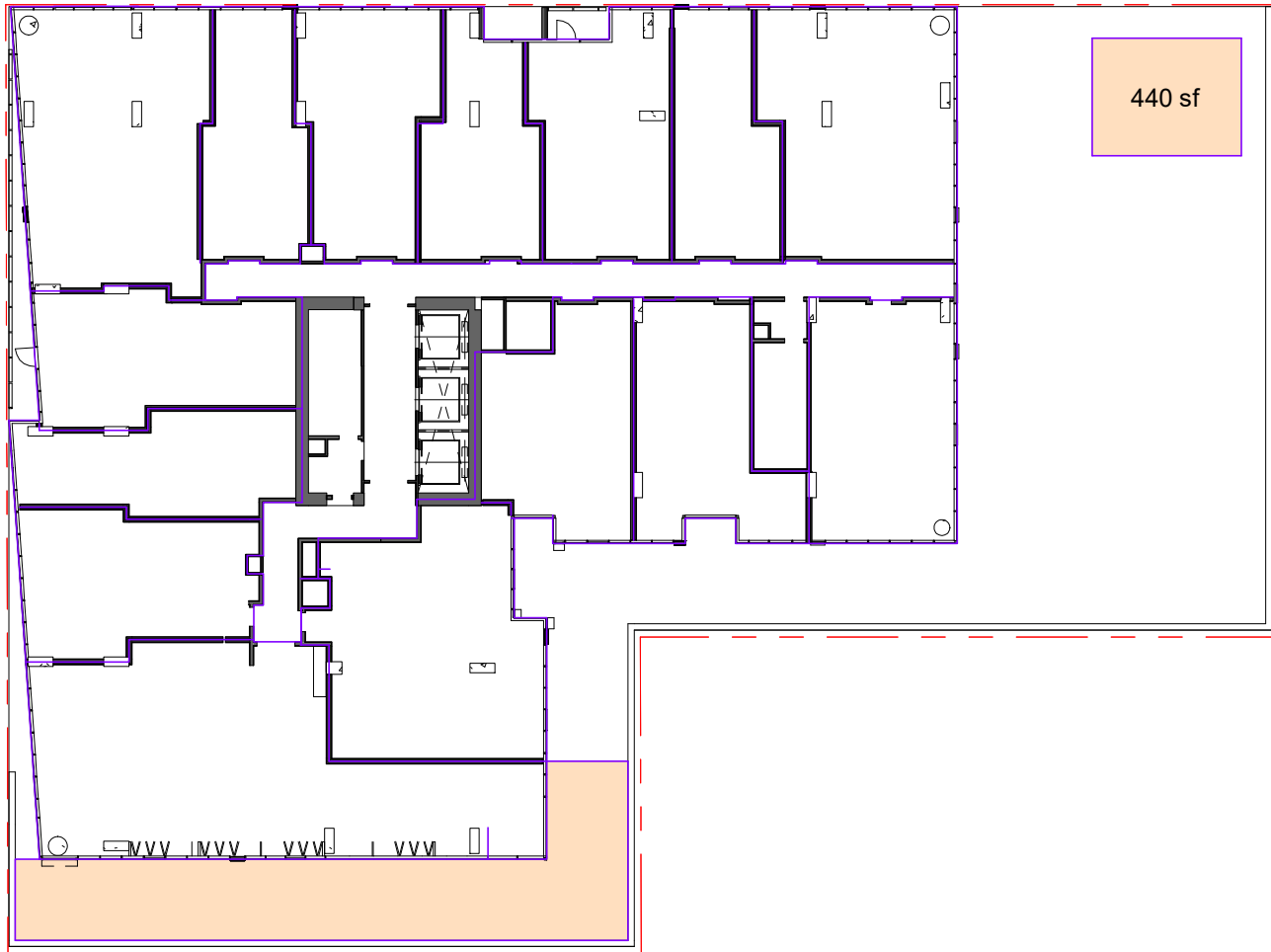
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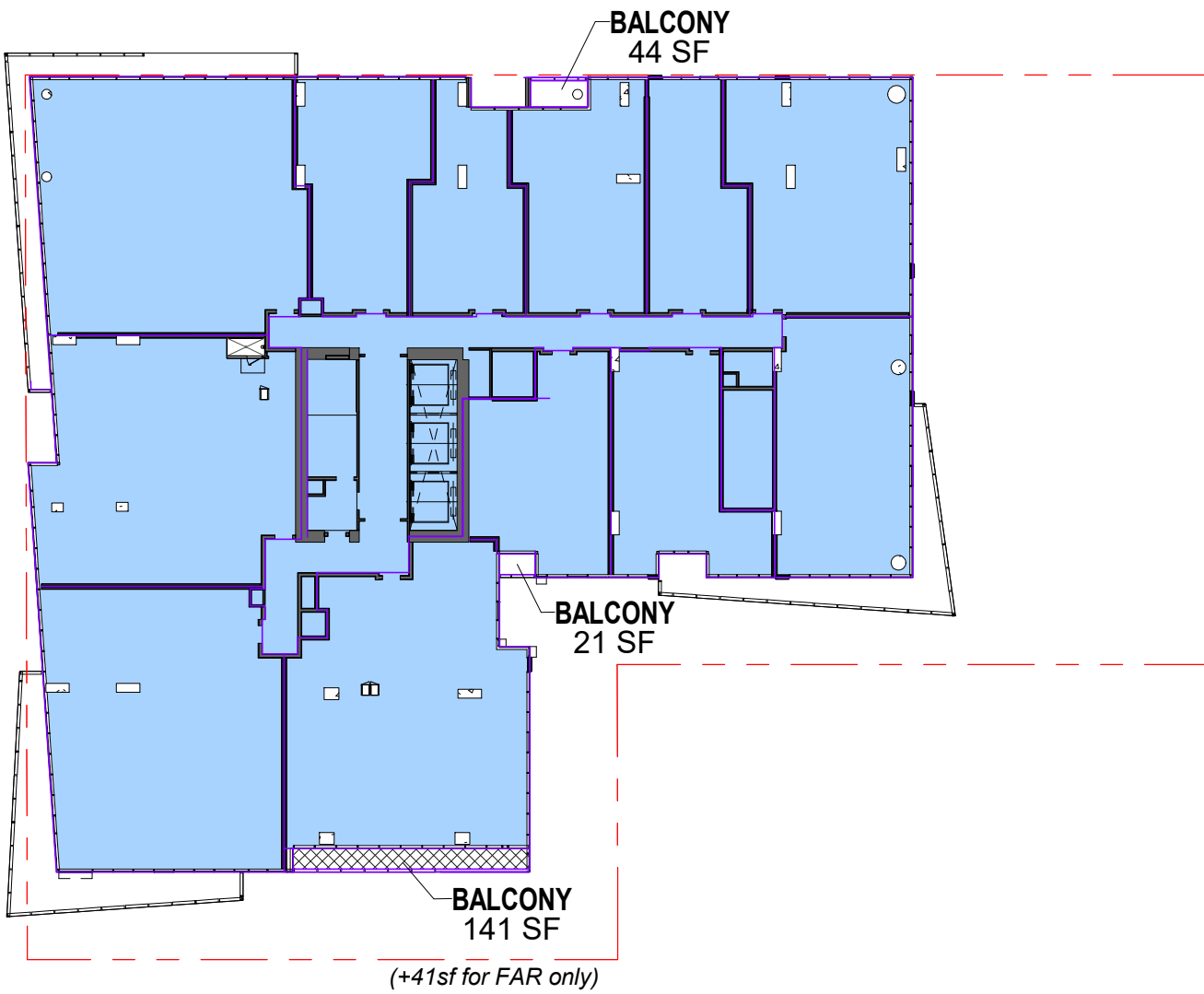
Level Breakdown  
Level 7 - Exterior Common Area

- Open Area - 1,444 sf
  - Pet Area - 440 sf
- Total - 1,884 sf



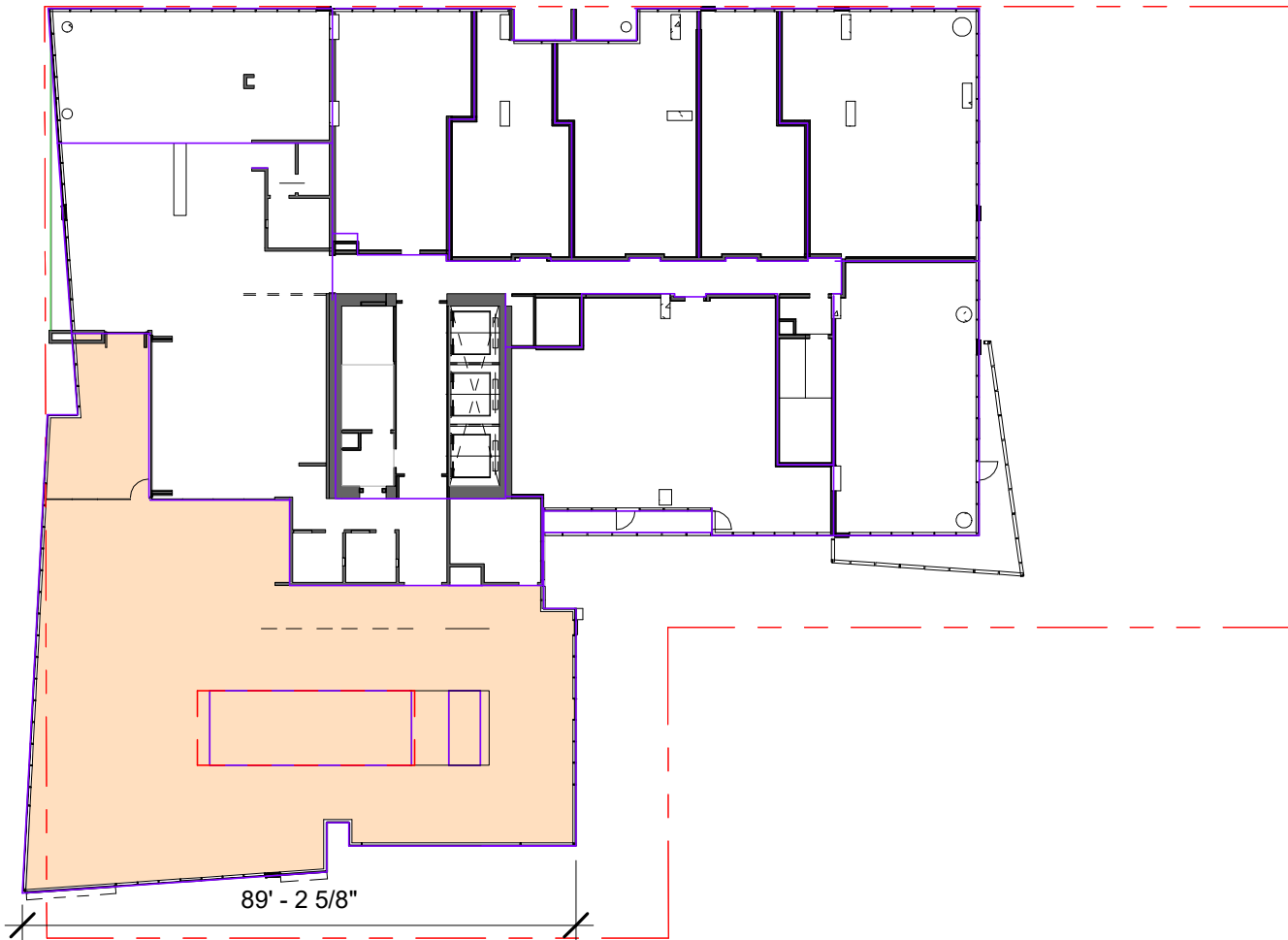
Level Breakdown  
Level 21

- Residential - 16,308 sf
  - Residential Balcony (first 100sf not included in FAR) - 41sf
- Total - 16,349 sf



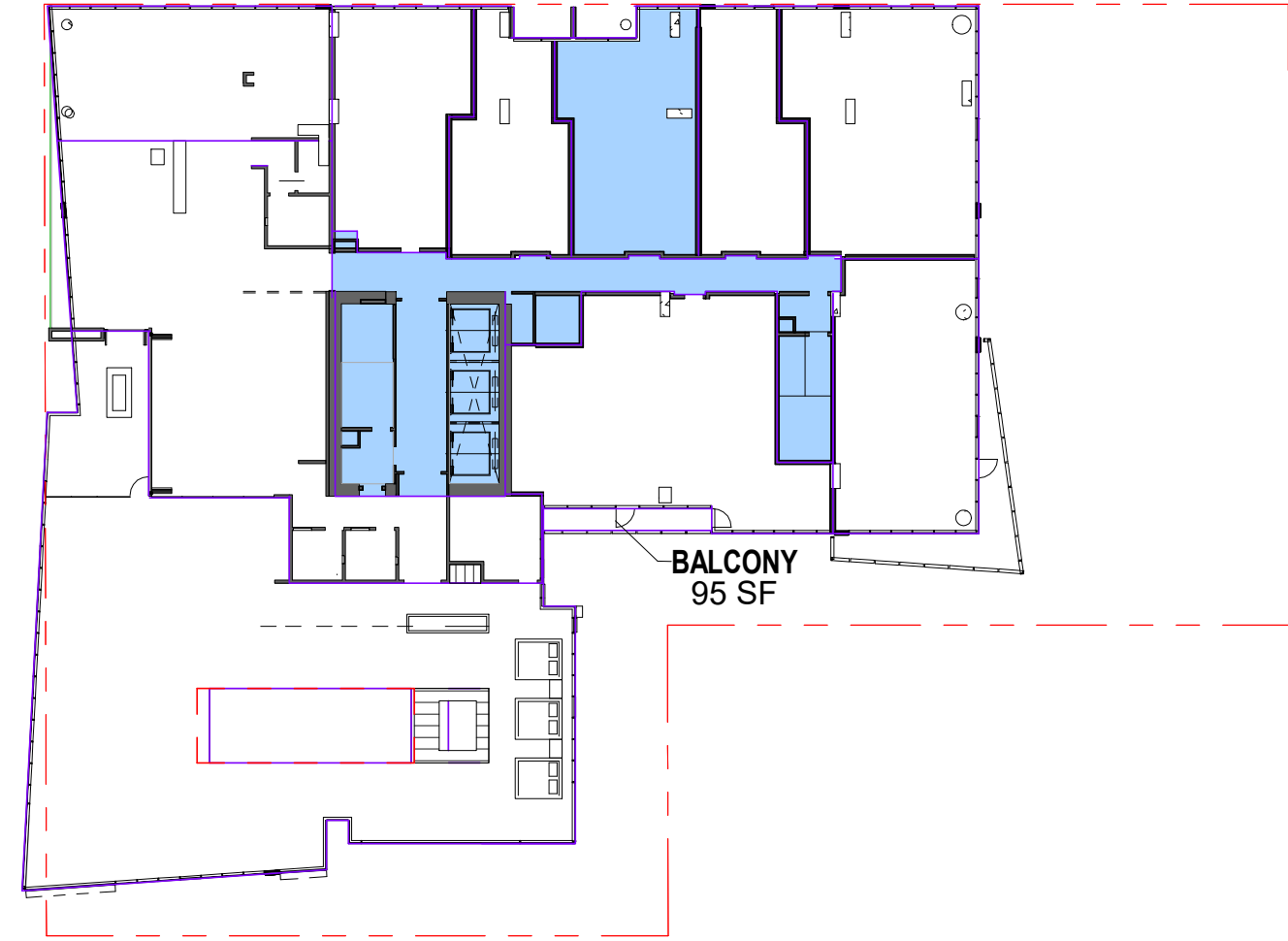
Level Breakdown  
Level 22 - Exterior Common Area

- Open Area - 4,250 sf
- Total - 4,250 sf



Level Breakdown  
Level 22

- Residential - 8,447 sf
  - Amenities - 3,639 sf
- Total - 12,086 sf



9G  
PLANNED DEVELOPMENT PERMIT  
PACKAGE

3rd SUBMITTAL  
6.9.20

ISSUES:

PROJECT NO:  
5937.00  
FILE NAME:  
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TITLE:

FAR Bonus  
Incentives and  
Exterior  
Common Areas



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architecture + environments + brand strategy + graphics  
185 W F St suite 500, San Diego, CA 92101  
phone 619.239.2353



**DOWNTOWN  
FINAL ENVIRONMENTAL IMPACT REPORT (Downtown FEIR)  
CONSISTENCY EVALUATION  
FOR THE  
9G TOWER PROJECT  
PTS# 649856**

**JULY 2020**

Prepared by: City of San Diego  
Development Services Department  
1222 First Avenue  
San Diego, CA 92101



## **DOWNTOWN FEIR CONSISTENCY EVALUATION**

**1. PROJECT TITLE:** 9G Tower ("Project")

**2. DEVELOPER:** Cisterra

**3. PROJECT LOCATION AND SETTING:** The Downtown Community Planning (DCP) area includes approximately 1,500 acres within the metropolitan core of the City of San Diego, bounded by Laurel Street and Interstate 5 on the north; Interstate 5, Commercial Street, 16th Street, Sigsbee Street, Newton Avenue, Harbor Drive, and the extension of Beardsley Street on the east and southeast; and San Diego Bay on the south and west and southwest. The major north-south access routes to downtown are Interstate 5, State Route 163, and Pacific Highway. The major east-west access route to downtown is State Route 94. Surrounding areas include the community of Uptown and Balboa Park to the north, Golden Hill and Sherman Heights to the east, Barrio Logan and Logan Heights to the South and the City of Coronado to the west across San Diego Bay.

The proposed redevelopment is located at a 0.57 acre site at 659 9th Avenue in the Employment/Residential Mixed-Use land use district of the Centre City Planned District Ordinance (CCPDO) and the East Village neighborhood of the DCP area.

**4. PROJECT DESCRIPTION:** Process Four Site Development Permit, Neighborhood Development Permit, Tentative Map, and Conditional Use Permit for the renovation of an existing historic building and construction of a new 432,175 square foot mixed-use development. The project is a 22-story high-rise consisting of 241 dwelling units, commercial retail at the basement level and ground floor, and 5 levels of above ground parking for 273 vehicles. The project would also include the installation of connections to existing utility lines and streetscape improvements to the surrounding property.

**5. CEQA COMPLIANCE:** The DCP, CCPDO, Redevelopment Plan for the Centre City Redevelopment Project and related activities have been addressed by the following environmental documents, which were prepared prior to this Consistency Evaluation and are hereby incorporated by reference:

FEIR for the DCP, CCPDO, and 10<sup>th</sup> Amendment to the Redevelopment Plan for the Centre City Project (State Clearinghouse Number 2003041001, certified by the Redevelopment Agency (Resolution No. R-04001) and the San Diego City Council (City Council) (Resolution No. R-301265), with date of final passage on March 14, 2006.

Addendum to the Downtown FEIR for the 11<sup>th</sup> Amendment to the Redevelopment Plan for the Centre City Redevelopment Project, Amendments to the DCP, CCPDO, Marina Planned District Ordinance, and Mitigation, Monitoring and Reporting Program of the Downtown FEIR for the DCP, CCPDO, and the Redevelopment Plan for the Centre City Redevelopment Project certified by the Redevelopment Agency (Resolution No. R-04193) and by the City Council (Resolution No. R-302932), with date of final passage on July 31, 2007.

Second Addendum to the Downtown FEIR for the proposed amendments to the DCP, CCPDO, Marina Planned District Ordinance, and Mitigation, Monitoring and Reporting



Program (MMRP) certified by the Redevelopment Agency (Resolution No. R-04508), with date of final passage on April 21, 2010.

Third Addendum to the Downtown FEIR for the RE District Amendments to the CCPDO certified by the Redevelopment Agency (Resolution No. R-04510), with date of final passage on April 21, 2010.

Fourth Addendum to the Downtown FEIR for the San Diego Civic Center Complex Project certified by the Redevelopment Agency (Resolution No. R-04544) with date of final passage on August 3, 2010.

Fifth Addendum to the Downtown FEIR for the Industrial Buffer Overlay Zone Amendments to the CCPDO certified by the City Council (Resolution No. R-308724) with a date of final passage on February 12, 2014.

Sixth Addendum to the Downtown FEIR for the India and Date Project certified by the City Council (Resolution No. R-309115) with a date of final passage on July 14, 2014.

The Final Supplemental Environmental Impact Report for the Downtown San Diego Mobility Plan certified by the City Council on June 21, 2016 (Resolution R-310561).

The City of San Diego FEIR for the Climate Action Plan (“CAP FEIR”) certified by the City Council on December 15, 2015, (City Council Resolution R-310175) which includes the Addendum to the CAP FEIR certified by the City Council on July 12, 2016.

The Downtown FEIR and the CAP FEIR are “Program EIRs” prepared in compliance with California Environmental Quality Act (CEQA) Guidelines Section 15168. The aforementioned environmental documents are the most recent and comprehensive environmental documents pertaining to the proposed Project. The Downtown FEIR and subsequent addenda are available for review at the City of San Diego, Development Services Department located at 1222 First Avenue, San Diego, CA 92101. The CAP FEIR is available at the offices of the City of San Diego Planning Department located at 1010 Second Avenue, Suite 1200, San Diego, CA 92101.

This Downtown FEIR Consistency Evaluation (“Evaluation”) has been prepared for the Project in compliance with State CEQA and Local Guidelines. Under these Guidelines, environmental review for subsequent proposed actions is accomplished using the Evaluation process, as allowed by Sections 15168 and 15180 of the State CEQA Guidelines. The Evaluation includes the evaluation criteria as defined in Section 15063 of the State CEQA Guidelines.

Under this process, an Evaluation is prepared for each subsequent proposed action to determine whether the potential impacts were anticipated in the Downtown FEIR and the CAP FEIR. No additional documentation is required for subsequent proposed actions if the Evaluation determines that the potential impacts have been adequately addressed in the CAP FEIR and the Downtown FEIR and subsequent proposed actions implement appropriate mitigation measures identified in the MMRP that accompanies the FEIR.

If the Evaluation identifies new impacts or a substantial change in circumstances, additional environmental documentation is required. The form of this documentation depends upon the nature of the impacts of the subsequent proposed action being proposed. Should a proposed



action result in: a) new or substantially more severe significant impacts that are not adequately addressed in the Downtown FEIR or CAP FEIR, or b) there is a substantial change in circumstances that would require major revision to the Downtown FEIR or the CAP FEIR, or c) that any mitigation measures or alternatives previously found not to be feasible or not previously considered would substantially reduce or lessen any significant effects of the Project on the environment, a Subsequent or Supplemental Environmental Impact Report (EIR) would be prepared in accordance with Sections 15162 or 15163 of the State CEQA Guidelines (CEQA Statutes Section 21166).

If the lead agency under CEQA finds that pursuant to Sections 15162 and 15163, no new significant impacts will occur or no new mitigation will be required, the lead agency can approve the subsequent proposed action to be within the scope of the Project covered by the Downtown FEIR and CAP FEIR, and no new environmental document is required.

**6. PROJECT-SPECIFIC ENVIRONMENTAL ANALYSIS:** See attached Environmental Checklist and Section 10 *Evaluation of Environmental Impacts*.

**7. MITIGATION, MONITORING AND REPORTING PROGRAM:** As described in the Environmental Checklist and summarized in **Attachment A**, the following mitigation measures included in the MMRP, found in Volume 1.B.2 of the Downtown FEIR, will be implemented by the proposed Project:

**AQ-B.1-1; HIST-A.1-1; HIST-A.1-2; HIST-B.1-1; PAL-A.1-1; NOI-B.1-1; NOI-C.1-1; TRF-A.1.1-2**

**8. DETERMINATION:** In accordance with Sections 15168 and 15180 of the CEQA Guidelines, the potential impacts associated with future development within the DCP area are addressed in the Downtown FEIR prepared for the DCP, CCPDO, and the six subsequent addenda to the Downtown FEIR listed in Section 6 above, as well as the Final Supplemental EIR for the Downtown San Diego Mobility Plan and the CAP FEIR. These documents address the potential environmental effects of future development within the Centre City Redevelopment Project based on build out forecasts projected from the land use designations, density bonus, and other policies and regulations governing development intensity and density. Based on this analysis, the Downtown FEIR and its subsequent addenda and the CAP FEIR, as listed in Section 6 above, concluded that development would result in significant impacts related to the following issues (mitigation and type of impact shown in parentheses):

**Significant but Mitigated Impacts**

- Air Quality: Construction Emissions (AQ-B.1) (D)
- Paleontology: Impacts to Significant Paleontological Resources (PAL-A.1) (D/C)
- Noise: Interior Traffic Level Increase on Grid Streets (NOI-B.1) (D)

**Significant and Not Mitigated Impacts**

- Air Quality: Mobile Source Emissions (AQ-A.1) (C)
- Historical Resources: Archeological and Built Environment (HIST-B.1) (D/C) and HIST-A.1-1 and HIST-A.1-2
- Water Quality: Urban Runoff (WQ-A.1) (C)



- Land Use: Physical Changes Related to Transient Activity (LU-B.6) (C)
- Noise: Exterior Traffic Level Increase on Grid Streets (NOI-A.1) (C)
- Traffic: Impact on Surrounding Streets (TRF-A.1) (C)
- Traffic: Impact on Freeway Ramps and Segments (TRF-A.2) (C)

In certifying the Downtown FEIR and approving the DCP, CCPDO, and 10th Amendment to the Redevelopment Plan, the City Council and Redevelopment Agency adopted a Statement of Overriding Considerations which determined that the unmitigated impacts were acceptable in light of economic, legal, social, technological or other factors including the following.

### **Overriding Considerations**

1. Develop downtown as the primary urban center for the region
2. Maximize employment opportunities within the downtown area
3. Develop full-service, walkable neighborhoods linked to the assets downtown offers
4. Increase and improve parks and public spaces
5. Relieve growth pressure on outlying communities
6. Maximize the advantages of downtown's climate and waterfront setting
7. Implement a coordinated, efficient system of vehicular, transit, bicycle, and pedestrian traffic
8. Integrate historical resources into the new downtown plan
9. Facilitate and improve the development of business and economic opportunities located in the downtown area
10. Integrate health and human services into neighborhoods within downtown
11. Encourage a regular process of review to ensure that the Plan and related activities are best meeting the vision and goals of the Plan.

The proposed activity detailed and analyzed in this Evaluation are adequately addressed in the environmental documents noted above and there is no change in circumstance, substantial additional information, or substantial Project changes to warrant additional environmental review. Because the prior environmental documents adequately covered this activity as part of the previously approved Project, this activity is not a separate Project for purposes of review under CEQA pursuant to CEQA Guidelines Sections 15060(c)(3), 15180, and 15378(c).

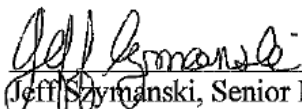
**SUMMARY OF FINDINGS:** In accordance with Public Resources Code Sections 21166, 21083.3, and CEQA Guidelines Sections 15168 and 15183, the following findings are derived from the environmental review documented by this Evaluation and the Downtown FEIR and CAP FEIR as amended:

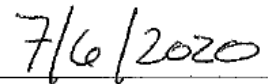
1. No substantial changes are proposed in the Centre City Redevelopment Project, or with respect to the circumstances under which the Centre City Redevelopment Project is to be undertaken as a result of the development of the proposed Project, which will require important or major revisions in the Downtown FEIR and the six subsequent addenda to the FEIR or with the CAP FEIR;
2. No new information of substantial importance to the Centre City Redevelopment Project has become available that shows the Project will have any significant effects not discussed previously in the Downtown FEIR or subsequent addenda to the Downtown FEIR or CAP FEIR; or that any significant effects previously examined will be substantially more severe than shown in the CAP FEIR and the Downtown



FEIR or subsequent addenda to the FEIR; or that any mitigation measures or alternatives previously found not to be feasible or not previously considered would substantially reduce or lessen any significant effects of the Project on the environment;

3. No Negative Declaration, Subsequent EIR, or Supplement or Addendum to the CAP EIR and the Downtown FEIR, as amended, is necessary or required;
4. The proposed actions will have no significant effect on the environment, except as identified and considered in the CAP FEIR and the Downtown FEIR and subsequent addenda to the Downtown FEIR for the Centre City Redevelopment Project. No new or additional project-specific mitigation measures are required for this Project; and
5. The proposed actions would not have any new effects that were not adequately covered in the CAP FEIR and Downtown FEIR or addenda to the Downtown FEIR, and therefore, the proposed Project is within the scope of the program approved under the CAP FEIR and Downtown FEIR and subsequent addenda listed in Section 6 above.

  
Jeff Szymanski, Senior Planner, City of San Diego  
Lead Agency Representative/Preparer

  
Date



## ENVIRONMENTAL CHECKLIST

### 10. EVALUATION OF ENVIRONMENTAL IMPACTS

This environmental checklist evaluates the potential environmental effects of the proposed Project consistent with the significance thresholds and analysis methods contained in the CAP FEIR and the Downtown FEIR for the DCP, CCPDO, and Redevelopment Plan for the Centre City Project Area. Based on the assumption that the proposed activity is adequately addressed in the Downtown FEIR and CAP FEIR, the following table indicates how the impacts of the proposed activity relate to the conclusions of the Downtown FEIR and CAP FEIR. As a result, the impacts are classified into one of the following categories:

- Significant and Not Mitigated (SNM)
- Significant but Mitigated (SM)
- Not Significant (NS)

The checklist identifies each potential environmental effect and provides information supporting the conclusion drawn as to the degree of impact associated with the proposed Project. As applicable, mitigation measures from the Downtown FEIR and CAP FEIR are identified and are summarized in **Attachment A** to this Evaluation. Some of the mitigation measures are plan-wide and not within the control of the proposed Project. Other measures, however, are to be specifically implemented by the proposed Project. Consistent with the Downtown FEIR and CAP FEIR analysis, the following issue areas have been identified as Significant and Not Mitigated even with inclusion of the proposed mitigation measures, where feasible:

- Air Quality: Mobile Source Emissions (AQ-A.1) (C)
- Historical Resources: Archeological (HIST-B.1) (D/C)
- Water Quality: Urban Runoff (WQ-A.1) (C)
- Land Use: Physical Changes Related to Transient Activity (LU-B.6) (C)
- Noise: Exterior Traffic Level Increase on Grid Streets (NOI-A.1) (C)
- Traffic: Impact on Surrounding Streets (TRF-A.1) (C)
- Traffic: Impact on Freeway Ramps and Segments (TRF-A.2) (C).

The following Overriding Considerations apply directly to the proposed Project:

1. Develop downtown as the primary urban center for the region
2. Maximize employment opportunities within the downtown area
3. Develop full-service, walkable neighborhoods linked to the assets downtown offers
4. Relieve growth pressure on outlying communities
5. Implement a coordinated, efficient system of vehicular, transit, bicycle, and pedestrian traffic
6. Facilitate and improve the development of business and economic opportunities located in the downtown area



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
<b>1. AESTHETICS/VISUAL QUALITY:</b>						
<p>(a) <i>Substantially disturb a scenic resource, vista or view from a public viewing area, including a State scenic highway or view corridor designated by the DCP?</i></p> <p>Views of scenic resources including San Diego Bay, San Diego-Coronado Bay Bridge, Point Loma, Coronado, Petco Park, and the downtown skyline are afforded by the public viewing areas within and around the downtown and along view corridor streets within the planning area.</p> <p>The CCPDO includes several requirements that reduce a project's impact on scenic vistas. These include view corridor setbacks on specific streets to maintain views and controls building bulk by setting limits on minimum tower spacing, street wall design, maximum lot coverage, and building dimensions.</p> <p>The site does not possess any significant scenic resources that could be impacted by the proposed Project therefore impacts to on-site scenic resources are not significant. Impacts associated with scenic vistas would be similar to the DCP FEIR and would not be significant.</p>					X	X
<p>(b) <i>Substantially incompatible with the bulk, scale, color and/or design of surrounding development?</i></p> <p>The bulk, scale, and design of the Project would be compatible with existing development in the area. Development of the site would improve the area by providing a new, modern building on a currently underutilized site. The Project would utilize high quality materials and contemporary design sensitive to the character of the surrounding neighborhood. Additionally, a variety of existing and planning low, mid, and high-rise buildings are located within the</p>					X	X



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
<p>vicinity of the Project site and the scale of the proposed Project would be consistent with that of surrounding buildings. The project would also, incorporate the historically designated Pacific Telephone and Telegraph Co. Garage building into the design of the project.</p> <p>Therefore, project-level and cumulative impacts associated with this issue would not occur.</p>						
<p><i>(c) Substantially affect daytime or nighttime views in the area due to lighting?</i></p> <p>The proposed project would not involve a substantial amount of exterior lighting or include materials that would generate substantial glare. Furthermore, outdoor lighting that would be incorporated into the proposed project would be shielded or directed away so that direct light or glare does not adversely impact adjacent land uses. The City's Light Pollution Law (San Diego Municipal Code (SDMC) Section 101.1300 et seq.) also protects nighttime views (e.g., astronomical activities) and light-sensitive land uses from excessive light generated by development in the downtown area. The proposed project's conformance with these requirements would ensure that direct and cumulative impacts associated with this issue are not significant.</p>					<b>X</b>	<b>X</b>
<b>2. AGRICULTURAL RESOURCES:</b>						
<p><i>(a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland) to non-agricultural use?</i></p> <p>The DCP Area is an urban downtown environment that does not contain land designated as prime agricultural soil by the Soils Conservation Service. In</p>					<b>X</b>	<b>X</b>



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
addition, it does not contain prime farmland designated by the California Department of Conservation. Therefore, no impact to agricultural resources would occur.						
<p><i>(b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?</i></p> <p>The DCP Area does not contain, nor is it near, land zoned for agricultural use or land subject to a Williamson Act Contract pursuant to Section 512101 of the California Government Code. Therefore, impacts resulting from conflicts with existing zoning for agricultural use or a Williamson Act Contract would not occur.</p>					<b>X</b>	<b>X</b>



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
<b>3. AIR QUALITY:</b>						
<p>(a) <i>Conflict with or obstruct implementation of an applicable air quality plan, including the County's Regional Air Quality Strategies (RFS) or the State Implementation Plan?</i></p> <p>The proposed Project site is located within the San Diego Air Basin, which is under the jurisdiction of the San Diego Air Pollution Control District (SDAPCD). The San Diego Air Basin is designated by state and federal air quality standards as nonattainment for ozone and particulate matter (PM) less than 10 microns (PM10) and less than 2.5 microns (PM 2.5) in equivalent diameter. The SDAPCD has developed a Regional Air Quality Strategy (RAQS) to attain the state air quality standards for ozone. The proposed Project is consistent with the land use and transit-supportive policies and regulations of the DCP and CCPDO; which are in accordance with those of the RAQS. Therefore, the proposed Project would not conflict with, but would help implement, the RAQS with its compact, high intensity land use and transit-supportive design. Therefore, no impact to the applicable air quality plan would occur.</p>					X	X
<p>(b) <i>Expose sensitive receptors to substantial air contaminants including, but not limited to, criteria pollutants, smoke, soot, grime, toxic fumes and substances, particulate matter, or any other emissions that may endanger human health?</i></p> <p>The Project could involve the exposure of sensitive receptors to substantial air contaminants during short-term construction activities and over the long-term operation of the Project. Construction activities associated with the Project could result in potentially significant impacts related to the exposure of sensitive receptors to substantial emissions of particulate matter. The potential for impacts to sensitive receptors</p>			X			X



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
<p>during construction activities would be mitigated to below a level of significance through compliance with the City’s mandatory standard dust control measures and the dust control and construction equipment emission reduction measures required by <b>FEIR Mitigation Measure AQ-B.1-1</b> (Attachment A).</p> <p>The Project could also involve the exposure of sensitive receptors to air contaminants over the long-term operation of the Project, such as carbon monoxide exposure (commonly referred to as CO “hot spots”) due to traffic congestion near the Project site. However, the FEIR concludes that development within the DCP Area would not expose sensitive receptors to significant levels of any of the substantial air contaminants. Since the land use designation of the proposed development does not differ from the land use designation assumed in the FEIR analysis, the Project would not expose sensitive receptors to substantial air contaminants beyond the levels assumed in the FEIR. Additionally, the Project is not located close enough to any industrial activities to be impacted by any emissions potentially associated with such activities. Therefore, impacts associated with this issue would not be significant. Project impacts associated with the generation of substantial air contaminants are discussed below in Section 3.c.</p>						
<p>(c) <i>Generate substantial air contaminants including, but not limited to, criteria pollutants, smoke, soot, grime, toxic fumes and substances, particulate matter, or any other emissions that may endanger human health?</i></p> <p>Implementation of the Project could result in potentially adverse air quality impacts related to the following air emission generators: construction and mobile-sources. Site preparation activities and construction of the Project would involve short-term, potentially adverse impacts associated with the creation of dust and the generation of</p>		<b>X</b>	<b>X</b>			



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
<p>construction equipment emissions. The clearing, grading, excavation, and other construction activities associated with the Project would result in dust and equipment emissions that, when considered together, could endanger human health. Implementation of <b>Downtown FEIR Mitigation Measure AQ-B.1-1</b> (see Attachment A) would reduce dust and construction equipment emissions generated during construction of the Project to a level below significance.</p> <p>The air emissions generated by automobile trips associated with the Project would not exceed air quality significance standards established by the San Diego Air Pollution Control District. However, the Project's mobile source emissions, in combination with dust generated during the construction of the Project, would contribute to the significant and unmitigated cumulative impact to air quality identified in the Downtown FEIR. No uses are proposed that would significantly increase stationary-source emissions in Downtown; therefore, impacts from stationary sources would be not significant.</p>						
<b>4. BIOLOGICAL RESOURCES:</b>						
<p>(a) <i>Substantially effect, either directly or through habitat modifications, any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by local, state or federal agencies?</i></p> <p>Due to the highly urbanized nature of the DCP Area, there are no sensitive plants or animal species, habitats, or wildlife migration corridors. In addition, the ornamental trees and landscaping included in the Project are considered of no significant value to the native wildlife in their proposed location. Therefore, no impact associated with this issue could occur.</p>					X	X



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
<p><i>(b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, and regulations by local, state or federal agencies?</i></p> <p>As identified in the FEIR, the DCP Area is not within a sub-region of the San Diego County Multiple Species Conservation Program (MSCP). Therefore, impacts associated with substantial adverse effects on riparian habitat or other sensitive natural communities identified in local or regional plans, policies, and regulations by local, state or federal agencies would not occur.</p>					<b>X</b>	<b>X</b>
<b>5. GEOLOGY AND SOILS:</b>						
<p><i>(a) Substantial health and safety risk associated with seismic or geologic hazards?</i></p> <p>The site is not located within a currently established State of California Earthquake Fault Zone but is within the Downtown Special Fault Zone.</p> <p>Nova Services Inc. prepared a Geotechnical Investigation (September 2019) and a Fault Investigation (April 2020) for the Project. It states that based on the results of the investigation, Nova Services finds that the Project site is not transected by active faults and the risk of surface rupture is considered to be very low and that the site can be developed as proposed provided the recommendations of the investigation are followed and implemented during design and construction.</p> <p>Although the potential for geologic hazards (landslides, liquefaction, slope failure, and seismically-induced settlement) is considered low due to the site's moderate to non-expansive geologic structure, such hazards could nevertheless occur. Conformance with, and implementation of, all</p>					<b>X</b>	<b>X</b>



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
seismic-safety development requirements, including all applicable requirements of the Alquist-Priolo Zone Act, the seismic design requirements of the International Building Code (IBC), the City of San Diego Notification of Geologic Hazard procedures, and all other applicable requirements would ensure that the potential impacts associated with seismic and geologic hazards are not significant.						
<b>6. GREENHOUSE GAS EMISSIONS:</b>						
<p>(a) <i>Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?</i></p> <p>The DCP provides for the growth and buildout of Downtown. The City's CAP FEIR analyzed greenhouse gas ("GHG") emissions on a citywide basis – inclusive of the anticipated assumptions for the growth and buildout of Downtown. The City's CAP outlines measures that would support substantial progress towards the City's 2035 GHG emissions reduction targets, which are intended to keep the City in-line to achieve its share of 2050 GHG reductions.</p> <p>The CAP Consistency Checklist was adopted to uniformly implement the CAP for project-specific analyses of GHG emission impacts. The Project has been analyzed against the CAP Consistency Checklist and based on this analysis, it has been determined that the Project would be consistent with the CAP and would not contribute to cumulative GHG emissions that would be inconsistent with the CAP. As such, the Project would be consistent with the anticipated growth and buildout assumptions of both the DCP and the CAP. Therefore, this impact is considered not significant.</p>					<b>X</b>	<b>X</b>



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
<p><i>(b) Conflict with any applicable plan, policy, or regulation of an agency adopted for the purpose of reducing the emissions of greenhouse gas?</i></p> <p>As stated above in Section 6.a., construction and operation of the proposed Project would not result in a significant impact related to GHG emissions on the environment. The Project is consistent with the City's CAP and growth assumptions under the DCP as stated in Section 6.a. Additionally, the Project would be consistent with the recommendations within Policy CE-A.2 of the City of San Diego's General Plan Conservation Element. Therefore, the Project does not conflict with any applicable plan, policy, or regulation adopted for the purpose of reducing the emissions of greenhouse gases. This impact is considered not significant.</p>					<b>X</b>	<b>X</b>
<b>7. HAZARDS AND HAZARDOUS MATERIALS:</b>						
<p><i>(a) Substantial health and safety risk related to onsite hazardous materials?</i></p> <p>The Downtown FEIR states that contact with, or exposure to, hazardous building materials, soil and ground water contaminated with hazardous materials, or other hazardous materials could adversely affect human health and safety during short-term construction or long term operation of a development. The Project is subject to federal, state, and local agency regulations for the handling of hazardous building materials and waste. Compliance with all applicable requirements of the County of San Diego Department of Environmental Health and federal, state, and local regulations for the handling of hazardous building materials and waste would ensure that potential health and safety impacts caused by exposure to on-site hazardous materials are not significant during short term, construction activities. In addition, herbicides and fertilizers associated with</p>					<b>X</b>	<b>X</b>



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
the landscaping of the Project could pose a significant health risk over the long term operation of the Project. However, the Project's adherence to existing mandatory federal, state, and local regulations controlling these materials would ensure that long-term health and safety impacts associated with on-site hazardous materials over the long term operation of the Project are not significant.						
<p><i>(b) Be located on or within 2,000 feet of a site that is included on a list of hazardous materials sites compiled pursuant to Government Code §65962.5 and, as a result, would it create a significant hazard to the public or the environment?</i></p> <p>The Project is not located on or within 2,000 feet of a site on the State of California Hazardous Waste and Substances Sites List; however, there are sites within 2,000 feet of the Project site that are listed on the County of San Diego's Site Assessment Mitigation (SAM) Case Listing. The Downtown FEIR states that significant impacts to human health and the environment regarding hazardous waste sites would be avoided through compliance with mandatory federal, state, and local regulations as described in Section 7.a above. Therefore, the Downtown FEIR states that no mitigation measures would be required.</p>					<b>X</b>	<b>X</b>
<p><i>(c) Substantial safety risk to operations at San Diego International Airport?</i></p> <p>According to the Airport Land Use Compatibility Plan (ALUCP) for San Diego International Airport (SDIA), the entire DCP area is located within the SDIA Airport Influence Area. The Downtown FEIR identifies policies that regulate development within areas affected by Lindbergh Field including building heights, use and intensity limitations, and noise sensitive uses. The Project does not exceed the intensity of development</p>					<b>X</b>	<b>X</b>



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
assumed under the DCP, nor does it include components that would in any way violate or impede adherence to the policies of the ALUCP nor involve impacts related to the creation of substantial safety risks. The Federal Aviation Administration (FAA) issued a determination that the Project does not exceed obstruction standards and would not be a hazard to air navigation on 9/12/19. As a condition of this determination, the structure is to be marked/lighted in accordance with FAA standards. On 2/19/20 the Airport Land Use Commission determined that the Project is conditionally consistency with the ALUCP, provided compliance with the aforementioned conditions of the FAA. Therefore, there are no potential direct or cumulative impacts related to this issue.						
<p><i>(d) Substantially impair implementation of an adopted emergency response plan or emergency evacuation plan?</i></p> <p>The Project does not propose any features that would affect an emergency response or evacuation plan. Therefore, no impact associated with this issue is anticipated.</p>					X	X
<b>8. HISTORICAL RESOURCES:</b>						
<p><i>(a) Substantially impact a significant historical resource, as defined in § 15064.5?</i></p> <p>The project site located at 660 10th Avenue is a designated historic resource and is listed as HRB Site #1355, The Pacific Telephone and Telegraph Co. Garage. The garage is a one story commercial building that was constructed in the Art Deco Style. Because of the proposed alterations to the historic structure a Treatment Plan (Union Architects, December 2019) was submitted to City of San Diego Plan Historic staff for review. After some revisions to the plan it was approved. The Project proposes to retain portions of</p>			X	X		



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
<p>the historic resource on-site and incorporate them into the design of the new construction, resulting in a substantial alteration of the historic resource.</p> <p>The restoration is consistent with DCP Goals 9.1-G-1, “Protect historical resources to communicate Downtown’s heritage,” 9.1-G-2, “Encourage the rehabilitation and reuse of historical resources,” and 9.2-G-1, “Integrate historical resources into the Downtown fabric while achieving policies for significant development and population intensification,” and with DCP Policies 9.2-P-1, “Incorporate elements of historical buildings in new projects to impart heritage,” and 9.2-P-3, “Promote the adaptive reuse of intact buildings (designated or not) and/or significant elements, as a cultural and suitability goal.”</p> <p>DCP FEIR <b>Mitigation Measure HIST-A.1-1</b> requires compliance with Chapter 14, Article 3, Division 2 of the SDMC, which regulates historic resources. <b>Mitigation Measure HIST-A.1-2</b> requires the applicant to submit a Treatment Plan for retained historic resources for review and approval. Implementation of SDMC §143.0201 et seq., as required by Mitigation Measures HIST-A.1-1 and HIST-A.1-2 (Attachment A), will further ensure that the inclusion of the Pacific Telephone and Telegraph Co. Garage will not significantly impact the historic resource.</p> <p>The Project would include substantial alterations to the Pacific Telephone and Telegraph Co. Garage that may be approved through the SDP review and approved by the City in conformance with the City’s Historical Resources Regulations as provided in <b>Mitigation Measure HIST-A.1-1 and HIST-A.1-2.</b></p>						



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
<p><i>(b) Substantially impact a significant archaeological resource pursuant to § 15064.5, including the disturbance of human remains interred outside of formal cemeteries?</i></p> <p>According to the Downtown FEIR, the likelihood of encountering archaeological resources is greatest for projects that include grading and/or excavation of areas on which past grading and/or excavation activities have been minimal (e.g., surface parking lots). Since archaeological resources have been found within inches of the ground surface in Downtown, even minimal grading activities can impact these resources. In addition, the likelihood of encountering subsurface human remains during construction and excavation activities, although considered low, is possible. Thus, the excavation, demolition, and surface clearance activities associated with development of the Project and the level of subterranean parking could have potentially adverse impacts to archaeological resources, including buried human remains. Implementation of <b>Downtown FEIR Mitigation Measure HIST-B.1-1</b>, (see Attachment A) would minimize, but not fully mitigate, these potential impacts. Since the potential for archaeological resources and human remains on the Project site cannot be confirmed until grading is conducted, the exact nature and extent of impacts associated with the proposed Project cannot be predicted. Consequently, the required mitigation may or may not be sufficient to reduce these direct project-level impacts to below a level of significance. Therefore, project-level impacts associated with this issue remain potentially significant and not fully mitigated, consistent with the analysis of the Downtown FEIR. Furthermore, project-level significant impacts to important archaeological resources would contribute to the potentially significant and unmitigated cumulative impacts</p>	<b>X</b>	<b>X</b>				



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
identified in the Downtown FEIR.						
<p><i>(c) Substantially impact a unique paleontological resource or site or unique geologic feature?</i></p> <p>The Project site is underlain by the San Diego Formation and Bay Point Formation, which has high paleontological resource potential. The Downtown FEIR concludes that development would have potentially adverse impacts to paleontological resources if grading and/or excavation activities are conducted beyond a depth of 1-3 feet. The Project's proposal for a basement level would involve excavation beyond the FEIR standard, resulting in potentially significant impacts to paleontological resources. Implementation of <b>Downtown FEIR Mitigation Measure PAL-A.1-1</b> (see Attachment A) would ensure that the Project's potentially direct impacts to paleontological resources are not significant. Furthermore, the Project would not impact any resources outside of the Project site. The mitigation measures for direct impacts fully mitigate for paleontological impacts, therefore, the Project's contribution to cumulative impacts to paleontological resources would be significant but mitigated because the same measures that mitigate direct impacts would also mitigate for any cumulative impacts.</p>			<b>X</b>	<b>X</b>		
<b>9. HYDROLOGY AND WATER QUALITY:</b>						
<p><i>(a) Substantially degrade groundwater or surface water quality?</i></p> <p>The Project's construction and grading activities may involve soil excavation at a depth that could surpass known groundwater levels, which would indicate that groundwater dewatering might be required. Compliance with the requirements of either (1) the San Diego Regional Water Quality Control Board under a National Pollution Discharge Elimination</p>		<b>X</b>			<b>X</b>	



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
<p>system general permit for construction dewatering (if dewatering is discharged to surface waters), or (2) the City of San Diego Metropolitan Wastewater Department (if dewatering is discharged into the City's sanitary sewer system under the Industrial Waste Pretreatment Program), and (3) the mandatory requirements controlling the treatment and disposal of contaminated dewatered groundwater would ensure that potential impacts associated with construction dewatering and the handling of contaminated groundwater are not significant. In addition, Best Management Practices (BMPs) required as part of the local Storm Water Pollution Prevention Plan (SWPPP) would ensure that short-term water quality impacts during construction are not significant. The proposed Project would result in hard structure areas and other impervious surfaces that would generate urban runoff with the potential to degrade groundwater or surface water quality. However, implementation of BMPs required by the local Standard Urban Storm water Mitigation Program (SUSMP) and Storm water Standards would reduce the Project's long-term impacts. Thus, adherence to the state and local water quality controls would ensure that direct impacts to groundwater and surface water quality would not be significant.</p> <p>Despite not resulting in direct impacts to water quality, the Downtown FEIR found that the urban runoff generated by the cumulative development in the downtown would contribute to the existing significant cumulative impact to the water quality of San Diego Bay. No mitigation other than adherence to existing regulations has been identified in the Downtown FEIR to feasibly reduce this cumulative impact to below a level of significance. Consistent with the Downtown FEIR, the Project's contribution to the cumulative water quality impact would remain significant and</p>						



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
unmitigated.						
<p><i>(b) Substantially increase impervious surfaces and associated runoff flow rates or volumes?</i></p> <p>The project site is currently developed and covered with impervious surfaces. Implementation of the Project would not substantially increase the runoff volume entering the storm drain system. The Downtown FEIR found that implementation of the DCP would not result in a substantial increase in impervious surfaces within Downtown because the area is a highly urbanized area paved with pervious surfaces and very little vacant land (approximately 3 percent of the planning area). Redevelopment of downtown is therefore anticipated to replace impervious surfaces that already exist and development of the small number of undeveloped sites would not result in a substantial increase in impermeable surface area or a significant impact on the existing storm drain system. The Project is also required to comply with the City of San Diego BMPs required as part of the local SWPPP. The Project incorporates a variety of pervious surfaces (such as landscape areas and open spaces), as well as features designed to utilize storm water. Implementation of these features is encouraged by the DCP as they capture rain water and reduce surface volume entering the storm drain system. Therefore, impacts associated within this issue are not significant. (Impacts associated with the quality of urban runoff are analyzed in Section 9a.)</p>					<b>X</b>	<b>X</b>
<p><i>(c) Substantially impede or redirect flows within a 100-year flood hazard area?</i></p> <p>The Project site is not located within a 100-year floodplain. Similarly, the Project would not affect off-site flood hazard areas, as no 100-year floodplains are</p>					<b>X</b>	<b>X</b>



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
located downstream. Therefore, impacts associated with these issues are not significant.						
<p><i>(d) Substantially increase erosion and sedimentation?</i></p> <p>The potential for erosion and sedimentation could increase during the short-term during site preparation and other construction activities. As discussed in the Downtown FEIR, the proposed Project's compliance with regulations mandating the preparation and implementation of a SWPPP would ensure that impacts associated with erosion and sedimentation are not significant.</p>					<b>X</b>	<b>X</b>
<b>10. LAND USE AND PLANNING:</b>						
<p><i>(a) Physically divide an established community?</i></p> <p>The Project does not propose any features or structures that would physically divide an established community. Impacts associated with this issue would not occur.</p>					<b>X</b>	<b>X</b>
<p><i>(b) Substantially conflict with the City's General Plan and Progress Guide, Downtown Community Plan or other applicable land use plan, policy, or regulation?</i></p> <p>The Land Use District for the site is Employment/Residential Mixed-Use (ER) which provides synergies between educational institutions and residential neighborhoods or transition between the Core District and residential neighborhoods. A variety of uses are permitted in this district, including office, residential, hotel, research and development, educational, and medical facilities.</p> <p>The Project would not conflict with other applicable land use plans, policies, or regulations. The Project complies with the goals and policies of the DCP and would not create adverse environmental impacts under</p>					<b>X</b>	<b>X</b>



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
CEQA. With the approval of the requested Neighborhood Development Permit for the proposed deviations, the Project will meet all applicable development standards of the CCPDO and SDMC. Therefore, no significant direct or cumulative impacts associated with an adopted land use plan would occur.						
<p><i>(c) Substantial incompatibility with surrounding land uses?</i></p> <p>Sources of land use incompatibility include, amongst others, lighting, industrial activities, shading, and noise. The Project would not result in or be subject to, adverse impacts due to substantially incompatible land uses. Compliance with the City's Light Pollution Ordinance would ensure that land use incompatibility impacts related to the Project's emission of, and exposure to, lighting are not significant. In addition, the Downtown FEIR concludes that existing mandatory regulations addressing land use compatibility with industrial activities would ensure that residents of, and visitors to, the Project are not subject to potential land use incompatibilities (potential land use incompatibilities resulting from hazardous materials and air emissions are evaluated elsewhere in this evaluation).</p> <p>Potentially significant impacts associated with the Project's incompatibility with traffic noise on adjacent grid streets are discussed in Sections 12.b and 12.c. No impacts associated with incompatibility with surrounding land use would occur.</p>					<b>X</b>	<b>X</b>



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
<p>(d) <i>Substantially impact surrounding communities due to sanitation and litter problems generated by transients displaced by Downtown development?</i></p> <p>Although not expected to be a substantial direct impact of the Project because substantial numbers of transients are not known to congregate on-site, the Project, in tandem with other Downtown development activities, would have a significant cumulative impact on surrounding communities resulting from sanitation problems and litter generation by transients who are displaced from Downtown into surrounding canyons and vacant land as discussed in the Downtown FEIR. Continued support of Homeless Outreach Teams (HOTs) and similar transient outreach efforts would reduce, but not fully mitigate, the adverse impacts to surrounding neighborhoods caused by the transient relocation. Therefore, the Project would result in cumulatively significant and not fully mitigated impacts to surrounding neighborhoods.</p>		<b>X</b>			<b>X</b>	
<b>11. MINERAL RESOURCES:</b>						
<p>(a) <i>Substantially reduce the availability of important mineral resources?</i></p> <p>The Downtown FEIR states that the viable extraction of mineral resources is limited in the DCP Area due to its urban nature and the fact that the area is not recognized for having high mineral resource potential. Therefore, no impact associated with this issue would occur.</p>					<b>X</b>	<b>X</b>
<b>12. NOISE:</b>						
<p>(a) <i>Substantial noise generation?</i></p> <p>A noise analysis report (Dbf Associates, February 2020) was prepared for the project and determined that no substantial noise generation impacts would occur from any stationary sources over the long-term. Short-</p>		<b>X</b>			<b>X</b>	



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
term construction noise impacts would be avoided by adherence to construction noise limitations imposed by the City's Noise Abatement and Control Ordinance. The Downtown FEIR defines a significant long-term traffic noise increase as an increase of at least 3.0 dB (A) CNEL for street. The Downtown FEIR identified nine street segments in Downtown that would be significantly impacted as a result of traffic generation; however, none of these identified segments are in the direct vicinity of the Project site. Nevertheless, automobile trips generated by the project, would, in combination with other development in Downtown significantly increase noise on several street segments resulting in cumulatively significant noise impacts. The Downtown FEIR concludes that there are no feasible mitigation measures available to reduce the significant noise increase in noise on affected roadways and this impact remains significant and unavoidable.						
<p><i>(b) Substantial exposure of required outdoor residential open spaces or public parks and plazas to noise levels (e.g. exposure to levels exceeding 65 dBA CNEL)?</i></p> <p>The Project contains a residential component and under the CCPDO, developments of this size are required to contain common outdoor open space areas. No public parks and/or plazas are proposed in the Project.</p> <p>The noise analysis prepared by Dbf Associates identified that future exterior traffic noise levels at the proposed project site would range from less than 60 dBA CNEL at the south building façade to approximately 71 dBA CNEL at the northeast building façade corner. Future exterior traffic noise levels at the Level 22 exterior amenity area would be 65 dBA CNEL or below, and would be considered “less than significant” by the City. Future exterior roadway</p>					<b>X</b>	<b>X</b>



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
<p>traffic noise levels at private balconies facing G Street, 9th Avenue, or 10th Avenue would exceed 65 dBA CNEL without abatement. Private balconies included in the project's required open space calculations must include a 3.5-foot-high noise barrier facing the roadway .</p> <p>According to NOI-B.1-2, an acoustical analysis must be performed to determine if any required outdoor open space areas would be exposed to noise levels in excess of 65 dBA CNEL and, provided noise attenuation would not interfere with the primary purpose or design intent of the exterior use, measures must be included in the building plan to reduce noise exposure, to the extent feasible. Full attenuation of noise may be contrary to the goal of creating outdoor open spaces for residences. If full enclosure of the open space would be required to fully attenuate noise, it would defeat the basic goal of providing "outdoor" open space. The project does not exceed the intensity of development assumed under the Downtown FEIR. Impacts associated with DCP buildout would remain significant and unavoidable, but the Project would not increase the severity of impacts related to exterior traffic noise in residential development.</p>						
<p><i>(c) Substantial interior noise within habitable rooms (e.g. levels in excess of 45 dBA CNEL)?</i></p> <p>According to the noise analysis interior noise levels within habitable rooms would not exceed 45 dBA CNEL (the standard set forth in the DCP FEIR) with a typical exterior wall, windows, and glass doors with a sound rating of STC 28 and mechanical ventilation in units to maintain a consistent level of acoustical quality around the Project sufficient to achieve compliant interior noise levels in all habitable units. Pursuant to <b>Downtown FEIR Mitigation Measure NOI-B.1-1</b>, the project would be required to implement the design features included in the</p>			<b>X</b>			<b>X</b>



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
Acoustical Analysis (2020). Therefore, project impacts related to the exposure of persons to or generation of noise levels in excess of noise standards would be less than significant with mitigation included in the Downtown FEIR.						
<b>13. POPULATION AND HOUSING:</b>						
<p><i>(a) Substantially induce population growth in an area?</i></p> <p>The Downtown FEIR concludes that build-out of Downtown would not induce substantial population growth that results in adverse physical changes. The Project is consistent with the DCP and CCPDO and does not exceed those analyzed throughout the Downtown FEIR. Therefore, project-level and cumulative impacts associated with this issue are not significant.</p>					<b>X</b>	<b>X</b>
<p><i>(b) Substantial displacement of existing housing units or people?</i></p> <p>The Project site, currently developed with a commercial warehouse building and does not contain any dwelling units. No existing housing units would be affected by the development or operation of the proposed project. Overall displacement of existing housing units or persons would not occur as a result of the proposed project, and the construction of replacement housing would not be required. Therefore, no direct or cumulative impacts associated with this issue would occur.</p>					<b>X</b>	<b>X</b>
<b>14. PUBLIC SERVICES AND UTILITIES:</b>						
<p><i>(a) Substantial adverse physical impacts associated with the provision of new schools?</i></p> <p>The population of school-aged children attending public schools is dependent on current and future residential development. In and of itself, the Project would not</p>					<b>X</b>	<b>X</b>



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
generate a sufficient number of students to warrant construction of a new school facility. However, the FEIR concludes that the additional student population anticipated at build out of Downtown would require the construction of at least one additional school, and that additional capacity could potentially be accommodated in existing facilities. The specific future location of new facilities is unknown at the present time. Pursuant to Section 15145 of CEQA, analysis of the physical changes in Downtown, which may occur from future construction of these public facilities, would be speculative and no further analysis of their impacts is required. Construction of any additional schools would be subject to CEQA. Environmental documentation prepared pursuant to CEQA would identify potentially significant impacts and appropriate mitigation measures. Therefore, implementation of the Project would not result in direct or cumulative impacts associated with this issue.						
<p><i>(b) Substantial adverse physical impacts associated with the provision of new libraries?</i></p> <p>The FEIR concludes that, cumulatively, development in Downtown would generate the need for a new Central Library which was completed in 2013 and possibly several smaller libraries in Downtown. In and of itself, the proposed Project would not generate additional demand necessitating the construction of new library facilities. However, according to the analysis in the Downtown FEIR, future development projects are considered to contribute to the cumulative need for new library facilities Downtown identified in the Downtown FEIR. Nevertheless, the specific future location of these facilities is unknown at present. Pursuant to Section 15145 of CEQA, analysis of the physical changes in Downtown, which may occur from future construction of these public facilities, would be speculative and no further analysis of their impacts is</p>					<b>X</b>	<b>X</b>



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
required. Construction of any additional library facilities would be subject to CEQA. Environmental documentation prepared pursuant to CEQA would identify potentially significant impacts and appropriate mitigation measures. Therefore, approval of the Project would not result in direct or cumulative impacts associated with this issue.						
<p><i>(c) Substantial adverse physical impacts associated with the provision of new fire protection/ emergency facilities?</i></p> <p>The Project would not generate a level of demand for fire protection/emergency facilities beyond the level assumed by the Downtown FEIR. Pursuant to Section 15145 of the California Environmental Quality Act (CEQA), analysis of the physical changes in Downtown that may occur from future construction of fire station facilities would be speculative and no further analysis of the impact is required. Environmental documentation prepared pursuant to CEQA would identify significant impacts and appropriate mitigation measures for any future fire station facilities.</p>					<b>X</b>	<b>X</b>
<p><i>(d) Substantial adverse physical impacts associated with the provision of new law enforcement facilities?</i></p> <p>The Downtown FEIR analyzes impacts to law enforcement service resulting from the cumulative development of Downtown and concludes the construction of new law enforcement facilities would not be required. Since the land use designation of the proposed development is consistent with the land use designation assumed in the Downtown FEIR analysis, the Project would not generate a level of demand for law enforcement facilities beyond the level assumed by the Downtown FEIR. However, the need for a new facility could be identified in the future. Pursuant to</p>					<b>X</b>	<b>X</b>



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
Section 15145 of the California Environmental Quality Act (CEQA), analysis of the physical changes in the downtown planning area that may occur from the future construction of law enforcement facilities would be speculative and no future analysis of their impacts would be required. However, construction of new law enforcement facilities would be subject to CEQA. Environmental documentation prepared pursuant to CEQA would identify potentially significant impacts and appropriate mitigation measures.						
<p><i>(e) Substantial adverse physical impacts associated with the provision of new water transmission or treatment facilities?</i></p> <p>The Public Utilities Department provides water service to Downtown and delivers more than 200,000 million acre-feet annually to over 1.3 million residents. During an average year the Department's water supply is made up of 10 to 20 percent of local rainfall, with the remaining amount imported from regional water suppliers including the San Diego County Water Authority (SDCWA) and the Metropolitan Water District (MWD). Potable water pipelines are located underneath the majority of Downtown's streets mimicking the above-ground street grid pattern.</p> <p>According to the Downtown FEIR, in the short term, planned water supplies and transmission or treatment facilities are adequate for development of Downtown. Water transmission infrastructure necessary to transport water supply to Downtown is already in place. Build out of the 2006 DCP, was considered in the updated 2015 SDCWA Urban Water Management Plan (UWMP). The Metropolitan Water District (MWD) and the SDCWA have developed water supply plans to improve reliability and reduce dependence on existing imported supplies. MWD's Regional Urban Water Management Plan (RUWMP)</p>					<b>X</b>	<b>X</b>



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
<p>and Integrated Water Management Plan (IWMP), and the SDCWA 2015 UWMP and annual water supply report include water infrastructure projects that meet long-term supply needs through securing water from the State Water Project, Colorado River, local water supply development, and recycled water. The SDCWA 2015 UWMP demonstrates that there will be sufficient water supplies available to meet demands for existing and planned future developments that are projected to occur by 2040.</p> <p>California Water Code Section 10910 requires projects analyzed under CEQA to assess water demand and compare that finding to the jurisdiction's projected water supply.</p> <p>Senate Bill 610 (SB 610) requires the preparation of a Water Supply Assessment (WSA) when a development project exceeds a threshold of 500 equivalent dwelling units. The Project does not exceed this threshold. Thus, the proposed project does not trigger the requirements of SB 610 and is consistent with the DCP, direct and cumulative impacts related to water supply would be considered not significant.</p>						
<p><i>(f) Substantial adverse physical impacts associated with the provision of new storm water facilities?</i></p> <p>The Downtown FEIR concludes that the cumulative development of the downtown would not impact the existing downtown storm drain system. Since implementation of the Project would not result in a significant increase of impervious surfaces, the amount of runoff volume entering the storm drain system would not create demand for new storm water facilities. Direct and cumulative impacts associated with this issue are considered not significant. Direct and cumulative impacts associated with this issue are considered not significant.</p>					<b>X</b>	<b>X</b>



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
<p><i>(g) Substantial adverse physical impacts associated with the provision of new wastewater transmission or treatment facilities?</i></p> <p>The Downtown FEIR concludes that new wastewater treatment facilities would not be required to address the cumulative development of the Downtown. In addition, sewer improvements that may be needed to serve the Project are categorically exempt from environmental review under CEQA as stated in the Downtown FEIR. Therefore, impacts associated with this issue would not be significant.</p>					<b>X</b>	<b>X</b>
<p><i>(h) Substantial adverse physical impacts associated with the provision of new landfill facilities?</i></p> <p>The Downtown FEIR concludes that cumulative development within the Downtown would increase the amount of solid waste to the Miramar Landfill and contribute to the eventual need for an alternative landfill. Although the proposed Project would generate a higher level of solid waste than the existing use of the site, implementation of a mandatory Waste Management Plan and compliance with the applicable provisions of the SDMC would ensure that both short-term and long-term project-level impacts are not significant. However, the Project would contribute, in combination with other development activities in Downtown, to the cumulative increase in the generation of solid waste sent to Miramar Landfill and the eventual need for a new landfill as identified in the Downtown FEIR. The location and size of a new landfill is unknown at this time. Pursuant to Section 15145 of CEQA, analysis from the physical changes that may occur from future construction of landfills would be speculative and no further analysis of their impacts is required. However, construction or expansion of a landfill would be subject to CEQA. Environmental documentation prepared pursuant to</p>					<b>X</b>	<b>X</b>



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
CEQA would identify potentially significant impacts of the proposed Project and appropriate mitigation measures. Therefore, cumulative impacts of the proposed Project are also considered not significant.						
<b>15. PARKS AND RECREATIONAL FACILITIES:</b>						
<p>(a) <i>Substantial increase in the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?</i></p> <p>The Downtown FEIR discusses impacts to parks and other recreational facilities and the maintenance thereof and concludes that build out of Downtown would not result in significant impacts associated with this issue. Since the land use designation of the proposed development does not differ from the land use designation assumed in the Downtown FEIR analysis, the Project would not generate a level of demand for parks and recreational facilities beyond the level assumed by the Downtown FEIR. Therefore, substantial deterioration of existing neighborhood or regional parks would not occur or be substantially accelerated as a result of the Project. No significant impacts with this issue would occur.</p>					X	X
<b>16. TRANSPORTATION/TRAFFIC:</b>						
<p>(a) <i>Cause the LOS on a roadway segment or intersection to drop below LOS E?</i></p> <p>The FEIR states that projects generating greater than 2,400 ADT would result in potentially significant impacts to the level of service (LOS) of roadway segment or intersection and requires implementation of mitigation measures at the Project level to mitigate the impact. The expected trip generation for this mixed-used project is approximately 1,967 average daily trips. This is based on approximately 964 average daily trips for the</p>		X			X	



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
<p>241 residential units at a rate of 4 trips/d.u. and 1,003 average daily trips at a rate of 28 trips/1,000 square feet. for the 35,813 square feet of commercial space. The rates are taken from the Centre City Cumulative Trip Rates of City of San Diego Trip Generation Manual. The Project's direct impacts on downtown roadway segments or intersections would not be significant.</p> <p>Traffic generated by the Project in combination with traffic generated by other downtown development would contribute to the significant cumulative impacts projected in the DCP FEIR to occur on a number of downtown roadway segments and intersections, and street within neighborhoods surrounding the DCP area at buildout of the downtown.</p> <p>The DCP FEIR includes mitigation measures to address impacts associated with buildout of the DCP, but the DCP FEIR acknowledges that the identified measures may or may not be able to fully mitigate these cumulative impacts due to constraints imposed by bicycle and pedestrian activities and the land uses adjacent to affected roadways. Pursuant to <b>Downtown FEIR Mitigation Measure TRF-A.1.1-2</b>, the applicant will also be required to pay development impact fees to fund a fair share fee towards transportation improvements for the DCP Area. As required by <b>Downtown FEIR Mitigation Measure TRF-A.1.1-3</b>, the City adopted the Downtown Community Public Facilities Financing Plan 2015 that established a transportation fee. The transportation fee is intended to fund street, transit, bicycle, pedestrian improvements, promenades, and below grade parking structures, as further set forth in the Downtown Community PFFP.</p>						
<p><i>(b) Cause the LOS on a freeway segment to drop below LOS E or cause a ramp delay in excess of 15 minutes?</i></p> <p>The Downtown FEIR concludes that development within Downtown will result in significant cumulative</p>		<b>X</b>			<b>X</b>	



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
impacts to freeway segments and ramps serving the Downtown area. Since the land use designation of the Project is consistent with the land use designation assumed in the Downtown FEIR analysis, the Project would contribute on a cumulative-level to the substandard LOS F identified in the Downtown FEIR on all freeway segments in the Downtown area and several ramps serving Downtown. <b>Downtown FEIR Mitigation Measure TRF-A.1.1-1</b> would reduce these impacts to the extent feasible, but not to below the level of significance. This mitigation measure is not the responsibility of the Project, and therefore is not included in Attachment A. The Downtown FEIR concludes that the uncertainty associated with implementing freeway improvements and limitations in increasing ramp capacity limits the feasibility of fully mitigating impacts to these facilities. Thus, the Project's cumulative-level impacts to freeways would remain significant and unavoidable, consistent with the analysis of the Downtown FEIR. The Project would not have a direct impact on freeway segments and ramps.						
<p><i>(c) Substantially discourage the use of alternative modes of transportation or cause transit service capacity to be exceeded?</i></p> <p>The proposed Project in and of itself does not include any features that would discourage the use of alternative modes of transportation. The Project's proximity to several other community serving uses, including nearby shopping and recreational activities also encourage walking. Additionally, visitors of the proposed Project would be encouraged to use alternative transportation means as there are several bus lines and the MTS facility/trolley station within a five-minute walk. Therefore, the Project will cause no significant impacts related to alternative modes of transportation or cause</p>					<b>X</b>	<b>X</b>



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
transit service capacity to be exceeded.						
<b>17. MANDATORY FINDINGS OF SIGNIFICANCE:</b>						
<p><i>(a) Does the Project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?</i></p> <p>As indicated in the Downtown FEIR, due to the highly urbanized nature of the Downtown area, no sensitive plant or animal species, habitats, or wildlife migration corridors are located in the Downtown area. Additionally, the Project does not have the potential to eliminate important examples of major periods of California history or pre-history at the Project level. No other aspects of the Project would substantially degrade the environment. Cumulative impacts are described in Section 17.b below.</p>					<b>X</b>	<b>X</b>
<p><i>(b) Does the Project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a Project are considerable when viewed in connection with the effects of past Projects, the effects of other current Projects, and the effects of probable future Projects)?</i></p> <p>As acknowledged in the Downtown FEIR, the buildout of Downtown would result in cumulative impacts associated with: air quality, historical resources, paleontological resources, physical changes associated with transient activities, noise, parking, traffic, and water quality. This Project would contribute to those impacts. Implementation of the</p>		<b>X</b>				



	Significant And Not Mitigated (SNM)		Significant But Mitigated (SM)		Not Significant (NS)	
	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)	Direct (D)	Cumulative (C)
<b>Issues and Supporting Information</b>						
mitigation measures identified in the Downtown FEIR would reduce some significant impacts; however, the impacts would remain significant and immitigable as identified in the Downtown FEIR and the Statement of Overriding Considerations adopted by the City. This Project's contribution would not be greater than anticipated by the Downtown FEIR and therefore no further analysis is required.						
<p><i>(c) Does the Project have environmental effects that would cause substantial adverse effects on human beings, either directly or indirectly?</i></p> <p>As acknowledged in the FEIR, the build-out of Downtown would result in cumulative impacts associated with: air quality, historical resources, paleontological resources, physical changes associated with transient activities, noise, traffic, and water quality. This Project would contribute to those impacts. However, the impacts associated with this Project would be no greater than those assumed in the Downtown FEIR and therefore no further environmental review is required under CEQA.</p>	<b>X</b>	<b>X</b>				



**9 G Tower Project (PTS# 649856)  
Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
AIR QUALITY (AQ)				
Impact AQ-B.1	Dust and construction equipment engine emissions generated during grading and demolition would impact local and regional air quality. (Direct and Cumulative)			
	<p><b>Mitigation Measure AQ-B.1-1:</b> Prior to approval of a Grading or Demolition Permit, the City shall confirm that the following conditions have been applied, as appropriate:</p> <ol style="list-style-type: none"><li>Exposed soil areas shall be watered twice per day. On windy days or when fugitive dust can be observed leaving the development site, additional applications of water shall be applied as necessary to prevent visible dust plumes from leaving the development site. When wind velocities are forecast to exceed 25 mph, all ground disturbing activities shall be halted until winds that are forecast to abate below this threshold.</li><li>Dust suppression techniques shall be implemented including, but not limited to, the following:<ol style="list-style-type: none"><li>Portions of the construction site to remain inactive longer than a period of three months shall be seeded and watered until grass cover is grown or otherwise stabilized in a manner acceptable to Civic San Diego.</li><li>On-site access points shall be paved as soon as feasible or watered periodically or otherwise stabilized.</li><li>Material transported off-site shall be either sufficiently watered or securely covered to prevent excessive amounts of dust.</li><li>The area disturbed by clearing, grading, earthmoving, or excavation operations shall be minimized at all times.</li></ol></li><li>Vehicles on the construction site shall travel at speeds less than 15 mph.</li><li>Material stockpiles subject to wind erosion during construction activities, which will not be utilized within three days, shall be covered with plastic, an alternative cover deemed equivalent to plastic, or sprayed with a nontoxic chemical stabilizer.</li><li>Where vehicles leave the construction site and enter adjacent public streets, the streets shall be swept daily or washed down at the end of the work day to remove soil tracked onto</li></ol>	Prior to Demolition or Grading Permit (Design)	Developer	City



**9 G Tower Project (PTS# 649856)  
Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>the paved surface. Any visible track-out extending for more than fifty (50) feet from the access point shall be swept or washed within thirty (30) minutes of deposition.</p> <p>6. All diesel-powered vehicles and equipment shall be properly operated and maintained.</p> <p>7. All diesel-powered vehicles and gasoline-powered equipment shall be turned off when not in use for more than five minutes, as required by state law.</p> <p>8. The construction contractor shall utilize electric or natural gas-powered equipment in lieu of gasoline or diesel-powered engines, where feasible.</p> <p>9. As much as possible, the construction contractor shall time the construction activities so as not to interfere with peak hour traffic. In order to minimize obstruction of through traffic lanes adjacent to the site, a flag-person shall be retained to maintain safety adjacent to existing roadways, if necessary.</p> <p>10. The construction contractor shall support and encourage ridesharing and transit incentives for the construction crew.</p> <p>11. Low VOC coatings shall be used as required by SDAPCD Rule 67. Spray equipment with high transfer efficiency, such as the high volume-low pressure spray method, or manual coatings application such as paint brush hand roller, trowel, spatula, dauber, rag, or sponge, shall be used to reduce VOC emissions, where feasible.</p> <p>12. If construction equipment powered by alternative fuel sources (liquefied natural gas/compressed natural gas) is available at comparable cost, the developer shall specify that such equipment be used during all construction activities on the development site.</p> <p>13. The developer shall require the use of particulate filters on diesel construction equipment if use of such filters is demonstrated to be cost-competitive for use on this development.</p> <p>14. During demolition activities, safety measures as required by City/County/State for removal of toxic or hazardous materials shall be utilized.</p> <p>15. Rubble piles shall be maintained in a damp state to minimize dust generation.</p> <p>16. During finish work, low-VOC paints and efficient transfer systems shall be utilized, to the extent possible.</p>			



**9 G Tower Project (PTS# 649856)  
Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	17. If alternative-fueled and/or particulate filter-equipped construction equipment is not feasible, construction equipment shall use the newest, least-polluting equipment, whenever possible. During finish work, low-VOC paints and efficient transfer systems shall be utilized, to the extent possible.			
<b>HISTORICAL RESOURCES (HIST)</b>				
<b>Impact HIST-A.1</b>	Future development in Downtown could impact significant architectural structures. (Direct and Cumulative)			
	<p><b>Mitigation Measure HIST-A.1-1:</b> For construction or development permits that may impact potentially historical resources which are 45 years of age or older and which have not been evaluated for local, state and federal historic significance, a site specific survey shall be required in accordance with the Historical Resources Regulations in the LDC. Based on the survey and the best information available, City Staff to the Historical Resources Board (HRB) shall determine whether historical resources exist, whether potential historical resource(s) is/are eligible for designation as designated historical resource(s) by the HRB, and the precise location of the resource(s). The identified historical resource(s) may be nominated for HRB designation as a result of the survey pursuant to Chapter 12, Article 3, Division 2, Designation of Historical Resource procedures, of the LDC.</p> <p>All applications for construction and development permits where historical resources are present on the site shall be evaluated by City Staff to the HRB pursuant to Chapter 14, Article 3, Division 2, Historical Resources Regulations of the LDC.</p> <p>1. <b>National Register-Listed/Eligible, California Register-Listed/Eligible Resources:</b> Resources listed in or formally determined eligible for the National Register or California Register and resources identified as contributing within a National or California Register District, shall be retained onsite and any improvements, renovation, rehabilitation and/or adaptive reuse of the property shall ensure its preservation and be consistent with the Secretary of the Interior's Standards for the Treatment of Historic Properties (1995) and the associated Guidelines.</p> <p>2. <b>San Diego Register-Listed Resources:</b> Resources listed in the San Diego Register of Historical Resources, or determined to be a contributor to a San Diego Register District, shall, whenever possible, be retained on-site. Partial retention, relocation, or demolition of</p>	<p>Prior to Development Permit (Design)</p> <p>Prior to Demolition, Grading, and/or Building Permit (Design)</p> <p>Prior to Certificate of Occupancy (Implementation)</p>	Developer	Civic San Diego /City



**9 G Tower Project (PTS# 649856)**  
**Downtown FEIR/SEIR**  
**Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	a resource shall only be permitted according to Chapter 14, Article 3, Division 2, Historical Resources Regulations of the LDC.			



	<p><b>Mitigation Measure HIST-A.1-2:</b> If the potential exists for direct and/or indirect impacts to retained or relocated designated and/or potential historical resources (“historical resources”), the following measures shall be implemented in coordination with a Development Services Department designee and/or City Staff to the HRB (“City Staff”) in accordance with Chapter 14, Article 3, Division 2, Historical Resources Regulations of the LDC.</p> <p><b>I. Prior to Permit Issuance</b></p> <p>A. Construction Plan Check</p> <ol style="list-style-type: none"> <li>1. Prior to Notice to Proceed (NTP) for any construction permits, including but not limited to, the first Grading Permit Building Permits, but prior to the first Preconstruction (Precon) Meeting, whichever is applicable, City Staff shall verify that the requirements for historical monitoring during demolition and/or stabilization have been noted on the appropriate construction documents. <ol style="list-style-type: none"> <li>(a) Stabilization work cannot begin until a Precon Meeting has been held at least one week prior to issuance of appropriate permits.</li> <li>(b) Physical description, including the year and type of historical resource, and extent of stabilization shall be noted on the plans.</li> </ol> </li> </ol> <p>B. Submittal of Treatment Plan for Retained Historical Resources</p> <ol style="list-style-type: none"> <li>1. Prior to NTP for any construction permits, including but not limited to, the first Grading Permit and Building Permits, but prior to the first Precon Meeting, whichever is applicable, the Applicant shall submit a Treatment Plan to City Staff for review and approval in accordance in accordance with the Secretary of the Interior’s Standards for the Treatment of Historic Properties (1995) and the associated Guidelines. The Treatment Plan shall include measures for protecting any historical resources, as defined in the LDC, during construction related activities (e.g., removal of non-historic features, demolition of adjacent structures, subsurface structural support, etc.). The Treatment Plan shall be shown as notes on all construction documents (i.e., Grading and/or Building Plans).</li> </ol> <p>C. Letters of Qualification have been submitted to City Staff</p> <ol style="list-style-type: none"> <li>1. The applicant shall submit a letter of verification to City Staff identifying the Principal Investigator (PI) for the project and the names of all persons involved in this MMRP (i.e., Architectural Historian, Historic Architect and/or Historian), as defined in the City of San Diego HRG.</li> <li>2. City Staff will provide a letter to the applicant confirming that the qualifications of the PI and all persons involved in the historical monitoring of the project meet the qualification standards established by the HRG.</li> </ol>			
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	<p>3. Prior to the start of work, the applicant must obtain approval from City Staff for any personnel changes associated with the monitoring program.</p> <p><b>II. Prior to Start of Construction</b></p> <p>A. Documentation Program (DP)</p> <p>1. Prior to the first Precon Meeting and/or issuance of any construction permit, the DP shall be submitted to City Staff for review and approval and shall include the following:</p> <p>(a) Photo Documentation</p> <p>(1) Documentation shall include professional quality photo documentation of the historical resource(s) prior to any construction that may cause direct and/or indirect impacts to the resource(s) with 35mm black and white photographs, 4x6 standard format, taken of all four elevations and close-ups of select architectural elements, such as, but not limited to, roof/wall junctions, window treatments, and decorative hardware. Photographs shall be of archival quality and easily reproducible.</p> <p>(2) Xerox copies or CD of the photographs shall be submitted for archival storage with the City of San Diego HRB and the Civic San Diego Project file. One set of original photographs and negatives shall be submitted for archival storage with the California Room of the City of San Diego Public Library, the San Diego Historical Society and/or other relative historical society or group(s).</p> <p>(b) Required drawings</p> <p>(1) Measured drawings of the building's exterior elevations depicting existing conditions or other relevant features shall be produced from recorded, accurate measurements. If portions of the building are not accessible for measurement, or cannot be reproduced from historic sources, they should not be drawn, but clearly labeled as not accessible. Drawings produced in ink on translucent material or archivally stable material (blue-line drawings) are acceptable. Standard drawing sizes are 19 by 24 inches or 24 by 36 inches, standard scale is 1/4 inch = 1 foot.</p> <p>(2) One set of measured drawings shall be submitted for archival storage with the City of San Diego HRB, the Civic San Diego Project file, the South Coastal Information Center, the California Room of the City of San Diego Public Library, the San Diego Historical Society and/or other historical society or group(s).</p>			
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	<p>2. Prior to the first Precon Meeting, City Staff shall verify that the DP has been approved.</p> <p>B. PI Shall Attend Precon Meetings</p> <p>1. Prior to beginning any work that may impact any historical resource(s) which is/are subject to this MMRP, the Applicant shall arrange a Precon Meeting that shall include the PI, Construction Manager (CM) and/or Grading Contractor, Resident Engineer (RE), Historical Monitor(s), Building Inspector (BI), if appropriate, and City Staff. The qualified Historian and/or Architectural Historian shall attend any grading/excavation related Precon Meetings to make comments and/or suggestions concerning the Historical Monitoring program with the Construction Manager and/or Grading Contractor.</p> <p>(a) If the PI is unable to attend the Precon Meeting, the Applicant shall schedule a focused Precon Meeting with City Staff, the PI, RE, CM or BI, if appropriate, prior to the start of any work that requires monitoring.</p> <p>2. Historical Monitoring Plan</p> <p>(a) Prior to the start of any work that is subject to an Historical Monitoring Plan, the PI shall submit an Historical Monitoring Plan which describes how the monitoring would be accomplished for approval by City Staff. The Historical Monitoring Plan shall include an Historical Monitoring Exhibit (HME) based on the appropriate construction documents (reduced to 11x17 inches) to City Staff identifying the areas to be monitored including the delineation of grading/excavation limits.</p> <p>(b) Prior to the start of any work, the PI shall also submit a construction schedule to City Staff through the RE indicating when and where monitoring will occur.</p> <p>(c) The PI may submit a detailed letter to City Staff prior to the start of work or during construction requesting a modification to the monitoring program. This request shall be based on relevant information such as review of final construction documents which indicate site conditions such as underpinning, shoring and/or extensive excavation which could result in impacts to, and/or reduce impacts to the on-site or adjacent historical resource.</p> <p>C. Implementation of Approved Treatment Plan for Historical Resources</p> <p>1. Implementation of the approved Treatment Plan for the protection of historical resources within the project site may not begin prior to the completion of the Documentation Program as defined above.</p>			
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	<ol style="list-style-type: none"> <li>2. The qualified Historical Monitor(s) shall attend weekly jobsite meetings and be on-site daily during the stabilization phase for any retained or adjacent historical resource to photo document the Treatment Plan process.</li> <li>3. The qualified Historical Monitor(s) shall document activity via the Consultant Site Visit Record (CSV). The CSV's shall be faxed by the CM to the RE the first day and last day (Notification of Monitoring Completion) of the Treatment Plan process and in the case of ANY unanticipated incidents. The RE shall forward copies to City Staff.</li> <li>4. Prior to the start of any construction related activities, the applicant shall provide verification to City Staff that all historical resources on-site have been adequately stabilized in accordance with the approved Treatment Plan. This may include a site visit with City Staff, the CM, RE or BI, but may also be accomplished through submittal of the draft Treatment Plan photo documentation report.</li> <li>5. City Staff will provide written verification to the RE or BI after the site visit or upon approval of draft Treatment Plan report indicating that construction related activities can proceed.</li> </ol> <p><b>III. During Construction</b></p> <p>A. Qualified Historical Monitor(s) Shall be Present During Grading/Excavation/Trenching</p> <ol style="list-style-type: none"> <li>1. The Qualified Historical Monitor(s) shall be present full-time during grading/excavation/trenching activities which could result in impacts to historical resources as identified on the HME. The Construction Manager is responsible for notifying the RE, PI, and City Staff of changes to any construction activities.</li> <li>2. The Qualified Historical Monitor(s) shall document field activity via the CSV. The CSV's shall be faxed by the CM to the RE the first day of monitoring, the last day of monitoring, monthly (Notification of Monitoring Completion), and in the case of ANY incidents involving the historical resource. The RE shall forward copies to City Staff.</li> <li>3. The PI may submit a detailed letter to City Staff during construction requesting a modification to the monitoring program when a field condition arises which could effect the historical resource being retained on-site or adjacent to the construction site.</li> </ol> <p>B. Notification Process</p> <ol style="list-style-type: none"> <li>1. In the event of damage to a historical resource retained on-site or adjacent to the project site, the Qualified Historical Monitor(s) shall direct the contractor to temporarily divert construction activities in the area of historical resource and</li> </ol>			
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	<p>immediately notify the RE or BI, as appropriate, and the PI (unless Monitor is the PI).</p> <p>2. The PI shall immediately notify City Staff by phone of the incident, and shall also submit written documentation to City Staff within 24 hours by fax or email with photos of the resource in context, if possible.</p> <p>C. Determination/Evaluation of Impacts to a Historical Resource</p> <p>1. The PI shall evaluate the incident relative to the historical resource.</p> <p>(a) The PI shall immediately notify City Staff by phone to discuss the incident and shall also submit a letter to City Staff indicating whether additional mitigation is required.</p> <p>(b) If impacts to the historical resource are significant, the PI shall submit a proposal for City Staff review and written approval in accordance with Chapter 14, Article 3, Division 2, Historical Resources Regulations of the LDC and the Secretary of the Interior's Standards for the Treatment of Historic Properties (1995) and the associated Guidelines. Direct and/or indirect impacts to historical resources from construction activities must be mitigated before work will be allowed to resume.</p> <p>(c) If impacts to the historical resource are not considered significant, the PI shall submit a letter to City Staff indicating that the incident will be documented in the Final Monitoring Report. The letter shall also indicate that that no further work is required.</p> <p><b>IV. Night Work</b></p> <p>A. If night and/or weekend work is included in the contract</p> <p>1. When night and/or weekend work is included in the contract package, the extent and timing shall be presented and discussed at the Precon Meeting.</p> <p>2. The following procedures shall be followed.</p> <p>(a) No Impacts/Incidents</p> <p>In the event that no historical resources were impacted during night and/or weekend work, the PI shall record the information on the CSVr and submit to City Staff via fax by 8 a.m. of the next business day.</p> <p>(b) Potentially Significant Impacts</p>			
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	<p>If the PI determines that a potentially significant impact has occurred to a historical resource, the procedures detailed under Section III - During Construction shall be followed.</p> <p>(c) The PI shall immediately contact City Staff, or by 8 a.m. of the next business day to report and discuss the findings as indicated in Section III-B, unless other specific arrangements have been made.</p> <p>B. If night and/or weekend work becomes necessary during the course of construction:</p> <ol style="list-style-type: none"> <li>1. The Construction Manager shall notify the RE, or BI, as appropriate, a minimum of 24 hours before the work is to begin.</li> <li>2. The RE, or BI, as appropriate, shall notify City Staff immediately.</li> </ol> <p>C. All other procedures described above shall apply, as appropriate.</p> <p><b>V. Post Construction</b></p> <p>A. Submittal of Draft Monitoring Report</p> <ol style="list-style-type: none"> <li>1. The PI shall submit two copies of the Draft Monitoring Report (even if negative), prepared in accordance with the Historical Resources Guidelines (HRG) and Appendices which describes the results, analysis, and conclusions of all phases of the Historical Monitoring Plan (with appropriate graphics) to City Staff for review and approval within 90 days following the completion of monitoring. <ul style="list-style-type: none"> <li>(a) The preconstruction Treatment Plan and Documentation Plan (photos and measured drawings) and Historical Commemorative Program, if applicable, shall be included and/or incorporated into the Draft Monitoring Report.</li> <li>(b) The PI shall be responsible for updating (on the appropriate State of California Department of Park and Recreation forms-DPR 523 A/B) any existing site forms to document the partial and/or complete demolition of the resource. Updated forms shall be submitted to the South Coastal Information Center with the Final Monitoring Report.</li> </ul> </li> <li>2. City Staff shall return the Draft Monitoring Report to the PI for revision or, for preparation of the Final Report.</li> <li>3. The PI shall submit revised Draft Monitoring Report to City Staff for approval.</li> <li>4. City Staff shall provide written verification to the PI of the approved report.</li> <li>5. City Staff shall notify the RE or BI, as appropriate, of receipt of all Draft Monitoring Report submittals and approvals.</li> </ol>			
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**9 G Tower Project (PTS# 649856)  
Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>B. Final Monitoring Report(s)</p> <ol style="list-style-type: none"> <li>1. The PI shall submit one copy of the approved Final Monitoring Report to the RE or BI as appropriate, and one copy to City Staff (even if negative), within 90 days after notification from City Staff that the draft report has been approved.</li> <li>2. The RE shall, in no case, issue the Notice of Completion until receiving a copy of the approved Final Monitoring Report from City Staff.</li> </ol>			
<b>Impact HIST-B.1</b>	Development in Downtown could impact significant buried archaeological resources. (Direct and Cumulative)			
	<p><b>Mitigation Measure HIST-B.1-1:</b> If the potential exists for direct and/or indirect impacts to significant buried archaeological resources, the following measures shall be implemented in coordination with a Development Services Department designee and/or City Staff to the HRB ("City Staff") in accordance with Chapter 14, Article 3, Division 2, Historical Resources Regulations of the LDC. Prior to issuance of any permit that could directly affect an archaeological resource, City Staff shall assure that all elements of the MMRP are performed in accordance with all applicable City regulations and guidelines by an Archaeologist meeting the qualifications specified in Appendix B of the San Diego LDC, Historical Resources Guidelines. City Staff shall also require that the following steps be taken to determine: (1) the presence of archaeological resources and (2) the appropriate mitigation for any significant resources which may be impacted by a development activity. Sites may include residential and commercial properties, privies, trash pits, building foundations, and industrial features representing the contributions of people from diverse socio-economic and ethnic backgrounds. Sites may also include resources associated with pre-historic Native American activities. Archeological resources which also meet the definition of historical resources or unique archaeological resources under CEQA or the SDMC shall be treated in accordance with the following evaluation procedures and applicable mitigation program:</p> <p>Step 1–Initial Evaluation</p> <p>An initial evaluation for the potential of significant subsurface archaeological resources shall be prepared to the satisfaction of City Staff as part of an Environmental Secondary Study for any activity which involves excavation or building demolition. The initial evaluation shall be guided</p>	<p>Prior to Demolition or Grading Permit (Design)</p> <p>Prior to Certificate of Occupancy (Implementation)</p>	Developer	City Staff



**9 G Tower Project (PTS# 649856)  
Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>by an appropriate level research design in accordance with the City's LDC, Historical Resources Guidelines. The person completing the initial review shall meet the qualification requirements as set forth in the Historical Resources Guidelines and shall be approved by City Staff. The initial evaluation shall consist, at a minimum, of a review of the following historical sources: The 1876 Bird's Eye View of San Diego, all Sanborn Fire Insurance Company maps, appropriate City directories and maps that identify historical properties or archaeological sites, and a records search at the South Coastal Information Center for archaeological resources located within the property boundaries. Historical and existing land uses shall also be reviewed to assess the potential presence of significant prehistoric and historic archaeological resources. The person completing the initial review shall also consult with and consider input from local individuals and groups with expertise in the historical resources of the San Diego area. These experts may include the University of California, San Diego State University, San Diego Museum of Man, Save Our Heritage Organization, local historical and archaeological groups, the Native American Heritage Commission (NAHC), designated community planning groups, and other individuals or groups that may have specific knowledge of the area. Consultation with these or other individuals and groups shall occur as early as possible in the evaluation process.</p> <p>When the initial evaluation indicates that important archaeological sites may be present on a project site but their presence cannot be confirmed prior to construction or demolition due to obstructions or spatially limited testing and data recovery, the applicant shall prepare and implement an archaeological monitoring program as a condition of development approval to the satisfaction of City Staff. If the NAHC Sacred Lands File search is positive for Native American resources within the project site, then additional evaluation must include participation of a local Native American consultant in accordance with CEQA Sections 15064.5(d), 15126.4(b)(3) and Public Resources Code Section 21083.2.</p> <p>No further action is required if the initial evaluation demonstrates there is no potential for subsurface resources. The results of this research shall be summarized in the Secondary Study.</p> <p>Step 2–Testing</p> <p>A testing program is required if the initial evaluation demonstrates that there is a potential for subsurface resources. The testing program shall be conducted during the hazardous materials remediation or following the removal of any structure or surface covering which may be underlain by potential resources. The removal of these structures shall be conducted in a manner</p>			



**9 G Tower Project (PTS# 649856)  
Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
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	<p>which minimizes disturbance of underlying soil. This shall entail a separate phase of investigations from any mitigation monitoring during construction.</p> <p>The testing program shall be performed by a qualified Historical Archaeologist meeting the qualifications specified in Appendix B of the San Diego LDC, HRG. The Historical Archaeologist must be approved by City Staff prior to commencement. Before commencing the testing, a treatment plan shall be submitted for City Staff approval that reviews the initial evaluation results and includes a research design. The research design shall be prepared in accordance with the City's HRG and include a discussion of field methods, research questions against which discoveries shall be evaluated for significance, collection strategy, laboratory and analytical approaches, and curation arrangements. All tasks shall be in conformity with best practices in the field of historic urban archaeology.</p> <p>A recommended approach for historic urban sites is at a minimum fills and debris along interior lot lines or other areas indicated on Sanborn maps.</p> <p>Security measures such as a locked fence or surveillance shall be taken to prevent looting or vandalism of archaeological resources as soon as demolition is complete or paved surfaces are removed. These measures shall be maintained during archaeological field investigations. It is recommended that exposed features be covered with steel plates or fill dirt when not being investigated.</p> <p>The results of the testing phase shall be submitted in writing to City Staff and shall include the research design, testing results, significance evaluation, and recommendations for further treatment. Final determination of significance shall be made in consultation with City Staff , and with the Native American community, if the finds are prehistoric. If no significant resources are found and site conditions are such that there is no potential for further discoveries, then no further action is required. If no significant resources are found but results of the initial evaluation and testing phase indicates there is still a potential for resources to be present in portions of the property that could not be tested, then mitigation monitoring is required and shall be conducted in accordance with the provisions set forth in Step 4 - Monitoring. If significant resources are discovered during the testing program, then data recovery in accordance with Step 3 shall be undertaken prior to construction. If the existence or probable likelihood of Native American human remains or associated grave goods area discovered through the testing program, the Qualified Archaeologist shall stop work in the area, notify the City Building Inspector, City staff, and immediately implement the procedures set forth in CEQA</p>			



**9 G Tower Project (PTS# 649856)  
Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
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	<p>Guidelines Section 15064.5 and the California PRC Section 5097.98 for discovery of human remains. This procedure is further detailed in the Mitigation, Monitoring and Reporting Program (Step 4). City Staff must concur with evaluation results before the next steps can proceed.</p> <p><b>Step 3–Data Recovery</b></p> <p>For any site determined to be significant, a Research Design and Data Recovery Program shall be prepared in accordance with the City’s Historical Resources Guidelines, approved by City Staff, and carried out to mitigate impacts before any activity is conducted which could potentially disturb significant resources. The archaeologist shall notify City Staff of the date upon which data recovery will commence ten (10) working days in advance.</p> <p>All cultural materials collected shall be cleaned, catalogued and permanently curated with an appropriate institution. Native American burial resources shall be treated in the manner agreed to by the Native American representative or be reinterred on the site in an area not subject to further disturbance in accordance with CEQA section 15164.5 and the Public Resources Code section 5097.98. All artifacts shall be analyzed to identify function and chronology as they relate to the history of the area. Faunal material shall be identified as to species and specialty studies shall be completed, as appropriate. All newly discovered archaeological sites shall be recorded with the South Coastal Information Center at San Diego State University. Any human bones and associated grave goods of Native American origin encountered during Step 2-Testing, shall, upon consultation, be turned over to the appropriate Native American representative(s) for treatment in accordance with state regulations as further outlined under Step 4-Monitoring (Section IV. Discovery of Human Remains).</p> <p>A draft Data Recovery Report shall be submitted to City Staff within twelve months of the commencement of the data recovery. Data Recovery Reports shall describe the research design or questions, historic context of the finds, field results, analysis of artifacts, and conclusions. Appropriate figures, maps and tables shall accompany the text. The report shall also include a catalogue of all finds and a description of curation arrangements at an approved facility, and a general statement indicating the disposition of any human remains encountered during the data recovery effort (please note that the location of reinternment and/or repatriation is confidential and not subject to public disclosure in accordance with state law). Finalization of draft reports shall be subject to City Staff review.</p> <p><b>Step 4 – Monitoring</b></p>			



**9 G Tower Project (PTS# 649856)  
Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>If no significant resources are encountered, but results of the initial evaluation and testing phase indicates there is still a potential for resources to be present in portions of the property that could not be tested, then mitigation monitoring is required and shall be conducted in accordance with the following provisions and components:</p> <p><b>I. Prior to Permit Issuance</b></p> <p>A. Construction Plan Check</p> <ol style="list-style-type: none"> <li>1. Prior to NTP for any construction permits, including but not limited to, the first Grading Permit, Demolition Permits and Building Permits, but prior to the first Precon Meeting, whichever is applicable, City Staff shall verify that the requirements for Archaeological Monitoring and Native American monitoring, where the project may impact Native American resources, have been noted on the appropriate construction documents.</li> </ol> <p>B. Letters of Qualification have been submitted to City Staff</p> <ol style="list-style-type: none"> <li>1. The applicant shall submit a letter of verification to City Staff identifying the PI for the project and the names of all persons involved in the archaeological monitoring program, as defined in the City of San Diego HRG. If applicable, individuals involved in the archaeological monitoring program must have completed the 40-hour Hazardous Waste Operations and Emergency Response training with certification documentation.</li> <li>2. City Staff will provide a letter to the applicant confirming that the qualifications of the PI and all persons involved in the archaeological monitoring of the project meet the qualifications established in the HRG.</li> <li>3. Prior to the start of work, the applicant must obtain written approval from City Staff for any personnel changes associated with the monitoring program.</li> </ol> <p><b>II. Prior to Start of Construction</b></p> <p>A. Verification of Records Search</p> <ol style="list-style-type: none"> <li>1. The PI shall provide verification to City Staff that a site-specific records search (1/4 mile radius) has been completed. Verification includes, but is not limited to a copy</li> </ol>			



**9 G Tower Project (PTS# 649856)  
Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>of a confirmation letter from South Coastal Information Center, or, if the search was in-house, a letter of verification from the PI stating that the search was completed.</p> <ol style="list-style-type: none"> <li>2. The letter shall introduce any pertinent information concerning expectations and probabilities of discovery during trenching and/or grading activities.</li> <li>3. The PI may submit a detailed letter to City Staff requesting a reduction to the 1/4 mile radius.</li> </ol> <p>B. PI Shall Attend Precon Meetings</p> <ol style="list-style-type: none"> <li>1. Prior to beginning any work that requires monitoring, the Applicant shall arrange a Precon Meeting that shall include the PI, Native American consultant/monitor (where Native American resources may be impacted), CM and/or Grading Contractor, RE, the Native American representative(s) (where Native American resources may be impacted), BI, if appropriate, and City Staff. The qualified Archaeologist and the Native American consultant/monitor shall attend any grading/excavation related Precon Meetings to make comments and/or suggestions concerning the Archaeological Monitoring program with the Construction Manager and/or Grading Contractor. <ol style="list-style-type: none"> <li>(a) If the PI is unable to attend the Precon Meeting, the Applicant shall schedule a focused Precon Meeting with City Staff, the PI, RE, CM or BI, if appropriate, prior to the start of any work that requires monitoring.</li> </ol> </li> <li>2. Archaeological Monitoring Plan (AMP) <ol style="list-style-type: none"> <li>(a) Prior to the start of any work that requires monitoring, the PI shall submit an Archaeological Monitoring Plan (with verification that the AMP has been reviewed and approved by the Native American consultant/monitor when Native American resources may be impacted) which describes how the monitoring would be accomplished for approval by City Staff and the Native American monitor. The AMP shall include an Archaeological Monitoring Exhibit (AME) based on the appropriate construction documents (reduced to 11 by 17 inches) to City Staff identifying the areas to be monitored including the delineation of grading/excavation limits.</li> </ol> </li> </ol>			



**9 G Tower Project (PTS# 649856)  
Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>(b) The AME shall be based on the results of a site-specific records search as well as information regarding existing known soil conditions (native or formation).</p> <p>(c) Prior to the start of any work, the PI shall also submit a construction schedule to City Staff through the RE indicating when and where monitoring will occur.</p> <p>(d) The PI may submit a detailed letter to City Staff prior to the start of work or during construction requesting a modification to the monitoring program. This request shall be based on relevant information such as review of final construction documents which indicate site conditions such as depth of excavation and/or site graded to bedrock, etc., which may reduce or increase the potential for resources to be present.</p> <p><b>III. During Construction</b></p> <p>A. Monitor(s) Shall be Present During Grading/Excavation/Trenching</p> <ol style="list-style-type: none"> <li>1. The Archaeological monitor shall be present full-time during all soil disturbing and grading/excavation /trenching activities which could result in impacts to archaeological resources as identified on the AME. The Construction Manager is responsible for notifying the RE, PI, and City Staff of changes to any construction activities.</li> <li>2. The Native American consultant/monitor shall determine the extent of their presence during soil disturbing and grading/excavation/trenching activities based on the AME, and provide that information to the PI and City Staff. If prehistoric resources are encountered during the Native American consultant/monitor's absence, work shall stop and the Discovery Notification Processes detailed in Sections III.B-C, and IVA-D shall commence.</li> <li>3. The archeological and Native American consultant/monitor shall document field activity via the CSV. The CSV's shall be faxed by the CM to the RE the first day of monitoring, the last day of monitoring, monthly (Notification of Monitoring Completion), and in the case of ANY discoveries. The RE shall forward copies to City Staff.</li> <li>4. The PI may submit a detailed letter to City Staff during construction requesting a modification to the monitoring program when a field condition such as modern</li> </ol>			

**9 G Tower Project (PTS# 649856)  
Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>disturbance post-dating the previous grading/trenching activities, presence of fossil formations, or when native soils are encountered that may reduce or increase the potential for resources to be present.</p> <p><b>B. Discovery Notification Process</b></p> <ol style="list-style-type: none"> <li>1. In the event of a discovery, the Archaeological Monitor shall direct the contractor to temporarily divert all soil disturbing activities, including but not limited to, digging, trenching, excavating, or grading activities in the area of discovery and in the area reasonably suspected to overlay adjacent resources and immediately notify the RE or BI, as appropriate.</li> <li>2. The Monitor shall immediately notify the PI (unless Monitor is the PI) of the discovery.</li> <li>3. The PI shall immediately notify City Staff by phone of the discovery, and shall also submit written documentation to City Staff within 24 hours by fax or email with photos of the resource in context, if possible.</li> <li>4. No soil shall be exported off-site until a determination can be made regarding the significance of the resource specifically if Native American resources are encountered.</li> </ol> <p><b>C. Determination of Significance</b></p> <ol style="list-style-type: none"> <li>1. The PI and Native American consultant/monitor, where Native American resources are discovered, shall evaluate the significance of the resource. If Human Remains are involved, follow protocol in Section IV below.               <ol style="list-style-type: none"> <li>(a) The PI shall immediately notify City Staff by phone to discuss significance determination and shall also submit a letter to City Staff indicating whether additional mitigation is required.</li> <li>(b) If the resource is significant, the PI shall submit an Archaeological Data Recovery Program which has been reviewed by the Native American consultant/monitor when applicable, and obtain written approval from City Staff and the Native American representative(s), if applicable. Impacts to</li> </ol> </li> </ol>			



**9 G Tower Project (PTS# 649856)  
Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>significant resources must be mitigated before ground disturbing activities in the area of discovery will be allowed to resume.</p> <p>(c) If the resource is not significant, the PI shall submit a letter to City Staff indicating that artifacts will be collected, curated, and documented in the Final Monitoring Report. The letter shall also indicate that that no further work is required.</p> <p><b>IV. Discovery of Human Remains</b></p> <p>If human remains are discovered, work shall halt in that area and no soil shall be exported off-site until a determination can be made regarding the provenance of the human remains; and the following procedures set forth in CEQA Section 15064.5(e), the California Public Resources Code (Sec. 5097.98) and State Health and Safety Code (Sec. 7050.5) shall be undertaken:</p> <p>A. Notification</p> <ol style="list-style-type: none"> <li>1. Archaeological Monitor shall notify the RE or BI as appropriate, City Staff, and the PI, if the Monitor is not qualified as a PI. City Staff will notify the appropriate Senior Planner in the Environmental Analysis Section of the Development Services Department to assist with the discovery process.</li> <li>2. The PI shall notify the Medical Examiner after consultation with the RE, either in person or via telephone.</li> </ol> <p>B. Isolate discovery site</p> <ol style="list-style-type: none"> <li>1. Work shall be directed away from the location of the discovery and any nearby area reasonably suspected to overlay adjacent human remains until a determination can be made by the Medical Examiner in consultation with the PI concerning the provenance of the remains.</li> <li>2. The Medical Examiner, in consultation with the PI, will determine the need for a field examination to determine the provenance.</li> </ol>			

**9 G Tower Project (PTS# 649856)  
Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>3. If a field examination is not warranted, the Medical Examiner will determine with input from the PI, if the remains are or are most likely to be of Native American origin.</p> <p>C. If Human Remains are determined to be Native American</p> <ol style="list-style-type: none"> <li>1. The Medical Examiner will notify the NAHC within 24 hours. By law, <b>ONLY</b> the Medical Examiner can make this call.</li> <li>2. NAHC will immediately identify the person or persons determined to be the Most Likely Descendent (MLD) and provide contact information.</li> <li>3. The MLD will contact the PI within 24 hours or sooner after the Medical Examiner has completed coordination, to begin the consultation process in accordance with CEQA Section 15064.5(e) and the California Public Resources and Health &amp; Safety Codes.</li> <li>4. The MLD will have 48 hours to make recommendations to the property owner or representative, for the treatment or disposition with proper dignity, of the human remains and associated grave goods.</li> <li>5. Disposition of Native American Human Remains will be determined between the MLD and the PI, and if: <ol style="list-style-type: none"> <li>(a) The NAHC is unable to identify the MLD, OR the MLD failed to make a recommendation within 48 hours after being notified by the Commission; OR;</li> <li>(b) The landowner or authorized representative rejects the recommendation of the MLD and mediation in accordance with PRC 5097.94 (k) by the NAHC fails to provide measures acceptable to the landowner, THEN,</li> <li>(c) In order to protect these sites, the Landowner shall do one or more of the following: <ol style="list-style-type: none"> <li>(1) Record the site with the NAHC;</li> <li>(2) Record an open space or conservation easement on the site;</li> <li>(3) Record a document with the County.</li> </ol> </li> </ol> </li> </ol>			



**9 G Tower Project (PTS# 649856)  
Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>6. Upon the discovery of multiple Native American human remains during a ground disturbing land development activity, the landowner may agree that additional conferral with descendants is necessary to consider culturally appropriate treatment of multiple Native American human remains. Culturally appropriate treatment of such a discovery may be ascertained from review of the site utilizing cultural and archaeological standards. Where the parties are unable to agree on the appropriate treatment measures the human remains and buried with Native American human remains shall be reinterred with appropriate dignity, pursuant to Section 5.c., above.</p> <p>D. If Human Remains are not Native American</p> <ol style="list-style-type: none"> <li>1. The PI shall contact the Medical Examiner and notify them of the historic era context of the burial.</li> <li>2. The Medical Examiner will determine the appropriate course of action with the PI and City staff (PRC 5097.98).</li> <li>3. If the remains are of historic origin, they shall be appropriately removed and conveyed to the San Diego Museum of Man for analysis. The decision for internment of the human remains shall be made in consultation with City Staff, the applicant/landowner and the San Diego Museum of Man.</li> </ol> <p><b>V. Night and/or Weekend Work</b></p> <p>A. If night and/or work is included in the contract</p> <ol style="list-style-type: none"> <li>1. When night and/or weekend work is included in the contract package, the extent and timing shall be presented and discussed at the Precon Meeting.</li> <li>2. The following procedures shall be followed. <ol style="list-style-type: none"> <li>(a) No Discoveries In the event that no discoveries were encountered during night and/or weekend work, the PI shall record the information on the CSVr and submit to City Staff via fax by 8 am of the next business day.</li> <li>(b) Discoveries</li> </ol> </li> </ol>			

**9 G Tower Project (PTS# 649856)  
Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>All discoveries shall be processed and documented using the existing procedures detailed in Sections III - During Construction, and IV – Discovery of Human Remains. Discovery of human remains shall always be treated as a significant discovery.</p> <p>(c) Potentially Significant Discoveries</p> <p>If the PI determines that a potentially significant discovery has been made, the procedures detailed under Section III - During Construction and IV-Discovery of Human Remains shall be followed.</p> <p>(d) The PI shall immediately contact City Staff, or by 8 am of the next business day to report and discuss the findings as indicated in Section III-B, unless other specific arrangements have been made.</p> <p>B. If night and/or weekend work becomes necessary during the course of construction</p> <ol style="list-style-type: none"> <li>1. The Construction Manager shall notify the RE, or BI, as appropriate, a minimum of 24 hours before the work is to begin.</li> <li>2. The RE, or BI, as appropriate, shall notify City Staff immediately.</li> </ol> <p>C. All other procedures described above shall apply, as appropriate.</p> <p><b>VI. Post Construction</b></p> <p>A. Submittal of Draft Monitoring Report</p> <ol style="list-style-type: none"> <li>1. The PI shall submit two copies of the Draft Monitoring Report (even if negative) prepared in accordance with the HRG and Appendices which describes the results, analysis, and conclusions of all phases of the Archaeological Monitoring Program (with appropriate graphics) to City Staff, for review and approval within 90 days following the completion of monitoring,</li> <li>(a) For significant archaeological resources encountered during monitoring, the Archaeological Data Recovery Program shall be included in the Draft Monitoring Report.</li> </ol>			



**9 G Tower Project (PTS# 649856)  
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Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>(b) Recording sites with State of California Department of Parks and Recreation</p> <p>The PI shall be responsible for recording (on the appropriate State of California Department of Park and Recreation forms-DPR 523 A/B) any significant or potentially significant resources encountered during the Archaeological Monitoring Program in accordance with the City's Historical Resources Guidelines, and submittal of such forms to the South Coastal Information Center with the Final Monitoring Report.</p> <ol style="list-style-type: none"> <li>City Staff shall return the Draft Monitoring Report to the PI for revision or, for preparation of the Final Report.</li> <li>The PI shall submit revised Draft Monitoring Report to City Staff for approval.</li> <li>City Staff shall provide written verification to the PI of the approved report.</li> <li>City Staff shall notify the RE or BI, as appropriate, of receipt of all Draft Monitoring Report submittals and approvals.</li> </ol> <p>B. Handling of Artifacts and Submittal of Collections Management Plan, if applicable</p> <ol style="list-style-type: none"> <li>The PI shall be responsible for ensuring that all cultural remains collected are cleaned and catalogued.</li> <li>The PI shall be responsible for ensuring that all artifacts are analyzed to identify function and chronology as they relate to the history of the area; that faunal material is identified as to species; and that specialty studies are completed, as appropriate.</li> <li>The PI shall submit a Collections Management Plan to City Staff for review and approval for any project which results in a substantial collection of historical artifacts.</li> </ol> <p>C. Curation of artifacts: Accession Agreement and Acceptance Verification</p> <ol style="list-style-type: none"> <li>The PI shall be responsible for ensuring that all artifacts associated with the survey, testing and/or data recovery for this project are permanently curated with an appropriate institution. This shall be completed in consultation with City Staff and the Native American representative, as applicable.</li> </ol>			

**9 G Tower Project (PTS# 649856)  
Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>2. The PI shall include the Acceptance Verification from the curation institution in the Final Monitoring Report submitted to the RE or BI and City Staff.</p> <p>3. When applicable to the situation, the PI shall include written verification from the Native American consultant/monitor indicating that Native American resources were treated in accordance with state law and/or applicable agreements. If the resources were reinterred, verification shall be provided to show what protective measures were taken to ensure no further disturbance in accordance with section IV – Discovery of Human Remains, subsection 5.(d).</p> <p>D. Final Monitoring Report(s)</p> <p>1. The PI shall submit one copy of the approved Final Monitoring Report to the RE or BI as appropriate, and one copy to City Staff (even if negative), within 90 days after notification from City Staff that the draft report has been approved.</p> <p>2. The RE shall, in no case, issue the Notice of Completion until receiving a copy of the approved Final Monitoring Report from City Staff which includes the Acceptance Verification from the curation institution.</p>			
<b>LAND USE (LND)</b>				
<b>Impact LU-B.2</b>	Noise generated by I-5 and highly traveled grid streets could cause noise levels in noise-sensitive uses not governed by Title 24 to exceed 45 dB(A). (Direct)			
	Mitigation Measures NOI-B.1-1 and NOI-C.1.1, as described below.	<p>Prior to Building Permit (Design)</p> <p>Prior to Certificate of Occupancy (Implementation)</p>	Developer	Civic San Diego/City



**9 G Tower Project (PTS# 649856)  
Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
<b>Impact LU-B.3</b>	Noise levels in Downtown areas within the 65 CNEL contour of SDIA could exceed 45 dB(A) for noise sensitive uses not covered by Title 24. (Direct)			
	Mitigation Measures NOI-B.1-1, as described below.	Prior to Building Permit (Design)  Prior to Certificate of Occupancy (Implementation)	Developer	Civic San Diego/City
<b>Impact LU-B.4</b>	Noise generated by train horns, engines and wheels as well as bells at crossing gates would significantly disrupt sleep of residents along the railroad tracks. (Direct)			
	<b>Mitigation Measure LU-B.4-1:</b> Prior to approval of a Building Permit which would expose habitable rooms to disruptive railroad noise, an acoustical analysis shall be performed. The analysis shall determine the expected exterior and interior noise levels related to railroad activity. As feasible, noise attenuation measures shall be identified which would reduce noise levels to 45 dB(A) CNEL or less in habitable rooms. Recommended measures shall be incorporated into building plans before approval of a Building Permit.	Prior to Building Permit (Design)  Prior to Certificate of Occupancy (Implementation)	Developer	City
<b>NOISE (NOI)</b>				
<b>Impact NOI-B.1</b>	Noise generated by I-5 and highly traveled grid streets could cause interior noise levels in noise-sensitive uses (exclusive of residential and hotel uses) to exceed 45 dB(A). (Direct)			
	<b>Mitigation Measure NOI-B.1-1:</b> Prior to approval of a Building Permit for any residential, hospital, or hotel within 475 feet of the centerline of Interstate 5 or adjacent to a roadway carrying more than 7,000 ADT, an acoustical analysis shall be performed to confirm that architectural or other design features are included which would assure that noise levels within habitable rooms would not exceed 45 dB(A) CNEL.	Prior to Building Permit (Design)  Prior to Certificate of Occupancy (Implementation)	Developer	Civic San Diego/City

**9 G Tower Project (PTS# 649856)**  
**Downtown FEIR/SEIR**  
**Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility



**9 G Tower Project (PTS# 649856)**  
**Downtown FEIR/SEIR**  
**Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
<b>PALEONTOLOGICAL RESOURCES (PAL)</b>				
<b>Impact PAL-A.1</b>	Excavation in geologic formations with a moderate to high potential for paleontological resources could have an significant impact on these resources, if present. (Direct)			

	<p><b>Mitigation Measure PAL-A.1-1:</b> In the event the Secondary Study indicates the potential for significant paleontological resources, the following measures shall be implemented as determined appropriate by Civic San Diego.</p> <p><b>I. Prior to Permit Issuance</b></p> <p>A. Construction Plan Check</p> <ol style="list-style-type: none"> <li>1. Prior to NTP for any construction permits, including but not limited to, the first Grading Permit, Demolition Permits and Building Permits, but prior to the first preconstruction meeting, whichever is applicable, Centre City Development Corporation Civic San Diego shall verify that the requirements for paleontological monitoring have been noted on the appropriate construction documents.</li> </ol> <p>B. Letters of Qualification have been submitted to Civic San Diego</p> <ol style="list-style-type: none"> <li>1. The applicant shall submit a letter of verification to Civic San Diego identifying the PI for the project and the names of all persons involved in the paleontological monitoring program, as defined in the City of San Diego Paleontology Guidelines.</li> <li>2. Civic San Diego will provide a letter to the applicant confirming the qualifications of the PI and all persons involved in the paleontological monitoring of the project.</li> <li>3. Prior to the start of work, the applicant shall obtain approval from Civic San Diego for any personnel changes associated with the monitoring program.</li> </ol> <p><b>II. Prior to Start of Construction</b></p> <p>A. Verification of Records Search</p> <ol style="list-style-type: none"> <li>1. The PI shall provide verification to Civic San Diego that a site-specific records search has been completed. Verification includes, but is not limited to a copy of a confirmation letter from San Diego Natural History Museum, other institution or, if the search was in-house, a letter of verification from the PI stating that the search was completed.</li> <li>2. The letter shall introduce any pertinent information concerning expectations and probabilities of discovery during trenching and/or grading activities.</li> </ol> <p>B. PI Shall Attend Precon Meetings</p> <ol style="list-style-type: none"> <li>1. Prior to beginning any work that requires monitoring, the Applicant shall arrange a Precon Meeting that shall include the PI, CM and/or Grading Contractor, RE, BI, if appropriate, and Civic San Diego. The qualified paleontologist shall attend any grading/excavation related Precon Meetings to make comments and/or suggestions</li> </ol>			
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**9 G Tower Project (PTS# 649856)  
Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>concerning the paleontological monitoring program with the Construction Manager and/or Grading Contractor.</p> <p>a. If the PI is unable to attend the Precon Meeting, the Applicant shall schedule a focused Precon Meeting with Civic San Diego, the PI, RE, CM or BI, if appropriate, prior to the start of any work that requires monitoring.</p> <p>2. Identify Areas to be Monitored</p> <p>a. Prior to the start of any work that requires monitoring, the PI shall submit a Paleontological Monitoring Exhibit (PME) based on the appropriate construction documents (reduced to 11 by 17 inches) to Civic San Diego identifying the areas to be monitored including the delineation of grading/excavation limits. The PME shall be based on the results of a site specific records search as well as information regarding existing known soil conditions (native or formation).</p> <p>3. When Monitoring Will Occur</p> <p>a. Prior to the start of any work, the PI shall also submit a construction schedule to Civic San Diego through the RE indicating when and where monitoring will occur.</p> <p>b. The PI may submit a detailed letter to Civic San Diego prior to the start of work or during construction requesting a modification to the monitoring program. This request shall be based on relevant information such as review of final construction documents which indicate conditions such as depth of excavation and/or site graded to bedrock, presence or absence of fossil resources, etc., which may reduce or increase the potential for resources to be present.</p> <p><b>III. During Construction</b></p> <p>A. Monitor Shall be Present During Grading/Excavation/Trenching</p> <p>1. The monitor shall be present full-time during grading/excavation/trenching activities as identified on the PME that could result in impacts to formations with high and moderate resource sensitivity. The Construction Manager is responsible for notifying the RE, PI, and Civic San Diego of changes to any construction activities.</p>			

**9 G Tower Project (PTS# 649856)  
Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>2. The monitor shall document field activity via the CSV. The CSV's shall be faxed by the CM to the RE the first day of monitoring, the last day of monitoring, monthly (Notification of Monitoring Completion), and in the case of any discoveries. The RE shall forward copies to Civic San Diego.</p> <p>3. The PI may submit a detailed letter to Civic San Diego during construction requesting a modification to the monitoring program when a field condition such as trenching activities that do not encounter formational soils as previously assumed, and/or when unique/unusual fossils are encountered, which may reduce or increase the potential for resources to be present.</p> <p>B. Discovery Notification Process</p> <p>1. In the event of a discovery, the Paleontological Monitor shall direct the contractor to temporarily divert trenching activities in the area of discovery and immediately notify the RE or BI, as appropriate.</p> <p>2. The Monitor shall immediately notify the PI (unless Monitor is the PI) of the discovery.</p> <p>3. The PI shall immediately notify Civic San Diego by phone of the discovery, and shall also submit written documentation to Civic San Diego within 24 hours by fax or email with photos of the resource in context, if possible.</p> <p>C. Determination of Significance</p> <p>1. The PI shall evaluate the significance of the resource.</p> <p>a. The PI shall immediately notify Civic San Diego by phone to discuss significance determination and shall also submit a letter to Civic San Diego indicating whether additional mitigation is required. The determination of significance for fossil discoveries shall be at the discretion of the PI.</p> <p>b. If the resource is significant, the PI shall submit a Paleontological Recovery Program and obtain written approval from Civic San Diego. Impacts to significant resources must be mitigated before ground disturbing activities in the area of discovery will be allowed to resume.</p>			



**9 G Tower Project (PTS# 649856)  
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Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>c. If resource is not significant (e.g., small pieces of broken common shell fragments or other scattered common fossils) the PI shall notify the RE, or BI as appropriate, that a non-significant discovery has been made. The Paleontologist shall continue to monitor the area without notification to Civic San Diego unless a significant resource is encountered.</p> <p>d. The PI shall submit a letter to Civic San Diego indicating that fossil resources will be collected, curated, and documented in the Final Monitoring Report. The letter shall also indicate that no further work is required.</p> <p><b>IV. Night Work</b></p> <p>A. If night work is included in the contract</p> <ol style="list-style-type: none"> <li>1. When night work is included in the contract package, the extent and timing shall be presented and discussed at the precon meeting.</li> <li>2. The following procedures shall be followed. <ol style="list-style-type: none"> <li>a. No Discoveries <p>(1)In the event that no discoveries were encountered during night work, The PI shall record the information on the CSVr and submit to Civic San Diego via fax by 9 a.m. the following morning, if possible.</p> </li> <li>b. Discoveries <p>(1)All discoveries shall be processed and documented using the existing procedures detailed in Sections III - During Construction.</p> </li> <li>c. Potentially Significant Discoveries <p>(1)If the PI determines that a potentially significant discovery has been made, the procedures detailed under Section III - During Construction shall be followed.</p> </li> <li>d. The PI shall immediately contact Civic San Diego, or by 8 a.m. the following morning to report and discuss the findings as indicated in Section III-B, unless other specific arrangements have been made.</li> </ol> </li> </ol>			

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Downtown FEIR/SEIR  
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Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>B. If night work becomes necessary during the course of construction</p> <ol style="list-style-type: none"> <li>1. The Construction Manager shall notify the RE, or BI, as appropriate, a minimum of 24 hours before the work is to begin.</li> <li>2. The RE, or BI, as appropriate, shall notify Civic San Diego immediately.</li> </ol> <p>C. All other procedures described above shall apply, as appropriate.</p> <p><b>V. Post Construction</b></p> <p>A. Submittal of Draft Monitoring Report</p> <ol style="list-style-type: none"> <li>1. The PI shall submit two copies of the Draft Monitoring Report (even if negative) which describes the results, analysis, and conclusions of all phases of the Paleontological Monitoring Program (with appropriate graphics) to Civic San Diego for review and approval within 90 days following the completion of monitoring, <ol style="list-style-type: none"> <li>a. For significant paleontological resources encountered during monitoring, the Paleontological Recovery Program shall be included in the Draft Monitoring Report.</li> <li>b. Recording Sites with the San Diego Natural History Museum <ol style="list-style-type: none"> <li>(1) The PI shall be responsible for recording (on the appropriate forms) any significant or potentially significant fossil resources encountered during the Paleontological Monitoring Program in accordance with the City's Paleontological Guidelines, and submittal of such forms to the San Diego Natural History Museum with the Final Monitoring Report.</li> </ol> </li> </ol> </li> <li>2. Civic San Diego shall return the Draft Monitoring Report to the PI for revision or, for preparation of the Final Report.</li> </ol>			



**9 G Tower Project (PTS# 649856)  
Downtown FEIR/SEIR  
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Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<ol style="list-style-type: none"> <li>3. The PI shall submit revised Draft Monitoring Report to Civic San Diego for approval.</li> <li>4. Civic San Diego shall provide written verification to the PI of the approved report.</li> <li>5. Civic San Diego shall notify the RE or BI, as appropriate, of receipt of all Draft Monitoring Report submittals and approvals.</li> </ol> <p>B. Handling of Fossil Remains</p> <ol style="list-style-type: none"> <li>1. The PI shall be responsible for ensuring that all fossil remains collected are cleaned and catalogued.</li> <li>2. The PI shall be responsible for ensuring that all fossil remains are analyzed to identify function and chronology as they relate to the geologic history of the area; that faunal material is identified as to species; and that specialty studies are completed, as appropriate</li> </ol> <p>C. Curation of fossil remains: Deed of Gift and Acceptance Verification</p> <ol style="list-style-type: none"> <li>1. The PI shall be responsible for ensuring that all fossil remains associated with the monitoring for this project are permanently curated with an appropriate institution.</li> <li>2. The PI shall include the Acceptance Verification from the curation institution in the Final Monitoring Report submitted to the RE or BI and Civic San Diego.</li> </ol> <p>D. Final Monitoring Report(s)</p> <ol style="list-style-type: none"> <li>1. The PI shall submit two copies of the Final Monitoring Report to Civic San Diego (even if negative), within 90 days after notification from Civic San Diego that the draft report has been approved.</li> <li>2. The RE shall, in no case, issue the Notice of Completion until receiving a copy of the approved Final Monitoring Report from Civic San Diego which includes the Acceptance Verification from the curation institution.</li> </ol>			
<b>TRAFFIC AND CIRCULATION (TRF)</b>				
<b>Impact TRF-A.1.1</b>	Increased traffic on grid streets from Downtown development would result in unacceptable levels of service on specific roadway intersections and/or segments within downtown. (Direct)			

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Downtown FEIR/SEIR  
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Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<b>Mitigation Measure TRF-A.1.1-2:</b> Prior to approval of any development which would generate a sufficient number of trips to qualify as a large project under the Congestion Management Program (i.e. more than 2,400 daily trips, or 200 trips during a peak hour period), a traffic study shall be completed. The traffic study shall be prepared in accordance with City's Traffic Impact Study Manual. If the traffic study indicates that roadways substantially affected by the project would operate at LOS F with the addition of project traffic, the traffic study shall identify improvements to grid street segments and/or intersections consistent with the Downtown San Diego Mobility Plan which would be required within the next five years to achieve an acceptable LOS or reduce congestion, to the extent feasible. If the needed improvements are already included in the City of San Diego's CIP, or the equivalent, no further action shall be required. If any of the required improvements are not included in the CIP, or not expected within five years of project completion, the City of San Diego shall amend the CIP, within one year of project approval, to include the required improvements and assure that they will be implemented within five years of project completion. At Civic San Diego's discretion, the developer may be assessed a pro-rated share of the cost of improvements as a condition of project approval.	Prior to Development Permit (Design)	Developer	Civic San Diego/City
<b>Impact TRF-A.2.1-1</b>	Elimination of Cedar St. off-ramp would impact other freeway ramps by redirecting traffic to other off ramps serving downtown. (Direct)			
	<b>Mitigation Measure TRF A.2.2-1:</b> Prior to elimination of the Cedar Street off-ramp from I-5, a traffic study shall be done by Civic San Diego in consultation with the City of San Diego and Caltrans to determine the potential effects associated with elimination of the off-ramp and the conversion of Cedar Street from one- to two-way. The report shall also identify roadway modifications that would minimize potential impacts on local surface streets and I-5.	Prior to elimination of Cedar Street off-ramp (Design/Implementation)	Civic San Diego/City	Civic San Diego/City





london moeder  
advisors

Cisterra — 9<sup>th</sup> & G  
Economic Alternative Analysis

May 27, 2020

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May 27, 2020

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## RE: 9<sup>th</sup> Avenue & G Street – Economic Alternative Analysis

Cisterra Development is currently in the entitlement phase of redeveloping the parcel that houses the Farkas Fixtures Store, considered of historical significance by the City of San Diego. The site includes full frontage on G Street between 9th and 10th avenues and represents 25,000 square feet of the 60,000 square-foot block.

London Moeder Advisors has completed an economic analysis of various development alternatives for the property. The purpose of this analysis is to analyze the proposed Base Project and the financial impacts and economic feasibility of the development alternatives.

We have analyzed five development alternatives for the property, in addition to the Base Project, which include:

- ➔ **Base Project**: Maintain the existing façade of the historic structure, add one subterranean level with 19,500 net square feet of retail, construct five levels of parking above ground consisting of 276 spaces, and construct 16 levels of residential consisting of 242 units.
- ➔ **Alternative 1**: rehabilitate the existing 25,000 square foot historic structure and use as 22,000 square feet of net retail space.
- ➔ **Alternative 2**: Maintain the existing façade of the historic structure, add one subterranean level of retail for a total of 34,000 net square feet, construct a loading dock, and construct a parking ramp to roof parking consisting of 34 spaces.
- ➔ **Alternative 3**: Maintain the existing façade of the historic structure, add one subterranean level of retail for a total of 34,000 net square feet, construct five levels of parking above ground consisting of 189 spaces, and construct ten levels of residential consisting of 155 units.
- ➔ **Alternative 3B**: Maintain the existing façade of the historic structure, add one subterranean level of retail for a total of 34,000 net square feet, construct five levels of parking above ground consisting of 189 spaces, and construct 16 levels of residential consisting of 227 units, of which 72 units will not have designated parking within the building.
- ➔ **Alternative 4**: Maintain the existing façade of the historic structure, add five subterranean levels of parking consisting of 202 spaces, rehab ground floor retail and add one second

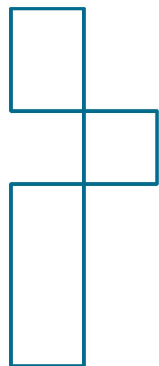


story level of retail for a total of 34,000 net square feet, and construct 11 levels of residential consisting of 168 units.

## Conclusions of Economic Alternatives

We analyzed the project performance of the Base Project that is proposed for the property. The Base Project includes construction of one subterranean level of retail space, five levels of above ground parking, and 16 levels of residential units. The average size of the residential units is 905 square feet.

We have assumed a 21-month construction period with the lease up of residential units commencing immediately after completion, including two months of pre-leasing. The project will be sold after a five-year holding period. The following table summarizes the impacts to the Base Project under each of the five alternatives:



**Cisterra - 9th & G**  
**Summary of Scenarios**

**Base Project**  
*22 Story Tower*

<u>Rentable Space:</u>	
Residential	242 units
Parking	276 spaces
Retail	34,000 SF
Total Net Rentable	253,000 SF
Total Gross	426,650 SF

Total Profit	\$45,955,665
Stabilized Yield On Cost	5.6%
IRR	16%

**Alternative 1**  
*Refurbish Existing Building*

<u>Rentable Space:</u>	
Residential	0 units
Parking	0 spaces
Retail	22,000 SF
Total Net Rentable	22,000 SF
Total Gross	25,000 SF

Total Profit	\$2,757,756
Difference from Base (\$)	-\$43,197,909
Difference from Base (%)	-94.0%
Stabilized Yield On Cost	3.9%
IRR	9%

**Alternative 2**  
*Retain Façade + Rooftop Parking*

<u>Rentable Space:</u>	
Residential	0 units
Parking	34 spaces
Retail	34,000 SF
Total Net Rentable	34,000 SF
Total Gross	42,000 SF

Total Profit	(\$2,538,708)
Difference from Base (\$)	-\$48,494,373
Difference from Base (%)	-105.5%
Stabilized Yield On Cost	3.4%
IRR	-4%

**Alternative 3**  
*16 Story Tower*

<u>Rentable Space:</u>	
Residential	155 units
Parking	189 spaces
Retail	34,000 SF
Total Net Rentable	173,400 SF
Total Gross	298,060 SF

Total Profit	\$15,145,802
Difference from Base (\$)	-\$30,809,863
Difference from Base (%)	-67.0%
Stabilized Yield On Cost	4.7%
IRR	7%

**Alternative 3B**  
*22 Story Tower*

<u>Rentable Space:</u>	
Residential	227 units
Parking	189 spaces
Retail	34,000 SF
Total Net Rentable	238,325 SF
Total Gross	373,069 SF

Total Profit	\$30,600,518
Difference from Base (\$)	-\$15,355,147
Difference from Base (%)	-33.4%
Stabilized Yield On Cost	4.7%
IRR	10%

**Alternative 4**  
*13 Story Tower*

<u>Rentable Space:</u>	
Residential	168 units
Parking	202 spaces
Retail	34,000 SF
Total Net Rentable	185,200 SF
Total Gross	347,400 SF

Total Profit	(\$12,279,294)
Difference from Base (\$)	-\$58,234,959
Difference from Base (%)	-126.7%
Stabilized Yield On Cost	3.9%
IRR	-6%

Source: London Moeder Advisors



We have determined that only the Base Project is economically feasible. This project is forecasted to generate a total profit of approximately \$46 million. In addition, this project is estimated to generate a Net Operating Income ("NOI") at stabilization of \$7,954,860, which when compared to the total costs of the project represents a Yield On Cost ("YOC") of 5.6%.

Based on performing feasibility analyses and consulting services on hundreds of real estate projects, it is our experience that a redevelopment project requires the Yield On Costs spread over existing cap rates to be 1.5% to be economically feasible and to qualify for project financing. Meaning if cap rates are approximately 4% for residential projects and approximately 6% for retail projects the targeted YOC is 5.5% and 7.5% respectively.

The internal rate of return ("IRR") of the Base Project is forecasted to be 16.5%. This also demonstrates that the project is economically feasible. The typical minimum IRR for rental housing projects ranges from 13% to 15%. Any IRR below this range would struggle to attract investors and achieve project financing.

Alternative 1 is not economically feasible. The refurbishing of the existing retail building results in an NOI at stabilization of \$566,620. When compared to the total costs of the project the YOC is 3.9%, which is below the 7.5% YOC threshold required. In addition, the total profit is reduced by \$43.2 million (-94.0%) compared to the Base Project.

Alternative 2 is not economically feasible. Due to the high cost of subterranean construction without the addition of sufficient revenue producing density, the estimated NOI at stabilization is \$952,796. When compared to the total costs of the project the YOC is 3.4%, which is below the 7.5% YOC threshold required. In addition, the total profit is reduced by \$48.5 million (-105.5%) compared to the Base Project. This alternative also results in a financial loss for the developer of \$2,538,708.

Alternative 3 is not economically feasible. Due to the expensive costs of high-rise construction without the addition of sufficient revenue producing density, the estimated NOI at stabilization is \$4,973,574. When compared to the total costs of the project the YOC is 4.7%, which is below the 5.5% YOC threshold required. The total profit in this alternative is also reduced by \$30.8 million (-67.0%) compared to the Base Project. In addition, the IRR for this alternative is only 7.2%, which is below the minimum targeted IRR of 13% to 15%.

Alternative 3B is not economically feasible. Due to the expensive costs of high-rise construction without the addition of sufficient revenue producing density, the estimated NOI at stabilization is \$6,482,227. When compared to the total costs of the project the YOC is 4.7%, which is below the 5.5% YOC threshold required. The total profit in this alternative is also reduced by \$15.4 million (-33.4%) compared to the Base Project. In addition, the IRR for this alternative is only 10.2%, which is below the minimum targeted IRR of 13% to 15%. A critical factor in this alternative is that 72 units will not have parking spaces provided. This represents a financial challenge because no projects have been financed without parking. It is likely that financing terms could be less favorable than the assumptions in this report for projects without parking due to increased marketability risk.

Alternative 4 is not economically feasible. Due to the expensive costs of high-rise and subterranean construction without the addition of sufficient revenue producing density and increased rents, the estimated NOI at stabilization is \$4,907,520. When compared to the total costs of the project the YOC is 3.9%, which is below the 5.5% YOC threshold required. The total profit in this alternative is also reduced by \$58.2 million (-126.7%) compared to the Base Project. In addition, the IRR for this alternative is negative 6% and represents a financial loss for the developer of an estimated \$12.3 million.



## Approach to Analysis

To determine the impact to the project, we prepared financial proformas for the five alternatives and compared the performances to the Base Project proforma. In each proforma, we assumed the following:

- Construction period of 21 months, 6 months, 15 months, 21 months, 19.25 months, and 23 months for the Base Case and Alternative 1 to Alternative 4, respectively.
- Rental residential units begin leasing immediately after construction is completed with two months of pre-leasing.
- Construction costs are provided by the developer and London Moeder Advisors based on similar projects and construction types.
- Rental rates and revenues were established by our survey of market rents for competitive projects in the area.
- Residential rental units that do not offer parking are discounted at an average of \$200 per month to reflect the cost of finding replacement parking within a four-block radius. The typical cost of monthly parking in a four-block radius ranges from \$165 to \$220 per space.
- Residential rental units that offer parking is estimated to stabilize at a 4.5% vacancy rate.
- Residential rental units that do not offer parking are more difficult to lease than those with parking; therefore, we have assumed these units will stabilize at a 10% vacancy rate.
- Rental residential units achieve a \$50 per floor rent premium per month. Top floor units achieve a \$100 per floor rent premium per month.
- The following summarizes the financial proformas we have prepared for analyzing the project, which are included in the [Appendix](#).

### Base Project

The Base Project includes retaining the existing façade of the historic structure, the addition of one subterranean level of retail space, construction of five levels of parking above the existing structure, and 16 levels of residential units above the levels of parking. The 242 residential units are to begin leasing after construction is complete with two months of pre-leasing. The project is to be sold after a five-year holding period.

The 242 market rate units include an average of 905 square feet. There will be a total of 276 parking spaces included in the parking levels of the building. In addition, there will be 19,500 square feet of below grade retail and 14,500 square feet of retail on the first level of the building.

When the 242 market rate units are leased after construction is completed, the forecasted average rent is estimated to be \$3.72 per square foot. The 34,000 square feet of total retail space is estimated to rent at \$3.37 per square foot.

The total gross profit generated from this investment is forecasted to be \$45,955,665. In addition, this project is estimated to generate an NOI at stabilization of \$7,954,860, which when compared

to the total costs of the project represents a YOC of 5.6%, which satisfies the minimum requirement of 5.5% for project feasibility.

The IRR of the investment is forecasted to be 16.5%. This also demonstrates that the project is economically feasible. The typical minimum IRR for rental housing projects ranges from 13% to 15%. Any IRR below this range would struggle to attract investors and achieve project financing.

### **Alternative 1**

Alternative 1 assumes rehabilitation of the existing warehouse retail structure (25,000 square feet) and returning the space to retail operations. The retail space is assumed to be leased immediately after construction is completed and the project sold after a five-year holding period.

The 22,000 square feet of retail space is estimated to rent at \$2.75 per square foot (today's dollars).

The forecasted sale price for the entire project is \$14,537,297. Total project costs are forecasted at \$14,627,435.

**With a total forecasted profit at disposition of \$2,757,756, Alternative 1 would generate approximately \$43,197,909 less total profit than the Base Project (94% reduction). But more importantly the project is not economically feasible because the forecasted YOC of 3.9% is below the minimum requirement of 7.5% for retail properties.**

### **Alternative 2**

Alternative 2 assumes retaining the existing façade of the historic structure, the addition of one subterranean level of retail space, and the replacement of the existing roof structure with rooftop parking. The retail space is assumed to be leased immediately after construction is completed and the project sold after a five-year holding period.

The 34,000 square feet of retail space is estimated to rent at \$3.00 per square foot (today's dollars).

The forecasted sale price for the entire project is \$24,509,162. Total project costs are forecasted at \$27,989,507. This results in a financial loss for the project, which is forecasted to be \$2,538,708 and therefore not economically feasible.

**With a total forecasted profit at disposition of negative \$2,538,708, Alternative 2 would generate approximately \$48,494,373 less total profit than the Base Project (105% reduction).**

### **Alternative 3**

Alternative 3 assumes retaining the existing façade of the historic structure, the addition of one subterranean level of retail space, construction of five levels of parking above the existing structure, and 10 levels of residential units above the levels of parking. The 155 residential units are to begin



leasing immediately after construction is complete with two months of pre-leasing. The project is assumed to be sold after a five-year holding period.

The 155 market rate units average 899 square feet in size. There will be a total of 189 parking spaces included in the parking levels of the building. In addition, there will be 19,500 square feet of below grade retail and 14,500 square feet of retail on the first level of the building.

When the 155 market rate units are leased after construction is completed, the forecasted average rent is estimated to be \$3.56 per square foot. The 34,000 square feet of total retail space is estimated to rent at \$3.37 per square foot.

The forecasted sale price for the entire project is \$116,078,147. Total project costs are forecasted at \$105,590,612.

With a total forecasted profit at disposition of \$15,145,802, Alternative 3 would generate approximately \$30,809,863 less total profit than the Base Project (67% reduction). But more importantly the project is not economically feasible because the forecasted YOC (4.7%) does not meet the minimum required of 5.5% to be feasible.

To further illustrate the infeasibility of Alternative 3, the IRR of this project is forecasted to be 7%. This also demonstrates that the project is infeasible because an IRR below 13% to 15% will struggle to attract investors and qualify for project financing.

### **Alternative 3B**

Alternative 3B assumes retaining the existing façade of the historic structure, the addition of one subterranean level of retail space, construction of five levels of parking above the existing structure, and 16 levels of residential units above the levels of parking. The 227 residential units are to begin leasing immediately after construction is complete with two months of pre-leasing. The project is assumed to be sold after a five-year holding period.

The 227 market rate units average 900 square feet in size. There will be a total of 189 parking spaces included in the parking levels of the building. In addition, there will be 19,500 square feet of below grade retail and 14,500 square feet of retail on the first level of the building.

When the 227 market rate units are leased after construction is completed, the forecasted average rent is estimated to be \$3.73 per square foot for units with parking and \$3.50 per square foot for units without parking. The 34,000 square feet of total retail space is estimated to rent at \$3.37 per square foot.

The forecasted sale price for the entire project is \$159,276,163. It is important to note that the assumed exit cap rate is 4.75% (25 basis point above market) to reflect the 72 units that do not have parking provided. Projects that do not provide parking represent increased marketability risk to the developer and lender. Total project costs are forecasted at \$136,477,630.

With a total forecasted profit at disposition of \$30,600,518, Alternative 3B would generate approximately \$15,355,147 less total profit than the Base Project (33.4% reduction). But more importantly the project is not economically feasible because the forecasted YOC (4.7%) does not meet the minimum required of 5.5% to be feasible.

To further illustrate the infeasibility of Alternative 3B, the IRR of this project is forecasted to be 10.2%. This also demonstrates that the project is infeasible because an IRR below 13% to 15% will struggle to attract investors and qualify for project financing.

A critical factor in this alternative is that 72 units will not have parking spaces provided. This represents a financial challenge because no projects have been financed without parking. It is likely that financing terms could be less favorable than the assumptions in this report for projects without parking due to increased marketability risk.

## Alternative 4

Alternative 4 assumes retaining the existing façade of the historic structure, the addition of five subterranean levels of parking, rehabilitation of ground floor retail and the addition of one second story level of retail for a total of 34,000 net square feet, and construction of 11 levels of residential units above. The 168 residential units are to begin leasing immediately after construction is complete with two months of pre-leasing. The project is assumed to be sold after a five-year holding period.

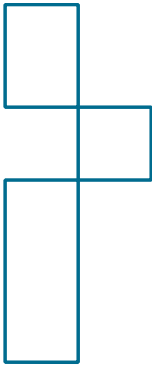
The 168 market rate units average 900 square feet in size. There will be a total of 202 parking spaces included in the parking levels of the building. In addition, there will be 14,000 square feet of retail on the first level, and 20,000 square feet of retail on the second level of the building.

When the 168 market rate units are leased after construction is completed, the forecasted average rent is estimated to be \$3.30 per square foot. The 34,000 square feet of total retail space is estimated to rent at \$3.37 per square foot.

The forecasted sale price for the entire project is \$110,749,569. Total project costs are forecasted at \$124,727,164. This results in a financial loss for the project, which is forecasted to be \$12,279,294 and therefore not economically feasible.

With a total forecasted loss at disposition of \$12,279,294, Alternative 4 would generate approximately \$58,234,959 less total profit than the Base Project (127% reduction). In addition, the IRR is forecasted to be negative 6%, and will therefore struggle to attract investors and qualify for project financing.





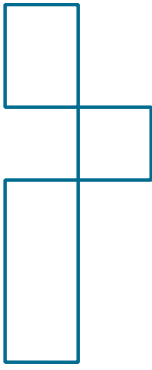
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Should you have any questions regarding this analysis, please feel free to contact our firm.

Sincerely,

Gary H. London

Nathan Moeder



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# Appendix



**Cisterra - 9th & G**  
**Base Project - 22 Story Tower**  
 Assumptions & Results

**HOLDING & DISPOSITION**

Holding Period:	5
Cap Rate On Sale (Residential):	4.50%
Cap Rate On Sale (Retail):	5.50%
Commissions & Closing Costs:	1.00%
Value at Time of Sale (Year 5)	\$0
Asset Value PSF	\$0

**BUILDING ASSUMPTIONS**

Project FAR	17.07
Units Per Acre	422
Land S.F.	25,000
Building Gross SF	426,650
Building FAR/NSF Efficiency	59.30%
Net Rentable Area	253,000
Parking Spaces	276
Total # of Units	242

**FINANCING**

<b>Construction Financing:</b>	
Loan Amount	\$92,824,932
Loan to Cost	65%
Interest Rate	5.75%
Term (Months)	24
<b>Refinance:</b>	
Refinance at End of Year:	2
Permanent Loan Amount	\$112,165,274
Less: Construction Loan	(\$92,824,932)
Less: Loan Fees	0.50% (\$560,826.37)
Net Proceeds From Refinance	\$18,779,516
<b>Permanent Loan Info:</b>	
Loan Amount	\$112,165,274
Amortization	30
Interest Rate	4.5%
Annual Debt Service	\$6,819,900
Next Year NOI @ Refi	\$7,210,625
Value at Refi	\$160,236,106
Loan To Value	70%
Debt Coverage Ratio	1.17
Debt Yield	6.43%

**RESIDUAL LAND VALUE**

Land S.F.	25,000
Land Value	\$10,000,000
\$/S.F. of Land	\$400
\$/Unit	\$41,322

**PROJECT SUMMARY**

Residential	% of		Total		Avg.	Total	\$/S.F.
Market Rate	# of Units	Total Mix	Unit Size	Net Rentable	Monthly Rent	Annual Rent	Rent
Studio	60	24.8%	700	42,000	\$2,500	\$1,800,000	\$3.57
1 Bed	52	21.5%	800	41,600	\$3,000	\$1,872,000	\$3.75
1 Bed + D	65	26.9%	900	58,500	\$3,300	\$2,574,000	\$3.67
2 Bed	40	16.5%	1,110	44,400	\$3,600	\$1,728,000	\$3.24
3 Bed	25	10.3%	1,300	32,500	\$4,100	\$1,230,000	\$3.15
Subtotal	242	100.0%	905	219,000	\$3,169	\$9,204,000	\$3.50
Affordable Units <sup>1</sup>							
Studio	0	0.0%	0	0	\$0	\$0	
Small 1 Bed	0	0.0%	0	0	\$0	\$0	
Large 1 Bed	0	0.0%	0	0	\$0	\$0	
2 Bed	0	0.0%	0	0	\$0	\$0	
Subtotal	0	0.0%		0		\$0	
Total	242	100.0%	905	219,000		\$9,204,000	
	Space	Avg.	Total	Total			
	276 spaces	Monthly Rent	Monthly Rent	Annual Rent			
Parking		\$0	\$0	\$0			
Retail	34,000 SF	\$3.18 PSF	\$108,120	\$1,297,440			
Other Income	\$0						

**CONSTRUCTION COSTS**

	Total Cost	Cost Per Unit	Cost Per Gross S.F.
Land Costs	\$10,000,000	\$41,322	\$23.44
<b>Hard Costs</b>			
Below Grade Retail	\$3,987,750	\$16,478	\$204.50
Above Grade Parking	\$11,637,500	\$48,089	\$95.00
Retail	\$2,240,250	\$9,257	\$154.50
Amenity	\$1,150,000	\$4,752	\$230.00
Outdoor Amenity	\$1,740,000	\$7,190	\$217.50
Type I Residential	\$64,298,875	\$265,698	\$242.50
Site	\$1,187,500	\$4,907	\$47.50
GC/GR Mark-Ups	\$14,130,735	\$58,391	\$33.12
Hard Costs Subtotal	\$100,372,610	\$414,763	\$235.26
Soft Costs	20.0% \$20,074,522	\$82,953	\$47.05
Contingency	5.0% \$6,022,357	\$24,886	\$14.12
Financing	\$6,338,100	\$26,190	\$14.86
<b>Total Project Costs</b>	<b>\$142,807,588</b>	<b>\$590,114</b>	<b>\$334.72</b>
Less: Loan Amount	\$92,824,932	\$383,574	\$217.57
Initial Investment:	\$49,982,656	\$206,540	\$117.15

**INVESTMENT PERFORMANCE**

Stabilized NOI	Year 4	\$7,954,860
Total Project Costs		\$142,807,588
<b>Stabilized Yield On Cost</b>		<b>5.57%</b>
	Return on Equity	Cash Flow
Initial		(\$49,982,656)
Year 1	0.0%	\$0
Year 2	37.6%	\$18,779,516
Year 3	0.6%	\$318,619
Year 4	2.1%	\$1,055,412
Year 5	151.6%	\$75,784,774
<b>Total Profit</b>		<b>\$45,955,665</b>
<b>Before Tax IRR</b>		<b>16.5%</b>

<sup>1</sup> @ 100% AMI

Cisterra - 9th & G  
Base Project - 22 Story Tower  
Cash Flow Forecast

	Initial	Year 1	Year 2	Year 3	Year 4	Year 5	Year 6
	0	2020	2021	2022	2023	2024	2025
		1	2	3	4	5	6
Total Market Rate Units				242	242	242	242
Units Leased (Market Rate)				217	231	231	231
Units Leased (Affordable)		Construction	Construction	0	0	0	0
Units Vacant (Market Rate)				26	11	11	11
Occupancy Rate (Market Rate)				89.5%	95.5%	95.5%	95.5%
Vacancy Rate (Market Rate)				10.5%	4.5%	4.5%	4.5%
Monthly Rent (Market Rate)		\$3,169	\$3,265	\$3,362	\$3,463	\$3,567	\$3,674
Monthly Rent Per S.F. (Market Rate)		\$3.50	\$3.61	\$3.72	\$3.83	\$3.94	\$4.06
Monthly Rent Per space (Parking)		\$0	\$0	\$0	\$0	\$0	\$0
Monthly Rent Per S.F. (Retail)		\$3.18	\$3.28	\$3.37	\$3.47	\$3.58	\$3.69
Annual Increase In Rent			3.0%	3.0%	3.0%	3.0%	3.0%
Gross Rental Income (Market Rate Units)				\$9,764,524	\$10,057,459	\$10,359,183	\$10,669,959
Gross Rental Income (Affordable Units)							
Gross Parking Income				\$0	\$0	\$0	\$0
Gross Retail Income (NNN)				\$1,376,454	\$1,417,748	\$1,460,280	\$1,504,089
Other Income				\$0	\$0	\$0	\$0
Less: Vacancy & Credit Loss (Parking)	5.0%						
Less: Vacancy & Credit Loss (Retail)	5.0%						
Less: Vacancy & Credit Loss (Residential)				(\$68,823)	(\$70,887)	(\$73,014)	(\$75,204)
Subtotal: Net Rental Income				(\$1,028,906)	(\$457,157)	(\$470,872)	(\$484,998)
				\$10,043,249	\$10,947,162	\$11,275,577	\$11,613,845
<b>Total: Net Rental Income</b>				<b>\$10,043,249</b>	<b>\$10,947,162</b>	<b>\$11,275,577</b>	<b>\$11,613,845</b>
Less: Operating Expenses <sup>1</sup>	Per Unit	% Increase					
	(\$6,000)	2.5%	\$0	\$0	(\$1,398,881)	(\$1,529,885)	(\$1,568,132)
Less: Property Taxes <sup>2</sup>	(\$5,583)	2.0%	\$0	\$0	(\$1,433,743)	(\$1,462,418)	(\$1,491,666)
<b>Operating Expenses - Residential</b>	<b>(\$11,583)</b>		<b>\$0</b>	<b>\$0</b>	<b>(\$2,832,624)</b>	<b>(\$2,992,302)</b>	<b>(\$3,059,798)</b>
<b>Net Operating Income</b>			<b>\$0</b>	<b>\$0</b>	<b>\$7,210,625</b>	<b>\$7,954,860</b>	<b>\$8,215,779</b>
Less: Capital Expenditures			\$0	\$0	(\$72,106)	(\$79,549)	(\$82,158)
Less: I/O (interim) financing			\$0	\$0	\$0	\$0	\$0
Less: Permanent Debt Service			\$0	\$0	(\$6,819,900)	(\$6,819,900)	(\$6,819,900)
<b>Subtotal</b>			<b>\$0</b>	<b>\$0</b>	<b>(\$6,892,006)</b>	<b>(\$6,899,448)</b>	<b>(\$6,902,057)</b>
<b>Net Proceeds from Refinance:</b>			<b>\$0</b>	<b>\$18,779,516</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>
<b>Cash Flow From Operations</b>			<b>\$0</b>	<b>\$18,779,516</b>	<b>\$318,619</b>	<b>\$1,055,412</b>	<b>\$1,313,722</b>
<b>Total Cash Flow Before Taxes</b>			<b>\$0</b>	<b>\$18,779,516</b>	<b>\$318,619</b>	<b>\$1,055,412</b>	<b>\$1,313,722</b>
<b>Disposition</b>							
Residential (incl. parking)							
Cap Rate						4.50%	
Next Year NOI						\$7,056,126	
Asset Value						\$156,802,800	
Asset Value Per Net SF						\$620	
Asset Value Per Unit						\$647,945	
Retail							
Cap Rate						5.50%	
Next Year NOI						\$1,428,884	
Asset Value						\$25,979,711	
Asset Value Per Space						\$94,129	
Sale Price						\$182,782,511	
Less: Commissions & Closing Costs						(\$1,827,825)	
Less: Principal Balance of Loan O/S						(\$106,483,634)	
<b>Net Proceeds from Disposition</b>						<b>\$74,471,052</b>	
<b>Total Cash Flow Before Taxes</b>			<b>(\$49,982,656)</b>	<b>\$0</b>	<b>\$18,779,516</b>	<b>\$318,619</b>	<b>\$1,055,412</b>

IRR 16%

Notes:

<sup>1</sup> \$500 per unit per month

<sup>2</sup> 1.1% of 90% of construction costs



**Cisterra - 9th & G**  
**Alternative 1 - Refurbish Existing Building**  
Assumptions & Results

**HOLDING & DISPOSITION**

Holding Period:	5
Cap Rate On Sale (Residential):	4.50%
Cap Rate On Sale (Retail):	5.50%
Commissions & Closing Costs:	1.00%
Value at Time of Sale (Year 5)	\$0
Asset Value PSF	\$0

**BUILDING ASSUMPTIONS**

Project FAR	1.00
Land S.F.	25,000
Building Gross SF	25,000
Building FAR/NSF Efficiency	88.00%
Net Rentable Area	22,000
Parking Spaces	0

**FINANCING**

<u>Construction Financing:</u>	
Loan Amount	\$9,507,833
Loan to Cost	65%
Interest Rate	5.75%
Term (Months)	24

<u>Refinance:</u>		YES
Refinance at End of Year:		1
Permanent Loan Amount	\$7,554,935	
Less: Construction Loan	(\$9,507,833)	
Less: Loan Fees	(\$37,774.68)	0.50%
Net Proceeds From Refinance	(\$1,990,672)	

<u>Permanent Loan Info:</u>	
Loan Amount	\$7,554,935
Amortization	30
Interest Rate	4.5%
Annual Debt Service	\$459,357
Next Year NOI @ Refi	\$566,620
Value at Refi	\$12,591,559
Loan To Value	60%
Debt Coverage Ratio	1.23
Debt Yield	7.50%

**RESIDUAL LAND VALUE**

Land S.F.	25,000
Land Value	\$10,000,000
\$/S.F. of Land	\$400

**PROJECT SUMMARY**

	Space	Avg. Monthly Rent	Total Monthly Rent	Total Annual Rent
Parking	0 spaces	\$0	\$0	\$0
Retail	22,000 SF	\$2.75 PSF	\$60,500	\$726,000
Other Income	\$0			

**CONSTRUCTION COSTS**

	Total Cost	Cost Per Gross S.F.
Land Costs	\$10,000,000	\$400.00
<b>Hard Costs</b>		
Below Grade Retail	\$0	\$0.00
Above Grade Parking	\$0	\$0.00
Retail	\$2,137,500	\$85.50
Amenity	\$0	\$0.00
Outdoor Amenity	\$0	\$0.00
Type I Residential	\$0	\$0.00
Site	\$562,500	\$22.50
G/C/GR Mark-Ups	\$441,580	\$17.50
Hard Costs Subtotal	\$3,141,580	\$125.66
Soft Costs	20.0%	\$628,316 \$25.13
Contingency	5.0%	\$188,495 \$7.54
Financing		\$669,045 \$26.76
<b>Total Project Costs</b>	<b>\$14,627,435</b>	<b>\$585.10</b>
Less: Loan Amount	\$9,507,833	\$380.31
Initial Investment:	\$5,119,602	\$204.78

**INVESTMENT PERFORMANCE**

Stabilized NOI	Year 2	\$566,620
Total Project Costs		\$14,627,435
<b>Stabilized Yield On Cost</b>		<b>3.87%</b>
	Return on Equity	Cash Flow
Initial		(\$5,119,602)
Year 1	0.0%	\$0
Year 2	2.0%	\$101,597
Year 3	2.3%	\$119,849
Year 4	2.7%	\$138,677
Year 5	146.8%	\$7,517,235
<b>Total Profit</b>		<b>\$2,757,756</b>
<b>Before Tax IRR</b>		<b>9.2%</b>

**Cisterra - 9th & G**  
**Alternative 1 - Refurbish Existing Building**

Cash Flow Forecast

	Initial 0	Year 1 2020 1	Year 2 2021 2	Year 3 2022 3	Year 4 2023 4	Year 5 2024 5	Year 6 2025 6
		Construction					
Monthly Rent Per space (Parking)		\$0	\$0	\$0	\$0	\$0	\$0
Monthly Rent Per S.F. (Retail)		\$2.75	\$2.83	\$2.92	\$3.00	\$3.10	\$3.19
Annual Increase In Rent			3.0%	3.0%	3.0%	3.0%	3.0%
Gross Parking Income			\$0	\$0	\$0	\$0	\$0
Gross Retail Income (NNN)			\$747,780	\$770,213	\$793,320	\$817,119	\$841,633
Other Income			\$0	\$0	\$0	\$0	\$0
Less: Vacancy & Credit Loss (Parking)		5.0%					
Less: Vacancy & Credit Loss (Retail)		5.0%					
Subtotal: Net Rental Income			\$710,391	\$731,703	\$753,654	\$776,263	\$799,551
<b>Total: Net Rental Income</b>			<b>\$710,391</b>	<b>\$731,703</b>	<b>\$753,654</b>	<b>\$776,263</b>	<b>\$799,551</b>
	<b>Total</b>	<b>% Increase</b>					
Less: Property Taxes <sup>1</sup>	(\$138,188.06)	2.0%	\$0	(\$143,771)	(\$146,646)	(\$149,579)	(\$152,571)
<b>Operating Expenses</b>	<b>(\$138,188)</b>		<b>\$0</b>	<b>(\$143,771)</b>	<b>(\$146,646)</b>	<b>(\$149,579)</b>	<b>(\$152,571)</b>
<b>Net Operating Income</b>			<b>\$0</b>	<b>\$566,620</b>	<b>\$585,056</b>	<b>\$604,075</b>	<b>\$623,693</b>
Less: Capital Expenditures			\$0	(\$5,666)	(\$5,851)	(\$6,041)	(\$6,237)
Less: I/O (interim) financing			\$0	\$0	\$0	\$0	\$0
Less: Permanent Debt Service			\$0	(\$459,357)	(\$459,357)	(\$459,357)	(\$459,357)
<b>Subtotal</b>			<b>\$0</b>	<b>(\$465,023)</b>	<b>(\$465,208)</b>	<b>(\$465,398)</b>	<b>(\$465,594)</b>
<b>Net Proceeds from Refinance:</b>			<b>(\$1,990,672)</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>
<b>Cash Flow From Operations</b>			<b>(\$1,990,672)</b>	<b>\$101,597</b>	<b>\$119,849</b>	<b>\$138,677</b>	<b>\$158,099</b>

**Disposition**

<u>Retail</u>	
Cap Rate	5.50%
Next Year NOI	\$799,551
Asset Value	\$14,537,297
Asset Value Per SF	\$661
Sale Price	\$14,537,297
Less: Commissions & Closing Costs	(\$145,373)
Less: Principal Balance of Loan O/S	(\$7,032,787)
<b>Net Proceeds from Disposition</b>	<b>\$7,359,137</b>
<b>Total Cash Flow Before Taxes</b>	<b>(\$5,119,602)</b>

IRR	9%
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**Notes:**

<sup>1</sup> 1.1% of 90% of construction costs



**Cisterra - 9th & G**  
**Alternative 2 - Existing Retail + Rooftop Parking**  
Assumptions & Results

**HOLDING & DISPOSITION**

Holding Period:	5
Cap Rate On Sale (Residential):	4.50%
Cap Rate On Sale (Retail):	5.50%
Commissions & Closing Costs:	1.00%
Value at Time of Sale (Year 5)	\$0
Asset Value PSF	\$0

**BUILDING ASSUMPTIONS**

Project FAR	1.68
Land S.F.	25,000
Building Gross SF	42,000
Building FAR/NSF Efficiency	80.95%
Net Rentable Area	34,000
Parking Spaces	34

**FINANCING**

<u>Construction Financing:</u>	
Loan Amount	\$18,193,179
Loan to Cost	65%
Interest Rate	5.75%
Term (Months)	24

<u>Refinance:</u>	
YES	
Refinance at End of Year:	2
Permanent Loan Amount	\$12,703,941
Less: Construction Loan	(\$18,193,179)
Less: Loan Fees	0.50% (\$63,519.70)
Net Proceeds From Refinance	(\$5,552,759)

<u>Permanent Loan Info:</u>	
Loan Amount	\$12,703,941
Amortization	30
Interest Rate	4.5%
Annual Debt Service	\$772,428
Next Year NOI @ Refi	\$952,796
Value at Refi	\$21,173,234
Loan To Value	60%
Debt Coverage Ratio	1.23
Debt Yield	7.50%

**RESIDUAL LAND VALUE**

Land S.F.	25,000
Land Value	\$10,000,000
\$/S.F. of Land	\$400

**PROJECT SUMMARY**

	Space	Avg. Monthly Rent	Total Monthly Rent	Total Annual Rent
Parking	34 spaces	\$0	\$0	\$0
Retail	34,000 SF	\$3.00 PSF	\$102,000	\$1,224,000
Other Income	\$0			

**CONSTRUCTION COSTS**

	Total Cost	Cost Per Gross S.F.
Land Costs	\$10,000,000	\$238.10
<b>Hard Costs</b>		
Below Grade Retail	\$3,501,225	\$204.75
Above Grade Parking	\$1,935,375	\$99.25
Retail	\$2,670,525	\$107.25
Amenity	\$0	\$0.00
Outdoor Amenity	\$0	\$0.00
Type I Residential	\$0	\$0.00
Site	\$937,500	\$37.50
G/C/GR Mark-Ups	\$4,232,752	\$101.00
Hard Costs Subtotal	\$13,277,377	\$316.13
Soft Costs	20.0% \$2,655,475	\$63.23
Contingency	5.0% \$796,643	\$18.97
Financing	\$1,260,012	\$30.00
<b>Total Project Costs</b>	<b>\$27,989,507</b>	<b>\$666.42</b>
Less: Loan Amount	\$18,193,179	\$433.17
Initial Investment:	\$9,796,327	\$233.25

**INVESTMENT PERFORMANCE**

Stabilized NOI	Year 3	\$952,796
Total Project Costs		\$27,989,507
<b>Stabilized Yield On Cost</b>		<b>3.40%</b>
	Return on Equity	Cash Flow
Initial		(\$9,796,327)
Year 1	0.0%	\$0
Year 2	-56.7%	(\$5,552,759)
Year 3	1.7%	\$170,840
Year 4	2.1%	\$201,918
Year 5	127.0%	\$12,437,621
<b>Total Profit</b>		<b>(\$2,538,708)</b>
<b>Before Tax IRR</b>		<b>-4.2%</b>

Cisterra - 9th & G  
Alternative 2 - Existing Retail + Rooftop Parking

Cash Flow Forecast

	Initial 0	Year 1 2020 1	Year 2 2021 2	Year 3 2022 3	Year 4 2023 4	Year 5 2024 5	Year 6 2025 6
		Construction	Construction				
Monthly Rent Per space (Parking)		\$0	\$0	\$0	\$0	\$0	\$0
Monthly Rent Per S.F. (Retail)		\$3.00	\$3.09	\$3.18	\$3.28	\$3.38	\$3.48
Annual Increase In Rent			3.0%	3.0%	3.0%	3.0%	3.0%
Gross Parking Income				\$0	\$0	\$0	\$0
Gross Retail Income (NNN)				\$1,298,542	\$1,337,498	\$1,377,623	\$1,418,951
Other Income				\$0	\$0	\$0	\$0
Less: Vacancy & Credit Loss (Parking)		5.0%					
Less: Vacancy & Credit Loss (Retail)		5.0%		(\$64,927)	(\$66,875)	(\$68,881)	(\$70,948)
Subtotal: Net Rental Income				\$1,233,615	\$1,270,623	\$1,308,742	\$1,348,004
<b>Total: Net Rental Income</b>				<b>\$1,233,615</b>	<b>\$1,270,623</b>	<b>\$1,308,742</b>	<b>\$1,348,004</b>
	<b>Total</b>	<b>% Increase</b>					
Less: Property Taxes <sup>1</sup>	(\$264,621.99)	2.0%	\$0	\$0	(\$280,819)	(\$286,435)	(\$292,164)
<b>Operating Expenses</b>	<b>(\$264,622)</b>		<b>\$0</b>	<b>\$0</b>	<b>(\$280,819)</b>	<b>(\$286,435)</b>	<b>(\$292,164)</b>
<b>Net Operating Income</b>			<b>\$0</b>	<b>\$0</b>	<b>\$952,796</b>	<b>\$984,188</b>	<b>\$1,016,578</b>
Less: Capital Expenditures			\$0	\$0	(\$9,528)	(\$9,842)	(\$10,166)
Less: I/O (interim) financing			\$0	\$0	\$0	\$0	\$0
Less: Permanent Debt Service			\$0	\$0	(\$772,428)	(\$772,428)	(\$772,428)
<b>Subtotal</b>			<b>\$0</b>	<b>\$0</b>	<b>(\$781,956)</b>	<b>(\$782,270)</b>	<b>(\$782,594)</b>
<b>Net Proceeds from Refinance:</b>			<b>\$0</b>	<b>(\$5,552,759)</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>
<b>Cash Flow From Operations</b>			<b>\$0</b>	<b>(\$5,552,759)</b>	<b>\$170,840</b>	<b>\$201,918</b>	<b>\$233,984</b>

Disposition

Retail	
Cap Rate	5.50%
Next Year NOI	\$1,348,004
Asset Value	\$24,509,162
Asset Value Per SF	\$720,858
Sale Price	\$24,509,162
Less: Commissions & Closing Costs	(\$245,092)
Less: Principal Balance of Loan O/S	(\$12,060,433)
<b>Net Proceeds from Disposition</b>	<b>\$12,203,637</b>
<b>Total Cash Flow Before Taxes</b>	<b>(\$9,796,327)</b>

IRR -4%

Notes:

<sup>1</sup> 1.1% of 90% of construction costs



**Cisterra - 9th & G**  
**Alternative 3 - 16 Story Tower**  
Assumptions & Results

**HOLDING & DISPOSITION**

Holding Period:	5
Cap Rate On Sale (Residential):	4.50%
Cap Rate On Sale (Retail):	5.50%
Commissions & Closing Costs:	1.00%
Value at Time of Sale (Year 5)	\$0
Asset Value PSF	\$0

**BUILDING ASSUMPTIONS**

Project FAR	11.92
Units Per Acre	270
Land S.F.	25,000
Building Gross SF	298,060
Building FAR/NSF Efficiency	58.18%
Net Rentable Area	173,400
Parking Spaces	189
Total # of Units	155

**FINANCING**

<b>Construction Financing:</b>	
Loan Amount	\$68,633,898
Loan to Cost	65%
Interest Rate	5.75%
Term (Months)	24
<b>Refinance:</b>	
Refinance at End of Year:	2
Permanent Loan Amount	\$69,077,415
Less: Construction Loan	(\$68,633,898)
Less: Loan Fees	0.50% (\$345,387.07)
Net Proceeds From Refinance	\$98,130
<b>Permanent Loan Info:</b>	
Loan Amount	\$69,077,415
Amortization	30
Interest Rate	4.5%
Annual Debt Service	\$4,200,061
Next Year NOI @ Refi	\$4,973,574
Value at Refi	\$110,523,864
Loan To Value	63%
Debt Coverage Ratio	1.18
Debt Yield	7.20%

**RESIDUAL LAND VALUE**

Land S.F.	25,000
Land Value	\$10,000,000
\$/S.F. of Land	\$400
\$/Unit	\$64,516

**PROJECT SUMMARY**

Residential	% of		Total		Total		\$/S.F. Rent
Market Rate	# of Units	Total Mix	Unit Size	Net Rentable	Monthly Rent	Annual Rent	
Studio	39	25.2%	700	27,300	\$2,350	\$1,099,800	\$3.36
1 Bed	33	21.3%	800	26,400	\$2,850	\$1,128,600	\$3.56
1 Bed + D	42	27.1%	900	37,800	\$3,150	\$1,587,600	\$3.50
2 Bed	25	16.1%	1,100	27,500	\$3,450	\$1,035,000	\$3.14
3 Bed	16	10.3%	1,275	20,400	\$3,950	\$758,400	\$3.10
Subtotal	155	100.0%	899	139,400	\$3,016	\$5,609,400	\$3.35
Affordable Units <sup>1</sup>							
Studio	0	0.0%	0	0	\$0	\$0	
Small 1 Bed	0	0.0%	0	0	\$0	\$0	
Large 1 Bed	0	0.0%	0	0	\$0	\$0	
2 Bed	0	0.0%	0	0	\$0	\$0	
Subtotal	0	0.0%		0		\$0	
Total	155	100.0%	899	139,400		\$5,609,400	
	Space	Avg.	Total	Total			
		Monthly Rent	Monthly Rent	Annual Rent			
Parking	189 spaces	\$0	\$0	\$0			
Retail	34,000 SF	\$3.18 PSF	\$108,120	\$1,297,440			
Other Income	\$0						

**CONSTRUCTION COSTS**

	Total Cost	Cost Per Unit	Cost Per Gross S.F.
Land Costs	\$10,000,000	\$64,516	\$33.55
<b>Hard Costs</b>			
Below Grade Retail	\$4,709,250	\$30,382	\$204.75
Above Grade Parking	\$7,650,000	\$49,355	\$90.00
Retail	\$2,243,875	\$14,477	\$154.75
Amenity	\$1,150,000	\$7,419	\$230.00
Outdoor Amenity	\$1,260,000	\$8,129	\$157.50
Type I Residential	\$41,360,800	\$266,844	\$242.50
Site	\$1,187,500	\$7,661	\$47.50
GC/GR Mark-Ups	\$12,580,255	\$81,163	\$42.50
Hard Costs Subtotal	\$72,141,680	\$465,430	\$242.04
Soft Costs	20.0% \$14,428,336	\$93,086	\$48.41
Contingency	5.0% \$4,328,501	\$27,926	\$14.52
Financing	\$4,692,096	\$30,272	\$15.74
<b>Total Project Costs</b>	<b>\$105,590,612</b>	<b>\$681,230</b>	<b>\$354.26</b>
Less: Loan Amount	\$68,633,898	\$442,799	\$230.27
Initial Investment:	\$36,956,714	\$238,430	\$123.99

**INVESTMENT PERFORMANCE**

Stabilized NOI	Year 3	\$4,973,574
Total Project Costs		\$105,590,612
<b>Stabilized Yield On Cost</b>		<b>4.7%</b>
	Return on Equity	Cash Flow
Initial		(\$36,956,714)
Year 1	0.0%	\$0
Year 2	0.3%	\$98,130
Year 3	2.0%	\$723,777
Year 4	2.4%	\$886,720
Year 5	136.4%	\$50,393,890
<b>Total Profit</b>		<b>\$15,145,802</b>
<b>Before Tax IRR</b>		<b>7.2%</b>

<sup>1</sup> @ 100% AMI

Cisterra - 9th & G  
Alternative 3 - 16 Story Tower  
Cash Flow Forecast

	Initial 0	Year 1 2020 1	Year 2 2021 2	Year 3 2022 3	Year 4 2023 4	Year 5 2024 5	Year 6 2025 6
Total Market Rate Units				155	155	155	155
Units Leased (Market Rate)				148	148	148	148
Units Leased (Affordable)		Construction	Construction	0	0	0	0
Units Vacant (Market Rate)				7	7	7	7
Occupancy Rate (Market Rate)				95.5%	95.5%	95.5%	95.5%
Vacancy Rate (Market Rate)				4.5%	4.5%	4.5%	4.5%
Monthly Rent (Market Rate)		\$3,016	\$3,106	\$3,199	\$3,295	\$3,394	\$3,496
Monthly Rent Per S.F. (Market Rate)		\$3.35	\$3.45	\$3.56	\$3.66	\$3.77	\$3.89
Monthly Rent Per space (Parking)		\$0	\$0	\$0	\$0	\$0	\$0
Monthly Rent Per S.F. (Retail)		\$3.18	\$3.28	\$3.37	\$3.47	\$3.58	\$3.69
Annual Increase In Rent			3.0%	3.0%	3.0%	3.0%	3.0%
Gross Rental Income (Market Rate Units)				\$5,951,012	\$6,129,543	\$6,313,429	\$6,502,832
Gross Rental Income (Affordable Units)							
Gross Parking Income				\$0	\$0	\$0	\$0
Gross Retail Income (NNN)				\$1,376,454	\$1,417,748	\$1,460,280	\$1,504,089
Other Income				\$0	\$0	\$0	\$0
Less: Vacancy & Credit Loss (Parking)			5.0%				
Less: Vacancy & Credit Loss (Retail)			5.0%				
Less: Vacancy & Credit Loss (Residential)				(\$68,823)	(\$70,887)	(\$73,014)	(\$75,204)
Subtotal: Net Rental Income				\$6,989,888	\$7,199,585	\$7,415,573	\$7,638,040
<b>Total: Net Rental Income</b>				<b>\$6,989,888</b>	<b>\$7,199,585</b>	<b>\$7,415,573</b>	<b>\$7,638,040</b>
		<b>Per Unit</b>	<b>% Increase</b>				
Less: Operating Expenses <sup>1</sup>		(\$6,000)	2.5%	\$0	\$0	(\$956,279)	(\$980,186)
Less: Property Taxes <sup>2</sup>		(\$6,444)	2.0%	\$0	\$0	(\$1,060,036)	(\$1,081,236)
<b>Operating Expenses - Residential</b>		<b>(\$12,444)</b>		<b>\$0</b>	<b>\$0</b>	<b>(\$2,016,315)</b>	<b>(\$2,061,422)</b>
				<b>(\$2,016,315)</b>	<b>(\$2,061,422)</b>	<b>(\$2,107,552)</b>	<b>(\$2,154,726)</b>
<b>Net Operating Income</b>		<b>\$0</b>	<b>\$0</b>	<b>\$4,973,574</b>	<b>\$5,138,163</b>	<b>\$5,308,021</b>	<b>\$5,483,314</b>
Less: Capital Expenditures		\$0	\$0	(\$49,736)	(\$51,382)	(\$53,080)	(\$54,833)
Less: I/O (interim) financing		\$0	\$0	\$0	\$0	\$0	\$0
Less: Permanent Debt Service		\$0	\$0	(\$4,200,061)	(\$4,200,061)	(\$4,200,061)	\$0
<b>Subtotal</b>		<b>\$0</b>	<b>\$0</b>	<b>(\$4,249,797)</b>	<b>(\$4,251,443)</b>	<b>(\$4,253,142)</b>	<b>(\$54,833)</b>
<b>Net Proceeds from Refinance:</b>		<b>\$0</b>	<b>\$98,130</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>
<b>Cash Flow From Operations</b>		<b>\$0</b>	<b>\$98,130</b>	<b>\$723,777</b>	<b>\$886,720</b>	<b>\$1,054,879</b>	<b>\$5,428,481</b>
<b>Disposition</b>							
<b>Residential (incl. parking)</b>							
Cap Rate						4.50%	
Next Year NOI						\$4,054,430	
Asset Value						\$90,098,435	
Asset Value Per Net SF						\$520	
Asset Value Per Unit						\$581,280	
<b>Retail</b>							
Cap Rate						5.50%	
Next Year NOI						\$1,428,884	
Asset Value						\$25,979,711	
Asset Value Per Space						\$137,459	
Sale Price						\$116,078,147	
Less: Commissions & Closing Costs						(\$1,160,781)	
Less: Principal Balance of Loan O/S						(\$65,578,355)	
<b>Net Proceeds from Disposition</b>						<b>\$49,339,011</b>	
<b>Total Cash Flow Before Taxes</b>		<b>(\$36,956,714)</b>	<b>\$0</b>	<b>\$98,130</b>	<b>\$723,777</b>	<b>\$886,720</b>	<b>\$50,393,890</b>

IRR -57%

Notes:

<sup>1</sup> \$500 per unit per month

<sup>2</sup> 1.1% of 90% of construction costs



**Cisterra - 9th & G**  
**Alternative 3B - 22 Story Tower**  
Assumptions & Results

**HOLDING & DISPOSITION**

Holding Period:	5
Cap Rate On Sale (Residential): <sup>4</sup>	4.75%
Cap Rate On Sale (Retail):	5.50%
Commissions & Closing Costs:	1.00%
Value at Time of Sale (Year 5)	\$159,276,163
Asset Value PSF	\$427

**BUILDING ASSUMPTIONS**

Project FAR	14.92
Units Per Acre	396
Land S.F.	25,000
Building Gross SF	373,069
Building FAR/NSF Efficiency	63.88%
Net Rentable Area	238,325
Parking Spaces	189
Total # of Units	227

**FINANCING**

<b>Construction Financing:</b>	
Loan Amount	\$88,710,460
Loan to Cost	65%
Interest Rate	5.75%
Term (Months)	24
<b>Refinance:</b>	
Refinance at End of Year:	2
Permanent Loan Amount	\$85,292,460
Less: Construction Loan	(\$88,710,460)
Less: Loan Fees	0.50% (\$426,462.30)
Net Proceeds From Refinance	(\$3,844,462)
<b>Permanent Loan Info:</b>	
Loan Amount	\$85,292,460
Amortization	30
Interest Rate	4.5%
Annual Debt Service	\$5,185,972
Next Year NOI @ Refi	\$6,482,227
Value at Refi	\$136,467,936
Loan To Value	63%
Debt Coverage Ratio	1.25
Debt Yield	7.60%

**RESIDUAL LAND VALUE**

Land S.F.	25,000
Land Value	\$10,000,000
\$/S.F. of Land	\$400
\$/Unit	\$44,053

**PROJECT SUMMARY**

Residential			% of	Avg.	Total	Avg.	Total	\$/S.F.
Market Rate - Parked	# of Units	Total Mix	Unit Size	Net Rentable	Monthly Rent	Annual Rent	Rent	
Studio	39	17.2%	700	27,300	\$2,500	\$1,170,000	\$3.57	
1 Bed	33	14.5%	800	26,400	\$3,000	\$1,188,000	\$3.75	
1 Bed + D	42	18.5%	900	37,800	\$3,300	\$1,663,200	\$3.67	
2 Bed	25	11.0%	1,100	27,500	\$3,600	\$1,080,000	\$3.27	
3 Bed	16	7.0%	1,275	20,400	\$4,100	\$787,200	\$3.22	
Subtotal	155	68.3%	899	139,400	\$3,166	\$5,888,400	\$3.52	
Market Rate - No Parking <sup>1,2</sup>								
Studio	18	7.9%	700	12,600	\$2,300	\$496,800	\$3.29	
1 Bed	15	6.6%	800	12,000	\$2,800	\$504,000	\$3.50	
1 Bed + D	19	8.4%	900	17,100	\$3,100	\$706,800	\$3.44	
2 Bed	13	5.7%	1,100	14,300	\$3,400	\$530,400	\$3.09	
3 Bed	7	3.1%	1,275	8,925	\$3,900	\$327,600	\$3.06	
Subtotal	72	31.7%	902	64,925	\$2,969	\$2,565,600	\$3.29	
Affordable Units <sup>3</sup>								
Studio	0	0.0%	0	0	\$0	\$0		
Small 1 Bed	0	0.0%	0	0	\$0	\$0		
Large 1 Bed	0	0.0%	0	0	\$0	\$0		
2 Bed	0	0.0%	0	0	\$0	\$0		
Subtotal	0	0.0%		0	\$0	\$0		
Total	227	100.0%	900	204,325	\$3,104	\$8,454,000	\$3.45	
	Space	Avg.	Total	Total				
	Monthly Rent	Monthly Rent	Annual Rent					
Parking	189 spaces	\$0	\$0	\$0				
Retail	34,000 SF	\$3.18 PSF	\$108,120	\$1,297,440				
Other Income	\$0							

**CONSTRUCTION COSTS**

	<b>Total Cost</b>	<b>Cost Per Unit</b>	<b>Cost Per Gross S.F.</b>
Land Costs	\$10,000,000	\$44,053	\$26.80
<b>Hard Costs</b>			
Below Grade Retail	\$4,766,750	\$20,999	\$207.25
Above Grade Parking	\$8,075,000	\$35,573	\$95.00
Retail	\$2,280,125	\$10,045	\$157.25
Amenity	\$1,175,000	\$5,176	\$235.00
Outdoor Amenity	\$1,320,000	\$5,815	\$165.00
Type I Residential	\$61,392,250	\$270,450	\$250.00
Site	\$1,250,000	\$5,507	\$50.00
GC/GR Mark-Ups	\$15,311,896	\$67,453	\$41.04
Hard Costs Subtotal	\$95,571,021	\$421,018	\$256.18
Soft Costs	20.0% \$19,114,204	\$84,204	\$51.24
Contingency	5.0% \$5,734,261	\$25,261	\$15.37
Financing	\$6,058,143	\$26,688	\$16.24
<b>Total Project Costs</b>	<b>\$136,477,630</b>	<b>\$601,223</b>	<b>\$365.82</b>
Less: Loan Amount	\$88,710,460	\$390,795	\$237.79
Initial Investment:	\$47,767,171	\$210,428	\$128.04

**INVESTMENT PERFORMANCE**

Stabilized NOI	Year 3	\$6,482,227
Total Project Costs		\$136,477,630
<b>Stabilized Yield On Cost</b>		<b>4.7%</b>
<b>Return on Equity</b>		<b>Cash Flow</b>
Initial		(\$47,767,171)
Year 1	0.0%	\$0
Year 2	-8.0%	(\$3,844,462)
Year 3	2.6%	\$1,231,432
Year 4	4.2%	\$2,016,453
Year 5	165.3%	\$78,964,265
<b>Total Profit</b>		<b>\$30,600,518</b>
<b>Before Tax IRR</b>		<b>10.2%</b>

<sup>1</sup> Rents discounted to reflect alternative Downtown parking within 4 blocks @ \$160 - \$225 /month.

<sup>2</sup> 10% stabilized vacancy reflective of local leasing agents.

<sup>3</sup> @ 100% AMI

<sup>4</sup> Assumed 25 basis points added to cap rate to reflect the 72 units that do not have parking provided. This represents increased market risk to both the developer and lender.

Source: London Moeder Advisors, Swinerton (construction estimates)

### Cash Flow Forecast

Disposition

Residential (incl. parking)

Cap Rate	4.75%
Next Year NOI	\$6,331,581
Asset Value	\$133,296,451
Asset Value Per Net SF	\$559
Asset Value Per Unit	\$587,209

<u>Retail</u>	
Cap Rate	5.50%
Next Year NOI	\$1,428,884
Asset Value	\$25,979,711
Asset Value Per Space	\$137,459

Sale Price	\$159,276,163
Less: Commissions & Closing Costs	(\$1,592,762)
Less: Principal Balance of Loan O/S	(\$80,972,040)

Net Proceeds from Disposition						\$76,711,362
Total Cash Flow Before Taxes	(\$47,767,171)	\$0	(\$3,844,462)	\$1,231,432	\$2,016,453	\$78,964,265

IRR	10%
-----	-----

**Notes:**

<sup>1</sup> \$500 per unit per month

<sup>2</sup> 1.1% of 90% of construction costs



**Cisterra - 9th & G**  
**Alternative 4 - 13 Story Tower**  
Assumptions & Results

**HOLDING & DISPOSITION**

Holding Period:	5
Cap Rate On Sale (Residential):	4.50%
Cap Rate On Sale (Retail):	5.50%
Commissions & Closing Costs:	1.00%
Value at Time of Sale (Year 5)	\$0
Asset Value PSF	\$0

**BUILDING ASSUMPTIONS**

Project FAR	13.90
Units Per Acre	293
Land S.F.	25,000
Building Gross SF	347,400
Building FAR/NSF Efficiency	53.31%
Net Rentable Area	185,200
Parking Spaces	202
Total # of Units	168

**FINANCING**

<b>Construction Financing:</b>	
Loan Amount	\$81,072,657
Loan to Cost	65%
Interest Rate	5.75%
Term (Months)	24
<b>Refinance:</b>	
Refinance at End of Year:	3
Permanent Loan Amount	\$68,160,004
Less: Construction Loan	(\$81,072,657)
Less: Loan Fees	0.50% (\$340,800)
Net Proceeds From Refinance	(\$13,253,452)
<b>Permanent Loan Info:</b>	
Loan Amount	\$68,160,004
Amortization	30
Interest Rate	4.5%
Annual Debt Service	\$4,144,281
Next Year NOI @ Refi	\$4,907,520
Value at Refi	\$109,056,007
Loan To Value	63%
Debt Coverage Ratio	1.18
Debt Yield	7.20%

**RESIDUAL LAND VALUE**

Land S.F.	25,000
Land Value	\$10,000,000
\$/S.F. of Land	\$400
\$/Unit	\$59,524

**PROJECT SUMMARY**

Residential Market Rate	# of Units	% of Total Mix	Avg. Unit Size	Total Net Rentable	Avg. Monthly Rent	Total Annual Rent	\$/S.F. Rent
Studio	43	25.6%	700	30,100	\$2,130	\$1,099,080	\$3.04
1 Bed	35	20.8%	800	28,000	\$2,630	\$1,104,600	\$3.29
1 Bed + D	45	26.8%	900	40,500	\$2,930	\$1,582,200	\$3.26
2 Bed	27	16.1%	1,100	29,700	\$3,230	\$1,046,520	\$2.94
3 Bed	18	10.7%	1,272	22,900	\$3,730	\$805,680	\$2.93
Subtotal	168	100.0%	900	151,200	\$2,797	\$5,638,080	\$3.11
<b>Affordable Units<sup>1</sup></b>							
Studio	0	0.0%	0	0	\$0	\$0	
Small 1 Bed	0	0.0%	0	0	\$0	\$0	
Large 1 Bed	0	0.0%	0	0	\$0	\$0	
2 Bed	0	0.0%	0	0	\$0	\$0	
Subtotal	0	0.0%	0	0		\$0	
<b>Total</b>	<b>168</b>	<b>100.0%</b>	<b>900</b>	<b>151,200</b>		<b>\$5,638,080</b>	

	Space	Avg. Monthly Rent	Total Monthly Rent	Total Annual Rent
Parking	202 spaces	\$0	\$0	\$0
Retail	34,000 SF	\$3.18 PSF	\$108,120	\$1,297,440
Other Income	\$0			

**CONSTRUCTION COSTS**

	Total Cost	Cost Per Unit	Cost Per Gross S.F.
Land Costs	\$10,000,000	\$59,524	\$28.79
<b>Hard Costs</b>			
Below Grade Retail	\$0	\$0	\$0.00
Below Grade Parking	\$15,275,000	\$90,923	\$130.00
Retail	\$6,570,500	\$39,110	\$193.25
Amenity	\$1,150,000	\$6,845	\$230.00
Outdoor Amenity	\$1,260,000	\$7,500	\$157.50
Type I Residential	\$46,293,250	\$275,555	\$242.50
Site	\$1,187,500	\$7,068	\$47.50
GC/GR Mark-Ups	\$14,921,458	\$88,818	\$42.95
Hard Costs Subtotal	\$86,657,708	\$515,820	\$249.45
Soft Costs	20.0% \$17,331,542	\$103,164	\$49.89
Contingency	5.0% \$5,199,462	\$30,949	\$14.97
Financing	\$5,538,453	\$32,967	\$15.94
<b>Total Project Costs</b>	<b>\$124,727,164</b>	<b>\$742,424</b>	<b>\$359.03</b>
Less: Loan Amount	\$81,072,657	\$482,575	\$233.37
Initial Investment:	\$43,654,507	\$259,848	\$125.66

**INVESTMENT PERFORMANCE**

Stabilized NOI	Year 4	\$4,907,520
Total Project Costs		\$124,727,164
<b>Stabilized Yield On Cost</b>		<b>3.9%</b>
	Return on Equity	Cash Flow
Initial		(\$43,654,507)
Year 1	0.0%	\$0
Year 2	0.0%	\$0
Year 3	-32.0%	(\$13,948,525)
Year 4	1.6%	\$714,164
Year 5	102.2%	\$44,609,574
<b>Total Profit</b>		<b>(\$12,279,294)</b>
<b>Before Tax IRR</b>		<b>-5.5%</b>

<sup>1</sup> @ 100% AMI

	Initial 0	Year 1 2020 1	Year 2 2021 2	Year 3 2022 3	Year 4 2023 4	Year 5 2024 5	Year 6 2025 6
Total Market Rate Units				168	168	168	168
Units Leased (Market Rate)				136	161	161	161
Units Leased (Affordable)		Construction	Construction	0	0	0	0
Units Vacant (Market Rate)				32	7	7	7
Occupancy Rate (Market Rate)				80.7%	95.8%	95.8%	95.8%
Vacancy Rate (Market Rate)				19.3%	4.2%	4.2%	4.2%
Monthly Rent (Market Rate)		\$2,797	\$2,881	\$2,967	\$3,056	\$3,148	\$3,242
Monthly Rent Per S.F. (Market Rate)		\$3.11	\$3.20	\$3.30	\$3.40	\$3.50	\$3.60
Monthly Rent Per space (Parking)		\$0	\$0	\$0	\$0	\$0	\$0
Monthly Rent Per S.F. (Retail)		\$3.18	\$3.28	\$3.37	\$3.47	\$3.58	\$3.69
Annual Increase In Rent			3.0%	3.0%	3.0%	3.0%	3.0%
Gross Rental Income (Market Rate Units)				\$5,981,439	\$6,160,882	\$6,345,709	\$6,536,080
Gross Rental Income (Affordable Units)							
Gross Parking Income				\$0	\$0	\$0	\$0
Gross Retail Income (NNN)				\$1,376,454	\$1,417,748	\$1,460,280	\$1,504,089
Other Income				\$0	\$0	\$0	\$0
Less: Vacancy & Credit Loss (Parking)				5.0%			
Less: Vacancy & Credit Loss (Retail)				5.0%			
Less: Vacancy & Credit Loss (Residential)				(\$68,823)	(\$70,887)	(\$73,014)	(\$75,204)
Subtotal: Net Rental Income				(\$1154,157)	(\$256,703)	(\$264,405)	(\$272,337)
<b>Total: Net Rental Income</b>				<b>\$6,134,914</b>	<b>\$7,251,039</b>	<b>\$7,468,570</b>	<b>\$7,692,627</b>
Less: Operating Expenses <sup>1</sup>	Per Unit	% Increase					
	(\$6,000)	2.5%	\$0	\$0	(\$876,051)	(\$1,066,283)	(\$1,092,940)
Less: Property Taxes <sup>2</sup>	(\$7,024)	2.0%	\$0	\$0	(\$1,252,192)	(\$1,277,236)	(\$1,302,780)
<b>Operating Expenses - Residential</b>	<b>(\$13,024)</b>		<b>\$0</b>	<b>\$0</b>	<b>(\$2,128,242)</b>	<b>(\$2,343,519)</b>	<b>(\$2,395,721)</b>
<b>Net Operating Income</b>			<b>\$0</b>	<b>\$0</b>	<b>\$4,006,672</b>	<b>\$4,907,520</b>	<b>\$5,072,850</b>
Less: Capital Expenditures			\$0	\$0	(\$40,067)	(\$49,075)	(\$50,728)
Less: I/O (interim) financing			\$0	\$0	(\$4,661,678)	\$0	\$0
Less: Permanent Debt Service			\$0	\$0	\$0	(\$4,144,281)	(\$4,144,281)
<b>Subtotal</b>			<b>\$0</b>	<b>\$0</b>	<b>(\$4,701,744)</b>	<b>(\$4,193,356)</b>	<b>(\$4,195,009)</b>
<b>Net Proceeds from Refinance:</b>			<b>\$0</b>	<b>\$0</b>	<b>(\$13,253,452)</b>	<b>\$0</b>	<b>\$0</b>
<b>Cash Flow From Operations</b>			<b>\$0</b>	<b>\$0</b>	<b>(\$13,948,525)</b>	<b>\$714,164</b>	<b>\$5,191,092</b>

#### Disposition

<b>Residential (incl. parking)</b>			
Cap Rate			4.50%
Next Year NOI			\$3,814,644
Asset Value			\$84,769,857
Asset Value Per Net SF			\$458
Asset Value Per Unit			\$504,582
<b>Retail</b>			
Cap Rate			5.50%
Next Year NOI			\$1,428,884
Asset Value			\$25,979,711
Asset Value Per Space			\$128,612
Sale Price			\$110,749,569
Less: Commissions & Closing Costs			(\$1,107,496)
Less: Principal Balance of Loan O/S			(\$65,910,339)
<b>Net Proceeds from Disposition</b>			<b>\$43,731,733</b>
<b>Total Cash Flow Before Taxes</b>	<b>(\$43,654,507)</b>	<b>\$0</b>	<b>\$0</b>
			<b>(\$13,948,525)</b>
			<b>\$714,164</b>
			<b>\$44,609,574</b>

IRR	-6%
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#### Notes:

<sup>1</sup> \$500 per unit per month

<sup>2</sup> 1.1% of 90% of construction costs



## Corporate Profile

### London Moeder Advisors

#### REPRESENTATIVE SERVICES

Market and Feasibility Studies	Development Services	Litigation Consulting
Financial Structuring	Fiscal Impact	Workout Projects
Asset Disposition	Strategic Planning	MAI Valuation
Government Processing	Capital Access	Economic Analysis

**London Moeder Advisors (formerly The London Group)** was formed in 1991 to provide real estate advisory services to a broad range of clientele. The firm principals, Gary London and Nathan Moeder, combine for over 60 years of experience. We have analyzed, packaged and achieved capital for a wide variety of real estate projects. Clients who are actively pursuing, developing and investing in projects have regularly sought our advice and financial analysis capabilities. Our experience ranges from large scale, master planned communities to urban redevelopment projects, spanning all land uses and development issues of all sizes and types. These engagements have been undertaken principally throughout North America and Mexico.

A snapshot of a few of the services we render for both the residential and commercial sectors:

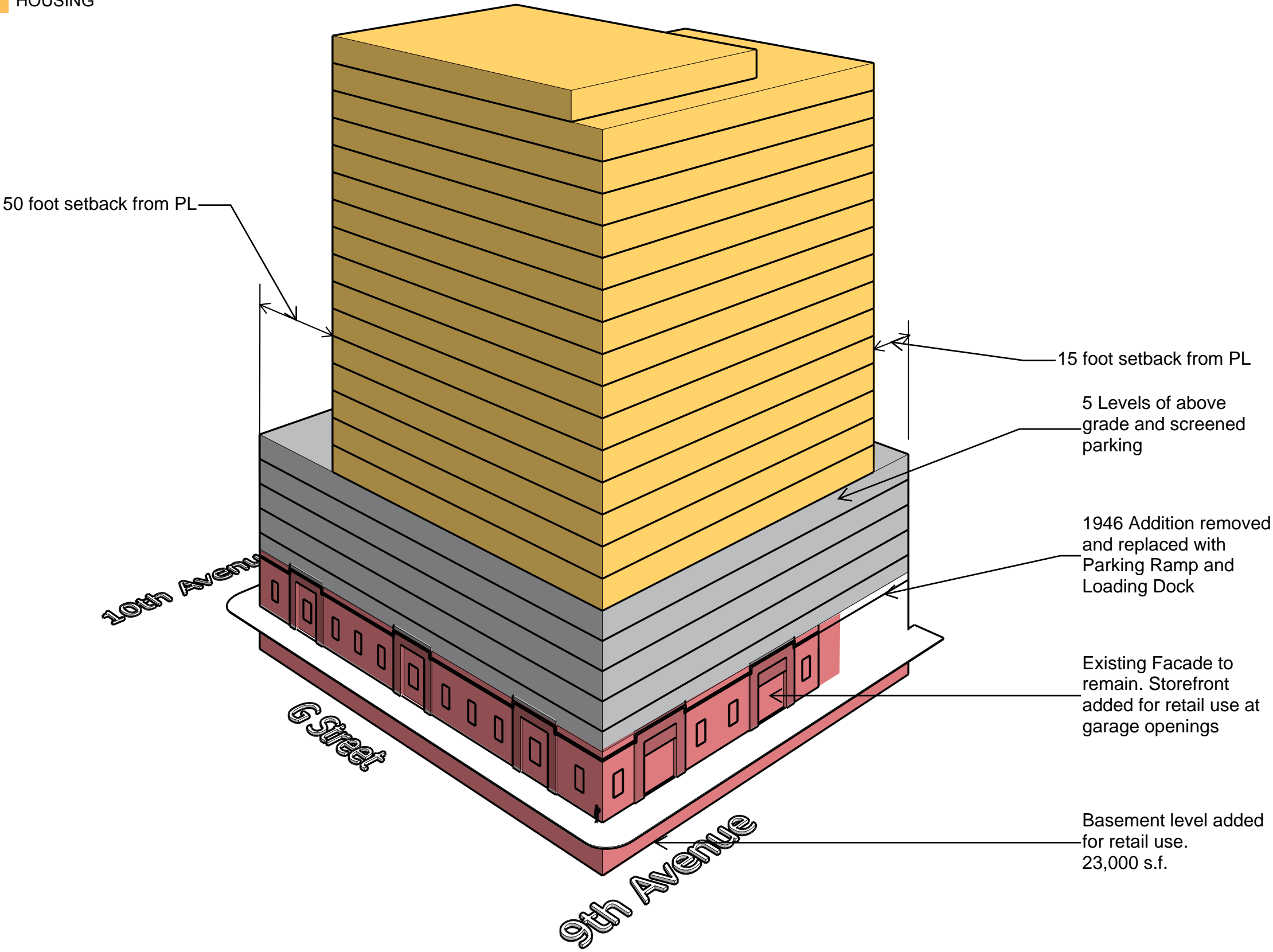
- **Market Analysis** for mixed use, urban and suburban properties. Studies concentrate on market depth for specific products, detailed recommendations for product type, absorption and future competition. It also includes economic overviews and forecasts of the relevant communities.
- **Financial Feasibility Studies** for new projects of multiple types, including condominium, apartment, office, and master-planned communities. Studies incorporate debt and equity needs, sensitivity analyses, rates of return and land valuations.
- **Litigation support/expert witness services** for real estate and financial related issues, including economic damages/losses, valuations, historic market conditions and due diligence. We have extensive deposition, trial, mediation and arbitration experience.
- **Investment studies for firms acquiring or disposing of real estate.** Studies include valuation, repositioning projects and portfolios, economic/real estate forecasts and valuation of partnerships. Often, the commercial studies include the valuation of businesses.
- **Estate Planning services** including valuation of portfolios, development of strategies for disposition or repositioning portfolios, succession planning and advisory services for high net worth individuals. We have also been involved in numerous marriage dissolution assignments where real estate is involved.
- **Fiscal Impact, Job Generation and Economic Multiplier Effect Reports**, traditionally prepared for larger commercial projects and in support of Environmental Impact Reports. We have been retained by both developers and municipalities for these reports. The studies typically relate to the tax revenues and employment impacts of new projects.

The London Group also draws upon the experience of professional relationships in the development, legal services, financial placement fields as well as its own staff. Clients who are actively investigating and investing in apartment projects, retail centers, commercial projects, mixed use developments and large master plans have regularly sought our advice and financial analysis capabilities.

**San Diego:** 825 10<sup>th</sup> Ave | San Diego, CA 92101 | (619) 269-4010  
**Carlsbad:** 2792 Gateway Road #104 | Carlsbad, CA 92009 | (619) 269-4012

9th and 'G' - Feasibility Study

- PARKING
- RETAIL
- HOUSING



Base Case

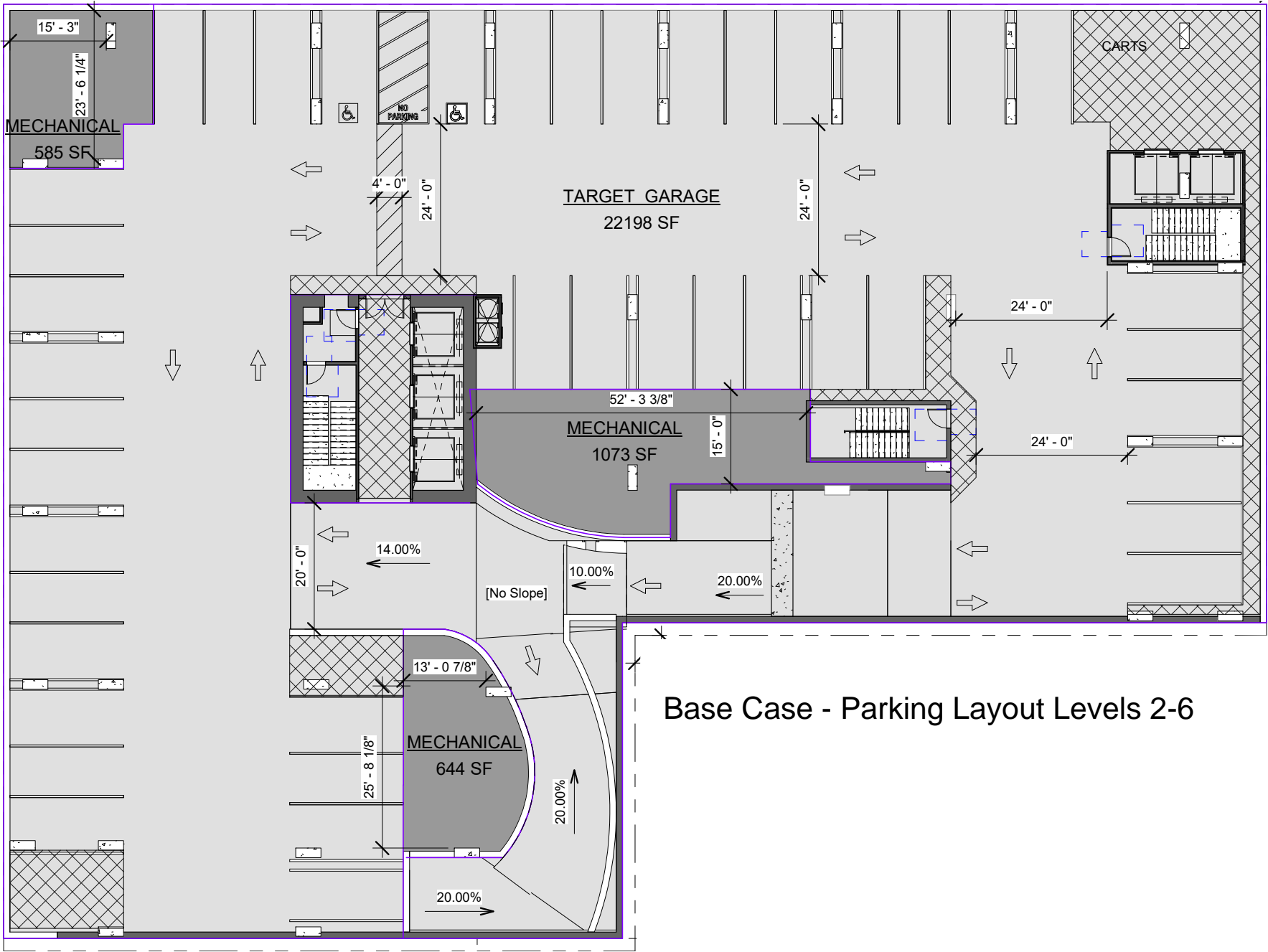
<b>Level B1</b>			
Retail	19,500 sf		
BOH/Util	3,500 sf		
Subtotal	23,000 sf		
<b>Level 1</b>			
Retail	14,500 sf		
Lobby/Leasing	3,400 sf		
BOH/Util	6,850 sf		
Subtotal	24,750 sf		
<b>Levels 2-6 Parking</b>			
Retail Parking	24,500 sf	34	
Residential Parking	98,000 sf	242	
Subtotal	122,500 sf	276	
<b>Levels 7-22</b>			
Residential Units	219,000 sf	242	
Residential Amenities	5,000 sf		
Core/Corridor	32,400 sf		
Subtotal	256,400 sf		
<hr/>			
<b>Total Gross</b>	<b>426,650 sf</b>		
<b>Retail N.R.</b>	<b>34,000 sf</b>		
<b>Residential N.R.</b>	<b>219,000 sf</b>		

<b>Units Mix</b>				
Type	Average Unit Size	# of Units	% of Units	Total SF
Studio	700 sf	60	24.8%	42,000 sf
1 Bedroom	800 sf	52	21.5%	41,600 sf
1 Bed + Den	900 sf	65	26.9%	58,500 sf
2 Bedroom	1,110 sf	40	16.5%	44,400 sf
3 Bedroom	1,300 sf	25	10.3%	32,500 sf
<hr/>				
Total / Ave.	905 sf	242	100.0%	219,000 sf



9th and 'G' - Feasibility Study

- PARKING
- RETAIL
- HOUSING



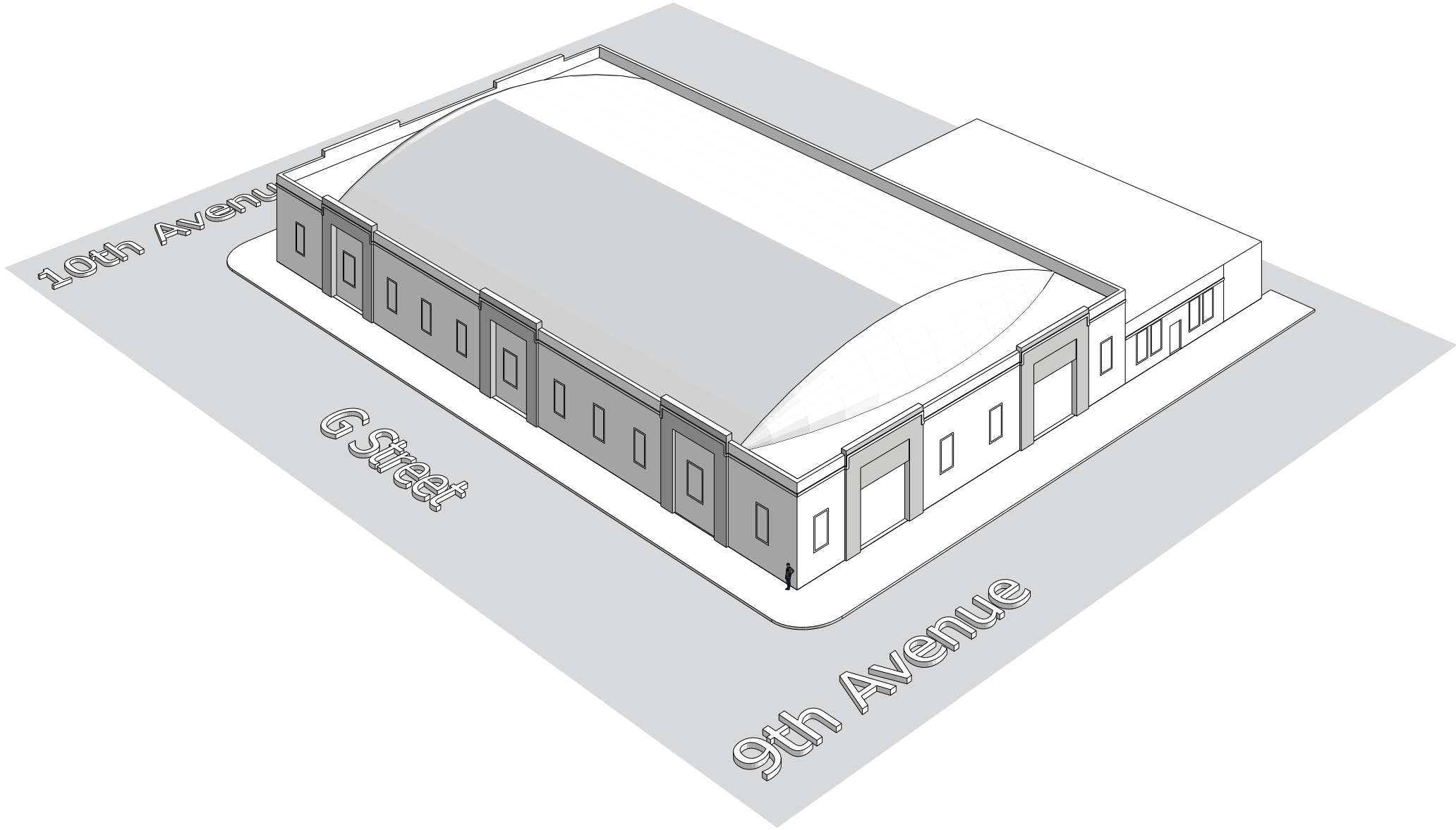
Base Case - Parking Layout Levels 2-6

Base Case

<b>Level B1</b>				
Retail	19,500 sf			
BOH/Util	3,500 sf			
Subtotal	23,000 sf			
<b>Level 1</b>				
Retail	14,500 sf			
Lobby/Leasing	3,400 sf			
BOH/Util	6,850 sf			
Subtotal	24,750 sf			
<b>Levels 2-6 Parking</b>				
Retail Parking	24,500 sf	34		
Residential Parking	98,000 sf	242		
Subtotal	122,500 sf	276		
<b>Levels 7-22</b>				
Residential Units	219,000 sf	242		
Residential Amenities	5,000 sf			
Core/Corridor	32,400 sf			
Subtotal	256,400 sf			
<hr/>				
<b>Total Gross</b>	<b>426,650 sf</b>			
<b>Retail N.R.</b>	<b>34,000 sf</b>			
<b>Residential N.R.</b>	<b>219,000 sf</b>			
<hr/>				
<b>Units Mix</b>				
Type	Average Unit Size	# of Units	% of Units	Total SF
Studio	700 sf	60	24.8%	42,000 sf
1 Bedroom	800 sf	52	21.5%	41,600 sf
1 Bed + Den	900 sf	65	26.9%	58,500 sf
2 Bedroom	1,110 sf	40	16.5%	44,400 sf
3 Bedroom	1,300 sf	25	10.3%	32,500 sf
<hr/>				
Total / Ave.	905 sf	242	100.0%	219,000 sf

Alternative 1

Level 1	
Retail	22,000 sf
BOH/Util	3,000 sf
No Parking	
<hr/>	
Total Gross	25,000 sf
Total Net Retail	22,000 sf



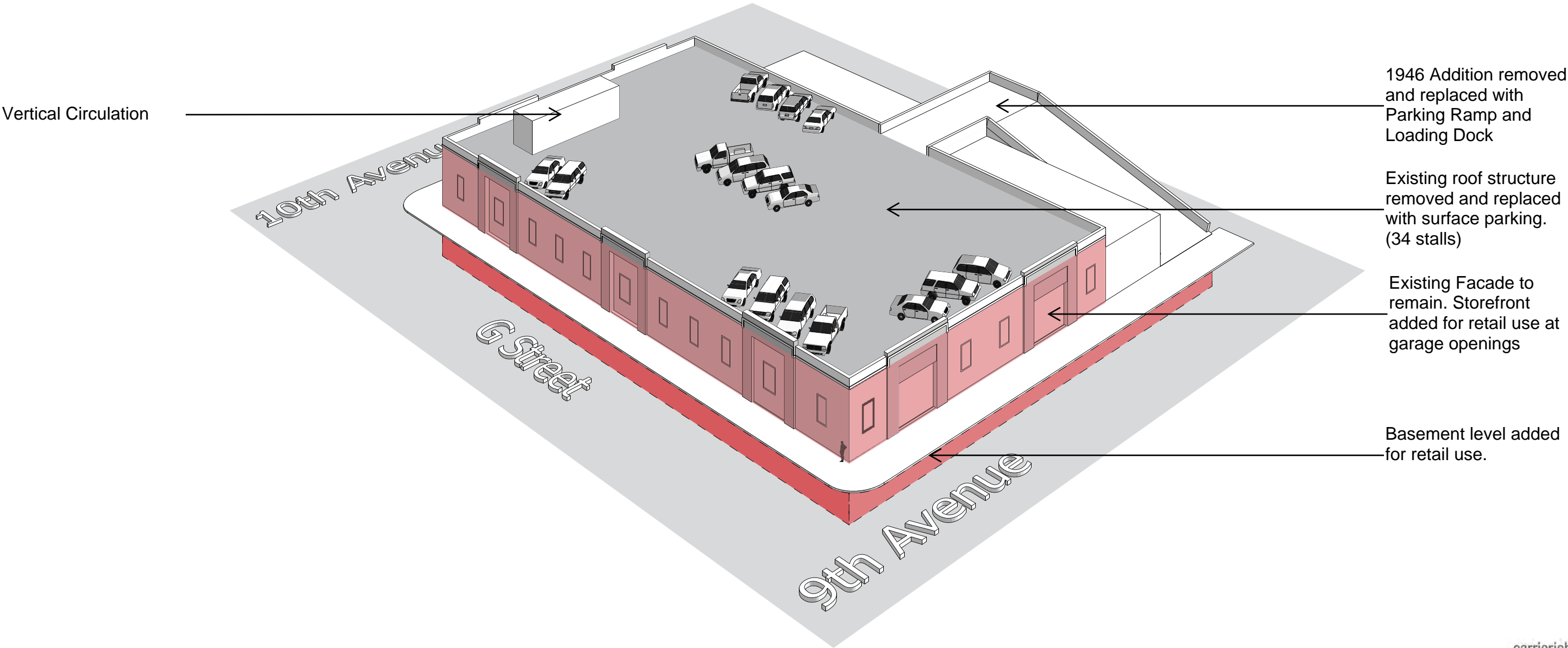


9th and 'G' - Feasibility Study

- PARKING
- RETAIL
- HOUSING

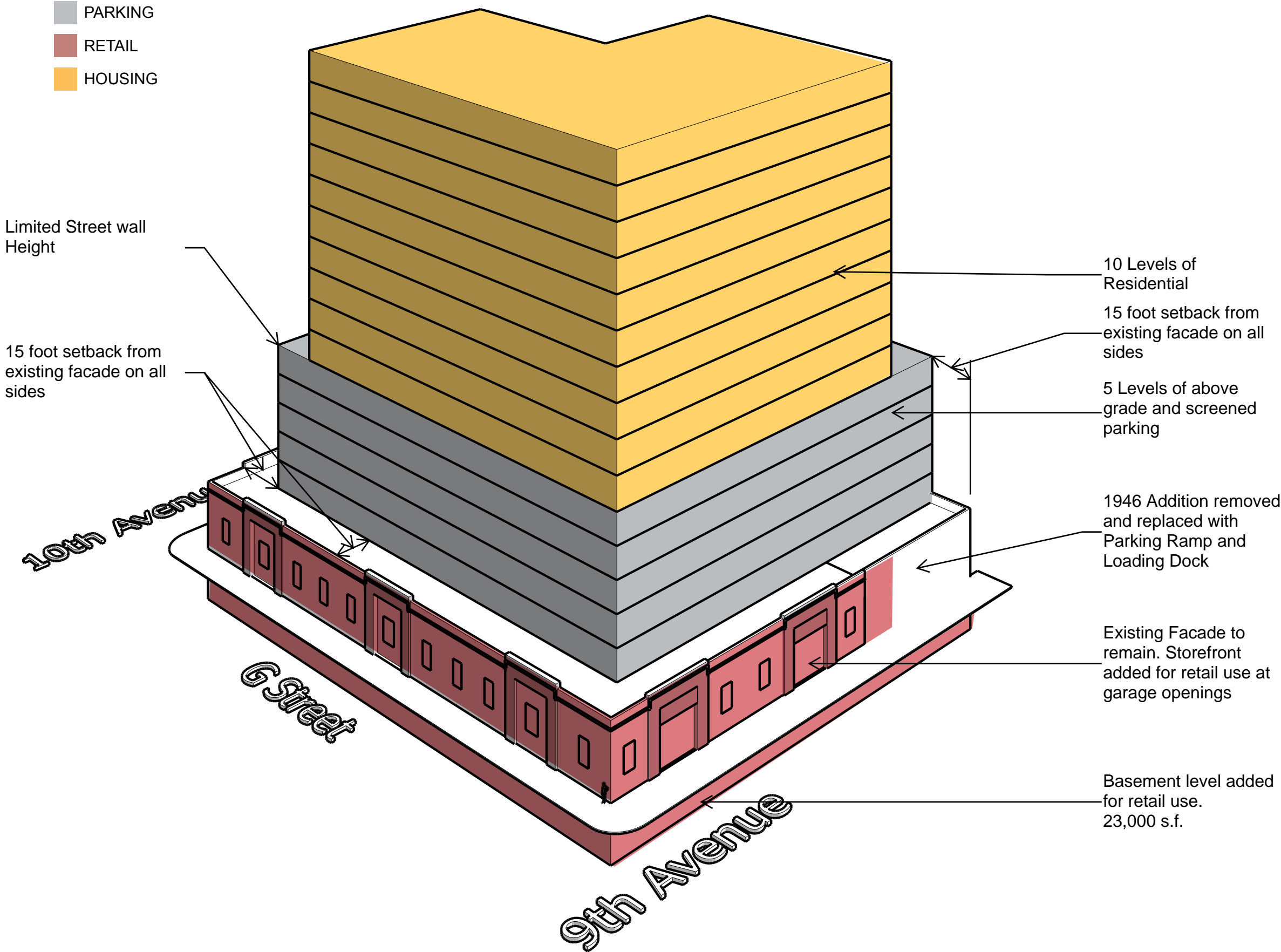
Alternative 2

Level B1		
Retail		14,500 sf
BOH/Util		2,600 sf
		17,100 sf
Level 1		
Retail		19,500 sf
BOH/Util		5,400 sf
	Subtotal	24,900 sf
Parking (open)		19,500 sf
Total Gross		42,000 sf
Total Net Retail		34,000 sf



9th and 'G' - Feasibility Study

- PARKING
- RETAIL
- HOUSING



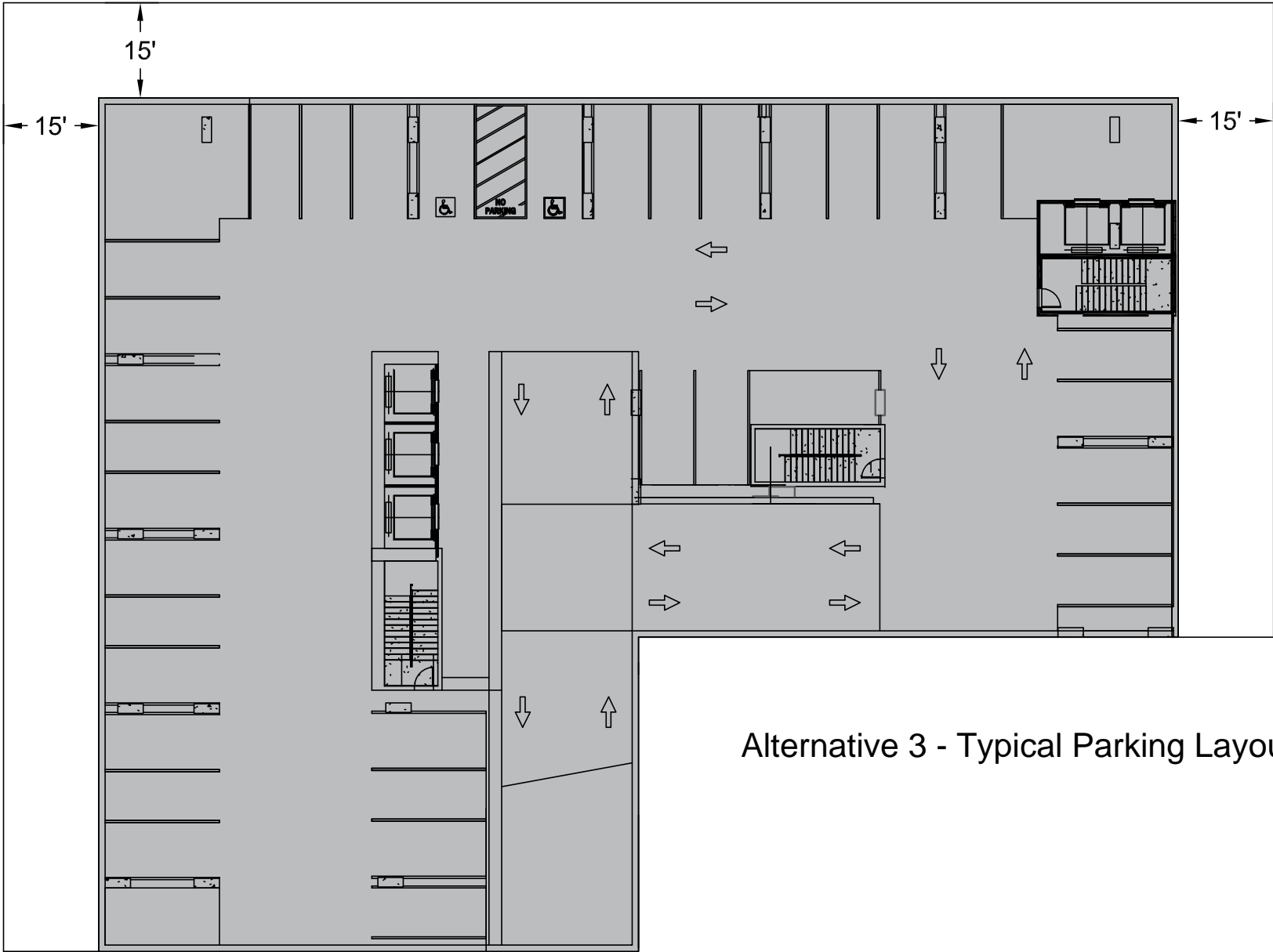
Alternative 3

<b>Level B1</b>				
Retail	19,500 sf			
BOH/Util	3,500 sf			
Subtotal	23,000 sf			
<b>Level 1</b>				
Retail	14,500 sf			
Lobby/Leasing	3,400 sf			
BOH/Util	6,850 sf			
Subtotal	24,750 sf			
<b>Levels 2-6 Parking</b>				
Retail Parking	15,300 sf	34		
Residential Parking	69,700 sf	155		
Subtotal	85,000 sf	189		
<b>Levels 7-16</b>				
Residential Units	139,400 sf	155		
Residential Amenities	5,000 sf			
Core/Corridor	20,910 sf			
Subtotal	165,310 sf			
<hr/>				
<b>Total Gross</b>	<b>298,060 sf</b>			
<b>Retail N.R.</b>	<b>34,000 sf</b>			
<b>Residential N.R.</b>	<b>139,400 sf</b>			
<hr/>				
<b>Units Mix</b>				
Type	Average Unit Size	# of Units	% of Units	Total SF
Studio	700 sf	39	25.2%	27,300 sf
1 Bedroom	800 sf	33	21.3%	26,400 sf
1 Bed + Den	900 sf	42	27.1%	37,800 sf
2 Bedroom	1,100 sf	25	16.1%	27,500 sf
3 Bedroom	1,275 sf	16	10.3%	20,400 sf
<hr/>				
Total / Ave.	899 sf	155	100.0%	139,400 sf



9th and 'G' - Feasibility Study

- PARKING
- RETAIL
- HOUSING



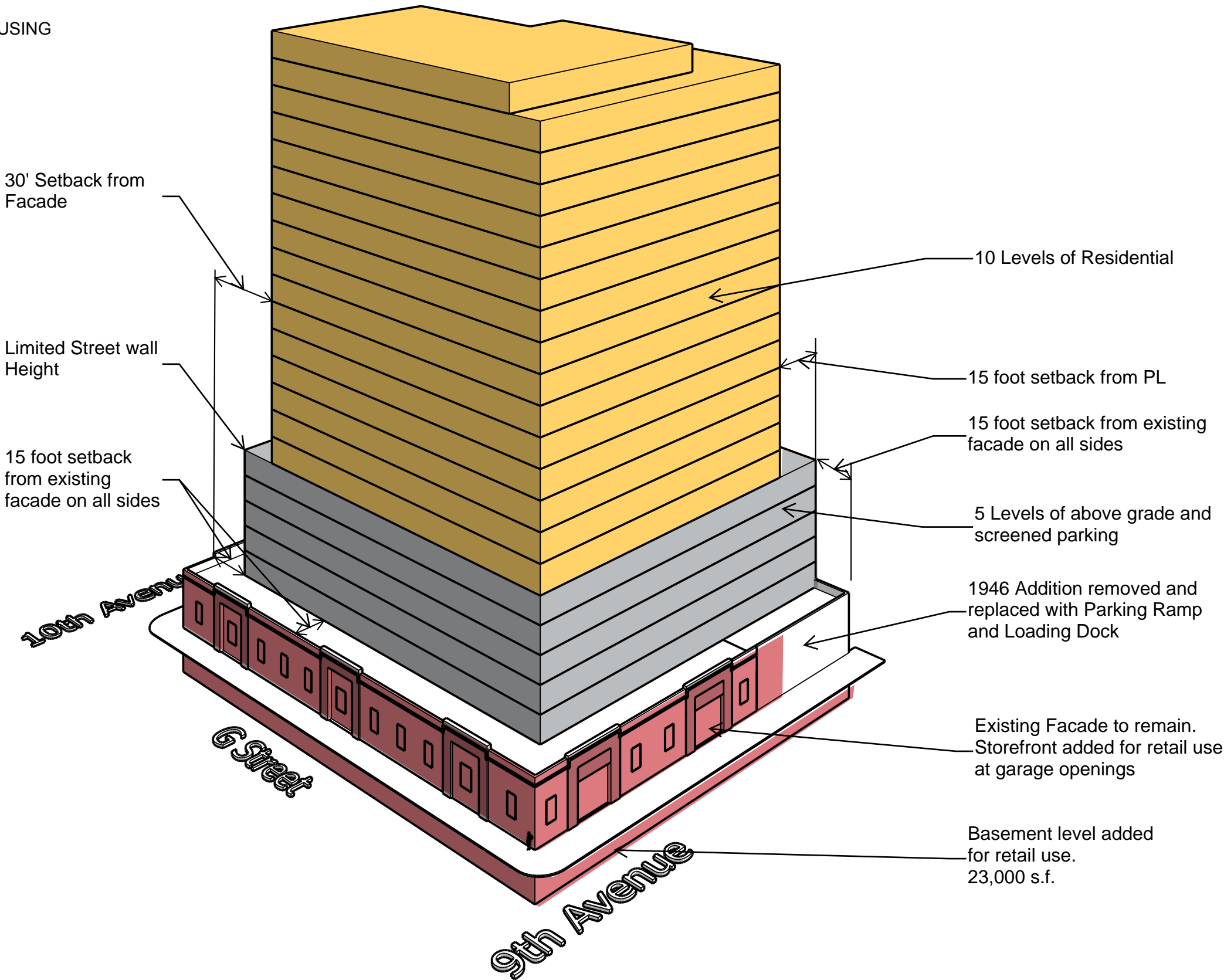
Alternative 3 - Typical Parking Layout

Alternative 3

<b>Level B1</b>				
Retail	19,500 sf			
BOH/Util	3,500 sf			
Subtotal	23,000 sf			
<b>Level 1</b>				
Retail	14,500 sf			
Lobby/Leasing	3,400 sf			
BOH/Util	6,850 sf			
Subtotal	24,750 sf			
<b>Levels 2-6 Parking</b>				
Retail Parking	15,300 sf	34		
Residential Parking	69,700 sf	155		
Subtotal	85,000 sf	189		
<b>Levels 7-16</b>				
Residential Units	139,400 sf	155		
Residential Amenities	5,000 sf			
Core/Corridor	20,910 sf			
Subtotal	165,310 sf			
<hr/>				
<b>Total Gross</b>	<b>298,060 sf</b>			
<b>Retail N.R.</b>	<b>34,000 sf</b>			
<b>Residential N.R.</b>	<b>139,400 sf</b>			
<hr/>				
<b>Units Mix</b>				
Type	Average Unit Size	# of Units	% of Units	Total SF
Studio	700 sf	39	25.2%	27,300 sf
1 Bedroom	800 sf	33	21.3%	26,400 sf
1 Bed + Den	900 sf	42	27.1%	37,800 sf
2 Bedroom	1,100 sf	25	16.1%	27,500 sf
3 Bedroom	1,275 sf	16	10.3%	20,400 sf
<hr/>				
Total / Ave.	899 sf	155	100.0%	139,400 sf

9th and 'G' - Feasibility Study

- PARKING
- RETAIL
- HOUSING



Alternative 3B

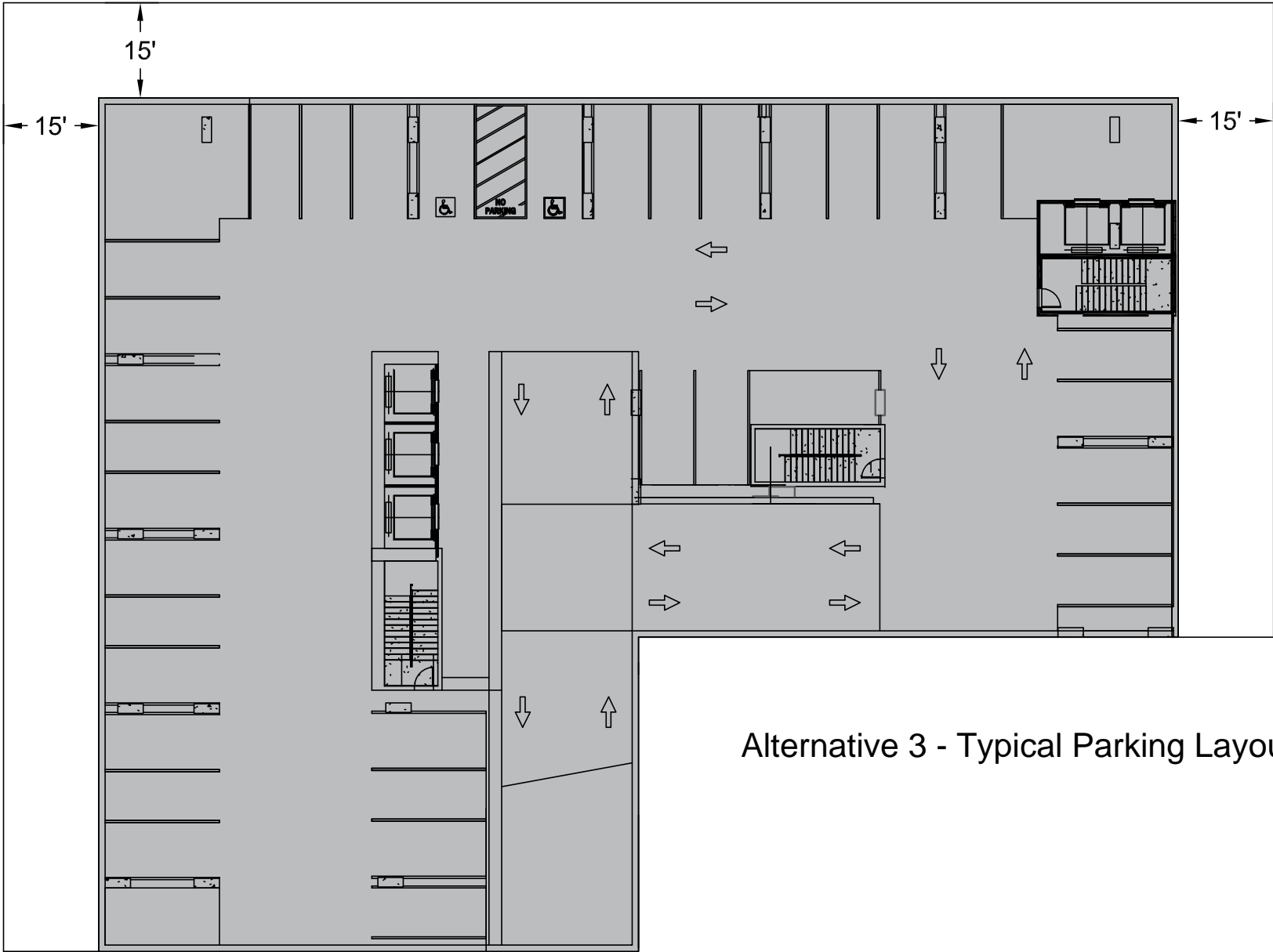
Parking rate at .68/Unit

<b>Level B1</b>				
Retail				19,500 sf
BOH/Util				3,500 sf
			Subtotal	23,000 sf
<b>Level 1</b>				
Retail				14,500 sf
Lobby/Leasing				3,400 sf
BOH/Util				6,850 sf
			Subtotal	24,750 sf
<b>Levels 2-6 Parking</b>				
Retail Parking				15,300 sf      34
Residential Parking				69,700 sf      155
			Subtotal	85,000 sf      189
<b>Levels 7-22</b>				
Residential Units				204,625 sf      227
Residential Amenities				5,000 sf
Core/Corridor				30,694 sf
			Subtotal	240,319 sf
<hr/>				
<b>Total Gross</b>				<b>373,069 sf</b>
<b>Retail N.R.</b>				<b>34,000 sf</b>
<b>Residential N.R.</b>				<b>204,625 sf</b>
<hr/>				
<b>Units Mix</b>				
Type	Average Unit Size	# of Units	% of Units	Total SF
Studio	700 sf	57	25.0%	39,900 sf
1 Bedroom	800 sf	47	21.0%	37,600 sf
1 Bed + Den	900 sf	61	27.0%	54,900 sf
2 Bedroom	1,100 sf	39	17.0%	42,900 sf
3 Bedroom	1,275 sf	23	10.0%	29,325 sf
<hr/>				
Total / Ave.	901 sf	227	100.0%	204,625 sf



9th and 'G' - Feasibility Study

- PARKING
- RETAIL
- HOUSING



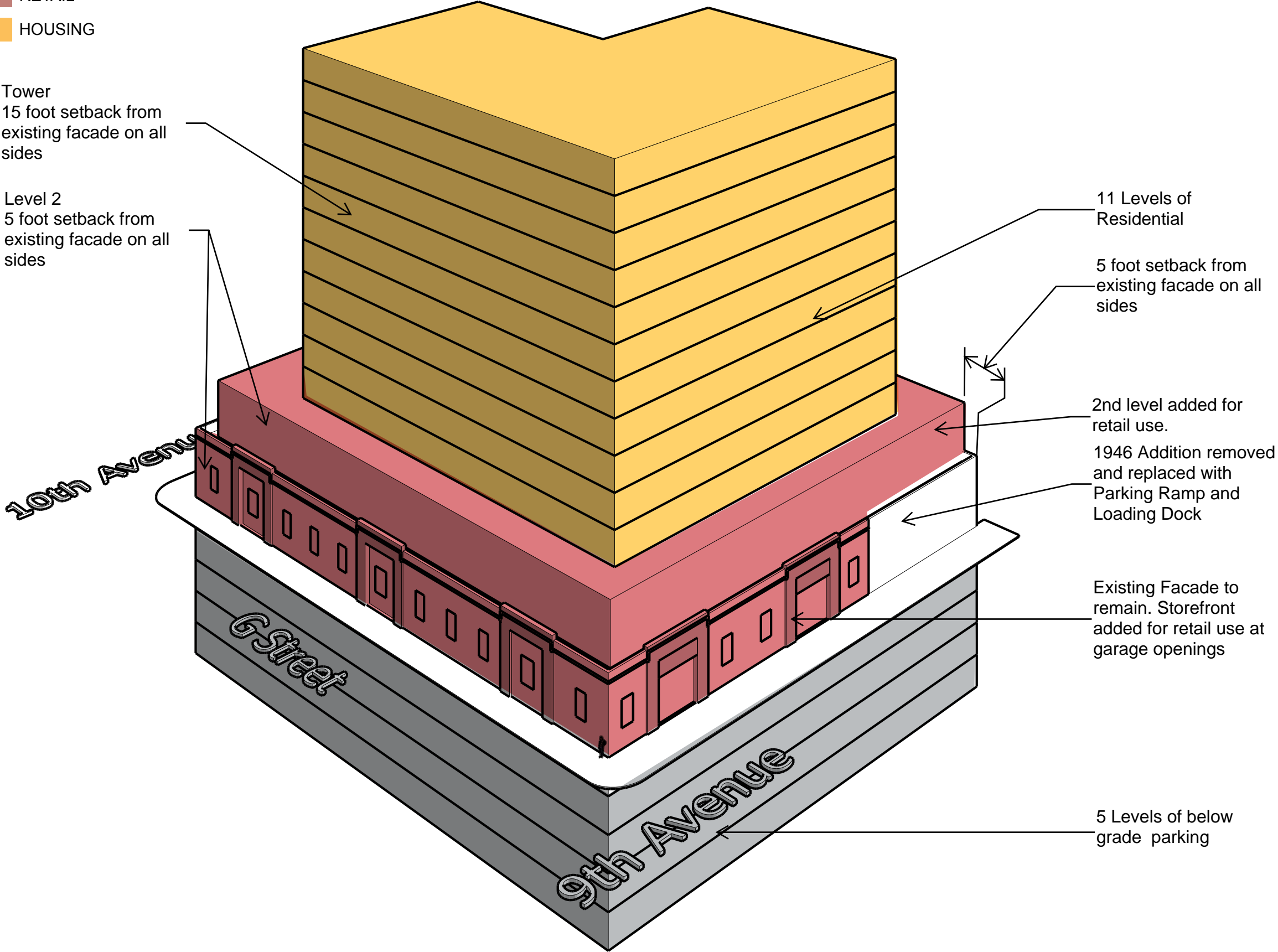
Alternative 3 - Typical Parking Layout

Alternative 3

<b>Level B1</b>				
Retail	19,500 sf			
BOH/Util	3,500 sf			
Subtotal	23,000 sf			
<b>Level 1</b>				
Retail	14,500 sf			
Lobby/Leasing	3,400 sf			
BOH/Util	6,850 sf			
Subtotal	24,750 sf			
<b>Levels 2-6 Parking</b>				
Retail Parking	15,300 sf	34		
Residential Parking	69,700 sf	155		
Subtotal	85,000 sf	189		
<b>Levels 7-16</b>				
Residential Units	139,400 sf	155		
Residential Amenities	5,000 sf			
Core/Corridor	20,910 sf			
Subtotal	165,310 sf			
<hr/>				
<b>Total Gross</b>	<b>298,060 sf</b>			
<b>Retail N.R.</b>	<b>34,000 sf</b>			
<b>Residential N.R.</b>	<b>139,400 sf</b>			
<hr/>				
<b>Units Mix</b>				
Type	Average Unit Size	# of Units	% of Units	Total SF
Studio	700 sf	39	25.2%	27,300 sf
1 Bedroom	800 sf	33	21.3%	26,400 sf
1 Bed + Den	900 sf	42	27.1%	37,800 sf
2 Bedroom	1,100 sf	25	16.1%	27,500 sf
3 Bedroom	1,275 sf	16	10.3%	20,400 sf
<hr/>				
Total / Ave.	899 sf	155	100.0%	139,400 sf

9th and 'G' - Feasibility Study

- PARKING
- RETAIL
- HOUSING



Alternative 4

<b>Levels B1-B5</b>			
Retail Parking	15,300 sf	34	
Residential Parking	98,700 sf	168	
BOH/Util	3,500 sf		
Subtotal	117,500 sf	202	

<b>Level 1</b>			
Retail	14,000 sf		
Lobby/Leasing	3,400 sf		
BOH/Util	7,500 sf		
Subtotal	24,900 sf		

<b>Level 2</b>			
Retail	20,000 sf		

<b>Levels 3-13</b>			
Residential Units	151,200 sf	168	
Residential Amenities	5,000 sf		
Core/Corridor	28,800 sf		
Subtotal	185,000 sf		

<b>Total Gross</b>	<b>347,400 sf</b>		
<b>Retail N.R.</b>	<b>34,000 sf</b>		
<b>Residential N.R.</b>	<b>151,200 sf</b>		

<b>Units Mix</b>				
Type	Average Unit Size	# of Units	% of Units	Total SF
Studio	700 sf	43	25.0%	30,100 sf
1 Bedroom	800 sf	35	21.0%	28,000 sf
1 Bed + Den	900 sf	45	27.0%	40,500 sf
2 Bedroom	1,100 sf	27	16.0%	29,700 sf
3 Bedroom	1,272 sf	18	11.0%	22,900 sf
Total / Ave.	900 sf	168	100.0%	151,200 sf



**DRAFT  
SITE DEVELOPMENT PERMIT FINDINGS  
FOR**

**660 10<sup>th</sup> Avenue and 659 9<sup>th</sup> Avenue**

**San Diego Historical Resource #1355**

**Centre City San Diego**

**Prepared by:**

**Marie Burke Lia, Attorney at Law, on behalf of the Project Applicants**

**April 10, 2020**

***Revised - June 8, 2020***

## **FINDINGS**

### **Site Development Permit – Section 126.0505**

#### **(a) Findings for all Site Development Permits**

##### **1. The proposed development will not adversely affect the applicable land use plan.**

The proposed project is the substantial alteration of The Pacific Telephone and Telegraph Company Garage, which has been designated as San Diego Historical Resource #1355 and is located at 660 10<sup>th</sup> Avenue and 659 9<sup>th</sup> Avenue in the East Village neighborhood of Centre City San Diego.

The property is currently owned by 4 Lee LLC and is used as a commercial fixtures retail store and warehouse. The Property is defined in the Legal Description as Assessor's Parcel Number 535-136-01, Lots A, B, C, K and L of Block 83 of Horton's Addition in the City of San Diego.

The Property was built and used as the Southern California/Pacific Telephone and Telegraph garage. From 1937 to 1984, 660 10<sup>th</sup> Avenue it was listed in the City Directories as the Southern California Telephone Co. (Garage)/Pacific Telephone and Telegraph Co. (Garage) and from 1990 to 1994, this portion of the building served as an airport and livery service. From 1997 to 2016, it served as the Farkas Store and We Buy Used Inc. 659 9<sup>th</sup> Avenue was listed in the City Directories as Pacific Telephone Telegraph Co. (Truck Repair) from 1971 to 1984. From 1988 to 2002, various tenants occupied this space, after which Chung Lee was listed. The building is currently occupied and has retained the design characteristics of the Art Deco style of architecture.

The Downtown Community Plan defines the vision for development of the entire Downtown region with land use and housing issues addressed in Chapter 3. Chapter 6 addresses the Community Plan Vision for the East Village with specific attention to the Northwest East Village Sub-District that the project resides in; and Chapter 9 addresses the Historic Preservation aspects of the project. The project is designed to strike a balance between the Community Plan Vision for the district to be a "high-intensity residential zone" where "intensities will parallel those of the Civic/Core" and Chapter 9's goals to preserve historic resources. This is a particularly difficult balance given the limited size of the site and the resulting constraints placed on any additional development. Compound that with the challenges of maintaining the historic façade,



which is 12' thick poured in place concrete, while excavating below grade and integrating new development density becomes quite a challenge.

According to Figure 3-4 describing Land Use, this property is located in the Employment/Residential Mixed-Use district. According to page 3-7, this classification provides synergies between educational institutions and residential neighborhoods or transition between the Core and residential neighborhoods. This classification permits a variety of uses, including office, residential, hotel, research and development and educational and medical facilities.

Chapter 6 calls for the Northwest sub-district to “yield downtown’s highest-intensity residential-emphasis district. Residential towers will share the area with offices, as well as ground-level commercial uses, and residents will enjoy creative pursuits in re-used civic landmark buildings.” It further states that “Establishing peak residential intensities in Northwest will help to maximize use of the area’s transit access.”

The desired development intensity for the area is described on page 3-17 where the Plan establishes intensity standards for various parts of downtown. Intensity is measured as Floor Area Ratio (FAR), which is obtained by dividing gross floor area by lot area. Figure 3-9 of the Plan shows the allowable minimum and maximum FARs for various sites. The minimum FAR for the subject property is 3.5 and the maximum is 6.0. “Proposed base development intensities in the Community Plan range from 2.0 to 10.0, modulated to provide diversity of scale, as well as high intensities in selected locations.” Policy 3.2-P-3 allows “intensity bonuses for development projects in specific locations established by this Plan that provide public amenities/benefits beyond those required for normal development approvals. Per figure 3-11 the subject property has a Total Bonus FAR available of 14 and per figure 3-12 the Maximum FAR with bonuses can be 20.

In light of the Community Plan’s Vision to maximize density on sites in the subject district, the project makes use of several bonus density provisions to achieve a total FAR density of 11.14. The bonus density provisions utilized include providing 10% three-Bedroom units; Construction the project to a LEED Silver standard; and purchasing FAR bonus density. Several factors limit the Project’s ability to achieve higher densities including:

- Maintaining the historic façade
- Height limit of building podiums to 80’
- Allowable Lot coverage
- Maximum Tower floor plate dimensions

- Financial feasibility of building higher than 240’

Several deviations to the CCPDO are being requested to achieve the proposed density. Those include increasing the Lot Coverage from 50% to 66% and increasing the allowable tower floor plate dimension in the east-west direction from 130’ to 150’.

The proposed project complies with Chapters 3 and 6 of the Downtown Community Plan as a mixed-use (residential and non-residential) project that will contain active retail and commercial ground floor uses. The design standards utilized will establish a highly pedestrian oriented development on the site.

Historic Preservation is addressed in Chapter 9 of the Downtown Community Plan. The existing structure on the project site is a designated local historical resource, #1355, The Pacific Telephone and Telegraph Company Garage. As indicated in Table 9-1 of the Plan, locally designated resources are to be retained on-site whenever possible.

The proposed development evaluated multiple options for how to maximize density on the site while maintaining as much of the historic structure as possible. Five concepts were studied in detail to define alternatives and evaluate the potential impacts on project configuration, historic preservation and economic impact. Those alternatives are addressed in detail in the Supplemental Findings herein, but the conclusion of those studies was that the optimal solution that was economically feasible, while best preserving the historic nature of the existing building, is the proposed development scheme.

“Partial retention, relocation or demolition of a resource shall only be permitted through applicable City procedures.” The applicable City procedures are established in San Diego Municipal Code Chapter 14, Article 3, Division 2, entitled “Historical Resources Regulations.” §143.0210 (2) (C) requires a Site Development Permit in accordance with Process Four for any development that proposes to deviate from the development regulations for historical resources described in this division. Substantial alteration of a designated resource by partial retention or other means is a deviation from the historical resource regulations and therefore a Site Development Permit, as authorized by Chapter 12, Article 6, Division 5, entitled “Site Development Permit Procedures” is required. The decision maker, in this instance the Planning Commission, must make all of the Findings in §126.0505(a) and §126.0505(i) before the partial retention of a locally designated historical resource can occur. Therefore, the processing of this Site Development Permit application is in compliance with and will not adversely affect this aspect of the applicable land use plan. The proposed project will be in compliance with the Downtown Community Plan.



**2. The proposed development will not be detrimental to the public health, safety and welfare.**

The proposed project would retain the existing façade of the designated structure on the site, add one subterranean level of retail space, construct five levels of parking above the existing structure and sixteen levels of residential units above the levels of parking.

The proposed development will be consistent with the Downtown Community Plan.

The project site is 24,829 (25,000) square feet bounded 9<sup>th</sup> Avenue, G Street and 10<sup>th</sup> Avenue. The Property is defined as Assessor's Parcel Number 535-136-01, Lots A,B,C, K and L of Block 83 of Horton's Addition in the City of San Diego. The construction will be Type 1, fire rated and sprinklered, meeting occupancy classification R1 as required by the California Building Code CBC 2010.

The proposed development complies with the Development Regulations of the Centre City Planned District Ordinance (§156.0310), including the Residential Development Regulations (§156.0310 (g) as they apply to developments containing more than 50 units in terms of Common Outdoor Open Space, Common Indoor Space, Private Open Space and Pet Open Space.

The proposed development complies with the Urban Design Regulations of the Planned District Ordinance (§156.0311), the Performance Standards of the Planned District Ordinance (§156.0312), the Residential Off-Street Parking Space Requirements of the Planned District Ordinance (§156.0313),

The proposed development complies with all San Diego Municipal Code and Uniform Building Code provisions intended ensure that the public health, safety and welfare are protected and enhanced by this construction.

**3. The proposed development will comply with the applicable regulations of the Land Development Code.**

The proposed project will construct a high density, 22 story high rise tower of mixed-use residential development with two levels of retail, five levels of parking and sixteen levels of residential use.

The proposed development will comply with the applicable provisions of the Centre City Planned District Ordinance in the following manner. It is located within the Employment/Residential Mixed-Use District that specifically calls for this type of

property use. The development will comply with the PDO's FAR regulations that call for a maximum base floor area ratio of 6.0 at this site with a maximum FAR with Bonuses of 20. It will comply with the PDO's Development Regulations pertaining to lot size, minimum building setbacks, building heights, building bulk, building base, ground floor heights, commercial space depth and residential development regulations.

The proposed development will request deviations from the PDO requirements for Personal Storage, Parking Standards, Maximum Lot Coverage, and Floor Plate Dimensions in the Tower. These deviations all address the proposed new construction on the site. One more deviation will be requested to protect the historic façade on the ground level as follows:

The preservation of the existing historic façade has created constraints for utility services needed to develop the site. The existing façade has limited penetrations which requires utilities to be placed in certain specific locations. The utilities along 10<sup>th</sup> Avenue include standard Back Flow Prevention Devices, that, per City standards, need to be spaced with proscribed separation distances to allow for servicing and separate points of connection. The transparency for the 10<sup>th</sup> Avenue façade will be reduced from 60% to 40%, but the transparency throughout all of the street facades combined will meet the Municipal Code requirements.

The building will comply with the PDO's Urban Design Regulations pertaining to building orientation, façade articulation, street level design, pedestrian entrances, blank walls, tower design, glass and glazing, exterior projecting balconies, rooftops, encroachments into public rights-of-way, building identification, regulations pertaining to historical resources requiring a Site Development Permit, additional standards for residential developments, additional standards for main streets, and urban open space design guidelines. It will comply with the PDO's Off Street Parking and Loading Standards.

The relevant Land Development Code's Planning and Development Regulations for topics not addressed in the Centre City Planned District Ordinance are contained in that Code's Chapter 14 and include: Grading Regulations, Draining Regulations, Landscape Regulations, Parking Regulations, Refuse and Recyclable Materials Storage, Mechanical and Utility Equipment Storage Regulations, Loading Regulations, Building Regulations, Electrical Regulations and Plumbing Regulations. The proposed development will comply with all of these regulations, since a building permit would not be issued without such compliance. Therefore, the proposed development will comply with all applicable regulations of the Land Development Code.



**(i) Supplemental Findings – Historical Resources Deviation for Substantial Alteration of a Designated Historical Resource**

**Supplemental Finding (1) There are no feasible measures, including a less environmentally damaging alternative, that can further minimize the potential adverse effects to the designated historical resource.**

The proposed project would retain the existing façade of the designated 1932 structure on the site, add one level of subterranean retail space, (19,500 sf), construct five levels of parking above the existing structure and sixteen levels of residential units above the levels of parking, resulting in 242 residential units. The 1946 addition would not be retained.

Various development alternatives were prepared to evaluate different programs for the site and evaluate their respective impacts to:

- the existing historic structure;
- the financial feasibility;
- the conformance with Community Plan goals;
- the ability to meet code & zoning regulations

The architectural plans for the Base Project were developed by the Project Architects, Carrier Johnson, and included site plans, floor plans, elevations and sections. That firm was also responsible for the architectural graphics illustrating the Base Project and the four project alternatives discussed below. These architectural graphics are attached to these Findings as *Exhibit A*.

The HABS documentation for the existing buildings was prepared by Union Architecture and included site plans, floor plans, elevations and sections. This documentation is attached as *Exhibit B*.

The construction cost estimates for the base project and each alternative were prepared by California licensed construction firms with historical and new property experience. The Economic Feasibility Analysis for the Base Project and each alternative was prepared by The London Moeder Group and was based on information provided by the above listed parties. Such information included feedback on the scope, schedule and budget for purposes of that analysis which is attached to these Findings as *Exhibit C*.

Base Project:

This is the proposed development for the site which includes, maintaining the existing facade of the 1932 historic structure for ground level retail use and adding one

subterranean level of retail for a total of 34,000 net square feet, five levels of parking above ground consisting of 276 spaces, and 16 levels of residential consisting of 242 units. Separation of the new development from the historic façade will be achieved by creating a horizontal “lens” at the first level of parking above. This lens is of dark colored metal panel and provides a negative space that separates the historic from new architecture. Changes to the historic structure include the removal of the existing roof and slab on grade and the 1946 addition, adding exiting doors on two facades, storefront infill within the original garage openings, and removal of a small section of the façade along 9<sup>th</sup> St.

#### Alternative 1:

Rehabilitate the existing 25,000 square foot historic structures and use 22,000 square feet as net retail space. Alternative 1 keeps the 1932 building and the 1946 addition as is with no alterations to the façade and/or roof trusses.

#### Alternative 2:

Maintain the existing facade of the 1932 historic structure, use the ground level for retail use, add one subterranean level of retail for a total of 34,000 net retail square feet, construct a loading dock, and construct a parking ramp to roof parking consisting of 34 spaces. This alternative was investigated to determine the minimum renovations needed for the retail space to meet market standards and incorporate on-site parking. Changes to the historic structure include the removal of the 1946 addition, the existing roof and slab on grade, and storefront infill within the original garage openings.

#### Alternative 3:

Maintain the existing facade of the 1932 historic structure, use the ground level for retail use, add one subterranean level of retail for a total of 34,000 net square feet, construct five levels of parking above consisting of 189 spaces, and construct ten levels of residential above consisting of 155 units. This alternative investigates the impacts of the requested 15’ foot setback on all streets from face of historic façade to proposed tower and above grade parking. Other changes to the historic structure would be the same as the Base Project.

The purpose of this study was to determine the impacts of providing a 15’ setback from the historic façade for all above grade development.

The physical yields for the development of Alternative 3 include:

- Reduced parking count (approximately 189 stalls) due to less floor plate,
- Street-wall height limits, which limit the levels of the garage (or tower base) above grade, preventing additional floors of garage to be added without



significantly reducing the parking count.

- Fewer residential units due to lower parking count.

#### Alternative 3B:

At request of City Planning and Historical, this Alternative is a variation of #3, providing the same setbacks behind the façade of the historic building but with a taller tower totaling 23 stories.

The physical yields for the development of Alternative 3 include:

- Reduced parking count (approximately 189 stalls) due to less floor plate,
- Street-wall height limits, which limit the levels of the garage (or tower base) above grade, preventing additional floors of garage to be added without significantly reducing the parking count.
- Reduced rates for the units due to lower parking count.

#### Alternative 4:

Maintain the existing facade of the 1932 historic structure, add five subterranean levels of parking consisting of 202 spaces, rehab ground floor retail and add one second story level of retail for a total of 34,000 net square feet, and construct eleven levels of residential consisting of 168 units. This proposal provides a 5' setback from the historic façade at the second floor and a 15 foot setback at the 3<sup>rd</sup> floor and above. Changes to the historic structure would be the same as the Base Project.

The purpose of this Alternative 4 study was to determine the impacts of providing all parking below grade.

The physical yields for the development of Alternative 4 include:

- Reduced parking count (approximately 202 stalls),
- 5' setback from historic façade (instead of 15') for 2<sup>nd</sup> floor retail to achieve minimum required retail area.

Fewer residential units due to lower parking count.

#### Analysis of the Base Project and the Four Alternatives

Alternative 1 was prepared to evaluate the impact of restoring the 1932 building and the 1946 addition and using them as a retail venue. While this maintains the designated historical resource, including the façade, roof and structure, it is not an optimal space for retail use, has no on-site parking for the use and is not economically viable.

Alternative 2 was prepared to evaluate the minimum renovation needed to accommodate a viable retail use in the 1932 building with on-site parking. While this alternative is feasible to construct it, requires nearly the same level of modifications to the existing building as the Base Project and is not economically viable.

Alternative 3 was prepared to evaluate the impact of providing a 15' setback from the historic façade of the 1932 building to the development above. This illustrates the significant impact to parking that happens when the façade is set back. Due to the proportions of the site, providing a setback causes the parking deck levels to be extremely inefficient, producing far less parking and resulting in far less residential units. It is not feasible to build additional residential units, even though the City does not require parking for the residences, because market studies show that a minimum amount of parking is required to make the project feasible to rent. Building the parking levels higher to achieve more parking and thus more residential is not feasible because the parking is so inefficient and, therefore, so expensive it becomes cost prohibitive. Either variation of this alternative is not economically viable.

Alternative 3B was prepared at request of the city to evaluate the unit count for a tower of equal height to the Base Project. Although the unit count is increased, there is still a reduced floor plate due to the 15 foot setback provided along 'G' Street and 9<sup>th</sup> Avenue. This reduced floor plate totals 227 units and 155 residential parking stalls, providing a parking ratio of .68 per unit. The average parking ratio for market rate apartments in Downtown San Diego is higher than 1 per Unit. The City does not require parking for the residences, however a minimum amount of parking is required to make the project feasible to rent at a competitive rate and to obtain financing for the project.

Alternative 4 was prepared to evaluate the impact of providing all the parking below grade. To achieve the equivalent retail area needed to make the retail viable, this alternative allows only a 5' setback from the historic façade to the 2<sup>nd</sup> level retail space. Not shown on graphic. Above that the residential structure is set back the full 15'. This alternative also yields less parking and less residential units. The parking is the maximum that can be built below grade as this alternative assumes 5 stories below grade. Another problem with this alternative is that the existing façade would need to be held up in place above 5 stories of shoring. This would be cost prohibitive if not totally infeasible. The alternative is not economically viable.

Other variations of these alternatives were evaluated and discarded as they did not improve the economics or lessen the impact to the existing historic structure. Any setback to the structure above from the face of the historic façade causes significant loss



of parking efficiency due to the dimensions of the site. The narrow width leaves no room for flexibility in the configuration of the above grade parking. Building parking higher is restricted by the building podium height defined in the CCPDO. The Base Program was, therefore, considered the optimal solution that strikes a balance between historic preservation and development density.

**Supplemental Finding (2 ) The deviation is the minimum necessary to afford relief and accommodate the development and all feasible measures to mitigate for the loss of any portion of the historical resource have been provided by the applicant.**

The initial building on this site was constructed in 1932 as a single story, approximately 22,000 square foot, garage for the Southern and later Pacific Telephone and Telegraph Company with Art Deco styling that was popular during that period. In 1946, an approximately 2,000 square foot, addition was constructed immediately to the south. Over the years, this building complex was converted from garage to warehouse use, but its exterior appearance was retained. The retained art deco style elements include large raised panels with vertical detailing. The three-sided framing of these panels extends above the roofline on the three street elevations. Raised flat horizontal bands are present around the edge of the roofline and connect the vertical panels. Some of the vertical panels contain metal foldup style garage doors, others contain metal-framed, fixed pane multi-lite window sections. Both buildings are in good condition and no significant exterior alterations have been noted. Both buildings were included in the designation of this property as San Diego Historical Resource #1355 in November of 2019.

The Mitigation Monitoring and Reporting Program of the Downtown FEIR/SEIR provides that resources listed on San Diego Register of Historical Resources shall, whenever possible, be retained on-site. Partial retention, relocation or demolition of a resource shall only be permitted according to Chapter 14, Article 3, Division 2 of the Historical Resources Regulations of the Land Development Code. Said Article 3 requires a Site Development Permit in accordance with Process Four be obtained for Multiple dwelling unit residential development on any size lot when a historical resource is present. (SDMC §143.0210(e)(2)(B)) A Site Development Permit decided in accordance with Process Four is also required for any development that deviates from the historical resources regulations as described in Section 143.0210 per (SDMC §126.0502(d)(20)(E)). The instant Site Development Permit proceedings are being undertaken for this purpose.

The City of San Diego applies the Secretary of the Interior's Standards for Rehabilitation provide the guidelines to be applied to rehabilitation projects affecting designated local resources.

Secretary of the Interior's Standards for Rehabilitation

1. *A property will be used as it was historically or will be given a new use that requires minimal change to its distinctive materials, features, spaces and spatial relationships.* The subject property will be given a new use that will require substantial change to its spaces and spatial relationships.
2. *The historic character of a property will be retained and preserved.* The removal of distinctive materials or alterations of features, spaces and spatial relationships that characterize a property will be avoided. But the historical character of the property will not be retained.
3. *Each property will be recognized as a physical record of its time, place and use.* Changes that create a false sense of historical development, such as adding conjectural features or elements from other historic properties, will not be undertaken. As the result of this project, the property will not be recognized as a physical record of its time, place and use.
4. *Changes to a property that have acquired historic significance in their own right will be retained and preserved.* No changes to the property that have acquired historic significance in their own right exist.
5. *Distinctive materials, features, finishes and construction techniques or examples of craftsmanship that characterize a property will be preserved.* The distinctive materials, features, finishes and construction techniques at the street level will be preserved but will be overshadowed by the new development.
6. *Deteriorated historic features will be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature will match the old in design, color, texture and, where possible, materials.* Replacement of missing features if any exist will be substantiated by documentary and physical evidence. Deteriorated historic features will be repaired rather than replaced.
7. *Chemical or physical treatments, if appropriate, will be undertaken using the gentlest means possible. Treatments that cause damage to historic materials will not be used.* This Standard can be met.
8. *Archaeological resources will be protected and preserved in place. If*



*such resources must be disturbed, mitigation measures will be undertaken. No such resources are anticipated.*

- 9. New additions, exterior alterations or related new construction will not destroy historic materials, features or spatial relationships that characterize the property. The new work shall be differentiated from the old and will be compatible with the historic materials, features, size, scale and proportion, and massing to protect the integrity of the property and its environment. It is the project's intent to meet this Standard.*
- 10. New additions and adjacent or related new construction will be undertaken in such a manner that, if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.*

Conclusion: It is indisputable that this project will not meet the Secretary of the Interior's Standards for Rehabilitation. If it did, the project would not need a Site Development Permit.

The 1932 historical resource will be retained and rehabilitated on its original site and it will be incorporated into a new residential and retail complex that will rise 21 stories above consistent with the current zoning for this site. The complex will be designed in a manner that laterally separates the historical resource from the new construction above in order to preserve the historical appearance of the designated resource.

The 1946 addition will be removed to allow for vehicular egress and loading dock. Other alternatives were studied for vehicular ingress and egress as well as a loading dock but they all required significant alteration to the 1932 façade and significant demolition of the 1946 façade. The proposed design minimizes any modification to the 1932 façade, utilizing existing openings for vehicular and occupant entry and with minimal modifications for required emergency exits and utility access. The full scope of the modifications to the façade is defined in the Treatment Plan, which is submitted as part of the Mitigation, Monitoring and Reporting Program presented in *Exhibit B* to this Report.

The proposed lateral separation at the second floor above the historic façade with a horizontal "lens" element is the only solution that worked for the site given the dimensional constraints of the property. This solution allows the historic façade to maintain its character and read as an independent element. Great care was taken in

exploring the dimensions, materiality and color of this lens to be respectful of the historic façade and create a “negative space” between the two elements of the project.

The Mitigation Monitoring and Reporting Program (MMRP) for the Downtown Community Plan requires the implementation of Mitigation Measure *HIST- A.1-3* if a (locally) designated historical resource would be only partially retained. That Mitigation Measure requires the submission of a Documentation Program that must include Photo Documentation and Measured Drawings of the resource to the Historical Resources Board Staff for review and approval. This Mitigation Measure will be implemented. Copies of the Mitigation Monitoring and Reporting Program Components are attached to these Findings as *Exhibit B*.

The subject property was evaluated in a *Historical Resources Technical Report* prepared by Marie Burke Lia and Dolores Mellon in July of 2019 and revised in September of 2019. That Report concluded that the subject property is eligible for the Local Historical Register under Criterion A and C, but not eligible for the California or National Registers. In November of 2019, the property was listed as Historical Resources #1355 under Criterion A as a special element of the City’s economic development, revealing the increasing importance of the automobile as America’s favored method of private transportation from 1900 – 1956, and under Criterion C through the retention of character-defining features of the Art Deco style, retaining a good level of architectural from its 1932 period of significance.

The Downtown Community Plan (DCP) and Centre City Planned District Ordinance (CCPDO) are subject to the Downtown FEIR, a “Program EIR” prepared in compliance with the California Environmental Quality Act (CEQA). A Downtown FEIR Consistency Evaluation will be prepared for the Project in compliance with CEQA and Local Guidelines. Under this process, an Evaluation is prepared for each subsequent proposed action to determine whether the potential impacts of a project were anticipated in the Downtown FEIR.

The incorporation of designated historical resources into new construction has been an established practice in the City of San Diego since 1990. There have been eight such projects that have used the Site Development Permit process under §126.0505 to achieve this solution that preserves the appearance and location of the historical resource and permits the redevelopment of the site in accordance with current zoning. These incorporation examples include the Howe House into the First and Robinson Apartment complex, the Carnation Building into the Icon Condominiums, the California Stamp Company into the Doma Condominiums, the Egyptian Theater into the Egyptian Condominiums, Station B into the Electra Condominiums, Exclusive Cleaners into the



Deca Condominiums, Griswold Pharmacy into the Paseo de Mission Hills and the John O'Day Commercial Building into the Kettner Lofts, aka AV8. A graphic exhibit documenting these incorporations is attached as *Exhibit D* to these Findings.

In each of these instances, the required Site Development Permit Findings were made that the deviation to incorporate the resource into the new development was found to be the minimum necessary to afford relief and accommodate the development and all feasible measures to mitigate the loss of any portion of the historical resource had been provided. The same Finding can be made for this project. The documentation herein demonstrates that the minimum necessary revisions to the existing historic structure will be undertaken and that a comprehensive mitigation and monitoring program will be put in place.

**Supplemental Finding (3) The denial of the proposed development would result in economic hardship to the owner. For purposes of this Finding, “economic hardship: means there is no reasonable beneficial use of a property and it is not feasible to derive a reasonable economic return for the property.**

In order to determine whether there are feasible alternative development plans for this property, that would minimize the potential adverse effects to the designated historical resource, the attached Economic Feasibility Analysis, conducted by the firm of London Moeder Advisors, was prepared based on the Base Project and 4 Alternatives (that were prepared by Carrier Johnson and are defined more fully above). That Analysis, which is attached as Exhibit C, reached the following conclusions.

#### Base Project

The Base Project would retain the existing façade of the historic structure, add one level of subterranean retail space, (19,500 sf), construct five levels of parking above the existing structure and sixteen levels of residential units above the levels of parking, resulting in 242 residential units. The total project costs would be \$142,807,588 and the total gross profit generated from this investment is forecasted to be \$45,955,665.

The Net Operating Income (NOC) at stabilization is estimated at \$7,954,860, which when compared to the total costs of the project, will result in a Yield on Costs (YOC) of 5.6% which satisfies the minimum 5.5% for project feasibility.

The Internal Rate of Return (IRR) on this investment is forecasted to be 16.5%, which also demonstrates that the project is economically feasible. The typical IRR for rental housing projects ranges from 13% to 15%. Any IRR below this range would struggle to attract investors and achieve project financing.

Four less environmentally damaging alternatives, that would minimize the potential adverse effects, have been evaluated.

### Alternative 1

Alternative 1 would retain and rehabilitate the existing warehouse retail structure (21,944 square feet) and return the space to retail use. The Economic Feasibility Analysis determined that the almost 22,000 square foot space would rent for \$2.75 a square foot.

The total forecasted rehabilitation costs for this Alternative are \$14,627,435, which would exceed the forecasted sale price of \$14,537,297.

This alternative would generate approximately \$43,197.909 less total profit than the Base Project. But more importantly, the forecasted YOC (yield on costs) would be 3.9% whereas the minimum YOC necessary to finance retail properties is 7.5%.

### Alternative 2

Alternative 2 would retain and rehabilitate the existing warehouse retail structure, add one subterranean retail level and replace the roof structure with rooftop parking. The 34,000 square foot retail space is estimated to rent for \$3.00 per square foot.

The total project costs are \$27,989,507 and the forecasted sale price is \$24,509,162. This results in a financial loss for the project forecast to be \$2,538,708, which is not economically feasible.

With a forecasted profit at disposition of negative \$2,538,708, this Alternative would generate \$48,494,373 less total profit than the Base Project.

### Alternative 3

Alternative 3 studies the effect of providing a 15' setback from the historic façade and would retain and rehabilitate the existing warehouse retail façade, add one level of subterranean retail space, add five levels of parking above and ten levels of residential units above the parking, resulting in 155 residential units.

The total project costs are forecasted to be \$105,590,612 and the forecasted sales price is \$116,078,147.



With a forecasted profit at disposition of \$15,145,802, this Alternative would generate \$30,809,863 less total profit than the Base Project.

This results in an economically infeasible project because the Yield on Costs is 4.7% where the minimum requirement is 5.5%. It is also the case that the Internal Rate of Return (IRR) would be 7%, which is below 13%-15% required to attract investors and qualify for financing.

### Alternative 3B

Alternative 3B assumes retaining the existing façade of the historic structure, the addition of one subterranean level of retail space, construction of five levels of parking above the existing structure, and 16 levels of residential units above the levels of parking. The 227 residential units are to begin leasing immediately after construction is complete with two months of pre-leasing. The project is assumed to be sold after a five-year period.

The 227 market rate units average 900 square feet in size. There will be a total of 189 parking spaces (155 assigned to residential) included in the parking levels of the building. In addition, there will be 19,500 square feet of below grade retail and 14,500 square feet of retail on the first level of the building.

When the 227 market rate units are leased after construction is completed, the forecasted average rent is estimated to be \$3.73 per Square foot for the units with parking and \$3.50 per square foot for units without parking. The 34,000 square feet of total retail space is estimated to at \$3.37 per square foot.

The forecasted sale price for the entire project is \$159,276,163. It is important to note that the assumed exit cap rate is 4.75% (25 basis point above market) to reflect the 72 units that do not have parking provided. Projects that do not provide parking represent increased marketability risk to the developer and lender. Total project costs are forecasted at \$136,477,630 resulting in a IRR of only 10.2%, well below the minimum returns needed to obtain financing commitments.

### Alternative 4

Alternative 4 studies the effect of putting all parking below grade and providing a smaller setback from the historic façade. This Alternate would retain and rehabilitate the existing warehouse retail façade, add one subterranean and one second story level of retail, five subterranean levels of parking and eleven levels of residential above the

second level of retail, resulting in 168 residential units.

The forecasted project costs are \$126,493,773 and the forecasted sales price for the entire project is \$110,331,239, a financial loss of \$14,556,301. This Alternative would generate \$60,511,966 less profit than the base project.

Also, the Internal Rate of Return (IRR) would be 6.5%, below the 13% - 15% required to attract investors and qualify for financing.

Five Alternatives to the proposed project were identified and evaluated by the London Moeder Advisors as discussed above. Each of those Alternatives fail to meet the minimum levels of financial performance that would make the project feasible. Each generates levels of Internal Rate of Return (IRR) and Yield on Costs (YOC) that is far below the minimum acceptable levels for financing. The required Finding of economic hardship can be made, there is no reasonable alternative beneficial use of the property and it is not feasible to derive a reasonable economic return from this property other than the proposed Base Project.

Therefore, the Supplemental Findings for the Historical Resources Deviation for Substantial Alteration of a Designated Historical Resource can be made.



## **UNION ARCHITECTURE INC.**

344 22<sup>nd</sup> STREET. SAN DIEGO, CA. 92102 619-269-4941

### **TREATMENT PLAN**

**DATE:** March 20, 2020

**PROJECT:** The Pacific Telephone and Telegraph Co. Garage  
Located at:  
660 10<sup>th</sup> Ave. and 559 9<sup>th</sup> Ave.  
San Diego, CA 92101  
APN: # 535-136-01-00  
HRB Site #1355  
Year built: 1932 and 1946  
Period of Significance: 1932-1956

**SUBJECT:** Treatment Plan for rehabilitation.

#### **PROJECT TEAM:**

Developer:	Paul Thometz, Cisterra Partners, LLC
Project Architect:	Frank A. Landry, Carrier Johnson + Culture.
Historic Architect & Monitor:	John H. Eisenhart, Union Architecture Inc.
Principal Investigator:	Marie Burke Lia, Attorney at Law
General Contractor:	T.B.D.
Structural Engineer:	T.B.D.

#### **PROPERTY DESCRIPTION:**

The Pacific Telephone and Telegraph Co. Garage building, a one story commercial building, is located at 660 10<sup>th</sup> Ave and 559 9<sup>th</sup> Ave. It was initially constructed in 1932 in the Art Deco Style, an addition to the south, similar in style was built in 1946 (period of significance of resource is 1932 to 1956). The parking garage, later warehouse building, is historically designated under City of San Diego Historic guidelines with the HRB #1355.

The City of San Diego Historical Resources Board based its designation of the resource on the following findings:

Criterion A: The property is historically significant as a special element of the City's economic development, revealing the increasing importance of the automobile as America's favored method of private transportation from 1900-1956, the building retains integrity to its 1932 – 1956 period of significance, specifically, the resource which was constructed in 1932 for use as a garage, continues to convey the significant character defining features of the automobile-related development as identified in the east Village

Historical Context Statement, including a single story, warehouse-like massing; limited ornamentation, raised vertical projections and numerous garage doors.

Criterion C: The property is historically significant through retention of character defining features of the Art Deco Style and retains a good level of architectural integrity from its 1932 period of significance. Specifically, the resource exhibits smooth wall surfaces, a flat roof, vertical projections above the roofline, fluted pilasters, geometric detailing and metal windows.

The footprint of the 1932 building is rectangular, it measures approximately 100' x 200'. The later addition to the south measures 36' x 60'.

Exterior walls are mostly constructed of board formed concrete, with the western portion of the southern elevation of the 1932 building consisting of concrete post and beam construction with brick in-fill.

The 3 main elevations (the street elevations) are laid out symmetrical. They are structured horizontally into a base zone, main wall field and a continuous decorative cornice / parapet banding at the top. All decorative detailing is cast in place concrete as part of the wall.

All door openings have a decorative motif at the top. Windows are commercial steel sash type with wire mesh glazing. They are covered with protective metal woven mesh on the exterior. The roof and ceiling are comprised of a wooden "bow" truss system. There are six skylights on the roof of the 1932 structure. All roofing is built-up roofing.

The building will be rehabilitated as part of a new development on the site. The 1932 building will be integrated with a new multi-story mixed-use building, the 1946 addition will be demolished. Modifications to the 1932 building include demolition of the ceiling / roof and demolition of the south elevation. Existing historic openings (doors and windows) on the east, north and west elevations will be modified in select locations to allow for the new use. This Treatment Plan is being prepared to rehabilitate the historic structure at the existing location.

## **INTRODUCTION:**

The implementation of the Treatment Plan for rehabilitation is under the supervision of the Project Architect and Historic Architect / Monitor in a manner consistent with the mitigation, monitoring, and reporting program for this project. This Treatment Plan is accompanied by a copy of HABS drawings of the property prepared by the Historic Architect / Monitor, drawings that outline the proposed stabilization and preparation of the structure for rehabilitation. This Treatment Plan and its related drawings will be included in all subsequent plans for the discretionary permit processing, construction documents, building permit, and observational process.



## **PREPARATION OF STRUCTURE:**

### **1. Preparation of the structure prior to any rehabilitation / demolition work:**

The entire structure at 660 10<sup>th</sup> Ave. is to be stabilized, braced, and secured. Specific procedures to be determined by qualified structural engineers and general contractor.

The implementation of these procedures will occur only after review from Historic Architect / Monitor. Exterior concrete trim pieces, windows and doors affected by this shall be protected from damage.

Windows and doors to remain should be protected in place, optionally, they may be removed, labeled and stored to be reinstalled during rehabilitation and fenestration openings to be covered with ¾" plywood nailed to wood bracing. This option may be acceptable per monitor's approval.

Any existing exterior plumbing pipes, wiring etc. shall be removed along with site utilities to be disconnected. Monitor to be notified prior to modification of structure required for rehabilitation. Consistent with Standards #6, 7, 9 and 10.

### **2. Removal of interior of structure / select demolition at exterior:**

Prior to the start of any demolition / removal process the Contractor and Monitor will meet on site to review the scope of demolition / removal work.

The entire 1946 structure, located at 559 9<sup>th</sup> Ave. will be demolished. The 1932 structure located at 660 10<sup>th</sup> Ave. will only be partially demolished as described in the Treatment Plan drawings and as follows: The wood stud walls, plaster, windows and doors on the first and second floor will be demolished. The roof / ceiling will be demolished. The concrete floor will be demolished. The southern concrete / brick-in-fill exterior wall will be demolished. Select existing openings will be enlarged on the street facades.

Contractor shall inform Monitor of discovery of items during demolition and removal. The Monitor will evaluate relevance of such materials on site. Consistent with Standards #6, 7, and 9.

### **3. Post-demo stabilization and protection of structure:**

The entire remaining structure is to be stabilized, braced, and secured during the rehabilitation process. Specific procedures to be determined by qualified structural engineers and general contractor.

The implementation of these procedures will occur only after review from Historic Architect / Monitor. Exterior concrete trim pieces, windows and doors shall be protected from potential damage during construction.

**EXISTING FOUNDATION:**

The Pacific Telephone and Telegraph Co. Garage building has concrete foundation walls and footings. The depth of the foundation and footing cannot be determined prior to excavation. Consistent with Standards #9 and 10.

**NEW FOUNDATION:**

The east, north and west elevations are to remain in place. They will be stabilized following the structural engineer / general contractor's recommendations. This may involve new shotcrete on the interior side of existing concrete exterior walls, new foundation walls, steel reinforcement on the interior side (new openings) and new footings.

Consistent with Standards #9 and 10

**EXISTING WALLS / EXISTING FRAMING:**

The existing walls consist of board formed concrete, painted at exterior and interior. The south elevation consists of brick-in fill and boardformed concrete, this exterior wall will be demolished entirely.

Consistent with Standards #2, 9 and 10.

**NEW WALLS / NEW FRAMING:**

The existing board formed concrete walls will remain in place at the east, north and west elevations. These elevations will be affected by modifications to allow for the new use. Select existing window and door openings will be modified.

The existing building will be incorporated with a new development. New construction will be differentiated from existing historic.

Consistent with Standards #2, 9 and 10.

**ROOF:**

The roof and ceiling will be demolished. Any new roofing, gutters and downspouts shall be located as not to detract / conflict with the existing facades. Consistent with Standards #6, 9 and 10.

**EXTERIOR FINISHES:**

The existing board formed concrete walls will remain in place in the east, north and west elevations. The concrete finish is painted. Any cracks and exposed steel reinforcement shall be repaired during the rehabilitation process to protect the existing concrete structure from progressing damage due to water intrusion. Repairs shall be done in a manner chemically compatible with the composition of the existing historic concrete mixture (follow Preservation Brief #15 of the National Park Service of the US



Department of the Interior, <https://www.nps.gov/tps/how-to-preserve/briefs/15-concrete.htm#preservation>)  
Consistent with Standards #2, 6, 7, 9, and 10.

## **EXTERIOR DOORS AND WINDOWS:**

The steel doors and windows are generally in good shape. The contractor should repair as needed for smooth operation. If a feature of the sash or frame is missing or deteriorated beyond repair (monitor to determine condition) a replicated profile of the missing element shall be used. If the sash and fixed units are a new unit, they should match the profile of the existing original rail and style (ie. Hope's steel windows, Torrance Co. or equal, shop drawings to be submitted for approval). Follow Preservation Brief #13, of the National Park Service of the US Department of the Interior, <https://www.nps.gov/tps/how-to-preserve/briefs/13-steel-windows.htm#repair>  
All of these options would be consistent with the standards, repair is generally the preferred option. The single leaf entry door on the east elevation is non-original.  
Consistent with Standards #2, 6, 7, 9 and 10.

## **ELECTRICAL & LIGHTING:**

Existing electrical and lighting system will be upgraded to conform to current code. All conduit and non-historic electrical and lighting on the exterior shall be removed. Electrical meters shall be located discretely away from public view. Exterior lighting fixtures to be surface mounted and to be sympathetic to Art Deco style influence.  
Consistent with Standards #9 and 10.

## **PLUMBING:**

All exterior plumbing and vent pipes to be dismantled. New interior plumbing and vents to be installed as required. The plumbing system will be upgraded to conform to current code. Consistent with Standards #9 and 10.

## **HEATING:**

New HVAC to conform with current code, any equipment is to be screened from public view. Consistent with Standards #9 and 10.

## **PAINTING:**

If lead paint is detected, follow current laws for careful removal or apply removal per EPA lead paint regulations.

New paint scheme on the exterior of the building shall be in Art Deco style colors. Historic monitor and City staff to approve final painting scheme.  
The colors can be existing scheme or a painting scheme from Dunn Edwards Company: Trade Winds. Det647 - Field

So Chic. Det614 - Trim, cornice, fluting.  
Red Clay. Det447 - Windows  
Consistent with Standards #6.

### **LANDSCAPING:**

The new site will be landscaped and hardscaped in accordance with all relevant regulations of the Land Development Code for the relocation, rehabilitation, and reuse of historic resources. Consistent with Standards #9 and 10.

### **REPAIR WORK:**

Repair work is preferred over use of replicated historic new materials. Retention of as much historic fabric as possible is paramount during rehabilitation. If a historic element is damaged but not in jeopardy of creating a performance issue with regard to the envelope of the building or building system, the feature should remain. Consistent with Standards #2, 5, 6 and 7.

### **RECONSTRUCTION / REHABILITATION:**

The cleaning of all historic material / fabric shall occur by using the gentlest means possible. An appropriate means of control and disposal of lead or other chemicals shall be provided. Historic fabric shall be retained as much as possible. Do not sandblast or water power wash materials.

The character defining massing / form of the structure is a one story commercial building, 100 ft. x 200 ft. with wood "bow" trusses spanning across at 15'-0" o.c.. The character defining material elements are: Board formed concrete walls with concrete detailing at base, cornice and parapet, large steel windows with wire-glass panes and overhead roll-up doors.

Should damage occur to the resource, it shall be repaired in conformance with the Secretary of the Interior's Standards for Rehabilitation or Reconstruction. Attachment of materials shall be similar to the original, historic method. Consistent with Standards #2, 6, 9, and 10.

### **ATTACHMENTS:**

Treatment Plan Drawings.

## **The Secretary of the Interior's Standards for Rehabilitation**

The Standards (Department of Interior regulations, 36 CFR 67) pertain to historic buildings of all materials, construction types, sizes, and occupancy and encompass the exterior and the interior, related landscape features and the building's site and environment as well as attached, adjacent, or related new construction. The Standards



are to be applied to specific rehabilitation projects in a reasonable manner, taking into consideration economic and technical feasibility.

1. A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment.
2. The historic character of a property shall be retained and preserved. The removal of historic materials or alteration of features and spaces that characterize a property shall be avoided.
3. Each property shall be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or architectural elements from other buildings, shall not be undertaken.
4. Most properties change over time; those changes that have acquired historic significance in their own right shall be retained and preserved.
5. Distinctive features, finishes, and construction techniques or examples of craftsmanship that characterize a property shall be preserved.
6. Deteriorated historic features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature shall match the old in design, color, texture, and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical, or pictorial evidence.
7. Chemical or physical treatments, such as sandblasting, that cause damage to historic materials shall not be used. The surface cleaning of structures, if appropriate, shall be undertaken using the gentlest means possible.
8. Significant archeological resources affected by a project shall be protected and preserved. If such resources must be disturbed, mitigation measures shall be undertaken.
9. New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment.
10. New additions and adjacent or related new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

## UNION ARCHITECTURE INC.

344 22<sup>nd</sup> STREET, SAN DIEGO, CA. 92102 T:619-788-2862

### MONITORING PLAN

**DATE:** March 20, 2020

**PROJECT:** The Pacific Telephone and Telegraph Co. Garage  
Located at:  
660 10<sup>th</sup> Ave. and 559 9<sup>th</sup> Ave.  
San Diego, CA 92101  
APN: # 535-136-01-00  
HRB Site #1355  
Year built: 1932 and 1946  
Period of Significance: 1932-1956

**SUBJECT:** Monitoring Plan for rehabilitation.

**PROJECT TEAM:**

Developer (D):	Paul Thometz, Cisterra Partners.
Project Architect (PA):	Frank A. Landry, Carrier Johnson + Culture.
Historic Architect & Monitor (HAM):	John Eisenhart, Union Architecture Inc.
Principal Investigator (PI):	Marie Burke Lia, Attorney at Law
General Contractor (GC):	T.B.D.
Structural Engineer (SE):	T.B.D.
Construction Manager (CM):	T.B.D.
Building Inspector (BI):	T.B.D.
City of San Diego, Environmental (EM):	Jennifer Wakem
City of San Diego, Planning Historical (HM) :	Suzanne Segur

**LOCATIONS:** Monitoring will take place on the site at 660 10<sup>th</sup> Avenue.

### **PROPERTY DESCRIPTION:**

The Pacific Telephone and Telegraph Co. Garage building, a one story commercial building, is located at 660 10<sup>th</sup> Ave and 559 9<sup>th</sup> Ave. It was initially constructed in 1932 in the Art Deco Style, an addition to the south, similar in style was built in 1946. The parking garage, later warehouse building, is historically designated and significant under City of San Diego Historic guidelines (HRB #1355) per Criteria "A" (special element of the City's Development) and "C" (Architecture). The typology of the resource as an auto garage is a significant character defining feature of the automobile-related development of the City and



its Architecture is a good example of the Art Deco Style. The period of significance is 1932-1956.

This Monitoring Plan will follow the Treatment Plan and supporting architectural documents prepared to rehabilitate this historic structure at its current location at 660 10<sup>th</sup> Avenue.

Treatment Plan documents shall be used by Monitor as guidelines.

**Monitoring actions are as follows:**

1. Pre-construction meeting (D, PA, HAM, GC, PI, CM, City of SD: BI, EM & HM)

Issue:

- a. Overview of Treatment Plan and Monitoring Plan as related to historic resource rehabilitation.

2. Preparation of structure for demolition (HAM, CM, PA, SE).

Issue:

- a. Monitor to be present **prior** to removal / demolition of any interior walls, any windows, doors, wooden roof, skylights and south elevation.
- b. Discuss pre-demo stabilization and protection of historic structure.
- c. Review work involved by CM to protect-in-place / remove / take-off typical template samples for rehabilitation. Windows, doors, any items that may be removed and taken from the structure to be rehabilitated / reinstalled shall be labeled, photographed, removed and protected for transport and storage.

*Any labels should include the following information:*

- *Property name and address item was taken from.*
- *Photo of item (applicable for typical windows, typ. concrete profile etc.).*
- *Information where item was taken (location for window)*
- *Contact information of dismantling contractor.*
- *Property Owner contact.*
- *Project Architect contact / Historic Architect contact.*
- *Historic Monitor contact.*
- *City Historic Planner contact.*
- *Dismantling date.*

- d. Monitor to approve structure is ready for select demolition.

### 3. Select demolition of structure (HAM, PA, CM).

Issue:

- a. Demolition of interior walls, windows, doors, channels at overhead doors, wooden roof, skylights and south elevation are completed. Monitor to review completed work and any items that have been removed and labeled for storage
- b. Discuss post-demo stabilization and protection of historic structure.
- c. Overview of Treatment Plan, Architectural, Landscaping and Engineering documents as related to rehabilitation on site.

### 4. Review of stabilization and protection of historic structure during rehabilitation on site (new footings, foundation, shotcrete, utilities, site preparation) (D, HAM, PA, CM, SE).

Issue:

- a. Review of preparation work for rehabilitation on site.

### 5. Continued monitoring of rehabilitation of structure. Monthly or as required by construction activity (HAM, PA, CM).

Issue:

- a. Review rehabilitation of resource in accordance with Treatment Plan and Architectural, Landscaping and Engineering Documents.
- b. Review of rehabilitation of windows, doors, concrete repair, concrete details.

### 6. Final Monitoring (HAM, PA, CM, D).

Issue:

- a. Final punch list of items to complete rehabilitation work according to Treatment Plan and Architectural, Landscaping and Engineering Documents.

### 7. Draft Report (HAM, PI).

Issue:

- a. Draft report of monitoring process to be submitted to PI for review.



8. Final Report (HAM, PI, D).

Issue:

- a. Final report of monitoring process, review updating of HABS and Treatment Plan documents to be submitted to PI for distribution to Developmental Services and San Diego History Center for archives.

End of Monitoring

Historical Resources Technical Report for  
the Property Located at  
660 10<sup>th</sup> Avenue & 659 9<sup>th</sup> Avenue  
San Diego CA 92101

Submitted to:

Historical Resources Board Staff  
Development Services Department  
1222 First Avenue, 5<sup>th</sup> Floor  
San Diego CA 92101

Prepared for:

Cisterra Partners LLC  
3580 Carmel Mountain Road, #460  
San Diego CA 92130

By:

Marie Burke Lia, Historical Property Consultant  
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Revised March, 2020



## TABLE OF CONTENTS

	<u>Page</u>
1.0 EXECUTIVE SUMMARY	1
2.0 INTRODUCTION	1
2.1 Report Organization	1
2.2 Project Area	2
2.3 Project Personnel	2
3.0 PROJECT SETTING	2
3.1 Physical Project Setting	2
3.2 Project Area and Vicinity	3
3.3 Historical Overview	3
4.0 METHODS AND RESULTS	6
4.1 Archival Research	6
4.2 Subject Property History	10
4.3 Field Survey	10
4.4 Description of Surveyed Resources	10
5.0 SIGNIFICANCE EVALUATIONS	11
5.1 Historic Context	11
5.2 Application of San Diego Register Criteria	12
5.3 Integrity	20
5.4 Application of California Register Criteria	21
5.5 Application of National Register Criteria	22
5.6 Findings and Conclusions	23
6.0 BIBLIOGRAPHY	27

## APPENDICES

## 1.0

### **EXECUTIVE SUMMARY**

This Historical Resources Technical Report for the commercial building located at 660 10<sup>th</sup> Avenue and 659 9<sup>th</sup> Avenue in the East Village neighborhood of San Diego, California (“Property”) was prepared at the request of the Plan-Historic Staff of the City of San Diego Development Services Department. The original building of this complex is addressed as 660 10<sup>th</sup> Avenue and its 1946 addition is addressed as 659 9<sup>th</sup> Avenue.

The purpose of the Report is to determine the potential historical and architectural significance of this one-story building and its addition, which have been evaluated as an automobile related property, and as possessing the design characteristics of the Art Deco style of architecture.

The Property is herein assessed in accordance with San Diego’s Historical Resource Guidelines, National Register Criteria and the California Register Criteria as required by the City’s Historical Resource Technical Report Guidelines and Requirements, Land Development Manual, Historical Resources Guidelines, Appendix E, Part 1.2, and this Report’s form and content are consistent with those Guidelines.

The Property is currently owned by 4 Lee Tenth LCC and is used as a commercial fixture retail store and warehouse. The Property is defined in the Legal Description as Assessor’s Parcel Number 535-136-01, Lots A, B, C, K and L of Block 83, of the Horton’s Addition in the City of San Diego.

The Property was built and used as Southern California/Pacific Telephone and Telegraph garage. From 1971 to 1984, 659 9<sup>th</sup> Avenue was listed in the City Directories as Pacific Telephone Telegraph Co. (Truck Repair). From 1988 to 2002, various tenants occupied this space. From 1937 to 1984, 660 Tenth Avenue was listed as Southern California Telephone Co. (Garage) and from 1990 to 1994 this portion of the building served as an airport & livery service. From 1997 to present, Farkas Store—New and Used Fixtures—has occupied this address. The building is currently occupied and has retained the design characteristics of the Art Deco style of architecture.

The changes to the Property have been documented through examination of historic records and a physical site inspection.

## 2.0

### **INTRODUCTION**

### 2.1

#### **Report Organization**

This Historical Resources Technical Report was prepared in order to determine the potential historical and/or architectural significance of this commercial building located at 660 10<sup>th</sup> Avenue and 659 9<sup>th</sup> Avenue and in the East Village Area of the Centre City Community Plan Area in San Diego, California. The large building fronting on G Street was constructed in 1932 and the addition on the 9<sup>th</sup> Avenue frontage was constructed in 1946.



Structures that are at least 45 years of age may be considered potential historic resources under the California Environmental Quality Act (CEQA). This Historical Resources Technical Report includes the following: Title Page; Table of Contents; Executive Summary; Introduction (Report Organization, Project Area and Project Personnel); Project Setting (Physical Project Setting, Project Area and Vicinity); Historical Overview of the East Village community); Methods and Results (Archival Research, Field Survey and Description of The Property with photographs); Significance Evaluations; Findings and Conclusions (Impacts Discussion, Location of Research Facilities, Discussion of any consultation with local historical societies, planning agencies, interested individuals, and interviews with knowledgeable individuals); Description of Surveyed Resources (Narrative description, character defining features, architectural style, other important features, contemporary color photographs of all elevations in the 4" x 6" size, historic photographs, any original drawings, and references for historic documentation); Significance Evaluations; Findings and Conclusions (Impacts Discussion, Mitigation Measures and Conclusion); Bibliography and Appendices.

The Appendices consist of specific Building Development Information, Ownership and Occupant Information, Maps and Preparer's Qualifications.

## 2.2 Project Area

The Property is located in the East Village neighborhood of the Centre City Community Plan Area addressed at 660 10<sup>th</sup> Avenue and 659 9<sup>th</sup> Avenue. The Property's Legal Description is as follows: Assessor's Parcel Number 535-136-01, Lots A, B, C, K and L of Block 83, Horton's Addition in the City of San Diego

## 2.3 Project Personnel

Project Personnel included Historic Property Consultant Maire Burke Lia, Historian Dolores Mellon and Researcher Amanda Daghalý who conducted the field survey. Dolores Mellon photographed the property and reviewed the historical photographs and other files at the San Diego History Center. Additional review, archival research, and preparation of the final report were undertaken by Kathleen Crawford, Amanda Daghalý and Marie Burke Lia. All chain of title research was conducted by California Lot Book, Inc.

Ms. Lia, Ms. Daghalý and Ms. Mellon visited the Property in May of 2017 to evaluate the property as well as to inspect the surrounding neighborhood. Photographs were taken of the building. Based upon site inspection and the other cited information, the building was compared to established architectural norms that are currently in use in the United States. Several architectural reference guides were consulted by the authors to fully substantiate the architectural details of the building.

## 3.0 PROJECT SETTING

### 3.1 Physical Project Setting

The Property is located at 659 9<sup>th</sup> Avenue and 660 10<sup>th</sup> Avenue in the City of San Diego. The Property is located on a block bounded by G Street on the North, 10<sup>th</sup> Avenue on the East, Market Street on the South and 9<sup>th</sup> Avenue on the West.

### 3.2 Project Area and Vicinity

The 660 10<sup>th</sup> Avenue and 659 9<sup>th</sup> Avenue properties and the immediate vicinity is much-modified mixed use area in the Centre City neighborhood of San Diego. A review of historic maps, archival materials, and aerial photographs, as well as physical inspection of the surrounding area, indicates that, by 1931, the 660 building was in place and no other buildings were present on this parcel. In 1946, the 659 building, a one story reinforced concrete 5,000 square foot addition was constructed on Lot C of this Block. Currently, a storage building has been added on the rear south façade, but it is not part of the historic resource.

A current aerial photo documents the vicinity today as mixed use of commercial and multi-family residential units. As documented by the 1921 Sanborn Maps, the area was originally residential, with a few commercial buildings.

### 3.3 Historical Overview

#### East Village History

The following history was prepared by Scott Moomjian for the East Village Combined Historical Property Survey Report in 2005. The history of the Bayside and Centre City East areas is inexorably linked to the development of downtown San Diego and its founder, Alonzo Horton. Horton, who came to San Diego in 1867, purchased 960 acres of land for \$265.00 for his “New Town” San Diego development. Horton divided the acres into blocks and lots, a number of which were present in the Bayside and Centre City East areas. Thus, “Horton’s Addition” was established.

During the 1860s and 1870s, the Bayside area, then known as “South San Diego,” relied heavily upon the wharf established at the foot of Fifth Avenue. Prior to the establishment of Horton’s wharf, a few warehouse structures were located along the bay, but none remain today. These early businesses transported stored grain, honey and other products. They also received incoming shipments of lumber, iron, ore, and other necessities from other parts of the nation. In 1869, McDonald’s store (later known as the San Diego Lumber Company) was erected at Sixth Avenue and L Street. Lumber floated down the bay from Northern California and Oregon was pulled from the bay by Native Americans and stacked at the lumber company building, which was erected on redwood planks in order to protect the wood from muddy soil. In 1872, Bailey’s Foundry (later known as San Diego Foundry) was constructed at the corner of 8<sup>th</sup> Avenue and M Street (now Imperial). This business provided much of the structural ironwork for businesses and commercial buildings in the Gaslamp Quarter (Lia/Brandes 1988:1; Lia/Brandes 1989:1; Lia 1999:1).

During the 1880s, New Town San Diego businesses spread north to H Street (currently Market Street), which was at the time considered the main cross road. Commercial growth was centered



around Fifth Avenue and Market Street. Ultimately, San Diego expanded to D Street (currently Broadway) and beyond as merchants moved northward. With the coming of the railroad, Horton and other prominent business leaders lobbied for a railroad link, which would facilitate transportation to the area. In 1885, the California Southern Railroad, a subsidiary of the Atchison, Topeka and the Santa Fe line, established tracks into San Diego from the north. This line greatly increased San Diego's population in the late 1880s. A later link with the Southern Pacific Railroad along the United States/Mexico border proved beneficial as well, particularly to the Bayside and Centre City East areas (Lia/Brandes 1988:1; Lia/Brandes 1989:1-2; Lia 1999:2). San Diegans, however, longed for an eastern railroad connection.

With the 1885 railroad connection, an influx of newcomers to the San Diego area sparked a four-year building boom. In the Bayside and Centre City East area, simple Victorian single-family cottages were erected on 16<sup>th</sup> and 17<sup>th</sup> Avenues to accommodate laborers, porters, clerks, and other blue-collar workers. The Rood Rental cottage, Bay View Hotel, Sheldon House and Joseph Ireland Building are examples of Victorian structures built during the boom. Standard Iron Works erected its manufacturing business on the corner of 7<sup>th</sup> Avenue and L Street, and the Silver Gate Warehouse, owned by local entrepreneur John Ginty, was constructed on 8<sup>th</sup> Avenue and M Street (later Imperial) (Lia/Brandes 1989:2; Lia 1999:2).

In 1886, the San Diego Gas Company enlarged its gas operations. The company, which had started from modest beginnings on 9<sup>th</sup> Avenue between M and N Streets (Imperial and Commercial) in 1881, expanded to order to supply the needs of San Diego's growing residential and business community. The San Diego Gas Company built a new electric generating plant at 10<sup>th</sup> Avenue and M (Imperial) adjacent to the old gas plant. In 1887, the San Diego Gas and Electric Light Company was incorporated as a successor to the San Diego Gas Company and began supplying electricity for arc lights on a sundown to midnight basis in downtown San Diego. After the bust of 1889, the gas and electric company found it could easily service the utility needs of San Diego with existing equipment. Therefore, no new major additions were made to the plant until 1905 (Lia/Brandes 1989:2; Lia 1999:2).

After 1900, businesses chose the Centre City East area from which to conduct operations due to the area's proximity to the railroad tracks and the wharf. San Diego's commercial center continued to expand as harbor facilities and the city's population grew. All types of commercial structures were built south of Broadway and along the water's edge. Warehouses, manufacturing centers, bars, restaurants, laundries, and hotels all provided services and goods for city residents and businesses. The entire area south of Broadway from the water to east was a large commercial center for the city. (Crawford 1996:6; Lia 1992:2).

Businesses anticipated the creation of a much needed railroad line that would link San Diego to Arizona and then eastward across the southern part of the United States. It was believed that such a line would encourage the exchange of goods between the West and East Coasts of the United States through Yuma. In 1905, the Southern Pacific Railroad officials approached sugar magnate and San Diego businessman, John D. Spreckels to act as the "front man" for an operation that would build the railroad from San Diego to Arizona. Although the San Diego and Eastern Arizona Railroad would not be completed until 1919, a railroad line and commercial

advantage speculation drew local businessmen to the Centre City East area (Lia/Brandes 1988:2; Lia/Brandes 1989:3; Lia 1999:3).

By 1906, three piers had been constructed in the Centre City East area. One was located at the foot of 6<sup>th</sup> Avenue, called the San Diego Lumber pier; one at the foot of 7<sup>th</sup> Avenue called the Sheldon pier; and one at the foot of 9<sup>th</sup> Avenue called the Bailey Pier. These piers, connected to various railroad spurs, enabled warehouses and commercial enterprises in the vicinity to receive goods from other parts of the nation and world. The railroad spurs themselves extended onto the piers, and goods were unloaded from ships, put on waiting boxcars, and taken directly to businesses, or put on one of the major railroad lines out of San Diego (Lia/Brandes 1988:2; Lia/Brandes 1989:3; Lia 1999:3).

Local companies took advantage of spur line use during the first three decades of the twentieth century. In 1909, the Western Metal Supply Company, located at 215 7<sup>th</sup> Avenue was constructed. Designed as an up-to-date modern building by Chicago architect, Henry Lord Gay, the company produced “everything in iron and steel from carpet tacks to structural beams.” Between 1910-1911, the Schiefer & Sons Warehouse was built on 8<sup>th</sup> Avenue. In 1911, the Simon-Levi Company building was erected at 7<sup>th</sup> Avenue and J Street in order to serve the wholesale grocery business. In 1912, development reached J Street. The Julian produce Company, the Hotel Salem (formerly identified as Loring stationers), and the Enid Apartments were all constructed in that same year. In 1913, the Nason and Company building was constructed in order to house the commission and wholesale produce enterprise. During this same year, the impressive Simon Levi Company Building was constructed on J Streets, as were the Broderick Apartments. Finally, in 1922, the San Diego Ice and Cold Storage Company took over the Silver Gate Warehouse, located at 800-822 Imperial Avenue and established “one of the finest plants in the city.” The company claimed that it was “located at the heart of the industrial district,” and had “excellent shipping facilities afforded by spur tracks to the San Diego and Arizona and Santa Fe railroads as well as to the waterfront” (Lia/Brandes 1988:2; Lia/Brandes 1989:3; Lia 1999:3).

During the 1920s, many San Diego businesses flourished in the Centre City East area. In 1921, the San Diego Broom Works Building was constructed on J Street. Three years later in 1924, the Showley Brothers Candy Factory was constructed at 305 8<sup>th</sup> Avenue. In 1926, the Ballinger Company warehouse was built at 944 K Street. In 1927, the Levi Wholesale Grocery Building was constructed at 330 8<sup>th</sup> Avenue, as was the Wheelworks Building on J Street. In 1928, the Qualitee Dairy commercial building was erected. Despite the fact that the Centre City East area of San Diego was affected by the Great Depression during the late 1920s and early 1930s, this area in general continued to benefit from new development. In 1930, the San Diego Gas & Electric Company constructed the San Diego Company Office Building on 10<sup>th</sup> Avenue. In some instances, however, businesses closed. Structures were left in a state of disrepair as some businesses migrated to locations north of Broadway (Lia/Brandes 1989:4; Lia 1995: 4; Lia 1999:3).

The Second World War affected the Centre City East area as it did the entire country. A surge of activity occurred in the Centre City East area. Very few structures were constructed during the war years, and although little new construction took place during the 1940s, many of the Centre



City East buildings were drafted into service. The Schiefer & Sons factory, located at 371 8<sup>th</sup> Avenue, was used by the Standard Parachute Corporation from 1941-1945. Although no longer in existence, the Standard Parachute Corporation also used the building located at 304 11<sup>th</sup> Avenue as a supply stockroom from 1943-1948. In addition, the San Diego Machine Company, an airline parts manufacturer, operated from 345 15<sup>th</sup> Avenue between 1934-1958. During the war years, the San Diego Gas & Electric Company had all new utility extensions put on hold, since copper wire and steel pipe were in short supply (Lia/Brandes 1988:4; Lia/Brandes 1989:4; Lia 1995: 4; Lia 1999:4).

During and after the Second World War, as suburbs developed, many businesses relocated to newer communities where land was cheaper and buyers more plentiful. The amount of people residing in the Centre City area also declined, resulting in less local support of goods produced by the downtown businesses. From the late 1970s to the present, Centre City East has slowly become revitalized with the development of the Gaslamp Quarter, which has brought new businesses and life into the old industrial area (Crawford 1996:6-7).

In 1999, portions of the Bayside and Centre City East areas were included in the cultural resources study prepared as part of the Final Subsequent Environmental Impact Report to the Final Master Environmental Impact Report for the Centre City Redevelopment Project and Addressing the Centre City Community Plan and Related Documents for the proposed Ballpark and Ancillary Development Projects, and Associated Plan Amendments.

In 2001, the subject property was included in the *Historic Site Inventory of Centre City East* for the Centre City Development Corporation by the Office of Marie Burke Lia. In 2003, that Inventory Form was updated by the same office. The 2003 Inventory Form found that the building merited designation under Criterion C as a building that embodied “the distinctive characteristics of a type, period or method of Art Deco construction.” In 2005, this Inventory was reviewed in the *East Village Combined Historical Surveys*, which were completed by Historical Resources Board Staff. That Survey agreed that the property was eligible under Criterion C for its Art Deco architecture.

## **4.0 METHODS AND RESULTS**

### **4.1 Archival Research**

Determinations of historical and architectural significance require a number of issues to be considered. Factors of significance include: the property's history, both construction and use; the history of the surrounding community; the potential for important persons or events to be associated with the property over its life span; the number of resources associated with the property; the potential for the resources to be the work of a master craftsman, architect, landscape gardener or artist; what historical, architectural or landscape influences have shaped the design of the property and its pattern of use; what alterations have taken place over the years and how have any changes affected the historical integrity of the property; and the current condition of the property. These questions and related issues must be answered before a final determination of significance can be achieved.

The archival research for this Historical Resources Technical Report included, but was not necessarily limited to, obtaining the Commercial-Industrial Building Record and the Lot Block Book pages from the San Diego County Assessor's/Recorder's Office; Chain of Title information prepared by California Lot Book, Inc.; historical and aerial photograph research, a review of the City of San Diego water and sewer department connection records; building permit applications at the City of San Diego Development Services Department; San Diego City Directories; Sanborn Fire Insurance Maps; vertical files, and the *San Diego Union* index and newspaper articles at the San Diego Public Library, California Room; the San Diego Historical Society archives and photographic collection; local, state, and federal inventories, surveys, and database material; the Historical Resources Board Files on this property, personal research archival material in the office of Marie Burke Lia; standard and authoritative sources related to local history, architecture, and building development information related to the building. The criteria for historical significance were obtained from the City's Guidelines for the Application of Historical Resources Board Designation Criteria, the National Register of Historical Resources Criteria and the California Environmental Quality Act (CEQA), which uses the California Register of Historical Resources Criteria.

A variety of resources provided the following history of the subject property.

The subject property contains a single story, One-Part Commercial building that occupies Lots A, B, C, K and L on the north almost 25,000 square feet of Block 83, which is bounded by 9<sup>th</sup> Avenue, G Street, and 10<sup>th</sup> Avenue in the East Village area of Centre City. The initial 20,000 square feet portion of the building was constructed on Lots A, B, L and K in 1932. In 1946, a one story, 1944 square foot addition was constructed on Lot C of this Block. Both buildings are constructed of poured in place concrete with painted exterior walls, have a general rectangular shaped plan, and were designed in the Art Deco architectural style, although the addition has minimal Art Deco features.<sup>1</sup> Both are in good condition.

Assessor's Office Information: According to the Property Information Sheet, the current building plus addition complex consists of 21,944 square feet of usable area on 24,829 square feet of land. A copy of this Property Detail Report is included in Appendix A.0 to this Report.

Assessor's Building Record: This Building Record identifies this property as "The Livery" and describes it as a single story parking garage and uses the 659 Ninth Avenue address for the entire building complex. It has poured in place concrete walls, concrete floors and a reinforced concrete foundation. Its exterior finish is painted concrete and its roof is flat and covered with built up roofing. Its usable square footage is estimated at 21,944 square feet including the 1946 addition. The second page graphic shows the large portion of the building on the G Street frontage as 100 x 200 square feet and it shows the 1946 addition as 54 x 36 or 1944 square feet. A copy of this Record is included in Appendix A.1 to this Report.

Notices of Completion: Two Notices were recorded for this property. In March of 1932, the first Notice was issued by the W.E. Kier Construction Company for the construction of a one-story reinforced concrete garage building at this site. The work was begun in December of 1931 and

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<sup>1</sup> Although the Assessor's Building Record cites concrete block as the building material, a current evaluation of these two buildings indicates that they are of the poured in place form.



completed by March of 1932. The property owner was identified as the Southern California Telephone Company. In July of 1946, the second Notice was issued by Contractor F. E. Young for the construction of a one-story reinforced concrete addition to the garage building at this site. This work of improvement was begun in October of 1945 and completed in July of 1946. The property owner was identified as the Southern California Telephone Company. Copies of these Notices are included in Appendix A.2.

Water and Sewer Records: Water and Sewer Records were located for 1932 building in that year. The Southern California Telephone Company was identified as the owner. Copies of these Records are included in Appendix A.3.

Building Permit Records: In January of 1969, Permit E41266 was issued to replace the wooden entrance doors with metal door and frame for the building used as a parking garage. The owner was the Pacific Telephone and Telegraph Company and the contractor was the Champion Construction Company. The estimated value of the work was \$300. In January of 1982, Permit AO2366 was issued to Pacific Telephone to reconstruct roof truss and ceiling due to fire damage. The contractor was the Champion Construction Company. The estimated value of the work was \$5,000. In November of 1982, Sign Permit S22336 was issued to Champion Construction Company. The estimated value of the work was \$29.00. In May of 1995 Plan File Number A103442-95 was approved for Clear Communications for a wall sign, Farkas Chung Lee is shown as the permittee. Copies of these permits are included in Appendix A.4.

Site Plan: A current Google Aerial Photograph of the property, with the two buildings labeled by address, and a recently drawn site plan have been included in Appendix A.5 to serve as the Site Plan for this Report.

Lot Block and Book Page: This page shows improvements on Lots A and B in 1932, when the property was owned by Southern California Telephone Company. A copy of this page is included in Appendix A.6.

Previous Survey Forms: In 2001, the subject property was included in the *Historic Site Inventory of Centre City East* for the Centre City Development Corporation by the Office of Marie Burke Lia. In 2003, that Inventory Form was updated by the same office, which then assumed that the entire building had been constructed in 1932. That Inventory Form found that the building merited designation under Criterion C as a building that embodied “the distinctive characteristics of a type, period or method of Art Deco construction.” This Form did not find that the property merited designation under any other criteria. In 2005, this Inventory was reviewed in the *East Village Combined Historical Surveys*, which were completed by Historical Resources Board Staff. That Survey agreed that the property was eligible under Criterion C for its Art Deco design, but also found it eligible under Criterion A as an “auto related” facility. Copies of these Survey Forms are included in Appendix A.7.

Chain of Title: In June of 1929, Lots A, B, C, K and L were conveyed by C A and Dessie S Minard to the Pacific Telephone and Telegraph Company. In 1932, the Assessor’s Lot Block Book Page in Appendix A.6. shows the first improvements on this property. In March of 1932 the first Notice of Completion was recorded and in July of 1946 the second Notice of

Completion was recorded for this property. These Notices are included in Appendix A.2. In March of 1985, Pacific Bell, formerly the Pacific Telephone and Telegraph Company, transferred this property to Elizabeth Ann Northern. In October of 1994, Fidelity Trust Deed Service Company, Trustee for Elizabeth Ann Northern Trust, conveyed the property to Gerry Hampshire and Nancy Owen. In March of 1995, Mr. Hampshire and Ms. Owen transferred the property to Chung H Lee and Young H Lee. In November of 2006, the Lees transferred the property to the Chung Lee and Young Lee Family Trust. In October of 2009, several transactions occur between Lee family members until the property is transferred to the 4 Lee Tenth LLC, the current owner. A copy of this Chain of Title is included as Appendix B.1.

City Directory: From 1988 to 2002, various tenants occupied this space. From 1937 to 1984, 660 Tenth Avenue was listed as Southern California Telephone Co. (Garage) and from 1990 to 1994 this portion of the building served as an airport & livery service. From 1997 to 2016, it served as Farkas Store and We Buy Used Inc.

From 1971 to 1984, 659 9<sup>th</sup> Avenue was listed in the City Directories as Pacific Telephone Telegraph Co. (Truck Repair). In 1988 it was listed as Automobile Leasing & Sales, in 1990 as the Livery Limousine, between 1997 and 1998 as VIP Spas and David Webb and in 2003 as Chung Lee, no other listings were found.

Newspaper research for these addresses found no articles. Newspaper research for Pacific Telephone found only advertisements dated 1916 and 1918, before the construction of this property.

Deeds from date of Construction: The June 1929 Deeds from CA and Dessie S Minard to Pacific Telephone and Telegraph Company conveying Lots A, B and C (document #37281) and Lots K and L (document #37284) are included in Appendix B.3

800 Scale Engineering Map and USGS Map: Copies of these Maps are included in Appendices C.1 and C.2.

Original Subdivision Map and Current Parcel Map: Copies of the original Map of Horton's Addition and the current Parcel Map are included in Appendices C.3 and C.4.

Sanborn Fire Insurance Maps: The Sanborn Map for 1921 shows the former buildings in place on this site. The Sanborn Maps for 1950 through 1972 show the current buildings in place. Copies of these Maps are included in Appendix C.5.

Historical Photographs: Photographs of all three street facades of these two buildings were located from July of 1980. They are included in Appendix D.1.

Current Photographs: Current photographs of all three street facades of these two buildings, with close, are included in Appendix D.2.



## 4.2 Subject Property History

The subject property is located at 660 10<sup>th</sup> Avenue and 659 9<sup>th</sup> Avenue and in the East Village, Downtown area of San Diego, California.

## 4.3 Field Survey

The field survey work was conducted by Marie Burke Lia, Dolores Mellon and Amanda Daghalay in 2017 and 2019. HABS documentation and photography of the building will be undertaken by John Eisenhart in 2019. An intensive survey of the subject property and surrounding neighborhood was undertaken. The Area of Potential Effect (APE) is, in this instance, the portion of the subject property containing the building addressed as at 660 10<sup>th</sup> Avenue and 659 9<sup>th</sup> Avenue.

## 4.4 Description of Surveyed Resources

### 660 Tenth Avenue and 659 9<sup>th</sup> Avenue

The subject property contains a two-part Art Deco style building complex that was originally used as a garage for the Southern Telephone Company and for the Pacific Telephone and Telegraph Company. This initial construction of the one-story building in 1932 measured 20,000 square feet of usable garage space. In 1946, a one-story, 1,944 square foot addition was constructed. In later years, the building complex was converted from garage to warehouse use.

Both buildings are one-story in height with reinforced concrete foundations, concrete floors, and poured in place concrete walls with painted exteriors. The roof is flat with built up sections. Both sections of the building are basically rectangular in shape.

Art Deco design elements that are present in the overall design of the building complex include: large raised panels with vertical detailing. The three-sided framing of these panels rises above the roofline and the panel sections are present on the main elevations. Raised flat horizontal bands are present around the edge of the roofline and connect the vertical panels. Some of the vertical panels contain metal foldup style garage doors; others contain metal framed, fixed pane, multi-lite window sections. Additional matching windows, but smaller in size, are evenly spaced along the main elevations outside the raised vertical sections. The building also contains multiple, metal, single door entrances. The 10<sup>th</sup> Avenue elevation presents a large window section which contains three narrow vertical windows in two rows which suggest the larger framed sections that are placed on either side of this window section. Metal garage door openings flank this window section. The 9<sup>th</sup> Avenue addition has significantly less décor.

Both buildings are in good condition with no significant major exterior alterations noted.

### Alterations

In 1946, the approximately 1,944 square foot addition was constructed. A review of the building permits filed for the property reveal that in January of 1969, Permit E41266 was issued to

replace the wooden entrance doors with metal door and frame for the building used as a parking garage. The owner was the Pacific Telephone and Telegraph Company and the contractor was the Champion Construction Company. The estimated value of the work was \$300. In January of 1982, Permit AO2366 was issued to Pacific Telephone to reconstruct roof truss and ceiling due to fire damage. The contractor was the Champion Construction Company. The estimated value of the work was \$5000. In November of 1982, Sign Permit S22336 was issued to Champion Construction Company. The estimated value of the work was \$29.00. In May of 1995 Plan File Number A103442-95 was approved for Clear Communications for a wall sign, Farkas Chung Lee is shown as the permittee. No other major interior or exterior alterations were noted.

## 5.0 SIGNIFICANCE EVALUATIONS

### 5.1 Historic Context

*According to the Guidelines for the Application of Historical Resources Board Designation Criteria, the significance of a historic property can be judged and explained only when it is evaluated in its historic context. Historic contexts are those patterns or trends in history by which a specific occurrence, property or site is understood and its meaning (and ultimately its significance) within history is made clear. In order to decide whether a property is significant within its historic context, the following things must be determined.<sup>2</sup>*

1. *Identify the themes, geographical limits and chronological period that the property represents:*

The subject building was constructed as a garage, later a warehouse, in the highly urbanized East Village area of the City of San Diego. The building is part of the pattern of the overall commercial development of the East Village area of San Diego in the 1930s and 1940s. The Art Deco architectural style was popular in the 1930s and other fine examples of this building style were constructed in the city during the 1930s. The theme of the context is the development of Art Deco style commercial buildings in the East Village area in the 1930s and 1940s.

2. *Determine how the theme of the context is significant in the history of the local area:*

The East Village area of the city of San Diego has been an important commercial and residential area of the city throughout the last decades of the 19<sup>th</sup> century and the first half of the twentieth century. The area evolved over time to include significant commercial development which included a wide range of industrial uses. The development of the subject property is part of this process of transition, growth, and commercial expansion of the East Village over the last one hundred years.

3. *Determine what the property type is and whether it is important in illustrating the historic context:*

The property type was originally that of a garage, later a warehouse, building. This use is consistent with the predominant industrial use of the properties in the East Village section of the city of San Diego. The building at 660 Tenth Avenue is a good example of an Art Deco style

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<sup>2</sup> Guidelines for the Application of Historical Resources Board Designation Criteria, page 2



commercial building. The 1946 addition has limited design features. The main building is important in illustrating the historic architectural context of the East Village area.

4. *Determine how the property represents the context through HRB Criteria:*

The property represents a *special element* of the City's development under Criterion A: Economic Development for its auto-related uses and the growing importance of the automobile. The property is not identified with persons or events significant in local, state or national history under Criterion B: Person. The property does embody the distinctive characteristics of a style, type, period or method of the Art Deco architectural style under Criterion C: Architecture to the level necessary to be determined architecturally significant. The building does not exemplify the use of indigenous materials or craftsmanship under Criterion C. The 1932 building was built by Contractor W.E. Kier; the 1946 addition was built by Contractor F. E. Young. Neither man has been determined to be a Master Builder by the City of San Diego and, therefore, they do not meet the criteria for Criterion D: Master Builder. None of the other established criteria are applicable to the subject property.

5. Determine what physical features the property must possess in order for it to reflect the significance of the historic context:

The property reflects 1930s Art Deco construction in the East Village. The building contains the key character defining features of the Art Deco style and it is a good example of Art Deco construction and design. The building also retains the character defining features of its auto-related uses.

5.2 Application of the City's Guidelines for the Application of Historical Resources Board Designation Criteria

According to the City's Land Development Manual's Historical Resources Guidelines, any improvement, building, structure, sign, interior element and fixture, site, place, district, area or object may be designated as a historic resource by the City of San Diego Historical Resources Board if it meets one or more of the following criteria.

*Criterion A: Community Development:* *If it exemplifies or reflects special elements of a City's, a community's or a neighborhood's historical, archaeological, cultural, social, economic, political, aesthetic, engineering, landscaping or architectural development. Special elements of development refer to a resource that is distinct among others of its kind or that surpass the usual in significance.*<sup>3</sup>

Pursuant to these Guidelines, "special elements of development refer to a resource that is distinct among others of its kind or that surpass the usual in significance." In order to qualify for designation under Criterion A, a property must exemplify or reflect *special elements* of the East Village's historical, archaeological, cultural, social, economic, political, aesthetic, engineering, landscaping or architectural development. Specifically, 660 10<sup>th</sup> Avenue and 659 9<sup>th</sup> Avenue was evaluated as an auto-related property.

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<sup>3</sup> Guidelines for the Application of Historical Resources Board Designation Criteria, page 7.

*Does the resource reflect or exemplify special elements of the development of the City, community or neighborhood? If so, under what aspect of development? For each aspect of development, the resource shall exemplify or reflect a special element of that development.*

The issue for Criterion A is whether the subject building is *distinct* among other commercial buildings in the Centre City area of San Diego and whether it is distinct from or surpasses the usual of such buildings in significance.

*Historical Development* shall exemplify or reflect a special or unique aspect of the City's general historical development; or shall exemplify or reflect a unique aspect of the City's history.

For a property to exemplify or reflect the *Historical Development* of Centre City, a property would need to represent building and historical patterns in a manner distinct from other properties in the area. Such as the first housing or commercial businesses to be built or founded in the area. The East Village of Centre City began its commercial development in the 1860s and 1870s because of the wharfs that supplied all the goods and necessities that were shipped in to support the young city, as discussed in the East Village History above in this Report.

*Archaeological Development* exemplifies subsurface deposits and features that were part of early human development on the site. No such deposits or features exist at the subject property therefore this Aspect of Criterion A does not pertain to the subject property.

*Cultural Development* exemplifies or reflects development that is associated with a group of people linked together by shared values or beliefs, properties associated with significant achievements in the visual and fine arts, or disciplines associated with the public or private institutions of higher learning and/or academic inquiry. There is no evidence of this property's association with a group of people linked together in this manner and, therefore, this Aspect of Criterion A does not pertain to the subject property.

*Social Development* exemplifies or reflects development that is associated with relations or interactions with others. The subject property was a parking garage for the telephone company employees. There is no evidence of this property's association with relations or interactions between the tenants over the years and, therefore, this Aspect of Criterion A does not pertain to the subject property.

*Economic Development* shall exemplify or reflect development associated with the local, regional, state or national economy or economics, including manufacturing, labor, agriculture, maritime or transportation industries.

The subject property, built as an employee parking garage in the East Village, was identified in *The Combined Historical Property Survey Report, 2005* for its auto-related uses and the growing importance of the automobile. Construction of the resource and the addition (1932 and 1946), fits within the rise of automobiles in San Diego, and serves as "a special element of the City's economic development, revealing the increasing importance of the automobile as America's favored method of private transportation from 1900-1956." <sup>4</sup>

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<sup>4</sup> City of San Diego Memorandum, Kelley Stanco to the Historical Resources Board, dated 3/12/2015.



From 1937 through 1984, 660 10<sup>th</sup> Avenue served as the Southern California Telephone Company Garage. The addition, constructed in 1946, also served the phone company as a truck repair station. This Aspect of Criterion A, *Economic Development*, therefore, does pertain to the subject property.

*Political Development* shall exemplify or reflect development associated politics or the political atmosphere or related movements. The subject property had no such association, therefore it cannot be significant per this Aspect of Criterion A.

*Aesthetic Development* shall exemplify or reflect development associated with an artistic arrangement in theory or practice. The subject property is an example of the Art Deco style of architecture that was popular during this period. The subject property is a standard commercial building and not representative of an artistic arrangement in theory or practice significant to the East Village's *Aesthetic Development* and therefore it cannot be significant under this Aspect of Criterion A.

*Engineering Development* shall exemplify or reflect development associated with engineering. The First Avenue Bridge is such a development. The subject property does not exemplify any new or innovative *Engineering* techniques or development therefore it cannot reflect significant *Engineering Development* and this Aspect of Criterion A.

*Landscape Development* shall exemplify or reflect development associated with garden and park design, subdivision design or ecosystem/habitat restoration. The subject property has no landscaping therefore it cannot exemplify this Aspect of Criterion A.

*Architectural Development* shall exemplify or reflect development associated with the city's built environment, especially that designed and constructed by non-architects, including real estate developers, contractors, speculators, homeowners and others associated with the building industry. This parking garage was built to serve the employees of the telephone company and it happened to include design features from the Art Deco style that was popular at the time, but it does not exemplify or reflect the Architectural Development Aspect of Criterion A.

Conclusion: The subject property does merit local designation under the City's Criterion A in that it does exemplify and reflect one of the ten Aspects of development as defined in that Criterion, namely *Economic Development*.

Criterion B: Person or event: *Is identified with persons or events significant in local, state, or national history. Persons significant in our past refer to individuals associated with San Diego whose activities, achievements and contributions are demonstrably important within the City, state or nation. Resources associated with historical events are those associated with a single event such as the place where an important battle occurred, a building in which an important invention was developed, or a factory district where a significant strike occurred.*<sup>5</sup>

No person or event of significance is identified with the development, construction or use of this property. Designation under Criterion B is not merited for this property.

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<sup>5</sup> Guidelines for the Application of Historical Resources Board Designation Criteria, page 15.

Criterion C: Architecture: Embodies distinctive characteristics of a style, type, period or method of construction or is a valuable example of the use of indigenous materials or craftsmanship; The subject property is located at 659 6<sup>th</sup> Avenue and 660 10<sup>th</sup> Avenue in East Village, San Diego. The building was constructed in the Art Deco style; therefore, it was evaluated as an example of that style.

*“Style of construction” means the composition, massing, scale, materials and details that exhibit the essential physical features of a recognized architectural style.*<sup>6</sup>

#### Art Deco Architectural Style

The Art Deco style was developed from European roots in the 1920s. The widely acknowledged source of the style was the Exposition Internationale des Artes Decoratifs et Industriels Modernes in Paris in 1925. According to David Gebhard, noted architectural historian,

a recurring theme of the 1920s and 1930s, ... was the desire to seek out new forms or modifications of old forms to express the continually changing character and accelerated tempo of the new age. The machine and technology, especially the automobile, were seen as new nontraditional sources for architecture...It was the Art Deco and the Streamline Moderne that caught the eye and held the attention of most Americans... Untold numbers of commercial and public buildings adopted a stylish image in the decades following World War I.<sup>7</sup>

Gebhard describes the main attributes of the style in the following discussion:

...the Art Deco is characterized by ‘Straight lines; it is angular, geometric and tends to follow cubist proportions...The lines are unvarying plain and severe, with touches of decoration in the way of color, wrought iron and glass work, for relief.’ What should also be noted as primary are the importance the style placed on ornament, especially sculptural ornament, and the direct manner in which the Art Deco style was nourished by its historical roots.”<sup>8</sup> Many of the leading examples of the style were produced by architects educated or indirectly educated within the Parisian Beaux-Arts system and the forms they produced were largely derived from classical precedent. Architects drew their design inspirations from ancient cultures, not just Greece and Rome, but Egypt, Mesopotamia, the Aztecs and Mayans of the New World, the Native American populations of North America and incorporated these ideas into a classical Beaux-Arts framework. According to Ave Pildas, in her book, *Art Deco Los Angeles*, “Art Deco was obsessed with total design, as exemplified by the bas reliefs, the sculptural fountains, exterior and interior ornamentation.”<sup>9</sup>

Gebhard goes on to say that,

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<sup>6</sup> Guidelines for the Application of Historical Resources Board Designation Criteria, page 21.

<sup>7</sup> Gebhard, David, *Art Deco in America*, p.1.

<sup>8</sup> Gebhard David, *Art Deco in America*, p.4

<sup>9</sup> Pildas, Ave, *Art Deco Los Angeles*, p.5



what separates Art Deco from other contemporaneous modes is, above all, it's approach to ornament and surface sheathing. The general tendency was to exhibit exterior walls that expressed little depth or projection. In many Art Deco buildings, the style's characteristic emphasis on verticality was manifest in a row of piers or pilasters that subtly represented a classical portico or temple front... typical of the style was an absence of a cornice or other device to provide a vertical conclusion. The 1920s styles, especially the Art Deco, delighted in experimenting with the numerous metal alloys introduced in the course of the decade. All sorts of mixtures of steel, bronze, nickel, silver, platinum, lead, and zinc were used for elevator doors, window frames, spandrels, decorative panels, and sculpture. Lightweight aluminum also came into its own in these years, and the Art Deco architects were obviously fascinated with it, both as a material in its own right and, with plating applied, as a substitute for other materials: bronze, nickel, silver, even gold.<sup>10</sup>

The color of buildings was a key element of the style and architects were influenced by the rise of the automobile. Gebhard quotes Sheldon Cheney in his discussion of color.

The automobile with its firm but soft coloring and its flashes of bright metal may again afford us a clue.” Gebhard expands on this idea by stating that: “Such a scheme - ‘firm but soft color’ – contrasted with ‘bright metal’ - was certainly one of the hallmarks of the Art Deco, employed in the production of objects both large and small. The Art Deco typically contrasted warm tans and pale shades of green and blue with shiny metals or with accents of strong pure color - vehement reds, cobalt blues, or golden yellows. The style also exploited the drama of light and shadow through the adroit use of electric lighting. The Art Deco’s’ most dramatic employment of artificial lighting was the nighttime illumination of building exteriors.<sup>11</sup>

The Art Deco took two approaches to ornament: the first was to make ornament integral to the surface upon which it was placed; the second was to confine the ornament to a panel that hovered (or seemed to hover) in front of the wall surface. Favorite motifs in Art Deco ornament included spirals, sunflowers, steps, zigzags, triangles, double triangles, hexagons, fragmented circles and seashells. The patterns containing these motifs were generally rendered in low relief with sharp angular contours. Architectural details underwent a reductive process; in many Art Deco buildings vertical fluting along an exterior surface constituted the only residue of a classical column.<sup>12</sup>

Additional viewpoints into the origins and development of the style come from the work of Carla Breese, noted architectural historian. Ms. Breese feels that, in addition to the Parisian origin points of the style, the style owes its development to avant garde artists and architects of earlier decades, as well as the early cultures of the Americas. The Navajo, Hopi, Anasazi, and Plains cultures influenced the Prairie school and the Arts and Crafts movement. Flora and fauna,

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<sup>10</sup> Gebhard, David, *Art Deco*, p.5.

<sup>11</sup> Gebhard, David, *Art Deco in America*, p.6.

<sup>12</sup> Gebhard, David, *Art Deco in America*, pp. 6-7.

building designs, and the cultural traditions of blending nature and life all had an effect as architects and artists explored new design concepts. The Mesoamerican cultures, combined with the later Hispanic heritage of the Southwest, brought new ideas into the design vocabulary. Modernism inspired a whole exploration of new materials; new technological advances brought new materials into play. Metal, mosaic, concrete and terra cotta were materials that architects and artists experimented with to create their designs and adapted them to the angular, mechanized aesthetic that grew out of mass produced objects. Ms. Breeze suggests that: “new materials, new metals, new structural materials, new fabrics, compounds, alloys and aggregates... while the old materials, such as glass, wood, ceramics, marble and established metals are being presented with new finishes, new properties and new advantages.”<sup>13</sup>

Ms. Breeze addresses the regional stylistic differences in her work and each region of the United States had influences and origins that differed somewhat from other sections of the country. In discussing the local area, she stated that,

California was devoted to design accommodating automotive traffic...The style rapidly spread to the suburbs as the predominant commercial style. Reflecting the region's unique cultural and environmental heritage, imagery, such as foliation, scenes of paradise, nudes, and fruit are frequently encountered in the Art Deco style. The Pacific Ocean inspired motifs - seashells, crabs and fish, mermaids, and mythological figures associated with the sea, such as Neptune.<sup>14</sup>

Numerous examples of the style exist across the country, and while many of the buildings are large skyscraper style structures, the design concepts filtered down to small town America. They were reduced, modified, or altered to fit the regional ideals and many Art Deco buildings were created in urban America. The style tended to be more commercial in nature and better suited to the urban development of the cities of America. Some residential examples exist, but in most cities, the primary examples of the style are to be found in the commercial centers of the city. Art Deco elements include: smooth wall surfaces, usually of stucco; zigzags, chevrons, and other stylized and geometric motifs occur as decorative elements on the facade; towers and other vertical projections above the roof line give a vertical emphasis.

#### Primary Characteristics

- \*Smooth wall surfaces, usually of stucco
- \*Zigzags, chevrons, other forms of geometric detailing
- \*Tower and other vertical projections
- \*Vertical and horizontal detailing
- \*Pilasters
- \*Vertical fluting
- \*Vertical projections above the roofline

#### Secondary Characteristics

- \*Use of bright colors
- \*Use of elements of cultures such as Anasazi, Egypt, Mesopotamia, Aztecs, Mayas

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<sup>13</sup> Breeze, Carla, American Art Deco, pp. 13-17.

<sup>14</sup> Breeze, Carla, American Art Deco, pp. 223-235.



- \*Wrought iron
- \*Glass work
- \*Use of lead, silver, bronze, aluminum and other metals
- \*Spandrels
- \*Decorative Panels

“Style of construction” means the composition, massing, scale, materials and details that exhibit the essential physical features of a *recognized architectural style*. In order to determine whether this commercial building exhibited the essential physical features of a recognized architectural style, the façade of the building was evaluated as an example of the Art Deco style of architecture.

The subject building contains many of the primary character defining features of the Art Deco architectural style, including:

- \*Geometric detailing in the design of the large framed sections that rise above the roofline
- \*Smooth stucco exterior
- \*Flat roof
- \*Straight lines
- \*Lack of ornamentation in the overall design
- \*Angular design

The subject building is considered to be a good example of c. 1930s Art Deco design for an industrial type building in the East Village area of the city of San Diego under Criterion C: Architecture. The City of San Diego has designated 25 properties as significant examples of the Art Deco style. Four of those properties are superior examples of the style, but most are mediocre. The subject property fits within the latter category.

“Type of construction” means the form and materials clearly demonstrate, through the presence of essential physical features, a specific purpose and/or function. <sup>15</sup>

The subject building is a garage and warehouse building, intended for commercial use, that lacks essential physical features that demonstrate a specific purpose and/or function as required by this Criterion.

“Method of construction” means it is a rare or an important example of building practices, construction innovations, or technological advances during a specific time in history. <sup>16</sup>

The former garage/warehouse building appears to be of standard construction. No evidence was located to indicate that the building used a unique or unusual method of construction in either the original construction or the 1946 addition. The method of construction of the subject property does not represent an important example of building practices or construction innovations and, therefore, a Method of Construction as defined by the Designation Guidelines for Criterion C.

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<sup>15</sup> Guidelines for the Application of Historical Resources Board Designation Criteria, pages 22-23.

<sup>16</sup> Guidelines for the Application of Historical Resources Board Designation Criteria, page 23.

*“Period of construction” means the age and physical features reflect the era when the specific recognized architectural style, building type, or method of construction became popular.*<sup>17</sup>

The subject building was built in 1932 with a subsequent complementary addition in 1946. The building as it stands today is a good example of 1930s Art Deco architectural style which was popular during this era.

*“Craftsmanship” means the resource exemplifies high craftsmanship and design with handcrafted, unique and rare elements and may or may not be catalogued as belonging to a specific architectural style. The resource’s design elements and/or features, or association with an individual’s design approach or philosophy is unique in nature and does not reflect a specific design or style, but is a reflection of a very personal and/or creative effort.*<sup>18</sup>

The building complex is a standard example of a 1930s era Art Deco style industrial garage/warehouse type of building. No evidence of rare, important, unique, or a design philosophy is present in the examination of the subject building. The building does not represent the concept of high craftsmanship in its overall design, construction or materials used for the building.

*“Indigenous Materials” means the resource is a valuable example of the use of indigenous materials and may include the interior. The resource’s design elements and/or features, including design context and period of design, or association with an individual’s design approach is reflective of indigenous materials and available craftsmanship.*<sup>19</sup>

Due to the fact that no indigenous materials went into the construction of the building, the building is not a valuable example of the use of indigenous materials or craftsmanship.

The building, therefore, is considered a good example of the Art Deco Style as constructed in East Village in the 1930s. The 1946 addition also reflects the original design. The building has maintained the key character defining features of the Style. The building qualifies for designation under Criterion C: Architecture.

*Criterion D: Master Builder: Is representative of notable work of a master builder, designer, architect, engineer, landscape architect, interior designer, artist, or craftsman. A resource must express a particular phase in the development of the master’s career, an aspect of his or her work, or a particular idea or theme in the craft. The property is not eligible under Criterion D simply because it was designed by a prominent architect, builder, etc., it must be the work of a master. Additionally, not all examples of a master’s work are eligible. Criterion D requires the resource to be representative of the notable work of the Master, as demonstrated in the nomination.*<sup>20</sup>

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<sup>17</sup> Guidelines for the Application of Historical Resources Board Designation Criteria, page 23.

<sup>18</sup> Guidelines for the Application of Historical Resources Board Designation Criteria, page 24.

<sup>19</sup> Guidelines for the Application of Historical Resources Board Designation Criteria, page 26.

<sup>20</sup> Guidelines for the Application of Historical Resources Board Designation Criteria, page 28.



Contractors Kier and Young have not been determined to be a Master Builders in the city of San Diego. The San Diego Historic Resource Board Guidelines state: "A master is a figure of generally recognized greatness in a field, a known craftsman of consummate skill, or an anonymous craftsman whose work is distinguishable from others, by its characteristic style and quality." There is no evidence to indicate that either contractor met that standard.

Conclusion: The subject property embodies the distinctive characteristics of a style and method of Art Deco Architecture and is eligible for local designation under that criterion.

Criteria A and C, used for designation by the City of San Diego, apply to this property.

### 5.3 Integrity

In addition to determining the significance of a property under local, state and federal criteria, it is necessary to assess whether the property has integrity. Integrity is the ability of a property to convey and maintain its significance. A property must not only be shown to be significant under the established criteria, it must also have integrity. In order to retain historic integrity, a property must possess several, and usually most, of the seven key aspects of integrity, which are location, design, setting, materials, workmanship, feeling and association.

*Application of the City's Guidelines for finding Integrity:*

*According to the City's Guidelines for the application of the Historical Resources Board Designation Criteria, there are two important principles for understanding Integrity:*

- 1. Integrity is the authenticity of a historical resource's physical integrity clearly indicated by the retention of characteristics that existed during the resource's period of significance.*
- 2. Integrity relates to the presence or absence of historic materials and character defining features.*

*Historical resources eligible for designation by the HRB must meet one or more of the designation criteria and retain enough of their historic character or appearance to be recognizable as historical resources and to convey the reasons for their significance.*<sup>21</sup>

Application of the seven aspects of integrity:

Location: *Location is the place where the historic property was constructed or the place where the historic event occurred.* The subject building remains in its original location.

Design: *Design results intentional decisions made during the conception and planning of a resource. Design includes form, plan, space, structure, and style of a property.* The building retains its basic 1932 design, as well as the design of the 1946 addition. Therefore, the form, plan, space, structure and style of the property are intact.

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<sup>21</sup> Guidelines for the Application of Historical Resources Board Designation Criteria, pages 3-4.

Setting: *Setting applies to the physical environment, the character of a resource's location, and a resource's relationship to the surrounding area.* As indicated by the review of the Sanborn Maps and current aerial photographs, as well as visual observation of the subject property and surrounding area, the property has not maintained its original historic physical environment due to the extensive redevelopment of this area since the 1970s, and, therefore, its 1930s/1940s setting is not intact.

Materials: *Materials comprise the physical elements combined or deposited in a particular pattern or configuration to form a property.* With the exception of 1982 fire damage that required the reconstruction of the roof truss and ceiling at 660 10<sup>th</sup> Avenue, the building has retained its original materials. Therefore, the building has retained this aspect of its integrity.

Workmanship: *Workmanship consists of the physical evidence of crafts employed by a particular culture, people or artisan, which includes traditional, vernacular, and high styles.* The original quality workmanship of the 1932 main building and 1946 addition has been retained.

Feeling: *Feeling relies on present physical features of a property to convey and evoke an aesthetic or historic sense of past time and place.* The present physical features convey a 1932 Art Deco architectural expression of an industrial garage/warehouse structure.

Association: *Association directly links a historic property with a historic event, activity, or person of past time and place; and requires the presence of physical features to convey the property's historic character.* The property has been determined to be directly linked, under HRB Criterion A, for its association with the rise of automobile-related uses as part of San Diego's economic development. Therefore, it does have an associative element.

**Conclusion:** Of the seven aspects of integrity, the building does not retain one of the seven aspects of integrity - setting. It has maintained the other six aspects of integrity. Therefore, it has retained sufficient Integrity.

#### 5.4 Application of California Register of Historical Resources Criteria

Properties that are subject to the California Environmental Quality Act (CEQA), must be evaluated for historical significance under the California Register of Historical Resources. The criteria for evaluating the significance of historical resources requires that the resource be significant at the local, state or national level under one or more of the following four criteria.

(1) Association with Events: *It is associated with events that have made a significant contribution to the broad patterns of local or regional history, or the cultural heritage of California or the United States.*

No historical evidence was found that would support the determination that the subject property was associated with events that made a significant contribution to the broad patterns of local or regional history, or the cultural heritage of California or the United States. The subject property does not merit designation under California Register Criterion (1).



(2) Association with Persons: *It is associated with the lives of persons important to local, California or National History.*

No historical evidence was found that would support the determination that the subject property was associated with the lives of persons important to local, California or National History. The subject property does not merit designation under California Register Criterion (2).

(3) Design/Construction: *It embodies the distinctive characteristics of a type, period, region, or method of construction, or represents the work of a master or possesses high artistic values.*

No evidence was found that would support the determination that the subject property embodied the distinctive characteristics of a significant type, period, region or method of construction to the extent necessary for listing the property on the California Register. While the subject property has been found to be architecturally significant as a local example of 1930's Art Deco design, it would not qualify for listing on the California Register under this criterion because: it is a simple and limited example of this style; it is not the work of a master; it does not possess high artistic values; and superior examples of this style are found elsewhere in the city. The subject property does not merit designation under California Register Criterion (3).

(4) Archaeology: *It has yielded or has the potential to yield information important to the prehistory or history of the local area, California or the nation.*

To be designated under this criterion the property must have information to contribute to our understanding of human history and prehistory and that information must be important. The subject property does not merit designation under California Register Criterion (4).

Conclusion: The property does not merit listing on the California Register.

## 5.5 Application of National Register of Historical Places Criteria

Criterion A: Event: *Properties can be eligible for the National Register if they are associated with events that have made a significant contribution to the broad patterns of our history.*

No historical evidence was found that would support the determination that the subject property was associated with events that made a significant contribution to the broad patterns of our history. The subject property does not merit designation under National Register Criterion A.

Criterion B: Person: *Properties may be eligible for the National Register if they are associated with the lives of persons significant in our past.*

No historical evidence was found that would support the determination that the Property was associated with persons significant in our past. The subject property does not merit designation under National Register Criterion B.

*Criterion C: Design/Construction: Properties may be eligible for the National Register if they embody the distinctive characteristics of a type, period, or method of construction, or that represents the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction.*

No evidence was found that would support the determination that the property embodied the distinctive characteristics of a “Type, Period and Method of construction,” which, under this criterion, refers to the manner in which properties are related to one another and is not applicable here. A property is eligible as a specimen of its type or period of construction under this criterion if it is an important example of building practices of a particular time in history, which is not the case here. The subject property was constructed under standard building practices that existed in the 1930s, 1940s and up to today. Neither of the contractors involved have been identified as masters in their field. “High artistic values” under this criterion refers to properties that so fully articulate a particular concept of design that they express an aesthetic ideal, which is not the case here. The terminology referring to “components of an entity” under this criterion are intended to address historic districts. The subject property does not merit designation under National Register Criterion C.

*Criterion D: Information Potential: Properties may be eligible for the National Register if they have yielded or are likely to yield information important in prehistory or history.*

This criterion is intended to address archaeological resources. To be designated under this criterion the property must have information to contribute to our understanding of human history and prehistory and that information must be important. This criterion is not applicable to this property. The subject property does not merit designation under National Register Criterion D.

**Conclusion:** The property does not merit listing on the National Register today.

## 5.6 Findings and Conclusions

This Historical Resource Technical Report for the building at 660 10<sup>th</sup> Avenue and 659 9<sup>th</sup> Avenue in the East Village area of Centre City San Diego has determined the overall historical and architectural significance of the property.

The Historical Resource Technical Report Guidelines require that this section of the Report summarize significant (eligible for designation) historical resources in the Project Area. The following information is included here for this purpose.

The Project is located at 660 Tenth Avenue and 659 Ninth Avenue in downtown San Diego. Its Project Area is bounded by Broadway on the north, 12<sup>th</sup> Avenue on the east, J Street on the south and 6<sup>th</sup> Avenue on the west. The Area consists of the 36 blocks that are south of Broadway and East of the Gaslamp Quarter.

*Two Properties in the Project Area are listed on the National Register:*

733 Eighth Avenue – Eagles Hall  
815 E Street – U.S. Post Office



*Twenty-five Properties in the Project Area were listed on the San Diego Register prior to 2005:*

765 Tenth Avenue – Buckner Hotel  
611 Island Avenue – Klauber-Wagenheim Building  
861 Sixth Avenue – Timken Building  
715 J Street – Simon Levi Building  
330 Eighth Avenue – Levi Wholesale Grocery  
354 Eleventh Avenue – Carnation/Qualitee Dairy  
629 J Street – Julian Produce Company  
400 Eighth Avenue – Fire Station #4  
901 E Street – Guymon-Fletcher-Lovett Building  
906 Tenth Avenue – First Baptist Church  
371 Eighth Avenue – Shieffer & Sons Warehouse  
808 J Street – Wellman Peck/TR Produce  
911 Sixth Avenue – Leland Hotel  
501 Seventh Avenue – Clermont/Coast Hotel  
927-945 Broadway – Frances Apartments  
950 Ninth Avenue – Carnegie Apartments  
901 Tenth Avenue – Amelia Apartments  
930 Tenth Avenue – First Baptist Church Annex  
820 E Street – Former City Library  
1401 J Street – Carter Hotel  
612-640 F Street – Maryland Hotel  
447 Ninth Avenue – Hiatt Family House  
701 Island Avenue – Bledsoe Furniture Company  
704 J Street – Western Wholesale Drugs  
941 Eleventh Avenue – Hamilton Apartments

*Three additional Properties in the Project Area were listed on the San Diego Register between 2005 and 2019:*

1327 – 1335 E Street – Custer and Carper Apartments  
612 E Street – The Maryland Hotel  
509 Park Boulevard – The New Bay View Hotel

Conclusion: 30 Significant Historical Resources are present within the 36 blocks of this Project Area.<sup>22</sup>

The subject building is eligible for the local Register of Historical Resources under Criterion A, economic development, for its auto-related uses, and as a local example of the Art Deco architectural style under local Criterion C. The subject property is not eligible for listing on the California Register of Historical Resources or the National Register of Historic Places.

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<sup>22</sup> Sources: *Historical Resources Report for Proposed Downtown Community Plan* by the Office of Marie Burke Lia, Historical Preservation Legal Consultant to CCDC and 2005 *Combined East Village Surveys* by Diane Kane of the Historical Resources Board Staff.

The Staff Report to the Historical Resources Board, dated November 8, 2019, concurred with these Findings and recommended a period of significance of 1932 and 1932-1956, and designation under HRB Criterion A as a special element of the City's economic development, revealing the increasing importance of the automobile as America's favorite method of private transportation from 1900 to 1956. The Report also found that the property retained integrity to its 1932 – 1956 period of significance, and, because the resource was constructed for use as a garage in 1932 and it continues to convey the significant character-defining features of auto-related development, the resource is consistent with the East Village Historical Context Statement. The building's single story, warehouse-like massing, limited ornamentation, raised vertical projections and numerous garage doors are all supportive of its significance under Criterion A.

The November 8<sup>th</sup> Staff Report also found that the resource embodies the distinctive characteristics of the Art Deco style though the retention of the character-defining features of that style from its 1932 period of significance, including smooth wall surfaces, a flat roof, vertical projections above the roofline, fluted pilasters, geometric detailing and metal windows.

The above-referenced East Village Historical Context Statement was prepared in conjunction with the 2005 East Village Combined Historical Property Survey Report that discussed commercial and industrial development in East Village area of the City. That Report identified automobile-related properties as a significant theme in the development of East Village and the City as a whole.

By the third decade of the twentieth century, automobiles provided an excellent alternative to previous modes of transportation in the United States and became the primary mode of transportation, which led to the construction of independent repair garages, gas stations and auto dealerships—building types that didn't exist 20 years before. Significant buildings were constructed for auto retail sales and simpler buildings were constructed for the independent garages needed to maintain and store the automobiles.

Similar features were found in the buildings constructed for use as garages to house the vehicles serving the users of large public and private enterprises, such as the Southern California Telephone Company, which would have had a large work force and an extensive fleet of service vehicles. The features of such a building would include a single-story warehouse-like massing, limited ornamentation, high ceilings and numerous garage doors. In this instance, the simplified Art Deco style with its raised vertical projections was not only appropriate for the intended use but also for its 1932 date of construction.

More recent developments in the East Village and the City have limited the auto-related properties in this area and the City as a whole. This property type offers insight into the introduction of a new mode of transportation in the early 20<sup>th</sup> Century and of the important role automobile businesses in the economic development of San Diego as America's favorite method of transportation from 1900 to 1956. These remaining auto-related buildings are reflective of the City in the early 20<sup>th</sup> Century and worthy of designation under Criterion A. The subject building is significant as one of only two existing auto-related buildings in the East Village today. The building was constructed as a garage and continues to convey the character-defining features of



auto-related development including a single story, warehouse-like massing, limited ornamentation, raised vertical projection and numerous garage doors.

#### Impacts Discussion:

*Discuss the impacts of the proposed project on each evaluated resource.* The proposed project would retain the existing building façade of the designated structure in place, add one subterranean level of retail space, construct five levels of parking above the existing structure and sixteen levels of residential units above the parking. As discussed in the Site Development Permit Findings for this project, the incorporation of designated historical resources into new construction has been an established practice in San Diego since 1990 as eight designated historical resources have been so incorporated into new construction. In this instance, the designated resource would serve as the ground floor level of this multi-story mixed-use project.

#### *Summarize the City's CEQA Significance Determination Thresholds*

The current CEQA Significance Determination Thresholds for Historical Resources date from 2011. The subject property is a City of San Diego Designated Historical Resource #1355, designated in November of 2019. This commercial building is considered a “structure” and it was designated under Criteria A and C. The proposed project will have a “direct impact” on the resource in that it will alter the resource. The proposed project will retain the existing building façade of the designated structure in place, add one subterranean level of retail space, construct five levels of parking above the existing structure and sixteen levels of residential units above the parking. The proposal will retain the physical appearance and content of the local historical resource but will incorporate the resource into a much larger building.

The subject property is subject to the Downtown FEIR/SEIR and its Mitigation Monitoring and Reporting Program (MMRP), specifically Mitigation Measure HIST-A-1, a copy of which is attached as *Appendix E*. San Diego Register-Listed Resources “shall whenever possible be retained on-site. Partial retention, relocation, or demolition of a resource shall only be permitted in accordance with Chapter 14, Article 3, Division 2, Historical Resources Regulations of the LDC (Land Development Code).”<sup>23</sup>

The proposed Site Development Permit, pursuant to SDMC §126.0505 of the San Diego Municipal Code is the mechanism allowing the implementation of the substantial alteration of this designated local resource. The MMRP contains the requirements for development of a Treatment Plan for the retained portions of the resource, the HABS (Historic American Building Survey) of the current building, the Historical Monitoring Plan, the Photo Documentation Plan, and the approved Treatment Plan. Selected elements of this documentation shall be archived with the Historical Resources Board and the San Diego History Center.

**Conclusion:** The existing single-story Art Deco commercial building that is the subject of this Historical Resources Technical Report is a City of San Diego designated local resource but is not eligible for the California or National Historical Registers. The proposed project will incorporate

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<sup>23</sup> Subsequent to the publication of this MMRP, the pertinent Code Sections are cross-referenced as SDMC §143.0260(b) referring to SDMC §126.0404, later renumbered as SDMC §126.0505.

this resource into a much larger, mixed-use commercial and residential building. Compliance with the Mitigation, Monitoring and Reporting Program (MMRP) for Centre City San Diego and the applicable provisions the San Diego Municipal Code will provide the required mitigation measures.

## **6.0            BIBLIOGRAPHY**

California Code of Regulations, California Register of Historical Resources, Chapter 11.5, *Criteria for Listing in the California Register*, Section 4852 (b)

California Environmental Quality Act, Significance Determination Thresholds, Development Services Department, City of San Diego, January 2011.

California Lot Book, Chain of Title for 660 10<sup>th</sup> Avenue and 659 9<sup>th</sup> Ave and, San Diego California, 92101, January 6, 2017.

California Room, San Diego Public Library, San Diego Union Tribune indexes and articles, other research materials.

City of San Diego, Building Permit files at the Development Services Department.

City of San Diego, *Guidelines for the Application of Historical Resources Board Designation Criteria*, Appendix E, Part 2.

City of San Diego, *Historical Resource Technical Report Guidelines and Requirements*, Land Development Manual, Historical Resources Guidelines, Appendix E, Part 1.2.

City of San Diego, Historical Resources Board Files and Records.

City of San Diego, Planning Department, Centre City Development Corporation.

City of San Diego, Municipal Code

City of San Diego, Water and Sewer Department Records.

County Assessor's Office for Property Detail Report, Assessor's Building Record, Current parcel map, Original subdivision map and Lot Block Book Page.

Google.com.

California Code of Regulations, Chapter 11.5, *California Register of Historical Resources*, Sections 4850-4852.

Downtown FEIR/SEIR Mitigation, Monitoring and Reporting Program, Centre City Development Corporation.



National Park Service, *How to Apply the National Register Criteria for Evaluation*, National Register Bulletin, 1997.

Sanborn Fire Insurance Maps, 1920-1970,

San Diego City Directories, 1935-2016.

San Diego History Center, Research and Photographic Archives.

San Diego Union Tribune Index and articles on-line.

**A.0 - PROPERTY DETAIL REPORT**

**ATTACHED**



# Property Detail Report

659 9th Ave, San Diego, CA 92101

APN: 535-136-01-00

San Diego County Data as of: 12/21/2016

## Owner Information

Owner Name: 4 Lee Tenth LLC  
Vesting: Company  
Mailing Address: 660 10th Ave, San Diego, CA 92101

## Location Information

Legal Description: Lots A Thru C&L& Lot K Blk 83 Tr Db0013pg  
APN: 535-136-01-00 Alternate APN:  
Munic / Twship: Twship-Rng-Sec:  
Subdivision: Hortons Add Lockling Tract #: Db0013pg  
County: San Diego, CA  
Census Tract / Block: 005200 / 1047  
Legal Lot / Block: A, C, L / 83  
Legal Book / Page: 535 / 13

## Last Market Sale

Sale / Rec Date: 10/09/2009 / 10/19/2009 Sale Price / Type: \$493,500 /  
Multi / Split Sale: Price / Sq. Ft.: \$22  
1st Mtg Amt / Type: \$493,064 / Conventional 1st Mtg Rate / Type: / Fixed  
2nd Mtg Amt / Type: 2nd Mtg Rate / Type:  
Seller Name: Lee C & Y Family Trust Transfer Doc #: 2009.577971  
Lender: Lee Chung & Young Family Trust  
Title Company: Attorney Only

## Prior Sale Information

Sale / Rec Date: 10/09/2009 / 10/19/2009 Sale Price / Type: \$493,500 /  
1st Mtg Amt / Type: \$493,064 / Conventional 1st Mtg Rate / Type: / Fix  
Prior Lender: Lee Chung & Young Family Trust  
Prior Deed Type: Quitclaim  
Prior Doc #: 2009.577970

## Property Characteristics

Gross Living Area: 21,944 Sq. Ft. Total Rooms:  
Living Area: 21,944 Sq. Ft. Bedrooms:  
Total Adj. Area: Baths (F / H):  
Above Grade: Pool:  
Basement Area: Fireplace:  
Style: Cooling:  
Foundation: Heating:  
Quality: Exterior Wall:  
Condition: Construction Type:  
Year Built / Eff: 1926 / 1926  
Stories:  
Parking Type: Covered  
Garage #:  
Garage Area:  
Porch Type:  
Patio Type:  
Roof Type:  
Roof Material:

## Site Information

Land Use: Parking Lot Zoning: C  
State Use: Lot Area: 24,829 Sq. Ft.  
County Use: Parking/Garage Lot Width / Depth:  
Acres: 0.57 Usable Lot:  
Site Influence: # of Buildings: 1  
Water Type:  
Sewer Type:  
Res / Comm Units: 9 / 9

## Tax Information

Assessed Year: 2016 Assessed Value: \$1,262,833 Market Total Value:  
Tax Year: 2016 Land Value: \$922,299 Market Land Value:  
Tax Area: 08242 Improvement Value: \$340,534 Market Imprv Value:  
Property Tax: \$21,347.56 Improved %: 26.97% Market Imprv %:  
Exemption: Total Taxable Value: \$1,262,833 Delinquent Year:

**A.1 - COUNTY ASSESSOR'S BUILDING RECORDS**

**ATTACHED**



**COMMERCIAL-INDUSTRIAL BUILDING RECORD** Parcel No. 535-136-0.1

ASSESSOR, SAN DIEGO COUNTY

ADDRESS 659 NINTH AVE.

SHEET \_\_\_\_\_ OF \_\_\_\_\_

CLASS & SHAPE		FRAME		TRUSSES		EXT. FINISH		ROOF		LIGHTING		FRONT		INTERIOR CONSTRUCTION														
C		Wood	Concrete Reinf.	Light	Heavy	FLRS	Stucco	X	Flat	X	Standard	Type	NUMBER OF ROOMS					MATERIALS										
		Steel	'Span Spaced'	Wood	Steel				Shed		Below Standard		Desc.	B	M	1	2	3	FLOORS	GD	WALLS	GD	CEILING	GD				
Stories /		Steel					Metal		Arch					All			X	X		CONC	CEMENT	WOOD						
Bsmt	Mezz	No Frame		FLOORS			Veneer		Gable		FIXTURES		Glass in															
USE	DESIGN	FLRS	WALLS	X	Concrete		Wood		Wood	X	Fluorescent	Metal	Wood															
Garage			Wood		Wood		Glass		Metal		Incandescent	Glass Doors																
Store			Brick		Sub-Floor		Unfinished		Concrete			Auto No.																
Office		X	Conc. Blk		Elevation	X	CONC. BLOCK				Quality	Bulkhead	Office				/	/										
Factory			Metal						ROOF COVER		Quantity	Back Trim	Lobby															
Warehouse			Tilt Up		FOUNDATION		WINDOWS		Composition		PLUMBING	Lighting	Hall															
X PARKING			Pilasters	X	Concrete Reinf.		Metal	X	Built-Up		Fixtures	Drop Ceiling	Bath				/	/										
			Party		Masonry		Wood		Metal		Quality	Disp. Platform	Restroom															
												Quality	SPECIAL FEATURES															

CONSTRUCTION RECORD				EFFECT. YEAR	APPR. YEAR	NORMAL % GOOD				RATING (E,G,A,F,P)					ITEM	NO.-CAPACITY	MATERIAL OR TYPE	COST
Permit		Amount	Date			Age	Rem. Life	Table	%	Cond.	Arch. Attr.	Func. Plan	Ade- quacy	Wkm- ship				
No.	For																	
PH	PARKING		5-29-86	1926										Heating				
														Air Cond				
														Sprinklers				
														Doors	4	METAL Roll up		
														Sky-Lites				
														Elevator				

[illegible]



**Ft.**

6 SIT



## THE LINEN PARKING GARAGE

CONCRETE FLOOR

100

$$\leq 146$$

154

## MISCELLANEOUS STRUCTURES

[illegible]

## COMPUTATIONS

AREA OF G:  $200 \times 100 = 20,000$

$$\underline{54 \times 36 = 1944}$$

21944b

REMARKS:



**A.2 - NOTICE OF COMPLETION**

**ATTACHED**

139

going instrument, and acknowledged to me that they executed the same.

WITNESS my hand and official seal the day and year in this certificate first above written.



Mary Starr,

Notary Public in and for said County and State.

My commission expires Jan. 15, 1933

Recorded at request of Union Title Insurance Co MAR 17 1932 at 9 A.M.

Fee \$1.00

O.M. SWORN. COUNTY RECORDER.

BY DEPUTY J.L. Squire.

12219

COMPANY

*J.O. M. Sample*  
COUNTY RECORDER

3/17/1932 #12246

Official Records Book 101, Page 139

RECEIVED  
MAR 12, 1932

ASSISTANT SECRETARY.

NOTICE OF COMPLETION

O.C.P. SEC. 1187

NOTICE is hereby given that the undersigned corporation to-wit:

Southern California Telephone Company is the owner of these certain lots, pieces or parcels of land situated in the City of San Diego, County of San Diego, State of California, and described as follows, to-wit:

LOTS A, B, C, I AND K in Block Eighty-three (83) of Horton's Addition in the City of San Diego, County of San Diego, -- according to Map No. 359 on file in the office of the County Recorder of said County.

THAT the undersigned, as owner of said land, did, on the nineteenth (19th) day of December 1931, enter into a contract with W.E. Kier Construction Company, for the erection of a one-story reinforced concrete garage building upon the land above described, which contract was filed in the office of the county recorder of the said County of San Diego, State of California, on the twenty-ninth (29th) day of December 1931.

That on the Seventh (7th) day of March 1932, the said contract or work of improvement, as a whole was actually completed by the said W.E. Kier Construction Company, General Contractors.

That the fee title to said property is that of Southern California Telephone Company, a corporation, and the address of said Corporation is as follows: 740 South Olive Street, Los Angeles, California.

Dated March 15, 1932.

SOUTHERN CALIFORNIA TELEPHONE COMPANY, OWNER,

H.W. Hitchcock, Chief Engineer.

J.R. Knapp, Assistant Secretary.

STATE OF CALIFORNIA  
COUNTY OF LOS ANGELES

SS



H.W. Hitchcock and J.R. Knapp, Chief Engineer and Assistant Secretary, respectively, of the Southern California Telephone Company, being duly sworn, say: That Southern California Telephone Company is the owner of the property described in the foregoing notice; that they have read the foregoing notice, and know the contents thereof, and that the facts therein stated are true of their own knowledge.

PHOTOGRAPHED BY M. BRADLEY, DEPUTY RECORDER





H.W. Hitchcock, Chief Engineer.

J.N. Knapp, Assistant Secretary.

Approved as to Form  
Lawler & Dignan, Attorneys  
By J.M. Hardy.

Subscribed and sworn to before me this )  
(15) day of March 1932. )



Vivien Barton.

Notary Public in and for the County of  
Los Angeles, State of California.

My commission expires May 20, 1934.

Recorded at request of Owner MAR 17 1932 at 13 min past 9 A.M.

Fee \$1.00

O.M. SNOPE, COUNTY RECORDER.

BY DEPUTY D. GOLF.

*J.M. Sample*  
DEPUTY COUNTY RECORDER

## CHATTEL MORTGAGE

THIS CHATTEL MORTGAGE, made as of date March 1st, 1932, by ADOLPH M. GASSER AND CAMILLE GASSER, husband and wife, of the City of San Diego, California, by occupation Restaurant and Dining Room Owners, Mortgagees, to THE N. HALL COMPANY, a corporation, organized and existing under the laws of the State of California, Mortgagee, WITNESSETH:

That the mortgagees hereby mortgage to the mortgagee all of the furniture, furnishings, fixtures and equipment contained in the buildings and improvements located on LOTS "D", "E", "F", "G", "H" AND "I", in Block 308 of Horton's Addition, in the City of San Diego, California, said buildings and improvements being also sometimes designated No. 2720 Fourth Street, San Diego, California, there being hereunto attached and marked "Exhibit A" and made a part hereof a general list or description of the said mortgaged property, and reference being had thereto for more complete particulars: also all such personal property, furnishings, and trade fixtures as the mortgagees, or their successors in interest, shall hereafter place in or use in said property in the conduct of the business therein, either in addition to or replacement of the present furniture, furnishings, equipment or trade fixtures, and such additions and replacements shall, upon being placed in said buildings and improvements, ipso facto become included in this mortgage and subject to the lien thereof, and subject to the conditions, provisions and covenants hereof; and the mortgagees, or their successors in interest, will, upon request, execute a further mortgage of such after-acquired property in terms similar to these presents, as further assurance that the same shall be as effectually held as security under the terms hereof as the original mortgaged property.

This mortgage is given as security to the mortgagee for the payment, when due, of all obligations, including rentals, and / or other liabilities and sums to become due to the mortgagee, and also as security for the full and faithful performance by the mortgagees, or their successors in interest, of all the covenants and promises to be kept and performed, which obligations, sums, liabilities, covenants,

PHOTOGRAPHED BY M. BRADLEY, DEPUTY RECORDER

IN WITNESS WHEREOF, said Union Title Insurance and Trust Company, as Trustee, has caused its corporate name and seal to be hereunto affixed by its Assistant Secretary hereunto duly authorized, this 6th day of July, 1946.

UNION TITLE  
INSURANCE AND  
TRUST COMPANY  
(Incorporated  
Sept. 25, 1903  
San Diego, Calif.)

(Approved)  
JRM

UNION TITLE INSURANCE AND TRUST COMPANY, Trustee  
By LLOYD BALDRIDGE Assistant Secretary

STATE OF CALIFORNIA }  
County of San Diego } ss.

On this 9 day of July, 1946 before me, G. LEONA DU PAUL, a Notary Public, in and for said County and State, personally appeared LLOYD BALDRIDGE, known to me to be the Assistant Secretary of Union Title Insurance and Trust Company, Trustee, the corporation that executed the foregoing instrument, and known to me to be the person who executed the same on behalf of the Corporation therein named, and acknowledged to me that such Corporation executed the same as such Trustee.

WITNESS my hand and Official Seal.

G. LEONA DU PAUL

Notary Public in and for said County and State.

RECORDED AT REQUEST OF UNION TITLE INS. &  
TRUST CO. JUL 22 1946 9 A.M.

ROGER N. HOWE, COUNTY RECORDER  
By Deputy H. ZERVAS

PEARL M. ROSS

Deputy County Recorder

7/22/1946 #78449

Official Records Book 2187, Page 299

1.00-5 78235

NOTICE OF COMPLETION

NOTICE is hereby given that the undersigned, SOUTHERN CALIFORNIA TELEPHONE COMPANY, a corporation, with principal offices at 740 South Olive Street, Los Angeles, California, as OWNER of the property hereinafter described caused the construction of a one-story reinforced concrete addition to the garage building upon the property hereinafter described, contract for doing which was heretofore made with F. E. YOUNG, an individual, whose address is 2141 Main, P.O. Box 2872, San Diego 1, California, as CONTRACTOR which contract was dated October 26, 1945. That the work on said improvement was actually completed on July 16, 1946.

That the nature of the title of said property is as follows: SOUTHERN CALIFORNIA TELEPHONE COMPANY is the OWNER of all of said real property; that the real property hereinbefore referred to and on which said improvement work was done is described as that certain lot, piece or parcel of land situated in the City of San Diego, County of San Diego, State of California, and described as follows, to wit:

Lots "A", "B", "C", "K" and "L" in Block 85 of Horton's Addition, in the City of San Diego, County of San Diego, State of California, according to Map thereof on file in the office of the County Recorder of said San Diego County.

That the hereinabove described property is located at 660 Tenth Avenue, San Diego, California.

Dated at Los Angeles, California. July 19, 1946.

SOUTHERN CALIFORNIA TELEPHONE COMPANY

BY H. W. HITCHCOCK  
Chief Engineer

STATE OF CALIFORNIA }  
COUNTY OF LOS ANGELES } ss.

H. W. HITCHCOCK, being first duly sworn, deposes and says:

That he is the Chief Engineer of SOUTHERN CALIFORNIA TELEPHONE COMPANY, a corporation, and is authorized to execute this NOTICE and to make this verification for and on behalf of said corporation; that SOUTHERN CALIFORNIA TELEPHONE COMPANY is the OWNER of the property described in the foregoing NOTICE; that he has read the foregoing NOTICE, and knows the contents thereof and that the facts stated therein are true.

H. W. HITCHCOCK

Subscribed and sworn to before me  
this nineteenth (19) day of July, 1946

KARL A. LUNE

Notary Public in and for the County of Los Angeles,  
State of California,  
My commission expires August 4, 1946.

Form Approved

11-2-45

LAWLER, FELIX & HALL

Attorneys

By L. C. TUPPER

RECORDED AT REQUEST OF SO. CALIF. TEL. CO.

JUL 22 1946 21 MIN. PAST 9 A.M.  
ROGER N. HOWE, COUNTY RECORDER  
By Deputy H. ZERVAS

1.00-4 78449



**A.3 - WATER/SEWER RECORDS**

**ATTACHED**

# OPERATING DEPARTMENT

CITY OF SAN DIEGO

## Work and Service Order No. A 30576

GENERAL FOREMAN WATER Please install 2" Service and 2" Water

Job Order No. 659 9M Lot A E K L Block 13 Altan Add.

Location 463 Budget Allocation No. P 94 Amt. Paid 140.00 Date 3/9/32

Meter Size and Make 2" of 22 Line of Street on 11' Reading 30.00 Credit on 2.50 meter

Remorse Lane

The undersigned hereby applies to the City of San Diego for Water Service and Meter at the above location, and in consideration of the installation of such Service and Meter agrees to pay all charges incurred upon such location for such Water Service and to abide by all rules, regulations and provisions prescribed by this City by Ordinance or otherwise relating to water service, regulation or meter.

By *do. Calif. Tel Co*

By *Bux*

Date Description of Remarks or Services Labor Material Total

3/9/32 (30.00 M) 15.40 01.67 94.07

Extended Field 1435461 Roads 211

Water Meter 1100 1100 1100

44.20

11.00 on 10.00 10.00

11.00 on 10.00 10.00

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11.00 on 10.00 10.00

11.00 on 10.00 10.00

Foreman

PRINTED IN SAN DIEGO



## CITY OF SAN DIEGO

A 30567

Full-service

1997

Budget Allocation No. P.

200

Q of 440 line of

838

167

20

1784

The undersigned hereby agrees to use City of San Diego and Water Service and Meter at the above location, and in consideration of the installation of such service and Meter agrees to pay all charges incurred upon such location for such Water Service and to abide by all rules, regulations and provisions prescribed by said City by Ordinance or otherwise, relating to water service, regulation or rates.

Cardinal's College, N.Y.



Date	Description of Repairs or Services	Labor	Material	Total
	M-F. GT. closed off	106 hr.	109 ft.	216.35
	1-12 ft. x 12 ft. concrete			
	1-12 ft. x 12 ft. concrete			
	1-12 ft. x 12 ft. concrete			
	1-12 ft. x 12 ft. concrete			
	1-12 ft. x 12 ft. concrete			
	Remained open until 12:30 PM			
	Good - 256 ft.			

100-100-100

Form 30-1

PRINTED IN GREAT BRITAIN



OPERATING DEPARTMENT

SEWER CONNECTION ORDER

Work and Service Order No. 34557  
Location 659 9th  
Make 6" Street Connection  
Lot A13 Blk. 83 Add. 1st

Owner So Calif Tel Co  
Amount Paid \$ 65.00 Date 7/9/32  
Job Order No. 358 Budget Allocation 1-96

Man on 9th st (Permit)  
64  
Permit Clerk

Service Taps

Main 78 Ft. S from S Line of  
G Street

Service Enters

Property 74 Ft. S from S Line of  
G Street

Foreman





**A.4 - BUILDING PERMIT RECORDS**

**ATTACHED**



Building Permit Application				APPLICANT FILL INSIDE HEAVY LINES		JOB ADDRESS	
OWNER	NAME (OR NAME OF BUSINESS)			COORD. INDEX		PLAN FILE NO.	PERMIT NO. <b>E41261</b>
	PACIFIC TELEPHONE & TELEGRAPH CO.			USE		SETBACK	LOT AREA
	MAILING ADDRESS (NUMBER) (STREET)			ZONE		FRONT YARD	
	1475 SIXTH AVENUE			CENSUS TRACT		REAR YD.	ALLOW. COVERAGE
DESIGNER	CITY			B.C. CODE		SIDE YD. (INT)	TOTAL AREA COVERED
	SAN DIEGO, CALIF. 92101			022		SIDE YD. (STR)	SQ. FT.
	TELEPHONE NUMBER			VARIANCE NO.			LOT SPLIT DATE
	235-6342						
BUILDER	NAME			CURB TO R.I.		STREET IMPROV'D	ENCR. PERMIT
	CHAMPION CONSTRUCTION CO.			F		YES	NO
	ADDRESS (NUMBER) (STREET)			METER SIZE		SERVICE SIZE	CLEARANCE
	1969 NEWTON AVE.						CHECKED BY
JOB LOCATION	CITY			REMARKS		NO. ADDITIONAL CONNECTIONS REQ'D.	
	SAN DIEGO 92113					TYPE CONN.	
	TELEPHONE NUMBER			REMARKS		CHECKED BY	
	233-0108						
PROPOSED WORK	STATE LICENSE NUMBER			VALUATION OF WORK		NO. UNITS	PER UNIT
	219114			PLAN CHECK FEE			TOTAL
	CLASS NO.			SUPPLEMENTAL PLAN CHK. FEE			300
	8-1			BUILDING PERMIT FEE			3
ATTENTION	LOT			SEWER PERMIT FEE			2
	ABC-L 83			SEWER FEE			50
	BLOCK			WATER FEE			
	SUBDIVISION						
CITY OF SAN DIEGO	HORTON			TOTAL FEES DUE		OF 6	
	JOB ADDRESS			FIRE ZONE		TYPE OF CONST.	
	659 NINTH AVENUE			1		OCCUP. GRP.	
	CONDITION OF SOIL AT JOB SITE			BLDG. AREA		NO. STORIES	
INSPECTION DEPARTMENT	ORIGINAL			TOTAL FLOOR AREA		PLAN CHK. RECPT. NO. & AMT.	
	COMPACTED FILL			7342		DATE	
	LOOSE FILL			7348		DATE	
	NO. OF EXISTING BUILDINGS ON LOT AND USE			7343		DATE	
CITY OF SAN DIEGO	WORK TO BE DONE			7343		DATE	
	NEW ADD			7343		DATE	
	ALTER REPAIR			7343		DATE	
	MOVE DEMO SH			7343		DATE	
CITY OF SAN DIEGO	DESCRIBE			7343		DATE	
	REPLACE WOOD ENTRANCE DOORS			7343		DATE	
	W/ METAL DOOR & FRAME			7343		DATE	
	EXISTING USE OF BUILDING OR PROPERTY			7343		DATE	
CITY OF SAN DIEGO	GARAGE			7343		DATE	
	PROPOSED USE OF BUILDING OR PROPERTY			7343		DATE	
	SAME			7343		DATE	
	NON RESIDENTIAL			7343		DATE	
CITY OF SAN DIEGO	RESIDENTIAL			7343		DATE	
	NUMBER OF DWELLING UNITS			7343		DATE	
	1			7343		DATE	
				7343		DATE	
CITY OF SAN DIEGO	I hereby acknowledge that I have read this application, that the information given is correct; and that I am the owner, or the duly authorized agent of the owner. I agree to comply with city and state laws regulating construction, and in doing the work authorized thereby, no person will be employed in violation of the Labor Code of the State of California relating to Workmen's Compensation Insurance.			7343		DATE	
	SIGNATURE (OWNER OR AGENT)			7343		DATE	
	DATE SIGNED			7343		DATE	
	1			7343		DATE	
CITY OF SAN DIEGO	AGENCY FOR:			7343		DATE	
	Pacific Telephone & Telegraph Co.			7343		DATE	
	ADDRESS			7343		DATE	
	1475 SIXTH AVE, SAN DIEGO 92101			7343		DATE	
CITY OF SAN DIEGO	COUNTY SANITATION DISTRICT			7343		DATE	
	PRIVATE DISPOSAL APPROVAL			7343		DATE	
	RECEIPT NO.			7343		DATE	
				7343		DATE	
CITY OF SAN DIEGO	HEALTH DEPT. APPROVAL:			7343		DATE	
	SPRINKLERS REQ'D FOR:			7343		DATE	
				7343		DATE	
				7343		DATE	
CITY OF SAN DIEGO	ATTENTION			7343		DATE	
	THIS PERMIT AUTHORIZES ONLY THE WORK NOTED			7343		DATE	
	INSPECTION DEPARTMENT			7343		DATE	
				7343		DATE	
CITY OF SAN DIEGO	APPLICATION APPROVAL			7343		DATE	
	THIS PERMIT DOES NOT BECOME VALID UNTIL SIGNED BY THE DIRECTOR OF BUILDING INSPECTION, OR HIS DEPUTY; AND FEES ARE PAID, AND RECEIPT IS ACKNOWLEDGED IN SPACE PROVIDED.			7343		DATE	
	SIGNATURE OF DEPT. OF BLDG. INSPECTION			7343		DATE	
	DATE			7343		DATE	
CITY OF SAN DIEGO	INSPECTOR			7343		DATE	
				7343		DATE	
				7343		DATE	
				7343		DATE	



BUILDING PERMIT APPLICATION				PROJECT ADDRESS	
<b>659-9th AV.</b> LOT NO. <b>1</b> BLOCK <b>13</b> SUBDIVISION NAME <b>NORTON'S ADDITION</b> UNIT NO.				<b>659-9th AV.</b> CENSUS TRACT NUMBER <b>52.00</b> PERMIT NUMBER <b>102838</b>	
NAME <b>PACIFIC TELEPHONE &amp; TELEGRAPH</b> ADDRESS <b>525 B ST. Room 1339</b> TEL NO. <b>231-2562</b> CITY <b>SAN DIEGO</b> ZIP CODE <b>92101</b>				CONDITION OF SOIL AT JOB SITE <input checked="" type="checkbox"/> ORIGINAL <input type="checkbox"/> COMPACTED FILL <input type="checkbox"/> LOOSE FILL	
CONTRACTOR <b>CHAMPION CONST. Co.</b> TEL NO. <b>233-0108</b> <b>1969 NEWTON AVE.</b> ZIP CODE <b>92113</b> <b>SAN DIEGO</b>				PLAN FILE NO. <b>F06796</b> COORD. INDEX <b>199-1721</b>	
STATE LIC NO. <b>303404</b> LIC CLASS <b>B</b> CITY LIC NUMBER <b>00150101</b>				DESCRIBE WORK TO BE DONE <b>RECONSTRUCT ROOF TRUSS &amp; CEILING DUE TO FIRE DAMAGE</b>	
<b>JOHNSON, JOHNSON &amp; SPURRIE</b> LIC <b>SE1825</b> <b>4121 NAPIER</b> TEL NO. <b>275-1550</b> <b>SAN DIEGO</b> ZIP CODE <b>92116</b>				CITY TREASURER VALIDATION <b>67-71</b> <b>106</b>	
LENDER'S SIGNATURE _____ DATE _____ LENDER'S ADDRESS _____				SERV. SIZE _____ METER SIZE _____ CURRENCY _____ SEWER CONN. <input type="checkbox"/> SEE ATT. RECPT	
I, the undersigned, hereby certify that I am licensed under the provisions of Chapter 9 (commencing with Section 26000) of the Business and Professions Code, and my license is in full force and effect.				COASTAL ZONE <input type="checkbox"/> YES <input checked="" type="checkbox"/> NO HILLSIDE REVIEW <input type="checkbox"/> YES <input checked="" type="checkbox"/> NO AGREEMENT NO. _____ VARIANCE NO. _____	
I, as owner of the property, or my employee with written authority, will do the work, and no other person is to be employed or utilized for such work.				FUND & ACCT. NO. _____ NO. UNITS _____ PER UNIT _____	
I, as owner of the property, am exclusively contracting with licensed contractors to construct the project.				100 73421 PLAN CHECK FEE 20.50 100 73422 BUILDING PERMIT FEE 32.00 63010 9022 STATE FEE 50.00 41506 77565 SEWER FEE _____ 41500 77530 WATER FEE _____ 73423 PARK FEE _____	
I am exempt under Sec. _____ B & P C for this reason _____				TOTAL FEES DUE <b>\$31.50</b>	
I, as owner of the property, hereby certify that I have a certificate of completion, or a certificate of workers' compensation insurance, or a certified copy thereof filed with the building department.				SPECIAL INSPECTION REQUIRED FOR: <input type="checkbox"/> CONCRETE <input type="checkbox"/> MASONRY <input type="checkbox"/> WELDING <input type="checkbox"/> PLUMBING <input type="checkbox"/> MECHANICAL <input type="checkbox"/> ELECTRICAL	
I, as owner of the property, hereby certify that there is a construction lending agency for the performance of the work for which this permit is issued (Sec. 3007 C, C).				BUILDING AREA <b>N/A</b> FLOOR AREA <b>N/A</b> SPRINKLERS REQUIRED FOR _____ OCCUP. CLASS PREPARED BY PLAN CHG. YES <input type="checkbox"/> NO <input type="checkbox"/>	
I, as owner of the property, hereby certify that I am the owner of the property.				WORK TO BE DONE: <input type="checkbox"/> MOVE <input type="checkbox"/> DEMO <input checked="" type="checkbox"/> REPAIR <input type="checkbox"/> NEW <input type="checkbox"/> ADD	
I, as owner of the property, hereby certify that I am the owner of the property.				DATE PLANS SUBMITTED _____ TITLE <b>24</b>	
I, as owner of the property, hereby certify that I am the owner of the property.				PLAN CHECK RECEIPT NUMBER _____ PLAN CHECK RECPT AMT \$ _____	
I, as owner of the property, hereby certify that I am the owner of the property.				VERIFICATION APPROVAL BY <b>1/14/82</b> DATE	

FILE









City of San Diego  
Permit Services Division  
Development Services Department  
Permit Center • 1222 First Ave. • MS-301  
San Diego, CA 92101  
(619) 236-6270

# Permit Application

1. Permit Type: ☐ Combination/Building ☐ Mobile Home ☐ Electrical ☐ Plumbing & Gas ☐ Mechanical ☐ Demolition/Relocation/Remove Building ☒ Sign

2. Project Information: Address including Building or Suite No. 1660-4810<sup>th</sup> Ave. Plan File No. For City Use Only A103442-95

Legal Description Lot 19+20 of Block 50 of S.D. Map 497

Lot No. 19+20 Block No. 50 Subdivision Name of S.D. Map 497 Unit No. SD Map 497 Map No. SD Map 497

Parcel No. Assessor's Parcel No. HPA 1626-201-10

Existing Use Retail/Wholesale Furniture Displays Condition of Soil at Site: ☐ Undisturbed ☐ Compact Fill ☐ Loose Fill

Description of Work 2-Non-Illuminated Wall Signs Total Floor Area 32.5796

3. Designer Name Address City State Zip Code Telephone License Number

4. Applicant Name ☐ Contractor ☒ Agent for Contractor ☐ Owner ☐ Agent for Owner

Address 235 N. Engel St. Escondido, CA 92029 City State Zip Code Telephone 619 737-7414

5. Property Owner/Lessor Name Please check one ☐ Owner ☐ Lessor or Tenant

Address 770 12th St. San Diego, CA 92101 City State Zip Code Telephone 334-3267

6. Contractor Name Same as Applicant City State Zip Code Telephone

State License No. 482325 License Class C-10 City Business Tax No. 87-002849

Licensed Contractor's Declaration: I hereby affirm that I am licensed under provisions of Chapter 9 (commencing with Section 7000) of Division 3 of the Business and Professions Code, and my license is in full force and effect.

Signature Manly D. Day Title Manager Date 5/22/95

7. Workers' Compensation

☒ Workers' Compensation Declaration: I hereby affirm that I have a certificate of consent to self-insure, or a certificate of Workers' Compensation Insurance, or a certified copy thereof filed with the Development Services Department (Sec. 3000, Lab. C).

Insurance Company Golden Eagle Policy No. 230255160 Expiration Date 4/7/96

☐ Certificate of Exemption: I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any manner as to be come subject to the Workers' Compensation Laws of California.

Signature Manly D. Day Date 5/22/95

8. Contractor's Declaration: I hereby affirm that I am aware that the Contractor's License Law for the following reason (Sec. 7001.5, Business and Professions Code): Any city or county which requires a permit to construct, alter, improve, demolish, or repair any structure, prior to its issuance, also requires the applicant for such permit to file a signed statement that he is insured pursuant to the provisions of the Contractors' License Law (Chapter 9, commencing with Section 7000, of Division 3 of the Business and Professions Code) or that he is exempt therefrom, and the basis for the signed statement. Any violation of Section 7001.5 by any applicant for a permit subjects the applicant to a civil penalty of not more than five hundred dollars (\$500).

☐ I am owner of the property, or my employee with whom I am sole proprietor, will do the work and I am insured as required for such (Sec. 7001.5, Business and Professions Code). The Contractor's License Law does not apply to an owner of property who builds or improves himself or through his own company, provided that such improvements are not intended to be let, sold, or leased, or to be used in any manner as to become subject to the Contractors' License Law.

☐ I am owner of the property, am exclusively contracting with licensed contractors to construct the project (Sec. 7001.5, Business and Professions Code). The Contractor's License Law does not apply to an owner of property who builds or improves himself, and contracts for such projects with duly licensed contractors to the Contractor's License Law.

☐ I am contractor (Sec. 7001.5, Business and Professions Code).

Signature Manly D. Day Date 5/22/95

9. Construction Lending Agency: I hereby affirm that I am a contractor lending agency for the performance of the work for which this permit is issued (Sec. 3000, Lab. C).

Lender's Name Lender's Address

10. Applicant's Signature: I certify that I have read this application and state that the above information is correct, and that I am the owner or the duly authorized agent of the owner. I agree to comply with all city and state laws relating to building construction. I hereby authorize representatives of the City of San Diego to enter upon the above-described property for inspection purposes. If, after making the Certificate of Exemption from the Workers' Compensation provisions of the Labor Code I should become subject to such provisions, I will forthwith comply. In the event I do not comply with the Workers' Compensation Law, this permit shall be deemed voided.

Signature Manly D. Day Date 5/22/95

This information is available in alternative formats for persons with disabilities.  
To request this information in alternative format, call (619) 236-7700 or (619) 236-6270 (TDD)  
DAS-3002 (REV. 8-94)

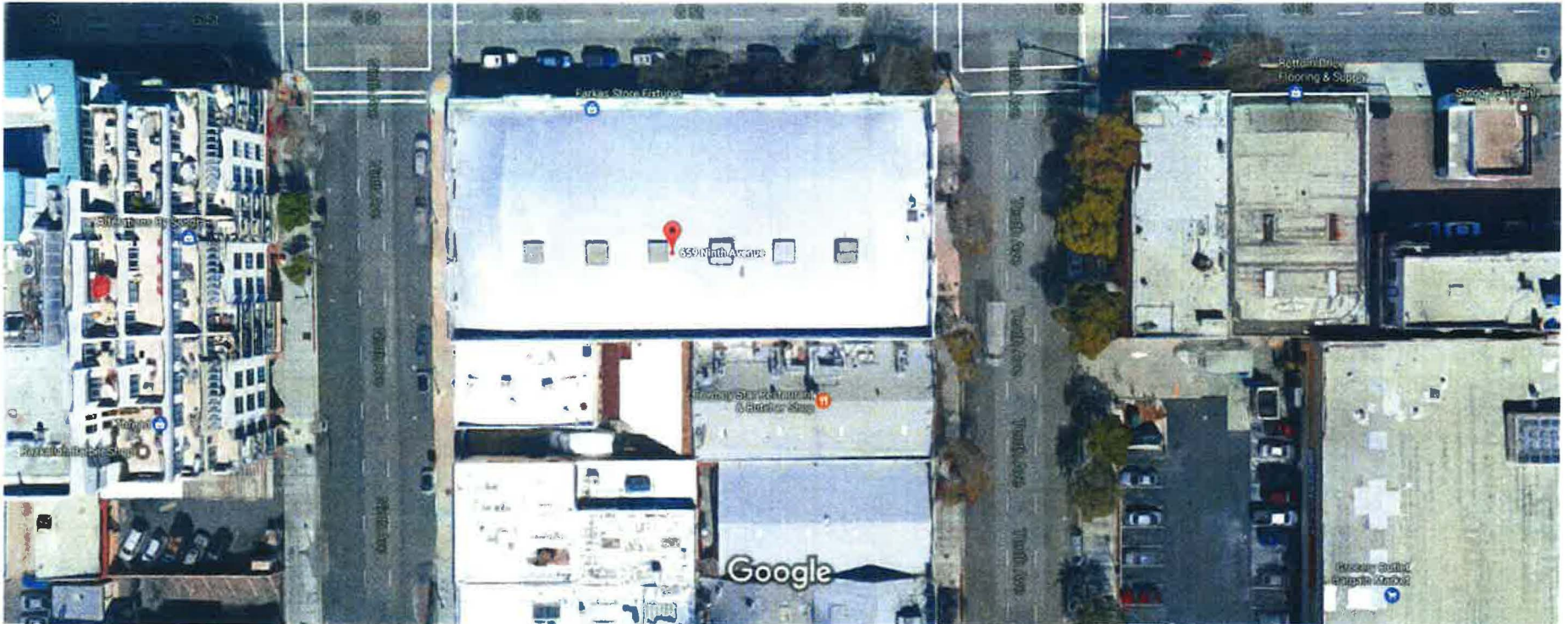
NE Corner



**A.5 - SITE PLAN**

**ATTACHED**

## Google Maps 659 Ninth Ave



Map data ©2017 Google 20 ft



G Street

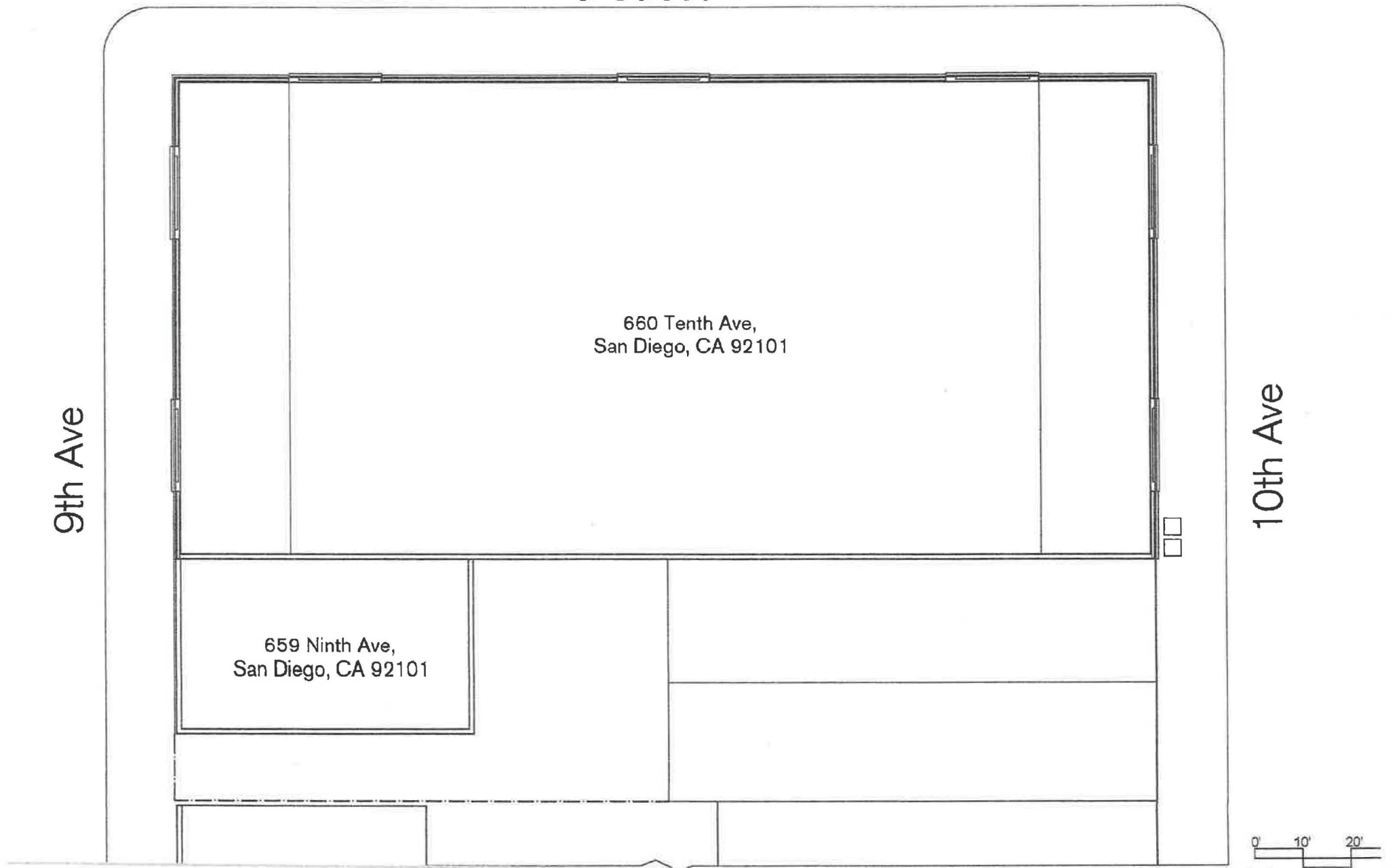
660 Tenth Ave,  
San Diego, CA 92101

9th Ave

659 Ninth Ave,  
San Diego, CA 92101

10th Ave

0' 10' 20'



**A.6 - LOT AND BLOCK BOOK PAGE**

**ATTACHED**



# INDEX TO PROPERTY

IN HORTON'S ADDITION

As per official map thereof, filed in the County Recorder's Office at San Diego, Cal.

ADDITION

SAN DIEGO COUNTY, CALIF.

SEE OFFICIAL MAP NO. 522

To Whom Assessed	To Whom Assessed	To Whom Assessed	To Whom Assessed	To Whom Assessed	Tax Sale	Description	Lot	Block	Range	Acres	Value of Real Estate				Value of Improvements					
1930	1931	1932	1933	1934							1930	1931	1932	1933	1934	1930	1931	1932	1933	1934
Scott E.A.				1513-6th St.		111. 1st 2nd & 3rd	A. 81				3660.	3480.	2500.	2000.	2000.					
"				1915		111. 1st 2nd & 3rd	B.				3120.	2960.	2200.	1760.	1760.					
Hampshire Mills				1915		111. 1st 2nd & 3rd	C.				2600.	2470.	1800.	1450.	1400.	770.	300.		240.	1180.
Young's Market Co.				711 - 5th St.		111. 1st 2nd & 3rd	D.				6600.	2600.	1900.	1500.	1700.	6170.	4650.		8910.	3910.
Rubin Isaac				713 1/2 Broadway		111. 1st 2nd & 3rd	E.				3250.		2200.	1760.	2000.					
"						111. 1st 2nd & 3rd	F.				6740.		4500.	3400.	4000.					
"						111. 1st 2nd & 3rd	G.				9550.		6700.	5400.	6000.					
"						111. 1st 2nd & 3rd	H.				5760.		4000.	3200.	4200.	12270.	9200.	7900.	7020.	7020.
Bondler Lane				1191 Inc.		111. 1st 2nd & 3rd	I.				5000.		3500.	2800.	1500.	3400.	2600.		2320.	2220.
Johnson Dicks				607 1/2 N.		111. 1st 2nd & 3rd	J.				5000.		3500.	2800.	3500.	950.	700.		2720.	
Remmon Ole L.				111. 1st 2nd & 3rd		111. 1st 2nd & 3rd	K.				4090.		2800.	2240.	2240.	3750.	2800.		2330.	2330.
Lucas Mary				3553-6th		111. 1st 2nd & 3rd	L. 81				8010.		5400.	4500.	5200.	2570.	1950.		1630.	1630.
Lippett & Stetson Co. Calif. Telephone						111. 1st 2nd & 3rd	A. 82.				3920.	3720.	2600.	2100.	2100.					
"						111. 1st 2nd & 3rd	B.				3340.	3170.	2200.	1760.	1760.	6560.	4700.	4300.	4160.	4160.
Payne Miss Mary						111. 1st 2nd & 3rd	C.				2800.		1900.	1500.	1500.	160.	100.		110.	110.
"						111. 1st 2nd & 3rd	D.				2800.		1900.	1500.	1500.	160.	100.		110.	110.
Payne Miss Mary						111. 1st 2nd & 3rd	E.				3180.		2200.	1760.	1760.	2180.	2150.		1790.	1790.
"						111. 1st 2nd & 3rd	F.				5700.		4000.	3200.	3200.					
Anderson & Hines Co. Bright Albert H.						111. 1st 2nd & 3rd	G.				5700.		4000.	3200.	3400.	3800.	2850.		2410.	2410.
Franklin Myrtle W.						111. 1st 2nd & 3rd	H.				3190.		2200.	1760.	1760.	1180.	900.		740.	740.
Ellis Rita						111. 1st 2nd & 3rd	I.				2600.		1900.	1500.	1500.	1080.	800.		670.	670.
Guatelli Mary						111. 1st 2nd & 3rd	J.				2600.	2470.	1800.	1450.	1450.	470.	350.		330.	330.
Little Pauline W.						111. 1st 2nd & 3rd	K.				3120.	2960.	2200.	1760.	1760.	7400.	5550.		4600.	4600.
"						111. 1st 2nd & 3rd	L. 82.				3660.	3480.	2500.	2000.	2000.	3200.	2400.		1890.	1890.
Payne Miss Mary						111. 1st 2nd & 3rd	A. 83				3900.	3700.	2600.	2100.	2100.					
"						111. 1st 2nd & 3rd	B.				4580.	4350.	3000.	2400.	2400.					
"						111. 1st 2nd & 3rd	C.				3700.	3500.	2000.	1730.	1730.	720.	410.	1890.	1890.	
Payne Miss Mary						111. 1st 2nd & 3rd	D.				3200.		2100.	1730.	1730.					
"						111. 1st 2nd & 3rd	E.				3670.		2600.	2000.	2400.					
"						111. 1st 2nd & 3rd	F.				5960.		4100.	3300.	3300.	440.	350.		4450.	4450.
Payne Miss Mary						111. 1st 2nd & 3rd	G.				4290.		3000.	2400.	2400.	620.	4500.		370.	370.
Payne Miss Mary						111. 1st 2nd & 3rd	H.				5700.		3600.	2900.	3450.	720.	5500.		4600.	4600.
Payne Miss Mary						111. 1st 2nd & 3rd	I.				3000.		2000.	1600.	1600.	660.	600.		670.	670.
Payne Miss Mary						111. 1st 2nd & 3rd	J.				3000.	2850.	1900.	1500.	1500.	1560.	1150.		890.	890.
Payne Miss Mary						111. 1st 2nd & 3rd	K.				3670.	3490.	2400.	1900.	1900.					
Payne Miss Mary						111. 1st 2nd & 3rd	L. 83				4480.	4260.	2800.	2200.	2200.					
Payne Miss Mary						111. 1st 2nd & 3rd	A. 84													
Payne Miss Mary						111. 1st 2nd & 3rd	B.													
Payne Miss Mary						111. 1st 2nd & 3rd	C.													
Payne Miss Mary						111. 1st 2nd & 3rd	D.													
Payne Miss Mary						111. 1st 2nd & 3rd	E.													
Payne Miss Mary						111. 1st 2nd & 3rd	F.													
Payne Miss Mary						111. 1st 2nd & 3rd	G.													
Payne Miss Mary						111. 1st 2nd & 3rd	H.													
Payne Miss Mary						111. 1st 2nd & 3rd	I.													
Payne Miss Mary						111. 1st 2nd & 3rd	J.													
Payne Miss Mary						111. 1st 2nd & 3rd	K.													
Payne Miss Mary						111. 1st 2nd & 3rd	L. 84													
Payne Miss Mary						111. 1st 2nd & 3rd	A. 85													
Payne Miss Mary						111. 1st 2nd & 3rd	B.													
Payne Miss Mary						111. 1st 2nd & 3rd	C.													
Payne Miss Mary						111. 1st 2nd & 3rd	D.													
Payne Miss Mary						111. 1st 2nd & 3rd	E.													
Payne Miss Mary						111. 1st 2nd & 3rd	F.													
Payne Miss Mary						111. 1st 2nd & 3rd	G.													
Payne Miss Mary						111. 1st 2nd & 3rd	H.													
Payne Miss Mary						111. 1st 2nd & 3rd	I.													
Payne Miss Mary						111. 1st 2nd & 3rd	J.													
Payne Miss Mary						111. 1st 2nd & 3rd	K.													
Payne Miss Mary						111. 1st 2nd & 3rd	L. 85													
Payne Miss Mary						111. 1st 2nd & 3rd	A. 86													
Payne Miss Mary						111. 1st 2nd & 3rd	B.													
Payne Miss Mary						111. 1st 2nd & 3rd	C.													
Payne Miss Mary						111. 1st 2nd & 3rd	D.													
Payne Miss Mary						111. 1st 2nd & 3rd	E.													
Payne Miss Mary						111. 1st 2nd & 3rd	F.													
Payne Miss Mary						111. 1st 2nd & 3rd	G.													
Payne Miss Mary						111. 1st 2nd & 3rd	H.													
Payne Miss Mary						111. 1st 2nd & 3rd	I.													



**A.7 - PREVIOUS SURVEY FORMS**

**ATTACHED**



TABLE 4. East Village Combined Historical Surveys 2005  
Staff Evaluation  
(Modified 2/10/05)

Form No.	Survey	Street No.	Street Name	APN	Lia Eval.	HRB Staff Eval.	HRB Criteria	Building Name	Architectural Style	Date	Architect or Builder	Comments
42	Over 45	705	6th Avenue	5351010300	6Z	5S2	C	MacMarr Grocery Store	One Part Commercial Block (Taxpayer)	c. 1929	Unknown	Taxpayer; attic added; conversion to restaurant meets Secretary of the Interior's Standards for Rehabilitation
43	East Village	701	7th Avenue	5351020600	5S2	3S	C, D	San Diego Hoffman Hospital and Clinic	Spanish Eclectic with Modern influence	1928	Louis Gill (Architect) Thomas M. Russell (Builder)	
44	East Village	615	8th Avenue	5351040300	7	5S2	C	Hotel Schneider	Two Part Commercial Block with Italian influence	1913	George E. Cornell (Builder)	First floor modifications
45	East Village	703-723	8th Avenue	5351030400	6Z	6Z	Note & File	The Revere Rooming House	Two Part Commercial Block	1886/ 1907 - 1909	Unknown	Exterior modified in 1930s and 1980s
46	East Village	660	10th Avenue	5351360100	5S2	5S2	A, C	Southern California Telephone Company Garage	Art Deco	1932	W.E. Kier Construction Company (Builder)	Auto-related resource
47	East Village	734	10th Avenue	5351310500	5S2	5S2	A, B, C	Ephraim & Mary Walker Morse House	Italianate	1887	Unknown	East Village residential cultural landscape; associated with Ephraim Morse
48	East Village	743	10th Avenue	5351320400	6Z	5S2	A, C	H.T. Christian House	Victorian vernacular	1872	Unknown	East Village residential cultural landscape
49	East Village	650	11th Avenue	5351350900	6Z	6Z	Note & File	Yale Apartments	Vernacular Townhouse with Italian Renaissance influence	1912	M.D. Goodbody	Undistinguished architecture
50	East Village	727-733 1/2	11th Avenue	5351331500	6Z/6Z	6Z/6Z	Note & File	Thomas J. Daley Apartment & James A. Bailey Apartment Buildings	Folk Victorian (rear structures) & Italianate (front structure)	1886/ 1907	J.D. Palmer	Two-story porch in 1907 building removed; window & door openings modified; 1886 buildings modified both front & rear.
51	East Village	741	11th Avenue	5351330300	6Z	6Z	Note & File	Woodford Residence	Italianate	1887	Unknown	Exterior stuccoed; windows replaced
52	East Village	760-770/ 1025-1055	11th Avenue/ F Street	5351320700	7	5S2	C	Schiller Book Bindery	One Part Commercial Block with Neo-Classical influence (Taxpayer)	1932	Scott Quintin (Architect) Walter Trepte (Builder)	Taxpayer; Loft conversion meets Secretary's Standards for Rehabilitation
53	East Village	941	11th Avenue	5343330200	5S2	5S2	A, C	Hamilton Apartment Building	Italianate	1886/ 1907	John Campbell (Builder)	East Village residential cultural landscape
54	East Village	509	12th Avenue	5351510500	3S	3S	C	The New Bay View Hotel	Italianate	1889	Unknown	Corner towers and first floor balcony railing removed
55	Over 45	999	12th Avenue	5343411000	5S2	5S2	C	Ephraim Harris Grocery Store	Art Moderne (Streamline Modernistic)	c. 1925	Unknown	
56	Over 45	1015-1025	12th Avenue	5342060300	5S2	5S2	A, C	Ford Apartments/ Ford Motor Building	Art Moderne (Streamline Modernistic)	1937	Unknown	Auto-related resource; windows boarded
57	East Village	1154	12th Avenue	5341930900	5S2	6Z	Note & File	Otto and Blanche Fox Residence	Colonial Revival	1895	Unknown	Porch & window modifications
58	East Village	1166	12th Avenue	5341931000	7	5S2	A, C	Biltmore Apartments	French Eclectic	1925	Unknown	East Village residential cultural landscape
59	Bayside	341-343	13th Street	5353720400	6Z	5S2	A	Mexican Presbyterian Church/Mexican & Spanish Presbyterian Church	Vernacular Folk Victorian	1906 - 1907	Unknown	Significant for association with San Diego Mexican community
60	Bayside	353-357	13th Street	5353720300	6Z	5S2	A, C	Simon Padilla Residence	Victorian	c. 1870s	Unknown	East Village residential cultural landscape



PRIMARY RECORD

Primary # \_\_\_\_\_  
HRI # \_\_\_\_\_  
Trinomial \_\_\_\_\_  
NRHP Status Code 592

Other Listings Update of November 1988 Centre City East Survey Documentation

Review Code \_\_\_\_\_ Reviewer \_\_\_\_\_ Date \_\_\_\_\_

Page 1 of 2 \*Resource Name or #: (Assigned by recorder) 660 10<sup>th</sup> Avenue / 659 9<sup>th</sup> Avenue

P1. Other Identifier: \_\_\_\_\_

\*P2. Location: ☐ Not for Publication ☒ Unrestricted \* a. County San Diego

and (P2b and P2c or P2d. Attach a Location Map as necessary.)

\*b. USGS 7.5' Quad Point Loma Date \_\_\_\_\_ T \_\_\_\_\_; R \_\_\_\_\_;  $\frac{1}{4}$  of \_\_\_\_\_  $\frac{1}{4}$  of Sec \_\_\_\_\_; \_\_\_\_\_ B.M.

c. Address 660 10<sup>th</sup> Avenue / 659 9<sup>th</sup> Avenue City San Diego Zip 92101

d. UTM: (Give more than one for large or linear resources) Zone \_\_\_\_\_; \_\_\_\_\_ mE/ \_\_\_\_\_ mN

e. Other Locational Data (e.g., parcel #, directions to resource, elevation, etc., as appropriate):

APN: 535-136-01-00, Horton's Addition, All of Lots A, B, C and All of Lots K & L, Block 50

\*P3a. Description (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries):

Originally constructed in 1932, this single-story, One-Part Commercial Block building was designed in an Art Deco architectural style. It has a general rectangular shaped plan and features a flat roof with a series of raised parapets, protruding vertical features above the four garage doors flanked on both sides by a pilaster, a simple cornice spanning around the perimeter of the 10<sup>th</sup> Avenue portion of the building, and Chicago and industrial style windows around the facades. A smaller one-story addition was later constructed and is identified as 659 9<sup>th</sup> Avenue. This portion also features a simple cornice, with similar windows. Overall, this building appears to be in good condition, and is a good example of the Art Deco style for a commercial use.

P3b. Resource Attributes: (List attributes and codes) HP6

\*P4. Resources Present: ☒ Building ☐ Structure ☐ Object ☐ Site ☐ District ☐ Element of District ☐ Other (Isolates, etc.)

P5b. Description of Photo: (View, date, accession #) \_\_\_\_\_

\*P6. Date Constructed/Age and Sources:

☒ Historic ☐ Prehistoric ☐ Both

1932

1988 Centre City East Historic Site Inventory

\*P7. Owner and Address:

Chung H and Young H Lee

660 10<sup>th</sup> Avenue

San Diego, CA 92101

\*P8. Recorded by (Name, affiliation, and address): Wendy L. Tinsley,

Office of Marie Lia, Attorney

at Law, 427 C Street, Ste. 416,

San Diego, CA 92101

\*P9. Date Recorded: April 2001 / Owner Information Updated June 2003

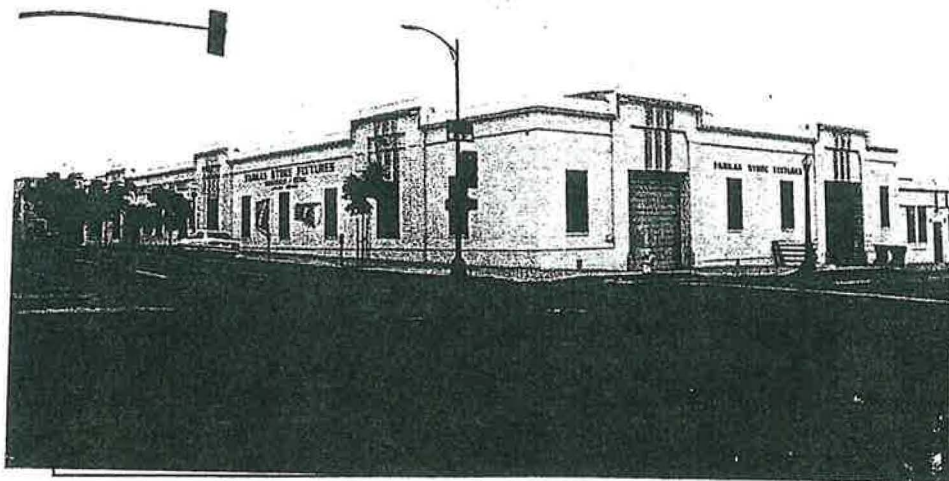
\*P10. Type of Survey: (Describe) \_\_\_\_\_

P11. Report Citation (Cite survey report and other sources, or enter "none"). Historic Site Inventory of Centre City East For Centre City Development Corporation

Attachments: ☐ NONE ☐ Location Map ☐ Sketch Map ☐ Continuation Sheet ☒ Building, Structure, and Object Record

☐ Archaeological Record ☐ District Record ☐ Linear Resource Record ☐ Milling Station Record ☐ Rock Art Record

☐ Artifact Record ☐ Photograph Record ☐ Other (List): \_\_\_\_\_





## BUILDING, STRUCTURE, AND OBJECT RECORD

Page 2 of 2

\*NRHP Status Code 5S2

\*Resource Name or # (Assigned by recorder) 660 10<sup>th</sup> Avenue, San Diego, CA 92101

B1. Historic Name: Southern California Telephone Company Garage

B2. Common Name: Farkas Store Fixture Building

B3. Original Use: Garage Space

B4. Present Use: Commercial

\*B5. Architectural Style: Art Deco

\*B6. Construction History: (Construction date, alternations, and date of alterations)

Constructed in 1932; the portion of the building identified as 659 9<sup>th</sup> Avenue appears to be a later addition.

\*B7. Moved? ☒ No ☐ Yes ☐ Unknown Date: \_\_\_\_\_ Original Location: \_\_\_\_\_

\*B8. Related Features:

B9a. Architect: Unknown b. Builder: W.E. Kier Construction Company

\*B10. Significance: Theme Architecture Area San Diego Period of Significance 1932

Property Type Garage Applicable Criteria C

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

Originally constructed in 1932, this building was previously documented in November 1988 under the supervision of the Office of Marie Lia and Dr. Ray Brandes. Previous documentation and San Diego City Directories reveal past occupants of the 660 10<sup>th</sup> Avenue garage include: Southern California Telephone Company (1932-c.1944/1945), and Pacific Telephone and Telegraph Company (1950, 1955, 1960, 1965, 1969/1970, 1975, 1980, and 1984). Presently, the building is occupied by the Farkas Store Fixtures Company.

Historical research has determined that the 660 10<sup>th</sup> Avenue building is not historically significant, as the building is not associated with any important events or individuals in terms of local, state, or national history. However, the 660 10<sup>th</sup> Avenue building does appear to be architecturally significant, as a building which embodies the distinctive characteristics of a type, period, or method of Art Deco construction. Constructed by the W.E. Kier Construction Company, the building does not represent the work of a master builder, or master craftsman, nor that of important, creative individual.

B11. Additional Resource Attributes: (List attributes and codes) \_\_\_\_\_

\*B12. References: (partial list)

Lia and Brandes, *Historic Site Inventory of Centre City East*, 1988.

McAlester, Virginia & Lee, *A Field Guide To American Houses*, 1986.

San Diego City Directories

San Diego County Records Office

B13. Remarks:

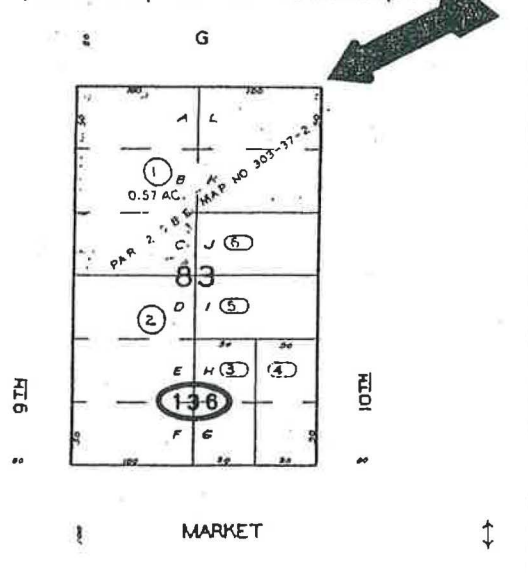
\*B14. Evaluator: Wendy L. Tinsley

Office of Marie Burke Lia

Date of Evaluation: April 2001

(This space reserved for official comments.)

(Sketch Map with north arrow required)



**B.1 - CHAIN OF TITLE**

**ATTACHED**



**California Lot Book, Inc.**  
***dba California Title Search Co.***  
P.O. Box 9004  
Rancho Santa Fe, CA 92067  
(858) 278-8797 Fax (858) 278-8393  
WWW.LOTBOOK.COM

## **Chain of Title Report**

Marie Burke Lia  
427 C St., Ste. 416  
San Diego, CA 92101

CTS Reference No.: 0117350  
Your Reference No.: 1274

**Title Search Through:** January 6, 2017

**Property Address:** 659 9<sup>th</sup> Avenue  
660 10<sup>th</sup> Avenue  
San Diego, CA 92101

**Assessor's Parcel No.:** 535-136-01-00

**Assessed Value:** \$1,262,833

**Exemption:** None

### **Property Characteristics**

**Improvements:** 21,944 square feet

### **Short Legal Description**

LOTS A, B, C, K AND L IN BLOCK 83 OF HORTON'S ADDITION, IN THE CITY OF SAN DIEGO, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO MAP THEREOF ON FILE IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY.

**California Lot Book, Inc., dba California Title Search Co.  
CTS Reference No.: 0117350**

**Chain of Title  
( June 29, 1929 through January 6, 2017 )**

**The following documents relate to Lots A, B and C:**

**1. Grant Deed**

Grantor: Norien A. Morris and A. H. Morris  
Grantee: C. A. Minard  
Recorded: June 29, 1929, #37280, Deed Book 1642, Page 383

**2. Grant Deed**

Grantor: C. A. Minard and Dessie S. Minard  
Grantee: The Pacific Telephone and Telegraph Company  
Recorded: June 29, 1929, #37281, Deed Book 1642, Page 384

**The following documents relate to Lots K and L:**

**3. Grant Deed**

Grantor: William F. Jungk and Henrietta Jungk  
Grantee: C. A. Minard  
Recorded: June 29, 1929, #37283, Deed Book 1653, Page 139

**4. Grant Deed**

Grantor: C. A. Minard and Dessie S. Minard  
Grantee: The Pacific Telephone and Telegraph Company  
Recorded: June 29, 1929, #37284, Deed Book 1653, Page 140

**The Following Documents relate to Lots A, B, C, K and L:**

**5. The San Diego County Assessor Lot Block Book Page shows the first year with assessed improvements as being 1932.**

**6. Notice of Completion**

Recorded: March 17, 1932, #12246, Official Records Book 101, Page 139

**Please be advised that this is not Title Insurance. The information provided herein reflects matters of public record which impart constructive notice in accordance with California Insurance Code 12340.10**



7. Notice of Completion

Recorded: July 22, 1946, #78449, Official Records Book 2187,  
Page 299

8. Corporation Grant Deed

Grantor: Pacific Bell, formerly known as The Pacific Telephone and  
Telegraph Company  
Grantee: Elizabeth Ann Nothhorn  
Recorded: March 19, 1985, Records File No. 85-090680

9. Trustee's Deed

Grantor: Fidelity Trust Deed Service Corporation  
Grantee: Gerry Hampshire, 70.9110 interest and Nancy Owen, 29.0890  
interest  
Recorded: October 5, 1994, Records File No. 1994-0589462

10. Grant Deed

Grantor: Gerry Hampshire and Nancy Owen  
Grantee: Chung H. Lee and Young H. Lee  
Recorded: March 31, 1995, Records File No. 1995-0135308

11. Grant Deed

Grantor: Chung H. Lee and Young H. Lee  
Grantee: Chung H. Lee and Young H. Lee  
Recorded: November 27, 2006, Records File No. 2006-0837121

12. Grant Deed

Grantor: Chung H. Lee and Young H. Lee  
Grantee: Chung Lee and Young Lee, Trustees of The Chung Lee and  
Young Lee Family Trust dated August 8, 2005  
Recorded: November 27, 2006, Records File No. 2006-0837122

13. Quitclaim Deed

Grantor: Chung Lee and Young Lee, Trustees of the Chung Lee and  
Young Lee Family Trust dated August 8, 2005  
Grantee: Elizabeth Lee, Trustee of the Elizabeth Lee Gift Trust dated  
October 9, 2009, 12.5% interest and Elizabeth Lee, Trustee of  
the Christie Lee Gift Trust dated October 9, 2009, 12.5% interest  
Recorded: October 15, 2009, Records File No. 2009-0573102

**Please be advised that this is not Title Insurance. The information provided herein  
reflects matters of public record which impart constructive notice in accordance with  
California Insurance Code 12340.10**

14. Quitclaim Deed

Grantor: Chung Lee and Young Lee, Trustees of the Chung Lee and Young Lee Family Trust dated August 8, 2005  
Grantee: Elizabeth Lee, Trustee of the Christie Lee Gift Trust dated October 9, 2009, 17 1/3% interest of Grantor's undivided 75% interest, which is an undivided 13% interest  
Recorded: October 19, 2009, Records File No. 2009-0577970

15. Quitclaim Deed

Grantor: Chung Lee and Young Lee, Trustees of the Chung Lee and Young Lee Family Trust dated August 8, 2005  
Grantee: Elizabeth Lee, Trustee of the Elizabeth Lee Gift Trust dated October 9, 2009, 17 1/3% interest of Grantor's undivided 75% interest, which is an undivided 13% interest  
Recorded: October 19, 2009, Records File No. 2009-0577971

16. Quitclaim Deed

Grantor: Chung Lee and Young Lee, Trustees of the Chung Lee and Young Lee Family Trust dated August 8, 2005, Elizabeth Lee, Trustee of the Elizabeth Lee Gift Trust October 9, 2009 and Elizabeth Lee, Trustees of the Christie Lee Gift Trust dated October 9, 2009  
Grantee: 4 Lee Tenth, LLC  
Recorded: October 26, 2009, Records File No. 2009-0592023

– End of Report –

\*\*\*\*\*

Please be advised that this is not Title Insurance. The information provided herein reflects matters of public record which impart constructive notice in accordance with California Insurance Code 12340.10. Note that we are not a Title Insurance Company, and that no express or implied warranty as to the accuracy or completeness of the information provided herein is granted. Our work has been performed under short time constraints with a quick turn around, and is based in part on the use of databases outside of our control. The recipient hereby acknowledges that California Lot Book, Inc. assumes no liability with respect to any errors or omissions related to the information provided herein. Also note that this search has been performed without the benefit of a Statement of Identification from the property owners, and if a search was performed for liens recorded against owner names, we cannot be sure that the information provided relates to the actual property owners, or is complete with respect to the property owners. In any event, our liability is limited to the amount of fees collected for the information provided herein.

\*\*\*\*\*



**B.2 - CITY DIRECTORY AND NEWSPAPER RESEARCH**

**ATTACHED**

## **SAN DIEGO CITY DIRECTORY**

**659 9<sup>th</sup> Ave  
San Diego, CA**

<b>Year</b>	<b>Occupant</b>
<b>1971-1984</b>	<b>Pacific Telephone Telegraph Co. (Truck Repair)</b>
<b>1988</b>	<b>Automotive Leasing &amp; Sales</b>
<b>1990</b>	<b>The Livery Limousine</b>
<b>1994</b>	<b>XXXX</b>
<b>1997-1998</b>	<b>VIP Spas</b>
	<b>Webb, David</b>
<b>2002</b>	<b>Lee, Chung</b>
<b>2006</b>	<b>No address</b>
<b>2010</b>	<b>XXXX</b>
<b>2014</b>	<b>XXXX</b>
<b>2016</b>	<b>XXXX</b>



## **SAN DIEGO CITY DIRECTORY**

**660 10<sup>th</sup> Ave  
San Diego CA**

<b>Year</b>	<b>Occupant</b>
<b>1937-45</b>	<b>Southern California Telephone Co. (Garage)</b>
<b>1946</b>	<b>No Book</b>
<b>1947-84</b>	<b>Pacific Telephone and Telegraph Co. (Garage)</b>
<b>1988</b>	<b>No Listing</b>
<b>1990</b>	<b>Airport Parking &amp; Livery Service</b>
	<b>Livery Parking &amp; Limo</b>
	<b>Livery Rent-a-Car</b>
	<b>Park &amp; Shuttle</b>
	<b>The Livery</b>
<b>1994</b>	<b>Airport Parking Service</b>
	<b>The Livery</b>
	<b>Park &amp; Shuttle</b>
<b>1997-98</b>	<b>Farkas Store</b>
<b>2002-16</b>	<b>Farkas Store Fixtures</b>
<b>2014-16</b>	<b>We Buy Used, Inc.</b>

**B.3 - DEED FROM DATE OF CONSTRUCTION**

**ATTACHED**



of San Diego, County of San Diego, according to map thereof on file in the  
Office of the County Recorder of San Diego County.

TO HAVE AND TO HOLD to said Grantee, his heirs or assigns.

WITNESS our hands this 26th day of June, 1929.

Norlen A. Morris

A. H. Morris

STATE OF CALIFORNIA }  
COUNTY OF SAN DIEGO }

On this 26th day of June, 1929, before me, Ruby Barger, a Notary Public in  
and for said County, personally appeared Norlen A. Morris, and A. H. Morris, known  
to me to be the persons whose names are subscribed to the foregoing instrument, and  
acknowledged to me that they executed the same.

WITNESS my hand and official seal.

Ruby Barger

Notary Public in and for the County of San Diego,  
State of California.



Recorded at request of Union Title Insurance Co. Jun 29, 1929. at 9 o'clock A. M.

John H. Ferry, County Recorder

37280 Fee \$ .40

By L. B. Woodard, Deputy

6/29/1929 #37281  
Deed Book 1642, Page 384

#### GRANT DEED

C. A. MINARD and DESSIE S. MINARD, as husband and wife,

in consideration of Ten (\$10.00) Dollars to them in hand paid, receipt of which is  
hereby acknowledged, do hereby

GRANT TO THE PACIFIC TELEPHONE AND TELEGRAPH COMPANY, a corporation, the real property  
in the City of San Diego, County of San Diego, State of California, described as:

Lots "A", "B" and "C" in Block 83, of Horton's Addition, in the City  
of San Diego, County of San Diego, according to map thereof on file in  
the office of the County Recorder of San Diego County.

TO HAVE AND TO HOLD to said Grantee, its successors or assigns.

WITNESS our hands this 27th day of June, 1929.

C. A. Minard

Dessie S. Minard

STATE OF CALIFORNIA }  
COUNTY OF LOS ANGELES }

On this 27th day of June, 1929, before me, J. R. Knapp, a Notary Public in  
and for said County, personally appeared C. A. Minard and Dessie S. Minard, known to  
me to be the persons whose names are subscribed to the foregoing instrument and acknowl-  
edged to me that they executed the same.

WITNESS my hand and official seal.

J. R. Knapp

Notary Public in and for the County of  
Los Angeles, State of California.

My Commission  
expires January 29,  
1930.



Recorded at request of Union Trust Insurance Co. Jan 20, 1929, at 9 o'clock A. M.

John H. Perry, County Recorder

37281 Fee \$3.00

By L. B. Woodard, Deputy

-----00000000-----

THIS DEED OF TRUST, made the 19th day of June, A. D. 1929, between EMMA E. DENT, a widow, as Trustor, and Corporation of America, a corporation duly organized and existing under and by virtue of the laws of the State of California, as Trustee and BANK OF AMERICA OF CALIFORNIA, a corporation organized and existing under and by virtue of the laws of the State of California, as Beneficiary; (It is distinctly understood that the word "Trustor" and the word "his" referring to the Trustor, as herein used, are intended to and do include the masculine, feminine and neuter genders, and the singular and plural numbers.)

WITNESSETH: That said Trustor hereby grants, conveys and confirms unto said Trustee with power of sale the following described real property situated in the City of San Diego, County of San Diego, State of California, to-wit:

All of Lots Fifteen (15) and Sixteen (16) in Block Forty (40) of Normal Heights, in the City of San Diego, County of San Diego, State of California, according to map thereof No. 985, filed in the office of the Recorder of said San Diego County, May 9, 1906; EXCEPTING from said Lots, the West 195 feet thereof.

TOGETHER with the appurtenances thereto and the rents, issues and profits thereof and warranting the title to said premises.

TO HAVE AND TO HOLD the same unto said Trustee and its successors, upon the trusts hereinafter expressed, namely:

FIRST: As security for the payment of FOURTEEN HUNDRED AND NO/100 Dollars in United States gold coin of the present standard of weight, fineness and value, with interest thereon in like gold coin according to the terms of the promissory note or notes for said sum executed and delivered by the Trustor to the Beneficiary and stated to be secured by this Deed of Trust.

SECOND: As security for the payment of such additional sum or sums as may be hereafter loaned by said Beneficiary to, evidenced by the note or notes of, said Trustor, with interest thereon as in said promissory notes provided.

THIRD: As security for the payment of all other moneys that may become due said Trustor and Beneficiary, or either of them, pursuant to this instrument.

FOURTH: During the continuance of these trusts, the Trustor agrees to pay, satisfy, and discharge at maturity all taxes, assessments, and all other charges and encumbrances which now are, or shall hereafter be, or appear to be, a lien upon above premises, or any part thereof, and in default thereof the Beneficiary or Trustee may without demand or notice, pay, satisfy or discharge the said taxes, assessments, charges or encumbrances, and pay and expend any sums of money that it may deem necessary therefor, and may remove, litigate, or compromise all adverse claims affecting said premises, and shall be the sole judge of the legality or validity of said taxes, assessments, charges, encumbrances, or adverse claims, and the amount necessary, to be paid in the satisfaction or discharge thereof; the Trustor also agrees at all times to keep the buildings and improvements which now are or shall hereafter be erected upon the above premises, insured against loss or damage by fire and such other casualties as may be designated by said Beneficiary in an amount required by said Beneficiary by some insurance company or companies to be approved by said Beneficiary, the policies of which insurance shall be made



WITNESS my hand and official seal.



Ruby Bargar.

Notary Public in and for the County of San Diego,  
State of California.

Recorded at request of Union Title Insurance Co JUN 29 1929 at 9 o'clock A.M.  
Fee \$ .90

John H. Ferry, County Recorder. 3/255  
By L.B. Woodard, Deputy

*Handwritten:* 6/29/1929

6/29/1929 #37284  
Deed Book 1653, Page 140

GRANT DEED

G.A. MINARD AND DESSIE S. MINARD husband and wife, in consideration of  
Ten (\$10.00) Dollars to them in hand paid, receipt of which is hereby acknowledged,  
do hereby GRANT TO THE PACIFIC TELEPHONE AND TELEGRAPH COMPANY, a corporation, the  
real property in the City of San Diego, County of San Diego, State of California, des-  
cribed as Lots "E" and "L" in Block 83 of Horton's Addition, in the City of San Diego,  
County of San Diego, State of California, according to Map thereof on file in the  
office of the County Recorder of said San Diego County.

TO HAVE AND TO HOLD to said grantee, its successors or assigns.  
WITNESS our hands this 27th day of June 1929.

G.A. Minard.  
Dessie S. Minard.

STATE OF CALIFORNIA }  
COUNTY OF LOS ANGELES }

On this 27th day of June 1929, before me, J.R. KNAPP a Notary Public in  
and for said County, personally appeared G.A. Minard and Dessie S. Minard known to me  
to be the persons whose names are subscribed to the foregoing instrument and acknowledged  
to me that they executed the same.

WITNESS my hand and official seal.



J.R. Knapp.  
Notary Public in and for the County of Los Angeles,  
State of California.  
My commission expires January 29, 1930

Recorded at request of Union Title Insurance Co JUN 29 1929 at 9 o'clock A.M.  
Fee \$ .90

John H. Ferry, County Recorder. 37284  
By L.B. Woodard, Deputy.

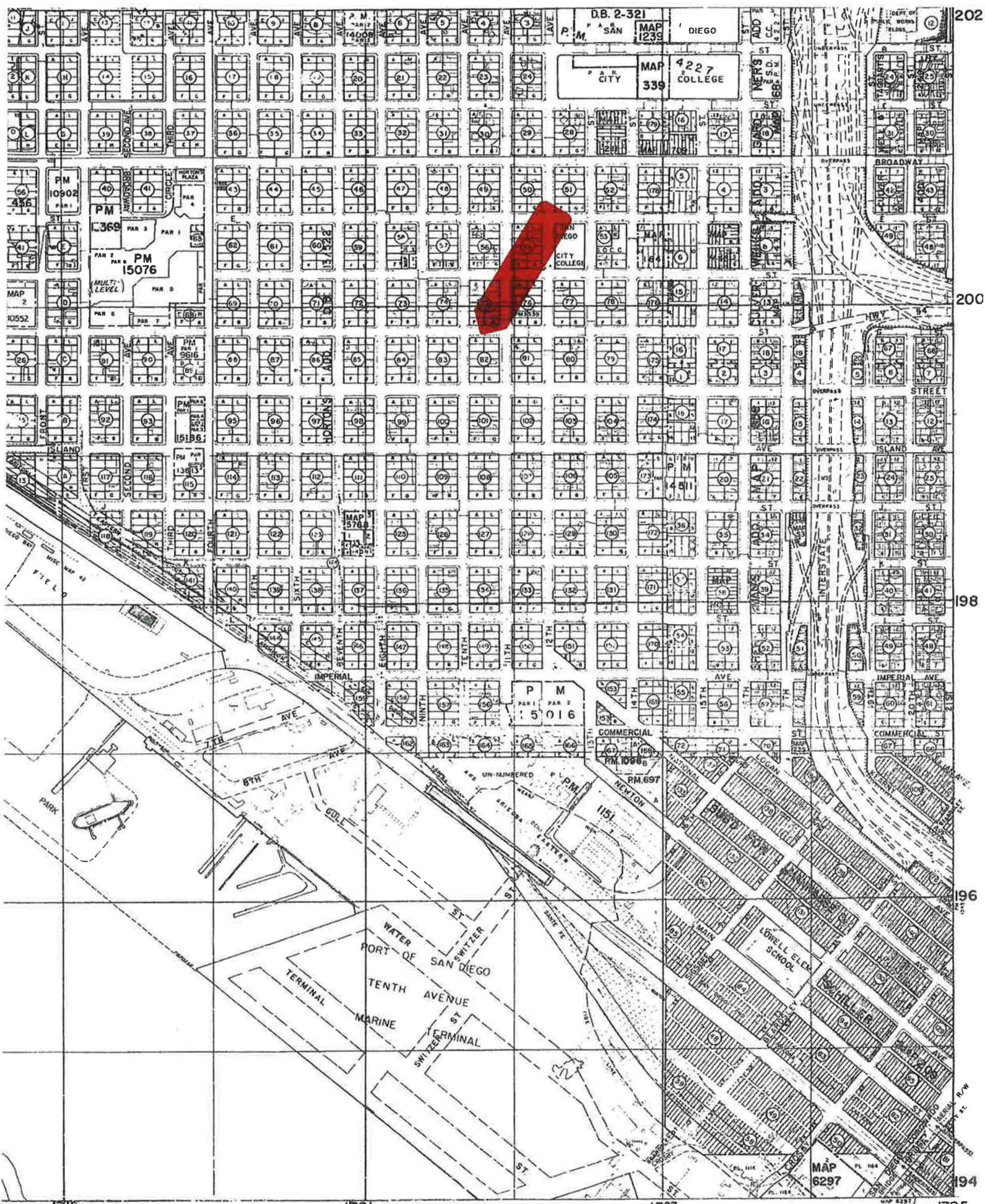
*Handwritten:* 6/29/1929

By L.B. WOODARD, Deputy Recorder

C.1 - CITY OF SAN DIEGO 800 SCALE ENGINEERING MAP

ATTACHED



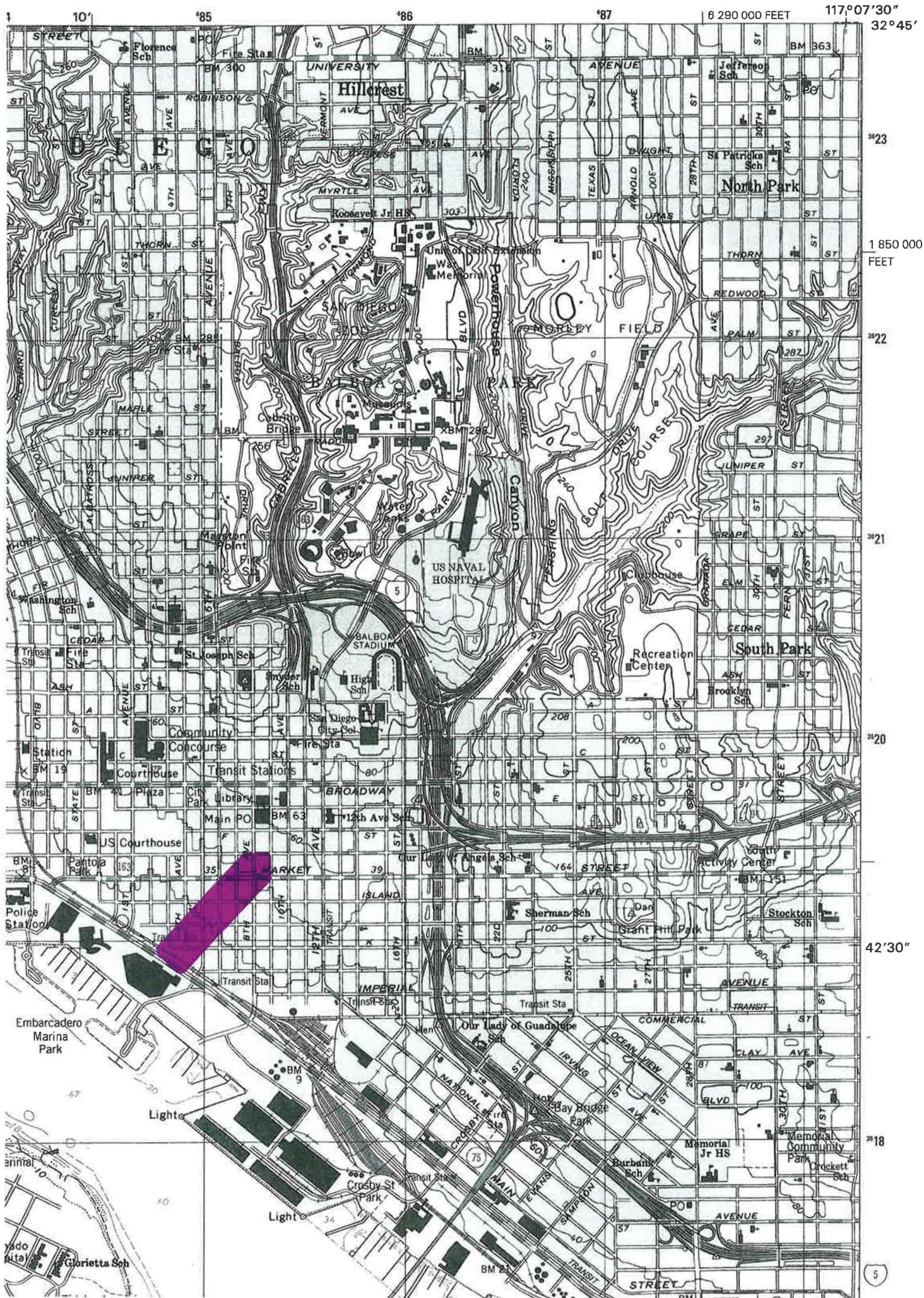




C.2 - CURRENT UNITED STATES GEOLOGICAL SURVEY MAP (USGS)

ATTACHED







**C.3 - ORIGINAL SUBDIVISION MAP**

**ATTACHED**



OFFICIAL MAP OF  
**HORTONS ADDITION**  
SAN DIEGO,

COMPRISING PUEBLO LOTS Nos 1156, 1147,  
1146, 1145, 1134, 1132 & 1133.

Surveyed by L.L. Lockling.  
Civil Engr. & Dep. Co. Surveyor in 1869 & 1870.  
JUNE 1871  
L.L.

SCALE: 300 Ft. to  $\frac{1}{8}$  of an Inch.

Blocks 200\*300 feet.  
or as marked.  
Lots 50\*100 feet.  
or as marked.  
Streets 80 ft. wide,  
Except H<sup>th</sup> & Front which are as marked.

I hereby certify that this is a correct copy  
of Map filed in Deed Book 13, Page 522,  
on file in the office of the Recorder of  
San Diego County, California.

Dated: June 11, 1915  
J. L. Stevens  
County Recorder  
San Diego County, Calif.

Filed for record with the Clerk of  
the County of San Diego, in the  
State of California, June, A.D. 1871.

A.E. Horton.

Filed for record June 21<sup>st</sup> 1871 at  
4 h. 30 m. P.M. and recorded  
July 21 1871 at 2 h. 30 m. at the  
request of A.E. Horton.

Chalmers Scott County Recorder.

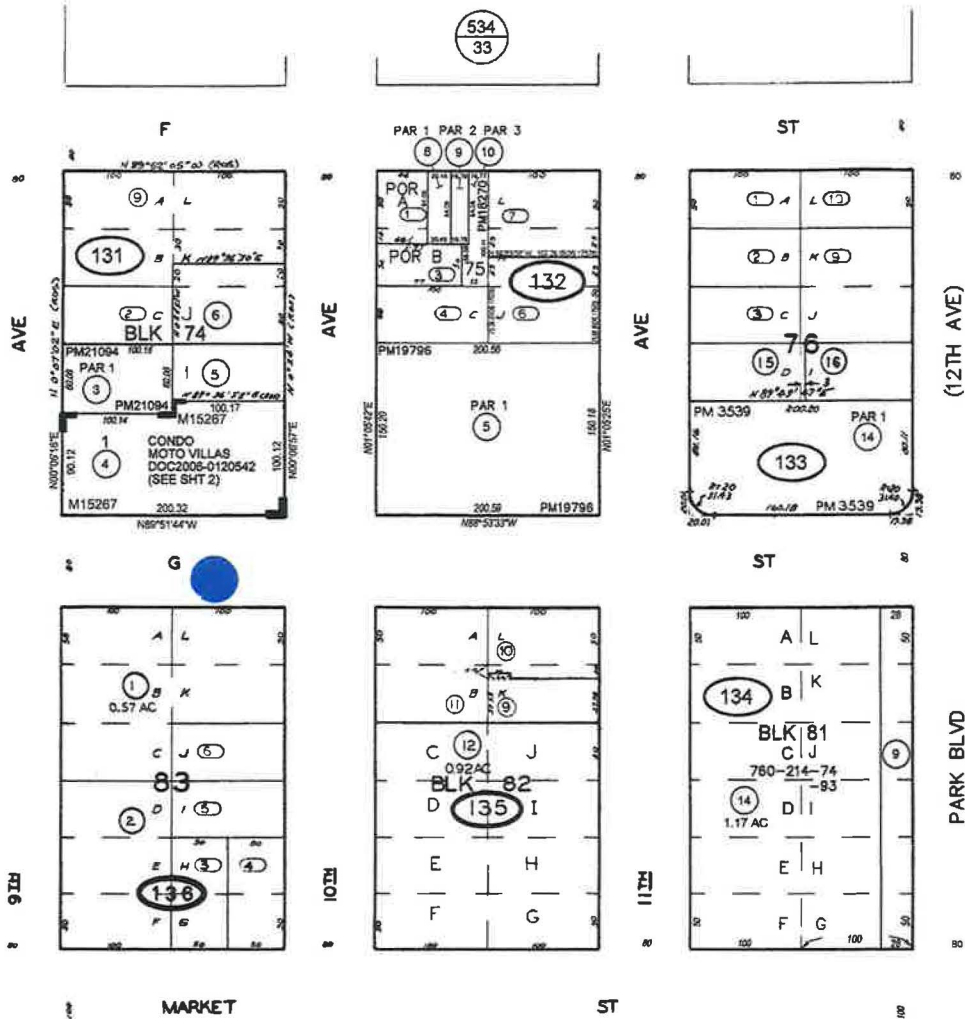


**C.4 - CURRENT ASSESSOR'S PARCEL MAP**

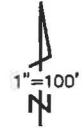
**ATTACHED**



THIS MAP WAS PREPARED FOR ASSESSMENT PURPOSES ONLY. NO LIABILITY IS ASSUMED FOR THE ACCURACY OF THE DATA SHOWN. ASSESSOR'S PARCELS MAY NOT COMPLY WITH LOCAL SUBDIVISION OR BUILDING ORDINANCES.



535-13  
SHT 1 OF 2



1/21/2015 CS ✓

CHANGES					
BLK	OLD	NEW	YR	CUT	
131	1788	9	63	3808	
133	418	1213	65	5496	
135	1	11	75	1358	
133	711	1110	75	3784	CANC
133	711	14	75	344	
133	1213	1516	76	1282	
135	2-6	12	86	2533	
132	2	8-10	00	1440	CC
131	4	SAME	06	738	
134	4-8	9-13	07	1183	
134	1-3 & 7 & 10-13	14	11	1326	

MAP 15267 - MOTO VILLAS CONDOMINIUMS  
DB 13 PG 522 - HORTONS ADD LOCKLING  
ROS 12596,12635,16340,17578

**C.5 - SANBORN FIRE INSURANCE MAPS**

**ATTACHED**



## 1921

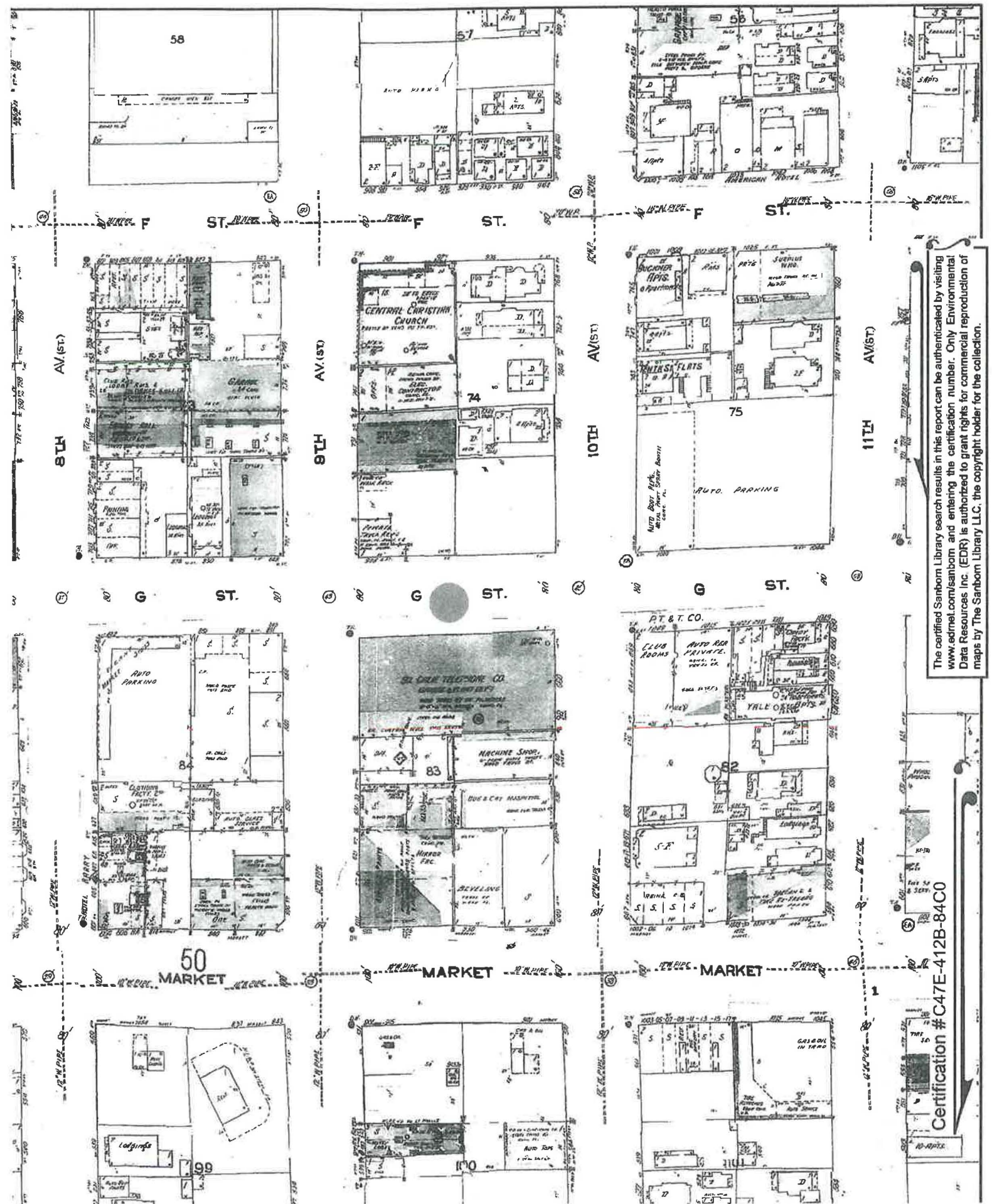


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# Certified Sanborn® Map

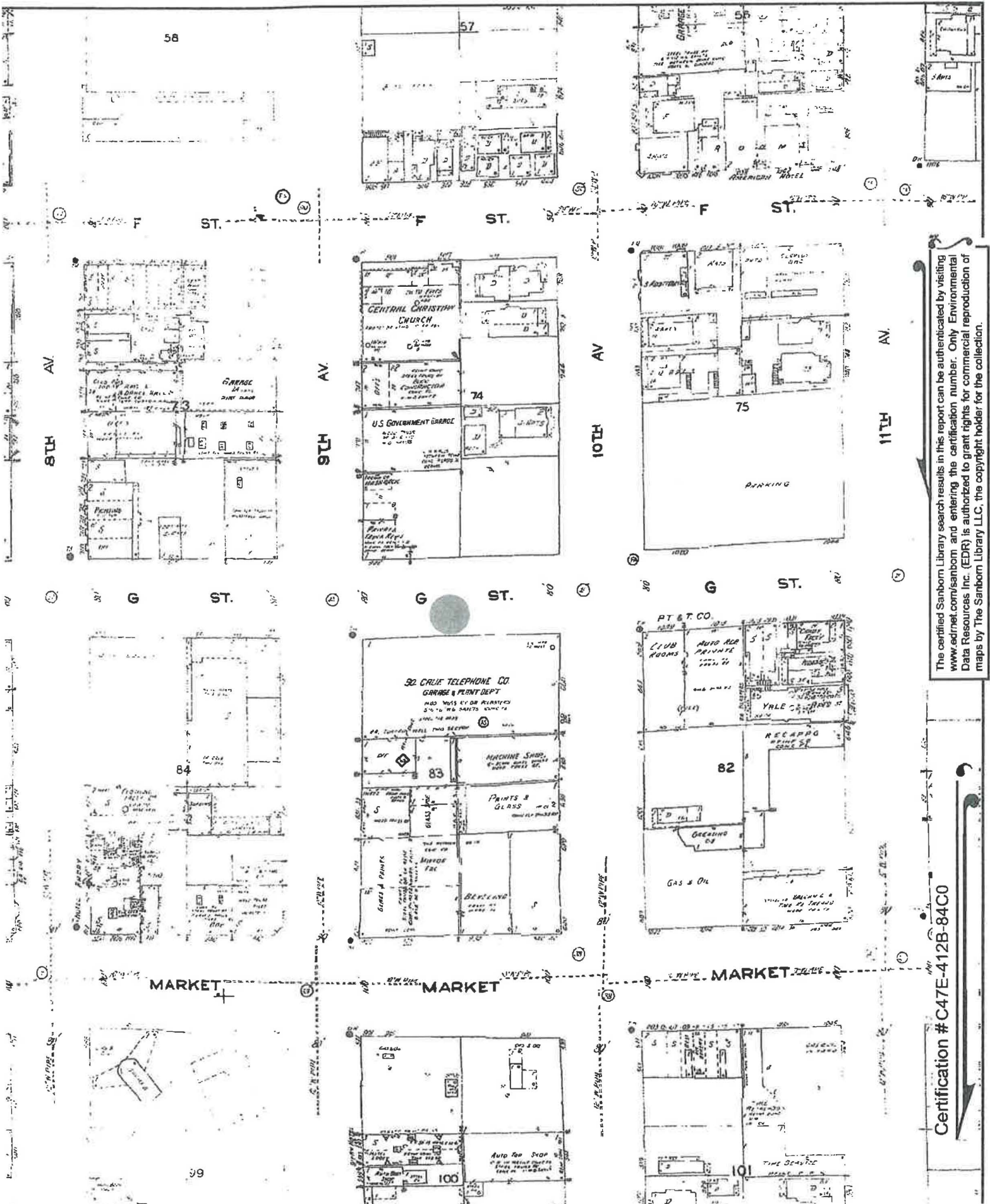
1950



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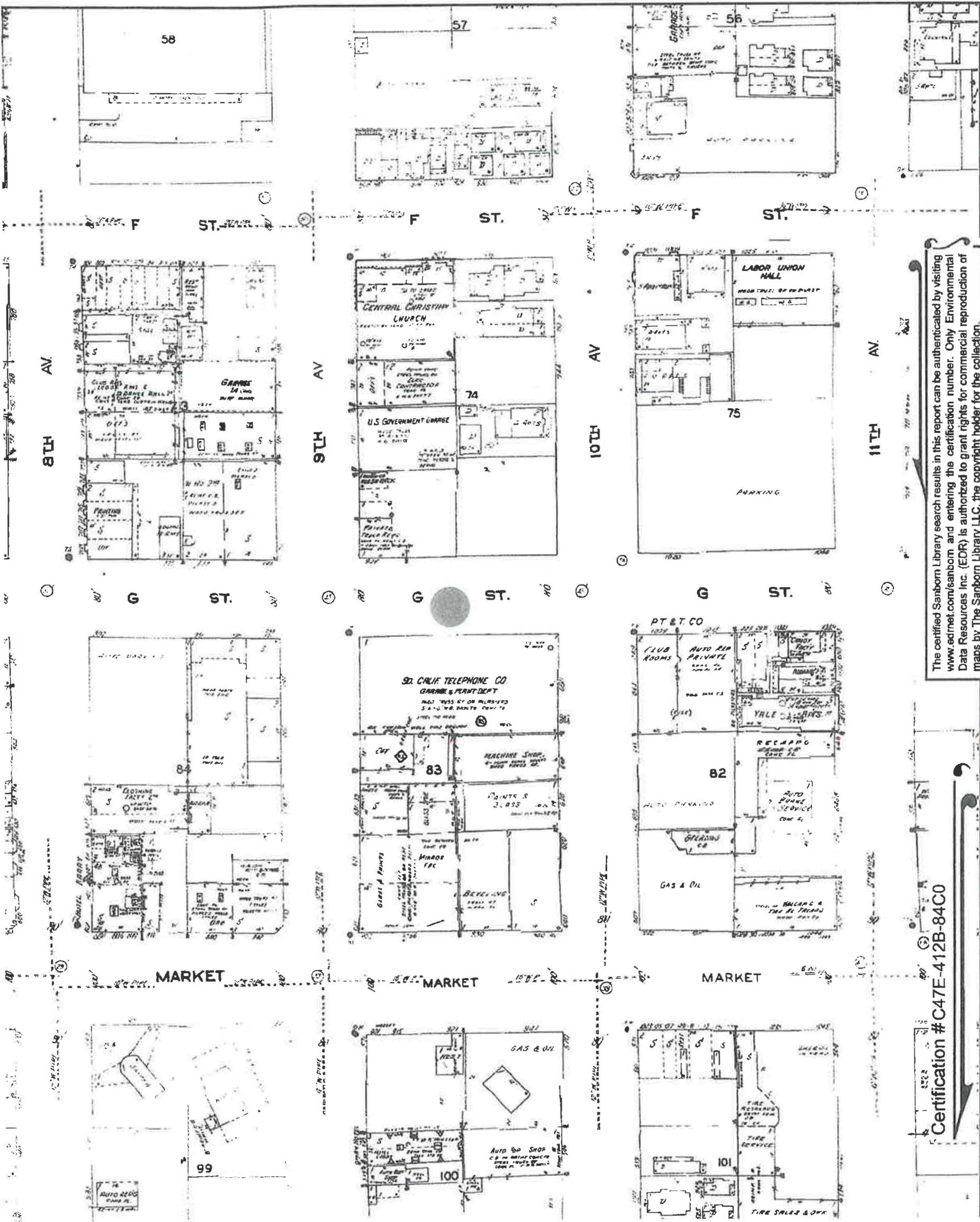
Certification #C47E-412B-84C0





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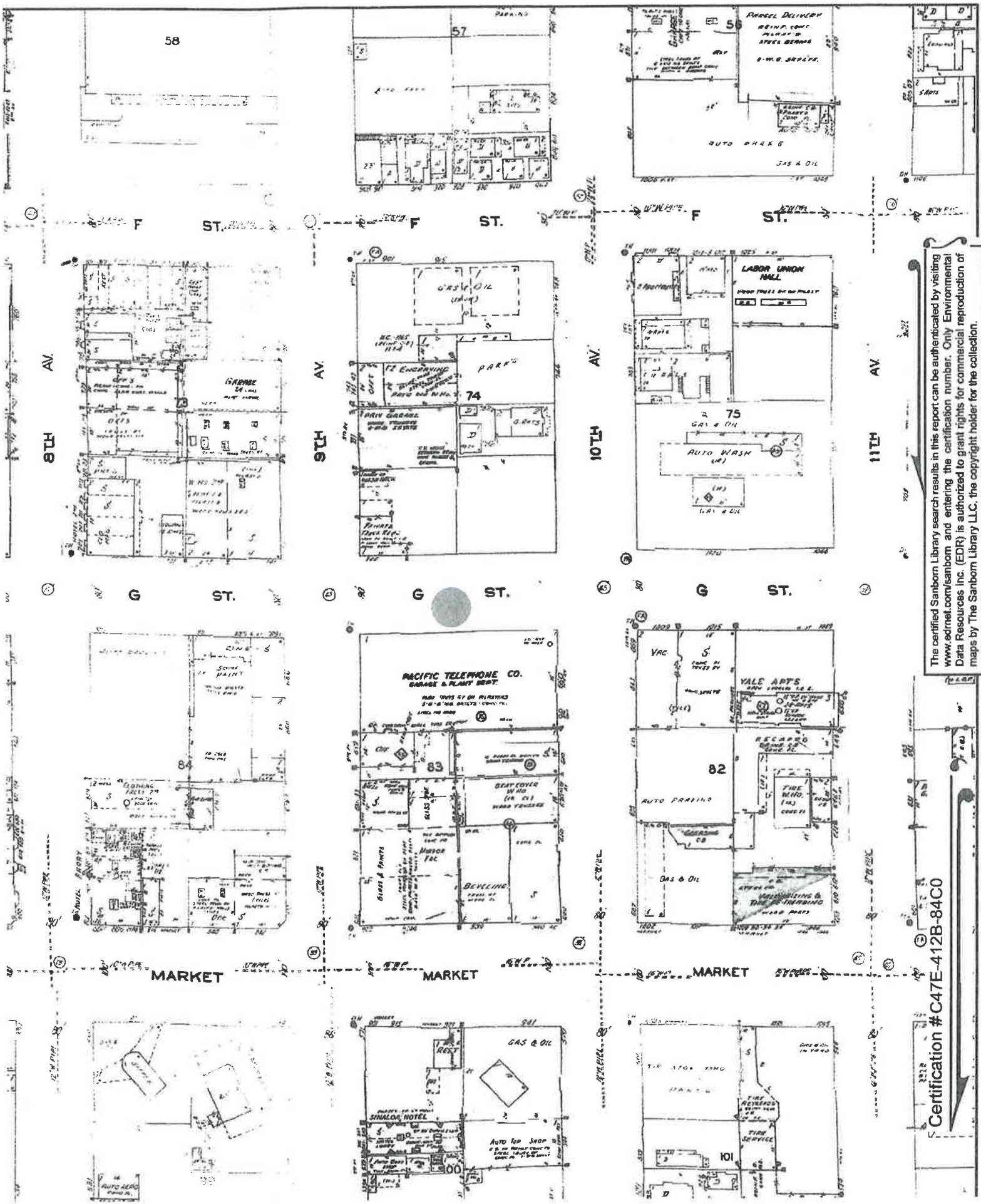
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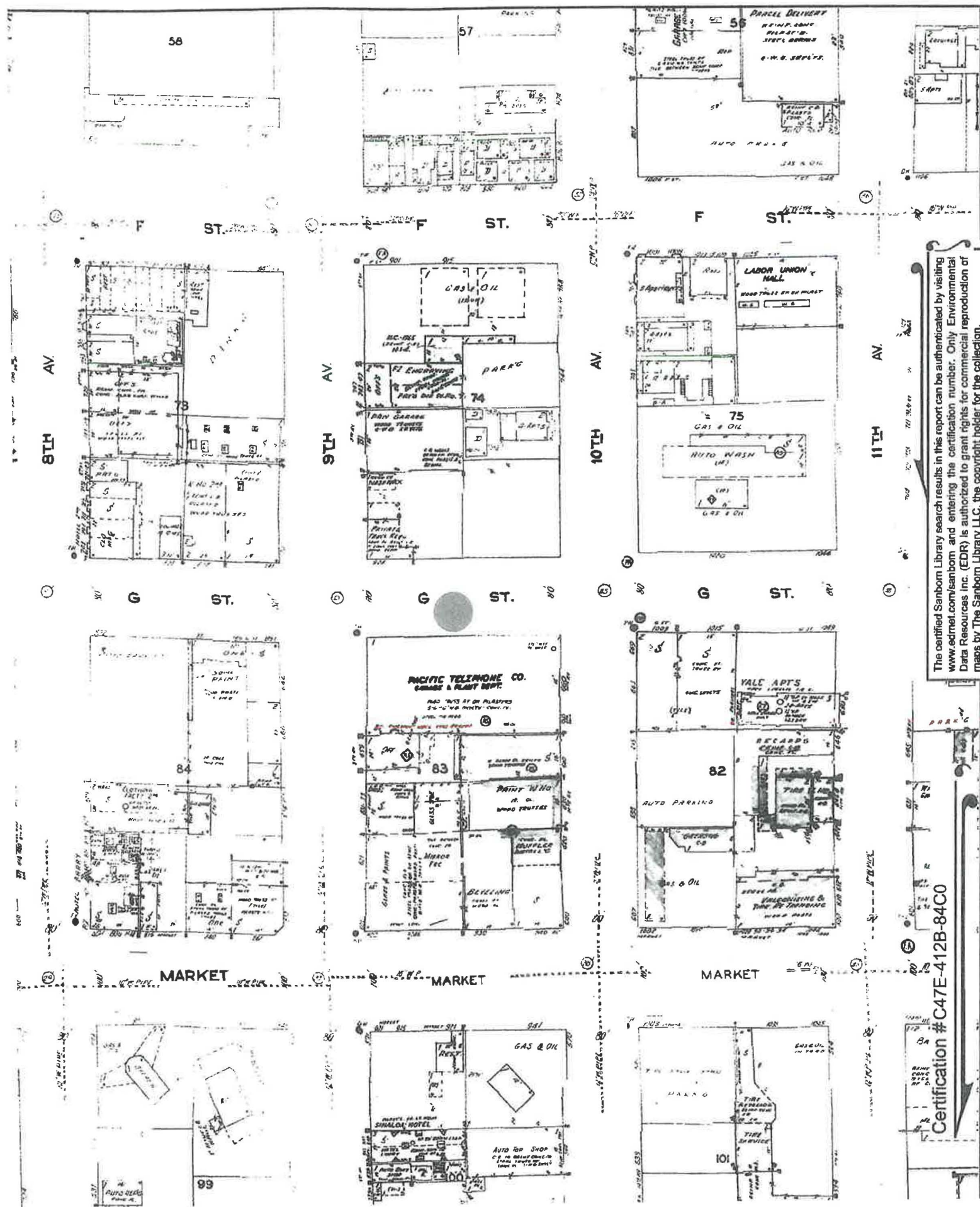
Certification #C47E-412B-84C0





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Certification # C47E-412B-84C0



D.1 - HISTORICAL PHOTOGRAPH

ATTACHED



80: 4686

S.W. 1980 9th Ave,

7/2/80



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Please contact:

San Diego History Center

1649 El Prado, Ste. 3, San Diego, CA 92101

Phone: (619) 232-6203 x 127 or Fax: (619) 232-1059

[collections@sandiegohistory.org](mailto:collections@sandiegohistory.org)



7/2/80

S.D. 1980 9th Ave.

80, 4685



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80: 4688

S.D. 1980 10th Ave

7/2/80



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[collections@sandiegohistory.org](mailto:collections@sandiegohistory.org)



S.D. 1980 10th Ave.

80:4687

7/2/80



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[collections@sandiegohistory.org](mailto:collections@sandiegohistory.org)

**D.2 - CURRENT PHOTOGRAPHS**

**ATTACHED**



660 10th Avenue  
Photo Exhibit – July 2019



Photograph #1: View southeast of northwest corner, 9<sup>th</sup> Avenue and G Street, Art Deco detailing.



Photograph #2: G Street façade, view south. Note 20-lite (typical), and 25-lite windows.

*660 10th Avenue  
Photo Exhibit – July 2019*



Photograph #3: Close-up, Art Deco detailing, G Street.



660 10th Avenue  
Photo Exhibit – July 2019



Photograph #4: G Street façade, view west.



Photograph #5: G Street façade, view southeast.

660 10th Avenue  
Photo Exhibit – July 2019



Photograph #6: Close-up, metal-framed windows.



*660 10th Avenue  
Photo Exhibit – July 2019*



Photograph #7: 25-lite casement.

*660 10th Avenue*  
*Photo Exhibit – July 2019*



Photograph #8: Close-up, metal casement windows, opaque glass.



660 10th Avenue  
Photo Exhibit – July 2019

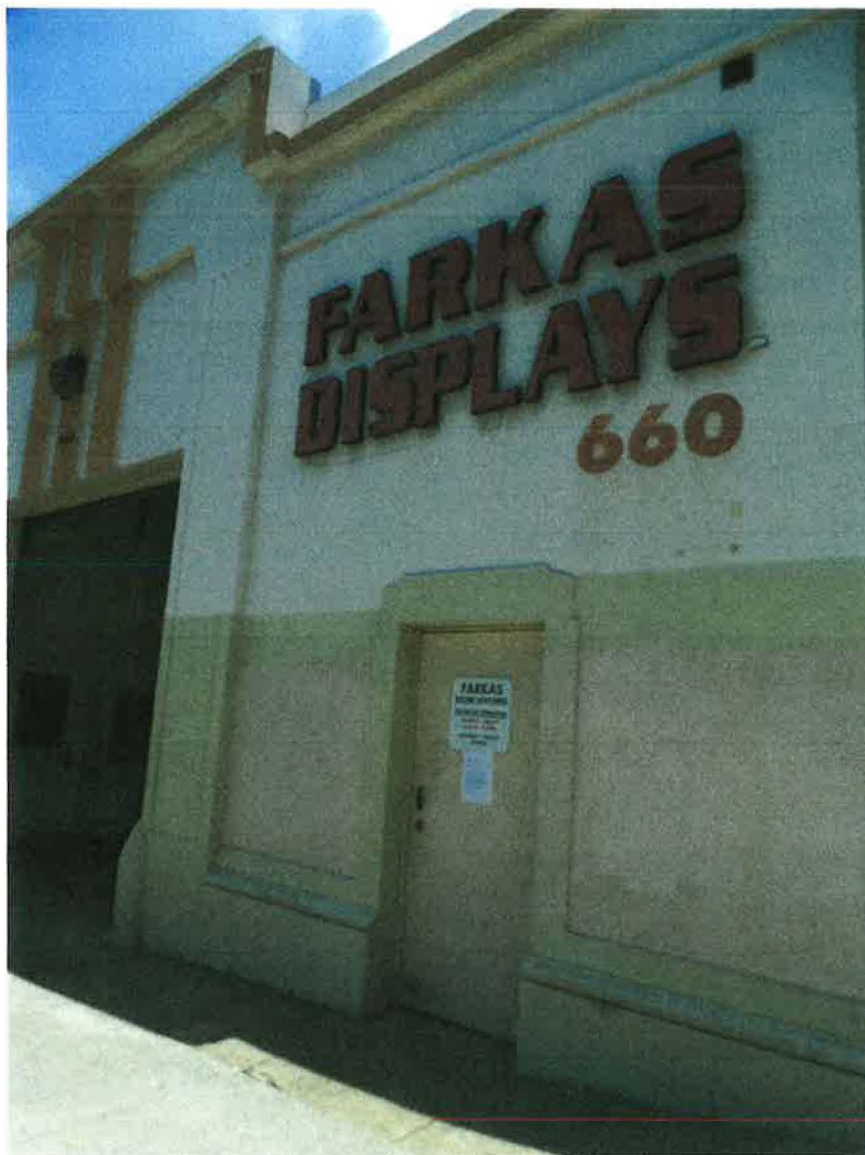


Photograph #9: G Street, northeast corner, view south.



Photograph #10: Northeast corner, 10<sup>th</sup> Avenue and G Street, view southwest.

660 10th Avenue  
Photo Exhibit – July 2019



Photograph #11: Entrance close-up, 660 10<sup>th</sup>. Note bulkhead.



*660 10th Avenue  
Photo Exhibit – July 2019*



Photograph #12: Entrance, 660 10<sup>th</sup> Avenue, view west.



Photograph #13: Southeast corner of building, 10<sup>th</sup> Avenue.

660 10th Avenue  
Photo Exhibit – July 2019



Photograph #14: Warehouse entrance.



Photograph #15: Lot line and adjacent building, southeast corner.



*660 10th Avenue  
Photo Exhibit – July 2019*

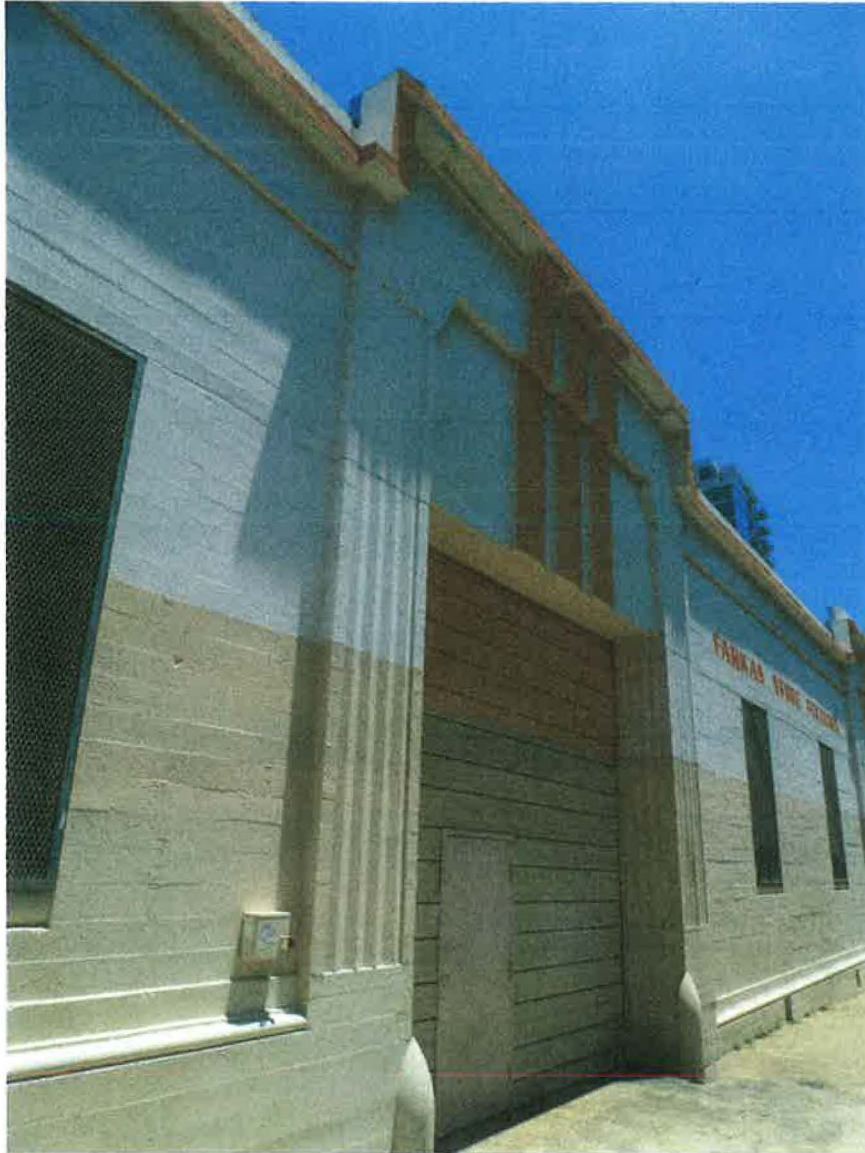


Photograph #16: Art Deco detailing.



Photograph #17: Northwest corner, 9th Avenue & G Street, view southeast.

**660 10th Avenue**  
**Photo Exhibit – July 2019**



**Photograph #18: Single door inserted in roll-up, view southeast.**



660 10th Avenue  
Photo Exhibit – July 2019



Photograph #19: Second 9<sup>th</sup> Avenue truck entryway.



Photograph #20: 1949 addition.

*660 10th Avenue  
Photo Exhibit – July 2019*



Photograph #21: Entrance, 1949 addition.



660 10th Avenue  
Photo Exhibit – July 2019

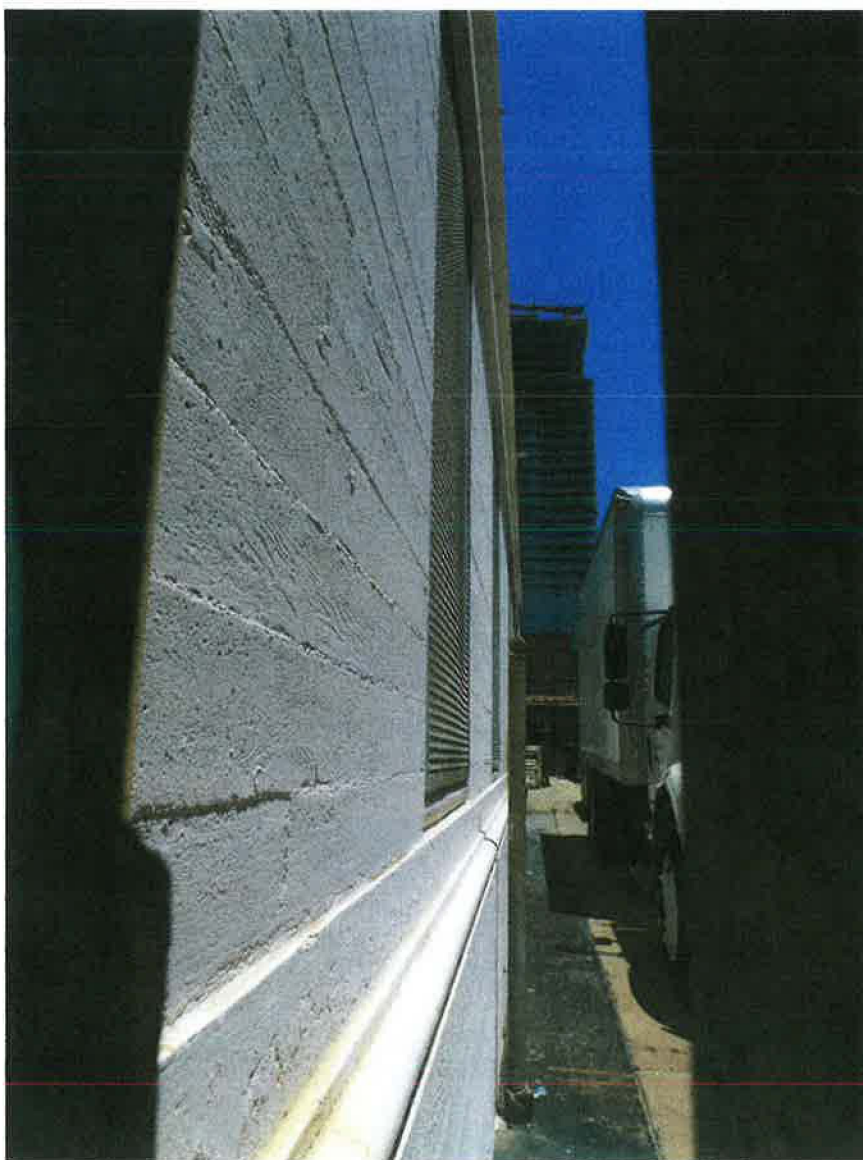


Photograph #22: Truck entrance, 9<sup>th</sup> Avenue addition, facing northeast.



Photograph #23: Gate and separate building to the south.

**660 10th Avenue**  
**Photo Exhibit – July 2019**



**Photograph #24: Southwest corner of addition and truck entrance, facing east.**



**E. - MITIGATION MONITORING AND REPORTING PROGRAM  
DOWNTOWN FEIR/SEIR**

**ATTACHED**

**Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
HISTORICAL RESOURCES (HIST)				
Impact HIST-A.1	Future development in Downtown could impact significant architectural structures. (Direct and Cumulative)			
	<p><b>Mitigation Measure HIST-A.1-1:</b> For construction or development permits that may impact potentially historical resources which are 45 years of age or older and which have not been evaluated for local, state and federal historic significance, a site specific survey shall be required in accordance with the Historical Resources Regulations in the LDC. Based on the survey and the best information available, City Staff to the Historical Resources Board (HRB) shall determine whether historical resources exist, whether potential historical resource(s) is/are eligible for designation as designated historical resource(s) by the HRB, and the precise location of the resource(s). The identified historical resource(s) may be nominated for HRB designation as a result of the survey pursuant to Chapter 12, Article 3, Division 2, Designation of Historical Resource procedures, of the LDC.</p> <p>All applications for construction and development permits where historical resources are present on the site shall be evaluated by City Staff to the HRB pursuant to Chapter 14, Article 3, Division 2, Historical Resources Regulations of the LDC.</p> <p>1. <b>National Register-Listed/Eligible, California Register-Listed/Eligible Resources:</b> Resources listed in or formally determined eligible for the National Register or California Register and resources identified as contributing within a National or California Register District, shall be retained onsite and any improvements, renovation, rehabilitation and/or adaptive reuse of the property shall ensure its preservation and be consistent with the Secretary of the Interior's Standards for the Treatment of Historic Properties (1995) and the associated Guidelines.</p> <p>2. <b>San Diego Register-Listed Resources:</b> Resources listed in the San Diego Register of Historical Resources, or determined to be a contributor to a San Diego Register District, shall, whenever possible, be retained on-site. Partial retention, relocation, or demolition of a resource shall only be permitted according to Chapter 14, Article 3, Division 2, Historical Resources Regulations of the LDC.</p>	Prior to Development Permit (Design)  Prior to Demolition, Grading, and/or Building Permit (Design)  Prior to Certificate of Occupancy (Implementation)	Developer	Civic San Diego /City



**Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p><b>Mitigation Measure HIST-A.1-2:</b> If the potential exists for direct and/or indirect impacts to retained or relocated designated and/or potential historical resources ("historical resources"), the following measures shall be implemented in coordination with a Development Services Department designee and/or City Staff to the HRB ("City Staff") in accordance with Chapter 14, Article 3, Division 2, Historical Resources Regulations of the LDC.</p> <p><b>I. Prior to Permit Issuance</b></p> <p>A. Construction Plan Check</p> <ol style="list-style-type: none"> <li>1. Prior to Notice to Proceed (NTP) for any construction permits, including but not limited to, the first Grading Permit Building Permits, but prior to the first Preconstruction (Precon) Meeting, whichever is applicable, City Staff shall verify that the requirements for historical monitoring during demolition and/or stabilization have been noted on the appropriate construction documents. <ol style="list-style-type: none"> <li>(a) Stabilization work cannot begin until a Precon Meeting has been held at least one week prior to issuance of appropriate permits.</li> <li>(b) Physical description, including the year and type of historical resource, and extent of stabilization shall be noted on the plans.</li> </ol> </li> </ol> <p>B. Submittal of Treatment Plan for Retained Historical Resources</p> <ol style="list-style-type: none"> <li>1. Prior to NTP for any construction permits, including but not limited to, the first Grading Permit and Building Permits, but prior to the first Precon Meeting, whichever is applicable, the Applicant shall submit a Treatment Plan to City Staff for review and approval in accordance in accordance with the Secretary of the Interior's Standards for the Treatment of Historic Properties (1995) and the associated Guidelines. The Treatment Plan shall include measures for protecting any historical resources, as defined in the LDC, during construction related activities (e.g., removal of non-historic features, demolition of adjacent structures, subsurface structural support, etc.). The Treatment Plan shall be shown as notes on all construction documents (i.e., Grading and/or Building Plans).</li> </ol>			

**Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>(b) Required drawings</p> <p>(1) Measured drawings of the building's exterior elevations depicting existing conditions or other relevant features shall be produced from recorded, accurate measurements. If portions of the building are not accessible for measurement, or cannot be reproduced from historic sources, they should not be drawn, but clearly labeled as not accessible. Drawings produced in ink on translucent material or archivally stable material (blue-line drawings) are acceptable. Standard drawing sizes are 19 by 24 inches or 24 by 36 inches, standard scale is 1/4 inch = 1 foot.</p> <p>(2) One set of measured drawings shall be submitted for archival storage with the City of San Diego HRB, the Civic San Diego Project file, the South Coastal Information Center, the California Room of the City of San Diego Public Library, the San Diego Historical Society and/or other historical society or group(s).</p> <p>2. Prior to the first Precon Meeting, City Staff shall verify that the DP has been approved.</p> <p>B. PI Shall Attend Precon Meetings</p> <p>1. Prior to beginning any work that may impact any historical resource(s) which is/are subject to this MMRP, the Applicant shall arrange a Precon Meeting that shall include the PI, Construction Manager (CM) and/or Grading Contractor, Resident Engineer (RE), Historical Monitor(s), Building Inspector (BI), if appropriate, and City Staff. The qualified Historian and/or Architectural Historian shall attend any grading/excavation related Precon Meetings to make comments and/or suggestions concerning the Historical Monitoring program with the Construction Manager and/or Grading Contractor.</p> <p>(a) If the PI is unable to attend the Precon Meeting, the Applicant shall schedule a focused Precon Meeting with City Staff, the PI, RE, CM or BI, if appropriate, prior to the start of any work that requires monitoring.</p> <p>2. Historical Monitoring Plan</p> <p>(a) Prior to the start of any work that is subject to an Historical Monitoring Plan,</p>			



**Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>C. Letters of Qualification have been submitted to City Staff</p> <ol style="list-style-type: none"> <li>1. The applicant shall submit a letter of verification to City Staff identifying the Principal Investigator (PI) for the project and the names of all persons involved in this MMRP (i.e., Architectural Historian, Historic Architect and/or Historian), as defined in the City of San Diego HRG.</li> <li>2. City Staff will provide a letter to the applicant confirming that the qualifications of the PI and all persons involved in the historical monitoring of the project meet the qualification standards established by the HRG.</li> <li>3. Prior to the start of work, the applicant must obtain approval from City Staff for any personnel changes associated with the monitoring program.</li> </ol> <p>II. Prior to Start of Construction</p> <p>A. Documentation Program (DP)</p> <ol style="list-style-type: none"> <li>1. Prior to the first Precon Meeting and/or issuance of any construction permit, the DP shall be submitted to City Staff for review and approval and shall include the following: <ol style="list-style-type: none"> <li>(a) Photo Documentation <ol style="list-style-type: none"> <li>(1) Documentation shall include professional quality photo documentation of the historical resource(s) prior to any construction that may cause direct and/or indirect impacts to the resource(s) with 35mm black and white photographs, 4x6 standard format, taken of all four elevations and close-ups of select architectural elements, such as, but not limited to, roof/wall junctions, window treatments, and decorative hardware. Photographs shall be of archival quality and easily reproducible.</li> <li>(2) Xerox copies or CD of the photographs shall be submitted for archival storage with the City of San Diego HRB and the Civic San Diego Project file. One set of original photographs and negatives shall be submitted for archival storage with the California Room of the City of San Diego Public Library, the San Diego Historical Society and/or other relative historical society or group(s).</li> </ol> </li> </ol> </li> </ol>			

**Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>the PI shall submit an Historical Monitoring Plan which describes how the monitoring would be accomplished for approval by City Staff. The Historical Monitoring Plan shall include an Historical Monitoring Exhibit (HME) based on the appropriate construction documents (reduced to 11x17 inches) to City Staff identifying the areas to be monitored including the delineation of grading/excavation limits.</p> <p>(b) Prior to the start of any work, the PI shall also submit a construction schedule to City Staff through the RE indicating when and where monitoring will occur.</p> <p>(c) The PI may submit a detailed letter to City Staff prior to the start of work or during construction requesting a modification to the monitoring program. This request shall be based on relevant information such as review of final construction documents which indicate site conditions such as underpinning, shoring and/or extensive excavation which could result in impacts to, and/or reduce impacts to the on-site or adjacent historical resource.</p> <p>C. Implementation of Approved Treatment Plan for Historical Resources</p> <ol style="list-style-type: none"> <li>1. Implementation of the approved Treatment Plan for the protection of historical resources within the project site may not begin prior to the completion of the Documentation Program as defined above.</li> <li>2. The qualified Historical Monitor(s) shall attend weekly jobsite meetings and be on-site daily during the stabilization phase for any retained or adjacent historical resource to photo document the Treatment Plan process.</li> <li>3. The qualified Historical Monitor(s) shall document activity via the Consultant Site Visit Record (CSV). The CSV's shall be faxed by the CM to the RE the first day and last day (Notification of Monitoring Completion) of the Treatment Plan process and in the case of ANY unanticipated incidents. The RE shall forward copies to City Staff.</li> <li>4. Prior to the start of any construction related activities, the applicant shall provide verification to City Staff that all historical resources on-site have been adequately stabilized in accordance with the approved Treatment Plan. This may include a site visit with City Staff, the CM, RE or BI, but may also be accomplished through submittal of the draft Treatment Plan photo documentation report.</li> </ol>			



**Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>5. City Staff will provide written verification to the RE or BI after the site visit or upon approval of draft Treatment Plan report indicating that construction related activities can proceed.</p> <p><b>III. During Construction</b></p> <p>A. Qualified Historical Monitor(s) Shall be Present During Grading/Excavation/Trenching</p> <ol style="list-style-type: none"> <li>1. The Qualified Historical Monitor(s) shall be present full-time during grading/excavation/trenching activities which could result in impacts to historical resources as identified on the HME. The Construction Manager is responsible for notifying the RE, PI, and City Staff of changes to any construction activities.</li> <li>2. The Qualified Historical Monitor(s) shall document field activity via the CSV. The CSV's shall be faxed by the CM to the RE the first day of monitoring, the last day of monitoring, monthly (Notification of Monitoring Completion), and in the case of ANY incidents involving the historical resource. The RE shall forward copies to City Staff.</li> <li>3. The PI may submit a detailed letter to City Staff during construction requesting a modification to the monitoring program when a field condition arises which could effect the historical resource being retained on-site or adjacent to the construction site.</li> </ol> <p>B. Notification Process</p> <ol style="list-style-type: none"> <li>1. In the event of damage to a historical resource retained on-site or adjacent to the project site, the Qualified Historical Monitor(s) shall direct the contractor to temporarily divert construction activities in the area of historical resource and immediately notify the RE or BI, as appropriate, and the PI (unless Monitor is the PI).</li> <li>2. The PI shall immediately notify City Staff by phone of the incident, and shall also submit written documentation to City Staff within 24 hours by fax or email with photos of the resource in context, if possible.</li> </ol>			

**Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>C. Determination/Evaluation of Impacts to a Historical Resource</p> <ol style="list-style-type: none"> <li>1. The PI shall evaluate the incident relative to the historical resource. <ol style="list-style-type: none"> <li>(a) The PI shall immediately notify City Staff by phone to discuss the incident and shall also submit a letter to City Staff indicating whether additional mitigation is required.</li> <li>(b) If impacts to the historical resource are significant, the PI shall submit a proposal for City Staff review and written approval in accordance with Chapter 14, Article 3, Division 2, Historical Resources Regulations of the LDC and the Secretary of the Interior's Standards for the Treatment of Historic Properties (1995) and the associated Guidelines. Direct and/or indirect impacts to historical resources from construction activities must be mitigated before work will be allowed to resume.</li> <li>(c) If impacts to the historical resource are not considered significant, the PI shall submit a letter to City Staff indicating that the incident will be documented in the Final Monitoring Report. The letter shall also indicate that that no further work is required.</li> </ol> </li> </ol> <p>IV. Night Work</p> <p>A. If night and/or weekend work is included in the contract</p> <ol style="list-style-type: none"> <li>1. When night and/or weekend work is included in the contract package, the extent and timing shall be presented and discussed at the Precon Meeting.</li> <li>2. The following procedures shall be followed. <ol style="list-style-type: none"> <li>(a) No Impacts/Incidents In the event that no historical resources were impacted during night and/or weekend work, the PI shall record the information on the CSV and submit to City Staff via fax by 8 a.m. of the next business day.</li> <li>(b) Potentially Significant Impacts If the PI determines that a potentially significant impact has occurred to a historical resource, the procedures detailed under Section III - During</li> </ol> </li> </ol>			



**Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>Construction shall be followed.</p> <p>(c) The PI shall immediately contact City Staff, or by 8 a.m. of the next business day to report and discuss the findings as indicated in Section III-B, unless other specific arrangements have been made.</p> <p>B. If night and/or weekend work becomes necessary during the course of construction:</p> <ol style="list-style-type: none"> <li>1. The Construction Manager shall notify the RE, or BI, as appropriate, a minimum of 24 hours before the work is to begin.</li> <li>2. The RE, or BI, as appropriate, shall notify City Staff immediately.</li> </ol> <p>C. All other procedures described above shall apply, as appropriate.</p> <p><b>V. Post Construction</b></p> <p>A. Submittal of Draft Monitoring Report</p> <ol style="list-style-type: none"> <li>1. The PI shall submit two copies of the Draft Monitoring Report (even if negative), prepared in accordance with the Historical Resources Guidelines (HRG) and Appendices which describes the results, analysis, and conclusions of all phases of the Historical Monitoring Plan (with appropriate graphics) to City Staff for review and approval within 90 days following the completion of monitoring. <ol style="list-style-type: none"> <li>(a) The preconstruction Treatment Plan and Documentation Plan (photos and measured drawings) and Historical Commemorative Program, if applicable, shall be included and/or incorporated into the Draft Monitoring Report.</li> <li>(b) The PI shall be responsible for updating (on the appropriate State of California Department of Park and Recreation forms-DPR 523 A/B) any existing site forms to document the partial and/or complete demolition of the resource. Updated forms shall be submitted to the South Coastal Information Center with the Final Monitoring Report.</li> </ol> </li> <li>2. City Staff shall return the Draft Monitoring Report to the PI for revision or, for preparation of the Final Report.</li> <li>3. The PI shall submit revised Draft Monitoring Report to City Staff for approval.</li> </ol>			

**Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<ol style="list-style-type: none"> <li>4. City Staff shall provide written verification to the PI of the approved report.</li> <li>5. City Staff shall notify the RE or BI, as appropriate, of receipt of all Draft Monitoring Report submittals and approvals.</li> </ol> <p>B. Final Monitoring Report(s)</p> <ol style="list-style-type: none"> <li>1. The PI shall submit one copy of the approved Final Monitoring Report to the RE or BI as appropriate, and one copy to City Staff (even if negative), within 90 days after notification from City Staff that the draft report has been approved.</li> <li>2. The RE shall, in no case, issue the Notice of Completion until receiving a copy of the approved Final Monitoring Report from City Staff.</li> </ol>			
	<p><b>Mitigation Measure HIST-A.1-3:</b> If a designated or potential historical resource ("historical resource") as defined in the LDC would be demolished, the following measure shall be implemented in accordance with Chapter 14, Article 3, Division 2, Historical Resources Regulations of the LDC.</p> <p><b>I. Prior to Issuance of a Demolition Permit</b></p> <p>A. A DP shall be submitted to City Staff to the HRB ("City Staff") for review and approval and shall include the following:</p> <ol style="list-style-type: none"> <li>1. Photo Documentation <ol style="list-style-type: none"> <li>(a) Documentation shall include professional quality photo documentation of the structure prior to demolition with 35 millimeter black and white photographs, 4x6 inch standard format, taken of all four elevations and close-ups of select architectural elements, such as, but not limited to, roof/wall junctions, window treatments, decorative hardware. Photographs shall be of archival quality and easily reproducible.</li> <li>(b) Xerox copies or CD of the photographs shall be submitted for archival storage with the City of San Diego HRB and the Civic San Diego Project file. One set of original photographs and negatives shall be submitted for archival storage with the California Room of the City of San Diego Public Library, the San Diego Historical Society and/or other relative historical society or group(s).</li> </ol> </li> </ol>			



**Downtown FEIR/SEIR  
Mitigation Monitoring and Reporting Program**

Significant Impact(s)	Mitigation Measure(s)	Implementation		
		Time Frame	Responsibility	Verification Responsibility
	<p>2. Required drawings</p> <p>(a) Measured drawings of the building's exterior elevations depicting existing conditions or other relevant features shall be produced from recorded, accurate measurements. If portions of the building are not accessible for measurement, or cannot be reproduced from historic sources, they should not be drawn, but clearly labeled as not accessible. Drawings produced in ink on translucent material or archivally stable material (blue-line drawings are acceptable). Standard drawing sizes are 19 by 24 inches or 24 by 36 inches, standard scale is 1/4 inch = 1 foot.</p> <p>(b) One set of measured drawings shall be submitted for archival storage with the City of San Diego HRB, the Civic San Diego Project file, the South Coastal Information Center, the California Room of the City of San Diego Public Library, the San Diego Historical Society and/or other historical society or group(s).</p> <p>B. Prior to the first Precon Meeting City Staff shall verify that the DP has been approved.</p> <p>C. In addition to the Documentation Program, the Applicant shall comply with any other conditions contained in the Site Development Permit pursuant to Chapter 14, Article 3, Division 2, Historical Resources Regulations of the LDC.</p>			
<b>Impact HIST-B.1</b>	Development in Downtown could impact significant buried archaeological resources. (Direct and Cumulative)			
	<p><b>Mitigation Measure HIST-B.1-1:</b> If the potential exists for direct and/or indirect impacts to significant buried archaeological resources, the following measures shall be implemented in coordination with a Development Services Department designee and/or City Staff to the HRB ("City Staff") in accordance with Chapter 14, Article 3, Division 2, Historical Resources Regulations of the LDC. Prior to issuance of any permit that could directly affect an archaeological resource, City Staff shall assure that all elements of the MMRP are performed in accordance with all applicable City regulations and guidelines by an Archaeologist meeting the qualifications specified in Appendix B of the San Diego LDC, Historical Resources Guidelines. City Staff shall also require that the following steps be taken to determine: (1) the presence of archaeological resources and (2) the appropriate mitigation for any significant resources which may be impacted by a development activity. Sites may include residential and commercial properties, privies, trash pits, building foundations, and industrial features</p>	<p>Prior to Demolition or Grading Permit (Design)</p> <p>Prior to Certificate of Occupancy (Implementation)</p>	Developer	City Staff



THE PACIFIC TELEPHONE AND TELEGRAPH COMPANY GARAGE (HRB #1355, 1946 ADDITION)  
559 NINTH AVENUE, SAN DIEGO, CA 92101  
PERIOD OF SIGNIFICANCE 1932–1956 — HISTORIC AMERICAN BUILDING SURVEY

HISTORIC  
AMERICAN  
BUILDING  
SURVEY



4 STREET ENTRANCE  
HA.1 PHOTO, MARCH 2020



3 SOUTH / EAST ELEVATION  
HA.1 PHOTO, MARCH 2020



2 SOUTH / WEST ELEVATION  
HA.1 PHOTO, MARCH 2020

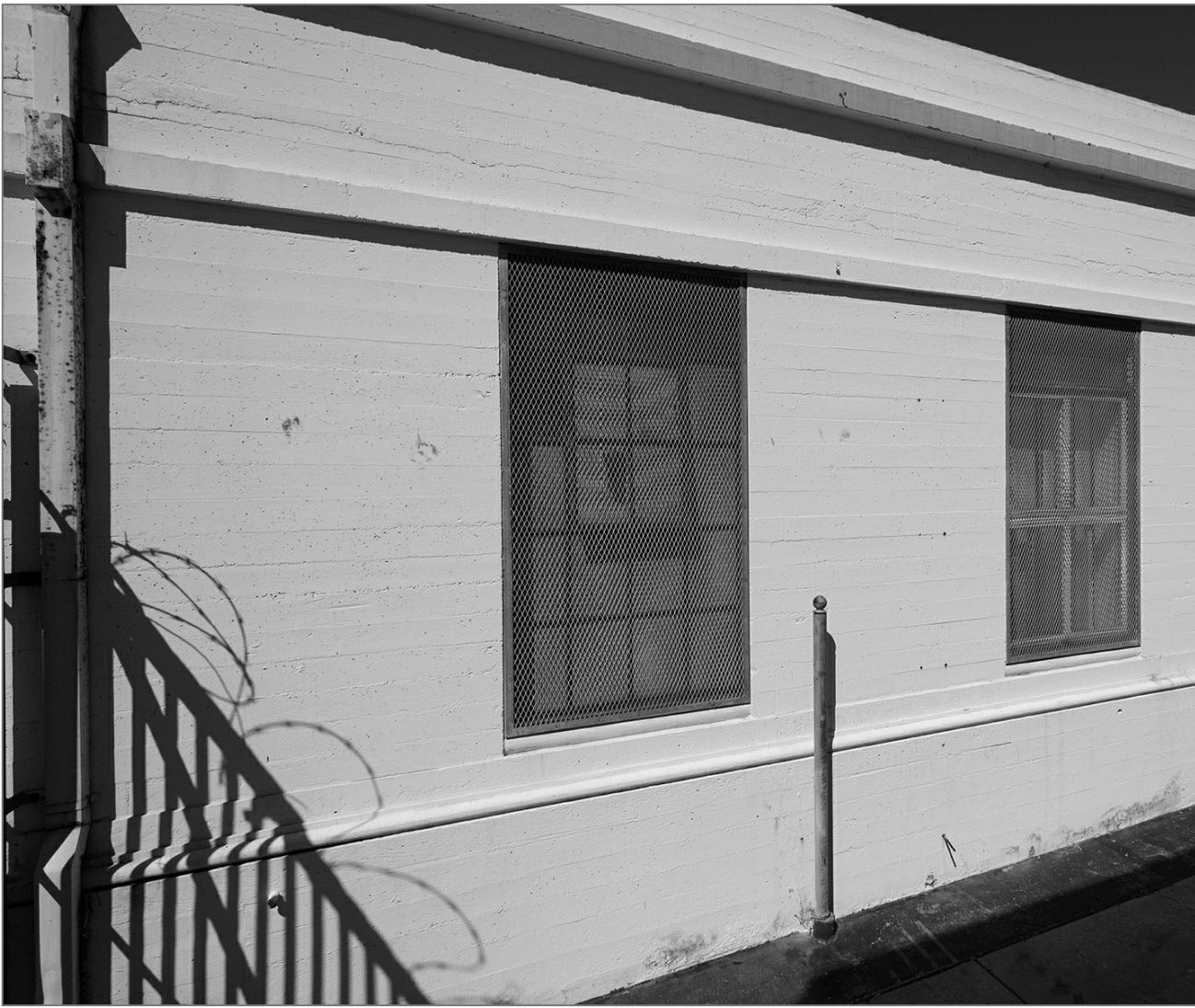
DESCRIPTION OF RESOURCE

THE DESIGNATED RESOURCE (HRB #1355) WITH A PERIOD OF SIGNIFICANCE OF 1932 – 1956 IS LOCATED AT 660 10TH AVE. AND 659 9TH AVE., SAN DIEGO, CA 92101 (APN:535-136-00-00). THE RESOURCE CONSISTS OF A ONE STORY COMMERCIAL BUILDING WITH A RECTANGULAR FOOTPRINT OF APPROX. 100' x 200', CONSTRUCTED IN 1932 IN THE ART DECO STYLE AND A ONE STORY ADDITION, SIMILAR IN STYLE, BUILT IN 1946 DIRECTLY ADJACENT TO THE SOUTH. THE ADDITION MEASURES APPROX. 36' x 60'. PLEASE NOTE THIS ADDITION IS HE SUBJECT OF THESE DOCUMENTS, THE 1932 MAIN BUILDING IS DISCUSSED IN A SEPARATE SET OF HABS DRAWINGS.

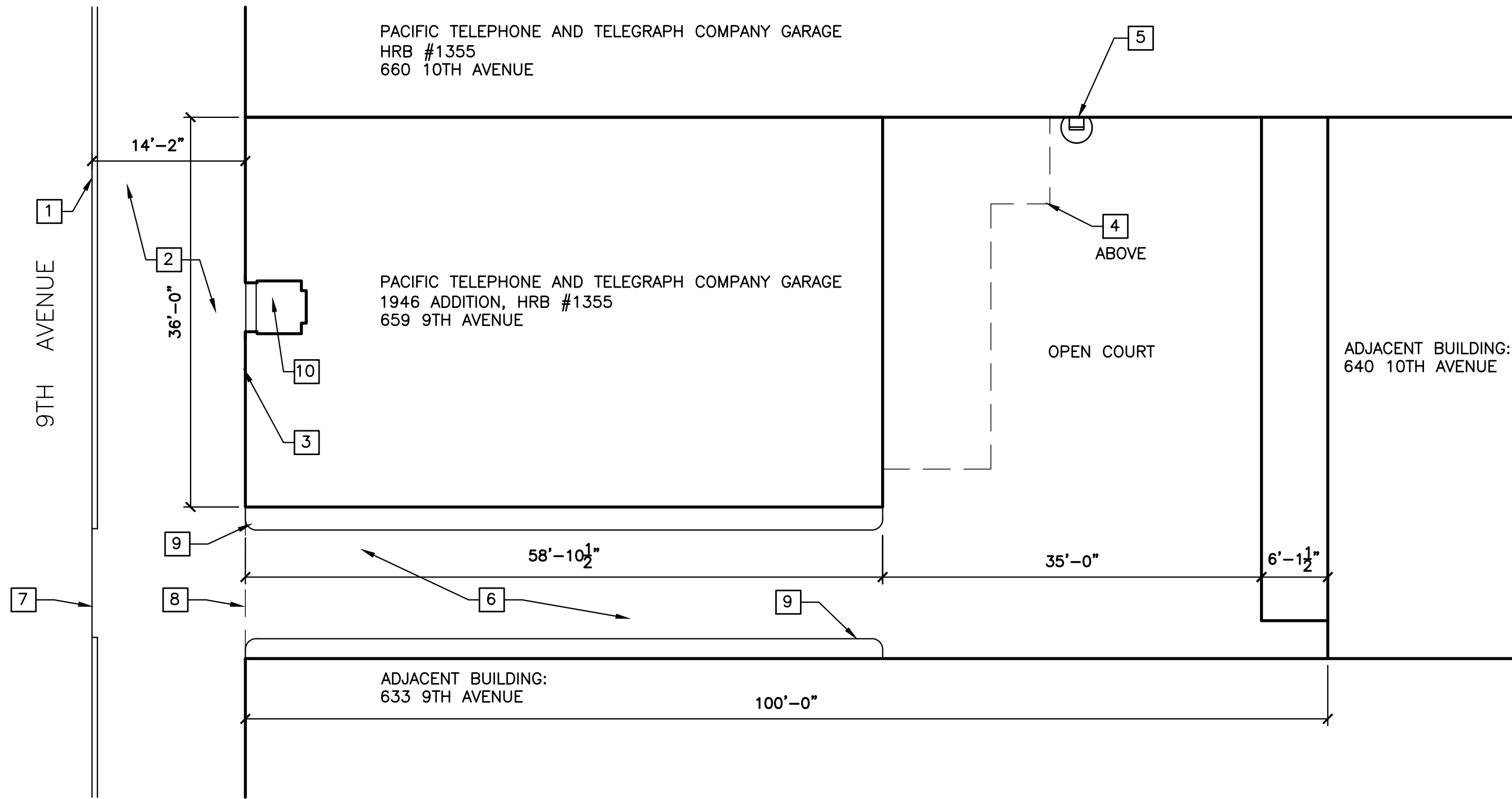
THE CITY OF SAN DIEGO HISTORICAL RESOURCES BOARD BASED ITS DESIGNATION OF THE RESOURCE ON THE FOLLOWING FINDINGS:

CRITERION A: THE PROPERTY IS HISTORICALLY SIGNIFICANT AS A SPECIAL ELEMENT OF THE CITY'S ECONOMIC DEVELOPMENT, REVEALING THE INCREASING IMPORTANCE OF THE AUTOMOBILE AS AMERICA'S FAVORED METHOD OF PRIVATE TRANSPORTATION FROM 1900–1956, THE BUILDING RETAINS INTEGRITY TO ITS 1932–1956 PERIOD OF SIGNIFICANCE. SPECIFICALLY, THE RESOURCE WHICH WAS CONSTRUCTED IN 1932 FOR USE AS A GARAGE, CONTINUES TO CONVEY THE SIGNIFICANT CHARACTER DEFINING FEATURES OF AUTOMOBILE-RELATED DEVELOPMENT AS IDENTIFIED IN THE EAST VILLAGE HISTORICAL CONTEXT STATEMENT, INCLUDING A SINGLE STORY, WAREHOUSE-LIKE MASSING; LIMITED ORNAMENTATION, RAISED VERTICAL PROJECTIONS AND NUMEROUS GARAGE DOORS.

CRITERION C: THE PROPERTY IS HISTORICALLY SIGNIFICANT THROUGH THE RETENTION OF CHARACTER DEFINING FEATURES OF THE ART DECO STYLE AND RETAINS A GOOD LEVEL OF ARCHITECTURAL INTEGRITY FROM ITS 1932 PERIOD OF SIGNIFICANCE. SPECIFICALLY, THE RESOURCE EXHIBITS SMOOTH WALL SURFACES, A FLAT ROOF, VERTICAL PROJECTIONS ABOVE THE ROOFLINE, FLUTED PILASTERS, GEOMETRIC DETAILING AND METAL WINDOWS.



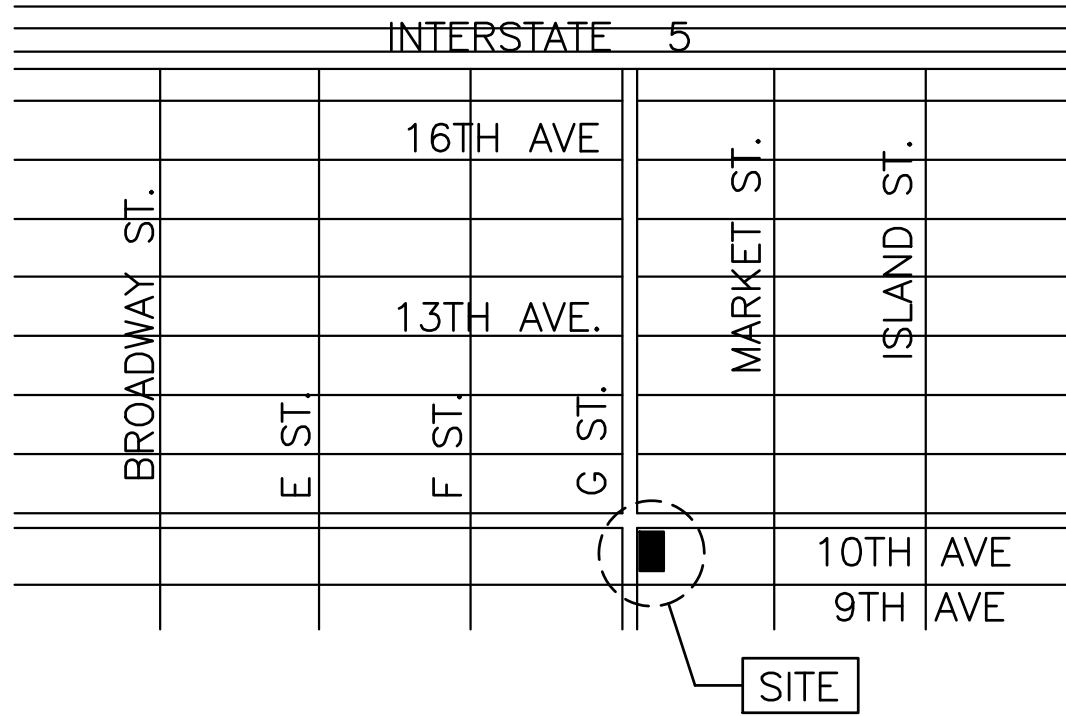
5 STEEL WINDOW  
HA.1 PHOTO, MARCH 2020



1 SITE PLAN  
HA.1 1" = 10'-0" 0' 5' 10' 20' N

SITE PLAN KEYNOTES

- |                              |                          |
|------------------------------|--------------------------|
| 1 STREET CURB                | 7 CURB CUT               |
| 2 PUBLIC SIDEWALK            | 8 LOCATION OF METAL GATE |
| 3 BOARD FOMRED CONCRETE WALL | 9 ELEVATED WALKWAY       |
| 4 CORRUGATED METAL ROOF      | 10 RECESSED ENTRYWAY     |
| 5 ROOF ACCESS LADDER         |                          |
| 6 DRIVEWAY                   |                          |



VICINITY MAP  
NO SCALE N

INDEX OF SHEETS

- |      |                                   |
|------|-----------------------------------|
| HA 1 | SITE PLAN, PHOTOS, VICINITY MAP   |
| HA 2 | FLOOR PLAN                        |
| HA 3 | ROOF PLAN                         |
| HA 4 | WEST, EAST AND SOUTH ELEVATION    |
| HA 5 | BLDG. SECTIONS A-A & B-B, DETAILS |
| HA 6 | WINDOW TYPES ELEVATIONS & DETAILS |

PROJECT :  
THE PACIFIC TELEPHONE & TELEGRAPH CO. GARAGE  
1946 ADDITION, HRB #1355  
559 9TH AVENUE, SAN DIEGO, CA 92101  
CLIENT :  
CISTERRA PARTNERS, LLC  
3580 CARREL MOUNTAIN ROAD, STE. # 460, SAN DIEGO, CA 92130

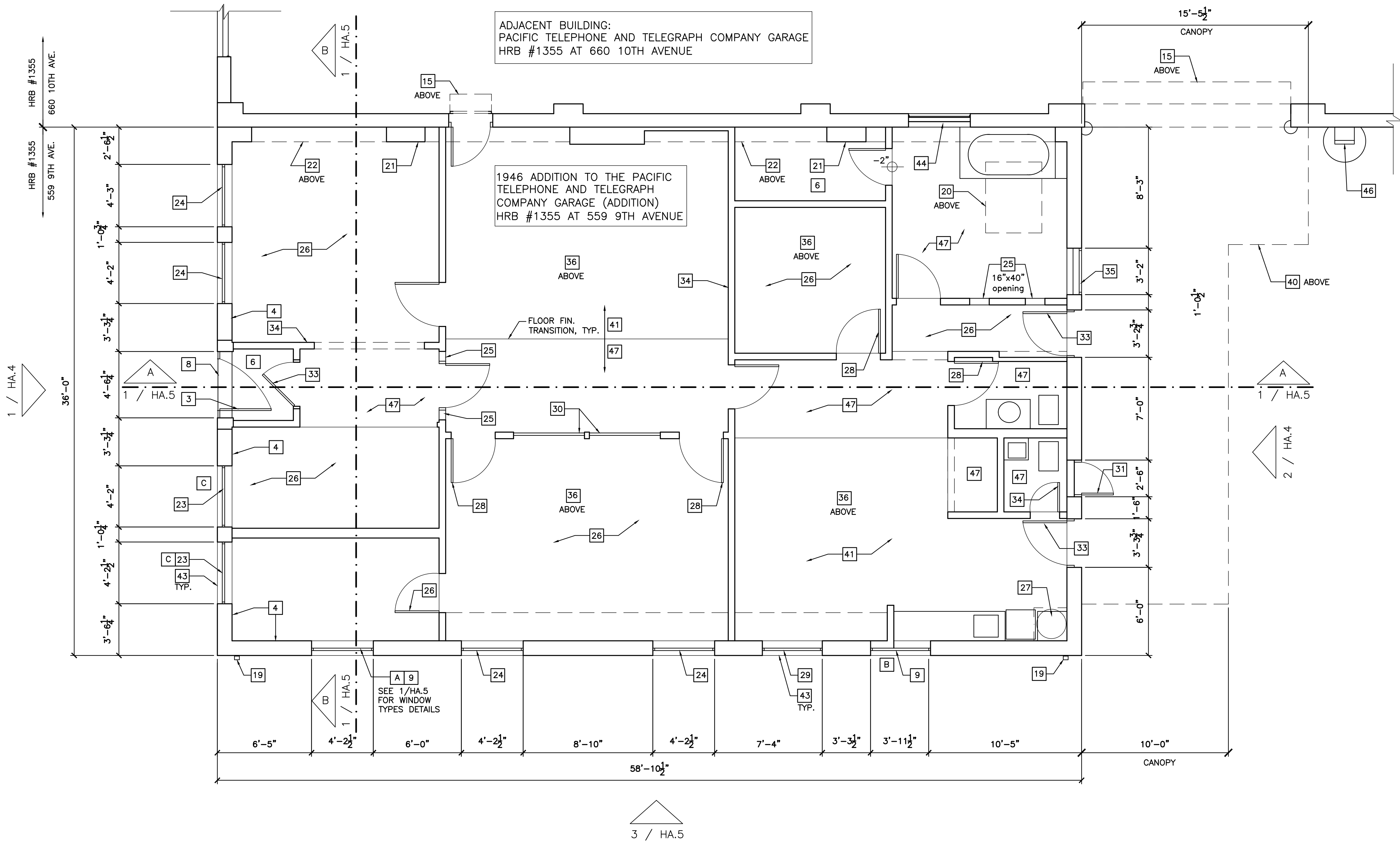
ARCHITECT:  
UNION ARCHITECTURE, INC.  
JOHN H. EISENHART, ARCHITECT  
EVA THORN, INTERIORS  
344 22ND STREET, SAN DIEGO, CA 92102  
TELEPHONE: 619.788.2862 / WWW.UNIONARCH.COM

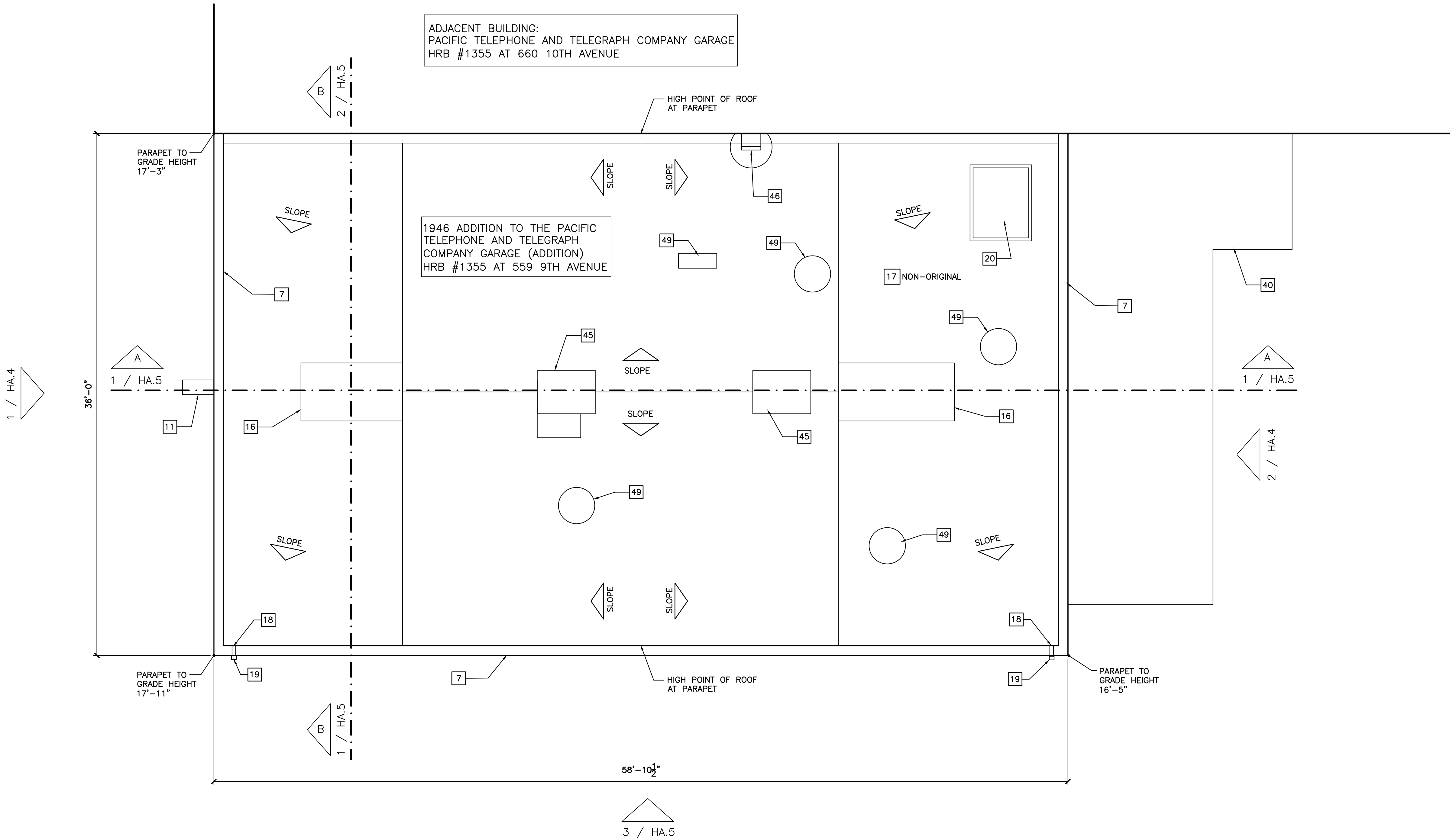
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permission of the architect.

DATE: 03.20.2020

HA.1  
SHEET 1 OF 6







1 ROOF PLAN  
HA.3  
1/4" = 1'-0"  
0' 2' 4' 8'  
N

NOTE:

- A. THE 1946 BUILDING IS ORIGINAL EXCEPT FOR:
1. THE WIRE MESH WINDOW GUARDS.
1. WINDOWS AS NOTED.
- B. CIP : ABBR. CAST-IN PLACE CONCRETE

KEYNOTES

- 1 CONCRETE SIDEWALK / GRADE
- 2 METAL GATE
- 3 METAL SCREEN DOOR
- 4 BOARD FORMED (CAST-IN-PLACE) CONCRETE WALL, PAINTED
- 5 CONCRETE CAST-IN-PLACE (CIP) BASE/TORUS MOULDING DETAIL

- 6 CONCRETE SLAB
- 7 PARAPET (CIP CONCRETE)
- 8 CONCRETE STEPS
- 9 STEEL SASH WINDOW W/ AWNING FUNCTION (SEE WINDOW DTLS. 3/HA.6)
- 10 BEAD MOULDING CONCRETE WINDOW SILL (CIP)
- 11 LIGHTBOX SIGNAGE W/ METAL FRAME
- 12 CIP CONCRETE HORIZ. BANDING

- 13 CIP CONT. CONCRETE HORIZ. BANDING AT WINDOW HEAD
- 14 CONCRETE CIP DOOR TRIM (JAMB & HEAD)
- 15 OVERHEAD STEEL ROLL-UP DOOR
- 16 METAL VENT
- 17 TPO OR PVC ROOFING WP MEMBRANE
- 18 ROOF OVERFLOW DRAIN
- 19 ROOF DRAIN / 4"x3" METAL DOWNSPOUT
- 20 4'-3"x5'-3" CURB MOUNT DOME TYPE SKYLIGHT

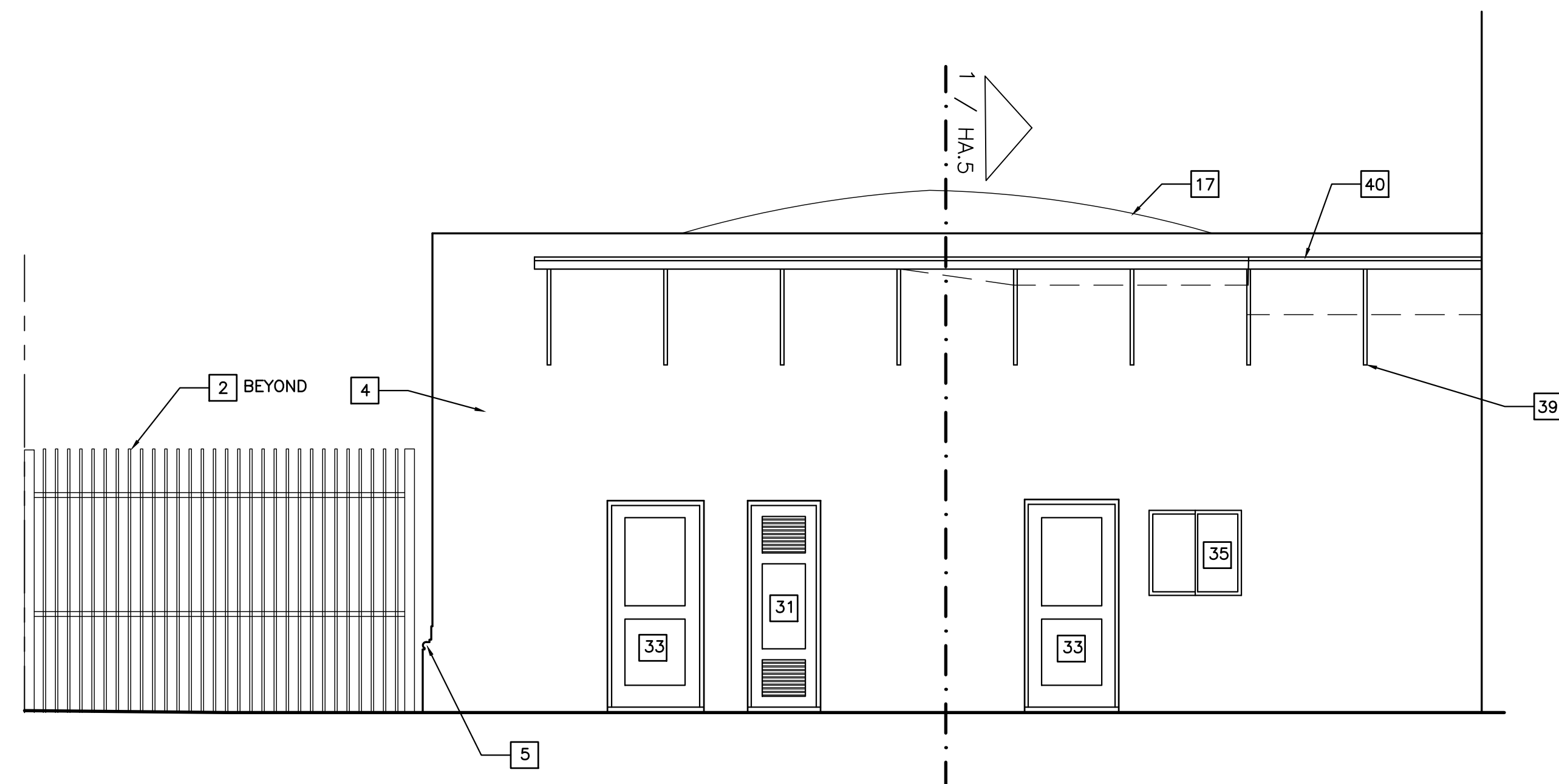
- 21 CONCRETE COLUMN
- 22 CONCRETE BEAM
- 23 DOUBLE HUNG WOOD WINDOW, SEE 3/HA.6
- 24 VINYL SLIDERS (MODIFIED METAL WINDOWS)
- 25 8x8 GLASS BLOCK
- 26 CARPET FLOOR
- 27 75 GAL. TANK TYPE WATERHEATER
- 28 3'-0"x6'-8" WOOD DOOR IN H.M. FRAME

- 29 4'-1"x4'-1" HORIZ. ALMN. SLIDER
- 30 5'-0"x4'-0" FIXED ALMN. FRAME WINDOW
- 31 2'-6"x7'-0" LOUVERED METAL DOOR
- 32 2'-6"x6'-8" DOOR
- 33 3'-0"x7'-0" WOOD DOOR IN H.M. FRAME
- 34 2x4 FRAMED WALL W/ GYP.BD. BOTH SIDES
- 35 3'-2"x2'-11" HORIZ. ALMN. SLIDER
- 36 2x4 ACOUSTIC LAY-IN CEILING (WITH PLASTER CEILING ABOVE)

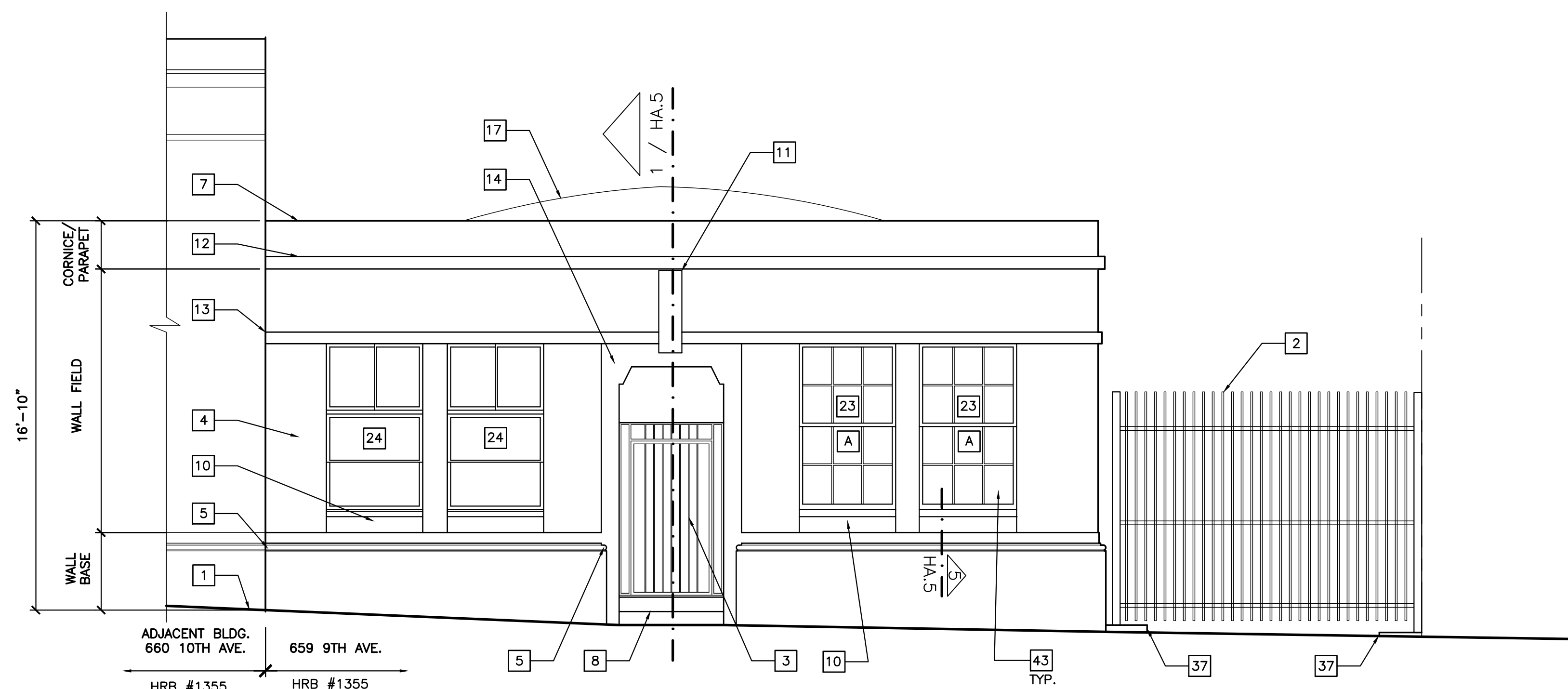
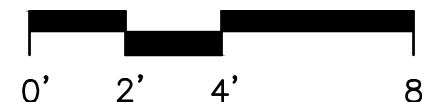
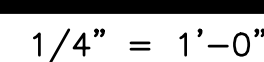
- 37 ELEVATED WALKWAY
- 38 SPRAY-ON ACOUSTIC CEILING
- 39 3 1/2" x 1 1/2" STEEL "C" CHANNEL
- 40 CORRUGATED METAL CANOPY
- 41 VINYL FLOORING
- 42 2x8 ROOF RAFTER
- 43 METAL MESH WINDOW GUARDS (NOT SHOWN IN DRAWING)
- 44 WALL RECESS/IN-FILLED WINDOW OPENING

- 45 MECHANICAL CURB MOUNT ROOF TOP UNIT
- 46 METAL ROOF ACCESS LADDER
- 47 TILE FLOOR
- 48 PLASTER SKIM COAT ON CONCRETE
- 49 ROOF TOP VENT
- 50 2X6 CEILING JOIST

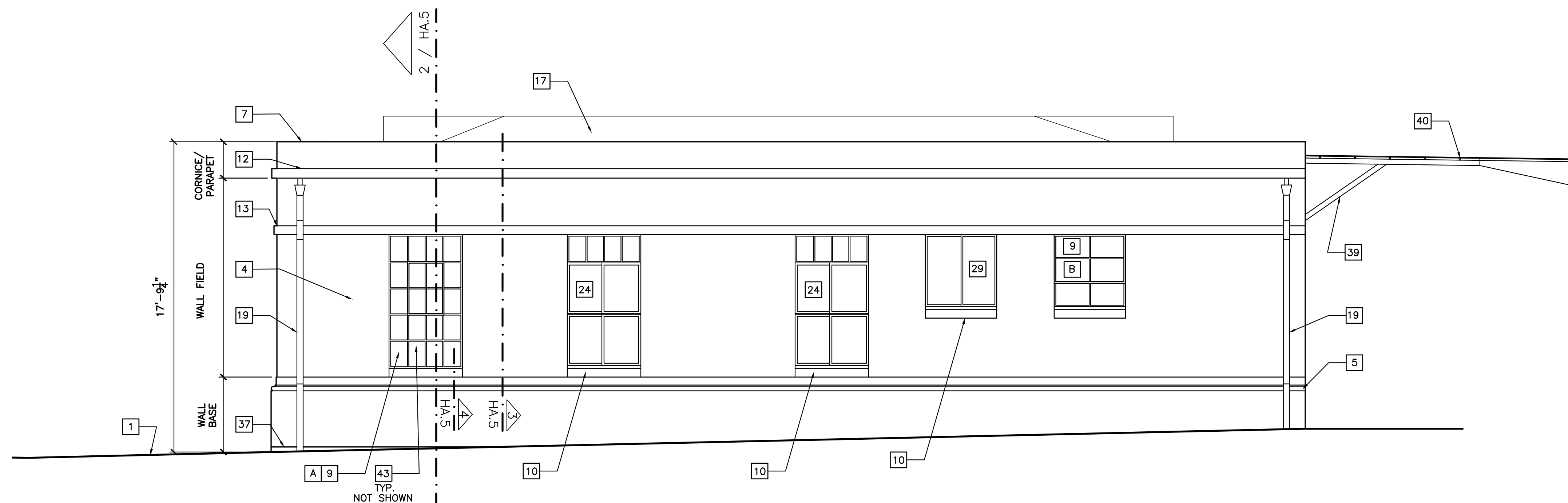
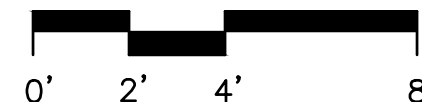
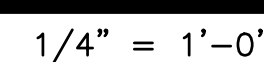




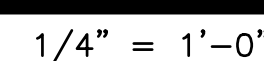
2 EAST ELEVATION  
HA.4



1 WEST ELEVATION  
HA.4



3 SOUTH ELEVATION  
HA.4



NOTE:

- A. THE 1946 BUILDING IS ORIGINAL EXCEPT FOR:
  - 1. THE WIRE MESH WINDOW GUARDS.
  - 1. WINDOWS AS NOTED.
- B. CIP : ABBR. CAST-IN-PLACE CONCRETE

## KEYNOTES

- 1 CONCRETE SIDEWALK / GRADE
- 2 METAL GATE
- 3 METAL SCREEN DOOR
- 4 BOARD FORMED (CAST-IN-PLACE)  
CONCRETE WALL, PAINTED
- 5 CONCRETE CAST-IN-PLACE (CIP)  
BASE/TORUS MOULDING DETAIL

- 6 CONCRETE SLAB
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- 10 BEAD MOULDING CONCRETE WINDOW SILL  
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- 19 ROOF DRAIN / 4"x3" METAL DOWNSPOUT
- 20 4'-3"x5'-3" CURB MOUNT DOME TYPE SKYLIGHT

- |           |  |
|-----------|--|
| <u>21</u> | CONCRETE COLUMN                        |
| <u>22</u> | CONCRETE BEAM                          |
| <u>23</u> | DOUBLE HUNG WOOD WINDOW, SEE 3/HA.6    |
| <u>24</u> | VINYL SLIDERS (MODIFIED METAL WINDOWS) |
| <u>25</u> | 8x8 GLASS BLOCK                        |
| <u>26</u> | CARPET FLOOR                           |
| <u>27</u> | 75 GAL. TANK TYPE WATERHEATER          |
| <u>28</u> | 3'-0"x6'-8" WOOD DOOR IN H.M. FRAME    |

- |    |  |
|----|--|
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| 33 | 3'-0"x7'-0" WOOD DOOR IN H.M. FRAME                      |
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| 36 | 2x4 ACOUSTIC LAY-IN CEILING (WITH PLASTER CEILING ABOVE) |

- |    |  |   |
|----|--|---|
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| 38 | SPRAY-ON ACOUSTIC CEILING                          | 4 |
| 39 | 3 1/2" x 1 1/2" STEEL "C" CHANNEL                  | 4 |
| 40 | CORRUGATED METAL CANOPY                            | 4 |
| 41 | VINYL FLOORING                                     | 4 |
| 42 | 2x8 ROOF RAFTER                                    | 5 |
| 43 | METAL MESH WINDOW GUARDS<br>(NOT SHOWN IN DRAWING) |   |
| 44 | WALL RECESS/IN-FILLED WINDOW OPENING               |   |

- |    |                                     |
|----|-------------------------------------|
| 45 | MECHANICAL CURB MOUNT ROOF TOP UNIT |
| 46 | METAL ROOF ACCESS LADDER            |
| 47 | TILE FLOOR                          |
| 48 | PLASTER SKIM COAT ON CONCRETE       |
| 49 | ROOF TOP VENT                       |
| 50 | 2X6 CEILING JOIST                   |

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ARCHITECT:

UNION ARCHITECTURE, INC.

JOHN H. EISENHART, ARCHITECT

EVA THORN, INTERIORS

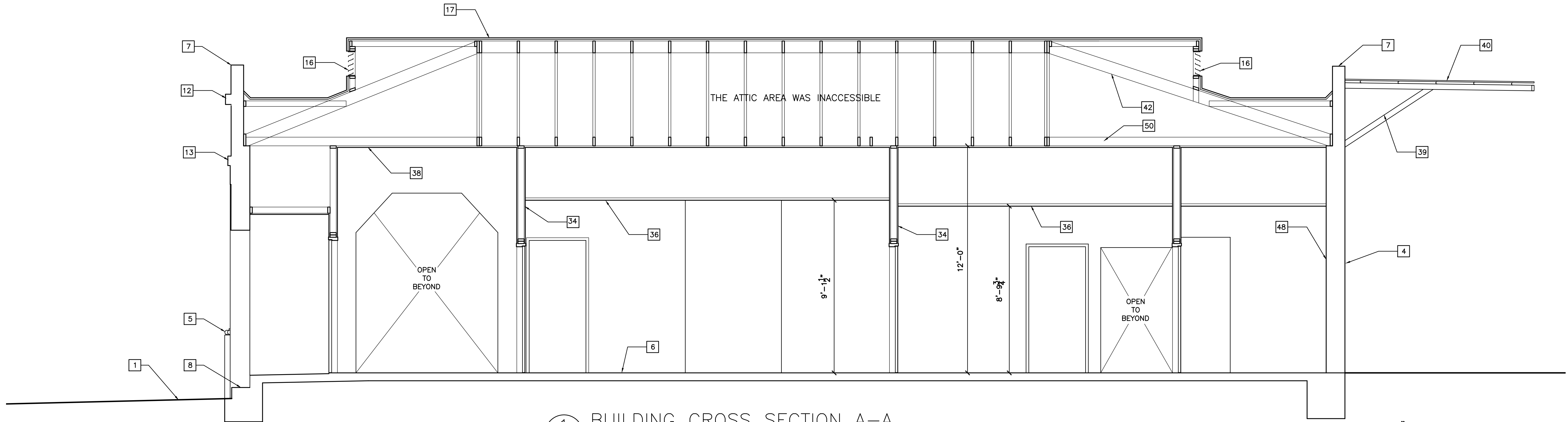
644 22ND STREET, SAN DIEGO, CA 92102

TELEPHONE: 619.788.2862 / WWW.UNIONARCH.COM

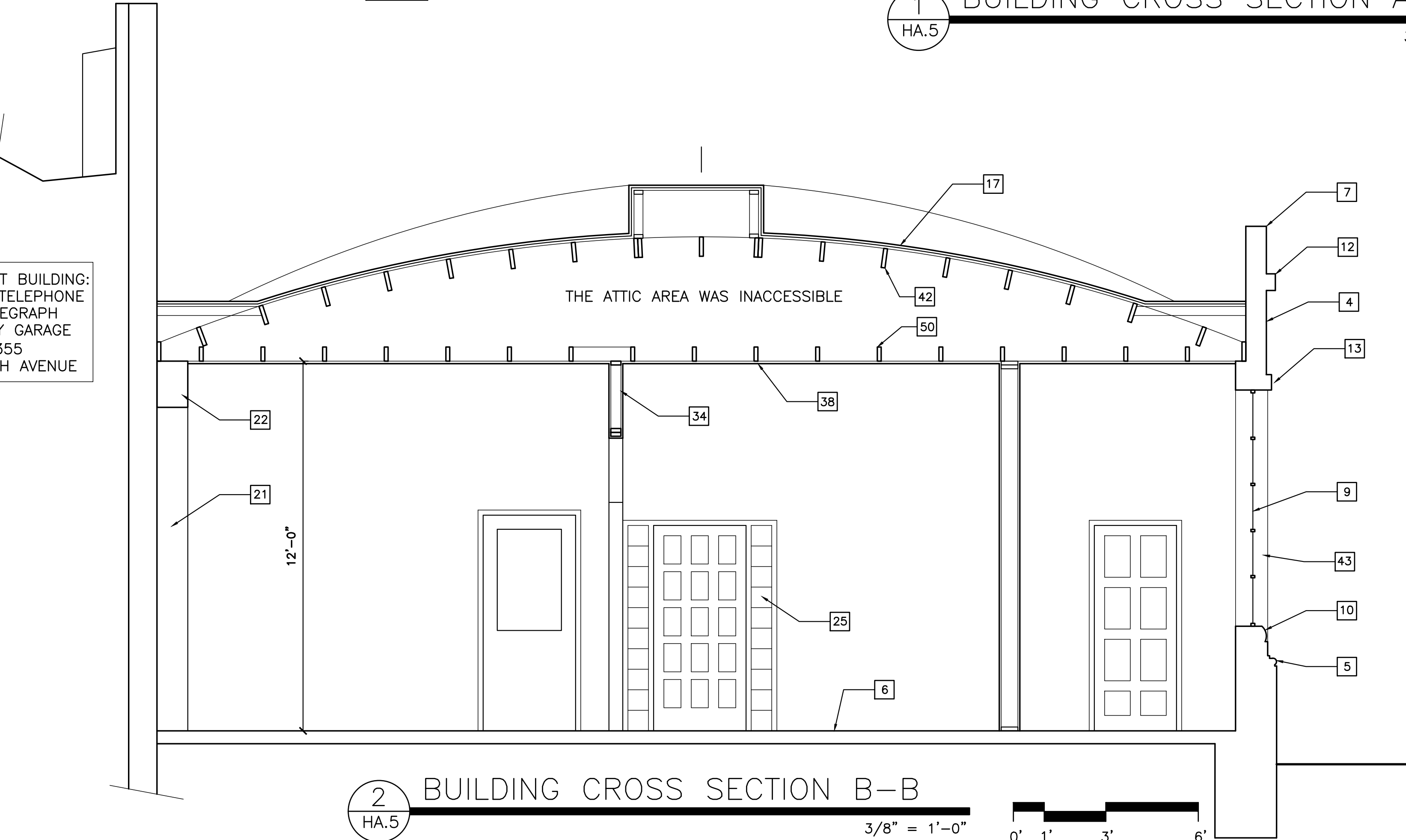
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## HA.4

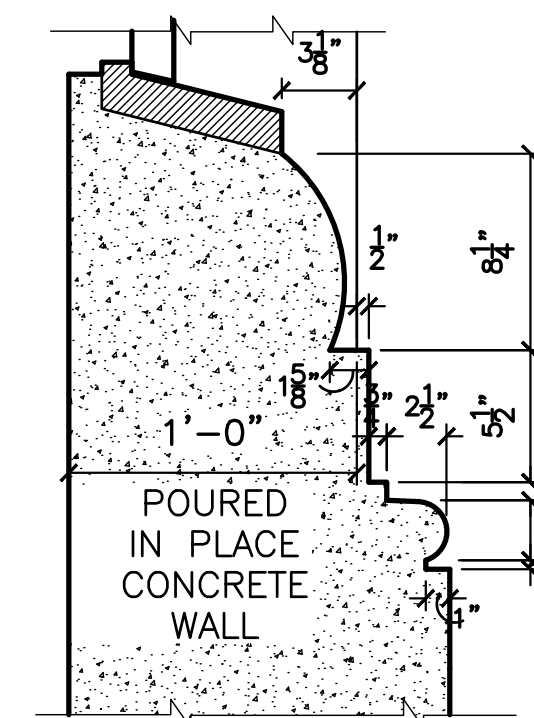
SHEET 4 OF 6



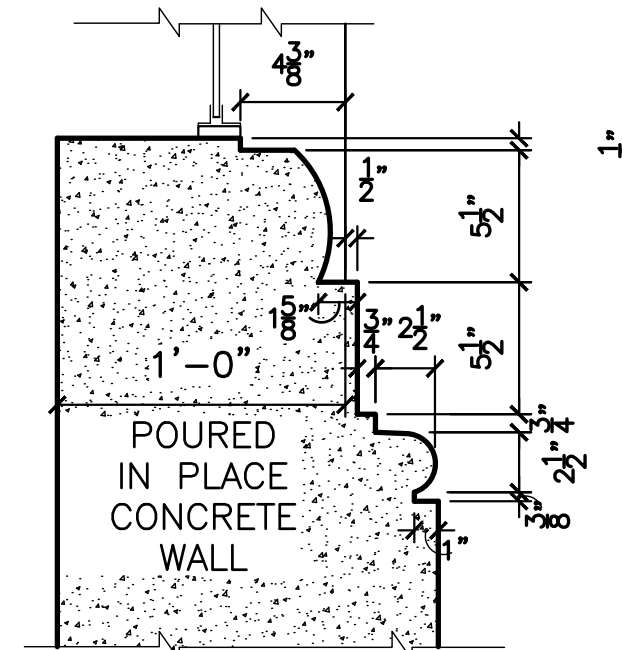
1 BUILDING CROSS SECTION A-A  
HA.5  
3/8" = 1'-0"



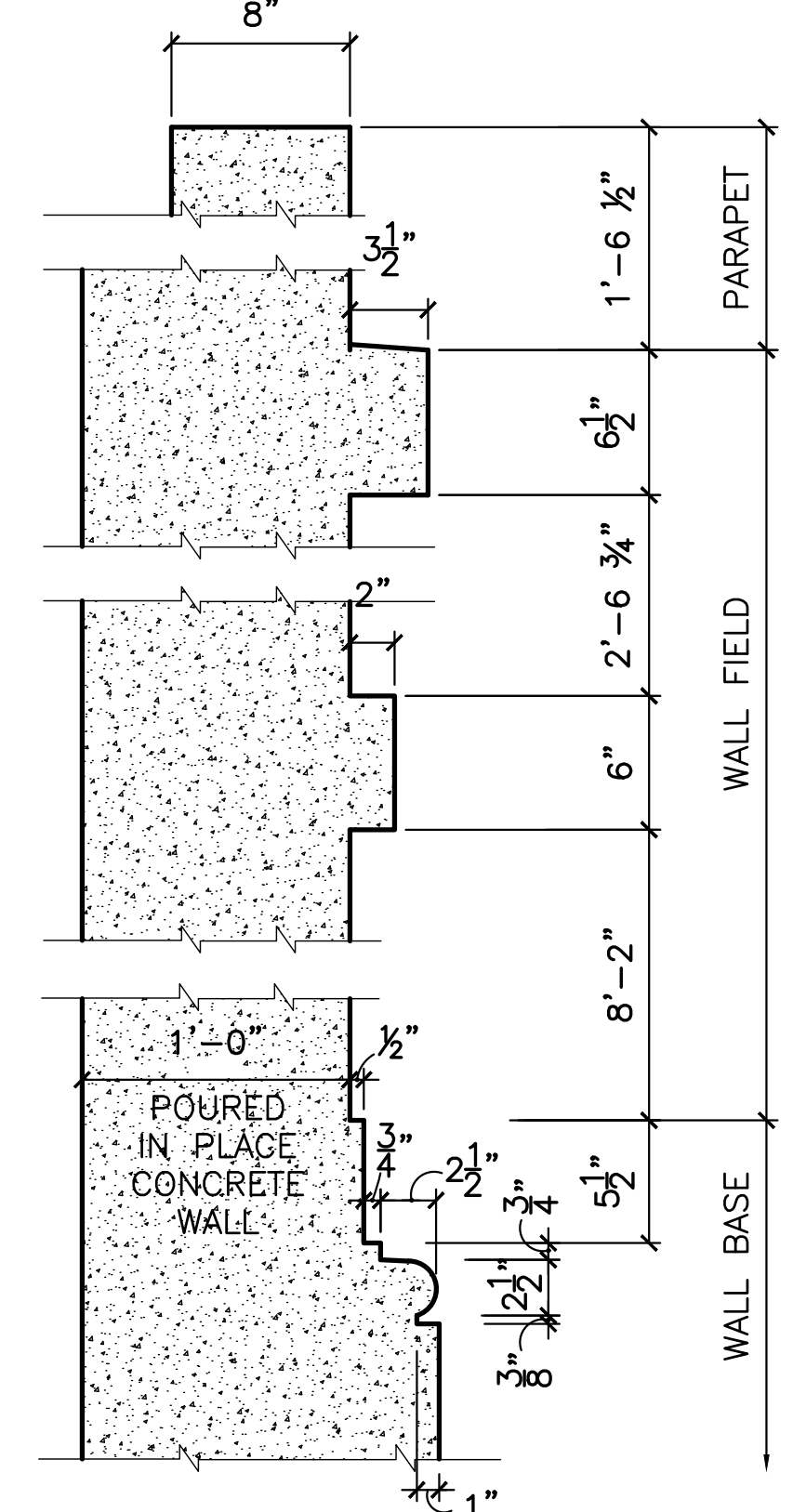
2 BUILDING CROSS SECTION B-B  
HA.5  
3/8" = 1'-0"



5 TYP. WINDOW SILL  
HA.5  
@ WEST ELEV.  
1 1/2" = 1'-0"



4 TYP. WINDOW SILL  
HA.5  
@ SOUTH ELEV.  
1 1/2" = 1'-0"



3 TYP. WALL DTL.  
HA.5  
1 1/2" = 1'-0"

NOTE:

- A. THE 1946 BUILDING IS ORIGINAL EXCEPT FOR:
- THE WIRE MESH WINDOW GUARDS.
  - WINDOWS AS NOTED.
- B. CIP : ABBR. CAST-IN-PLACE CONCRETE

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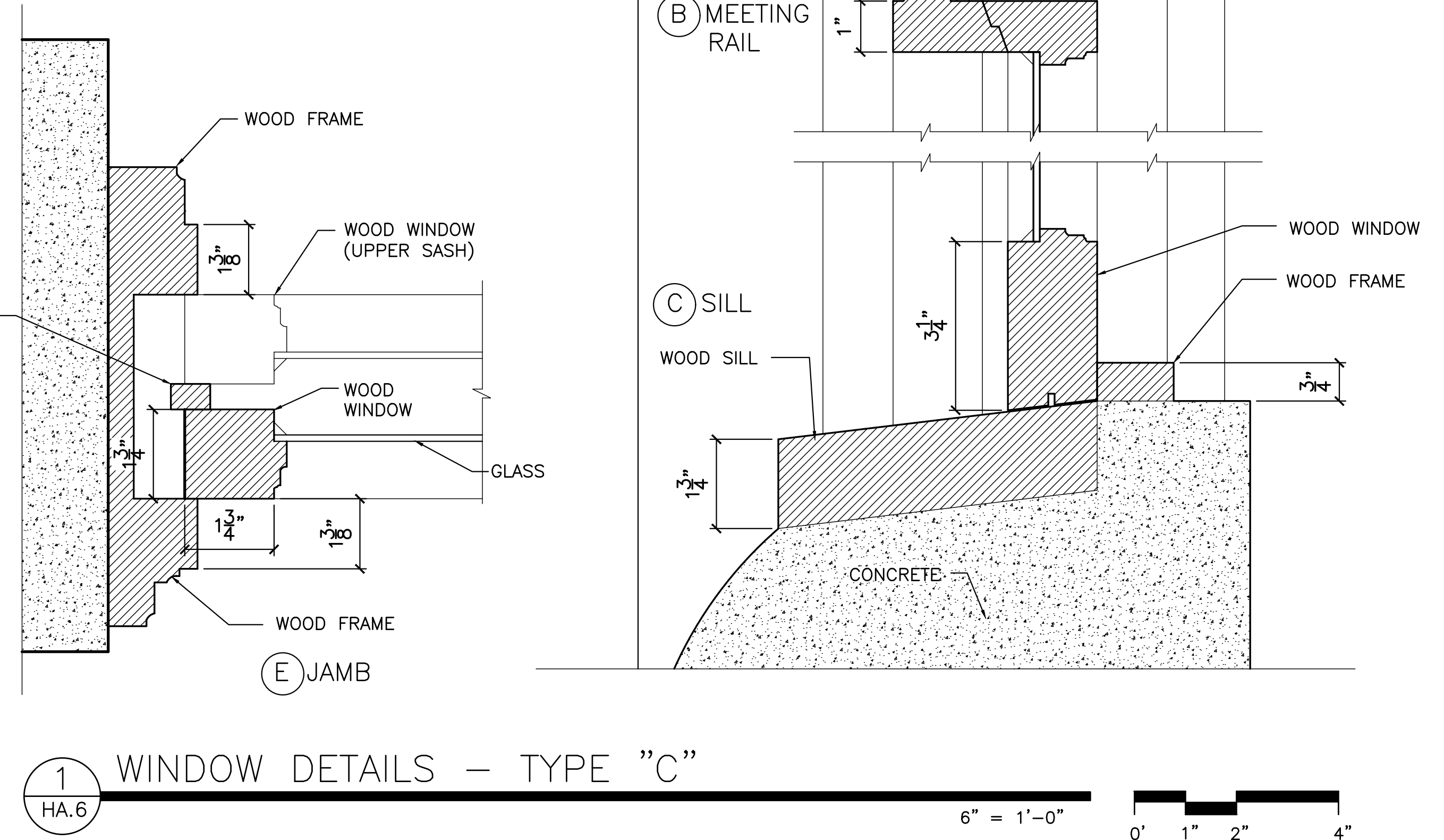
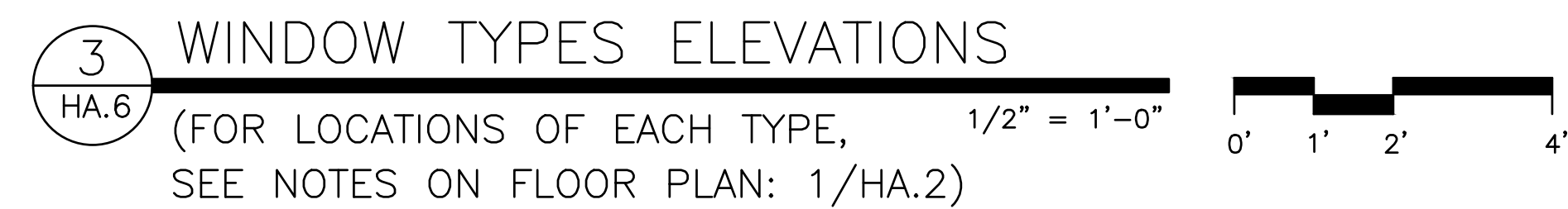
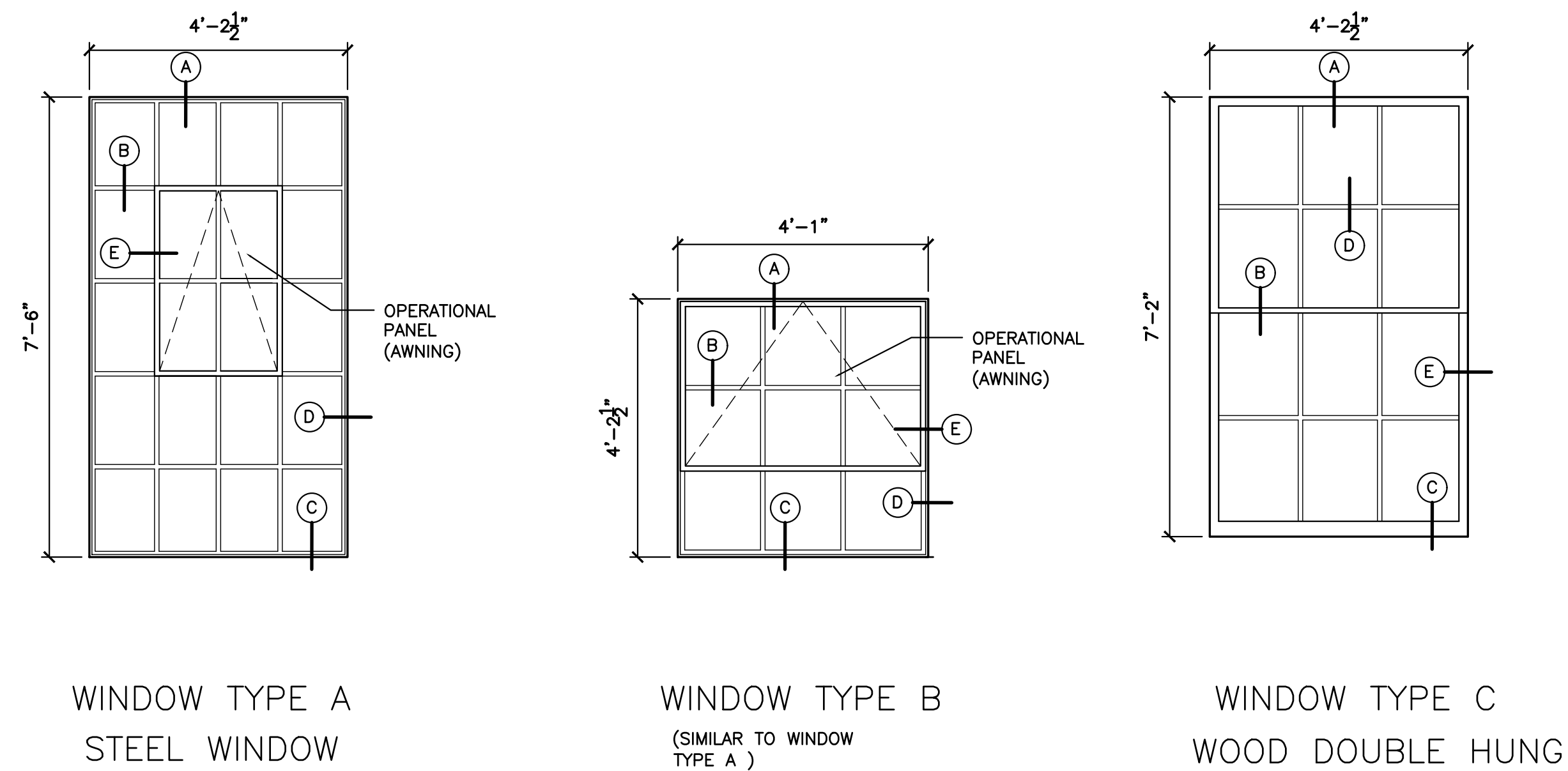
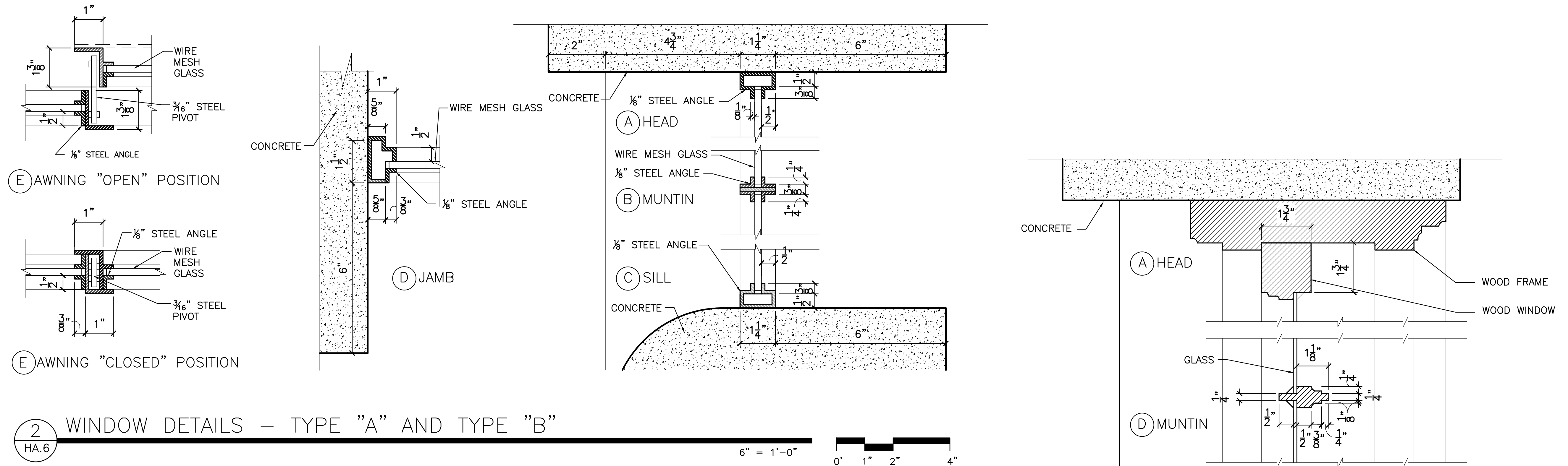
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- ROOF TOP VENT
- 2X6 CEILING JOIST





THE PACIFIC TELEPHONE AND TELEGRAPH COMPANY GARAGE (HRB #1355)  
660 TENTH AVENUE, SAN DIEGO, CA 92101  
PERIOD OF SIGNIFICANCE 1932–1956 — HISTORIC AMERICAN BUILDING SURVEY

HISTORIC  
AMERICAN  
BUILDING  
SURVEY



6 INTERIOR CEILING  
HA.1 PHOTO, JULY 2019



5 NORTH ELEVATION  
HA.1 PHOTO, JULY 2019



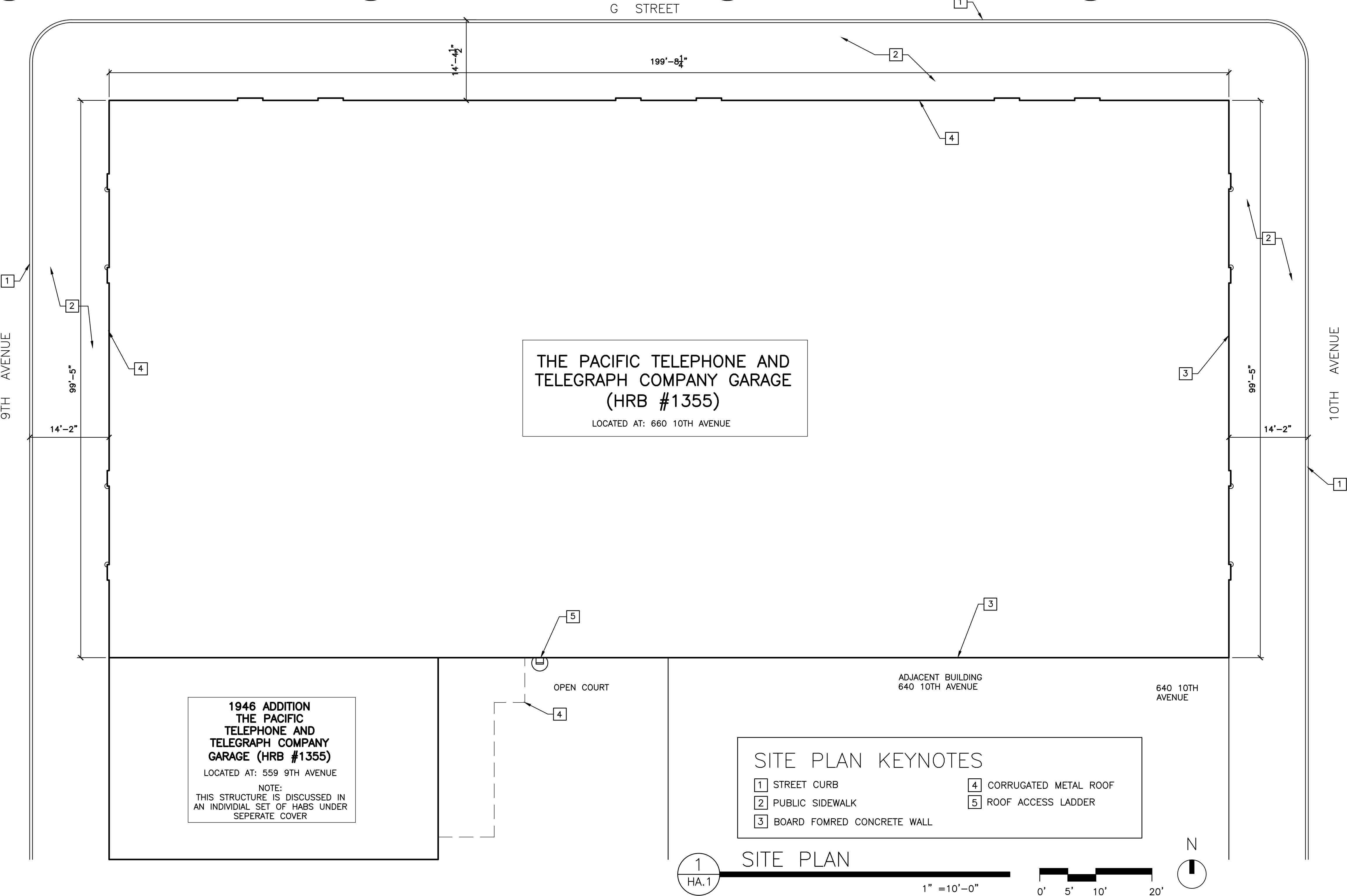
4 WEST ELEVATION  
HA.1 PHOTO, JULY 2019



3 EAST ELEVATION  
HA.1 PHOTO, JULY 2019



2 SOUTH ELEVATION  
HA.1 PHOTO, JULY 2019



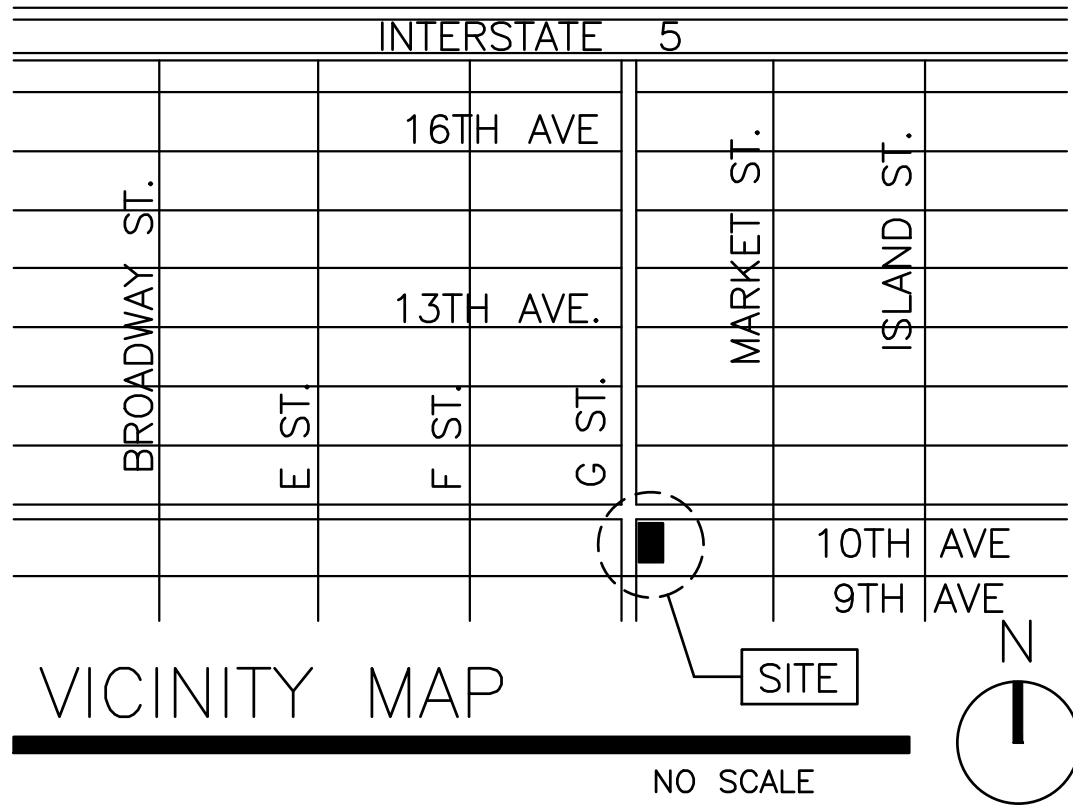
DESCRIPTION OF RESOURCE

THE DESIGNATED RESOURCE (HRB# 1355) WITH A PERIOD OF SIGNIFICANCE OF 1932 – 1956 IS LOCATED AT 660 10TH AVE. AND 659 9TH AVE., SAN DIEGO, CA 92101 (APN:535–136–00–00). THE RESOURCE CONSISTS OF A ONE STORY COMMERCIAL BUILDING WITH A RECTANGULAR FOOTPRINT OF APPROX. 100' x 200', CONSTRUCTED IN 1932 IN THE ART DECO STYLE AND A ONE STORY ADDITION, SIMILAR IN STYLE, BUILT IN 1946 DIRECTLY ADJACENT TO THE SOUTH. THE ADDITION MEASURES APPROX. 36' x 60'.

THE CITY OF SAN DIEGO HISTORICAL RESOURCES BOARD BASED ITS DESIGNATION OF THE RESOURCE ON THE FOLLOWING FINDINGS:

CRITERION A: THE PROPERTY IS HISTORICALLY SIGNIFIGANT AS A SPECIAL ELEMENT OF THE CITY'S ECONOMIC DEVELOPMENT, REVEALING THE INCREASING IMPORTANCE OF THE AUTOMOBILE AS AMERICA'S FAVORED METHOD OF PRIVATE TRANSPORTATION FROM 1900–1956, THE BUILDING RETAINS INTEGRITY TO ITS 1932–1956 PERIOD OF SIGNIFICANCE. SPECIFICALLY, THE RESOURCE WHICH WAS CONSTRUCTED IN 1932 FOR USE AS A GARAGE, CONTINUES TO CONVEY THE SIGNIFICANT CHARACTER DEFINING FEATURES OF AUTOMOBILE-RELATED DEVELOPMENT AS INDENTIFIED IN THE EAST VILLAGE HISTORICAL CONTEXT STATEMENT, INCLUDING A SINGLE STORY, WAREHOUSE-LIKE MASSING; LIMITED ORNAMENTATION, RAISED VERTICAL PROJECTIONS AND NUMEROUS GARAGE DOORS.

CRITERION C: THE PROPERTY IS HISTORICALLY SIGNIFICANT THROUGH THE RETENTION OF CHARACTER DEFINING FEATURES OF THE ART DECO STYLE AND RETAINS A GOOD LEVEL OF ARCHITECTURAL INTEGRITY FROM ITS 1932 PERIOD OF SIGNIFICANCE. SPECIFICALLY, THE RESOURCE EXHIBITS SMOOTH WALL SURFACES, A FLAT ROOF, VERTICAL PROJECTIONS ABOVE THE ROOFLINE, FLUTED PILASTERS, GEOMETRIC DETAILING AND METAL WINDOWS.



INDEX OF SHEETS

- HA 1 SITE PLAN, PHOTOS, VICINITY MAP
- HA 2 FLOOR PLAN
- HA 3 REFLECTED CEILING PLAN
- HA 4 ROOF PLAN
- HA 5 WEST AND EAST ELEVATION
- HA 6 NORTH ELEVATION
- HA 7 SOUTH ELEVATION
- HA 8 BLDG. SECTION A–A
- HA 9 BLDG. SECTION B–B, WINDOW DTLS.
- HA 10 WALL SECTIONS

PROJECT :  
THE PACIFIC TELEPHONE & TELEGRAPH CO. GARAGE  
HRB #1355  
660 10TH AVENUE, SAN DIEGO, CA 92101  
CLIENT :  
CISTERRA PARTNERS, LLC  
3580 CARMEL MOUNTAIN ROAD, STE. # 460, SAN DIEGO, CA 92130

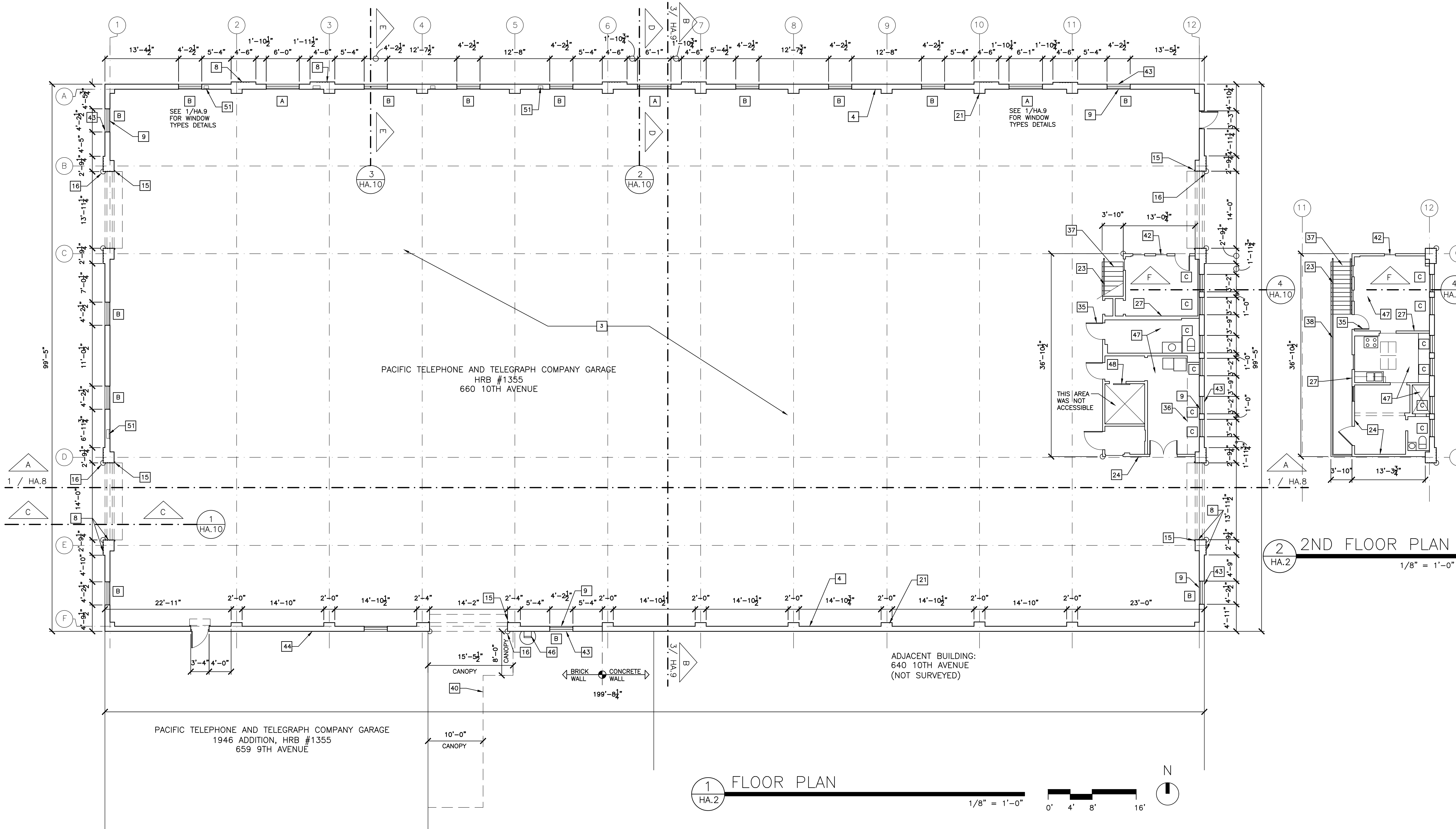
ARCHITECT:  
UNION ARCHITECTURE, INC.  
JOHN H. EISENHART, ARCHITECT  
EVA THORN, INTERIORS  
344 22ND STREET, SAN DIEGO, CA 92102  
TELEPHONE 619.786.2862 / WWW.UNIONARCH.COM

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DATE: 08.30.2019  
REV: 03.20.2020

HA.1  
SHEET 1 OF 10





## NOTE:

- A. THE 1932 BUILDING IS ORIGINAL EXCEPT FOR: THE METAL OVERHEAD ROLL-UP DOORS.
1. THE WIRE MESH WINDOW GUARDS
- B. CIP : ABBR. CAST-IN PLACE CONCRETE

## KEYNOTES

- 1 CONCRETE SIDEWALK / GRADE
- 2 KEYSTONE TYPE DECORATIVE CAST-IN PLACE CONCRETE MOULDING
- 3 CONCRETE FLOOR
- 4 BOARD FORMED (CAST-IN-PLACE) CONCRETE WALL, PAINTED
- 5 CONCRETE CAST-IN-PLACE (CIP) BASE/TORUS MOULDING DETAIL, SEE 5/HA.10

- 6 CONCRETE (CIP) MOULDED CORNICE BAND
- 7 PARAPET (CIP CONCRETE)
- 8 "FLUTING" DETAIL AT PIER (CIP CONCRETE)
- 9 STEEL SASH WINDOWS W/ AWNING FUNCTION (SEE DTLs. 1 + 2/HA.9)
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- 11 RECTANGULAR BLOCK CIP CONCRETE
- 12 CIP VERTICAL CONCRETE STRIP DECORATIVE RELIEF (PROTRUDING FROM WALL PLANE)

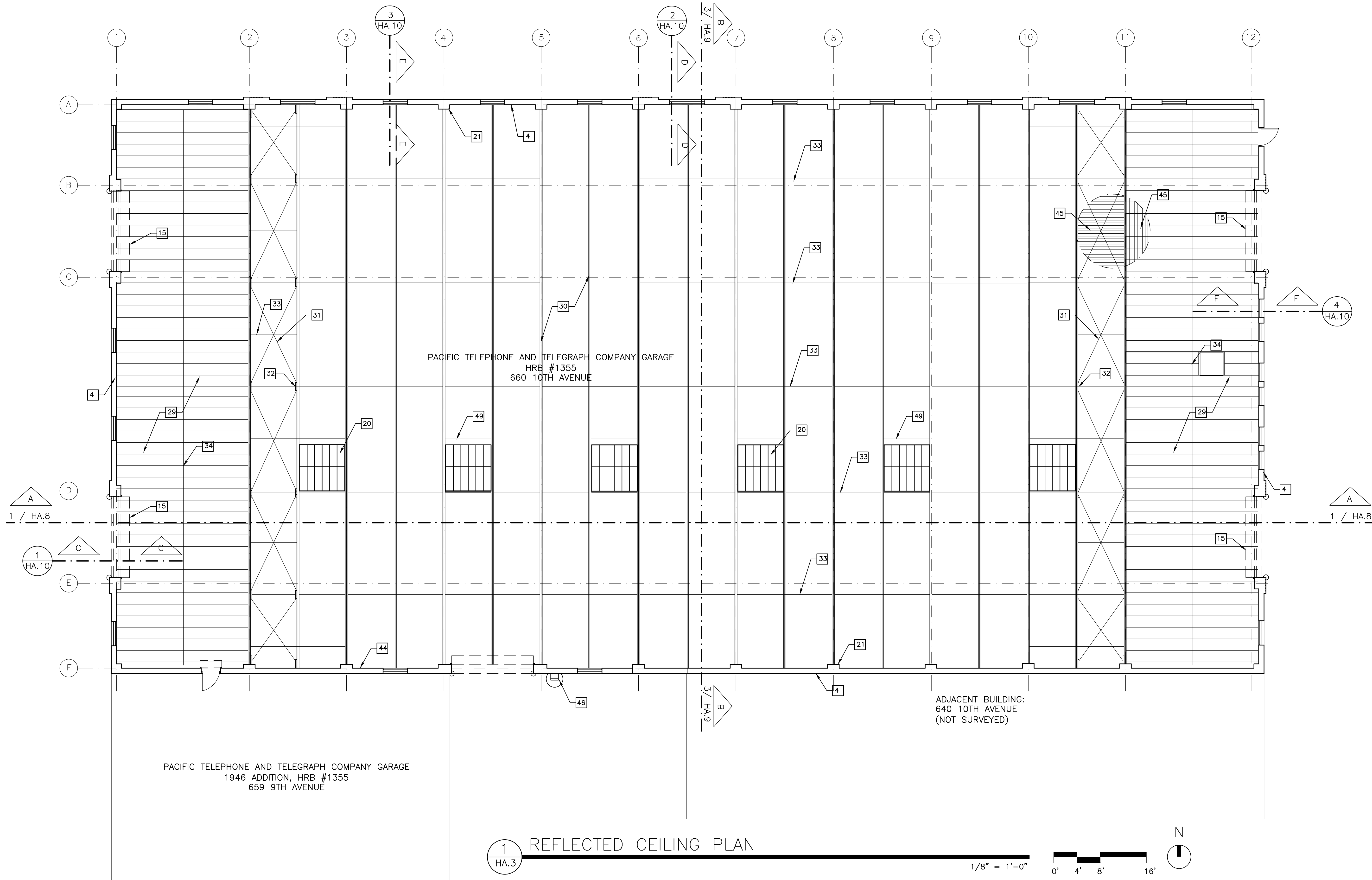
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- 19 ROOF DRAIN / DOWNSPOUT
- 20 METAL SASH SKYLIGHT

- 21 CONCRETE CIP PIER
- 22 CONCRETE CIP BEAM / CORBEL
- 23 METAL HANDRAIL
- 24 2X4 STUD WALL @ 16" O.C.
- 25 2x CEILING JOIST @ 24" O.C.
- 26 2X10 FLOOR JOIST @ 16" O.C.
- 27 PLASTER FINISH WALL
- 28 7/8" WOOD SHEATHING

- 29 2x14 ROOF RAFTERS @2'-0" O.C.
- 30 (2) 3X14 WOODEN "BOW" TRUSSES
- 31 3/4" STEEL TIE RODS
- 32 STEEL ANGLE
- 33 3X8 ROOF RAFTER
- 34 2X6 WOOD CROSS BRIDGING
- 35 WOOD DOOR
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- 47 TILE FLOORING
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- 49 2X8 ROOF RAFTER
- 50 CIP CONCRETE HORIZ. BANDING
- 51 RECESSED WALL CAVITY W/ UTILITY CONNECTION (APPROX. 7.5" DEEP)



1 REFLECTED CEILING PLAN  
HA.3

1/8" = 1'-0"

0' 4' 8' 16'



NOTE:

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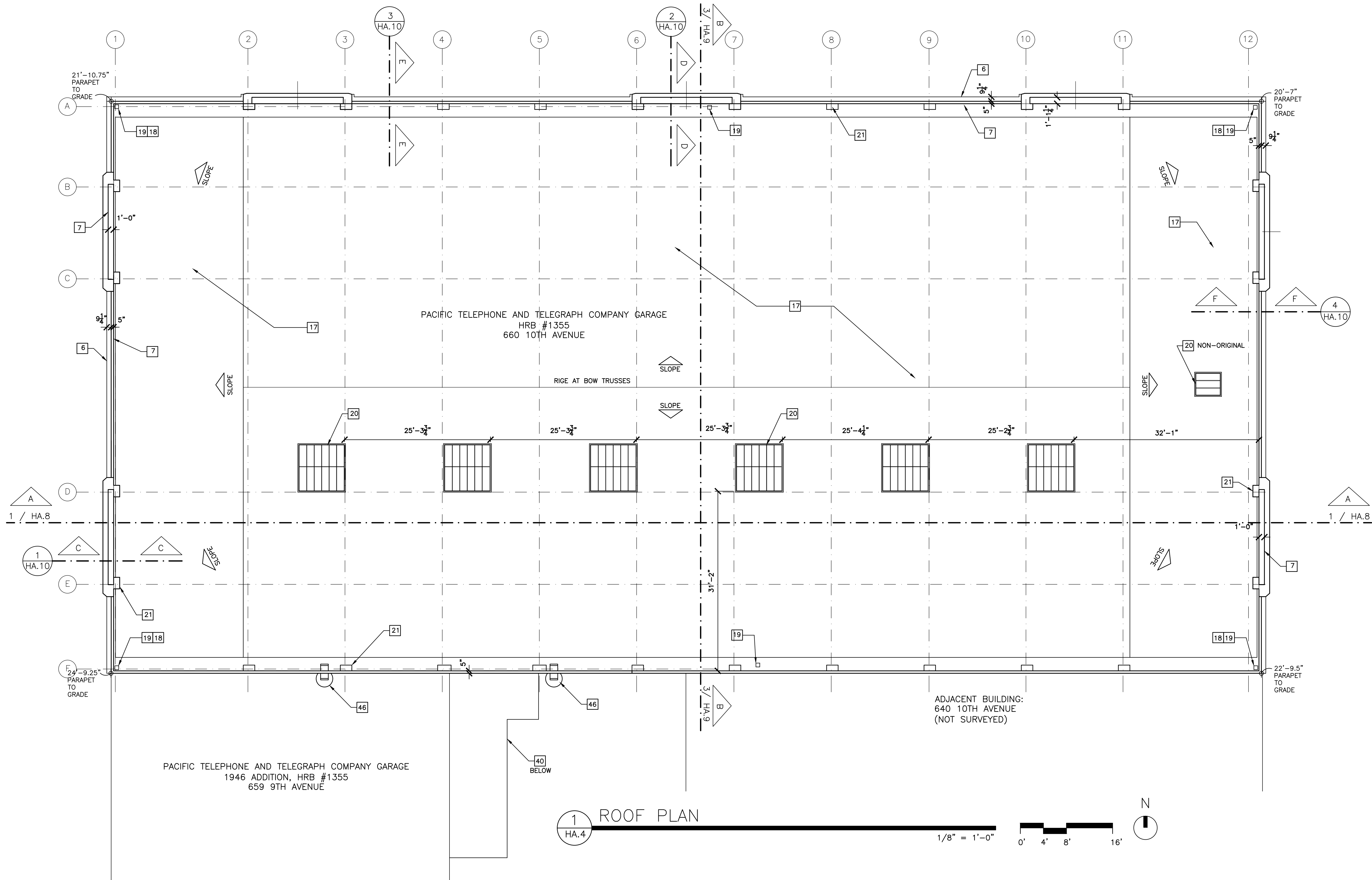
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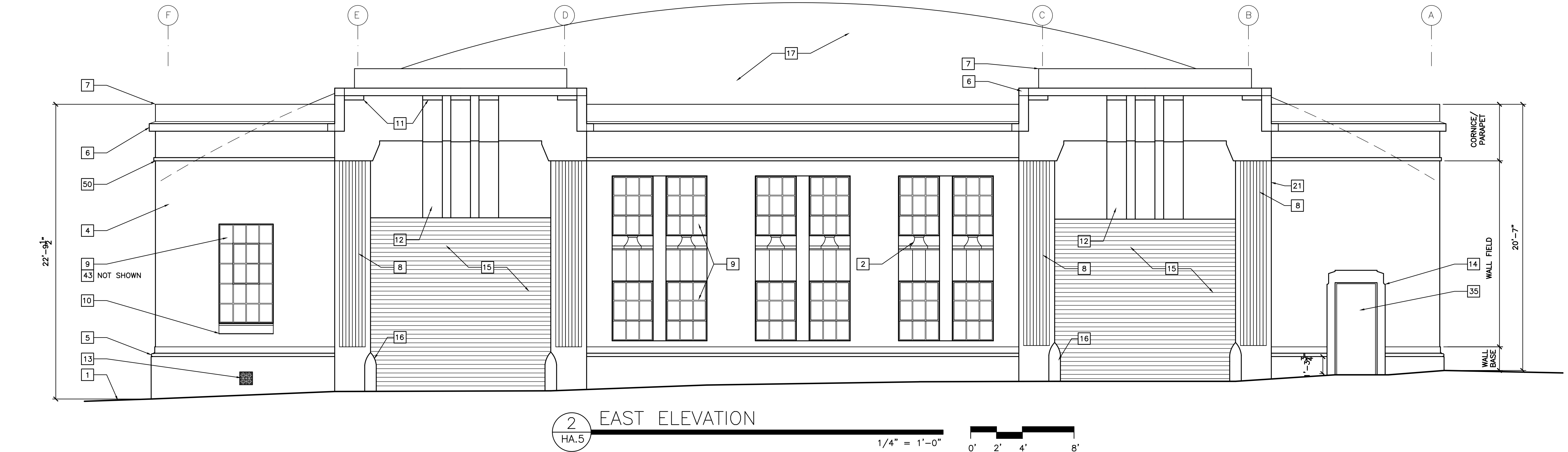
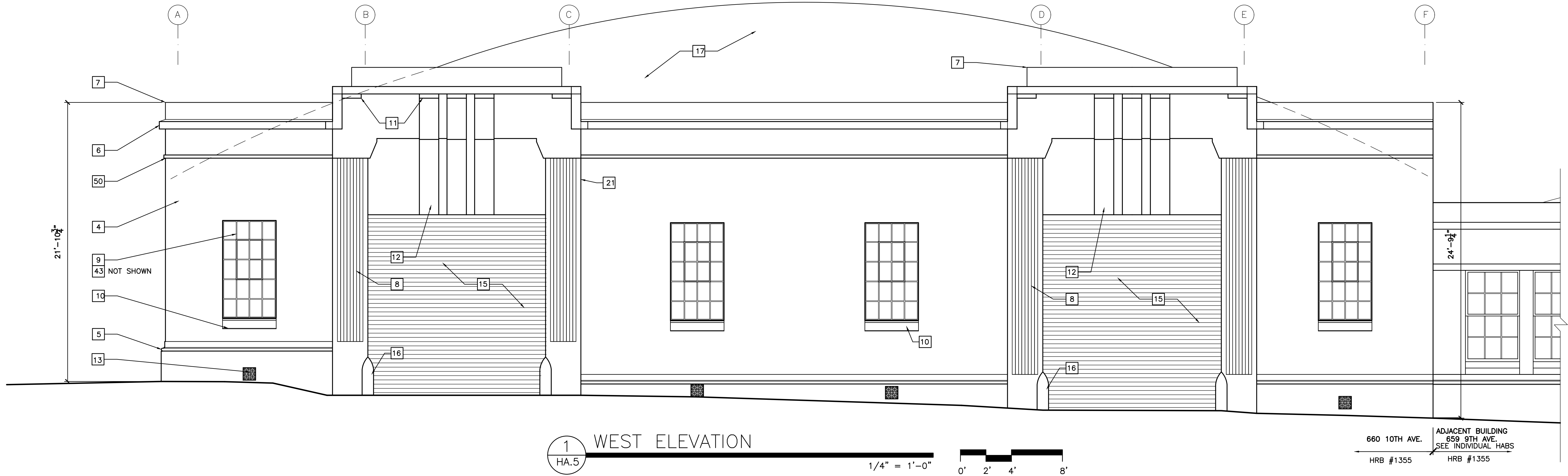
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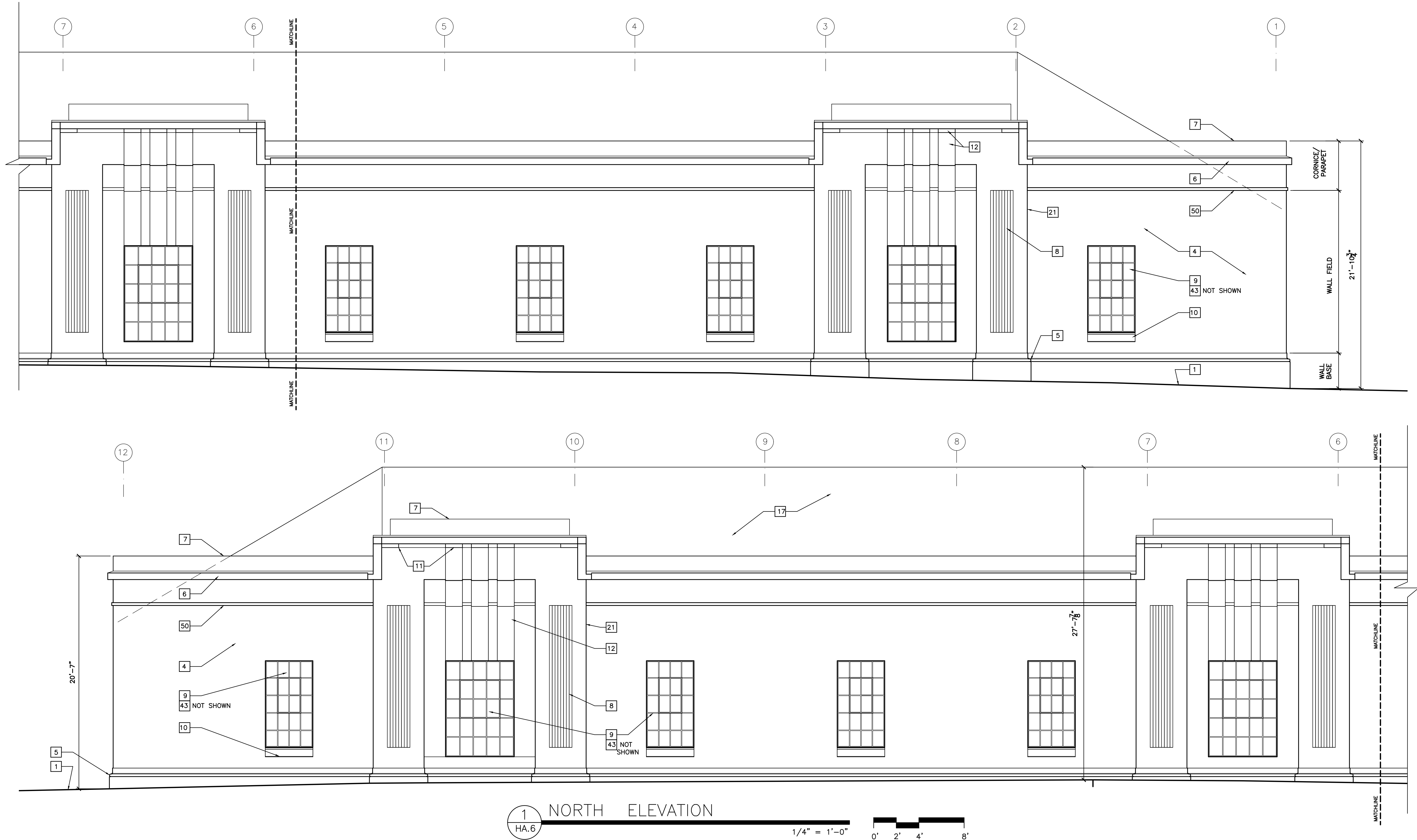
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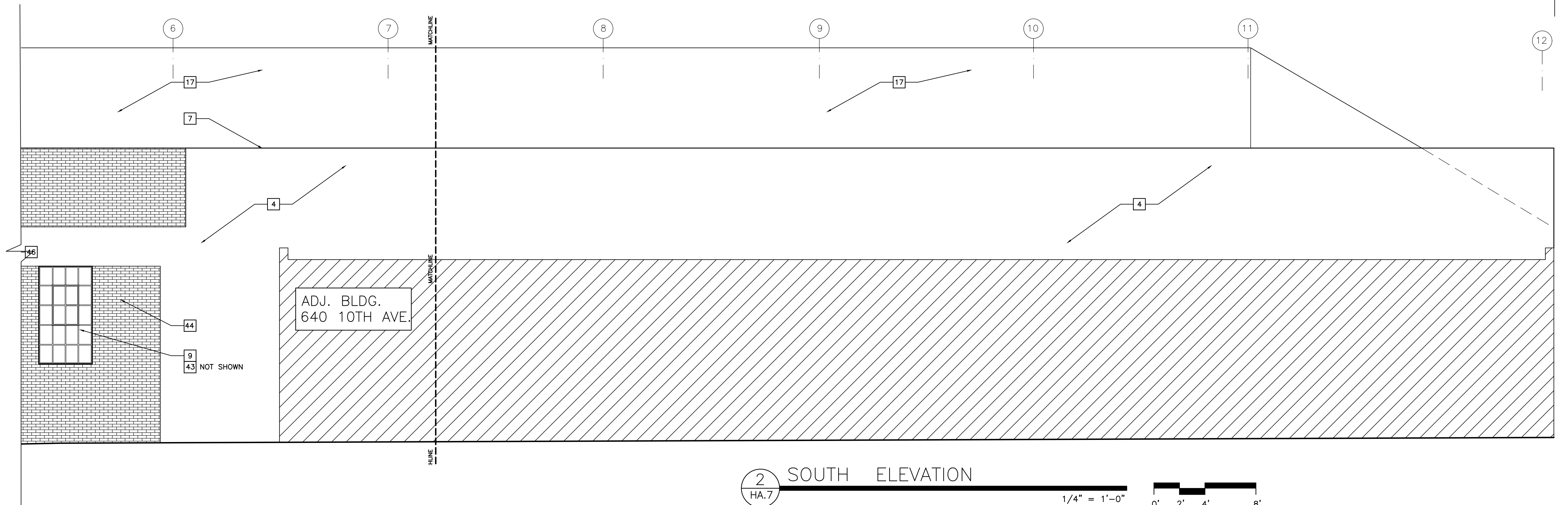
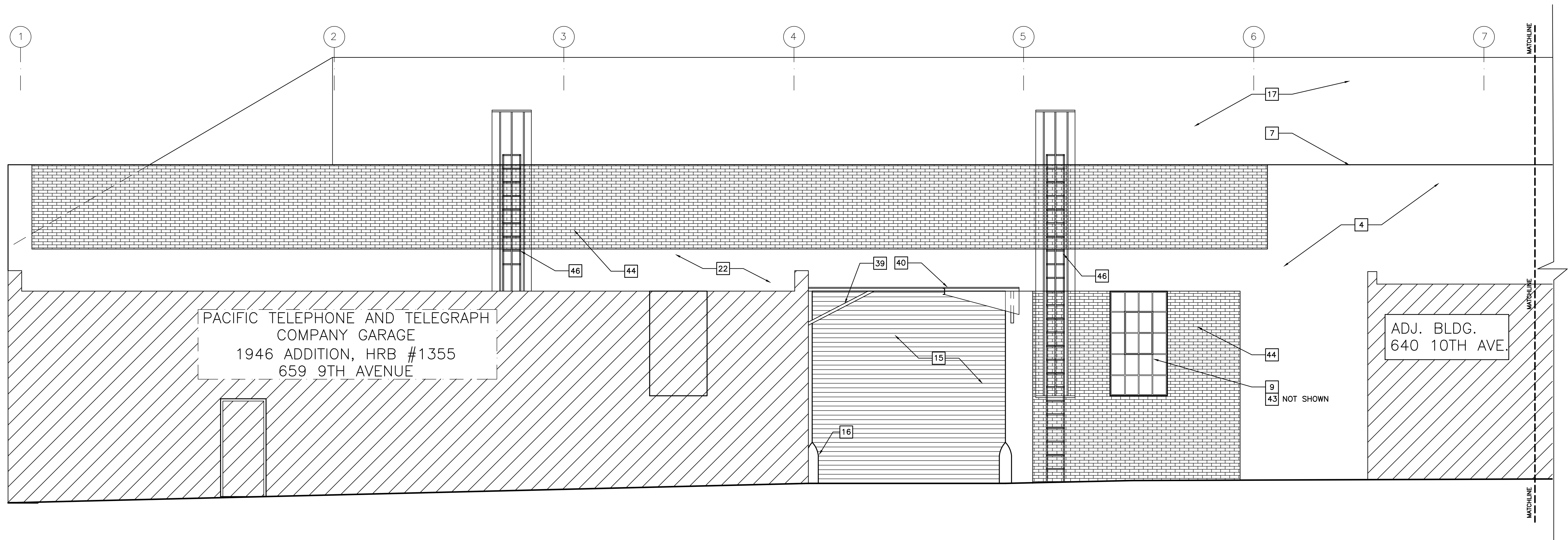


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|--|---|---|--------------------------------|---|---------------------------------------|---|
| 1 CONCRETE SIDEWALK / GRADE  | 6 CONCRETE (CIP) MOULDED CORNICE BAND   | 13 DECORATIVE METAL VENT                | 21 CONCRETE CIP PIER           | 29 2x14 ROOF RAFTERS @2'-0" O.C.                            | 37 1X WOODEN TREAD AT STAIR           | 45 1X6 DOUGLAS FIR SHEATHING                                      |
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| 3 CONCRETE FLOOR   | 8 "FLUTING" DETAIL AT PIER (CIP CONCRETE)                                     | 15 OVERHEAD STEEL ROLL-UP DOOR          | 23 METAL HANDRAIL              | 31 3/4" STEEL TIE RODS                                      | 39 STEEL "C" CHANNEL                  | 47 TILE FLOORING  |
| 4 BOARD FORMED (CAST-IN-PLACE) CONCRETE WALL, PAINTED                  | 9 STEEL SASH WINDOWS W/ AWNING FUNCTION (SEE DTLs. 1 + 2/HA.9)                | 16 "BULLET" BOLLARD                     | 24 2X4 STUD WALL @ 16" O.C.    | 32 STEEL ANGLE  | 40 CORRUGATED METAL CANOPY (OVERHANG) | 48 STEEL DOOR   |
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|  |   | 20 METAL SASH SKYLIGHT                  | 28 7/8" WOOD SHEATHING         | 36 2x4 ACOUSTIC LAY IN CEILING (WITH PLASTER CEILING ABOVE) | 44 BRICK                              |   |



2 SOUTH ELEVATION  
HA.7

1/4" = 1'-0"  
0' 2' 4' 8'

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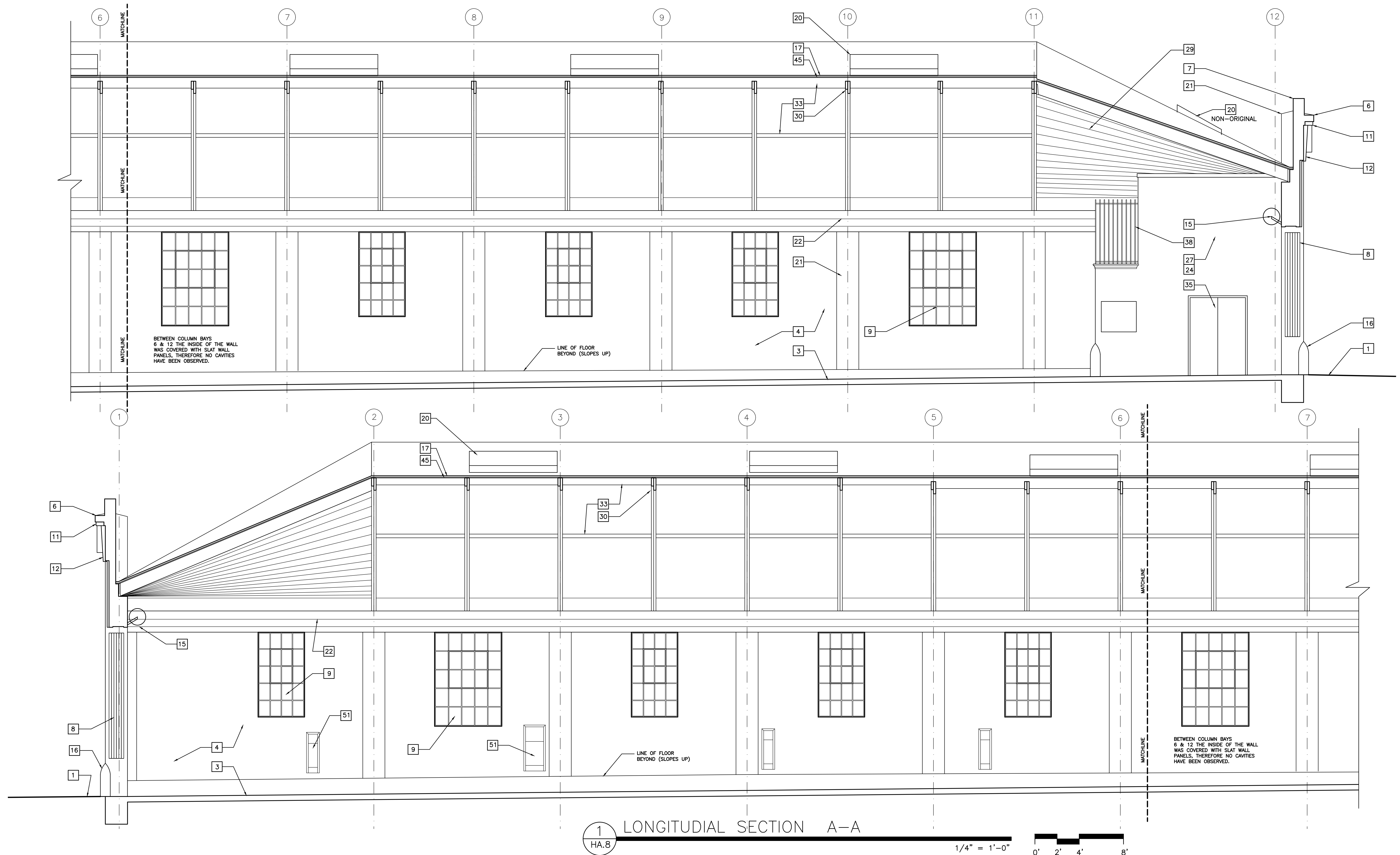
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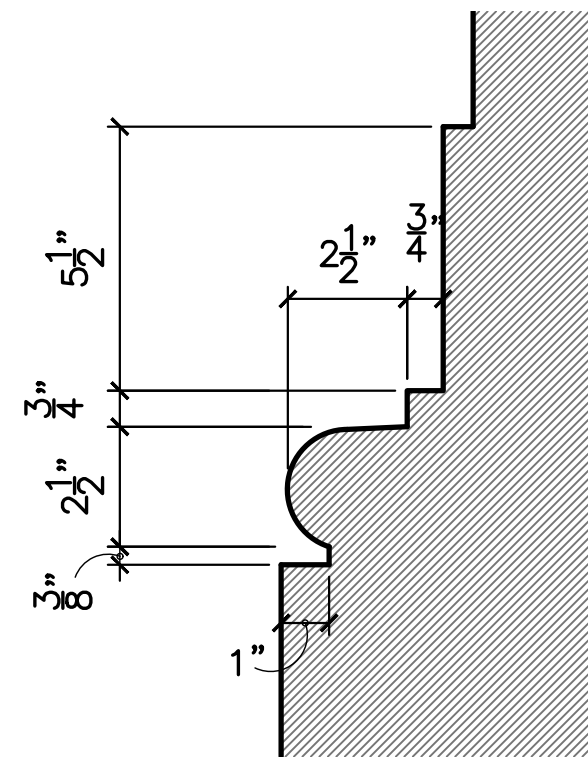
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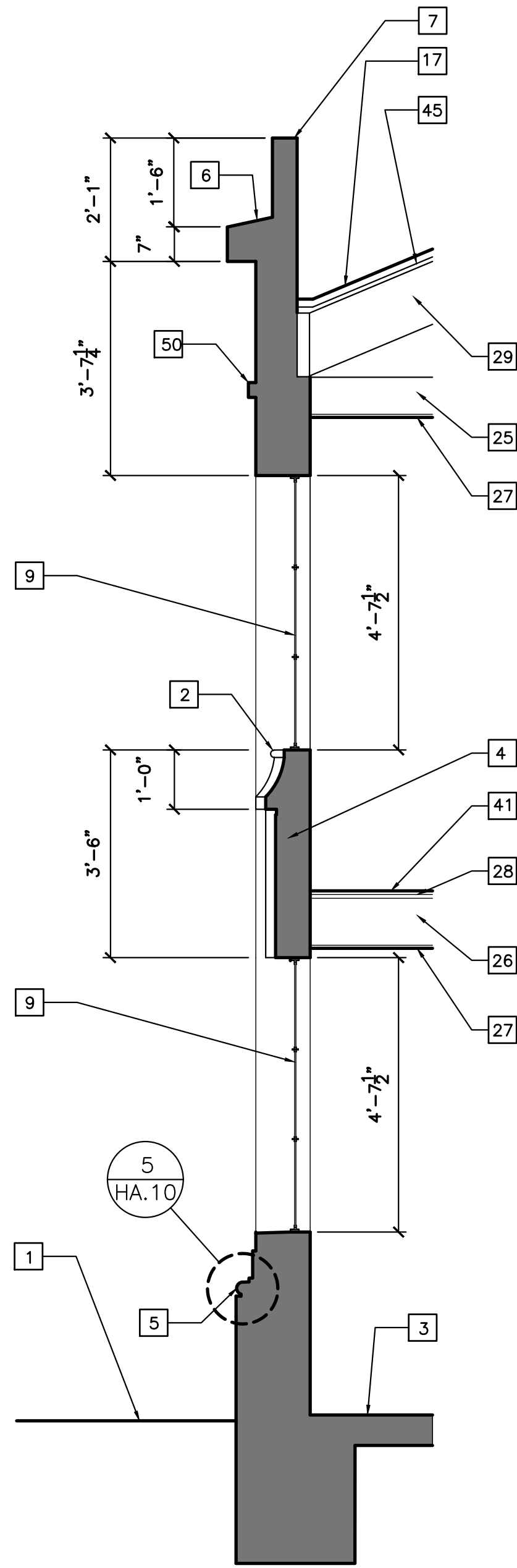
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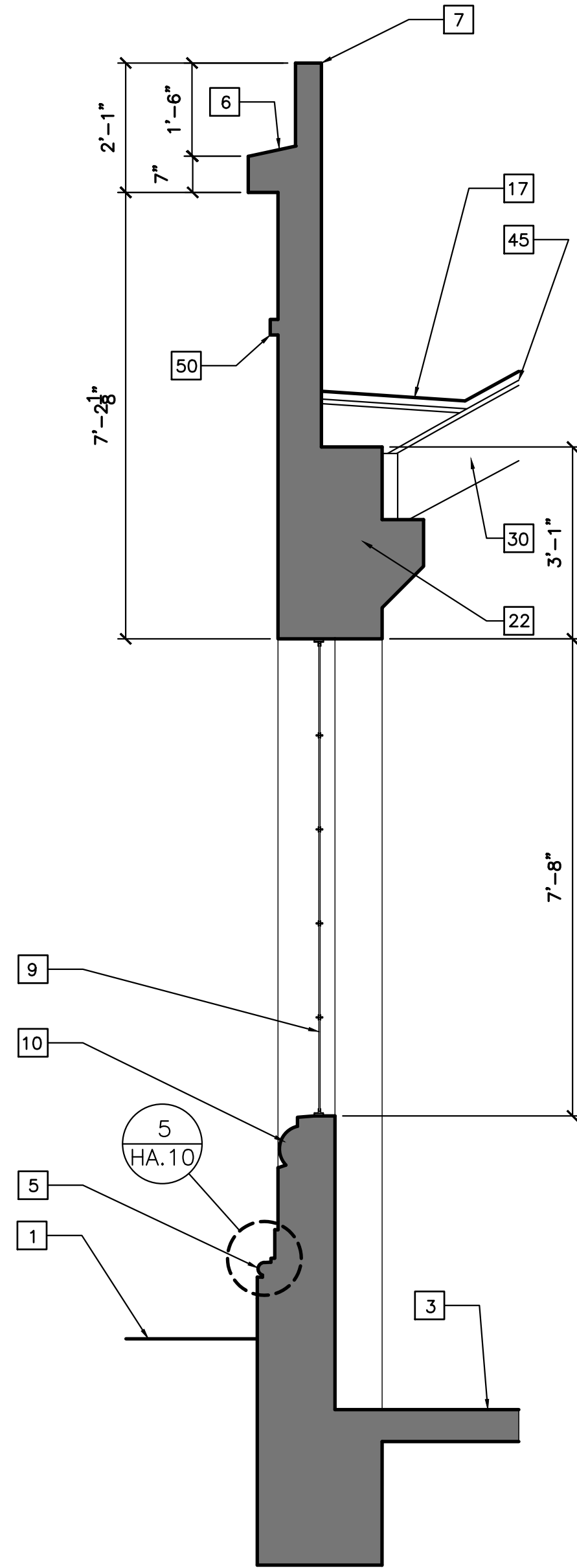




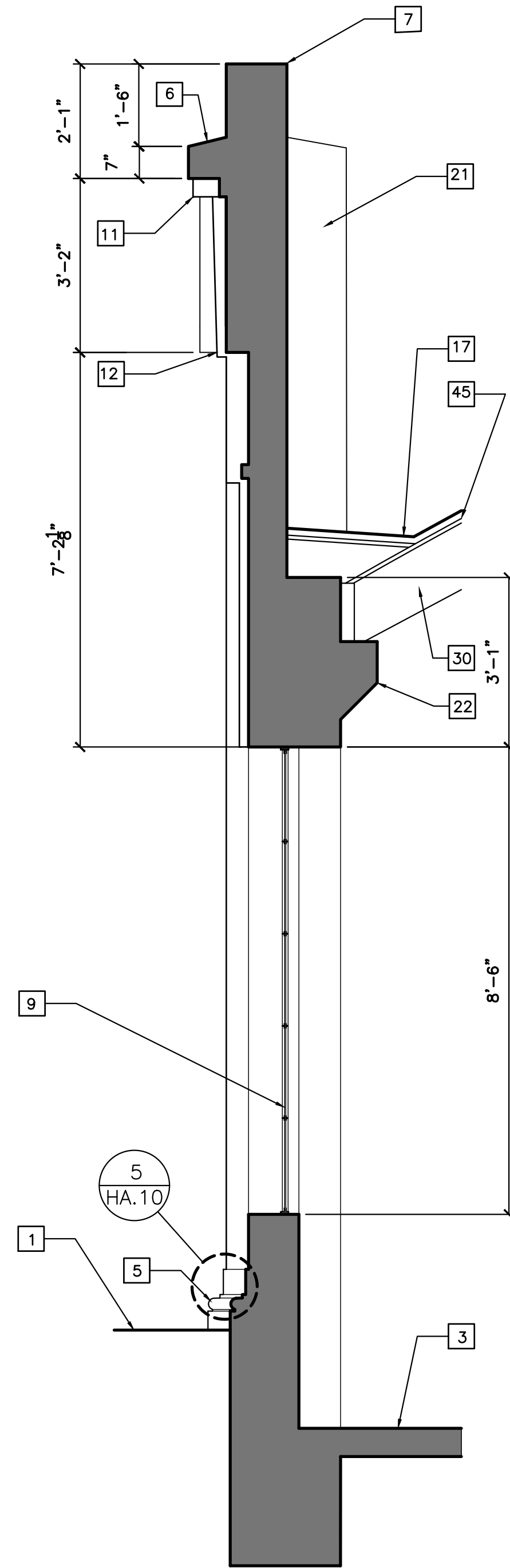
5 TYP. WALL BASE  
HA.10 3" = 1'-0"



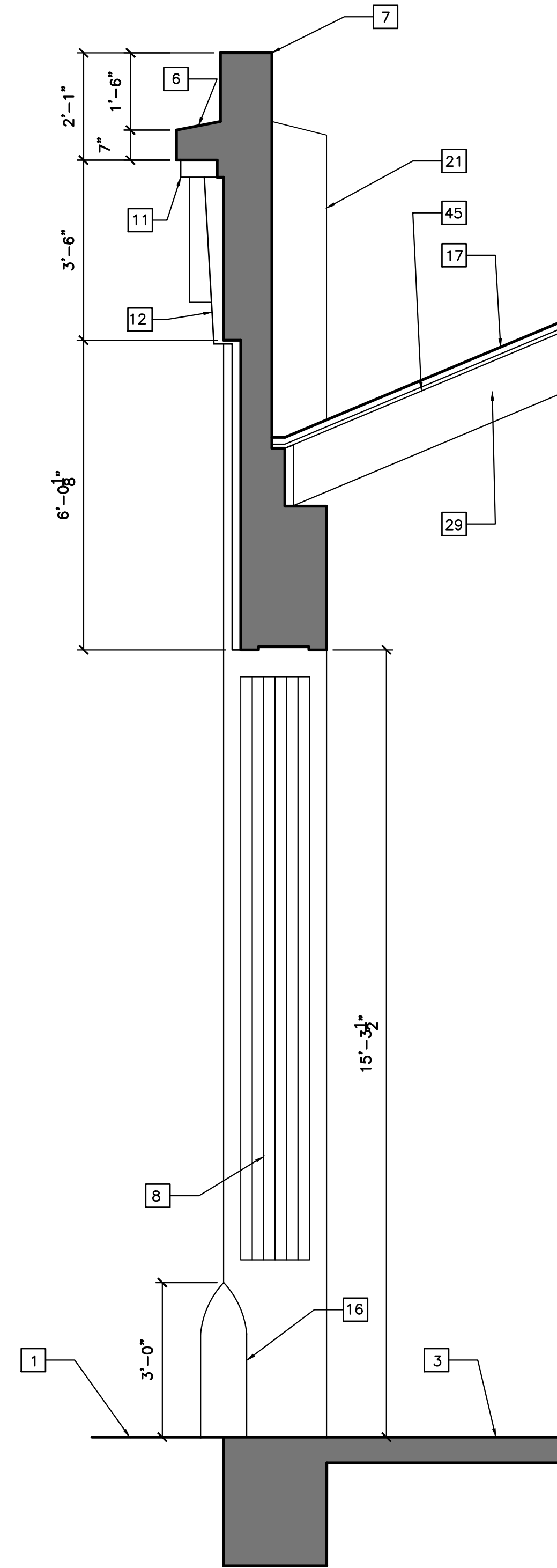
4 WALL SECTION F  
HA.10 1/2" = 1'-0"



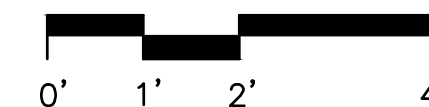
3 WALL SECTION E  
HA.10 1/2" = 1'-0"



2 WALL SECTION D  
HA.10 1/2" = 1'-0"



1 WALL SECTION C  
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