

CITY COUNCIL
CONDITIONS FOR VESTING TENTATIVE MAP NO. 632543
SHAWNEE/CG7600 MASTER PLAN - PROJECT NO. 174988 [MMRP]
ADOPTED BY RESOLUTION NO. R-_____ ON _____

GENERAL

1. This Vesting Tentative Map will expire _____. (Three years from approval date)
2. Compliance with all of the following conditions shall be completed and/or assured, to the satisfaction of the City Engineer, prior to the recordation of the Final Map, unless otherwise noted.
3. Prior to the Vesting Tentative Map expiration date, a Final Map shall be recorded in the Office of the San Diego County Recorder.
4. Prior to the recordation of the Final Map, taxes must be paid on this property pursuant to Subdivision Map Act section 66492. To satisfy this condition, a tax certificate stating that there are no unpaid lien conditions against the subdivision must be recorded in the Office of the San Diego County Recorder.
5. The Final Map shall comply with the provisions of the Shawnee/CG7600 Master Plan, Planned Development Permit No. 632544 and Site Development Permit No. 632545.
6. The Subdivider shall defend, indemnify, and hold the City (including its agents, officers, and employees [together, "Indemnified Parties"]) harmless from any claim, action, or proceeding, against the City and/or any Indemnified Parties to attack, set aside, void, or annul City's approval of this project, which action is brought within the time period provided for in Government Code section 66499.37. City shall promptly notify Subdivider of any claim, action, or proceeding and shall cooperate fully in the defense. If City fails to promptly notify Subdivider of any claim, action, or proceeding, or if City fails to cooperate fully in the defense, Subdivider shall not thereafter be responsible to defend, indemnify, or hold City and/or any Indemnified Parties harmless. City may participate in the defense of any claim, action, or proceeding if City both bears its own attorney's fees and costs, City defends the action in good faith, and Subdivider is not required to pay or perform any settlement unless such settlement is approved by the Subdivider.

Project No. 174988
VTM No. 632543

7. Pursuant to City Council Policy 600-20, the subdivider shall provide evidence to ensure that an affirmative marketing program is established.

ENGINEERING

8. The Subdivider shall underground any new service run to any new or proposed structures within the subdivision.
9. The Subdivider shall ensure that all existing onsite utilities serving the subdivision shall be undergrounded with the appropriate permits. The Subdivider shall provide written confirmation from applicable utilities that the conversion has taken place, or provide other means to assure the undergrounding, satisfactory to the City Engineer.
10. The Owner/Subdivider shall denote on the final map and the improvement plans "Subject to Inundation" all areas lower than the base flood elevation plus 1 foot.
11. Conformance with the "General Conditions for Tentative Subdivision Maps," filed in the Office of the City Clerk under Document No. 767688 on May 7, 1980, is required. Only those exceptions to the General Conditions which are shown on the Vesting Tentative Map and covered in these special conditions will be authorized. All public improvements and incidental facilities shall be designed in accordance with criteria established in the Street Design Manual, filed with the City Clerk as Document No. RR-297376.
12. The Subdivider shall comply with all current street lighting standards according to the City of San Diego Street Design Manual (Document No. 297376, filed November 25, 2002) and the amendment to Council Policy 200-18 approved by City Council on February 26, 2002 (Resolution R-296141) satisfactory to the City Engineer. This may require (but not be limited to) installation of new street light(s), upgrading light from low pressure to high pressure sodium vapor and/or upgrading wattage.

PARKS & RECREATION REQUIREMENTS:

13. Prior to recordation of the Final Map the Owner/Permittee shall provide a recreation easement over Lot OS-02, (2.22 acres) and Lot OS-03, (0.35 acres) for a total of 2.57 useable acres of population-based park land.

MAPPING

14. "Basis of Bearings" means the source of uniform orientation of all measured bearings shown on the map. Unless otherwise approved, this source shall be the

California Coordinate System, Zone 6, North American Datum of 1983 [NAD 83].

15. "California Coordinate System" means the coordinate system as defined in Section 8801 through 8819 of the California Public Resources Code. The specified zone for San Diego County is "Zone 6," and the official datum is the "North American Datum of 1983."
16. The Final Map shall:
 - a. Use the California Coordinate System for its "Basis of Bearing" and express all measured and calculated bearing values in terms of said system. The angle of grid divergence from a true median (theta or mapping angle) and the north point of said map shall appear on each sheet thereof. Establishment of said Basis of Bearings may be by use of existing Horizontal Control stations or astronomic observations.
 - b. Show two measured ties from the boundary of the map to existing Horizontal Control stations having California Coordinate values of Third Order accuracy or better. These tie lines to the existing control shall be shown in relation to the California Coordinate System (i.e., grid bearings and grid distances). All other distances shown on the map are to be shown as ground distances. A combined factor for conversion of grid-to-ground distances shall be shown on the map.

WASTEWATER

17. All proposed public sewer facilities are to be designed and constructed in accordance with established criteria in the most current City of San Diego sewer design guide.
18. All proposed private sewer facilities located within a single lot are to be designed to meet the requirements of the California Uniform Plumbing Code and will be reviewed as part of the building permit plan check.
19. No trees or shrubs exceeding three feet in height at maturity shall be installed within ten feet of any sewer facilities.

WATER

20. The Subdivider shall design and construct all proposed public water facilities, including services and meters, in accordance with established criteria in the most current edition of the City of San Diego Water Facility Design Guidelines and

City regulations, standards and practices pertaining thereto. Public water facilities and associated easements, as shown on the approved vesting tentative map Exhibit "A", shall be modified at final engineering to comply with standards. Proposed facilities that do not meet the current standards shall be private.

21. The Subdivider shall grant water easements, as shown on the approved tentative map Exhibit "A", satisfactory to the Director of Public Utilities and the City Engineer. Easements shall be located within singles lots, when possible, and not split longitudinally.
22. The Subdivider shall install fire hydrants at locations satisfactory to the Fire Marshal, the Director of Public Utilities and the City Engineer. If more than two (2) fire hydrants or thirty (30) dwelling units are located on a dead-end water main then the Subdivider shall install a redundant water system satisfactory to the Director of Public Utilities.
23. No trees shall be installed within ten feet of any water facilities or in any water easement. No shrubs exceeding three feet in height at maturity shall be installed within 10 feet of any public water main.
24. The Subdivider shall process encroachment maintenance and removal agreements, for all acceptable encroachments into the water easement, including but not limited to structures, enhanced paving, or landscaping. No structures or landscaping of any kind shall be installed in or over any vehicular access roadway.

LANDSCAPE/BRUSH MANAGEMENT

25. Prior to the recordation of the Final Map, the subdivider shall submit complete landscape construction documents, including plans, details, and specifications (including a permanent automatic irrigation system unless otherwise approved), for the required right-of-way and median (if applicable) improvements, slope revegetation and hydroseeding of all disturbed land in accordance with the Landscape Standards, Exhibit 'A' and to the satisfaction of the Development Services Department.
26. Prior to the recordation of the Final Map, the subdivider shall submit interim landscape/erosion control and permanent irrigation (if applicable) construction documents for slope revegetation and hydroseeding of all disturbed land in accordance with the Exhibit 'A', the Landscape Standards and to the satisfaction of the Development Services Department.

TRANSPORTATION

27. Prior to recordation of the first final map, the Subdivider shall dedicate 11 feet of project frontage along Mission Gorge Road between the Private Drive south of Street "E" and Street "E" , satisfactory to the City Engineer.
28. Prior to recordation of the first final map, the Subdivider shall dedicate 2 feet of project frontage along Mission Gorge Road from Old Cliffs Road/Project Entrance to the southern property line, satisfactory to the City Engineer.

INFORMATION:

- The approval of this Tentative Map by the City Council of the City of San Diego does not authorize the subdivider to violate any Federal, State, or City laws, ordinances, regulations, or policies including but not limited to, the Federal Endangered Species Act of 1973 and any amendments thereto (16 USC § 1531 et seq.).
- If the Subdivider makes any request for new water and sewer facilities (including services, fire hydrants, and laterals), the Subdivider shall design and construct such facilities in accordance with established criteria in the most current editions of the City of San Diego water and sewer design guides and City regulations, standards and practices pertaining thereto. Off-site improvements may be required to provide adequate and acceptable levels of service and will be determined at final engineering.
- Subsequent applications related to this Vesting Tentative Map will be subject to fees and charges based on the rate and calculation method in effect at the time of payment.
- Any party on whom fees, dedications, reservations, or other exactions have been imposed as conditions of approval of the Vesting Tentative Map, may protest the imposition within ninety days of the approval of this Vesting Tentative Map by filing a written protest with the San Diego City Clerk pursuant to Government Code sections 66020 and/or 66021.
- Where in the course of development of private property, public facilities are damaged or removed, the Subdivider shall at no cost to the City, obtain the required permits for work in the public right-of-way, and repair or replace the public facility to the satisfaction of the City Engineer (San Diego Municipal Code § 142.0607).

Internal Order No. 23432145

Project No. 174988
VTM No. 632543

DRAFT

RESOLUTION NUMBER R-_____

ADOPTED ON _____

APPROVING AN AMENDMENT TO THE NAVAJO COMMUNITY PLAN TO
REDESIGNATE LAND FROM INDUSTRIAL TO MIXED USE

WHEREAS, on ____ [date] _____, the City Council of the City of San Diego held a public hearing for the purpose of considering an amendment to the Navajo Community Plan adopted as Document RR-257606, and including its subsequent amendments, to redesignate a 22.9-acre site located at 6910-6980 Mission Gorge Road, excepting 6966 Mission Gorge Road, North of Mission Gorge Rd at Old Cliffs Road from Industrial to Mixed Use; and

WHEREAS, the Navajo Community Plan is a component of the General Plan; and

WHEREAS, the proposed changes to the Navajo Community Plan are attached to this resolution; and

WHEREAS, the Planning Commission of the City of San Diego has recommended certification of Environmental Impact Report No. 174988 which also addressed an accompanying development project; and

WHEREAS, the Planning Commission has found, based on its hearing record, that this amendment retains internal consistency with the Navajo Community Plan and the 2008 General Plan and that the proposed amendment helps achieve long-term community and citywide goals; and

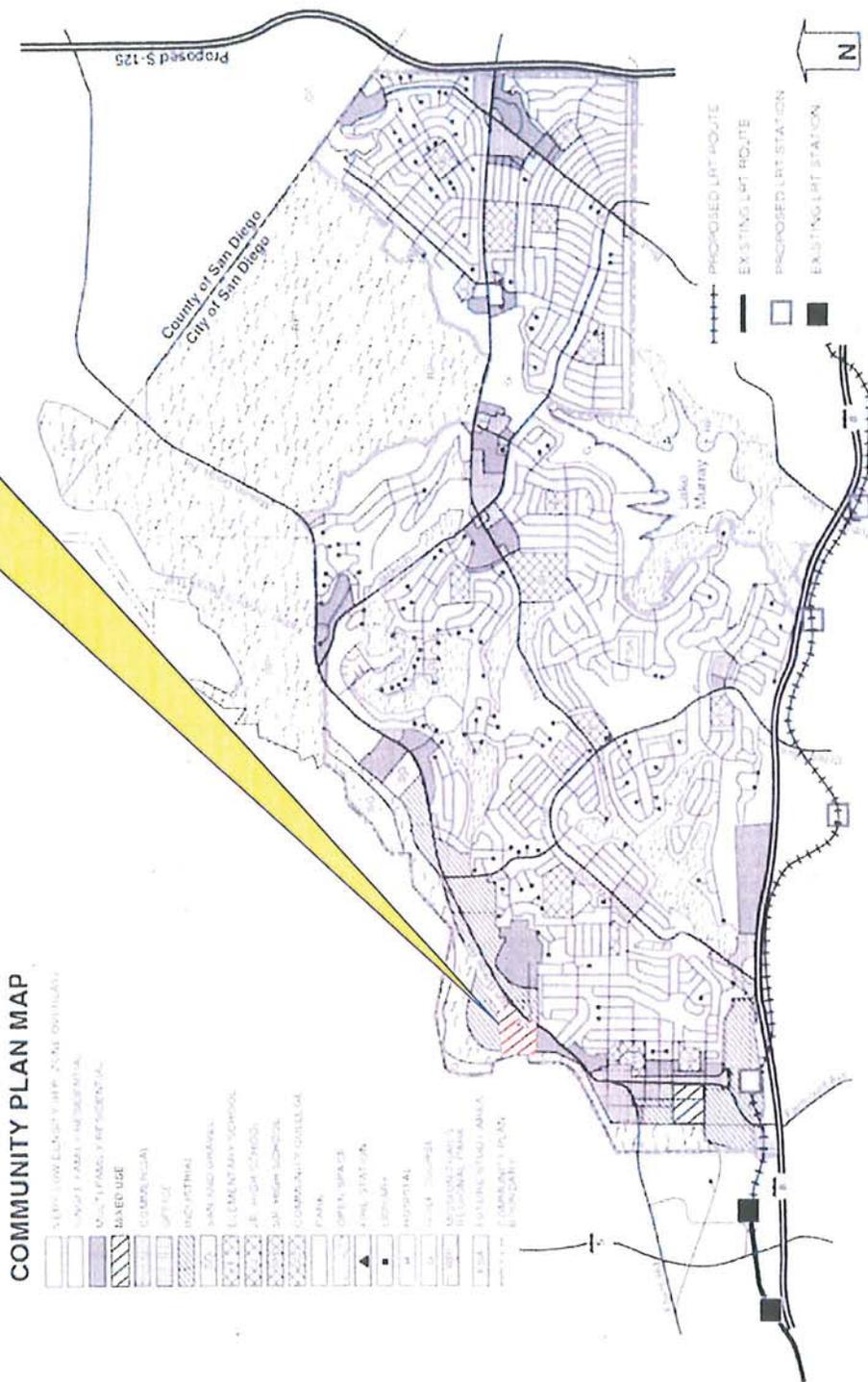
WHEREAS, the City Council has considered the Planning Commission record and recommendation as well as all maps, exhibits, and written documents contained in the file for this amendment on record in the City of San Diego, and has considered the oral presentations given at the public hearing; NOW, THEREFORE,

BE IT RESOLVED, by the City Council of the City of San Diego, that it adopts the amendment to the Navajo Community Plan, with a copy of said amendment being on file in the office of the City Clerk as Document No. RR - _____.

APPROVED: JAN GOLDSMITH, City Attorney

By: _____
Name
Deputy City Attorney

Project Site



Land Use Map

**SHAWNEE/CG 7600 6910 -6980 MISSION GORGE ROAD
PROJECT NUMBER 174988**



NAVAJO COMMUNITY PLAN

Shawnee/CG 7600

Proposed Amendments

3rd Draft

PTS 174988

May 2011

Prepared by the
NAVAJO COMMUNITY PLANNERS
and
THE CITY OF SAN DIEGO

City of San Diego Planning Department
202 C Street, MS 4A
San Diego, CA 92101



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This information, or this document (or portions thereof), will be made available in alternative formats upon request.

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NAVAJO COMMUNITY PLAN AMENDMENTS

The following amendments have been incorporated into this September 2007 posting of this Plan:

Amendment	Date Approved by Planning Commission	Resolution Number	Date Adopted by City Council	Resolution Number
Navajo Community Plan adopted	July 29, 1982	3946	December 7, 1982	R-257606
Grantville Amendment- updates existing conditions, provides design guidelines, and establishes supplemental development regulations	January 5, 1989	0333-PC	April 4, 1989	R-273164
The Circulation and Public Transportation Element was added			August 5, 2002	R-296956
Centerpoint at Grantville			May 15, 2007	R-302636
Shawnee/CG 7600			TBD	TBD



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In addition to the Navajo Community Planners, the following City Department staff has made a significant contribution to development of the Navajo Community Plan.

PARK & RECREATION

Mason Milliken

TRANSPORTATION PLANNING

Allen Holden

We also wish to thank J. V. Ward, San Diego Unified School District.

ILLUSTRATION CREDITS

Unless otherwise noted, illustrations are by the City of San Diego Planning Department.

Pages 22, 35, 119

Community Design Manual Fremont General Plan Program, Williams Cook and Mocine, City and Regional Planning San Francisco, California, 1967

Pages 35, 88

Dale Nagle Architect, La Jolla California

Page 88

The Visual Environment of Los Angeles Department of City Planning, Los Angeles, California, April 1971

Page 107

Toward a Better Tomorrow, a Transportation Report for Southern California by the Automobile Club of Southern California, Highway Engineering Department, 1971

Page 120

Lysander, New Community Final Planning Report submitted to the New York Urban Development Corporation by David A. Crane and Associates, January, 1971

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THE CITY OF
SAN DIEGO

CITY ADMINISTRATION BUILDING • 202 C STREET • SAN DIEGO, CALIF 92101

July 29, 1982

The Honorable Mayor and City Council
The City Planning Commission
City of San Diego, California

Herein transmitted is the revision to the Navajo Community Plan. This revision represents a comprehensive, long-range policy guide for the physical development of this community and is intended to replace the community plan adopted by the City Council on November 7, 1973.

In June of 1971, the Navajo Community Planners came into existence with the endorsement of the City Council and City Planning Commission. The Committee's purpose is to represent the Navajo area, composed of the communities of Allied Gardens, Grantville, Del Cerro and San Carlos, and to work with the City Manager's office and City Planning Department in a cooperative effort to develop and maintain a plan reflecting the goals and the aspirations of the citizens of the community.

The Committee has met regularly with City staff, numerous agencies and individuals directly concerned with the future of the area, and with the citizens at large in well attended public meetings, the result of which is a revised document expressing a cross-section of community and City opinions. Involvement and expertise have been combined to produce a series of working solutions, all based on the assumption that Navajo will continue to grow as a healthy, predominantly single-family, yet varied area in which to live and to work.

The Plan is a vitally needed step in realizing the development potential of Navajo for present and future residents of the community, as well as for the people of the City at large. It is recommended that the Planning Commission and the City Council adopt the Plan as revised to serve as a comprehensive policy framework for the long-range development of the Navajo community area.

Respectfully submitted,

Jack Van Cleave
Planning Director

NAVAJO COMMUNITY PLANNERS, INC.

P.O. BOX 20304, SAN DIEGO, CALIFORNIA 92120

Dear Community Resident:

The goal of the Navajo Community Planners is a Plan which provides for the health, safety and welfare of the existing and future residents and maintains the area as a desirable neighborhood in which to live.

The Plan is a cross section of community options and desires blended with professional knowledge and sound planning policy. Throughout many months the volunteer members of the Community Planning Board met with the City Planning Department Staff in workshops with representatives of various City, School and State Departments. Many ideas and concepts were considered and refined to meet the needs of the community, some were rejected. The Plan presented is a comprehensive, long-range policy guide for the Navajo Community. It discusses residential, commercial, industrial, open space, circulation, environmental and community facilities in terms of existing conditions, states projected needs, objectives and proposals for each.

We hope you will take advantage of the detailed information to better understand the suggestions for the future of the community. For the Plan to serve as a useful document statements were made as precise as possible. Often it became necessary to use special phrasing and planning vocabulary. A section of definitions and concepts has been included to more clearly establish the concepts as worded in the text. Tables, charts, etc. were included where necessary for explanation and reference. The end result does give the Plan a technical textbook appearance, but it does make material available where and when it is needed.

The Navajo Community Plan is simply a first step in the never-ending desire of the citizens to participate in the destiny of their community. We are grateful for your interest in the community and hope you will actively support and participate in the community planning process.

Respectfully,

NAVAJO COMMUNITY PLANNERS

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INTRODUCTION

PREFACE

The Navajo area of San Diego is approximately 8,000 acres in size and is located in the easterly portion of the City of San Diego. It includes the community areas of Allied Gardens, Del Cerro, Grantville and San Carlos. It is bounded on the north by Mission Gorge, on the east by the cities of El Cajon and La Mesa, on the south by Highway 8 and on the west by the San Diego River channel.

Navajo is conveniently located within the metropolitan area. Employment and shopping are readily accessible within the community and in nearby areas. Grantville, a major employment center, is located within the westerly portion of the community. The Kearny Mesa industrial area, another major employment center, is somewhat farther away, but still convenient to the residents of Navajo. These areas, plus downtown employment centers, are quite accessible by major streets and freeways. In addition to the many industrial facilities within and close to the study area, regional and community shopping centers are located within Navajo and in nearby Kearny Mesa, Mission Valley, La Mesa, El Cajon and downtown San Diego.

The overriding objectives for the long-range development of Navajo are to retain the residential character of the area, provide adequate community services, such as police and fire protection, rubbish collection, etc., establish guidelines for the utilization of canyons and hillsides and enhance the environment of the area as a pleasant community in which to live. To assist in accomplishing these objectives, this community plan provides guidelines for public and private development to the year 2000. The Plan represents a policy framework that will enable the community and the City to work jointly on more specific studies and action programs. The Plan is only an initial step.

While this Plan sets forth many proposals for implementation, it does not establish new regulations or legislation nor does it rezone property. However, under the provisions of Section 65860 of the Government Code that requires that the City Zoning Ordinance be consistent with the General Plan, the Plan does establish the guidelines for the future preparation or amendment of City ordinances. Zoning, subdivision, housing, building or other development controls must be enacted separately through the legislative process in order to implement the intent of this Plan. The implementation section of this Plan identifies those steps to be taken.

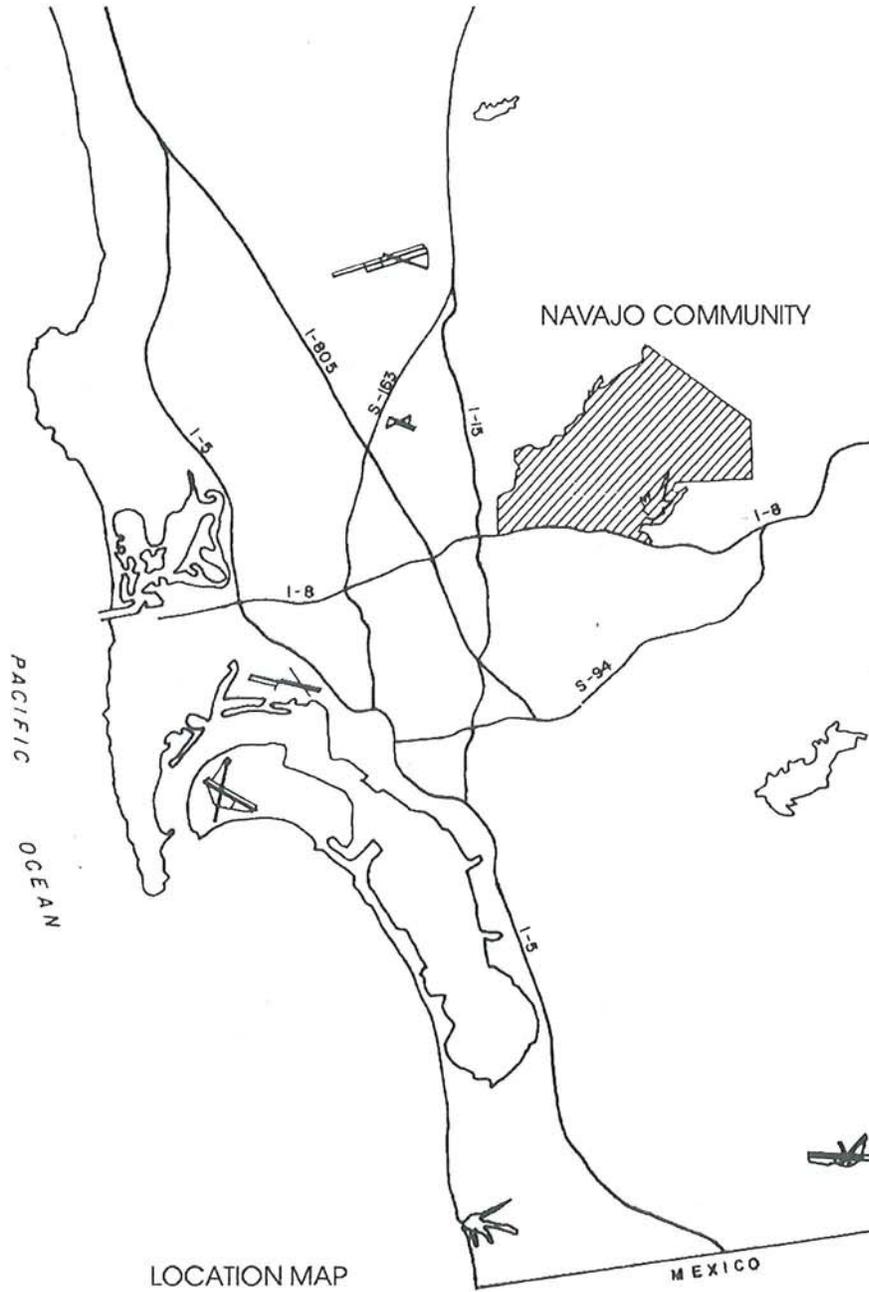
The need for a flexible document is acknowledged. Nothing can be rigid in view of changing life styles, needs, and technology. However, the recommendations as expressed in this Plan, together with the planning principles, should not be allowed to be eroded by individual interpretations but should be kept intact and followed unless amended by due process.

The Plan discusses the community environment and major land uses: residential, commercial, open space, industrial, community facilities, and circulation. Elements are presented in terms of existing conditions, development potential or projected needs, objectives and proposals. A Plan map presents a composite of all major land use proposals. Finally, the Plan concludes with an implementation section which sets forth major projects, public and private, needed to carry out the Plan.

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Periodic updating of the Plan will be necessary as conditions in the community change. Once adopted by Council, any amendments, additions or deletions from the document will require that the Planning Commission and the City Council follow the same procedure of holding public hearings as was followed in adopting the Plan originally. Future decisions affecting the environment of the area will be based on the general objectives above as well as the specific recommendations that follow.

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PHYSIOGRAPHY

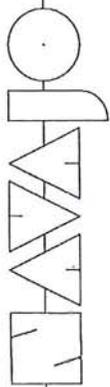
The community is characterized by a wide variety of natural features typical of many other San Diego areas, including flat mesas, steep canyons, and rolling hills. The most prominent feature in the area is Cowles Mountain. Elevations within the community range from a low of around 100 feet above sea level at the westerly edge of Mission Gorge to 1,591 feet at the peak of Cowles Mountain, the highest point in the City.

Within the slopes of Cowles Mountain, there are approximately 4,250 acres of undeveloped land. Approximately 2,230 acres of this land are in steep slopes of 35 percent or greater; 1,070 acres have slopes of 15-35 percent; and the remaining 950 acres are relatively level with slopes of less than 15 percent. Natural vegetation in the community consists mostly of chaparral, sage, and other cover typical of semiarid regions, with some oak and sycamore trees in the canyons. The soil composition is basically alluvium, slope wash and sedimentary rocks. The high quality hard blue rock located in Mission Gorge is processed into sand and gravel by commercial enterprises.

Three sedimentary formations exist in areas of the community that, when combined, form an unstable soil condition. This unstable condition can be responsible for groundwater seepage and landslides. Measures should be taken when development is proposed in these areas that would reduce the geological hazard impacts to a level of insignificance. One such measure is the application of the Geologic Hazard Overlay category. The overlay identifies areas that, where such soil conditions exist, the developer is to provide an as-built geologic report prior to issuance of building permits by the City, and is required to provide homeowner warranties against landslides for a period of ten years following the first sale of any developed property (Council Resolution No. R-254954 adopted August 31, 1981).

There are several unique features in the area. Mission Gorge on the north is the site of the early Mission Dam and is a State Historical Landmark. Centered in the southeastern part of the community is Lake Murray, a City-owned reservoir which also serves as a recreation facility. The lake, proposed for continued aquatic recreational use, contains 140 surface acres of water surrounded by approximately 416 acres of City-owned land. A public golf course is located within this area.

Natural runoff from the community drains into the San Diego River to the north and west and Alvarado Canyon to the south. The natural groundwater of the San Diego River is being used by the sand and gravel processors. The river is subject to flooding that, of course, should be considered when allocating land use for the floodplain. Rainfall in the area is slightly less than ten inches a year, which is consistent with the overall San Diego average.



TOPOGRAPHIC RELIEF MAP

COWLES MOUNTAIN

MISSION GORGE

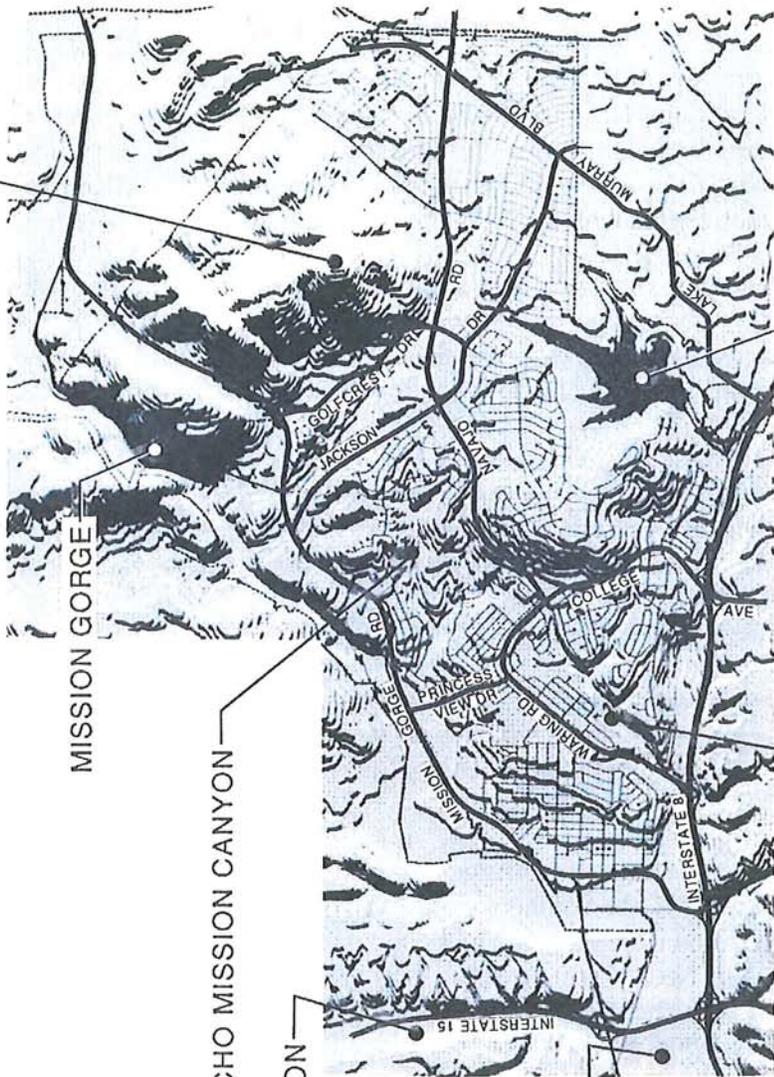
RANCHO MISSION CANYON

MURPHY CANYON

MISSION VALLEY

NAVAJO CANYON

LAKE MURRAY



BACKGROUND

The study area is closely tied to early California history. Navajo has always been a prime location in the San Diego area--near water, in the vicinity of good land for cultivation and at the crossroads of five major trails: the Mission Trail (now known as Friars Road), Murphy Canyon Road, Yard Road, Alvarado Trail and Mission Gorge Road. California's first mission was located on the Rancheria Nipaguay, adjacent to the area, probably because of the choice location.

Formal dedication of the Presidio of San Diego de Alcalá by Father Junipero Serra occurred on July 16, 1769. The City of Saint Didacus began to produce changes on the landscape. The good Fathers needed a guaranteed supply of water; thus, the first irrigation project in California was begun. From 1812 to 1816, Indian laborers constructed what is known as the Old Mission Dam in Mission Gorge with a six-mile transmission ditch to the Mission.

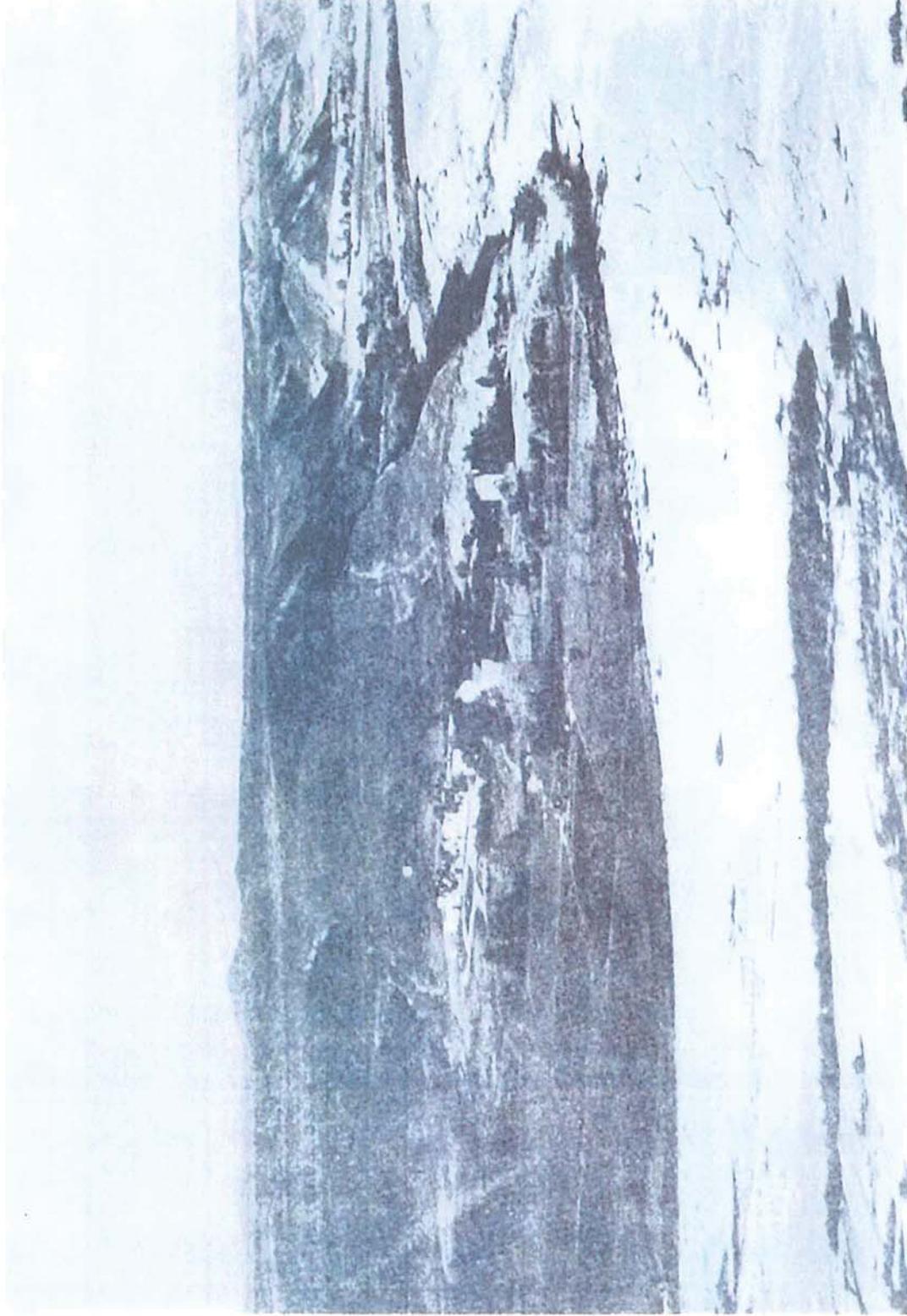
In 1835, the Missions were secularized by the Mexican Government. At that time, Mission Ranch of San Diego de Alcalá encompassed 58,875.38 acres. It was the second largest Rancho in San Diego County, extending eastward from the San Diego Pueblo boundary to El Cajon Rancho, and northward from Rancho de la Nación to what is now Miramar Naval Air Station.

Before secularization, the area (El Cajon Rancho) was included in the lands of the San Diego Mission and was one of the Mission's most valuable grazing areas. As defined by an early land commission, this rancho extended northeasterly from La Mesa to a point north of the San Diego River above El Monte Park and over the area now occupied by El Cajon, Bostonia, Santee, Lakeside and Flinn Springs.

In 1887, plans for a town site were laid out by the Junipero Land and Water Company. Plans for a Soldier's Home to be located at Grant Circle were included. The official name registered with the Post Office was Orchard, but the area became known as Grantville in honor of President U. S. Grant.

In 1948, the so-called Waring Tract, comprising approximately 460 acres, was annexed to the City of San Diego. Following this, the Southern Title and Trust Company Tract was annexed in 1951. This second annexation covered 1,152 acres and was subsequently developed as Allied Gardens. Waring Tract No.2 was annexed in 1954. This annexation was one of the largest in the City of San Diego and consisted of almost 5,000 acres.

The San Carlos and Del Cerro communities have been developed within this area. Since the Waring Tract No.2 annexation there have been numerous smaller annexations on the northern periphery lying generally along Mission Gorge. The last annexation was in 1982, with most annexations occurring between 1953 and 1954.



GRANTVILLE FROM KENSINGTON 1927
Historical collection, Title Insurance and Trust Company, San Diego, Ca.



NAVAJO - 1958

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On January 28, 1971, concerned residents of Allied Gardens, Del Cerro and San Carlos met to organize a community plan committee. As a result of that meeting, the Mission-Navajo-Del Cerro-San Carlos Community Plan Committee (now Navajo Community Planners) was formed, consisting of members representing the numerous community groups that already existed in the area.

In June of 1971, the City Council endorsed the Navajo Community Planners. This Committee was asked to represent the Navajo area and to work with the City Manager's office and Planning Department in the development of a community plan consistent with community goals and objectives.

The role of the Committee was to review and analyze background information, formulate community objectives, and to conduct a program to keep the community informed of its progress. City staff provided the necessary technical studies. Liaison was maintained with area residents and other groups both inside and outside the community.



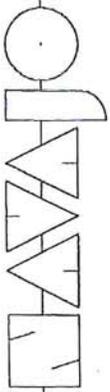
EXISTING LAND USE AND ZONING

Of the total zoned land in the Navajo area, 52 percent, or 4,021 acres, is zoned for single-family homes; 4.5 percent, or 353 acres, is zoned for multiple family use two percent, or 155 acres, is zoned for commercial use; and about three percent, or 238 acres, is zoned for industrial use. The remaining 38.5 percent of the area, or 3029 acres, located predominantly in the southern and eastern sections of the area, including Cowles Mountain, is zoned for agriculture and the San Diego River floodway.



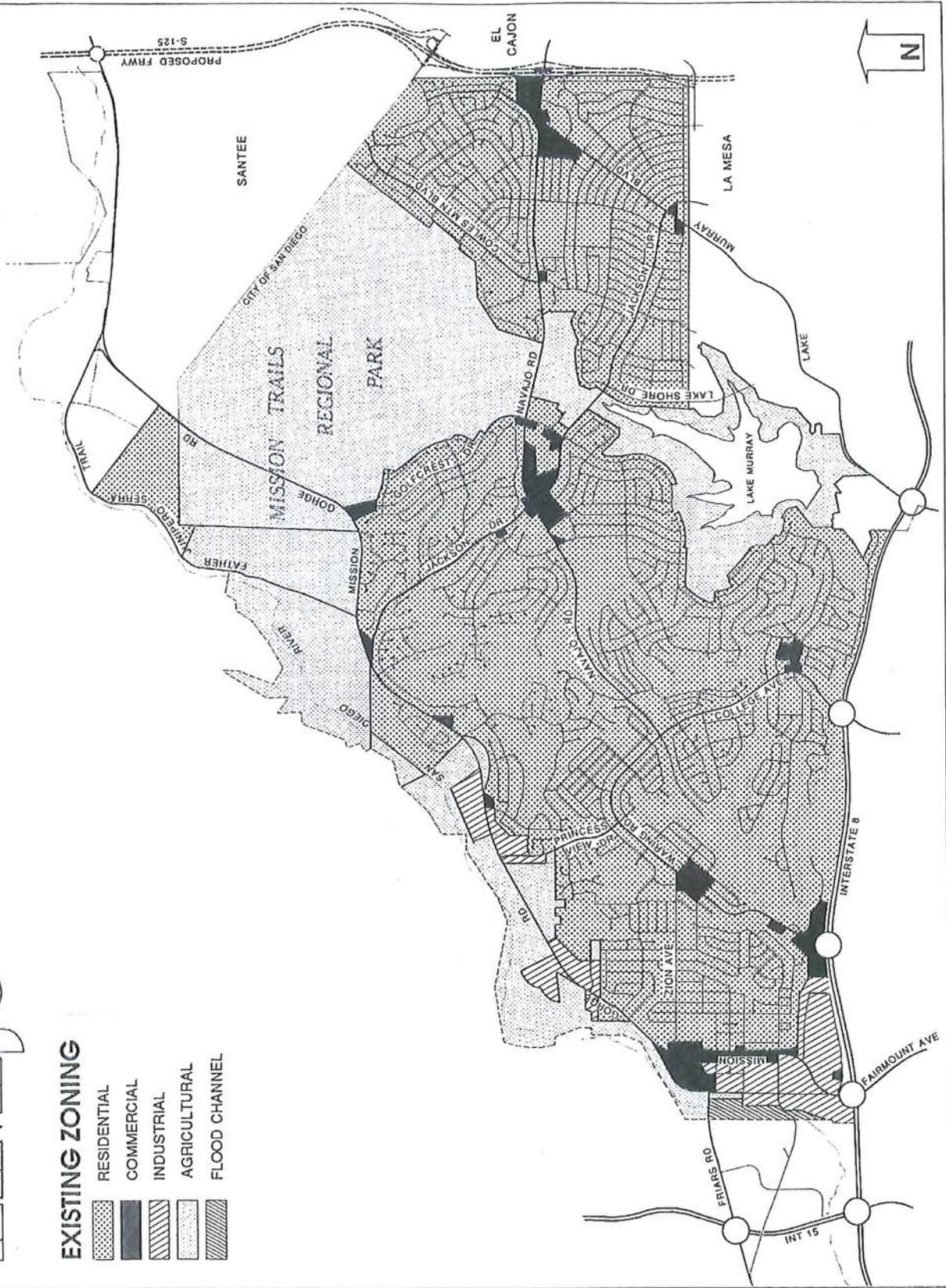
agriculture and the San Diego River floodway.

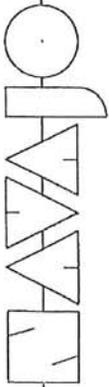
Public and semi-public uses and single-family homes are the predominant land uses within the community. Public and semi-public uses occupy 39.5 percent of the area or 3,099 acres. Single-family homes occupy approximately 37.5 percent or 2,924 acres while multifamily, commercial, and industrial uses comprise only nine percent of the area or 700 acres. Approximately 14 percent of the developed area is in streets and the remaining area is vacant.



EXISTING ZONING

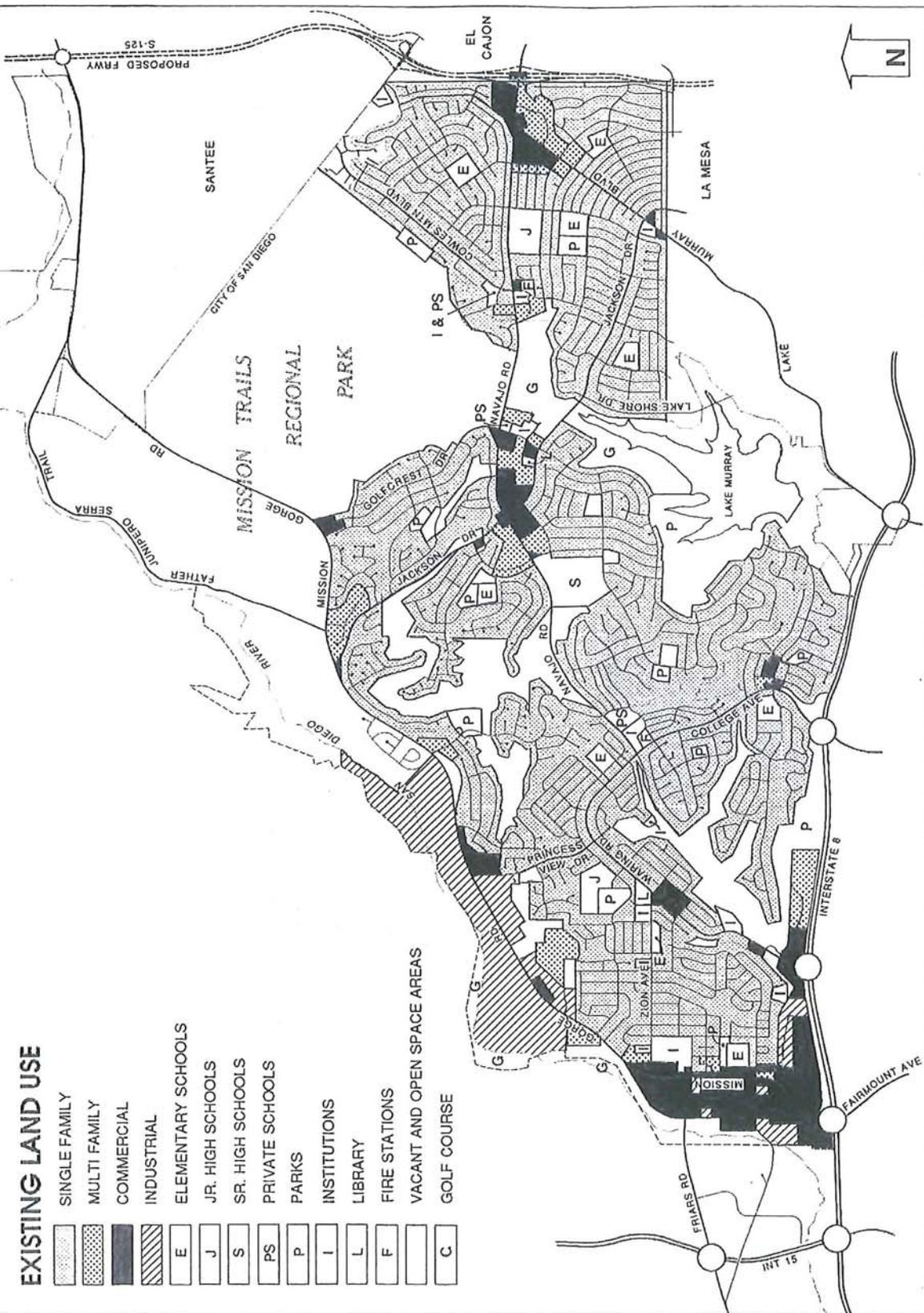
-  RESIDENTIAL
-  COMMERCIAL
-  INDUSTRIAL
-  AGRICULTURAL
-  FLOOD CHANNEL





EXISTING LAND USE

-  SINGLE FAMILY
-  MULTI FAMILY
-  COMMERCIAL
-  INDUSTRIAL
-  ELEMENTARY SCHOOLS
-  JR. HIGH SCHOOLS
-  SR. HIGH SCHOOLS
-  PRIVATE SCHOOLS
-  PARKS
-  INSTITUTIONS
-  LIBRARY
-  FIRE STATIONS
-  VACANT AND OPEN SPACE AREAS
-  GOLF COURSE



POPULATION CHARACTERISTICS

The Navajo community is one of San Diego's established communities. The 1980 census counted 50,005 people in this community. This population count represents an increase of 32,640 people, or 187 percent, since the 1960 census.

Approximately 82 percent of the dwelling units within Navajo are owner occupied. The occupancy rate is 95.01 percent. The development of tract homes is subsiding, while condominium and Planned Residential Development activity in the area is increasing.

The 1975 census shows a decline from 1970 in the number of children in the 0-5 age group within the area. If the trend continues, the demand for additional elementary grade classrooms may not be as acute as previous projections have indicated. As the community grows toward total development, one might expect the age composition to change in the direction of smaller average family size and an increase in the adult population, a trend which seems to be established in older developed areas.

NOTE: All statistics compiled by City of San Diego Planning Department.

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PLAN ELEMENTS

RESIDENTIAL

EXISTING CONDITIONS

Navajo is a family-oriented community of attractive single-family homes. In 1968, approximately 95 percent of the population resided in single-family homes. These homes accounted for 92 percent of all dwelling units in the Navajo area.

In 1970, five percent of the population resided in multifamily units, which comprised 28 percent of all dwelling units in the Navajo area. In 1970, almost 75 percent of all dwelling units were owner-occupied, compared to 50 percent in the City as a whole. Vacancy rates were approximately four percent as compared to 5.6 percent for the entire City.



In 1988, single-family homes accounted for 77 percent of all dwelling units in the Navajo area. Multifamily homes accounted for 21 percent of the dwelling units. Vacancy rates were approximately 3.4 percent, while the citywide rate was 4.4 percent. According to 1980 census data, almost 79 percent of all dwelling units are owner-occupied, compared to 50 percent in the City as a whole.

Densities in the single-family residential areas of the community vary from one to six dwelling units per acre. In the multifamily areas, densities vary from 16 dwelling units per acre in the vicinity of Navajo Road and Jackson Drive to 40 dwelling units per acre in Grantville, **and up to 72 dwelling units per acre.**

Two mobile home parks are located in the community in the vicinity of Mission Gorge Road. One is located near the intersection of Old Cliffs Road and Mission Gorge Road and the other is located at the eastern end of Old Cliffs Road. The Mobile Home Park Overlay Zone has been applied to both of these areas. This overlay zone provides protection for the residents of the mobile home parks against development of the sites for other uses and ensures the availability of varied housing types to create a more balanced community.

OBJECTIVES

In the course of its deliberations, the Navajo Community Planners adopted objectives for each of the major Plan elements. The principal or overriding residential objective to guide the long-range development of Navajo is to: **MAINTAIN AND ENHANCE THE QUALITY OF EXISTING RESIDENCES AND ENCOURAGE THE DEVELOPMENT OF A VARIETY OF NEW HOUSING TYPES WITH DWELLING UNIT DENSITIES PRIMARILY IN THE LOW TO LOW-MEDIUM DENSITY RANGE AS SHOWN.**

TABLE 1
DENSITY RANGES

Very low density	0-4 dwelling units per acre
Low density	5-9 dwelling units per acre
Low-medium density	10-14 dwelling units per acre
Medium density	15-29 dwelling units per acre
Medium-high density	30-43 dwelling units per acre
High density	44-72 dwelling units per acre

To achieve this principal objective, the following additional objectives were also adopted:

- Promote a healthy environment by careful planning and sensitive development of well-defined, balanced and distinct communities which encompass a variety of residential density patterns and housing types.
- Prevent and/or limit development in proposed open space areas which serve to enhance community identity--steep slopes and canyons, floodplains, and areas with unique views and vistas.
- Foster techniques of land development that will encourage imagination and variety in building site layouts, housing types, and costs, and that will capitalize on the unique topographic assets of the community. All housing developments within the study area should relate to existing topography in order to minimize grading and preserve the natural terrain of the area. The use of retaining walls, terraces, split level or cantilevered houses should be considered in steep terrain.
- Assurance that any individual or family may be free to choose accommodations within their economic capacity from a range of housing varying in type, quality and location.
- Continuation of community support for those programs at all governmental levels that would effectively increase the economic ability of the disadvantaged to obtain adequate housing.
- Promotion of site selection for federally assisted housing programs which would ensure dispersal throughout the community of various ethnic and minority groups. Encourage enhancement of the existing residential development through the use of environment and conservation programs such as cleanup, painting and landscaping programs.
- Encourage the design of residential areas so as to prevent the encroachment of incompatible uses and minimize conflict (e.g., traffic noise) with more intensive nonresidential uses.

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- Within each new development and where possible in developed areas, plazas, squares, and other similar open space areas should be created. Emphasis should be placed on developing interconnected bikeways and walkways separated from auto traffic as part of the internal circulation system within the study area.
- Parking and storage areas should be screened from the street and other public areas.

PROPOSALS

General

- The Navajo Community Planners strongly support City Council Policy 600-19, Fostering of Balanced Community Development, which states:

It shall be the policy of the City Council to effect the development of economically and racially balanced communities in newly developing peripheral areas of the City and in all City sponsored or approved redevelopment projects, and to do what is reasonably and practically possible in all parts of the City.

- Housing types and densities should be varied in residential development to create interest and provide a mix of people with various economic and social characteristics.
- Dwelling units should relate to topography and intensity of activity. Where it will provide for more effective land utilization and high quality living environments, residential development proposals within the community should be carried out under the Planned Residential Development or Planned Infill Residential Development concept.
- Dwelling units should front on local streets.
- Multifamily residential development along major roadways, such as Mission Gorge Road, should be adequately **mitigated for** ~~set back from the~~ roadway ~~to mitigate~~ noise impacts associated with high traffic volumes. If perimeter noise walls are necessary to mitigate noise impacts, they should not be located in the required setback. Noise walls should be well designed with landscaping provided on both sides of the wall.
- Adequate off-street parking and storage must be provided and screened from living areas and public areas. Mounded earth forms should be used in level terrain to add interest to the landscape, to hide parking and to separate functions.
- Development of the area north of Highwood Drive and the terminus of Lake Murray Boulevard should not exceed 168 dwelling units as per Council Resolution No. 257606, December 7, 1982.
- Residential development should conform to the guidelines provided in the Mission Trails Design District when applicable. The Mission Trails Design District applies to those

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portions of the Navajo, Tierrasanta, and East Elliott communities consisting of, and including, all the commercial and multifamily residential zones; the steep hillsides under the HR (Hillside Review) Overlay Zone, as well as those non-HR and undeveloped areas contiguous to HR areas; and the two areas of land in Mission Gorge contiguous to the Mission Trails Regional Park at the east and west sides of the park. The Design District provides that no structure shall exceed four stories and in no case shall a structure exceed fifty (50) feet in height.

- There are eleven single-family residences located north of Greenbrier Avenue that take access from Mission Gorge Road. Following the completion of improvements to Mission Gorge Road, the parking strip adjacent to these homes was replaced with an additional traffic lane. The residents of these homes now experience significant problems in entering and exiting their properties as they are forced to back out of their driveways into the flow of traffic. Due to the effects that the high traffic volumes have on these residences, single-family residential use at this location may no longer be appropriate. Medium density residential (15-22 dwelling units per net residential acre) or low intensity commercial office uses could be appropriate alternatives for these properties if designed to minimize impacts to the adjacent single-family neighborhood. A rezone to permit one of these alternative uses could be considered without the need for an amendment to this Plan, provided the following conditions are met:
 1. The eleven properties are consolidated into one or two parcels.
 2. A planned development permit is approved for the proposed development that addresses the design issues identified below for Area 1 of the Community Plan Implementation Overlay Zone.
 3. The proposed use and site design are compatible with the single-family neighborhood to the southeast. Proposed building elevations and cross sections shall show the relationship of the proposed development to adjacent properties. For informational purposes, the plans shall indicate whether existing views from adjacent properties may be impaired as a result of the project.
 4. The proposed development is designed to minimize traffic impacts to Mission Gorge Road and the single-family neighborhood to the southeast. Access to this site shall be limited to Mission Gorge Road.
- In order to ensure quality site design along Mission Gorge Road, it is recommended that the Type B Community Plan Implementation Overlay Zone (CPIOZ) be applied to the residential properties generally located between Old Cliffs Road and Zion Avenue and abutting Mission Gorge Road. This area is identified as Area 1 on the Grantville/Mission Gorge Road Planning Area Map (see Figure 18). ~~If any additional properties on Mission Gorge Road are rezoned to residential zones, Area 1 should be expanded to include those properties at the time the community plan is amended to change the land use designation.~~
- The development regulations of the underlying zones will not implement the specific

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objectives and proposals of the community plan for this area; however, through the application of the CPIOZ and the requirement for a Type B permit, these objectives can be achieved. The specific issues to be addressed in an application for a Type B permit for this area are listed below.

1. Architectural Design: New development shall be compatible in design with the existing neighborhood. The bulk and scale of new buildings should be similar to the surrounding buildings. Where adjacent development is single-family, large building masses shall be avoided. Several smaller buildings should be used to maintain the pattern of development.
2. Building Height: New development shall be limited to 30 feet in height where adjacent development is single-family.
3. Roof Treatment: Roof forms shall be predominantly sloped. Rooftop ventilation or other mechanical equipment shall be screened from adjacent residential areas and from the public right-of-way.
4. Setbacks, Landscaping and Noise Walls: An extensively landscaped street yard shall be provided for any new residential development along Mission Gorge Road **except for new residential or commercial development where commercial retail, office use or shopkeeper units are located on the ground floor**. If noise walls are proposed, the walls should be well-designed, incorporating articulation, pilasters and other design features to achieve an attractive design. Noise walls shall not be permitted in the setback. In addition, landscaping should be used to soften the appearance of perimeter walls and residential structures from Mission Gorge Road and from adjacent uses.
5. Traffic and Access: New development should be designed to minimize further traffic impacts on Mission Gorge Road.
6. Parking: Parking areas shall be well-screened from Mission Gorge Road using a combination of landscaped berms, tall trees and shrubs. Parking areas shall be located in areas least disruptive to adjacent single-family uses. Tree plantings shall be incorporated throughout the parking area.
7. Streetscape Improvements: New development shall be required to provide sidewalks and undergrounding of utilities on-site and construction of a median along the Mission Gorge Road frontage. The feasibility of landscaping the median in Mission Gorge Road should be studied as new development occurs. Landscaping and paving in the median should continue the pattern established in the existing median on Mission Gorge Road.

Dwelling Unit Density

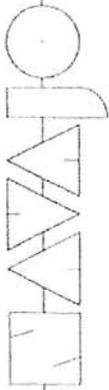
Based upon the proposed land use, which assumes that the canyons and sloped areas will remain open, it is projected that by 1990 the number of dwelling units will increase approximately 32 percent above the 1970 level--an increase of some 4,950 units. Approximately one-half of the new housing units will be in the medium density range of 15-29 dwelling units per acre. By 1990, medium density housing will comprise approximately 25 percent of all residential units, compared to eight percent in 1970.

~~As Navajo continues to evolve, While Navajo will continue to be a relatively low density area,~~ it is proposed that a wide range of residential densities be permitted to develop in the community. This range would include **44-72 (high density)**, 30-43 (medium high density), 15-29 (medium density), 10-14 (low-medium density), 5-9 (low density) and 0-4 (very low density) dwelling units per acre of land. These densities will allow single-family houses, duplexes, townhouses, **condominiums** and apartments which will appeal to a wide segment of the population and provide for a diverse balanced population in the community.

To provide a more complete variety of housing types, the Mobile Home Park Overlay Zone should be retained on the existing mobile home park sites.

Site Design

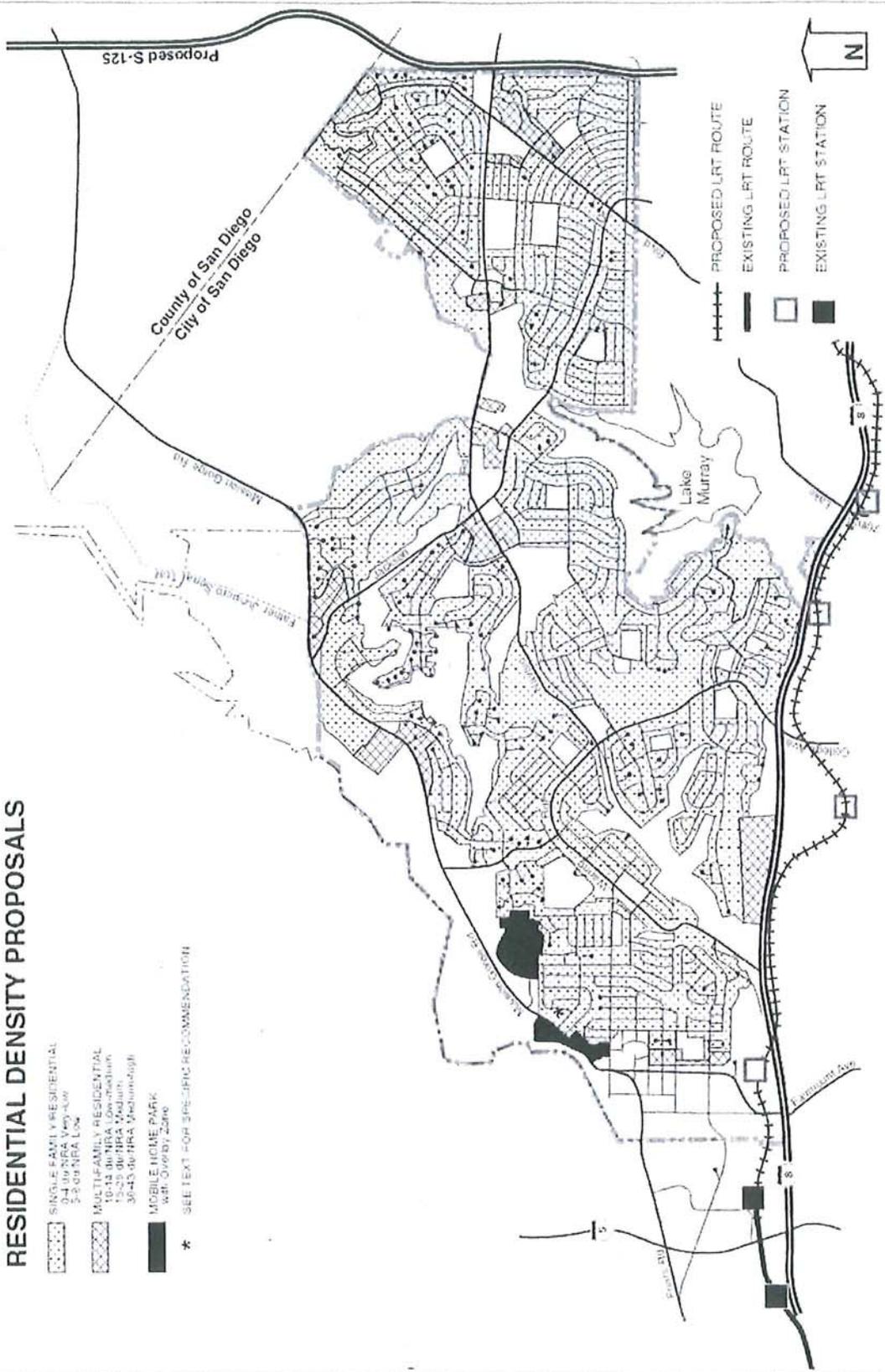
- Fit house to land rather than land to house. Choose the appropriate house plan to fit the basic slope type of the site--up, down, or across slope. Correct selection will minimize grading and preserve the maximum groundcover and trees. Use retaining walls, terraces, split level or platform houses to minimize grading. This would eliminate the need for flat building pads involving extensive earth cuts.
- 
- If earth moving is necessary, re-contour rather than cut and fill. If a new form must be given to the land, the final form should have a strong, smoothly flowing character typical of the existing hills. The basic character of the original site should provide the theme with adjustments to make the slopes gentle. Particular attention should be paid to the transition areas where the existing terrain stops and earthwork begins. Additional shaping in some areas may be necessary due to the unique subsoil and groundwater conditions present.
 - Create privacy for each house and protect its outdoor spaces from view and noise. Develop alternate methods of handling setbacks to increase usable open space such as to minimize narrow, useless side yards, as well as to create an interesting streetscape.

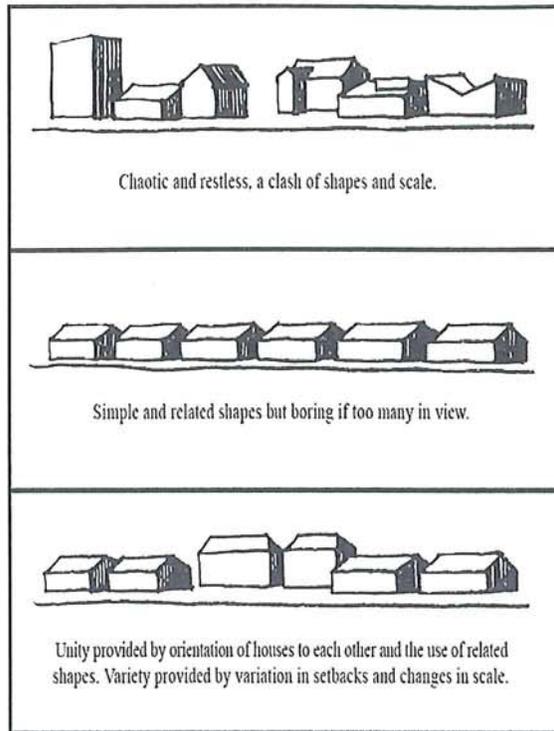


RESIDENTIAL DENSITY PROPOSALS

-  SINGLE FAMILY RESIDENTIAL
 24-30 du/RA Very-low
 25-60 du/RA LG
-  MULTIFAMILY RESIDENTIAL
 10-14 du/RA Low-medium
 15-20 du/RA Medium
 30-45 du/RA Medium-high
-  MOBILE HOME PARK
 with Overlay Zone

* SEE TEXT FOR SPECIFIC RECOMMENDATION





STREETSCAPE

Under this arrangement, the patchwork appearance of most hillside developments would be largely eliminated. Contour fencing could take the form of trellises with vines and other plant materials growing over the side, while still providing a barrier between dwelling units.

- Cluster developments should be encouraged to minimize tampering with the natural topography.
- Outstanding natural physical features such as the highest crest of a hill, natural rock outcroppings, major tree belts, etc., should be preserved at all costs.
- Roads should follow natural courses wherever possible to minimize cutting and grading.
- Imaginative and innovative building techniques should be encouraged to create buildings suited to natural hillside surroundings.
- Detailed and effective arrangements must be formulated for the preservation, maintenance and control of open space and recreational lands resulting from Planned Residential Developments.

- Create harmonious form relationships among houses rather than endless strings of houses. Groups of houses should appear to be related to one another rather than jumbled together without pattern. Strive for consistency within groups of buildings through the use of recurring shapes and materials. All the houses in one eye span should be designed to tie together and relate to one another, yet should not be repetitive and monotonous.

- If hillsides are developed, they should be designed to complement the existing terrain. Hillside developments are usually laid out in a rigid geometrical lot pattern, and thus fences contrast sharply with the natural terrain. A more logical pattern would be for front and rear lots lines to follow the horizontal contours.

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- Apartment developments should be arranged in such a way as to harmonize with adjacent single-family developments. They should be designed to present less apparent bulk and to minimize the clash of scale and activity between apartments and houses.
- Variety in apartment design should be facilitated by introducing optional rear and

side setbacks and a front yard requirement based on floor area rather than an absolute dimension. Variable front yard spaces can give an interesting character to the street in apartment districts. When used, side yard setbacks should be increased from present regulations to better provide for daylight and visual privacy. To assure adequate outdoor space for residents, a minimum percentage of the floor space could be in the form of balconies and landscaped roof terraces,

Residential Street Design

- Streets should be designed and developed to be pleasant places to walk as well as drive. The arrangement of houses should create a pleasant streetscape. Alignment, paving, landscaping and tree planting should all be designed to enhance the visual effect.
- Streets should be designed and developed to be pleasant places to walk as well as drive. The arrangement of houses should create a pleasant streetscape. Alignment, paving, landscaping and tree planting should all be designed to enhance the visual effect.
- Protect residential areas from the noise, pollution and physical danger of excessive traffic. The speed and volume of traffic on residential streets should be limited. Techniques for doing this include making streets discontinuous to divert traffic from a straight path, narrowing streets and intersections, creating the appearance of narrowness through landscaping and other improvements. Where possible, walkways should pass through the interior of blocks. Such changes in streets should be designed so that they will not limit the access of vehicles for police and fire protection and other emergency purposes in the protected areas. The total effect of these changes in residential streets should be to emphasize their residential qualities and encourage pedestrian usage. When major streets and other streets having heavy traffic must go through residential areas, steps should be taken to screen dwellings from the noise, fumes and other adverse effects of traffic.
- Provide buffering for residential properties when heavy traffic cannot be avoided. Heavy landscaping at the side of streets and in center islands may provide an effective barrier, as can walls, differences in elevation and the setting back of dwellings from the roadways.

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Dwellings along streets with heavy traffic should, where possible, have the main orientation of their living space and access away from the traffic. In some cases further measures such as soundproofing may be required. Businesses that attract or produce heavy traffic, such as service stations, should be screened from nearby residential areas. Screening should be provided, as well, for all open parking lots within or adjacent to residential areas. All of the aforementioned considerations should apply to recreation areas as well as to dwellings.

- Underground all utilities. This should be done not only in new subdivisions but also programmed in stages in older parts of the community. With overhead wires out of the way, it is possible to allow street trees to grow; and thereby, establish a more desirable environment.
- Design all curves, intersections and cul-de-sacs and their relationship to houses for the best visual effect. Every opportunity should be taken to make street alignment and other street features contribute positively to good urban design. For example, use should be made of long radius curves connected by short curves in aligning streets rather than long tangents connected by short sharp curves. The former gives a much more sweeping, elegant feeling at eye level while the latter is sharply defined as one enters and leaves the curve.
- Provide the maximum street tree planting. One principal characteristic of memorable streets throughout the world is their tree planting. The finest examples have mature specimens that arch across the street creating a green canopy. From an urban design standpoint, a various tree planting program is the most important single thing that the City can do. Trees should be spaced close enough together to create an effect of enclosure and to provide protection of trees from hot drying winds and sun scald.

COMMERCIAL

EXISTING CONDITIONS

As of 1988, approximately 123 acres of the 155 acres of commercially zoned land are being used for commercial purposes, representing almost two percent of the land in the Navajo community. An additional 90 acres of industrially zoned land are being used for commercial purposes in the Grantville area.

Existing development is typified by businesses, stores and offices which provide goods and services for local consumption. Most commercial development is clustered into shopping centers and is characterized by its community rather than regional nature. Four neighborhood centers provide for the daily shopping needs of the residents. Typical establishments include food markets, service stations, barber shops, beauty parlors, drug, hardware and liquor stores. Sites vary from less than an acre to almost ten acres.



Three community centers provide convenience goods--personal, professional, financial and automotive services--and a limited variety of shopping goods. Establishments include variety stores, apparel and shoe stores, banks, professional offices plus those establishments normally found in neighborhood centers. These centers are located on major streets and are easily accessible from most points in their respective trade areas.

The largest community center, which includes the Navajo and Ralphs shopping centers and adjacent development, is located at the intersection of Navajo Road and Lake Murray Boulevard. The total complex of over 50 business establishments and professional offices covers 35 acres. The other two community centers are located at Waring Road and Orcutt Street and Navajo Road and Jackson Drive. The center at Waring Road and Orcutt Street has over 30 business establishments and professional offices on 12 acres of land. The Navajo Road and Jackson Drive center is 45 acres and has numerous commercial and professional uses in addition to multifamily residential use.

Major concentrations of professional offices are located in the three community shopping centers. Other professional offices are scattered throughout the area usually in conjunction with commercial centers.

Visitor-oriented commercial uses are located at Interstate 8 (I-8) and Waring Road. The

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potential for visitor-oriented facilities in Navajo is very limited.

There are no movie theaters, bowling alleys or other similar forms of commercial recreation in the community. Commercial facilities such as record stores are also absent. The centers serve only commercial functions, rarely being used for community purposes such as art shows and other cultural events.

Grossmont Center, a regional shopping center readily accessible to area residents, emphasizes such shopping goods as apparel, major household appliances and furnishings. It is located just outside the study area in the city of La Mesa. Also easily accessible are Mission Valley establishments and Parkway Plaza in El Cajon, as well as many other adjacent areas. Time distance even to downtown areas is only about 20 minutes.

Mission Gorge Road, a major entry point into the community, contains strip commercial development with a mix of land uses. The visual clutter created by numerous curb cuts, unscreened parking areas, excessive signs and billboards, and above ground utilities, as well as the condition of much of the development along Mission Gorge Road does not project a positive impression of the community. In addition, neighborhood centers along Mission Gorge Road have developed without regard to other development, resulting in a lack of coordinated design.

The commercial centers are constructed for the convenience of the automobile and not the shopper, which is accentuated by the lack of other forms of transportation within the community. The commercial buildings, if built by a chain, often follow some standard facade treatment that may not relate to the character of the particular site in which it is placed. Promotional and store signs are generally geared to a through traffic, high pressure merchandising situation rather than a neighborhood situation. Almost without exception landscaping of the sites is at a minimum and frequently not maintained. In particular, parking lots consist of an expanse of unrelieved asphalt and are often laid out so that shoppers are forced to thread their way between parked cars.

Table 2 contains site and trade population standards for neighborhood, community, and regional shopping centers according to the Progress Guide and General Plan for San Diego.

**TABLE 2
SITE AND TRADE POPULATION STANDARDS FOR COMMERCIAL CENTERS**

Criteria	Neighborhood	Community	Regional
Population in Trade Area	2,000-10,000	10,000-25,000	100,000 or more
Acres/1000 Population	1.0	.8	.7
Site Area	1-10 acres	8-20 acres	50 acres or more

The development potential for commercial facilities that serve Navajo residents is based on these standards as applied to the projected 1990 population of 65,000 to 70,000. Accordingly, the area could support six or seven neighborhood centers with a combined area of approximately 55 acres and two or three community centers with a combined area of approximately 60 acres. Commercial facilities in adjacent communities, particularly La Mesa

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and El Cajon, however, serve Navajo and reduce the need for commercial land within the planning area.

OBJECTIVES

The principal or overriding objective for long-range commercial development in Navajo is to ENCOURAGE NEIGHBORHOOD AND COMMUNITY SHOPPING FACILITIES WHICH ARE ADEQUATE TO PROVIDE A WIDE VARIETY OF GOODS AND SERVICES TO NAVAJO, BUT ALSO BLEND INTO AND ENHANCE THE COMMUNITY ENVIRONMENT.

To achieve this principal objective, the following additional objectives were also adopted:

- Develop commercial areas as centers for community activities. Such centers should include community facilities, such as cultural, recreational, entertainment and residential facilities as well as retail establishments and professional offices.
- Develop commercial areas that have desirably distinctive qualities in their design, appearance and operation.
- Provide neighborhood convenience centers that are complementary to adjacent residential areas and strategically located throughout the residential areas of the community, preferably near public facilities. These shops should be accessible to pedestrians and bicyclists and be in scale and character with the neighborhoods they serve. In addition, existing centers should be encouraged to add safe facilities for pedestrians and bicyclists.
- Prevent the overdevelopment of any one type of commercial use (for example, service stations).
- Prohibit the expansion of strip commercial development on Mission Gorge Road north of Zion Ave.
- Restrict retail development to **areas designated for** commercial and mixed use ~~ly-designated areas~~; limit commercial office and service uses in the industrially designated areas to those that are accessory to industrial uses.
- Improve the appearance of the existing strip commercial development on Mission Gorge Road between I-8 and Zion Avenue by reducing signs, improving landscaping and architectural design, providing consistent building setbacks and providing adequate off-street parking.
- Limit the development of drive-thru restaurants to sites that can accommodate the stacking of vehicles, as well as accommodate driveways in a manner that will not conflict with the smooth operation of intersections.
- Reduce the number of curb cuts serving individual commercial uses on Mission Gorge

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Road to minimize traffic conflicts and provide a continuous sidewalk and landscape strip.

PROPOSALS

General

- In addition to retail stores necessary to accommodate the needs of the community, commercial centers should also provide for professional and business offices, entertainment and cultural activities, public and semipublic facilities, and residential uses. The existing centers, due to their location, size, and the character of adjacent development, could easily be improved to meet these criteria.
- All the centers should be accessible by pedestrians, bicycles and adequate public transportation as well as by car.
- The shopping center at Lake Murray Boulevard and Navajo Road, the community shopping center at Waring Road and Orcutt Avenue, and the center at Navajo Road and Jackson Drive are to be retained as community commercial centers. These establishments should fulfill the need for convenience goods and personal, professional, financial and recreational services through the year 2000.
- Neighborhood commercial centers should be retained at the intersections of Golfcrest Drive and Mission Gorge Road, Conestoga Road at Mission Gorge Road, and Zion Avenue at Mission Gorge Road. The centers should be an integral part of the residential development and geared to accommodate pedestrian and bicycle oriented trade in addition to the automobile.
- Specialized commercial services are to be retained along Mission Gorge Road in the Grantville area.
- Professional offices are to be retained at existing locations. The expansion of professional offices is recommended at all community shopping centers.
- Visitor-oriented facilities (hotels, motels, and associated uses) should be limited to those existing at the intersections of I-8 and Waring Road, and Mission Gorge Road and Alvarado Canyon Road. No additional visitor-oriented facilities should be developed.
- The number and location of service stations should continue to be regulated. No more than one station should be located at an intersection and the overall number of stations should be based on service to the community. Existing facilities appear to be sufficient to serve community needs through the year 2000.
- The removal of off-premise signs and the consolidation of multiple on-premise signs should be pursued during project reviews in an effort to reduce sign clutter and enhance the visual appearance of Mission Gorge Road.

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- Any rezones or tentative maps for new commercial center development and redevelopment should require processing in accordance with the planned commercial development regulations to ensure comprehensive review of the center and its compatibility with adjacent development.
- Mission Gorge Road has been identified as a major entry point into the Navajo Community; however, development along this corridor lacks the quality of design necessary to provide a positive impression of the community. This is due in part to the limited design standards of the underlying C-1 and CA zones. Therefore, the following design standards are provided as supplemental development regulations to the C-1 and CA zones in Grantville along Mission Gorge Road. This area is indicated as Area 2 on the Grantville/Mission Gorge Road Area Map (see Figure 18).

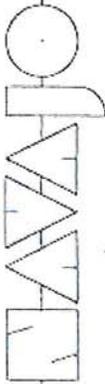
Community Plan Implementation Overlay Zone (CPIOZ)

Compliance with these regulations will be determined through a ministerial (Type A) Planning Director review in accordance with the procedures of the Community Plan Implementation Overlay Zone (Municipal Code Section 101.0457).

Development proposals that do not comply with the following supplemental regulations and the regulations of the underlying C-1 or CA zone, as appropriate, shall apply for a discretionary (Type B) CPIOZ permit or a planned commercial development (PCD) permit. Applications for a Type B CPIOZ permit or a PCD permit shall meet the purpose and intent of the regulations of the underlying zone and the supplemental regulations. Deviations from these regulations may be granted by the Planning Director in accordance with the procedures of the Community Plan Implementation Overlay Zone (Municipal Code Sections 101.0457.D through G).

Within the commercially zoned area in Grantville, no building, improvement, or portion thereof shall be erected, constructed, converted, altered, enlarged or established until a CPIOZ permit is obtained. A CPIOZ permit is not required for any of the following:

- a) Interior improvements to an existing building that do not involve a change in use or provide additional floor area;
- b) Interior modifications or repairs, or exterior repairs or maintenance for which a building permit is not required;
- c) Demolition, excavation, grading, or removal of vegetation;
- d) Any development processed as a planned commercial development.



COMMERCIAL PROPOSALS

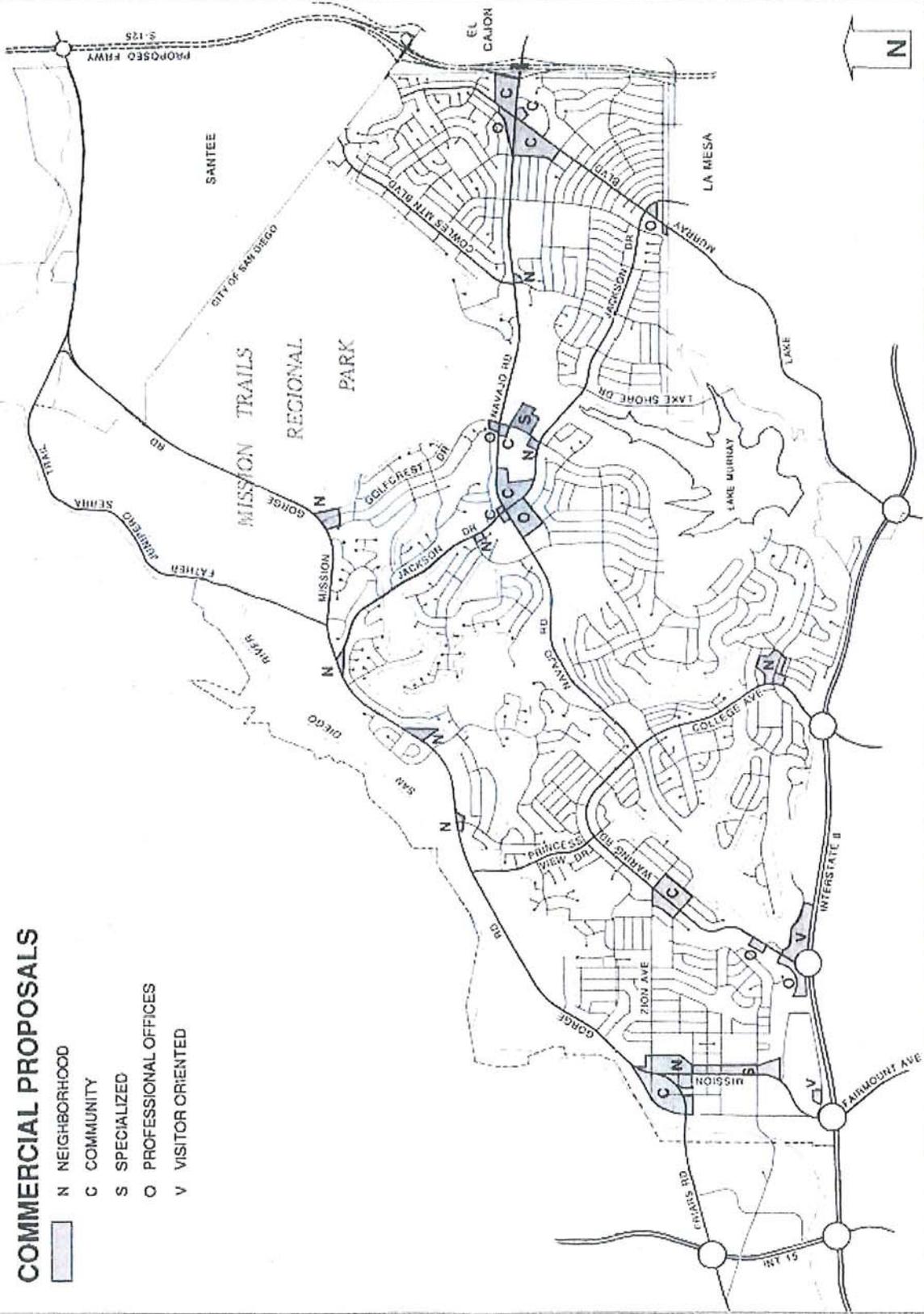
■ NEIGHBORHOOD

C COMMUNITY

S SPECIALIZED

O PROFESSIONAL OFFICES

V VISITOR ORIENTED



Supplemental Development Regulations

Floor Area Ratio (FAR):

New development processed under ministerial (CPIOZ Type A) review shall not exceed a floor area ratio of 1.0. Development proposals that exceed a floor area ratio of 1.0 shall apply for a discretionary permit. The discretionary review process should address: 1) the design and fit of the project in relation to surrounding development, including conformance with the design guidelines of this Commercial Element; 2) the ability of Mission Gorge Road and adjacent streets in Grantville to accommodate additional travel demand; and 3) promotion of the Employer Transit Assistance Program (ETAP) in which employers subsidize monthly transit passes for employees to encourage transit use. The program is administered through Metropolitan Transit Development Board (MTDB) and Ridelink.

Building Setback Adjacent to the River:

From the edge of the floodway 35 feet shall be reserved for the San Diego River Park Path Corridor and an additional 10 feet minimum building setback shall be observed. Building height shall be stepped back relative to the edge of the 35 foot-wide Path Corridor as follows: building height shall be 25 feet maximum at 10 foot setback, 35 feet maximum at 20 foot setback, 65 feet at 30 foot setback, 70 feet at 35 foot setback and after a distance of 35 feet from the Path Corridor, the building height shall step back at a ratio of one foot for each foot of building height. After a distance of 200 feet from the River Corridor the building height is governed by the underlying zone. All structures within 150 feet of the San Diego River's 100-year floodway shall be designed to step back from the floodway so that low-story buildings are adjacent to the river with the higher stories tiered away from the river. Buildings shall be set back or stepped back from the floodway at a ratio of one foot for each foot of building height with a minimum setback of 20 feet.

Building Setback Adjacent to Mission Gorge Road:

All structures on Mission Gorge Road shall observe a minimum ten-foot setback. Structures over 30 feet in height shall be set back or stepped back an additional one foot for each foot of building height over 30 feet.

Building Transparency:

For any building facade which faces a public street, at least 40 percent of the total area of all building walls must be devoted to pedestrian entrances, display windows, or windows affording views into retail, customer services, office, gallery or lobby space.

Building Reflectivity:

No more than 30 percent of any single elevation of a building's exterior may be constructed of a material with a light reflectivity factor greater than 25 percent.

Equipment Enclosure:

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All mechanical equipment and appurtenances shall be screened on all sides so that they appear to be an integral part of the overall architectural design of the building. The screening may include grillwork, louvers, or latticework.

Wind generated turbines shall not be screened but shall be painted to match the rooftop color.

No merchandise, material or equipment shall be stored or displayed on the roof of any building.

Outdoor Storage and Display:

Outdoor storage areas shall be located in interior side or rear yards only, except that no outdoor storage area shall be located between the building wall line and the San Diego River.

Outdoor storage areas shall be screened with a solid six-foot fence or wall or an enclosed structure. All such fences, walls or structures shall be of a similar material and color as the main building. No material or equipment shall exceed the height of the fence, wall or structure.

Outdoor display of the following merchandise sold on the premises shall not be subject to the storage requirements above but shall meet the landscaping requirements for vehicular use areas of the Citywide Landscape Ordinance:

Automobiles (usable)
Trailers
Artwork and pottery

Boats (usable)
Equipment and tools
Flowers and plants

Loading Areas:

Loading and service areas shall be located in interior side or rear yards only, except that no loading or service area shall be located between the building wall line and the San Diego River.

Refuse Collection Areas:

Refuse collection areas shall be located in interior side or rear yards only, except that no refuse collection area shall be located between the building wall line and the San Diego River.

Refuse collection areas shall be screened with a solid six-foot fence or wall or an enclosed structure. All such fences, walls or structures shall be of a similar material and color as the main building. Deposited refuse shall not be visible from outside the refuse screening.

Parking Requirements:

No parking area shall be located between the building wall line and the San Diego River unless a landscape buffer is provided between the parking area and the required setback from the river. The landscape buffer shall be a minimum of eight feet wide and shall be planted in

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accordance with Sections 101.0706.A.6 and C of the Citywide Landscape Ordinance; however, in no case shall the minimum landscape area width of eight feet be reduced by the use of a site wall.

Off-street parking in the C-1 Zone shall be provided by use as follows:

	<u>Spaces/Square Feet of Gross Floor Area</u>
For business and professional offices	1/300
For medical and dental facilities	1/250
For commercial uses that take access from Mission Gorge Road	1/250
For all other commercial uses	1/300

These parking requirements shall replace the parking requirements of the C-1 Zone (Municipal Code Section 101.0428E). If the citywide commercial parking requirements are revised, the new standards will replace those identified above.

Curb Cuts and Driveways:

On Mission Gorge Road, south of Friars Road, one curb cut shall be permitted for each lot with frontage on Mission Gorge Road. One additional curb cut may be permitted for each 150 feet of frontage on Mission Gorge Road. No driveway shall exceed a width of 25 feet measured at the property line.

On Mission Gorge Road, north of Friars Road, curb cuts shall be in conformance with the Street Design Manual standards for primary arterials.

Pedestrian Access:

A continuous ten-foot minimum width pedestrian/bicycle path shall be provided along the San Diego River frontage within the 20-foot minimum setback.

All structures within 150 feet of the San Diego River's 100-year floodway shall provide at least one pedestrian entrance from the structure to the river path.

Signs:

Signs shall be in conformance with the Citywide Sign Regulations (Municipal Code Section 101.1100) with the following exceptions:

On Mission Gorge Road, ground signs shall not exceed the height of the building or the citywide height limit of 30 feet, whichever is less, unless they meet the definition of freeway oriented signs in Municipal Code Section 101.1101.56.

A landscaped area shall be provided at the base of all ground signs. The size of the landscaped area shall be equal to or greater than the area of the sign face.

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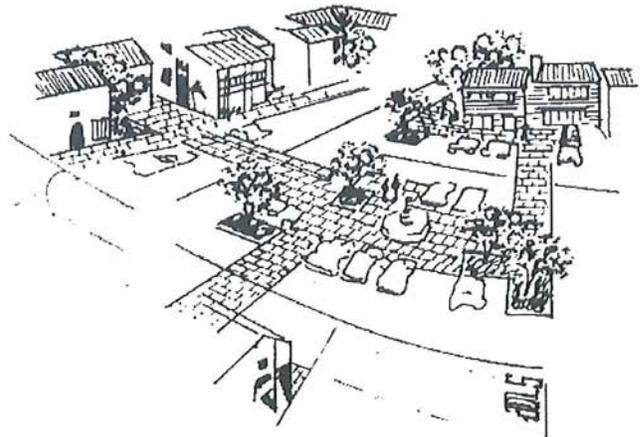
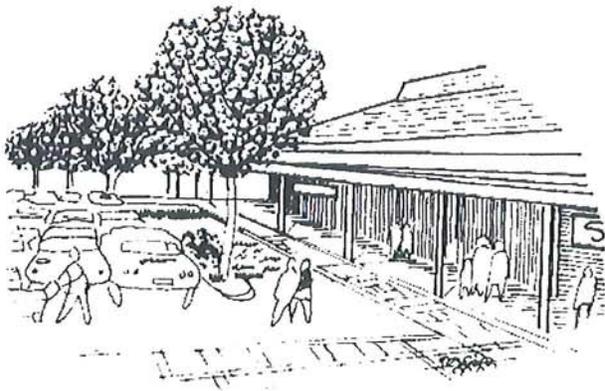
Landscaping:

Landscaping shall be provided as required by the Citywide Landscape Ordinance. For the streets identified on the next page, trees in the public right-of-way shall be chosen from the following species to be consistent with the predominant species in the area.

Street	Tree Species	
	Botanical Name	Common Name
Friars Road	<i>Platanus acerifolia</i>	London Plane
	<i>Liquidambar styraciflua</i>	Sweetgum
	<i>Platanus racemosa</i>	California Sycamore
Mission Gorge Road, northeast of Friars Road	<i>Platanus acerifolia</i>	London Plane
	<i>Liquidambar styraciflua</i>	Sweetgum
San Diego River	<i>Platanus racemosa</i>	California Sycamore
	<i>Populus fremontii</i>	Cottonwood
	<i>Salix hindsiana</i>	Black Willow
	<i>Sambucus</i>	Elderberry
Mission Gorge Road, south of Friars Road	<i>Liquidambar styraciflua</i>	Sweetgum
	<i>Jacaranda mimosifolia</i>	Jacaranda
	<i>Koelreuteria paniculata</i>	Goldenrain Tree
Riverdale Street	<i>Liquidambar styraciflua</i>	Sweetgum
	<i>Jacaranda mimosifolia</i>	Jacaranda
	<i>Cupaniopsis anacardioides</i>	Carrotwood
Twain Avenue	<i>Pinus canariensis</i>	Canary Island Pine
	<i>Koelreuteria paniculata</i>	Goldenrain Tree
Alvarado Canyon Road	<i>Koelreuteria paniculata</i>	Goldenrain Tree
	<i>Liquidambar styraciflua</i>	Sweetgum
Fairmount Avenue	<i>Liquidambar styraciflua</i>	Sweetgum
	<i>Jacaranda mimosifolia</i>	Jacaranda
	<i>Koelreuteria paniculata</i>	Goldenrain Tree
Vandever Avenue	<i>Liquidambar styraciflua</i>	Sweetgum
	<i>Jacaranda mimosifolia</i>	Jacaranda
	<i>Koelreuteria paniculata</i>	Goldenrain Tree

Design

In this example of a proposed design treatment, an effort has been made to reconcile the character of the commercial center with that of the surrounding neighborhood. An arcade has been introduced as one means of providing a human scale at the edge of the building as well as providing sun control, weather protection and an organized framework for signs. Materials have been chosen to blend in with those found in the environs to further lessen the clash of scale. Signs, lighting and displays have been limited to those necessary and compatible with the center's surroundings. The introduction of decorative paving and planting make the center a pleasant place to visit. Bold colors and handsome graphics advertise the center yet do not overpower the surroundings. A major improvement is made in the parking lot. Large trees are provided, either existing trees that have been saved or new plantings. These trees break up the hot expanse of parked cars with pools of shade. Denser tree plantings are provided around the street side of the site. In this example, the parking rows are laid out so that the aisles lead conveniently toward the stores.

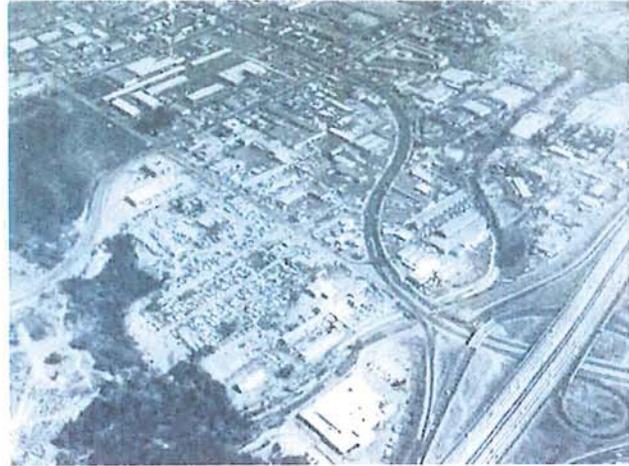


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INDUSTRIAL

EXISTING CONDITIONS

As of 1988, approximately 238 acres within the Navajo community were zoned for industrial development. The industrial acreage, all of which is located in the Grantville/Mission Gorge Road area, represents three percent of the total land area in the community.



Sand and gravel extraction was once the predominant industrial use in Grantville, accounting for most of the industrially zoned land along the San Diego River as well as most of the agriculturally zoned land. The manufacturing of cement block, brick, and associated secondary

sand and gravel uses were also prevalent near the river. Most of the sand and gravel activities have now been discontinued and are being replaced with multi-tenant buildings.

A 250-acre site is still being used for sand and gravel processing on the north side of Mission Gorge Road, generally between Princess View Drive and Margerum Avenue. This quarry has been in operation since 1927 and is currently operating under a Conditional Use Permit (CUP) that expires in 2033. The CUP regulates the mining, processing, storage and sale of natural resource material. A master reclamation plan for the 250 acres covered within the CUP, as well as 170 acres from which sand deposits have previously been mined, establishes goals and general guidelines for the reclamation of the project area upon completion of mining activity. Final reclamation is to be accomplished in phases with the approval of precise reclamation plans.

Because of the central location of Grantville within the metropolitan area and its proximity to the interstate highway system, the 1973 Navajo Community Plan recommended that the Fairmount Avenue area be developed as the nucleus of a distribution complex. A number of warehousing, wholesaling, and distributive uses are located in this area, however the largest of these uses, the 7-Up bottling plant, has moved out of the area. A 114,000-square-foot Kaiser medical office facility and a 530-car parking structure are under construction on the site of the former 7-Up plant.

Much of the land in Grantville that is designated for industrial use has been developed with office and retail uses due to the multi-tenant office and commercial uses permitted in the M-1A Zone. Typical uses within the Grantville's industrial area are real estate and other business service uses; heating, plumbing, and related services; engineering and electrical services; small equipment manufacturing and repair; building contractors; wholesale and retail trade; and auto sales and repair.

Industrial Zoning

Approximately 148 acres of industrial land are in the M-1A Zone and approximately 90 acres are in the M-1B Zone. The M-1A Zone permits a broad range of light industrial uses as well as retail commercial and office uses allowed in the M-1 and C (commercial) zones. The M-1B Zone permits light industrial uses and a limited number of heavy commercial uses (auto sales, lumber yards, garden supplies, etc.); retail uses are limited to those that are accessory to manufacturing, warehousing or distribution of products on the same premises.

The M-1A and M-1B zones were applied to Grantville in the mid-1970s to replace M-1 zoning. The M-1 Zone is a permissive industrial zone, permitting most commercial uses as well as residential uses, with minimal development standards. Much of the development that occurred under the M-1 Zone, or prior to 1954 when the majority of Grantville was annexed to the City, has little or no off-street parking, landscaping, or setbacks, and is typically on small lots.

Requirements for landscaping, screening, and off-street parking areas have been increased in the M-1A and M-1B zones, and development that has occurred more recently under these zones has improved the appearance of Grantville. However, the M-1A and M-1B development standards were originally designed for low intensity (typically one story) industrial uses and are not adequate for multi-story office or multi-tenant development.

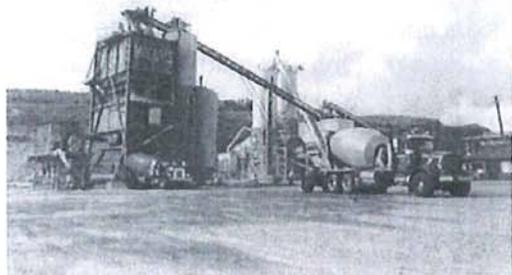
Both the M-1A and M-1B zones permit a variety of nonindustrial uses which may have greater parking requirements than are specified in these zones. The M-1A and M-1B zones require that 40 to 50 percent of the lot area be devoted to parking, circulation, and loading. This requirement results in large expanses of pavement while not necessarily providing an appropriate amount of parking based on the needs of the particular uses. (Descriptions of industrial zone requirements are as of 1988.)

OBJECTIVES

The principal or overriding industrial objective to guide the long-range development of Navajo is to: ENCOURAGE INDUSTRIAL DEVELOPMENT AND REDEVELOPMENT WHICH CAPITALIZE ON THE AREA'S CENTRAL LOCATION WITHIN THE METROPOLITAN AREA.

To achieve this principal objective, the following additional objectives were also adopted:

- Ensure that the appearance and effects of industrial uses are compatible with the character of the surrounding residential and commercial areas and the sensitive resources of the San Diego River.
- Ensure that industrial development along the San Diego River is designed to minimize impacts to this sensitive resource.
- Develop a circulation network that will provide for less congested access to the Grantville industrial area.
- Ensure the provision of adequate off-street parking and loading facilities for new uses in proportion to the need of the particular uses.



PROPOSALS

Based on the existing conditions and objectives identified above, the following general proposals were established to encourage industrial development that is compatible with the residential character of the Navajo community.

- A Planned Industrial Development (PID) or master PID should be required for any property rezoned to an industrial zone to ensure quality site design, and compatibility with the San Diego River environment and surrounding residential areas where appropriate. A master PID establishes design guidelines and standards to be used in the review of subsequent detailed site-specific projects. The PID document shall include a conceptual site plan showing the general location of proposed uses; a list of permitted uses; a preliminary grading plan; and master circulation, landscaping and sign plans. After approval of the master PID, projects will be reviewed and approved ministerially by the Planning Department if they are found to be consistent with the approved master PID. The design guidelines identified in this Industrial Element shall be consulted in the design and review of the master PID.
- Future development of the remaining sand and gravel operation and the previously mined 170 acres should be accomplished under a master planned industrial development (PID) permit process. This property is bounded on the south by a major element of the regional transportation network (Mission Gorge Road) and on the north by the San Diego River, a regionally significant natural resource. A master PID will provide an opportunity for comprehensive review of the relationship between proposed development and the ultimate reclamation plan for the San Diego River, coordination of open space and pathways with Mission Trails Regional Park, traffic impacts to Mission Gorge Road and the proposed State Highway 52 interchanges.
- The area south of Mission Gorge Road generally between Old Cliffs Road and Princess View Drive is designated for development as an industrial park complex. This property is highly visible from Mission Gorge Road, as well as from the residential areas above the river basin in Allied Gardens and Tierrasanta. Future development under the M-1B Zone is recommended; however, a planned industrial development permit should be required to ensure coordinated, quality design and compatibility with the residential uses adjacent to this site.
- The permitted uses in the Grantville industrial area south of Friars Road and near the Mission Gorge Road and Princess View Drive intersection should continue to be those of the M-1A and M-1B zones; however, the development standards of these zones are not adequate to ensure that new development will meet the objectives of this community plan. The following design standards are provided as supplemental development regulations to the M-1A and M-1B zones for the area indicated as Area 3 on the Grantville/Mission Gorge Road area map (see Figure 18).

Community Plan Implementation Overlay Zone (CPIOZ)

Compliance with these regulations will be determined through a ministerial (Type A) Planning Director review in accordance with the procedures of the Community Plan Implementation Overlay Zone (Municipal Code Section 101.0457).

Development proposals that do not comply with the following supplemental regulations and the regulations of the underlying zone shall apply for a discretionary (Type B) CPIOZ permit or a Planned Industrial Development (PID) permit. Applications for a Type B CPIOZ permit or a PID permit shall meet the purpose and intent of the regulations of the underlying zone and the supplemental regulations. Deviations from these regulations may be granted by the Planning Director in accordance with the procedures of the Community Plan Implementation Overlay Zone (Municipal Code Sections 101.0457.D through G).

Within the industrially zoned area in Grantville, no building, improvement, or portion thereof shall be erected, constructed, converted, altered, enlarged or established until a CPIOZ permit is obtained. A CPIOZ permit is not required for any of the following:

- a) Interior improvements to an existing building that do not involve a change in use or provide additional floor area;
- b) Interior modifications or repairs, or exterior repairs or maintenance for which a building permit is not required;
- c) Demolition, excavation, grading, or removal of vegetation; and
- d) Any development processed as a planned industrial development.

Supplemental Development Regulations

Floor Area Ratio (FAR):

New development processed under ministerial (CPIOZ Type A) review shall not exceed a floor area ratio of 1.0. Development proposals that exceed a floor area ratio of 1.0 shall apply for a discretionary permit. The discretionary review process should address; 1) the design and fit of the project in relation to surrounding development, including conformance with the design guidelines of this Industrial Element; 2) the ability of Mission Gorge Road and adjacent streets in Grantville to accommodate additional travel demand; and 3) the opportunity for instituting travel demand management strategies such as participation in a transportation management association.

Building Setback Adjacent to the River:

From the edge of the floodway 35 feet shall be reserved for the San Diego River Park Path Corridor and an additional 10 feet minimum building setback shall be observed. Building height shall be stepped back relative to the edge of the 35 foot-wide Path Corridor as follows: building height shall be 25 feet maximum at 10 foot setback, 35 feet maximum at 20 foot setback, 65 feet at 30 foot setback, 70 feet at 35 foot setback and after a distance of

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35 feet from the Path Corridor, the building height shall step back at a ratio of one foot for each foot of building height. After a distance of 200 feet from the River Corridor the building height is governed by the underlying zone. ~~All structures within 150 feet of the San Diego River's 100-year floodway shall be designed to step back from the floodway so that low-story buildings are adjacent to the river with the higher stories tiered away from the river. Buildings shall be set back or stepped back from the floodway at a ratio of one foot for each foot of building height with a minimum setback of 20 feet.~~

Offsetting Planes and Facade Variations:

Any building facade which faces a public street or the San Diego River shall have a minimum of three offset building planes or three distinct building facade variations, or a combination of offset building planes and facade variations which meets the intent of this requirement.

An offset building plane is distinguished by an average horizontal or vertical difference of two feet measured perpendicular to the adjacent plane. Each offset plane shall constitute at least 20 percent, but not more than 50 percent of each building facade.

A building facade variation is distinguished by a distinct change in materials, textures, colors, or any combination thereof. Each variation shall constitute at least 20 percent, but not more than 50 percent of each building facade.

Building Reflectivity:

No more than 30 percent of any single elevation of a building's exterior may be constructed of a material with a light reflectivity factor greater than 25 percent.

Equipment Enclosure:

All mechanical equipment and appurtenances shall be screened on all sides so that they appear to be an integral part of the overall architectural design of the building. The screening may include grillwork, louvers, or latticework.

Wind generated turbines shall not be screened but shall be painted to match the rooftop color.

No merchandise, material or equipment shall be stored or displayed on the roof of any building.

Outdoor Storage and Display:

Outdoor storage areas shall be located in interior side or rear yards only, except that no outdoor storage area shall be located between the building wall line and the San Diego River.

Outdoor storage areas shall be screened with a solid six-foot fence or wall or an enclosed structure. All such fences, walls or structures shall be of a similar material and color as the main building. No material or equipment shall exceed the height of the fence, wall or structure.

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Outdoor display of the following merchandise sold on the premises shall not be subject to the storage requirements above but shall meet the landscaping requirements for vehicular use areas of the Citywide Landscape Ordinance:

Automobiles (usable)	Boats (usable)
Trailers	Equipment and tools
Artwork and pottery	Flowers and plants

Refuse Collection Areas:

Refuse collection areas shall be located in interior side or rear yards only, except that no refuse collection area shall be located between the building wall line and the San Diego River.

Refuse collection areas shall be screened with a solid six-foot fence or wall or an enclosed structure. All such fences, walls or structures shall be of a similar material and color as the main building. Deposited refuse shall not be visible from outside the refuse screening.

Loading Areas:

Loading and service areas shall be located in interior side or rear yards only, except that no loading or service area shall be located between the building wall line and the San Diego River.

Parking Requirements:

No parking area shall be located between the building wall line and the San Diego River unless a landscape buffer is provided between the parking area and the required setback from the river. The landscape buffer shall be a minimum of eight feet wide and shall be planted in accordance with Sections 101.0706.A.6. and C. of the Citywide Landscape Ordinance; however, in no case shall the minimum landscape area width of eight feet be reduced by the use of a site wall.

Off-street parking shall be provided by use as follows:

	<u>Spaces/Square Feet of Gross Floor Area</u>
For wholesale, distribution and manufacturing uses	1/1500
For business and professional offices	1/300
For medical and dental facilities	1/250
For commercial uses that take access from Mission Gorge Road	1/250
For all other commercial uses	1/300

These parking requirements shall replace the parking area requirement of the M-1B and M-1A zones (Municipal Code Sections 101.0435.2E and 101.0436B). If the citywide industrial parking requirements are revised, the new standards will replace those identified above.

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Curb Cuts and Driveways:

On Mission Gorge Road, south of Friars Road, one curb cut shall be permitted for each lot with frontage on Mission Gorge Road. One additional curb cut may be permitted for each 150 feet of frontage on Mission Gorge Road. No driveway shall exceed a width of 25 feet measured at the property line.

On Mission Gorge Road, north of Friars Road, curb cuts shall be in conformance with the Street Design Manual standards for primary arterials.

Pedestrian Access:

A continuous ten-foot minimum width pedestrian/bicycle path shall be provided along the San Diego River frontage within the 20-foot minimum setback.

All structures within 150 feet of the San Diego River's 100-year floodway shall provide at least one pedestrian entrance from the structure to the river path.

Signs:

Signs shall be in conformance with the Citywide Sign Regulations (Municipal Code Section 101.1100) with the following exceptions:

Ground signs other than monument signs shall not be permitted in the industrial zones except on lots that have frontage on Mission Gorge Road or the southerly 500 feet of Fairmount Avenue (i.e., 500 feet south of the intersection of Mission Gorge Road and Fairmount Avenue). Monument signs shall not exceed a height of six feet.

On Mission Gorge Road, ground signs shall not exceed the height of the building or the citywide height limit of 30 feet, whichever is less, unless they meet the definition of freeway oriented signs in Municipal Code Section 101.1101.56.

A landscaped area shall be provided at the base of all ground signs. The size of the landscaped area shall be equal to or greater than the area of the sign face.

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Landscaping:

Landscaping shall be provided as required by the Citywide Landscape Ordinance. For the streets identified below, trees in the public right-of-way shall be chosen from the following species to be consistent with the predominant species in the area.

Street	Tree Species	
	Botanical Name	Common Name
Friars Road	<i>Platanus acerifolia</i>	London Plane
	<i>Liquidambar styraciflua</i>	Sweetgum
	<i>Platanus racemosa</i>	California Sycamore
Mission Gorge Road, northeast of Friars Road	<i>Platanus acerifolia</i>	London Plane
	<i>Liquidambar styraciflua</i>	Sweetgum
San Diego River	<i>Platanus racemosa</i>	California Sycamore
	<i>Populus fremontii</i>	Cottonwood
	<i>Salix hindsiana</i>	Black Willow
	<i>Sambucus</i>	Elderberry
Mission Gorge Road, south of Friars Road	<i>Liquidambar styraciflua</i>	Sweetgum
	<i>Jacaranda mimosifolia</i>	Jacaranda
	<i>Koelreuteria paniculata</i>	Goldenrain Tree
Riverdale Street	<i>Liquidambar styraciflua</i>	Sweetgum
	<i>Jacaranda mimosifolia</i>	Jacaranda
	<i>Cupaniopsis anacardioides</i>	Carrotwood
Twain Avenue	<i>Pinus canariensis</i>	Canary Island Pine
	<i>Koelreuteria paniculata</i>	Goldenrain Tree
Alvarado Canyon Road	<i>Koelreuteria paniculata</i>	Goldenrain Tree
	<i>Liquidambar styraciflua</i>	Sweetgum
Fairmount Avenue	<i>Liquidambar styraciflua</i>	Sweetgum
	<i>Jacaranda mimosifolia</i>	Jacaranda
	<i>Koelreuteria paniculata</i>	Goldenrain Tree
Vandever Avenue	<i>Liquidambar styraciflua</i>	Sweetgum
	<i>Jacaranda mimosifolia</i>	Jacaranda
	<i>Koelreuteria paniculata</i>	Goldenrain Tree

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Design Guidelines

The following design guidelines will be used in the review of discretionary projects:

- Architecture, building color and texture should be coordinated within larger industrial developments. In smaller developments similar or complementary architectural elements should be used to provide continuity between existing and new developments.
- Building design should include variations in wall texture, color or material, variations in upper floor setbacks and the use of varied roof forms. All buildings should incorporate some form of shadow relief where pop-outs, offsetting planes, overhangs, and recesses are used to add visual interest. Large, unbroken expanses of wall should be avoided.
- Exterior building walls should be constructed of durable, permanent materials such as textured concrete, stone, brick, stucco, wood or glass. Reflective glass should not be used, particularly in areas adjacent to the San Diego River or heavily traveled roadways because of problems with reflected heat and glare.
- The rear elevations of buildings that face the San Diego River or are visible from the street should be as well detailed and visually interesting as the front elevations.
- Industrial developments should consist of several smaller buildings rather than large building masses to prevent the appearance of a wall of development along the street, particularly along Mission Gorge Road.
- Building height should be restricted where industrially zoned property abuts residential property to reduce impacts to the residential areas.
- All outdoor storage, refuse collection, and loading areas should be located in interior side or rear yards. Where industrial development abuts residentially zoned property, special consideration shall be given to locating these facilities in areas least disruptive to adjacent residential uses. Where industrial development abuts the San Diego River these facilities shall not be located between the building and the river.
- Because building roofs are visible from surrounding residential properties, they should be carefully designed. Roof-mounted equipment should be avoided. If roof-mounted equipment is provided, all equipment and appurtenances shall be designed so that they appear to be an integral part of the overall architectural design of the building.
- Multi-building industrial development should provide a coordinated sign program. Pole signs should not be permitted in the industrial zones except on Mission Gorge Road, south of Twain Avenue, where development is more commercial in nature. Monument and wall signs should be used instead of pole signs in the industrial areas.
- Fences should be constructed of wood, masonry, wrought iron, or a wood-masonry combination. Fencing should use pilasters, offsets or some other form of visual relief to break up the linear nature of the fence.

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- Curb cuts should be minimized by the use of common, joint use driveways and/or consolidation of lots.

The following additional guidelines are provided for new industrial development on both sides of Mission Gorge Road, from just south of Old Cliffs Road to Margerum Avenue:

- A 25-foot landscaped setback should be provided along Mission Gorge Road. Landscaped parkways should be provided between the sidewalk and the curb on Mission Gorge Road and on any interior circulation system. A landscaped median should be provided in Mission Gorge Road. Paving patterns and landscaping should be consistent with the existing medians to the south on Mission Gorge Road. Maintenance should be assured through formation of an assessment district or a similar mechanism.
- Access and circulation design should provide continuous pedestrian and bicycle access along public streets and to uses within the development. Bicycle parking facilities should be conveniently located near the entrances of buildings, without blocking pedestrian traffic. No parking areas or driveways should be located between the structures and Mission Gorge Road.
- Development adjacent to the San Diego River **shall observe the Design Guidelines of the San Diego River Park Master Plan.** ~~should be designed to avoid impacts to riparian species including noise, glare and shading impacts. Buildings should be set back 150 feet from the riparian habitat. A buffer of 100 feet should be provided in which no improvements are permitted. This buffer should be planted with vegetation native to the river to provide separation and screening for the protection of wildlife habitat from human disturbance. Passive uses such as pathways or viewing areas should be provided within the next 50 feet. Paths within this 50-foot buffer should be designed to focus activity (and potential access) away from sensitive habitat areas. Viewing or picnic areas should be located where they will not impact the riparian habitat. Access to the habitat should be discouraged through the use of planting clusters and screens.~~

MIXED-USE

INTRODUCTION

This chapter builds upon City Council adopted policies set forth in the Strategic Framework Element (SFE) and further developed in the General Plan update. A series of policies are established in these documents that set the framework for new neighborhoods designed with a mixture of land uses that promote walkability, social interaction and a sense of place. This chapter provides guidelines for implementation of SFE policies during the transition of land uses in areas of the Navajo community that are suitable for mixed-use development.

Mixed-use centers within the Navajo community are intended to contain commercial, office, retail and residential land uses in close proximity so as to encourage pedestrian activity, economic diversification, and increased reliance on bicycle, bus, and trolley use. This will include higher-density residential development to serve many market segments, along with a mix of commercial, retail, civic and recreational uses to serve the overall community.

In providing for an increased supply of housing and employment opportunities, development plans for mixed-use projects should focus particular attention on the pedestrian scale and architectural design of buildings, usable public gathering spaces, and tree-lined streets and other neighborhood amenities.

The SFE defines the future direction of land use transitions within the Navajo community in Grantville. Specifically, the SFE defines the area surrounding the Grantville Trolley Station and northward along Mission Gorge Road as a potential Subregional District, recommending a mix of uses that include residential, commercial, retail, open space and other community-serving uses compatible with the area's future use, occupancy and social environment. A Subregional District, as defined in the SFE, is a major employment and/or commercial district containing corporate or multiple-use office, industrial, and retail uses with some adjacent multifamily residential uses. Where appropriate, the collocation of medium- to high-density residential uses with employment could occur.

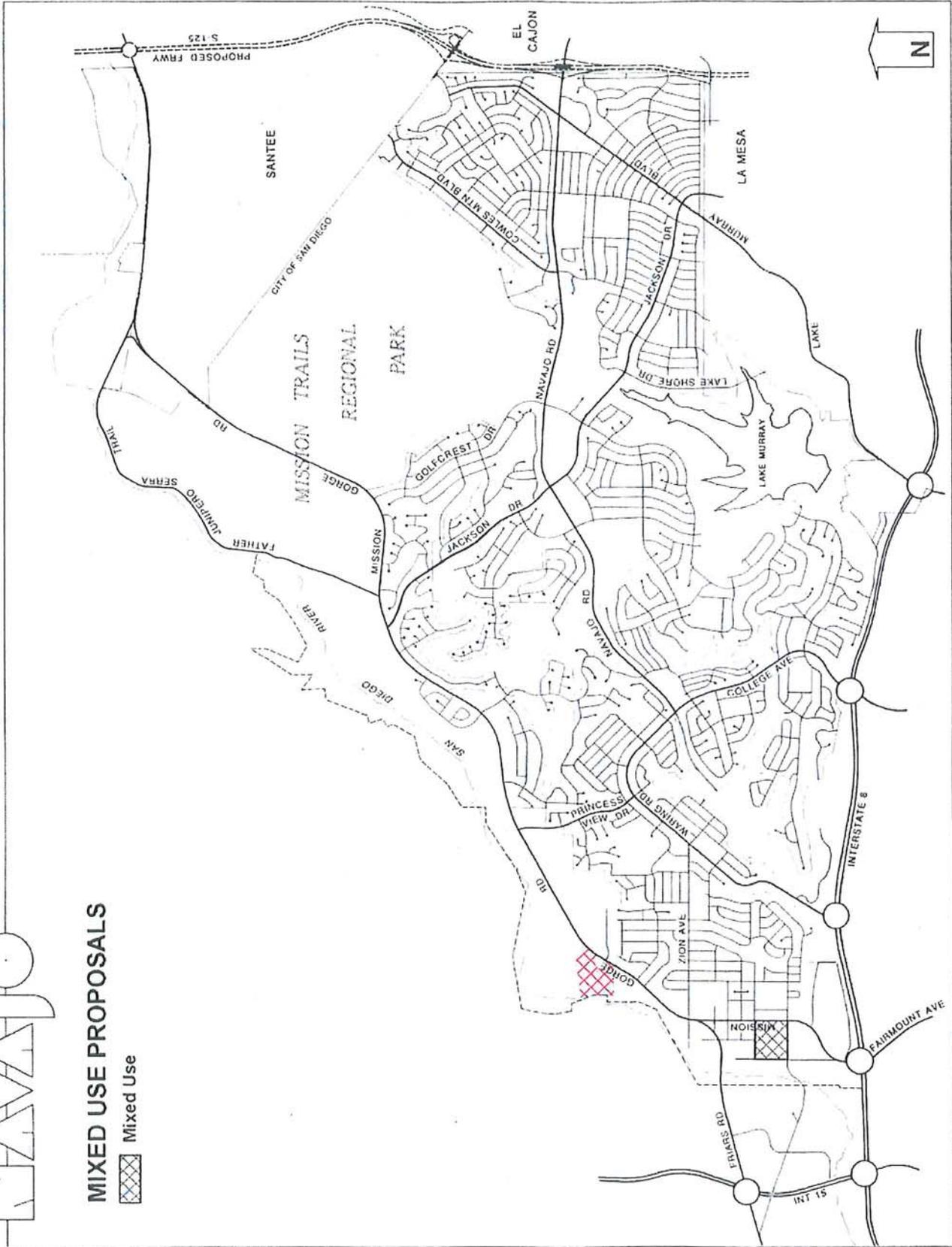
Within the Subregional District are Urban Village Centers, Neighborhood Village Centers and Transit Corridors that will be higher-density/intensity areas characterized by a cluster of employment, residential, and regional and subregional commercial uses that maximize walkability and support transit. These types of mixed land uses are encouraged to create live, work, shop, and recreate environments and more pedestrian-oriented social and economic neighborhood centers, which are conducive to transit use and can reduce dependence on automobiles for single-purpose trips.

The Mission Gorge Road area, south of Friars Road, is designated in the SFE as a Transit Corridor to promote economic revitalization and diverse retail and other small business opportunities that will create a more "Main Street" ambiance for the Grantville neighborhood. The SFE recommends that revitalization be assisted through plan designations



MIXED USE PROPOSALS

 Mixed Use



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and zoning that permits a higher intensity of mixed-use development. The mix of uses could include some combination of residential over storefront businesses, retail, office and other employment uses, or higher-density residential development. The applicable density ranges are medium- to high-density for residential uses. In addition to providing valuable new housing, increased residential densities provide a built-in population base to support the local street level businesses. A high level of transit service and a variety of streetscape improvements will also characterize these transit corridors.

In the future, more areas within Navajo may be designated as Neighborhood Villages, Community Villages and Urban Villages. This new Mixed-Use Element sets the framework for new infill development of these areas and provides guidelines to implement design standards to ensure compatibility with existing development in the Navajo community. In general, Neighborhood Villages are expected to allow development with a residential density above 15 dwelling units per acre and .25+ Floor Area Ratio (FAR); and Community Villages and Urban Villages may allow a residential density above 30 dwelling units per acre and .25+ FAR. The upper range of density and FAR in these designations may be limited by adopted community plans and implementing ordinances. As new projects are proposed and/or master plans are created, more specificity will be included in this Element to assist in the implementation of those projects. The new Mixed-Use Community Plan Implementation Overlay Zone (CPIOZ) defines the sequence of procedures and the design standards to be followed to bring new developments forward.

EXISTING CONDITIONS

In 2005, a new Grantville Trolley Station opened in the Navajo Community on Alvarado Canyon Road. It is expected that the trolley station will become increasingly frequented by Navajo residents, employees and visitors. This will contribute to an enhanced quality of life, a reduction in additional traffic congestion over time, improved local air quality, and an opportunity for residents and visitors to engage in more frequent pedestrian-oriented and transit-reliant excursions.

Mission Gorge Road, which provides a primary access route south from Friars Road to the Grantville Trolley Station, is developed with a mix of mostly older commercial and industrial buildings containing a variety of auto-dependent commercial service uses. Many properties in the area are occupied by outdated and deteriorated buildings, lack adequate parking and landscaping, and are in need of substantial capital reinvestment to stimulate economic development and create a more attractive and pleasant environment. Existing industrial zoning south of Friars Road has not adequately accomplished the objective of the existing CPIOZ, which is intended to “encourage industrial development and redevelopment which will capitalize on the area’s central location within the metropolitan area.”

Because of the age and declining physical condition of the industrial properties, many of which have already been converted to office or storage facilities, this area was identified by the SFE as prime for neighborhood revitalization with a more lively mix of employment, commercial, higher-density residential, and civic uses.

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A shift in land uses to accommodate a mixed-use development would allow many of the properties containing outdated buildings and uses to be better utilized. This would add more appropriate residential, workforce, and local population-serving retail, as well as new, quality commercial uses to the area. Over time, this would shift ~~this portion of~~ **areas of** Grantville away from the industrial designations that no longer serves the needs of current and projected future market conditions. **The properties at Old Cliffs Road that are bounded by Mission Gorge Road, the San Diego River , the Archstone property and the Superior Ready Mix property are proposed to be converted from industrial land into mixed-use with medium and high density residential.** Industrial uses will continue to be designated in Navajo further to the north along Mission Gorge Road and be concentrated in a technology park, currently envisioned for future development, where the high quality design guidelines and development standards of the industrial CPIOZ can be accomplished.

The Urban Village Center designation for the Centerpointe property encourages “an intensive mix of employment, commercial and higher density uses near transit hubs.” The City of San Diego Progress Guide and General Plan’s Urban Design Element and Transit-Oriented Development (TOD) Guidelines provide further direction for mixed-use environments. The new Mixed-Use CPIOZ additionally defines the sequence of processes and the design standards to be followed to bring new developments and redevelopments forward.

OBJECTIVES

The principal objective for the mixed-use designation in Navajo is to:

PROMOTE WALKABILITY AND A NEIGHBORHOOD ENVIRONMENT BY THE INTEGRATION OF EMPLOYMENT, RESIDENTIAL, AND REGIONAL AND SUBREGIONAL COMMERCIAL USES.

To achieve this principal objective, the following additional objectives were also adopted:

- Mixed-use developments are intended to provide a mix of housing, jobs, shopping, commercial services and public or semi-public open space in a land use plan that implements the vision and values of the City’s Strategic Framework Element.
- Promote higher residential densities in the mixed-use areas. The higher residential densities will provide more housing with variations in dwelling unit sizes or occupancies in close proximity to public transportation, which will facilitate the reduction of traffic congestion and help improve local air quality.
- Mixed-use developments should include variations in dwelling unit sizes or occupancies. Smaller unit sizes for student, workforce and senior housing would be appropriate.
- Multifamily residential above commercial is highly encouraged, as is upper-floor office or other non-retail commercial uses.
- Develop pedestrian-oriented commercial land uses and minimize automobile access points along major thoroughfares. Commercial and mixed-use buildings should provide

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direct pedestrian access from the sidewalk, have minimum setbacks, and provide parking at the rear or side of buildings, or underground.

- Ensure excellence in urban design. This excellence is defined as an attention to human-scale detail in the site planning and in the architectural scale, ensuring that transitions between uses are compatible, and that a project addresses all potential environmental impacts and possesses a high quality of aesthetic appeal.
- Ensure that there is an appropriate amount of community open space, allowing for public use based on the size of the projects to be developed or redeveloped, the proposed land uses, and the need for open space amenities in order to achieve the objectives of the mixed-use designation. The design of the open space is critical to the success and overall perception of the new mixed-use center by inviting active and regular pedestrian use.
- Create multifamily housing with high quality of design and livable environments for senior and workforce housing, student and faculty residences, and new housing products for many current residents of the community that are being priced out of suitable housing opportunities. Higher residential densities in close proximity to retail, dining, and office uses, as well as the improved opportunities for social interaction, will directly support the expansion of local businesses and services.
- Encourage projects within the mixed-use designation to develop parking management programs to share parking between residential and commercial uses.

These objectives can be accomplished by encouraging the establishment of a diverse cross-section of the City's evolving residential population base, creating new opportunities for retail office, entertainment, cultural, and civic activities, and improving access to public transportation.

PROPOSALS

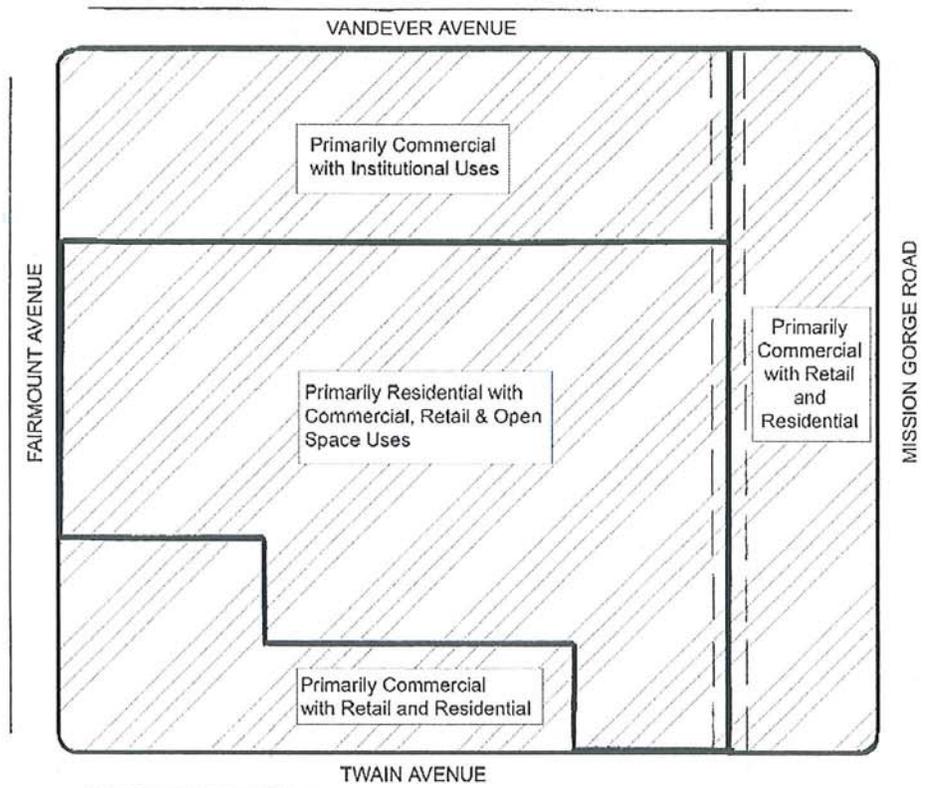
General

Based on the existing conditions and objectives identified above, the following general proposals were established to encourage mixed-use development that is compatible with the surrounding neighborhoods of the Navajo community:

- Achieve a change from a predominately low-wage, service industry job center in the Grantville area by providing a more community-serving mix of professional offices and retail businesses.
- Mixed-use should be implemented either by commercial zones with defined allowed uses or multifamily residential zones which encourage a variety of housing types and can fulfill the transition of demographics and market segments evolving within the Navajo community.

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- Floor areas of new commercial projects should approach the maximum FAR of the underlying zone where economically feasible and adverse effects on nearby properties do not occur.

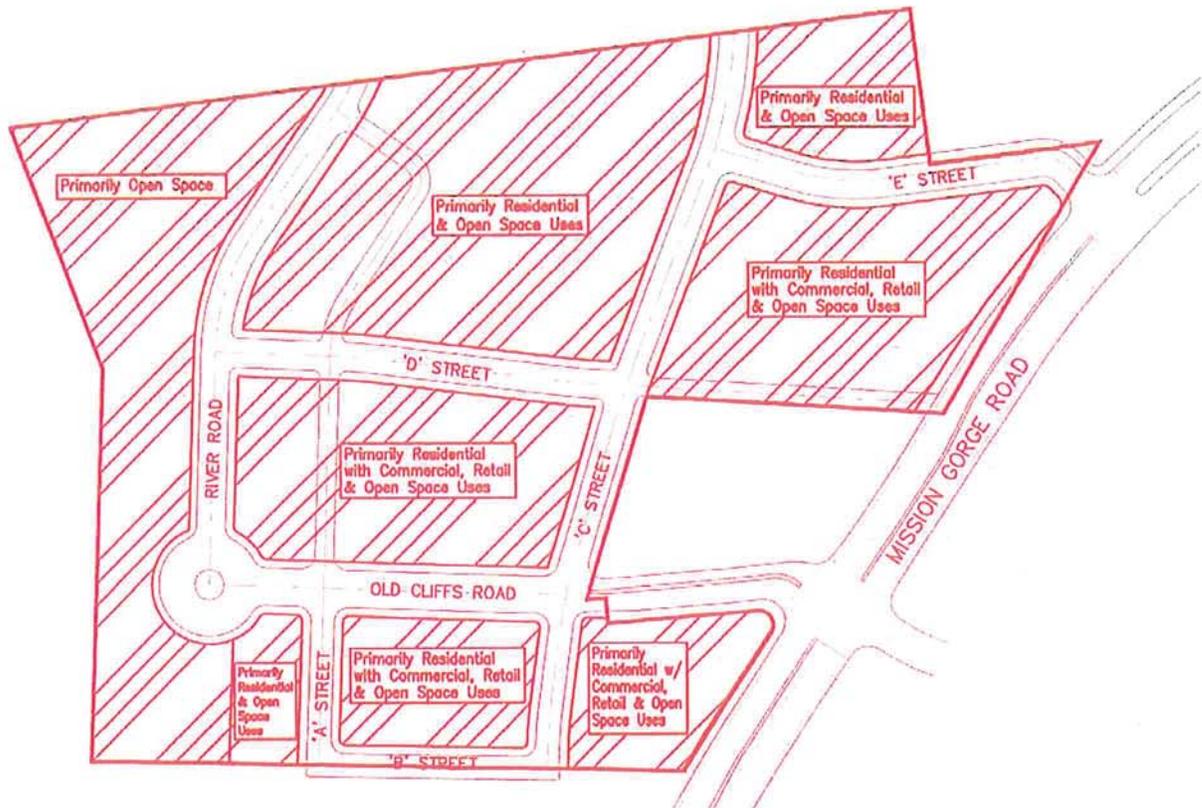


Map 1: Mixed-Use Overlay Zone

- To encourage an urban village character for multifamily and mixed-use development, minimum front yard setbacks of zero to ten feet for residential buildings and zero to five feet for buildings with commercial uses fronting on public streets, are encouraged. Off-street parking shall not be permitted between the street and the front of any building.
- The Centerpointe at Grantville project proposes to rezone approximately ten acres from an industrial zone to a multifamily residential zone. Other adjacent properties within the block bordered by Mission Gorge Road, Twain Avenue, Fairmount Avenue and Vandever Avenue, for which a rezone to permit multifamily residential use has not been requested by the property owner, shall retain their existing industrial zone. However, when redeveloped, multifamily residential and/or commercial zoning shall be applied, and redevelopment shall be consistent with the Mixed-Use CPIOZ designation, San

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Diego Municipal Code, and the objectives of the adopted Strategic Framework Element and General Plan.



Map B: Mixed-Use Overlay Map

- The Shawnee/CG7600 Master Plan proposes to rezone approximately 23 acres from industrial and agricultural zones to a multifamily residential zone. Other adjacent properties bounded by the site and Mission Gorge Road, for which a rezone to permit multifamily residential use has not been requested by the property owner, shall retain their existing industrial and agricultural zones. However, when redeveloped, multifamily residential and /or commercial zoning shall be applied, and redevelopment shall be consistent with the Mixed-Use CPIOZ designation, San Diego Municipal Code, and the objectives of the Strategic Framework Element and the General Plan.

Community Plan Implementation Overlay Zone (CPIOZ)

Compliance with these supplemental regulations will be determined pursuant to Process One (CPIOZ Type A) in accordance with the procedures of the Community Plan Implementation Overlay Zone (Municipal Code Article 2, Division 14).

Development proposals that do not comply with the following supplemental development regulations but comply with the regulations of the underlying zone shall be required to obtain discretionary approval of a Site Development Permit (Process Three). Examples of proposals

that would follow a discretionary process would include land uses that are allowed in the underlying zone but are not consistent with the Objectives stated in this mixed-use element, and proposals containing design deviations from the CPIOZ Type A design guidelines. Applications for a CPIOZ Type B permit or a Site Development Permit shall meet the purpose and intent of the regulations of the underlying zone and the supplemental development regulations. Exceptions from these regulations may be granted by the Planning Director in accordance with the procedures of the Community Plan Implementation Overlay Zone (Municipal Code Article 2, Division 14).

Applications for a CPIOZ Type B permit will require a discretionary review process and should address: 1) the design and compatibility of the project in relation to surrounding development, including conformance with the design guidelines of the mixed-use element. Projects may propose design solutions that differ from, but are of equal or higher quality to the design concepts set forth in the mixed-use element; 2) the ability of Mission Gorge Road and adjacent streets in Grantville to accommodate additional travel demand; and 3) the opportunity for instituting travel demand management strategies such as participation in a transportation demand association.

If a Planned Development Permit or Conditional Use Permit is requested or required by Section 126.0602 of the Municipal Code, the decision shall be made in accordance with Process Three. Minor modifications to an approved development permit may be submitted to the Planning Director to determine if the revision is in substantial conformance to the submitted permit in accordance with Section 126.0112 of the Municipal Code. Amendments to an approved development permit would be processed in accordance with Section 126.0113 of the Municipal Code. If a zone reclassification is necessary to allow the land uses proposed for the mixed-use development, the project would require a Process Five decision.

Properties along Twain and Fairmount avenues that are not part of the Centerpointe at Grantville Site Development Permit will not be concurrently rezoned as part of the adoption of the mixed-use land use designation. Commercial uses will be allowed to be developed within the current industrial zone with a CPIOZ Type A permit. Industrial uses allowed under the existing industrial zone will require a Site Development Permit (Process Three). Redevelopment of these properties to permit mixed-use residential and commercial projects may include a rezone to a commercial or multifamily residential zone (Process Five).

Within the area designated as mixed-use by the Navajo Community Plan, no building, improvement, or portion thereof shall be erected, constructed, converted, altered, enlarged, or

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established until a CPIOZ permit is obtained. A CPIOZ permit is not required for any of the following:

- a) Interior improvements to an existing building that do not involve a change in use or provide additional floor area;
- b) Interior modifications or repairs, or exterior repairs or maintenance for which a building permit is not required.

Supplemental Development Regulations:

General Design Guidelines

These broad perspective design guidelines are intended to become the basis for more finite site design, building architecture, and detailed landscape and environmental character design. The purpose here is to generate creative approaches that should guide the ultimate design and enable the transition to new land uses that are not incompatible with existing development in the project area.



Façade Articulation and Architectural Elements

Areas designated for mixed-use development are intended to create pleasant places to walk, shop, live, work and relax. The enjoyment of these activities will be enhanced when the visual effects of the built environment create opportunities for interesting, interactive and lively, and rewarding pedestrian experiences.

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Outdoor gathering spaces and places, such as eating areas, plazas, or other public-use pedestrian spaces, should be used to create a traditional village atmosphere and be furnished with amenities such as benches, awnings, umbrellas, textured hardscape, trellis plants, planter boxes and other ornamentations.

Public art should be encouraged to be included in the project and artists should be engaged in the project-level development as a team member working with the architects, landscape architects, land planners, and engineers. This would include use of artists and design professionals to work with the development team from the concept phase throughout the duration of the design development phases.

Pedestrian access should be encouraged throughout the project through the incorporation of a double row of street trees, sidewalks throughout the project where needed to provide access to primary building entries and to connect with common areas, urban furniture of a consistent theme, and ground level transparency on all buildings that front on the surrounding public street frontages. Provide on-street parking and locate required parking in side or rear yards, or underground, but not within the front yard. These design features will enhance the walkability of the project and promote non-vehicular use to reduce traffic congestion and promote improved local air quality.



Walkable Streets

The area should be developed as an extension of the existing street and block fabric of the Grantville area. New streets should be at grade with the existing streets. New streets, whether publicly or privately designated, should look, feel, and function as a public street.

Building materials and architectural details should reinforce the character of revitalizing a neighborhood where function and size are secondary to form and proportion in creating visually stimulating urban environments.

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A subtle consistency of harmonious forms, each containing unique elements of architectural detail and scale, is desired but should not become repetitive or formulaic.

Architecture of franchise commercial shops and restaurants should not be based on off-the-shelf designs required to be followed by all franchisees, but rather adaptations expressing accepted traditional or blending designs to complement the larger neighborhood.

Colors for the architectural materials should relate to the soil and shrubs found in the nearby San Diego River open space. Buildings with towers should, however, strive to relate to the sky colors instead, while lower buildings should relate to the earth tones and natural landscaping of the environment.

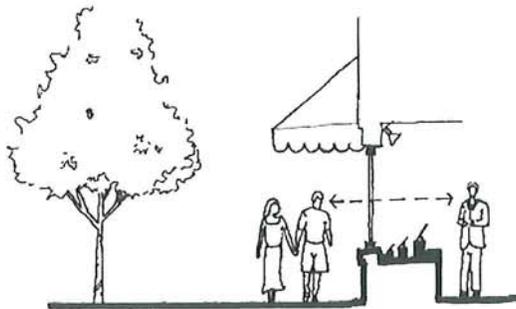
The City's Urban Design Element, which guides mixed-use development in Village Centers and Transit Corridors, shall be used in the project design to support walkability, strengthen connectivity and enhance community identity.



Street Treatments and Building Transparency

The village feel and comfort provided to pedestrians are enhanced by design elements provided between the curb and the street-level building walls. These elements include:

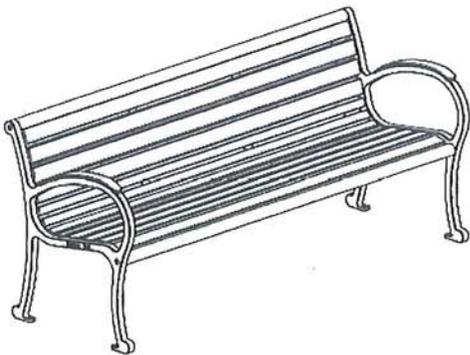
- Direct primary access and visual connections from the sidewalk to the interior of the building.



Retail Transparency

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- Maximum setback for new commercial uses on Mission Gorge Road, Twain Avenue, and other streets suitable for pedestrian-oriented commercial uses, should be **zero to** five feet from the edge of the public right-of-way and be varied only as needed to provide public use areas such as plazas, café seating, and other amenities consistent with the objectives of the mixed-use designation.
- For non-residential buildings or portions of buildings, at least 50 percent of the total area of public-street-facing building walls must be devoted to pedestrian entrances or windows, which extends the perceived public space into the businesses and allows “eyes on the street” to increase public safety.
- Provide a continuous sidewalk separated from the curb with landscaping, tree wells, or raised planters along these streets.
- Where there is ground-floor parking, buildings shall provide enhanced façade materials such as brick, stone, stacked shale, to enhance the pedestrian experience.
- Include street furniture such as benches, trash receptacles, newsstands and lighting. The following samples or equivalent are suggested.



Bench Specification

Redwood Bench



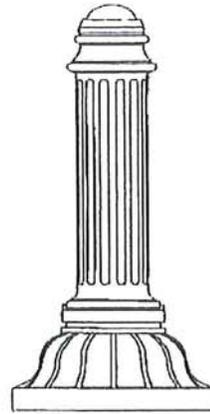
Trash Receptacle Specification

Redwood Receptacle



Light Standard Specification

Sternberg Lighting



Light Bollard Specification

Sternberg Bollard

Sample Street Furniture

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Floor Area Ratio (FAR):

New development processed under ministerial (CPIOZ Type A) review shall not exceed the FAR of the zone. Development proposals that exceed the specified FAR shall obtain discretionary approval of a planned development permit. The discretionary review process shall address: 1) the design and compatibility of the project in relation to surrounding development, including conformance with the design guidelines of this mixed-use element; 2) the ability of Mission Gorge Road and adjacent streets in Grantville to accommodate additional travel demand generated by the proposed project; and 3) the opportunity for instituting travel demand management strategies such as participation in a transportation management association.

Building Setback Adjacent to the San Diego River:

From the edge of the floodway 35 feet shall be reserved for the San Diego River Park Path Corridor and an additional 10 feet minimum building setback shall be observed. Building height shall be stepped back relative to the edge of the 35 foot-wide Path Corridor as follows: building height shall be 25 feet maximum at 10 foot setback, 35 feet maximum at 20 foot setback, 65 feet at 30 foot setback, 70 feet at 35 foot setback and after a distance of 35 feet from the Path Corridor, the building height shall step back at a ratio of one foot for each foot of building height. After a distance of 200 feet from the River Corridor the building height is governed by the underlying zone. All structures within 150 feet of the San Diego River's 100-year floodway shall be designed to step back from the floodway so that low-story buildings are adjacent to the river with the higher stories tiered away from the river. Buildings shall be set back or stepped back from the floodway at a ratio of one foot for each foot of building height with a minimum setback of 20 feet.

Building Setback Adjacent to Mission Gorge Road:

~~All structures on Mission Gorge Road shall observe a maximum five-foot setback. Structures over 30 feet in height shall be set back or stepped back an additional one foot for each foot of building height over 30 feet.~~

Building Transparency:

Building walls for residential buildings or residential portions of buildings that face streets should include transparent walls for at least 50 percent of the first residential floor of building frontage along the street, measured from the finished residential floor elevation.

Building Reflectivity:

Glass transparency shall be a minimum of 80 percent and with a maximum light reflectivity of 20 percent.

Equipment Enclosure:

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All mechanical equipment and appurtenances shall be screened on all sides so that they appear to be an integral part of the overall architectural design of the building. The screening may include grillwork, louvers, or latticework.

Outdoor Storage:

Outdoor storage is not permitted.

No merchandise, material, or equipment shall be stored or displayed on the roof of any building.

Outdoor Display:

Outdoor display shall comply with Chapter 14, Article 2, Division 11 of the Municipal Code.

Loading Areas:

Loading and service areas shall be located in interior side yards or rear yards only, except that no loading or service area shall be permitted between the building wall line and the San Diego River.

Refuse and Recycling Collection Areas:

Refuse and recycling collection areas shall be located in interior side or rear yards only, except that no outdoor storage area shall be located between the building wall line and the San Diego River.

Refuse and recycling collection areas shall be screened with a solid six-foot fence or wall or an enclosed structure. All such fences, walls, or structures shall be of a similar material and color as the main building. Deposited materials shall not be visible from outside the screening enclosure.

Parking Requirements:

No parking area shall be located between the building wall line and the San Diego River unless a landscape buffer is provided between the parking area and the required setback from the river. The landscape buffer shall be a minimum of eight feet wide and shall be planted in accordance with Section 142.0403 of the City Landscape Regulations; however, in no case shall the minimum landscape area width of eight feet be reduced by the use of a site wall.

The parking regulations in Chapter 14, Article 2, Division 5 of the Municipal Code permit reduced parking ratios for development in the Transit Area and Urban Village overlay zones. These reduced ratios may be approved for projects in the Mixed-Use CPIOZ. The shared parking requirements in Section 142.0545 should also be used when they can further reduce the required amount of parking. Minimum off-street parking for the following types of commercial uses shall be as follows:

Spaces/Square Feet of Gross Floor Area

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For business and professional offices	1/400
For medical and dental facilities	1/400
For commercial uses that take access from Mission Gorge Road	1/400
For all other commercial uses	1/400

These parking requirements shall replace the parking requirements of Article 2, Division 5 of the Municipal Code. If the citywide commercial parking requirements are revised, the new standards will replace those identified above.

Curb Cuts and Driveways:

On Mission Gorge Road, south of Friars Road, one curb cut shall be permitted for each lot with frontage on Mission Gorge Road. One additional curb cut may be permitted for each 150 feet of frontage on Mission Gorge Road. No driveway shall exceed a width of 25 feet measured at the property line.

On Mission Gorge Road, north of Friars Road, curb cuts shall be in conformance with the Street Design Manual standards for primary arterials.

Pedestrian Access:

A continuous ten-foot minimum width pedestrian/bicycle path shall be provided along the San Diego River frontage within the 20-foot minimum setback.

All structures within 150 feet of the San Diego River's 100-year floodway shall provide at least one pedestrian entrance from the structure to the river path.

Signs:

Signs shall be in conformance with the citywide Sign Regulations (Section 95.0101 et seq. and Section 142.1201 et seq. of the Municipal Code).

On Mission Gorge Road, ground signs shall not exceed the height of the building or the citywide height limit of 30 feet, whichever is less, unless they meet the definition of freeway-oriented signs in the Municipal Code.

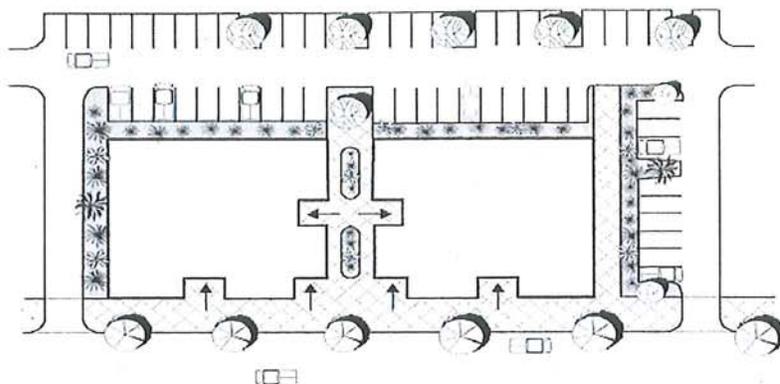
A landscaped area shall be provided at the base of all ground signs. The size of the landscaped area shall be equal to or greater than the area of the sign face.

Parcels Fronting onto Mission Gorge Road, Twain Avenue, and Other Streets Suitable for Pedestrian-Oriented Commercial Uses:

Commercial, office, retail and residential uses of parcels adjacent to the above-mentioned streets shall have building frontage and access onto these streets and shall provide their main pedestrian entry off of these streets. Commercial, office, or retail uses shall include ground floor locations on these streets. Future improvement or redevelopment of properties fronting on these streets shall comply with these guidelines to encourage increased pedestrian use.

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Parking for these sites shall be located along the back or side of street-oriented buildings. Refuse collection areas shall be screened from the street.



Pedestrian Orientation

Landscaping:

Landscaping shall be provided as required by the City Landscape Regulations. For the streets identified below, trees in the public right-of-way shall be chosen from the following species to be consistent with the predominant species in the area. Root barriers shall be installed where needed to protect water and sewer lines or other facilities.

Tree Species

<u>Street</u>	<u>Botanical Name</u>	<u>Common Name</u>
Mission Gorge Road	<i>Jacaranda mimosifolia</i>	Jacaranda
South of Friars Road	<i>Koelreuteria paniculata</i>	Goldenrain Tree
	<i>Brachychiton acerifolius</i>	Flame tree
	<i>Quercus ilex</i>	Holly oak
Twain Avenue	<i>Pinus canariensis</i>	Canary Island Pine
	<i>Koelreuteria paniculata</i>	Goldenrain Tree
Fairmount Avenue	<i>Brachychiton acerifolius</i>	Flame tree
	<i>Jacaranda mimosifolia</i>	Jacaranda
	<i>Koelreuteria paniculata</i>	Goldenrain Tree
Vandever Avenue	<i>Brachychiton acerifolius</i>	Flame tree
	<i>Jacaranda mimosifolia</i>	Jacaranda
	<i>Koelreuteria paniculata</i>	Goldenrain Tree

Centerpointe at Grantville-Supplemental Development Regulations:

Centerpointe at Grantville consists of approximately 12 acres and is intended to be developed pursuant to these Supplemental Development Regulations, which allow a greater variety and intensity of uses than base zoning and provides greater flexibility for creative site design. This is envisioned to result in reduced automobile dependency, increased pedestrian activity and transit use, and the creation of high-quality, interactive neighborhoods.

Buildings with primarily residential uses are to be located in the central and west portion of the site and be served from a central landscaped corridor from Fairmount Avenue for their primary vehicular and pedestrian access. These buildings may also include ground-floor retail or office uses. Mixed-use commercial and residential buildings on Twain Avenue are to serve as an example for future mixed-use development in the neighborhood. Commercial and retail uses are to be located on Mission Gorge Road and will serve as the appropriate pathway to the interior of the project in a manner that encourages pedestrian access and public activity in the central mixed-use core.

The Centerpointe Commons central open space should connect all uses and be located in the eastern portion of the site, allowing for transit and pedestrian access for the community at large. This central open space will further serve the recreational and social needs for future residents of the project.

As surrounding uses evolve, they are projected to be compatible with the Centerpointe at Grantville land uses, architectural design, and landscape and circulation elements. The design purpose and relationship of land uses are to encourage pedestrian rather than vehicular activity and to better interface the mix of uses intended by these guidelines and the Strategic Framework Element.

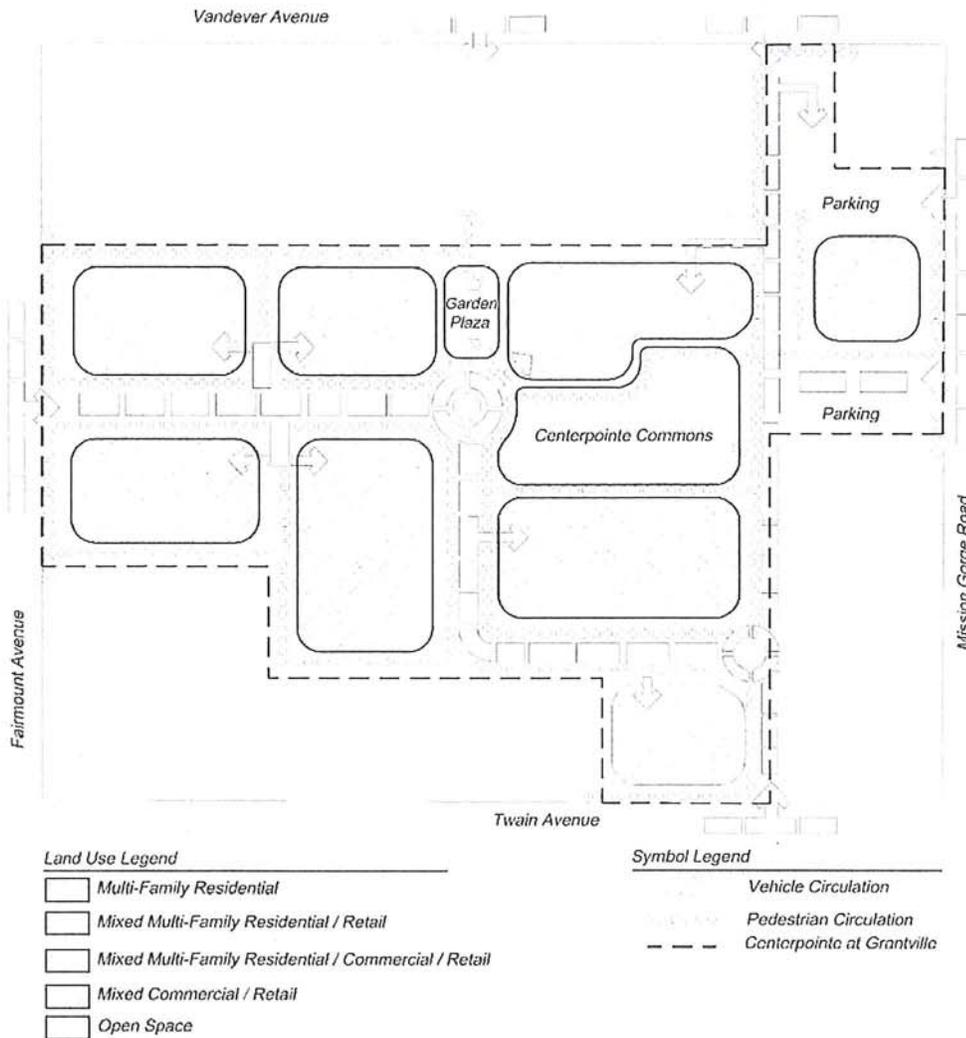
The land use plan for Centerpointe at Grantville is shown on Map 2.

The following land use designations apply to the Centerpointe at Grantville project:

Multifamily Residential. Development in this designation would primarily consist of residential uses providing a mix of dwelling unit sizes and may also include special-purpose occupancy, such as for seniors, students, or workforce housing. Secondary uses may also include offices, retail and other commercial uses per the list of permitted uses, below.

Mixed Multifamily Residential/Retail. Development in this designation would primarily consist of residential uses providing a mix of dwelling unit sizes and may also include special-purpose occupancy, such as for seniors, students, or workforce housing and shall also include offices, retail and other commercial uses per the list of permitted uses, below, some of which shall be located on the ground floor. These commercial uses are to be located along the Twain Avenue frontage and adjacent to Centerpointe Commons.

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Map 2: Centerpointe at Grantville

Mixed Multifamily Residential/Commercial/Retail. Development in this designation would permit a variety of mixed-use development opportunities. Residential uses may provide a mix of dwelling unit sizes and may also include special-purpose occupancy, such as for seniors, students, or workforce housing; and shall also include offices, retail and other commercial uses, with no limitation on the maximum space allotted to commercial uses, per the list of permitted uses, below, which are to be located adjacent to Centerpointe Commons. Alternatively, primarily office development may be permitted, in combination with retail, restaurant, or other commercial services to be located adjacent to Centerpointe Commons.

Mixed Commercial/Retail. This designation is intended to accommodate retention of the existing office building on Mission Gorge Road and permit future transition to retail and commercial service uses on the ground floor per the list of permitted uses, below. Alternatively, redevelopment with primarily multifamily residential or other special-purpose residential occupancy on the upper floors, while retaining commercial uses on the ground floor, would also be permitted.

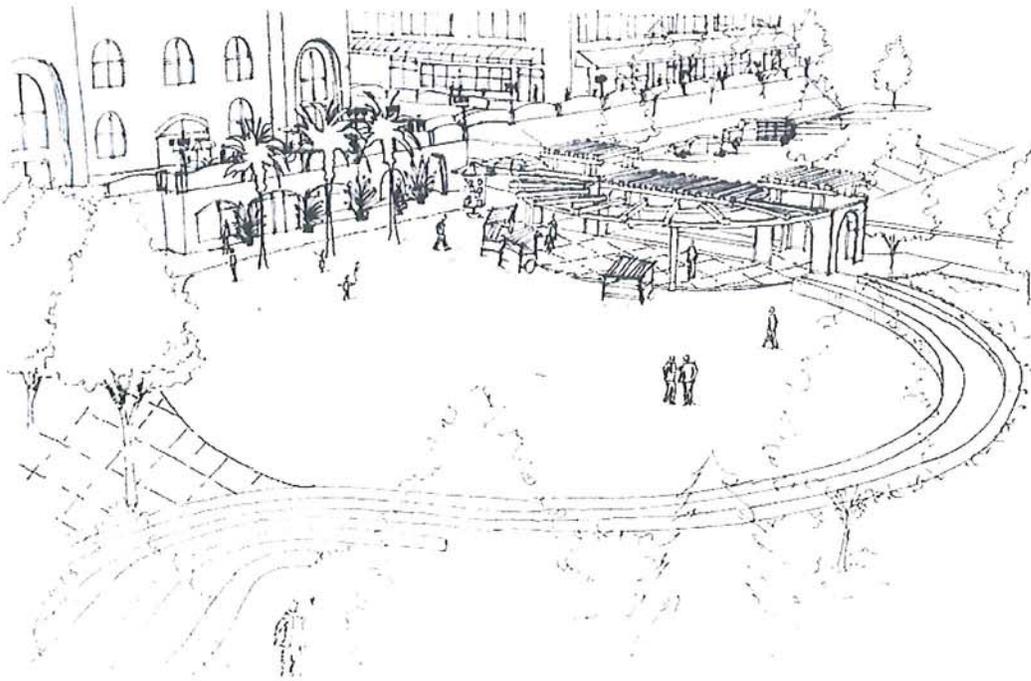
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Centerpointe Commons and Garden Plaza. The combined acreage of exterior open space available for public use shall be at least ten percent of the total acreage for the proposed project and shall consist of Centerpointe Commons, Garden Plaza and potentially other outdoor plazas and public-use gathering spaces.

The proposed development shall comply with the following provisions of the Mixed-Use CPIOZ and the RM-3-9 and CO-1-1 zones.

Development Concept

Centerpointe at Grantville shall be developed with a mix of office and retail uses along the Mission Gorge Road frontage and a mix of commercial and residential uses along the Twain Avenue frontage in compliance with the list of permitted uses, below. Privately-owned community open space for public use shall also be included within the project and shall be improved, maintained, and managed to permit public access and use during daylight hours and at other times when commercial uses are open.



Public Use Open Space

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Permitted Uses:

Permitted uses within Centerpointe at Grantville shall be residential and commercial, as follows:

- Multiple Dwelling Units
- Separately Regulated Residential Uses:
 - Student Dormitories (C – Permitted by conditional use permit and subject to compliance with Section 141.0304 of the Municipal Code, but excluding Fraternities or Sororities)
 - Home Occupations (L – Permitted subject to compliance with Section 141.0308 of the Municipal Code)
 - Housing for Senior Citizens (C – Permitted by conditional use permit and subject to compliance with Section 141.0310 of the Municipal Code)
- Sales of:
 - Food, Beverages and Groceries
 - Consumer Goods, Furniture, Appliances, Equipment (limited to 2,500 square feet for each premises)
 - Sundries, Pharmaceuticals, and Convenience Sales
 - Wearing Apparel and Accessories
- Commercial Services:
 - Building Services (limited to 2,500 square feet for each premises)
 - Business Support (Hiring Halls are not permitted)
 - Eating and Drinking Establishments (the sale of alcoholic beverages is not permitted as a primary use)
 - Financial Institutions
 - Maintenance and Repair (limited to 2,500 square feet for each premises)
 - Personal Services
 - Pet Grooming and Pet Supplies (Pet Sales or Boarding Kennels are not permitted)
- Separately Regulated Commercial Services Uses:
 - Child Care Centers (L – Permitted subject to compliance with Section 141.0606 of the Municipal Code)
 - Instructional Studios

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- Outpatient Medical Clinics (N – Permitted with a Neighborhood Use Permit subject to compliance with Section 141.0615(b) of the Municipal Code)
- Private Clubs, Lodges and Fraternal Organizations
- Pushcarts on Private Property (L – Permitted subject to compliance with Section 141.0619(a) of the Municipal Code)
- Sidewalk Cafés (N – Permitted with a Neighborhood Use Permit subject to compliance with Section 141.0621 of the Municipal Code)
- Offices
 - Business and Professional
 - Government
 - Medical, Dental, and Health Practitioner
 - Regional and Corporate Headquarters
- Industrial
 - Research and Development
- Signs
 - Allowable Signs
- Separately Regulated Institutional Uses
 - Minor Telecommunication Facility (L – Permitted subject to compliance with Section 141.0405(e) of the Municipal Code)
 - Satellite Antennas (L – Permitted subject to compliance with Section 141.0405(g) of the Municipal Code)

Open spaces are to be shared amongst the uses when appropriate to achieve contiguous, functional common open space areas, and public access and use of the privately-owned and publicly-used open space outside of the confines of each building and its security requirements shall be encouraged. Management of the open space shall be provided to prohibit activities that would create a nuisance to residents, business and the local community, and to maintain public safety.

A Transit Shuttle Service is also envisioned as a permitted use to provide a transit center stop within or adjacent to the project to link other key commercial, institutional, employment and park uses with the Grantville Trolley Station. This shuttle depends on economics, community support and the workforce and residential population involvement, as well as logistical support by the Metropolitan Transit Service.

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Density and Floor Area Ratio (FAR):

Centerpointe at Grantville shall comply with the density and FAR of the RM-3-9 and CO-1-1 zones.

Building Setbacks and Height for Residential Uses:

Residential buildings shall comply with development regulations for the RM-3-9 Zone per Table 131-04G of the Municipal Code, with the following exceptions:

- Buildings fronting on the interior street (Centerpointe Circle) shall have a front yard setback of zero to ten feet. If residential uses are proposed for buildings adjacent to the Fairmount Avenue street frontage, the building setback shall be zero to ten feet and no off-street parking shall be permitted within this setback.
- Buildings within this area shall be limited to a maximum height of 75 feet, plus an additional five feet for architectural roof treatments and/or appurtenances.

Ground Level Land Use:

Ground-level retail and commercial uses shall be provided along the project frontage on Mission Gorge Road and Twain Avenue and shall abide by the transparency requirements of the Supplemental Development Regulations to ensure pedestrian visibility into and out of buildings at the ground floor.



Pedestrian Scale and Public Space

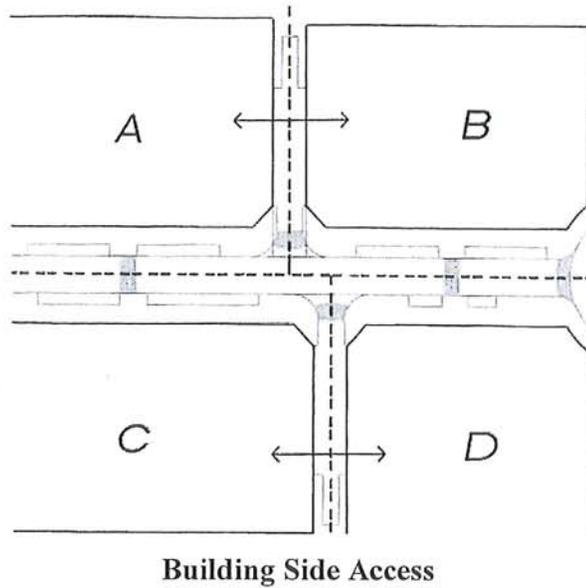
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Centerpointe Circle:

Major entries of buildings shall be located fronting onto Centerpointe Circle, and individual building courtyards shall be enclosed along the property line to maintain a strong street wall continuity and to provide a clear space for pedestrian circulation.

Façade treatment should emphasize the front façade as fronting on Centerpointe Circle. Façade treatments should ensure that there is a clear definition of public open space along the street frontage.

Along Centerpointe Circle, accesses to parking and loading spaces for adjacent buildings shall be consolidated.



Street trees shall be selected for wide canopy shape with the use of one or two varieties, to be selected according to the City of San Diego’s urban forestry list of acceptable street trees. Street trees shall be chosen for oval, rounded, or umbrella shapes to complement the pedestrian spaces and to provide needed shade along the street. Supplemental specimen trees shall be allowed to visually reduce the pedestrian scale of buildings, preferably in clusters to soften building edges and emphasize transitions amongst buildings and open spaces.

Centerpointe Circle Street Trees

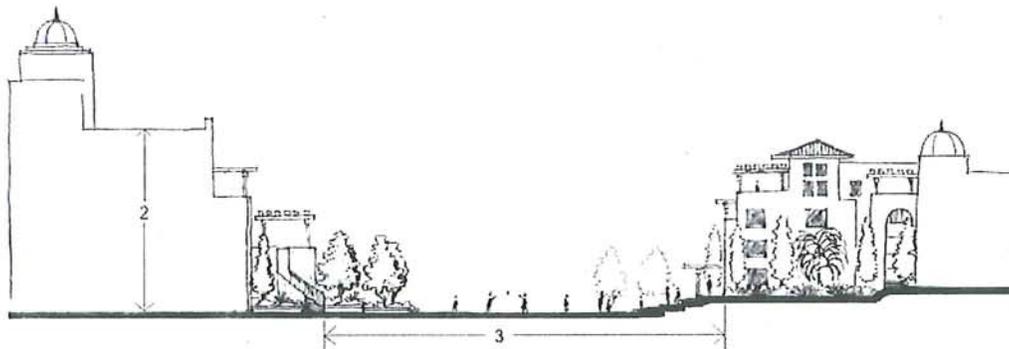
<u>Common Name</u>	<u>Botanical Name</u>	<u>Height at Maturity</u>
Street Tree:		
Jacaranda	<i>Jacaranda mimosifolia</i>	40 ft.
Maidenhair tree	<i>Gingko biloba</i>	50 ft.
Flame tree	<i>Brachychiton acerifolius</i>	40 ft.
Accent Tree (outside of street right of way):		
Raywood Ash	<i>Fraxinus oxycarpa</i> ‘Raywood’	70 ft.
Weeping Willow	<i>Salix babilónica</i>	50 ft.
Cottonwood	<i>Populus freemontii</i>	70 ft.



Centerpointe Circle Streetscape

Plaza/Public Access Common Areas (Centerpointe Commons and Paseos):

The ratio of adjacent buildings to open space should be a 2:3 ratio of the ground plane. The width of the open space shall not exceed 1.5 times the adjacent building height. This is intended to create urban village plazas with clearly defined open space areas that visually relate to the adjacent buildings. This means that urban village plazas should be clearly defined open spaces. These spaces are generally defined by building facades or by massing of landscaping, such as a row of specimen trees to delineate the edge of the open space. In this manner, people are able to better recognize these important neighborhood amenities as a place for public use and social gatherings.



The Commons

Public art should be encouraged to be located within this space and it is recommended that an artist be retained with the initial design team to ensure a seamless design of softscape, hardscape, and art.

Vehicular Accesses:

Centerpointe Circle may have limited curb cuts, as denoted on Map 2. Vehicular access, parking, and curb cuts may also be made from surrounding streets, as well as delivery, visitor, emergency, and trash collection. Street trees shall be located along accesses and include broad canopy tree selections that are consistent with the Centerpointe Circle theme. Sidewalks and streetlights shall also be included on Centerpointe Circle and in the Commons, the pedestrian Paseos, and where internal accesses intersect with surrounding streets.

Shawnee/CG7600 Master Plan – Supplemental Regulations

Shawnee/CG7600 Master Plan consists of approximately 23 acres and is intended to be developed pursuant to these Supplemental Regulations, which allow a greater variety and intensity of uses than base zoning and provides greater flexibility for creative design. This is envisioned to provide elements of smart growth that reduces automobile dependency, increased pedestrian activity, restoration of open space and public access to the San Diego River, and the creation of a high-quality neighborhood.

The primary land use of the site is reserved for multi-family residential. Single family residential townhomes with rear alley access would occupy the first tier of buildings facing the San Diego River. The buildings that front Mission Gorge Road and Old Cliffs Road are multi-family residential uses with ground floor commercial. Roadway connections to adjacent properties are an important element that will provide convenient vehicular and pedestrian circulation.

River Road will connect the site to the adjacent proposed RiverPark at Mission Gorge project. River Road will run almost the entire length of the site and connect to Old Cliffs Road where primary access to the site will lead to Mission Gorge Road. On the west side of River Road the principle open space area will be developed and integrated with the San Diego River Park Master Plan trail corridor.

Adjacent property owners to the north and south are proposing compatible land uses with Shawnee in scale, intensity of development, uses, landscape and circulation elements. Integration of the street network and pedestrian paths are a central theme of the master plan that will encourage pedestrian activity, provide public access to the San Diego River and more closely link the community.

The existing commercial industrial land uses along “C” Street with Old Cliffs Road and “E” Street with Mission Gorge Road present a challenge for residential uses. All repair work for automobiles at the auto body shop is internalized within the concrete walls of the building which eliminates much of the problems associated with industrial uses such as noise and odor. The proposed intervening streets and private drive will provide a physical separation and buffer from potential noise coming from within the building. A fence and landscape shall be placed around the perimeter of the auto body property’s western and northern boundary and the western and southern boundaries of the auto dismantling yard at the northeast corner of the site to provide sufficient visual screening. Street trees shall be planted to provide additional layers of screening for residential units.

The land use plan for Shawnee/CG7600 is shown in Map 3.

The following land use designations apply to the Shawnee/CG7600 site:

Multifamily Residential: Development in this designation would primarily consist of residential uses providing a mix of dwelling unit sizes and may also include special-purpose occupancy such as seniors, students, or workforce housing.

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Single Family Attached: Development in this designation would consist of attached townhomes units with rear alley access.

Mixed Multifamily Residential/Retail/Commercial: Development in this designation would primarily consist of residential uses providing a mix of dwelling sizes and may also include special-purpose occupancy such as for seniors, students, or workforce housing and shall also include residential, office, retail and other commercial uses per the list of permitted uses below, which shall be located on the ground floor. These commercial uses are to be located along the Mission Gorge Road and Old Cliffs Road frontage.



Land Use Legend

-  Single Family Residential
-  Multi-Family Residential
-  Mixed Multi-Family Residential/Commercial/Retail
-  Open Space

Map 3: Shawnee/CG7600

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Open Space (Park): The open space west of River Road shall be reserved for open space and provide population-based parks for the site. Additional open space for passive and active recreation will be linked throughout the site that will serve as an open space network.

The proposed development shall comply with the following provisions of the Mixed-Use CPIOZ and the RT-1-4, RM-3-9, RM-4-10 and OP-2-1 zones.

Development Concept

Shawnee/CG7600 shall allow the development of a mix of retail, office and residential uses along the Mission Gorge Road and Old Cliffs Road frontage in compliance with the list of permitted uses below. Privately owned community open space for public use shall also be included within the site and shall be maintained to permit public access.

Permitted Uses

Permitted uses within Shawnee/CG7600 shall be those permitted in the applicable base zone in the Municipal Code as well as residential and commercial uses as follows:

- Multiple Dwelling Units

- Separately Regulated Residential Uses
 - Home Occupations (L – Permitted subject to compliance with Section 141.0308 of the Municipal Code)
 - Housing for Senior Citizens (C – Permitted by Conditional Use Permit and subject to compliance with Section 141.0310 of the Municipal Code)
 - Shop keeper Units
 - Live/Work Quarters

- Sale of
 - Food, Beverages and Groceries
 - Consumer Goods, Furniture, Appliance Equipment (limited to 2,500 square feet for each premises)
 - Sundries, Pharmaceuticals, and Convenience Sales
 - Wearing Apparel and Accessories

- Commercial Services
 - Building Services (limited to 2,500 square feet for each premises)
 - Business Support (Hiring Halls are not permitted)
 - Eating and Drinking Establishments (the sale of alcoholic beverages is not permitted as a primary use)
 - Financial Institutions
 - Personal Services
 - Pet Grooming and Pet Supplies (Pet Sales or Boarding Kennels are not permitted)

- Separately Regulated Commercial Service Uses

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- Child Care Centers (L - Permitted subject to compliance with Section 141.0606 of the Municipal Code)
- Instructional Studios
- Outpatient Medical Clinics (N – Permitted with a Neighborhood Use Permit subject to compliance with Section 141.0615(b) of the Municipal Code)
- Private Clubs, Lodges and Fraternal Organizations
- Sidewalk Cafes
- Offices
 - Business and Professional
 - Government
 - Medical, Dental and Health Practitioners
- Signs
 - Allowable Signs

Development Regulations

Once approved by City Council action, the Shawnee/CG7600 Master Plan will regulate the development of the Shawnee/CG7600 site. Consult the Shawnee/CG7600 Master Plan for development regulations. Approval of the PDP, SDP, VTM and Rezone for the site will be concurrent with approval of the Shawnee/CG7600 Master Plan.

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OPEN SPACE RETENTION AND UTILIZATION

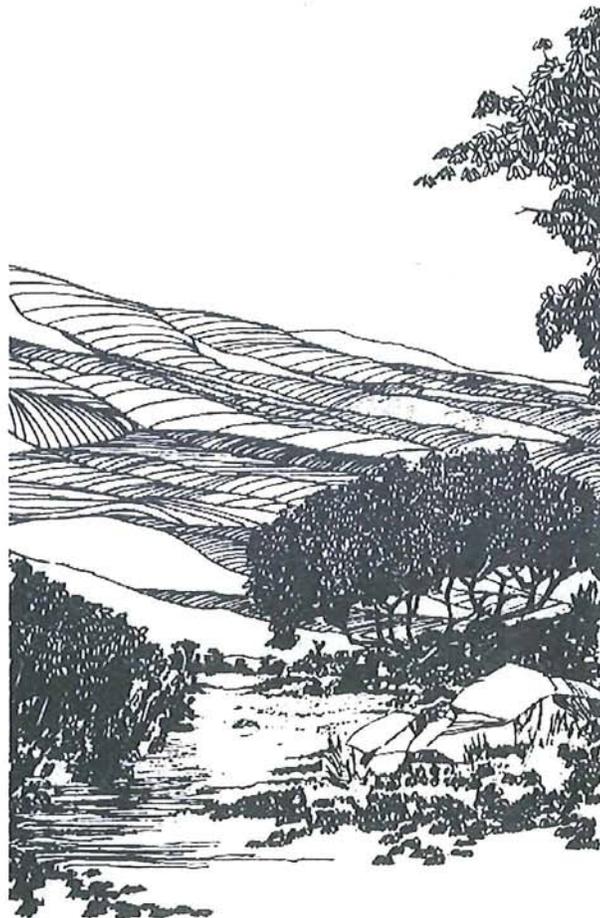
INTRODUCTION

In its broadest sense, the term "open space" refers to all land that is not used for buildings or structures. It may be either urban or non-urban. Open spaces affect the character of development and vice versa. Among other things, open space offers aesthetic variety and relief, shapes the environment, stabilizes other land uses, reduces noise and conserves natural resources.

The reasons for retention of open space are many. The more important of these were spelled out by the late President Kennedy in a special message to Congress: "Open space must be reserved to provide parks and recreation, conserve water and other natural resources, prevent buildings in undesirable locations, prevent erosion and floods and avoid the wasteful extension of public services and control the rate and character of community development."¹

Urbanization is frequently unattractive and ugly. However, it need not be so; if man desires, he can remedy the physical blight and decay within aesthetically offensive areas by strategically introducing openness and greenery, thus providing sorely needed leavening for both visitor and resident.

Open space can serve a most important function by inhibiting that amorphous type of development commonly referred to as "urban sprawl", an affliction visited upon so many large urban areas. Open space also serves to limit concentrations of people and improvements under aircraft flight patterns or on floodplains. It should be used to minimize development in areas subject to geological hazards such as earth slippage and landslides.



1. President John F. Kennedy, "Our Nation's Housing," an address to the first session of the 87th Congress, March 9, 1961.

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Open space has economic value and indirect benefits that are not generally recognized. For example some lands within highly urbanized areas may be more expensive to develop from the public standpoint than they are worth. As has been pointed out in a recent study, the "(d)irect public benefits of a regional open space system would include:



- Income from leases on open space lands for agricultural, recreational, or other compatible uses.
- User benefits (in terms of dollar values) accruing to the public.
- The non-recoverable value of the public's investment in open space land.²

Indirect benefits deriving from the impact of the open space program on urban development patterns would include a series of non-measurable social, economic, and environmental benefits. In addition, a savings in utility costs might also be realized.

Open space may prove directly profitable in other cases as well. A number of examples have been reported where urban open space, especially in the form of city parks, has enhanced the value of surrounding properties to the point where the tax received from those properties exceeds any tax yield that might have been realized had the area of open space been built upon. A good example in the City of San Diego is the area surrounding Balboa Park.

This Plan is an attempt at fulfilling our obligation of leaving a meaningful legacy to future generations. For the long-range good of the entire community, generous expansions of natural open space must be preserved.

In summary, the case for open space in aggregate is impressive. While specific quantitative standards, like those utilized in determining needs for neighborhood and community park facilities, have not been developed for open space systems, the City of San Diego and the Navajo community have recognized this need.

A unique feature in the Navajo Community Plan is the open space element designed to preserve the river, scenic canyon and hillside areas, and to link elements of the community. The proposed open space areas will become green belts and will provide areas for pedestrian, bicycle or equestrian uses. The open space system conceived for the Navajo community envisions that the canyon and hillside areas could be used for both active and passive recreational uses. The majority of open space, however, will probably remain in its natural state, with pathways and picnic areas.

2. **AN OPEN SPACE SYSTEM** for the San Diego Region, San Diego Comprehensive Planning Organization, Open Space Study: Report No. 1, Livingston & Blayney, Royston, Hanamoto, Beck and Beck, April 1972.

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The study area contains many outstanding examples of open space as defined above. There are over 700 acres of scenic canyons, including Mission Gorge, which are dominant topographical features of the Navajo community. These canyons contain a natural reserve of trees and wildlife--resources that are quickly vanishing from the San Diego scene.

The San Diego River, another significant feature of the community, traverses Mission Trails Regional Park through Mission Gorge and is responsible for creating much of the existing topography. That portion of the river located in the northeast section of the community has been significantly altered as a result of an ongoing sand and gravel extraction operation. Much of the area in and around the river has already been mined and is currently being used for industrial and contractor storage and operation uses.

A mix of retail, industrial and industrial office park uses have been developed along that portion of the river that forms the western boundary of the Navajo planning area. The existing development has not taken advantage of the aesthetic qualities of the river environment, nor the passive recreation value of the river and wetlands abutting these sites, but has instead turned its back on the river. It is critical that future development proposals along the river be required to incorporate sensitive site design in addition to providing measures for protecting riparian habitat.

OBJECTIVES

Because there is pressure for intense use of land located within the urban complex, the following principal or overriding open space objective was adopted:

DESIGNATE AND PRESERVE OPEN SPACE BEFORE DEVELOPMENT TAKES PLACE. In this way, it is possible for the best land available for recreation and open space to be preserved to provide a framework for subsequent development. The assignment of a high priority to recreational open space development requires immediate action if preservation is to take place.

The Navajo community recognizes that there is a need to provide adequate and accessible open space for the needs of the population and that without positive action the community may lose this valuable open space through the development of the river area, canyons and hillsides. Therefore, the following additional objectives were adopted:

- Preserve, improve and reconstruct the wetlands and riparian habitat areas in and along both sides of the San Diego River.
- Enhance and maintain the aesthetic and recreational qualities of the San Diego River corridor as part of the open space system.
- Conserve the present amenity of Navajo, Rancho Mission, Mission Gorge and other canyons for the enjoyment of this generation and as a legacy for succeeding generations.
- Establish and preserve a total open space system in perpetuity and guard against its commercialization. Preserve the natural environment including wildlife, vegetation, and terrain.

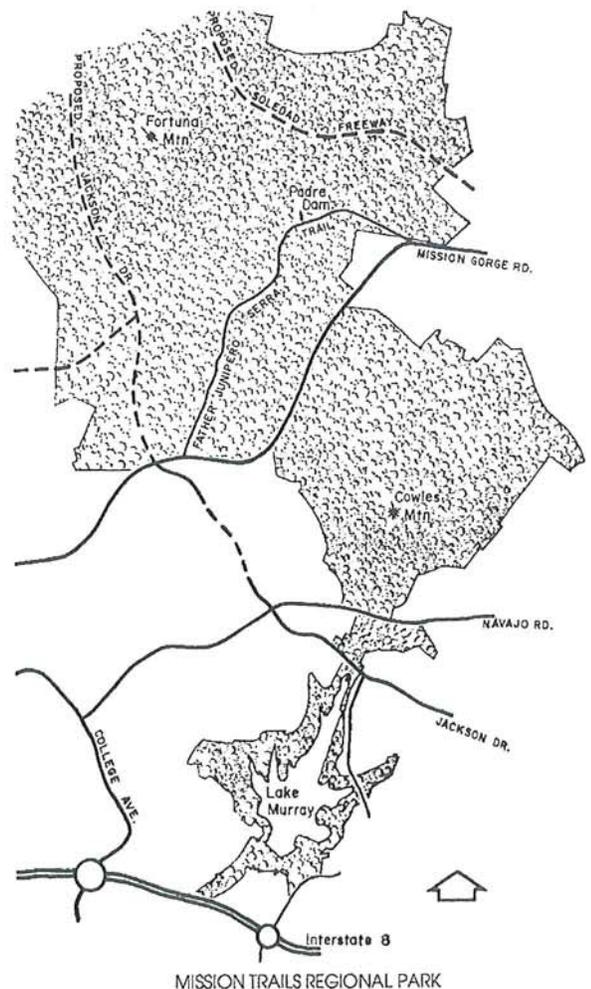
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- Permit only those uses within the system that are compatible with the open space concept.
- Ensure that any public improvements such as roads, drainage channels and utility services and any private lessee developments be compatible with the objectives of the open space system.
- Ensure that development of properties adjoining the open space system is in a manner compatible with the natural environment and in conformance with the Mission Trails Design District and Manual, the San Diego River Wetlands Management Plan, and any subsequently adopted programs which address the San Diego River area.

PROPOSALS

The overall system entails a network of open space belts connecting larger open space areas. These areas for the most part are located in the canyons, along the San Diego River, and on Cowles Mountain as shown on the following map.

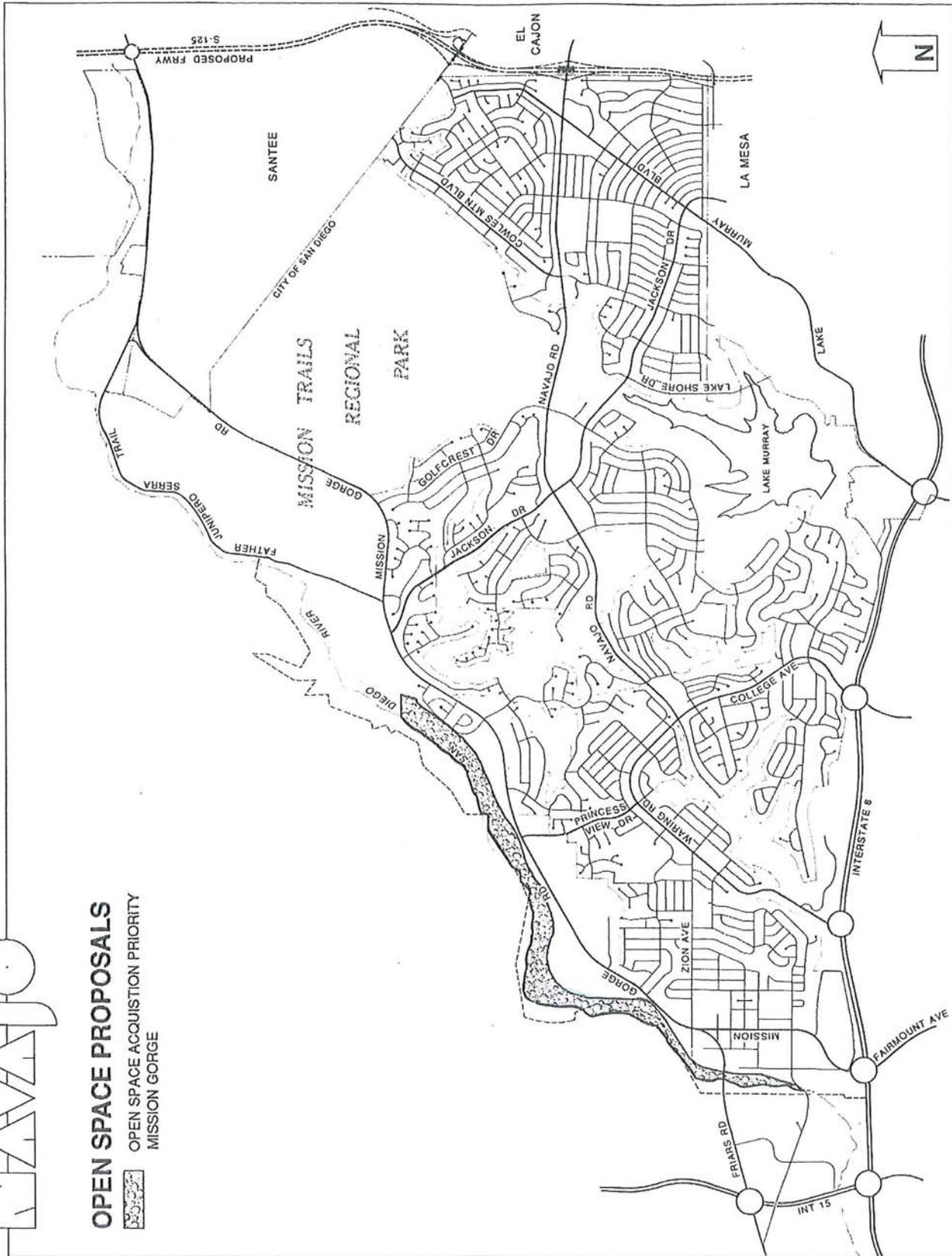
- The open space areas, including over 700 acres divided among Navajo Canyon (179 acres), Rancho Mission Canyon (258 acres), and Mission Gorge (300 acres), should be preserved in a substantially open character. Mission Gorge (the San Diego River System) should be given high priority for acquisition as a part of the City's open space system.
- Open space should initially be maintained in its natural condition. Studies, however, should be undertaken to determine uses compatible with the open space concept. Such uses, designed with consideration for topography, vegetation and access, may include archery ranges, hiking, biking trails, picnic facilities, wildlife preserves, and non-vehicular camping facilities.
- Any public improvements such as roads, drainage channels and utility services as well as any private lessee developments should be compatible with the objectives of the open space system. No through roads will be permitted except for the extension of Navajo Road, designed to parkway standards, through Navajo Canyon and the extension of Jackson Drive.
- Safe, convenient access should be established and maintained to all open space areas.





OPEN SPACE PROPOSALS

OPEN SPACE ACQUISITION PRIORITY
MISSION GORGE



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In the event that those open space areas illustrated on Figure 9 within Area 1 are not acquired they should be permitted to develop according to the following guidelines:

- Low residential densities not to exceed one dwelling unit per acre in those areas falling within the Hillside Review (HR) Zone, except as noted below.
- In all cases improvements should implement the intent of the HR Zone, which is to “ensure that development results in minimum disturbance of natural terrain.”

The implication of this requirement is that densities for a given ownership will be transferred and clustered on the flattest and most developable land.

- Development in open space areas, including those in the HR Zone, should be guided by the following additional criteria:
 - Slopes of 0-12 percent should be permitted to develop up to two dwelling units per acre.
 - Slopes of 13-24 percent should be permitted to develop up to two dwelling units per acre.
 - Slopes of 25 percent and greater should be permitted to develop no more than one dwelling unit per acre.

An exception to the above guidelines is:

- That portion of the Navajo Canyon westerly of Waring Road, which should be limited to residential use of one dwelling unit per acre due to its location, restricted access and projected traffic conditions. A Planned Commercial Development for this area would be considered to allow for uses compatible with the open space concept, such as plant nurseries, etc.
- Residential development within the canyons should be designed to preserve natural amenities such as topography, trees and streams in an open space linkage system. Further studies would have to be undertaken to minimize problems such as drainage, unattractive hillside cuts, access, and inadequate public facilities resulting from increased population.
- If the canyons adjacent to existing or future school sites are not obtained for open space, a neighborhood park with a minimum of five acres should be reserved next to each school.
- Development along the San Diego River should be regulated to minimize disturbance to wetland habitat areas. A Wetlands Management Plan, completed as an element of the Mission Valley Community Plan, has established guidelines for development of the southernmost portion of the river located in the Navajo community (from Friars Road to Camino Del Rio North). These guidelines have been incorporated into the Community Plan Implementation Overlay Zone (CPIOZ) supplemental regulations described in the Industrial Element. All development **adjacent to the San Diego River shall observe the Draft Design Guidelines of the San Diego River Park Master Plan.** ~~proposals for property within 150 feet of the San~~