

# NORTH BAY CONCEPTUAL PLAN

Commissioned by:

North Bay Association—3045 Rosecrans Street, Ste. 207 San Diego, CA 92110

Phone 619-778-5888

Prepared by:

New City America, Inc.—2130 Columbia Street San Diego, CA 92101

Phone 619-233-5009

# **NORTH BAY CONCEPTUAL PLAN:** *A Vision of the Midway/Sports Arena Community*

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## FIGURES

All photographs, maps and drawings by the design team except those noted herein.

### Chapter One

Figure 1-1 source: *San Diego California Cornerstone* by Iris Engstrand

### Chapter Four

drawings by Hung Le  
Graduate Student, NewSchool of Architecture & Design

## MAPS:

(Base map figure provided by SanGis.)

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## CREDITS

Project Coordinator: Marco Li Mandri, New City America, Inc.

Editor and Urban and Architectural Design Standards: Kurt Christian Hunker,  
Kurt Hunker, Architect

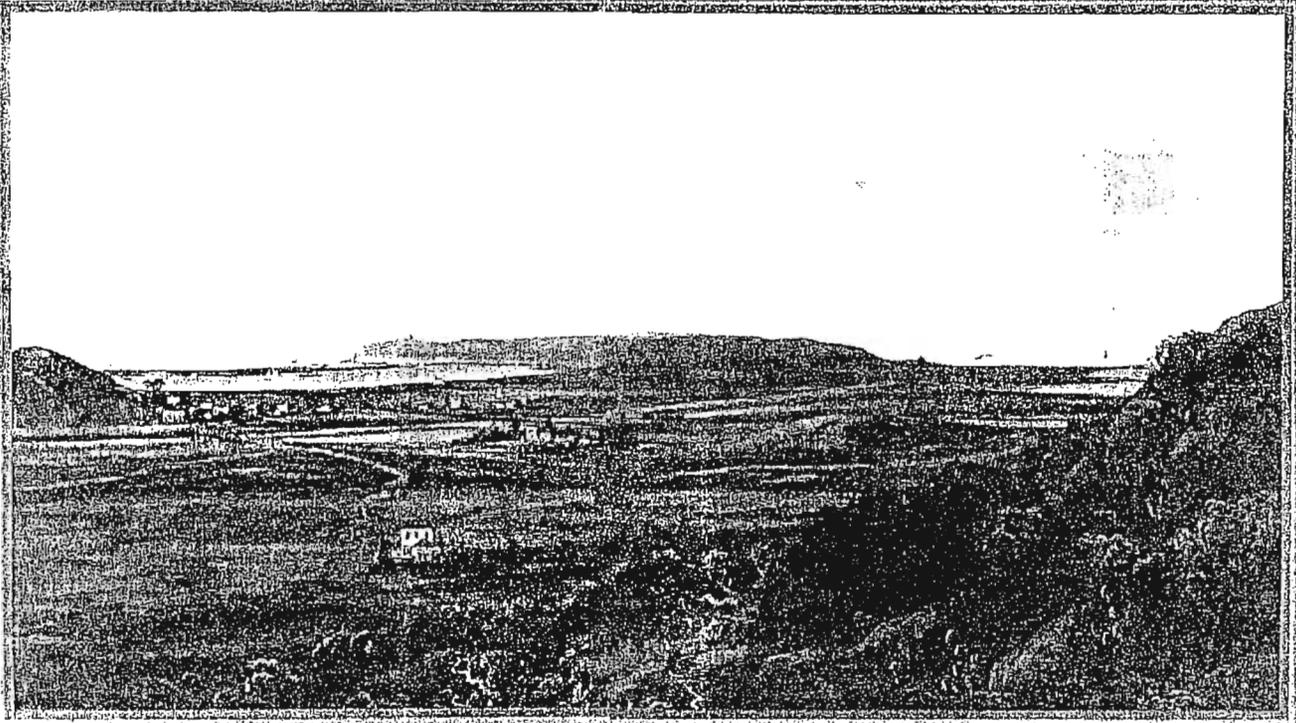
Landscape Design Standards: Martin Schmidt, Environs

Traffic and Transit Plan: Alan Hoffman, The Mission Group

Layout and editorial assistance: Christopher M. Gomez, New City America,  
Inc.; Kenny Price, Kurt Hunker, Architect.



**SECTION I - CONTEXT**



**North Bay District  
Circa 1900 looking from current site of University of San Diego**

## CHAPTER ONE - INTRODUCTION

It is clear that the North Bay area—the part of San Diego now known as “Midway”—suffers from an “identity crisis”. Two perceptions exist, neither positive. To many, the area simply does not have an identity. That is, the North Bay is not viewed as a whole with definable, comprehensible elements and boundaries. It does not provide a memorable set of images by which to distinguish itself from any other neighborhood of San Diego.

The second perception is likewise problematic: North Bay has an undesirable identity. The images associated with it are not those the business or residential community wishes to promote. The preponderance of heavy traffic and the existence of some uses, such as adult clubs and bookstores, lend the area an unfavorable identity. In addition, the one indisputable landmark of the North Bay, the San Diego Sports Arena, is seen by many as outdated and unattractive.

Details of these perceptions are discussed in later sections of the document. They are however a critical starting point for many of the changes proposed herein. Perhaps any other district in the city, the North Bay is an area of unrealized potential and opportunity. Decades of haphazard, incoherent and (at best) indifferent development have created a commercial corridor that is simultaneously vibrant and disjointed, filled with a bewildering variety of architectural styles, land uses, densities, scales, and so on. It seems as if the area became the “district of leftovers”—adult strip clubs, traffic congestion, a chronic homeless problem and an unusually high concentration of publicly owned land, all co-existing with some of the most successful “big box” retailers in the County of San Diego. Thus, the identity problem.

For some, the North Bay represents San Diego’s last significant urban frontier. Its central location, available open land and commercial vitality all make it especially attractive for new development. It was the widespread recognition of this fact, and the desire on the part of many to alter an unfortunate image, that led to the call for a conceptual plan for the district.



*A Lack of Identity...*



*...an Undesirable Identity*



## Goals and Objectives

This conceptual plan document has been developed to improve the physical appearance and image of the entire North Bay, area as well as to guide new development, and the improvement of existing development. It articulates a broad “vision” for the area, and it establishes mandated urban, architectural and landscape design standards. For clarity, a few overarching *goals* are defined below that provide the focus for this Conceptual Plan:

- 1) To create the framework for a “new” North Bay that has a strong, positive identity.
- 2) To provide guidance for long-range development that does not exist.
- 3) To consolidate the work of many groups, and the standards of several documents, into a single coherent, comprehensive, whole.

Several *objectives* have been set forth in the Conceptual Plan that together provide the means towards achieving these goals:

- 1) A “conceptual model” is created that defines the area through the application of accepted urban design elements such as “portals” and “nodes”, which provides the means for strengthening or inventing a positive identity;
- 2) Architectural design standards for three “styles” or vocabularies are mandated to promote visual consistency at the scale of individual buildings;
- 3) Landscape standards are provided for street trees and shrubs, to further regulate the image;
- 4) Traffic standards are included for revisions to existing streets, to complement the urban and landscape standards;
- 5) A transit plan is proposed for the North Bay to strengthen connections to other parts of the city;
- 6) Community signage, in the form of banners and monuments, is included to better express the area’s history and reinforce its identity;
- 7) Consistent street lighting is specified;
- 8) Land use and zoning changes are identified that will promote the vision of the Plan.

## Acknowledgments

The planning team wishes to acknowledge the considerable contributions of the members of several organizations without whose enthusiasm, interest, energy and vision the creation of this Conceptual Plan would have been impossible:

North Bay Association

Midway Community Planning Group

Council District 2

North Bay Project Area Committee

City of San Diego Planning Department

City of San Diego Community & Economic Development Department

San Diego Redevelopment Agency

Community Volunteers:

Charles Pretto, President of the North Bay Association

Eric Munro, Vice President of the North Bay Association

Nancy Rossi, Treasurer of the North Bay Association

Tom Whalen, Secretary of the North Bay Association

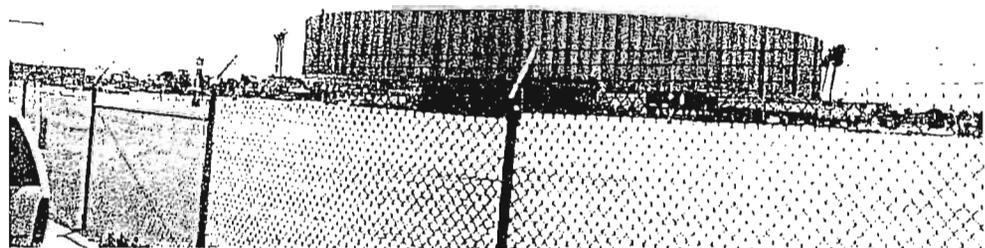
Sunday Garrett, Board Member for the North Bay Association

Special Mention:

*For their vision and leadership, for without which this effort would not have been undertaken or accomplished.*

Leslie Sanguinetti, Chair of the Midway Community Planning Group

Joe Mannino, Executive Director of the North Bay Association



*Local Landmark: The San Diego Sports Arena*

## Executive Summary

The North Bay Association (NBA) is the local non-profit management corporation that operates the North Business Improvement District. The NBA's mission is to represent the North Bay Business Community, and to beautify and improve the area. To that end, the NBA conducted a competitive bidding process for the creation of a Conceptual Plan, after which New City America, Inc. was hired.

The Conceptual Plan ("Plan") is a desire to develop and implement guidelines for development in the area, which had no conceptual plan in place. It reflects the association's keen interest in improving the image of the North Bay and in planning for a vibrant future. Standards established in the Plan will amend those in the Midway/Pacific Highway Corridor Community Plan and others, where applicable.

The planning team for New City America consisted of:

Marco Li Mandri, President, New City America, Inc. – Project Coordinator.

Martin Schmidt, Principal, Environs – landscape architecture standards.

Kurt Hunker, Principal, Kurt Hunker, Architect – urban/architectural design standards; general editorship.

Alan Hoffman, Principal, The Mission Group – transit recommendations.

This Plan starts with the general and proceeds to the specific. Chapter 1 establishes the context in which changes will occur, first by defining the broad intent behind the conceptual plan. Chapter 2 defines a brief history of the North Bay area, which helps explain why development has happened in the particular way it has. The Last part of Chapter 2 discusses the current situation.

Section II contains the elements of the proposed conceptual plan. A vision for the North Bay is defined first. The second section outlines the conceptual model, based upon widely-accepted urban design principles. Following this are chapters on various new standards for elements of the physical environment, such as architecture and signage.

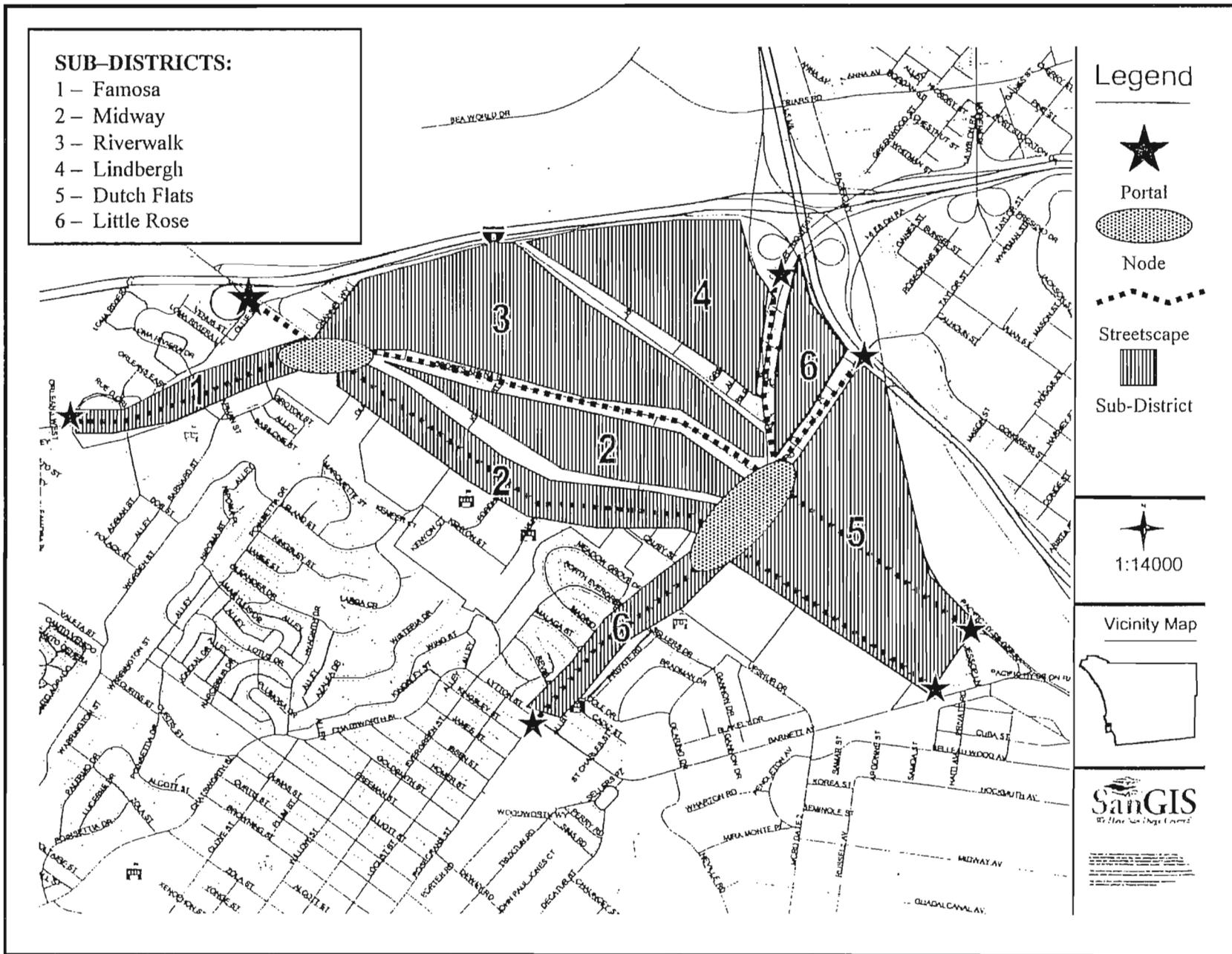


# NORTH BAY DISTRICT



*New Community Signage: see Chapter 7*

# THE CONCEPTUAL MODEL



MAP 2 – Proposed Sub-Districts & Elements

While the North Bay area is plagued with serious long-standing problems, there exists at the same time tremendous opportunities. There is also, importantly, widespread agreement that change must occur. The Conceptual Plan sets forth the guidelines needed to affect change on a community-wide basis. How change occurs in specific cases, such as at the scale of a single parcel, is beyond the scope of this document.

*A summary of the primary elements of the Conceptual Plan are as follows:*

**Portals** – seven distinct entry gateways are created, each with its own community sign and accent landscape planting.

**Nodes** – two, one East and the other West, are created. Each has a community sign, accent landscaping, enhanced paving and uniform street furniture.

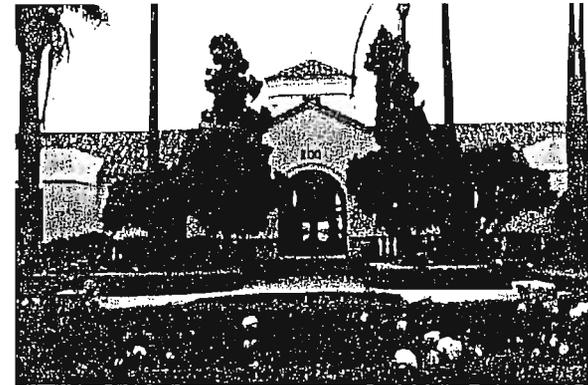
**Streetscapes** – five key commercial streets, Rosecrans Street, Sports Arena Blvd., Midway Drive, West Point Loma Blvd. and Camino Del Rio West, have mandated sidewalk landscaping elements and street furniture. Landscape medians are created on Sports Arena and Rosecrans.

**Sub-Districts** – six “sub-districts”, or neighborhoods are created: Famosa, Midway, Riverfront, Lindbergh, Dutch Flats and Little Rose. Each contains specific street trees and signage.

**Open Space** – a major public park space, or spaces, is recommended at the Sports Arena redevelopment site as an integral part of any proposed redevelopment plans. A pedestrian connection is also made to the linear open space along the San Diego River to the north.

**Architectural Design Standards** – three “styles” or vocabularies are mandated for new development along the major commercial corridors, “NTC Revival”, “Warehouse/Loft” and “Neo-Modernist”.

**Landscape Standards** – a tree and plant list is mandated for public rights-of-way. A consistent pattern of street trees is created, with accent trees at portals, nodes and other special locations.



*NTC: Architectural Inspiration  
(See Chapter 4)*

*Vehicular Traffic* – locations for new streets are identified. In addition, street configurations are modified as indicated in the *Streetscapes* section.

*Transit* – a “flex trolley” transit route through the district is identified, with its own travel lanes. This “Sun ‘n’ Fun” line travels Rosecrans Street and Sports Arena Blvd., with stops at the East Node and Kemper Street. Also, revised and/or new bus routes are recommended for the major corridors.

*Signage* – controls for commercial signage are created and a community—wide signage program is identified.

*Lighting* – a uniform “acorn”-type pole standard is specified for the public right-of-way and other public open space.

*Land Use* – the Sports Arena redevelopment site is identified as the primary location for medium or high-density housing. Mixed-use development is indicated for that site, the East Node and other areas.

Where the Conceptual Plan revises existing planning standards, amendments are indicated. These are included in the appendices.



*Community Lighting Standards: Dual Acorn with Granite Stem (along Orange Avenue in Coronado, CA.)*



*Single Acorn with Granite Stem (along Orange Avenue in Coronado, CA.)  
(See Chapter 8)*

## CHAPTER TWO - BACKGROUND

This chapter provides the context for the Conceptual Plan set forth in Section II. The first part, a “Historic Overview” of a few key events in the North Bay’s long history, notes elements that may be integrated into strategies that will strengthen the area’s identity. The second part is an analysis of existing conditions that we feel must be addressed in order for the Plan to succeed.

### Historic Overview

Of the centuries of history connected with the North Bay, a handful of significant events are important in understanding the area’s development. They also form the core of a “cultural history” that may find expression on community signage and banners and in public art projects that will help reinforce a unique identity. Five are briefly discussed here.

#### *The La Playa Trail*

*ROSECRANS STREET IS THE OLDEST CONTINUOUS COMMERCIAL CORRIDOR ON THE WEST COAST.* This district is truly where the colonial interests of the old world met the pastoral and nomadic lifestyle of the Native American populations on the West Coast of the United States. The Pacific voyage of Juan Rodriguez Cabrillo sailing under the flag of Spain in 1542 marked the first time that Europeans recorded their impression of San Diego.

On September 28, 1542, Cabrillo and his crew landed in San Diego Bay, anchored on the lee side of Point Loma near Ballast Point and stepped ashore. Cabrillo was greeted by natives living near the bay and departed a few days later to continue his journey.

In 1769, over land and sea bearing expeditions centered on San Diego. The land and sea parties converged in San Diego by July 1, 1769 and set up an encampment on a hill where the presidio would be built, above today’s Old Town. The significance of Rosecrans, previously known as “La Playa Trail”, gave the



*Historic marker for La Playa Trail: The Oldest Commercial Trail in the Western United States.*

North Bay district the distinction of being the link between the landing at the Point and the first overland trail to the fresh water of Mission Valley.

### *Midway*

In the late 1800s as the electric car system was defining the City through the development of the early suburbs, the area took on a functional name. The early wealth in San Diego was not based upon any strategic natural resources, except its excellent deep-water bay, which served as a significant trading harbor, and its large expanses of land. Since San Diego was relatively flat, it was easy to run electric lines to new land development communities in the beach areas and along the upper mesa south of Mission Valley. The Midway District was named appropriately since it was midway between Downtown San Diego and the newly developed suburbs of Ocean Beach and Mission Beach. Midway Drive was the road that took one from Downtown to the beach in Mission Beach.

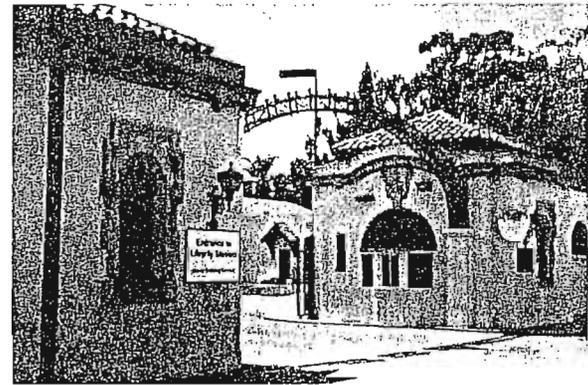
### *The United States Navy*

The district represents the introduction of the Navy to the West Coast and San Diego. William Kettner, a charter member of the Aero Club of San Diego and a long promoter of the city was elected to Congress in 1912 and began to promote the city as an ideal location for the military.

With Fort Rosecrans and the Marine Base established in 1919, followed by the opening of the West Coast's first Naval Training Center in (Enter date here) near the current North Bay district, the military was destined to shape the city for the next 100 years.

### *The Aerospace Industry*

Due to Kettner and other city leaders' work, the Midway District became the home of the commercial aerospace industry in the country. Based on the former Dutch Flats (the Post Office site), and through the entrepreneurial vision of businessmen such as Ryan and Montgomery and the heroics of Lindbergh, San Diego became the leader in the emerging aerospace industry between World



*The US Navy presence: the Naval Training Center*

Wars I and II.

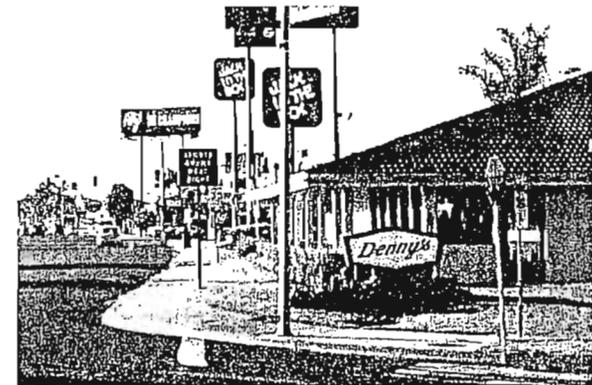
(Marco to add something on Lindbergh.)

### *Commercialization*

By the 1960's the Midway area was a mixture of industrial and commercial operations. Problems with traffic congestion, signage, and overhead utility lines were evident throughout the community. Today most of the government wartime housing has been replaced by a variety of commercial land uses. Light industrial land uses have remained along the fringe of the Midway commercial core and are interspersed with commercial development along most of the Pacific Highway Corridor.

Although Midway was once considered almost exclusively as an industrial area, rising land values have caused a shift from industrial activity to commercial. Today, most of the industrial land has been encroached upon by commercial uses with only the Kurtz Street/Camino Del Rio area and the Pacific Highway Corridor left as industrial districts.

(Sources: The Midway Community Plan, San Diego Daily Transcript, the San Diego Historical Society and San Diego, California's Cornerstone by Iris Engstrand).



*Commercialization: Camino del Rio West*



*After—Additional Street Lighting and Gateway Signs*

## An Analysis of Current Conditions: Key Relevant Issues

Any balanced analysis of the current state of the North Bay area indicates that, while significant constraints to improvement exist, a number of opportunities are also available. Rather than presenting information about the district's status which may be found in other sources (such as the Midway/Pacific Highway Corridor Community Plan, pages 13-46, for example), this section aims to highlight the critical issues—both constraints and opportunities—at the present time. Alternatively, constraints can be read as “problems”; opportunities as “strategies” as the case may be. Discussed in the following paragraphs (and listed in no particular order) are:

### Key Relevant Issues

Traffic Issues

Transit Coverage

Industrial vs. Commercialization

Adult Entertainment Land Uses

Building Height Limitations

Housing Shortages

Homeless Population

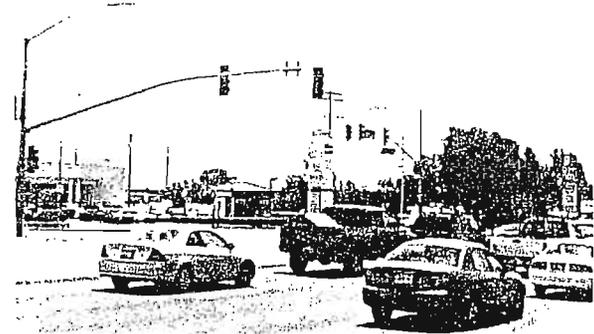
Sports Arena Site

Kobey's Swap Meet

Bay—to—Bay

Area Image

Signage



*Traffic Issues: high volume and lack of pedestrian amenities.*



*Parking Issues: Overwhelming Automobile presence and lack of spatial definition of street edges.*

Site Planning

Redevelopment Agency Participation

Open Space

Maintenance Assessment District

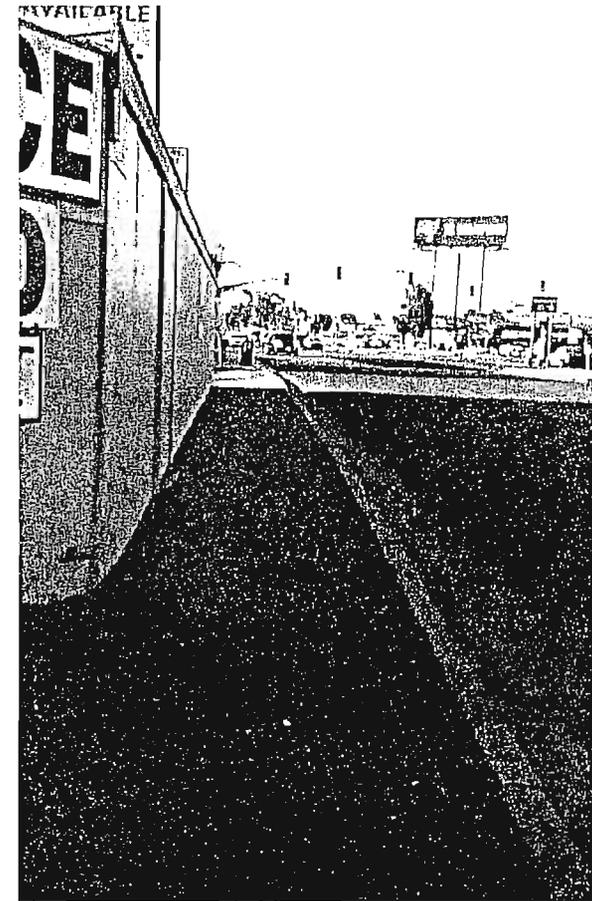
BIG BOX Superstores

*Traffic Issues*

One of the most frequent complaints about the Midway area concerns the huge volume of daily traffic serviced by the area's commercial corridors that dominates the physical environment. Interestingly, there is a decline in the traffic at the busiest location, Rosecrans Street between Sports Arena Blvd. and Midway Drive, from almost 64,000 trips in 1990 to 51,300 in 2000. This is characteristic of much of the heaviest traffic volume in the area, and may be attributable to the closure of the Naval Training Center and its subsequent redevelopment. As construction ends at NTC (now called Liberty Station), the volume may increase.

Also contributing to a widespread perception of the overwhelming presence of the automobile is the existence of acres of surface parking, much of it insufficiently landscaped and prominently displayed along commercial streets. It does very little to contribute to any sense of "place".

Despite the intensity of traffic, opportunities exist. Plans for enhanced streetscapes via more consistent and generous landscaping and street furniture, lane narrowing in places and separation of local and through traffic will be discussed in Section II.



*Existing streetscapes: No well-defined pedestrian environment.*

*Transit Coverage*

The Transit Plan contained in Section II provides a detailed analysis of the present shortcomings of mass transit in the North Bay and will not be repeated here. The primary criticism of the analysis is that “existing transit services offer slow and infrequent connections to and from the North Bay”.

MTDB’s “Transit First” Showcase route, discussed in Section II, is an opportunity for highlighting transit. “Flex trolleys” and other transit forms are being proposed and built in cities with transportation problems much like the North Bay’s. There are opportunities for incorporating dedicated transit lanes on very wide streets such as Sports Arena Blvd.

*The Industrial Economic Base vs. Commercialization*

While there has been a transformation of the North Bay from an industrial zone to a commercial center, opportunities exist within this change. Obviously, heavy industrial uses are not desirable in a location so close to San Diego’s downtown and population center. Fortunately, the small industrial sector in the North Bay, mostly contained in the northeast quadrant defined by Interstate 8 on the north, Kurtz Street to the south and west and the I-5 off-ramp to the east, is a viable presence in the area and a useful reminder of its industry past.

At the same time, an appropriate blend of mixed—use retail/commercial, while contributing to the traffic congestion mentioned earlier, is vital to the health of the North Bay and critical to any residential development that will occur. There is strong leadership in this sector.

A wide variety of commercial uses can be found throughout the Midway/Pacific Highway Corridor Community. Approximately 273 acres of the total net land area in this community (excluding streets) is used for commercial purposes. The community is generally known as a commercial-retail area, since most of its major streets are lined with varying types and sizes of commercial uses. It contains more than a dozen shopping centers, ranging in size from two to 40 acres, a variety of strip commercial areas, and several stores on individual lots.



*Commercializing: “big box” retail*



*Former military housing In North Bay.*

According to The Midway Community Plan, “While nearly 50 percent of the community’s total acreage is occupied by commercial use, only about 30 percent of the total area is zoned for commercial purposes. Commercial uses have encroached into the industrial areas of the community under the regulations of the Midway Planned District. Although the Planned District was developed as an industrial zone, approximately 65 percent of the uses in the planned district are commercial in nature.” Though some would say the increase of commercial space at the expense of industrial space is a good development, this is not necessarily true.

Historically, industrial and manufacturing industries create higher paying jobs than those in the retail, commercial and clerical sectors related to professional services. While it is a positive move that industrial and manufacturing firms that have polluted communities in the past have been segregated now from where people live, there are industrial concerns which may have a positive benefit due to their non-polluting nature.

This issue is brought to the attention of the North Bay community since one of the last strongholds of industrial space in the city proper is to be found along Pacific Highway and along Interstate 8, west of I 5 in the North Bay District. It is important to realize that the conversion of industrial space might have short term benefits in terms of yielding sales taxes for the city, however the long term impact will drive such high paying jobs out of the city, further aggravating the limited freeway system as people drive to their relocated jobs.

San Diego County’s office vacancy, as reported in the San Diego Daily Transcript on January 24<sup>th</sup>, 2003, rose from 9.7% in the 4<sup>th</sup> quarter of 2001 to 11.6% in the fourth quarter of 2002. (Update these numbers.)

In comparison, the vacancy rates for industrial space regionally is 8.1% and only 5% for multi-tenant industrial uses. It is clear that industrial space is at a premium and will continue to be in the foreseeable future.

#### *Adult Entertainment Land Uses*

Strip clubs and other forms of adult entertainment have a long history in the North Bay District. For many years, they served the booming Naval Training



*Signage at the gateway to North Bay and Point Loma.*

Center, MCRD and other military establishments. Over the years market forces and favorable zoning conditions in North Bay, namely, the lack of housing, schools and churches; has resulted in an influx of an over abundance of adult uses in the area. At present there are five strip clubs and five pornographic peepshows all within the Midway Community Planning Area. It is claimed we have more adult entertainment facilities than any other area of equal size in California, except for San Francisco. Without changes to the law, or the siting of uses that preclude adult entertainment, such as housing, schools and churches; the influx of numerous additional adult uses is inevitable.

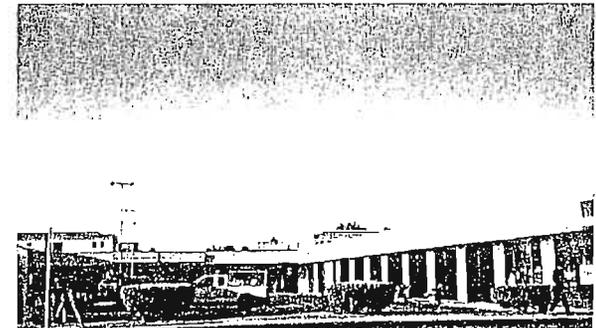
### *Building Height Limitations*

On November 7, 1972, the People's Ordinance # 0-10896 was passed which limited building heights to 30 feet west of Interstate 5. Known as Proposition D, the initiative sought to limit the height of buildings in the coastal zone of San Diego. Unfortunately, I-5 was used as a boundary for the "Coastal Zone," more for administrative ease than substantive reason. Consequently, all of the parcels in the city, with the exception of downtown, were considered to be "coastal zone" regardless of distance from the water.

The ordinance passed in every single council district of the city and was motivated by fear of the "Miami—ization" of the San Diego coastline. Unfortunately, what might have been appropriate for Ocean Beach, Point Loma, Pacific Beach and La Jolla, does not really seem applicable to the old Midway district.

Development of property above 30 feet in the Midway Area would not impede view corridors or lead to the "Miami—ization" of the San Diego coastline. Midway is far from the coast and already has a number of structures such as the Sports Arena, Sharp Hospital, even the I—8; that range from 85 to 50 feet respectfully.

The Ordinance on coastal height limits has been amended over the past few years, by a vote of the people, to accommodate greater—than—30—foot structures at the Gateway of the Americas in San Ysidro and in Sea World. A vote of the people is necessary each time the ordinance is to be amended.



*Characteristic Building Height: near Sports Arena  
Boulevard and Rosecrans Street*

Serious consideration should be given to exempting specific sub—districts of the North Bay to allow for more dynamic housing, entertainment and tourist—related developments. One may imagine how stagnant downtown San Diego would be if it were restricted to a 30 foot height limit, even though it is clearly in the coastal zone. This is a decision for the people of San Diego and one that will require much further study and debate.

### *Housing Shortages*

A lack of suitable housing is a constraint on the entire San Diego region. The North Bay is one of the rare, significant locations within the region ripe for redevelopment with a strong residential component. The area’s convenient location and availability of buildable real estate create an important opportunity.

The City’s Strategic Framework Element is a proposed amendment to the City’s 1979 General Plan. It contains a comprehensive strategy developed by the Planning Department to meet the challenges of increased density and was named “The City of Villages Plan.” Less than 10% of the raw land in the city remains available for development. An estimated 135,000 additional dwelling units must be built to accommodate SANDAG’s growth projections. Many believe this growth will not be accommodated within the framework of existing community plans.

In late 2001, the Planning Department revealed a preliminary map to the Midway Community Planning Group, which implied the massive conversion of industrial and commercially zoned land in the North Bay District to high density residential. This represents an opportunity to introduce housing into the North Bay and should be studied carefully.

### *The Homeless Population*

Much like permissive zoning and the absence of certain land—uses permit adult entertainment uses, a variety of factors attract homeless to the area, including but not limited to: the County Mental Health Facility, fertile ground for pan—handling, access to cheap food and alcohol, and numerous illegal lodging opportunities.

The North Bay Community has experienced incidents' involving homeless individuals that have resulted in all types of arrests and even the use of deadly force on three occasions. The over abundance of homeless in the area, and the notoriety of publicized incidences, contribute to the areas negative image. It is reasoned that a reduction in the homeless population will require a concerted effort led by the North Bay Association, working with the SDPD, local businesses and residents, the City and the County; to limit and/or remove all factors that attract and enable homeless.

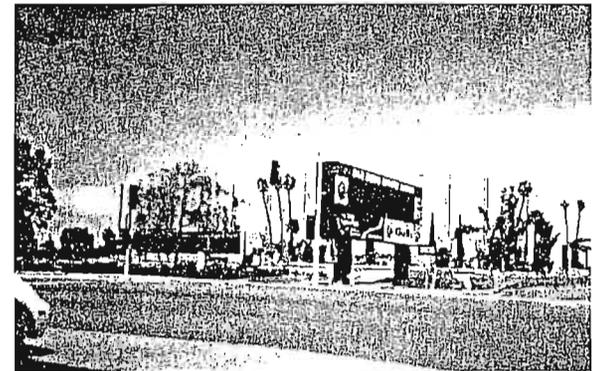
### *Sports Arena Site*

The City of San Diego owns approximately 95 acres of property in the Midway area, encumbered with a total of 14 ground leases. The uses vary from multi—family residential to office, retail, light industrial, to the San Diego Sports Arena itself. Currently the City's annual return is \$2.37 million in lease revenue. The City's Real Estate Assets Department has estimated that market returns should be in the range of \$8 to \$10 million per year at minimum.

While many Of the properties within this area are blighted and in need of substantial upgrade and investment, the 43 acre Sports Arena site stands as a unique opportunity for development, and as a potential catalyst for neighborhood revitalization. The ailing Sports Arena was built in 1966, and now operates at a competitive disadvantage, with its productive days numbered. Whether a new facility will be built on the existing site, or elsewhere, it is expected that a comprehensive and progressive plan for the site needs to be comprised. With community input, City staff and political leadership, this underutilized City asset with hopefully either be rebuilt or be transformed into a mixed—use model of urban development, eliminating a blighted area, and becoming an asset for citizens and the City alike.

### *Kobey's Swap Meet*

Kobey's Swap Meet is a San Diego landmark for 20 years, draws 1.5 million customers annually and is the third largest swap meet on the West Coast. Every Saturday and Sunday 1,000 individual businesses occupy spaces that bustle with activity. Kobey's is an economic engine for the surrounding business commu-



*Sports Arena Site (Kobey's at Northwest end)*

nity and is critical to the areas success. In addition to the millions of sales tax revenues that Kobey's generates annually well over 350 of its vendors hold San Diego business licenses.

Kobey's history as a incubator for budding entrepreneurs is unparalleled, with thousand of jobs being created from ideas brought to life at the market. Kobey's also benefits hundreds of non-profit groups who are allowed to hold their fundraisers for no charge. Thousands of dollars are raised every weekend to the benefit of these community organizations.

For the above stated reasons, and most notably due to Kobey's ability to generate such large volumes of customers to our business community, they are seen as a community asset, making it highly desirable for the business community to maintain their presence in the area and for such a land use to be incorporated into the long—term planning of the North Bay area.

#### *Bay—to—Bay Project*

The proposed *Bay—to—Bay* project would link the San Diego Bay and Mission Bay through a series of canals and waterways cutting through the heart of the North Bay District. Though the idea is decades old, it was resurrected in a State of the City speech by Mayor Susan Golding and given new life.

In 1998 the Midway Community Plan was amended to accommodate the Bay—to—Bay project, placing various restrictions on properties and parcels throughout the area. For example, the Bay—to—Bay project traverses the city—owned property at the Sports Arena site and greatly restricts the development potential of the footprint of the site. The Redevelopment Agency recently hired a consulting firm to who determined that a water link between the two bays would be unfeasible, and would cost \$455 million.

With the Bay—to—Bay projects unfeasible determination, the North Bay Association led an effort to oppose its implementation and remove it from the Midway Community Plan. Both the Midway Planning Committee and the North Bay PAC then voted to oppose Bay—to—Bay. Subsequently, on April 12, 2004, the San Diego City Council voted to delete the Bay—to—Bay concept



*Crowds filling the Sports Arena Parking lot for the Kobey's Swap Meet*

from the Midway Community Plan through a Community Plan amendment.

### *Image*

As stated in the introduction, North Bay has an undesirable and negative identity. Reasons for this are varied. The areas most negative images are that of adult entertainment and traffic congestion, having 10 adult entertainment facilities and the City's highest volume traffic intersection. Homeless brought into the community by the County's Mental Health Facility and other factors also contribute to the areas negative image. Lastly, additional contributing negative factors are various non—conforming elements regarding mixed land—uses, architectural styles, landscaping, lighting and street furniture.

### *Signage*

One of the most important issues from an aesthetic point of view is the proliferation of signage in the area. The existence of signs of all shapes, sizes, colors and materials contributes to the strong negative perceptions of the North Bay. They lend the streets a sense of uncontrolled visual chaos. The success of a streetscape program such as that proposed in this Plan, including its community signage component, depends on the moderation of private commercial signs and associated advertising. Details are covered in Section II.

### *Site Planning*

Another factor in the analysis of the North Bay involves site planning. Irregular site development patterns throughout the area have contributed to the perception of a weak, unappealing image. On some streets, buildings sited directly on the sidewalk's edge abut properties with structures placed well back on their lots. Building orientation to the street varies in many locations. Buildings sit frontally and in other instances at an angle to the street. The lack of consistency reinforces a sense of placelessness. Street design guidelines presented in Section II discuss building placement and related issues and advocate specific standards.

### *Redevelopment Agency Participation*

In May 1998, the City designated North Bay as a “redevelopment area.” As a result, The Redevelopment Agency now has several powers that can be used to improve North Bay Redevelopment Project Area. These powers include: the ability to assist property owners in the rehabilitation and development of their properties, to undertake and pay for public improvements, to finance redevelopment activities through bonds or other forms of financing, to oppose land—use restrictions and, if necessary, to acquire property and dispose of it for public or private development.

Redevelopment is intended to support and be consistent with the policies of the Midway Community Plan. In North Bay, redevelopment will focus on several projects and programs, as described in two policy documents: the North Bay Redevelopment Plan (as amended September 1998) and the North Bay 5—Year Implementation Plan (2nd Plan, July 2003). These plans in vision redevelopment activities that focus on:

- Building market rate and affordable housing;
- Building new retail and office space in several strategic areas;
- Preserving existing small businesses;
- Building traffic improvements; and
- Building open space and other community facilities.

To finance these efforts, the North Bay Redevelopment Project Area has already issued \$13 million of bonds, and additional bonds are planned in the future.

### *Open Spaces*

We recognize the need for open space to be incorporated into the area’s land—use goals, and the benefits associated with appropriate open space. To accomplish this goal, it is suggested that a variety of open space opportunities be explored, including but not limited to: traditional full—service parks occupying numerous acres of land, pocket parks of various sizes of a few acres or smaller,



*Existing Signage—Visual Chaos*

and mixed use open space incorporating commercial uses with community uses, such as open air markets with public plazas.

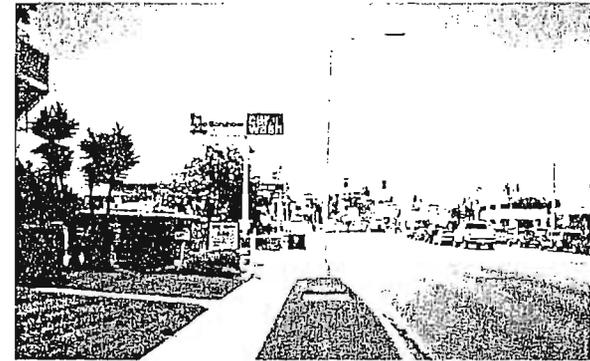
### *Maintenance Assessment District*

The formation of a Property—Based Maintenance Assessment District (MAD) is seen as a significant positive factor for North Bay. The North Bay Association wrote and was awarded a grant to investigate the formation of such a district for the area. The purpose of this district would be to fund special service conferred to real property owners and the community—at—large, including, but not limited to: sidewalk and gutter sweeping, enhanced security, homeless intervention, landscape and median maintenance, beautification of the public rights—of—way, economic development, planning, and other issues that have long—term impact on the district.

The results of the MAD investigation will determine if there is adequate support of effective property owners. If appropriate support is indicated the effort would continue, potentially forming the MAD. Without adequate support the initiative would be postponed until more favorable circumstances existed.

### *BIG BOX Superstores*

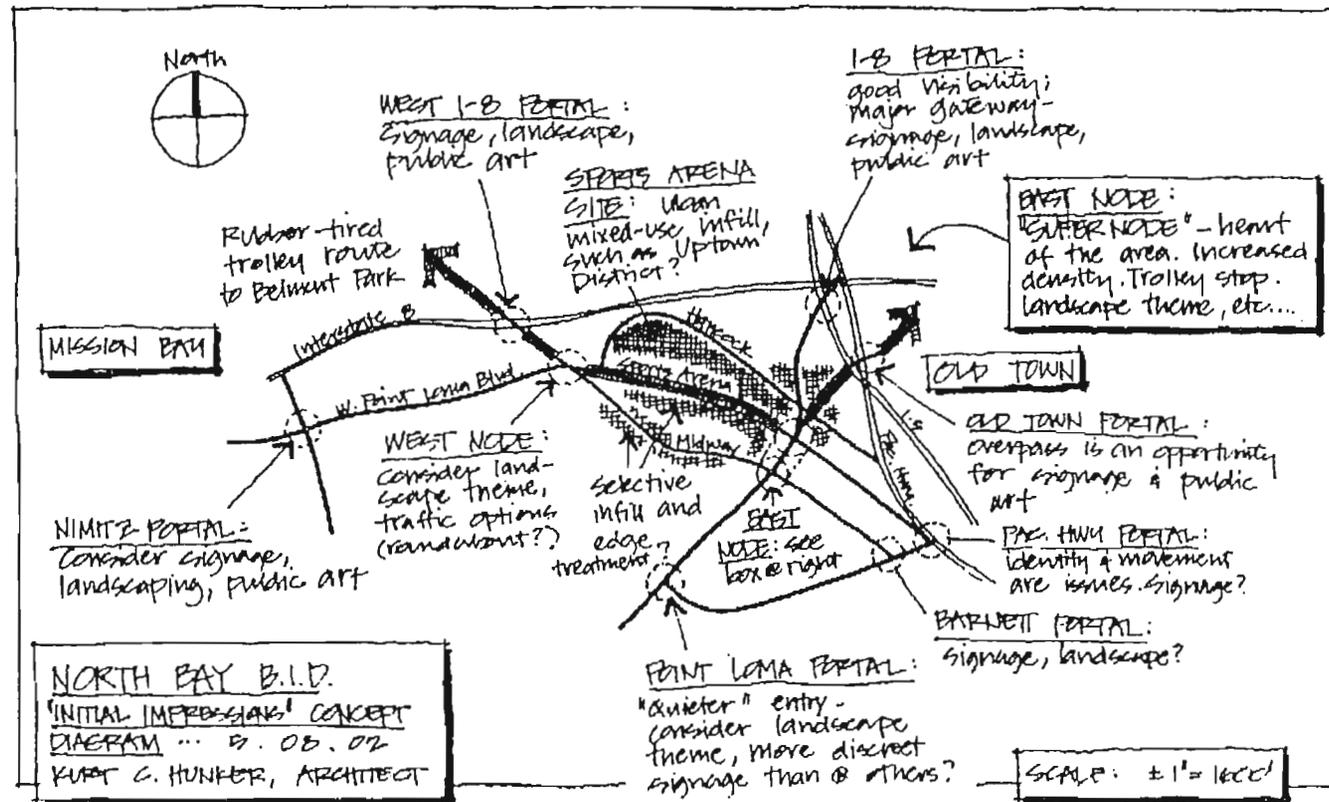
The North Bay Association and the Midway Community Planning and Advisory Committee have taken similar positions in opposition of locating BIG BOX Superstores in North Bay. Although such land—use will be guided by ordinance it should be recognized that North Bay opposes such a use, and believes it would eliminate numerous local businesses, change the character of the community, and increase traffic dramatically.



*Midway Drive with varying set-back treatment and conditions.*



## SECTION II - CONCEPTUAL PLAN



"Initial Impressions" sketch by Kurt Hunker, Architect

## CHAPTER THREE – THE CONCEPTUAL PLAN

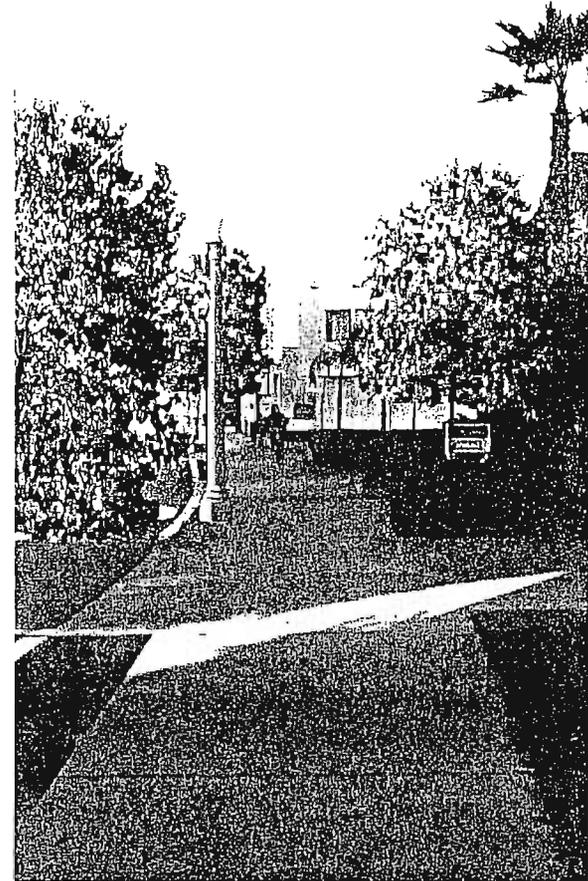
### Vision Statement: A New North Bay

We have a vision of a new North Bay. This large urban area, so ideally situated in the larger geography of San Diego, through its “rebirth” will become a model of effective and responsible urban redevelopment. Good examples already exist elsewhere, of course, and we learn from them. Think of the history in Boston’s Back Bay, the vitality of Greenwich Village, the cool ambiance of San Antonio’s Riverwalk or, in San Diego itself, the mixed-use diversity of the Uptown District.

Imagine a place of diverse and visually distinct neighborhoods, all interconnected by landscaped streets and boulevards. Imagine, along those shaded streets and boulevards attractive, well-scaled, consistently placed buildings that help define the public realm. There is pedestrian activity on those thoroughfares, drawn not only by the great variety of commerce but also by the comfortable nature of the environment. Cafes open up to sidewalk dining. Trolleys stop frequently at carefully placed, sensitively designed transit stations convenient to shoppers, tourists and residents alike. There are places to sit and relax and enjoy the company of others.

Imagine a more vital business community, mindful of the historical and cultural significance of this place as displayed on striking monument signs and banners that highlight the area’s rich history: the La Playa Trail, Charles Lindbergh, the important military presence. Imagine a mixed and substantial resident population of shopkeepers, blue collar workers, downtown business people and college students. The emphasis is one of healthy retail, commercial enterprise, and urban residential.

Imagine an area clearly defined by “portals” or gateways at its major points of entry, but connected to assets just beyond its borders. There is ready pedestrian access to a linear park along the San Diego River. Old Town is a few short blocks to the east—a pleasant walk. Liberty Station, the former Naval Training Center, forms a southern edge that attracts visitors to its many offerings. Trol-



*A New North Bay  
(Midway Drive near Kemper)*

ley service connects riders to points beyond the North Bay. Routes take them to Sea World, Belmont Park, and even the San Diego Zoo.

This is our vision of the North Bay. It is about economics, aesthetics, security, variety, history and diversity. It proposes an improved quality of life, and we believe it is achievable. It will require a serious and sustained commitment from the City of San Diego, the local business community (vis-à-vis the North Bay Association), the local planning committee, residents, developers, planners, architects and many others who will make the vision tangible.

The following parts of this section outline the steps we believe the vision entails:

- The next paragraphs explain the “conceptual model” used to organize the many components of the vision;
- Chapter 4 proposes a set of architectural design standards that prescribe three “styles” or themes for new development on the major corridors;
- Chapter 5 outlines landscape guidelines and street tree planting on those corridors;
- Chapter 6 discusses revisions to streets that are intended to mitigate the oppressive presence of the automobile, improving the flow of traffic;
- Chapter 7 proposes a new transit strategy for the North Bay, focusing on bus and “flex trolley” service;
- Chapters 8 and 9 establish standards for consistent identity signage and street lighting, respectively;
- Chapter 10 - discusses specific opportunities for creative land—use as regards to public open space and the San Diego River, as well as changes to existing zoning; and



*A New North Bay*

- Finally, Chapter 11 outlines the implementation process by which the Conceptual Plan is enforced.

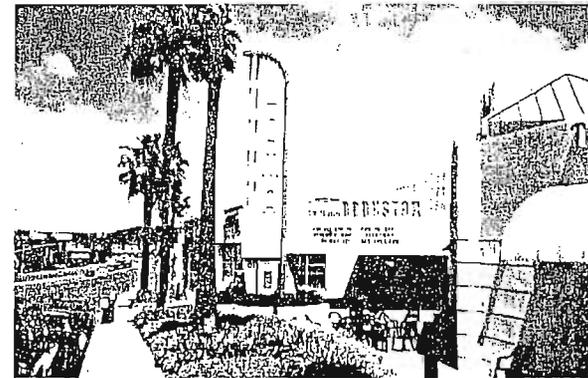
## The Conceptual Model

In thinking about a suitable plan for the North Bay, it is convenient and logical to organize the area, from a spatial, urbanistic perspective, according to its constituent elements. For the purposes of this document, those elements consist of five things: portals, nodes/open space, streetscapes, landmarks and neighborhoods or sub—districts. Connecting the elements are the circulation systems—vehicular, transit and pedestrian—that are discussed in later chapters.

*Portals* are the places where one enters the North Bay, primarily by automobile or bus. Portals are important to this plan as they are the points at which “first impressions” may be made. It follows that if one key purpose of the plan is to improve the area’s image, first impressions—vis-à-vis its portals—must be carefully analyzed and strengthened where necessary. This plan identifies and makes recommendations regarding seven portals.

*Nodes* are the significant public gathering places within an area. They are important for the simple reason that people want to be in them, for any number of reasons. Two nodes, both occurring at busy street intersections, have been identified and discussed in this document. Another kind of node is public open space, such as parks. Opportunities for significant park space in the North Bay are also identified in the plan.

*Streetscapes* include not only the rights—of—way for streets and sidewalks, but the frontage of the built form along them. Paving, street furniture, landscape and other components are part of the streetscape. Like portals and nodes, streetscapes in the North Bay must be analyzed if the image of the area is to be improved. Five streetscapes are included: Rosecrans Street, Sports Arena Blvd., Midway Drive, West Point Loma Blvd., and Camino del Rio West. These five streets form the core of commercial and retail activity and thus are significant to the planning process.



*A local landmark: the Loma Theater (now Bookstar) on Rosecrans*



Another important urban element is the *landmark*, roughly defined as something visually memorable that gives a location a sense of orientation and focus. A few landmarks exist in the North Bay, most notably the San Diego Sports Arena, the Loma Theater, the Historic Playa Trail, the military installations, the Marine Corps Recruit Depot (“MCRD”) and the former Naval Training Center (“NTC”) now called “Liberty Station”. For the purposes of the plan the latter two are not specifically dealt with as discrete urban elements. The Sports Arena site will be discussed as a prime location for redevelopment; the military sites stand at the periphery of the North Bay and are significant herein mainly as inspiration for the creation of the design standards.

Finally, six *neighborhoods*, or *sub-districts* are created that provide a means for further refining the image of the area. Each has its own distinctive signage and trees.

### **Highlights of the Conceptual Plan**

Map 3, page \_\_, shows the distribution of portals, nodes and streetscapes, each of which is reviewed later in more detail. A number of opportunities exist within the North Bay for making positive change possible. Highlights include:

Portals - great potential exists at these diverse and geographically distinct locations for landscaping, signage and public art that will “announce” one’s arrival in the North Bay. These are treated individually.

Nodes and Open Space – opportunities for more effective land use, particularly at the East Node at Rosecrans and Sports Arena/Midway, will strengthen the “sense of place” largely absent at the present time.

Streetscapes – infill strategies and standards will help reinforce the positive aspects of the existing streets, particularly along Rosecrans Street, Sports Arena Blvd. and Midway Drive, promoting visual consistency and coherence.

Landmarks – a renovated or rebuilt Sports Arena would remain as the area’s most significant landmark while its relocation or removal would open up acres of prime, centrally-located real estate for housing and mixed-use development

that will invigorate the neighborhood and comply with the City of San Diego's desire to create more housing in the area.

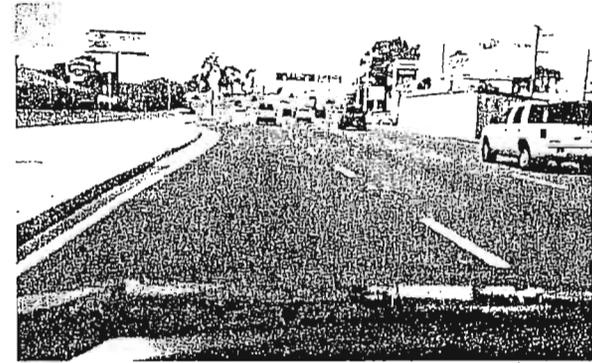
Neighborhoods or sub-districts – six are named with titles that relate to the area's heritage. They subdivide the North Bay into more manageable and identifiable areas.

### Portals

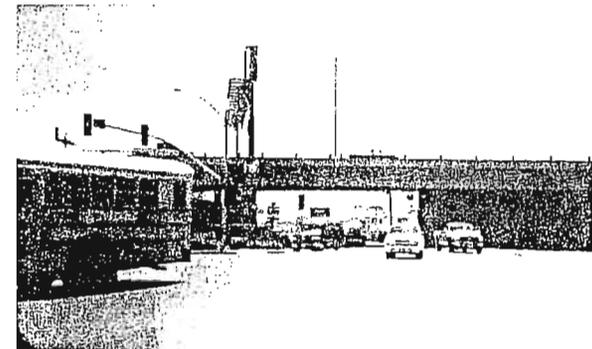
Automobile and bus traffic on the primary North Bay corridors (see Map 4, page \_\_) enters the area at seven locations. These locations are portals. They are the points along the edge of the retail/commercial zone that forms the heart of the conceptual plan. As such, the portals have the potential to establish an image for the area that will help reinforce a positive identity. A unified landscape scheme and signage are the two critical elements proposed to accomplish this. The portals are discussed in turn, beginning with that at I-8 and Camino del Rio West and proceeding in a clockwise direction around the North Bay.

The *I-8 Portal* is comprised of freeway exit ramps that feed directly into Camino del Rio West and then Rosecrans Street. The vehicular traffic volume is especially heavy at this point. The ramps descend noticeably into the area, giving drivers brief views into the North Bay from a raised vantage point. Landscape plans should be coordinated with Caltrans to create a planting plan consistent with other portals. Signage should be located in this portal that "announces" arrival in the North Bay. Where possible, plantings should be added along the edges of the portal's view corridor to help strengthen the image.

The *Old Town Portal*, occurring where Taylor and Rosecrans Streets meet at Pacific Highway, is the most "architectural" of the seven portals due to the dominant presence of the I-5 overpass. Here, the overpass creates a tunnel into the North Bay that dramatically signals a change. Planting along Rosecrans should extend as close to the intersection as possible, to establish an identity before traffic passes under the freeway. In addition, signage on the Old Town side of the intersection may announce the North Bay area. The overpass itself offers



*View towards I-8 portal from the south—existing conditions.*



*Old Town portal—existing conditions*



*General view towards Pacific Highway and Barnett portals—existing conditions.*

opportunities for public art that should be explored.

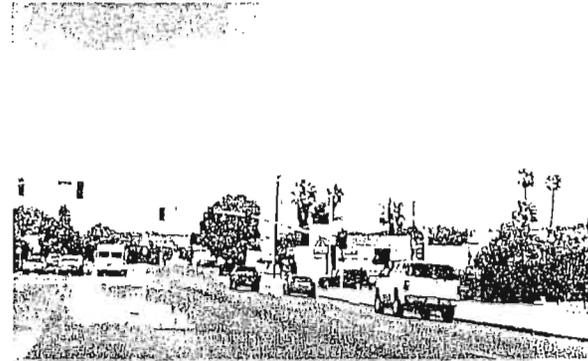
The *Pacific Highway Portal*, located at the convergence of Sports Arena Boulevard with Pacific Highway at the far southeastern edge of the North Bay, is currently in the process of transformation. The Navy's plans for the area may impact this intersection. Currently, traffic flows out of the North Bay at this point, thus the potential for creating a true portal is minimized. Future alterations to traffic patterns on Sports Arena Blvd., such as connecting Sports Arena Blvd. through the Rosecrans intersections, may allow for the development of a portal here, which should include signage and landscaping consistent with other portals.

The *Barnett Portal* is at the junction of Midway Drive and Barnett Avenue. Strategies for strengthening the sense of "entry" into the North Bay at this portal should include streetscape and landscape improvements on Midway, possibly including the traffic island at the intersection, and signage.

The *Point Loma Portal* occurs at the intersection of Rosecrans and Lytton Streets. Street plantings on Rosecrans should extend up to this point, which offers a view down and into the North Bay. Signage should be included as an element of the portal treatment. Potential future redevelopment of the service station site (southeast corner), and properties fronting Rosecrans, east of the service stations, should be undertaken with a view towards reinforcing the sense of entry that this portal should provide.

The *Famosa Portal* lies at the western end of the plan area, near the intersection of Famosa and West Point Loma Boulevards. Its distance from the center of the North Bay reinforces the need for an "entry marker" at this location. Signage and landscaping consistent with the other portals should be considered here.

Finally, the *West I-8 Portal* is the point at which traffic enters the North Bay from both West Mission Bay Drive and the I-8 off-ramp. The street landscaping on Midway Drive should extend to this location, supplemented by signage.



*Point Loma portal—existing conditions*

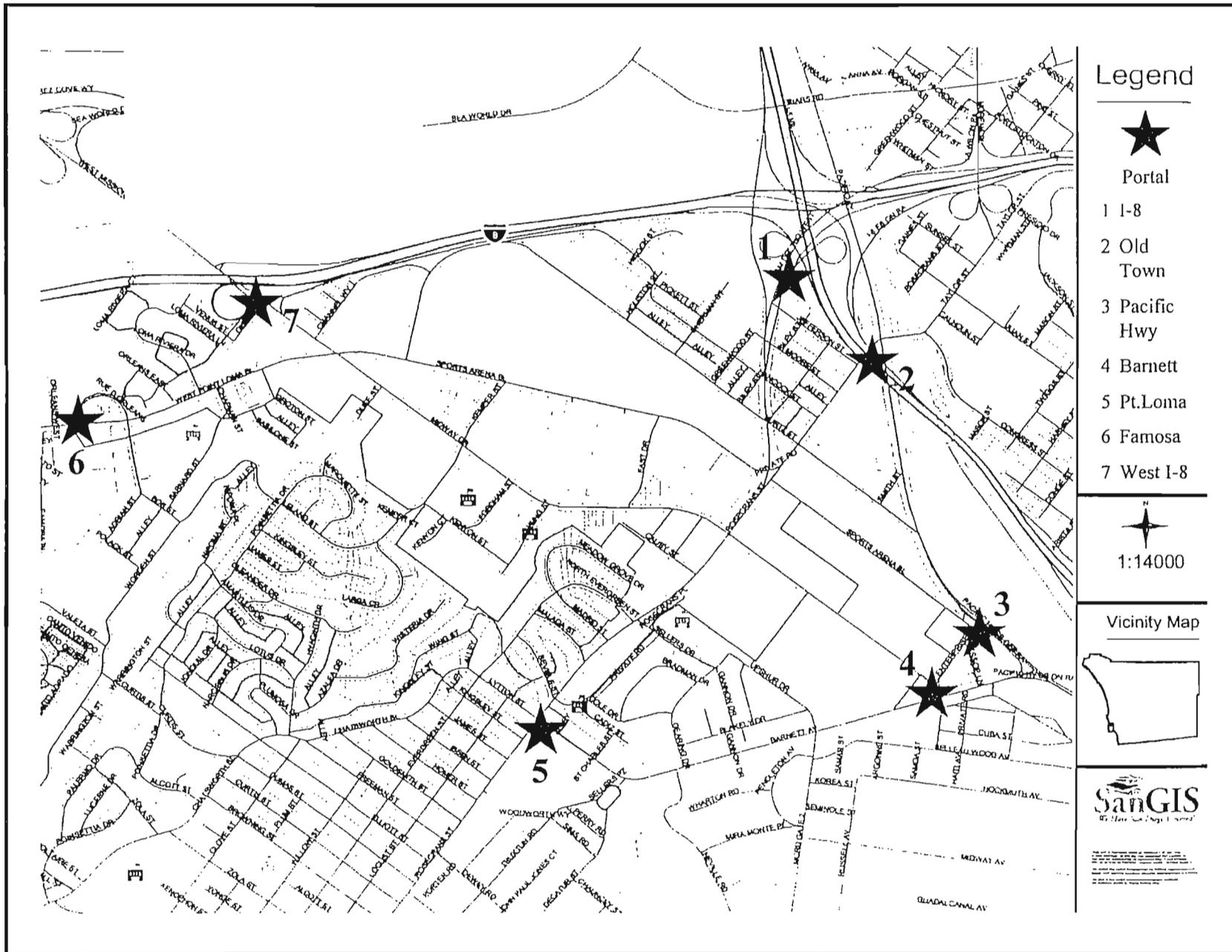


*Vista towards Famosa portal—existing conditions*

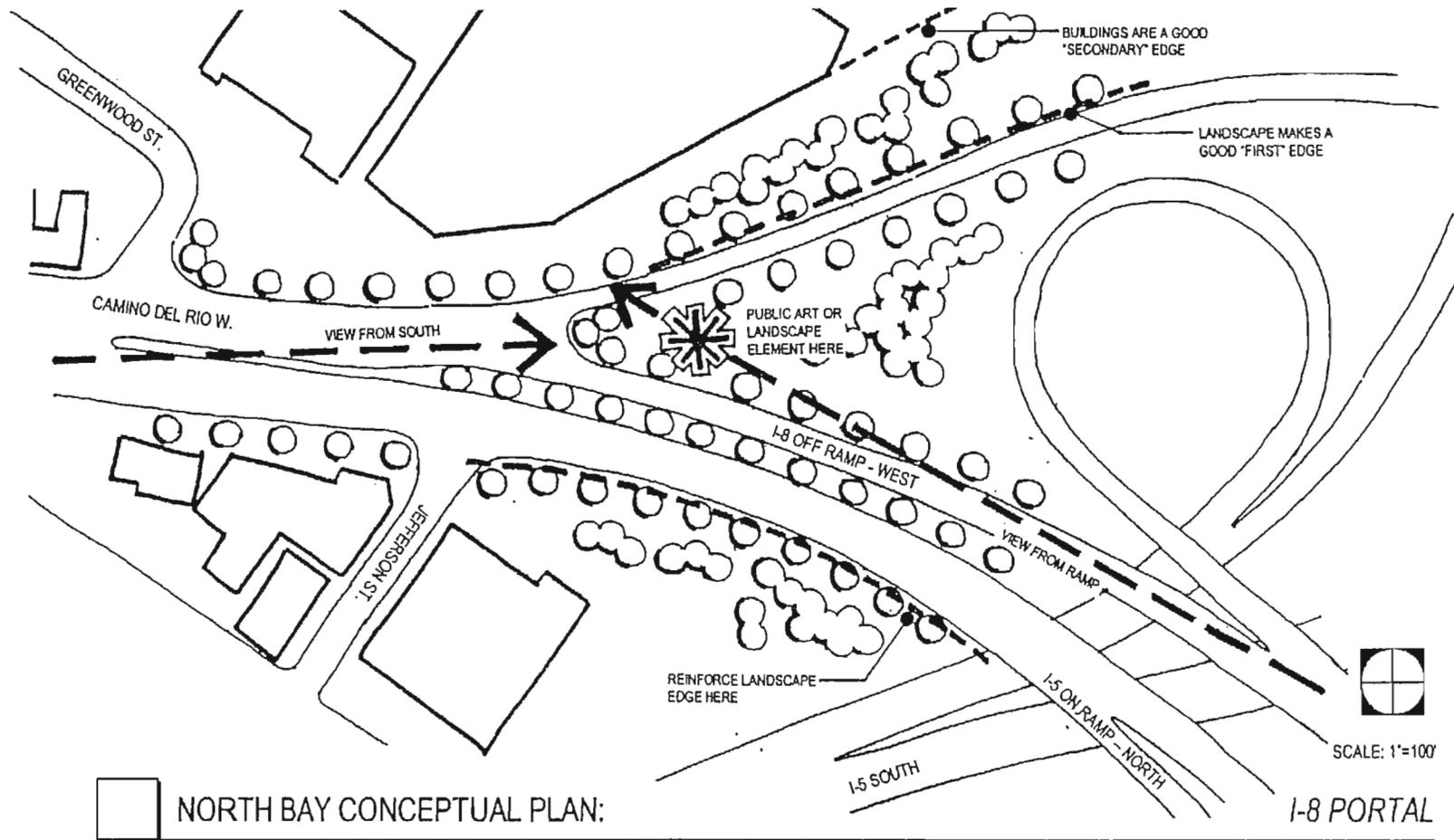


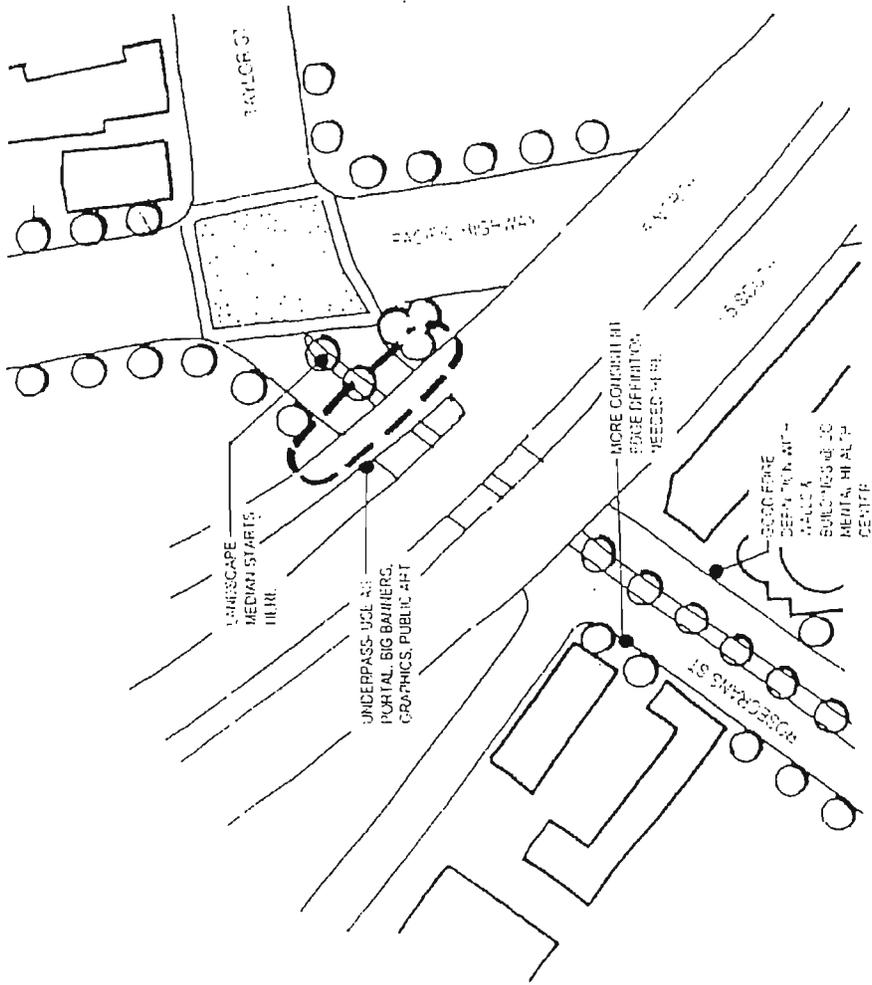
*Looking northwest towards the West I-8 portal—existing conditions.*

# PORTALS



MAP 4 – The seven entry points into the North Bay District.

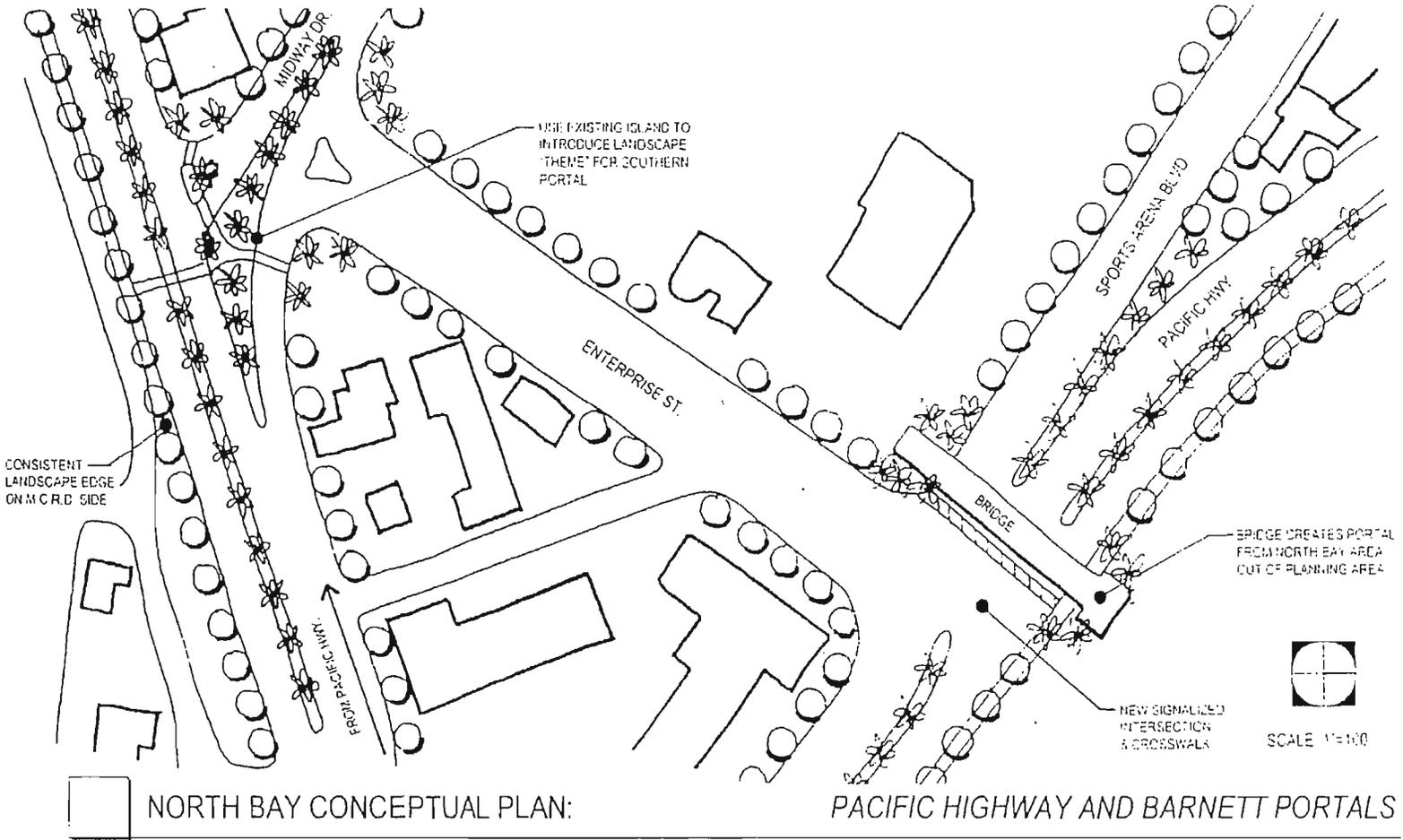


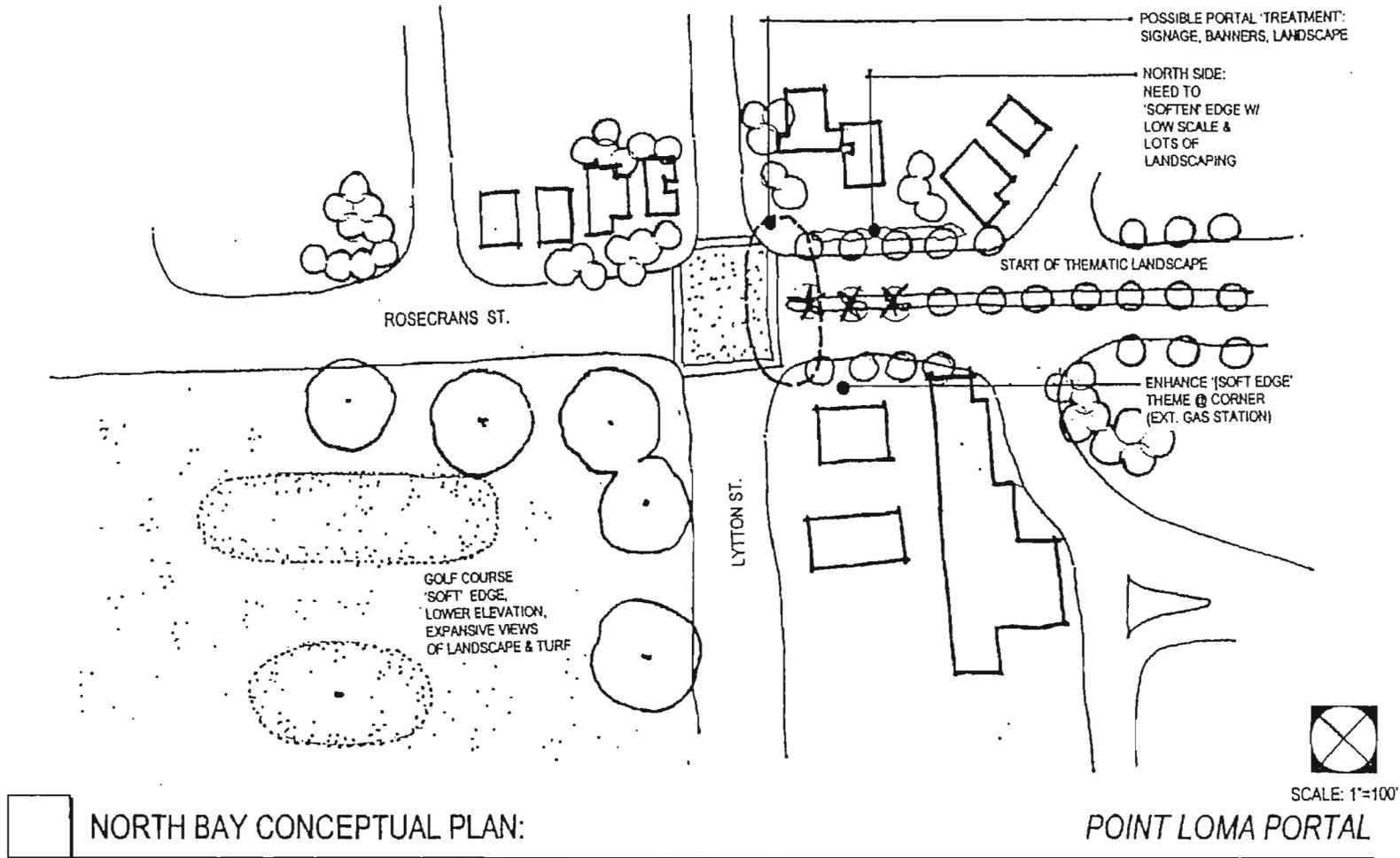


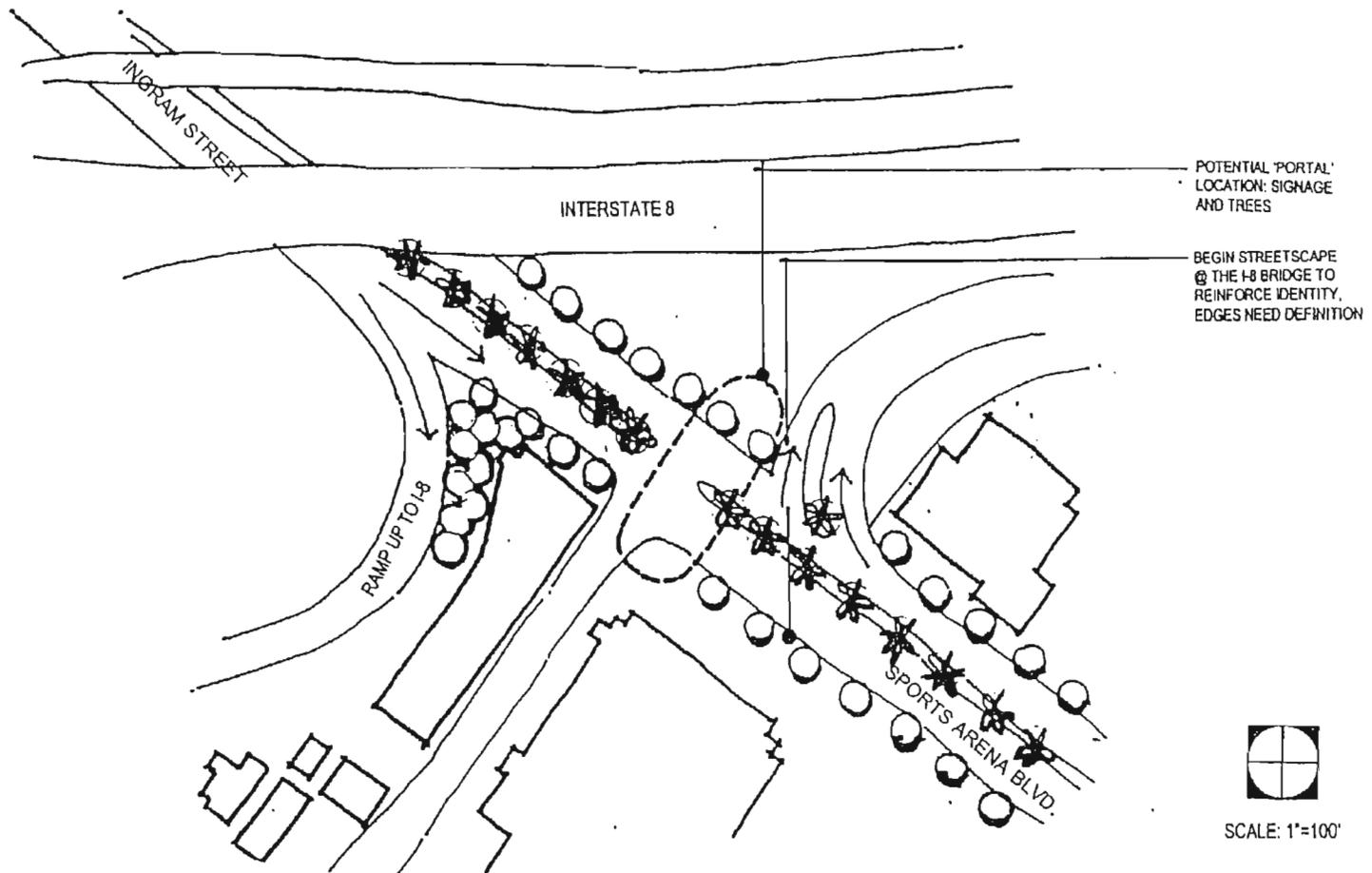
SCALE: 1" = 100'

OLD TOWN PORTAL

NORTH BAY CONCEPTUAL PLAN:







NORTH BAY CONCEPTUAL PLAN:

WEST I-8 PORTAL

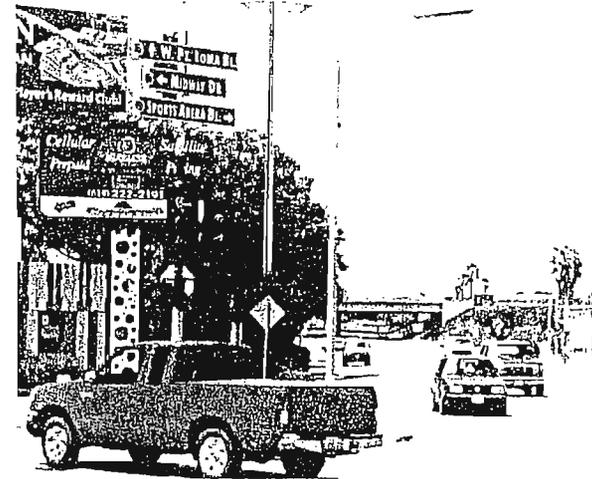
## Nodes and Open Space

Two significant locations within the North Bay area offer opportunities for creating that essential ingredient of a neighborhood, a sense of “place”. These areas are called “Nodes,” and are both highly visible, heavily traveled street intersections where thematic streetscape treatment, banners, signage, enhanced paving and careful positioning of future buildings can create a positive identity and signal “arrival” in the North Bay.

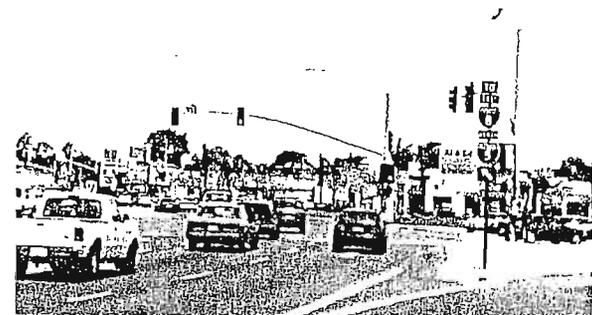
The smaller of the two is the *West Node*, located at the intersection of Midway Drive, Sports Arena Blvd. and West Point Loma Blvd. Existing well landscaped residential developments on the north and south sides of the intersection provide a pleasant backdrop. The establishment of a node here involves additional landscape planting (including the use of a “signature” tree type), enhanced paving for the traffic surface, and thoughtful positioning of future infill building to strengthen the perception of a well-defined public space.

The *East Node* is a larger, busier area that centers on two important intersections. At the northeast end is the Camino del Rio West/Sports Arena Blvd./Rosecrans intersection. One block southwest is the other end at Rosecrans and Midway. Strategies similar to those proposed for the West Node apply here, albeit on a more extensive scale. Streetscape enhancements, thematic landscaping, signage, enhanced paving and studied placement of future buildings will create a “place” at this busiest location in the entire North Bay.

*Open space* possibilities are extensive, given the amount of under—developed and publicly-held land. The Famosa Slough is one existing open space that marks the west end of the area. The San Diego River, with its linear open space along the south bank, should be explored as a significant open space opportunity. A pedestrian connection over I—8 is proposed, tying the river to the North Bay at the Sports Arena site. Finally, the Sports Arena site itself signifies an opportunity for a new park in the district.

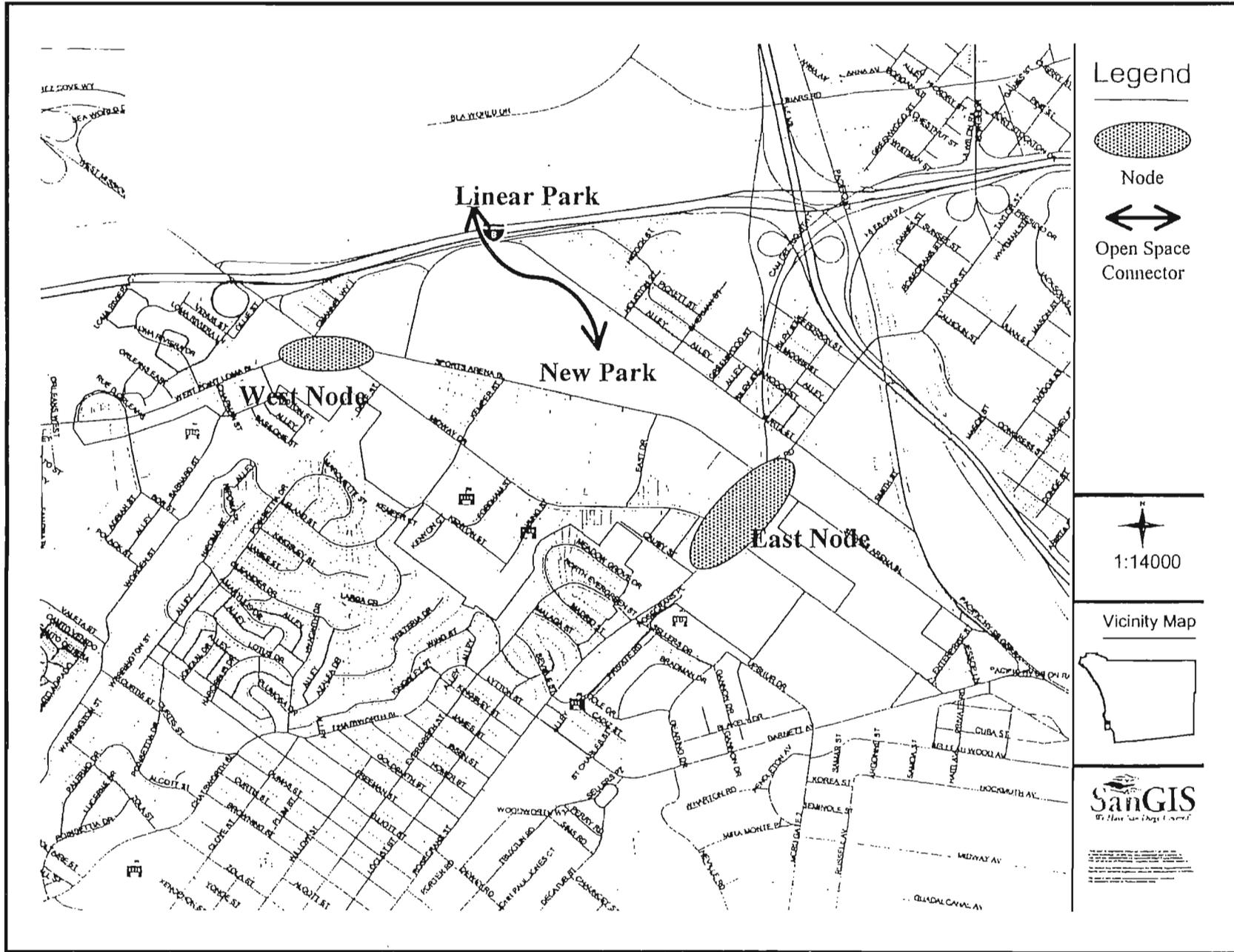


*West Node—existing conditions*

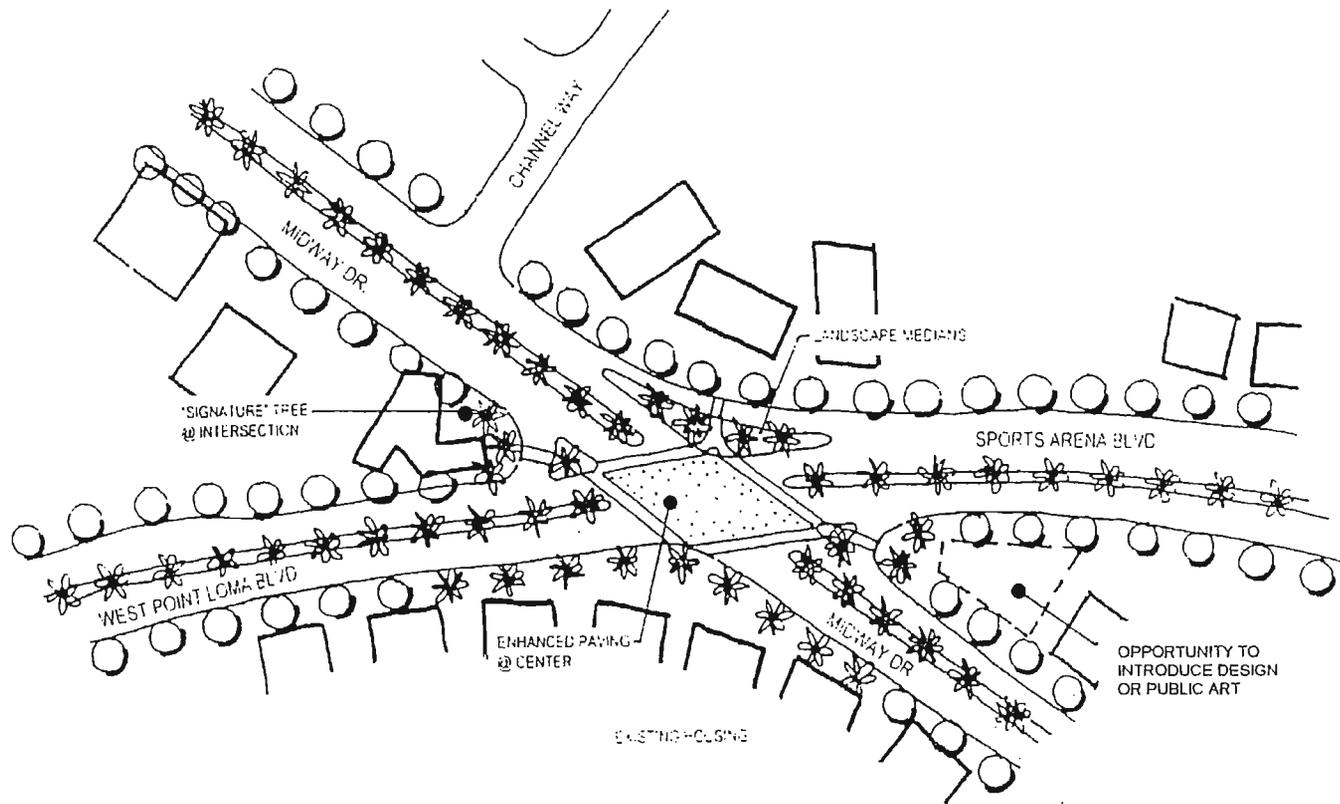


*East Node—existing conditions*

# NODES & OPEN SPACE CONNECTOR

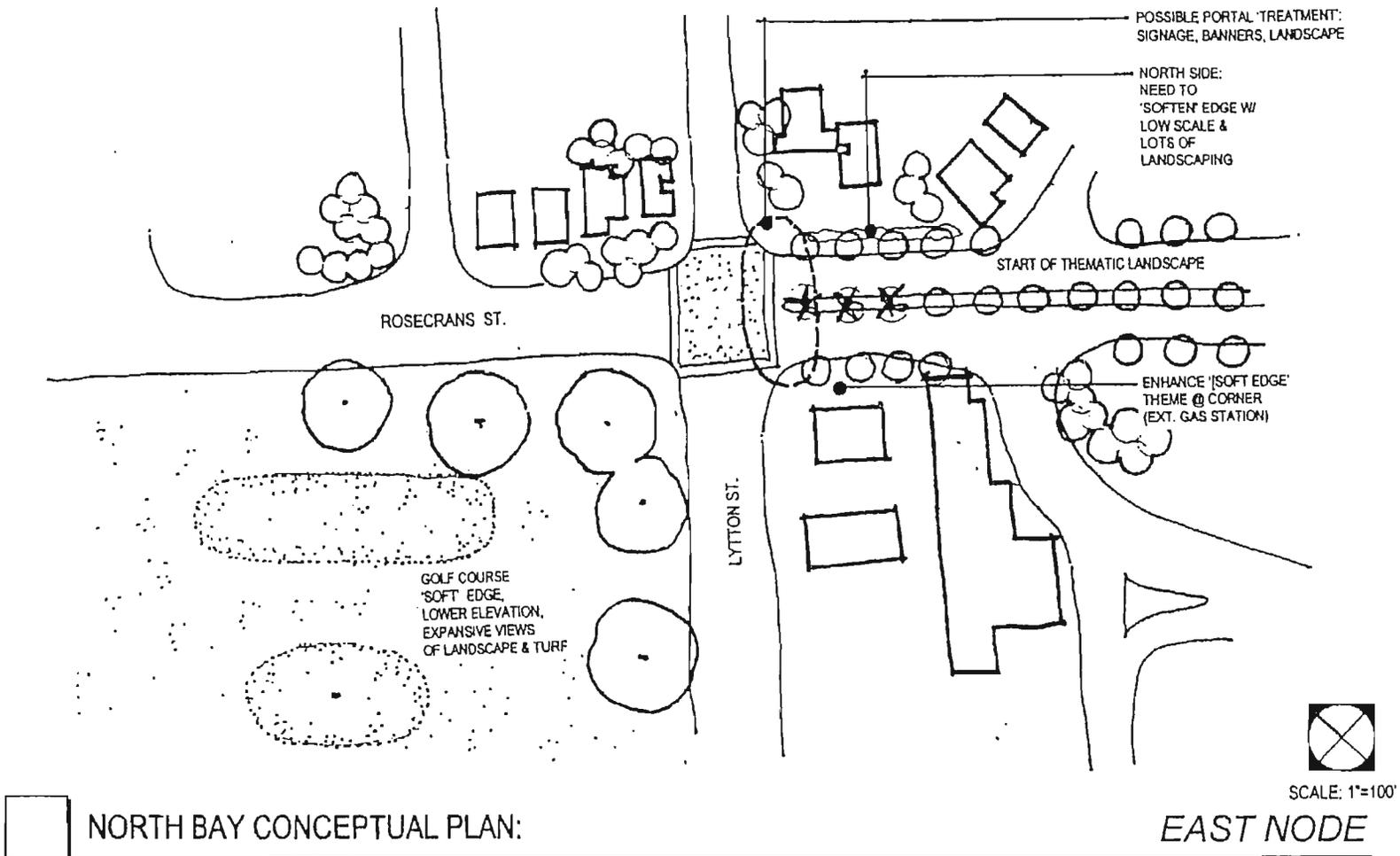


MAP 5 – Highly visible intersections (East and West Nodes).



NORTH BAY CONCEPTUAL PLAN:

WEST NODE



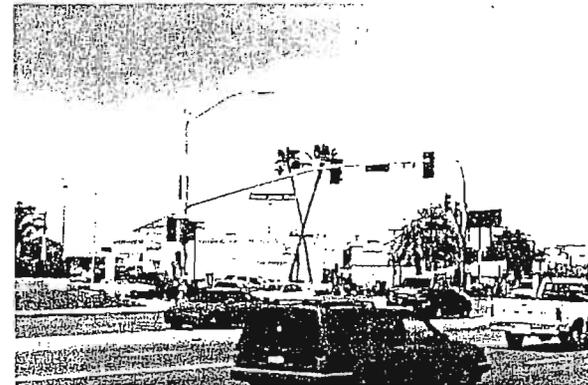
## Streetscapes

The Conceptual Plan focuses on five primary streets for improvement: Camino del Rio West, Rosecrans Street, Sports Arena Blvd., Midway Drive and West Point Loma Blvd. The elements of the plan that are proposed to enhance the streets are:

- widened sidewalks in some areas, to facilitate pedestrian use;
- canopy trees to provide shade along sidewalks;
- a consistent vocabulary of street furniture in areas frequented by pedestrians;
- changes in pavement pattern, materials and colors to enliven walking surfaces;
- banners displaying the North Bay's historical and cultural attributes;
- landscaped setbacks and landscaped medians, where feasible;
- revised building front setbacks that reinforce a strong public edge; and
- effective landscaping for parking lots that abut the street front.

*Camino del Rio West* and *Rosecrans Street* can both accommodate landscaped medians as well as sidewalk treatments. The goal is to create a visible connection and continuity both in the medians and at the street edge. Future built developments should be sited to further reinforce the street edge, especially where large parking lots now weaken the spatial definition of the area.

*Sports Arena Boulevard* would change dramatically if the Sports Arena Site were to be redeveloped, and/or if the main boulevard was connected with its' south side. This spacious street can accommodate a landscaped median, while a



*Existing Streetscape: Rosecrans at Sports Arena*

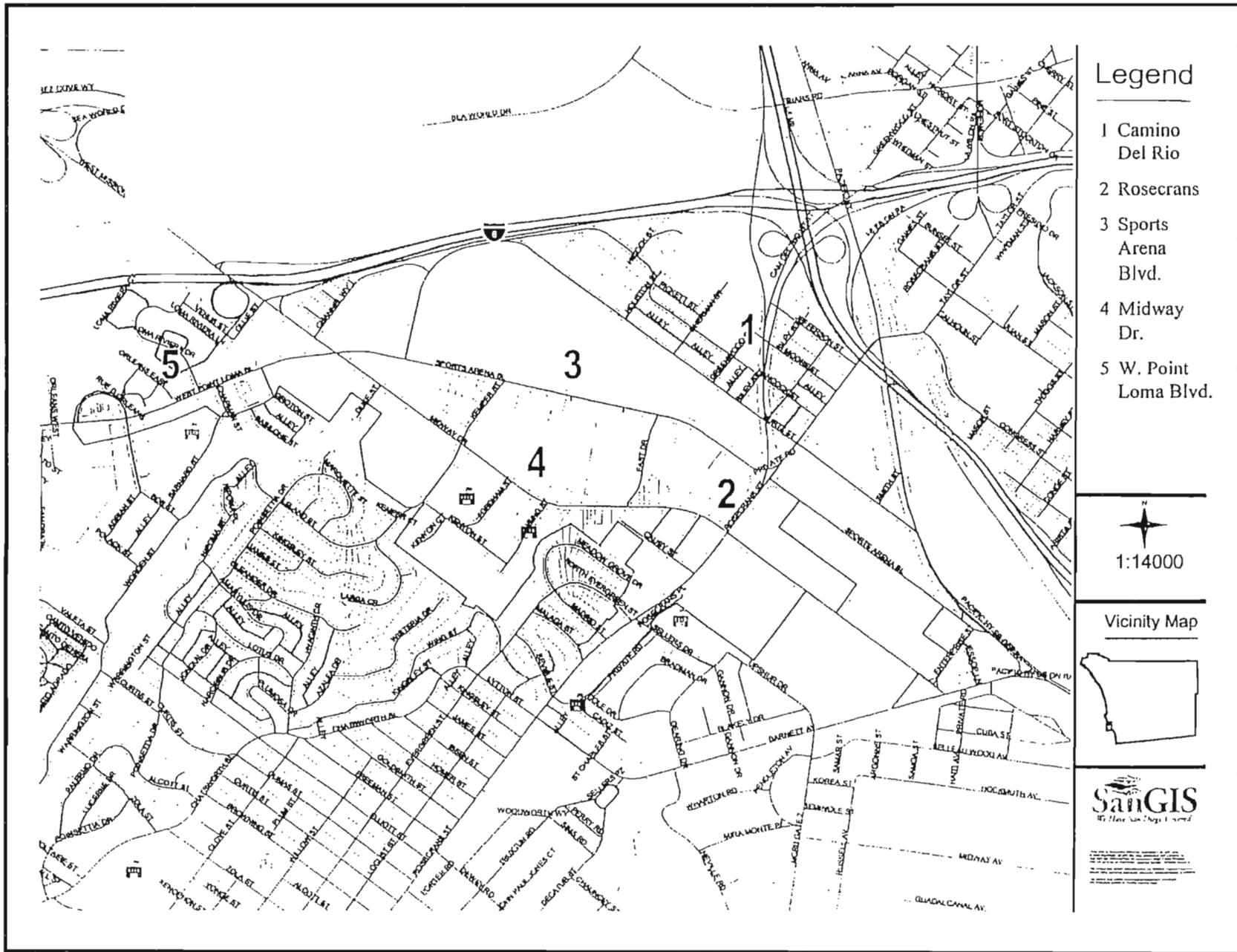


*Existing Streetscape—Sports Arena Boulevard*



*Existing Streetscape—Rosecrans Street*

# STREETSCAPES



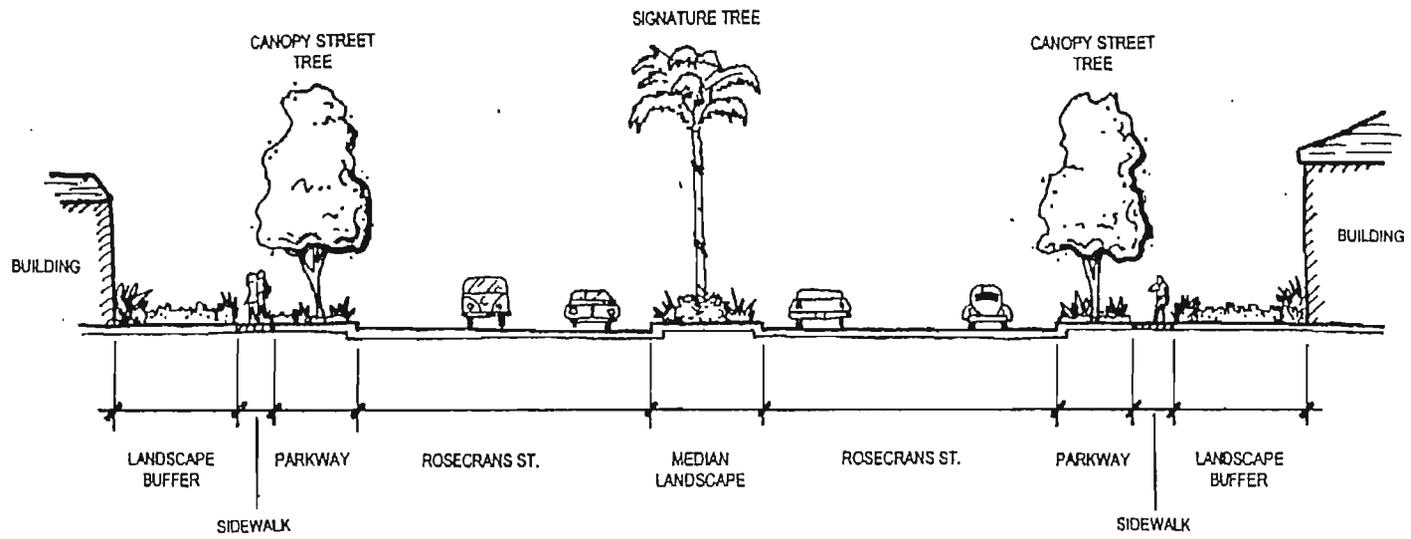
MAP 6 – North Bay's five primary streets.

strong street edge should also accompany development.

*Midway Drive* is currently the narrowest and most clearly defined street of the five. Median landscaping is not possible. Streetscape strategies that strengthen the definition of the street edge are proposed.

*West Point Loma Boulevard* occupies a short distance within North Bay, and is a lower priority than the preceding four streets. Nevertheless opportunities for median and street edge landscaping would create significant visual improvements.

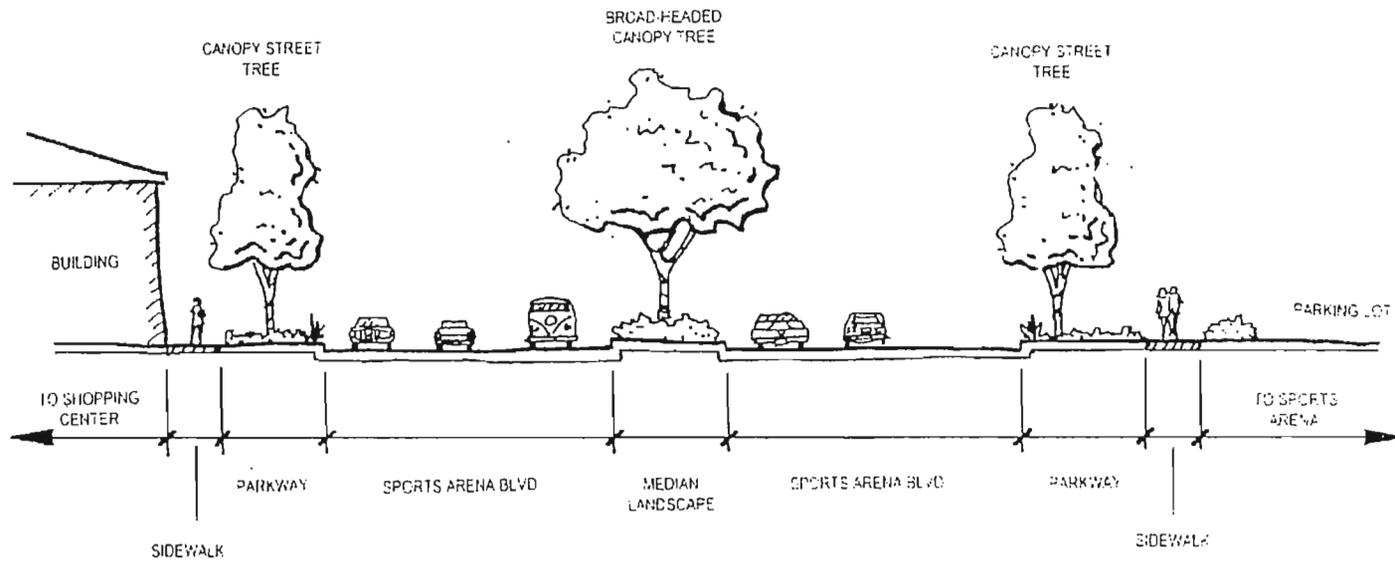
Important streetscape elements are covered in several of the following chapters, such as landscape, lighting and signage.



SCALE: 1"=20'

NORTH BAY CONCEPTUAL PLAN:

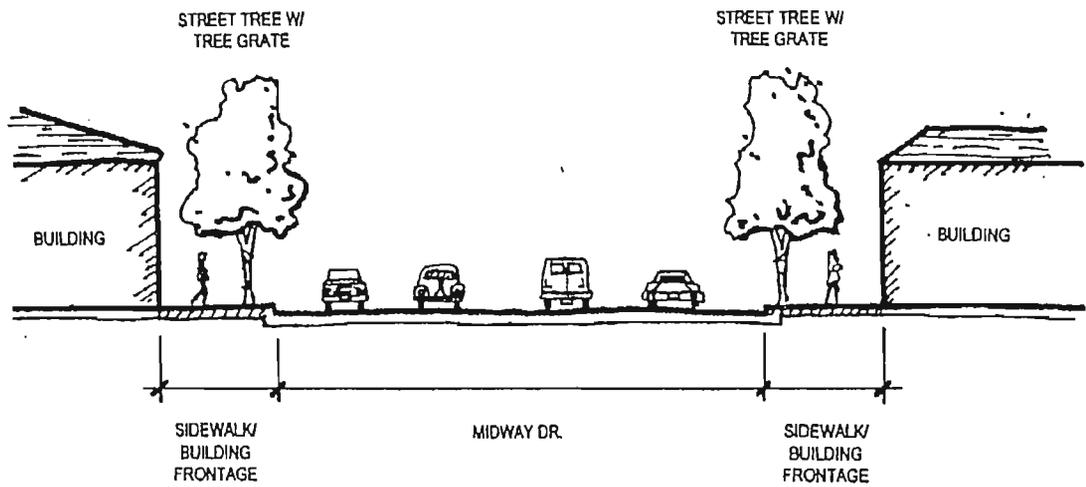
ROSECRANS SECTION



SCALE: 1"=20'

NORTH BAY CONCEPTUAL PLAN:

*SPORTS ARENA BLVD. SECTION*



SCALE: 1"=20'



NORTH BAY CONCEPTUAL PLAN:

MIDWAY SECTION

## Landmarks

Ironically, the potential removal of the North Bay's most recognizable landmark, the *San Diego Sports Arena*, offers the opportunity for the most significant changes to the character of the entire district.

The Arena's 42-acre site does not contribute greatly to a positive image of the North Bay. The structure itself is old, technologically outdated and architecturally uninspiring (its architect once bragged in the local press that the building was very cheap, and looked it). It sits within a vast asphalt parking lot, empty during most daytime hours. Landscaping is sparse and concentrated along Sports Arena Blvd. and directly around the building. Chain link fencing presents an unappealing barrier along Kurtz Street.

New development, carefully considered, will not necessarily recreate a landmark. It is recognized that rather than building a new Sports Arena on this or another site an entirely new use for this 42—acres might be beneficial. Such a use could possibly provide a new focus to North Bay as a model neighborhood. Under this scenario, a moderately—dense, mixed—use development of residential units and ground-level retail/commercial uses is envisioned. Open space and green space, must also be an integral part of the master planning. Views across Interstate 8 and the San Diego River should be incorporated into the design of housing for the site. Walk—able connections over I-8 to the riverfront are proposed. Buildings should reinforce a strong street edge. Pedestrian traffic, as well as vehicular, must be planned for throughout. Corners and intersections should receive special architectural treatment. New development will be strengthened by the incorporation of a bus or trolley line into the site. Locally, the Uptown District east of the North Bay offers a successful model for such a development.



*North Bay Landmark: the San Diego Sports Arena*

## Neighborhoods or Sub-Districts

Six “Sub-Districts”, or neighborhoods, are created within the North Bay as key components of the Conceptual Plan (Map 7, page \_\_). They serve several purposes:

- to further accommodate and encourage pedestrian activity;
- to provide a means for exhibiting the community’s heritage;
- to allow for some variation while maintaining the uniformity of the overall plan.

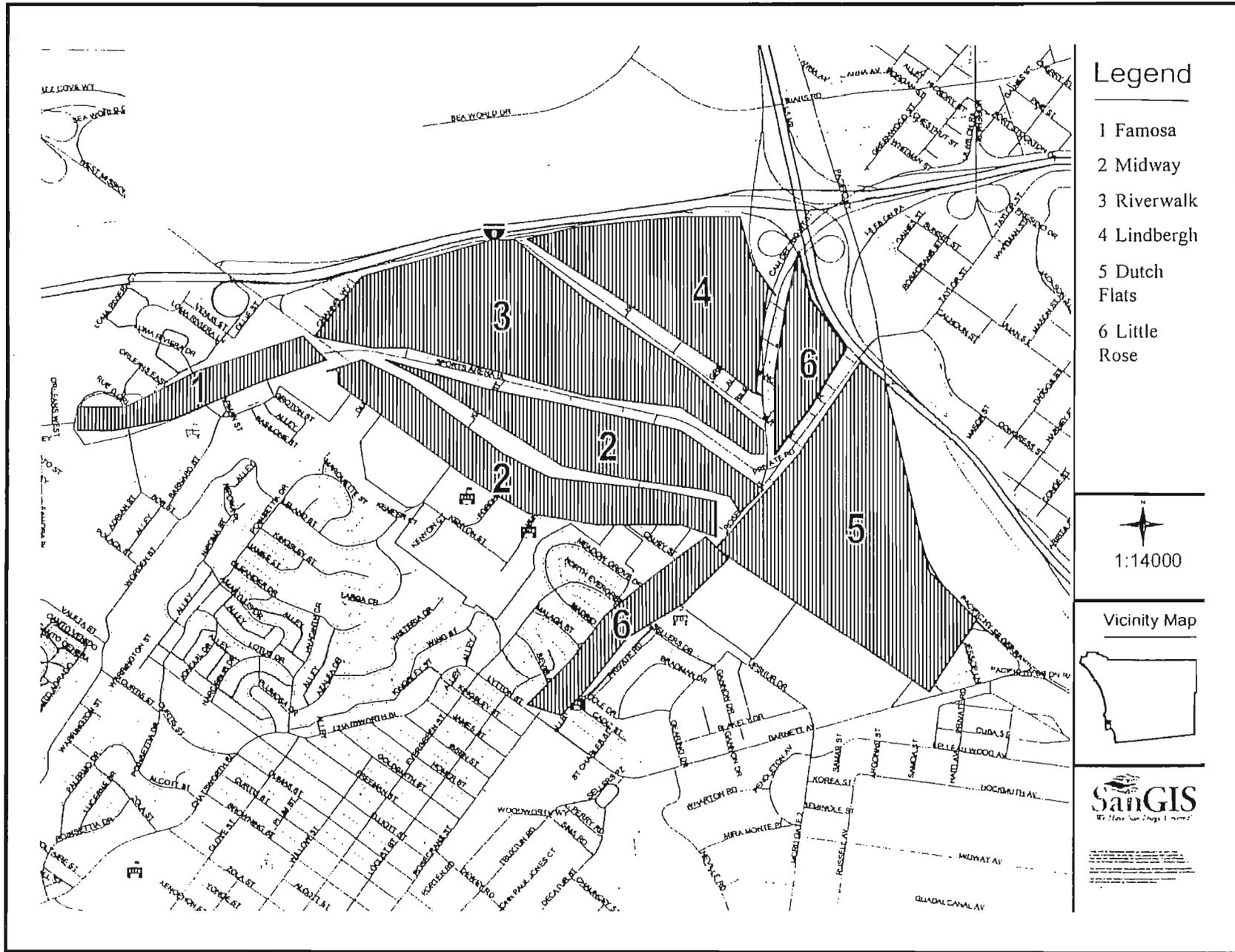
(All four variables are described in greater detail in the chapters that follow this section).

The most westerly neighborhood is called **Famosa** and is identified with the Famosa Slough at its south side. The elements described above are located along West Point Loma Blvd. The Famosa Portal forms the entry to the area as well as to the North Bay District. Changes to existing land uses are not anticipated. The recommended architectural standards are NTC Revival and Neo-Modernist. The signage “theme” is keyed to the Slough.

The **Midway Neighborhood** sub-district is the length of Midway Drive between the East and West Nodes. Land use is primarily commercial. The NTC Revival and Neo-Modernist design standards are recommended for new buildings. Signage themes might include the historic development of the area’s military presence.

The **Riverwalk** sub-district or neighborhood is named for the San Diego Sports Arena’s proximity to the San Diego River. Here, the emphasis is on mixed—use redevelopment, with medium to high—density residential as a major presence. A combination of parks and open or public spaces will be here. The East and West Nodes anchor the extreme ends of this area. A bridge connection is proposed to the River Park beyond I—8 to the north of the site., and along the south edge is the flex trolley route discussed in Chapter Six. Architectural expression may be in any of the three vocabularies. Signage continues the current theme of highlighting events from the Sports Arena’s history.

# SUB-DISTRICTS



MAP 7 – Six Proposed North Bay Neighborhoods.

On the north side of Kurtz Street lies the **Lindbergh** neighborhood, which presently is primarily light industrial and commercial in character. New industrial and commercial development will adopt the Warehouse/Loft or Neo-Modernist vocabularies. Signage will emphasize a theme based on Charles Lindbergh's historic flight and the rise of a pioneering aviation industry near this area.

**Dutch Flats**, the southeast sector of the North Bay, borrows the old name for the area. New development will be commercial and mixed—use. The Warehouse/Loft and Neo—Modernist styles are recommended here. The signage theme might focus on the 19th century development of the area.

Finally, the **Little Rose** sub-district is the portion of Rosecrans Street, once known as the La Playa Trail, between the East Node and the Point Loma Portal. The NTC Revival and Neo-Modernist standards are the most appropriate for new building in this area. Signage illustrates the colonial Spanish influence on this first trade route in San Diego.

## **CHAPTER FOUR - ARCHITECTURAL DESIGN STANDARDS**

- I Background**
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## I Backgrounds

It has been argued elsewhere in this document that the North Bay District suffers from an “identity crisis”. Two perceptions exist. The first is that the area really does not have an identity; the second is that there is an identity, but it is an undesirable one.

Among the factors contributing to the perception of “no identity” are:

- irregular development patterns that frustrate attempts to perceive the area as a whole;
- confusing and unexpected street and traffic patterns;
- inconsistent architectural scale and adjacency conditions;
- inconsistent landscaping;
- a lack of memorable places (except for the Sports Arena or, at the periphery, MCRD and the NTC).

The “undesirable” image may result from:

- the intensity, volume and predominance of automobile traffic;
- existing uses, such as adult bookstores and clubs;
- the Sports Arena itself, dated and unattractive;
- elements from the first category as well.

Obviously, these informal observations will vary from one individual to another. In any case, there exists a pervasive feeling that the North Bay has an inadequate identity, and any improvements contemplated for the area must address the issue. Architecture has a critical role to play in the changes that are proposed; thus, control of architectural expression vis-à-vis a set of design standards.

## II Intent and Goals

This portion of the Conceptual Plan has been prepared as *a guideline for the creation and reinforcement of a suitable physical and visual identity for the structures built in the District, primarily on its major retail corridors.*

Design guidelines or standards at an architectural scale will help reinforce and



*Design influence—NTC architecture*

maintain a desirable, consistent identity. The “architectural scale” refers to individual buildings or groupings of buildings in terms of vocabulary, composition, materials, color and landscape.

The use of design standards in American cities is not new. The early Virginia Capital, Williamsburg, had them, for example. Today cities such as Santa Fe, New Mexico control development with design standards and several “Neotradition” towns, including the highly successful Seaside, Florida, have strict controls.

*The urban and architectural design standards described in this chapter are **mandatory** for development along these corridors in the North Bay: Rosecrans Street, Sports Arena Blvd., Midway Drive, West Point Loma Blvd. and Camino del Rio West. Opportunities for architectural variation have been provided through the development of standards for three distinct “styles”.*

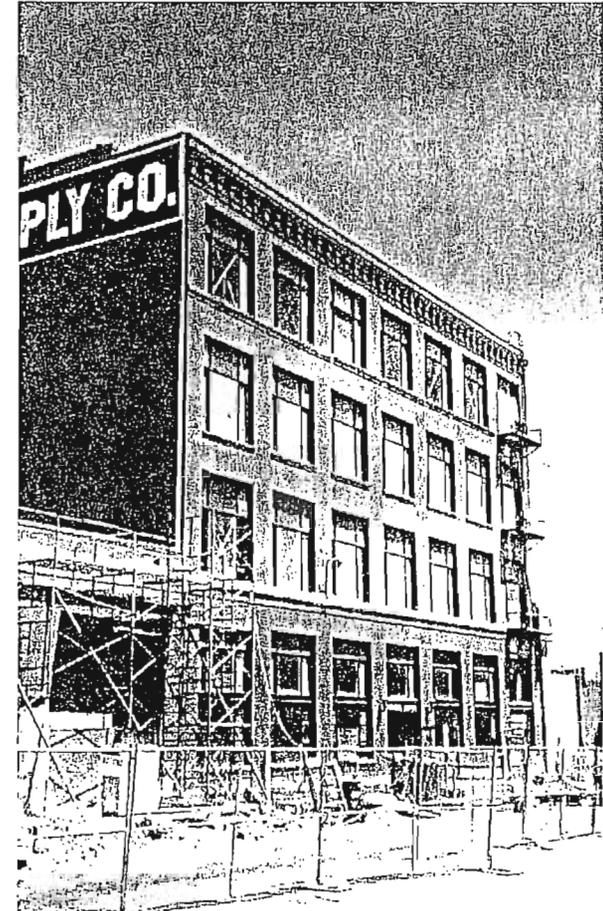
Nothing in the following design standards may be construed as allowing or permitting deviation from any and all applicable zoning standards, municipal ordinances and plans and/or building codes, except where specifically noted. These standards amend the *Midway/Pacific Highway Corridor Community Plan* of May 28, 1991 and the January 19, 1999 amendment to it. See Appendix “A”.

### III General Urban Design Guidelines

All projects must conform to certain design guidelines, regardless of the architectural style selected. This requirement is intended to insure that visual and physical consistency is maintained and will help achieve a more coherent community-wide identity. General urban design guidelines are as follows:

*New building projects must:*

1. be placed so that their fronts face the street upon which they are located.
2. conform to an existing, consistent setback pattern, where one exists. Where there is no established pattern, they must front the setback or meet the sidewalk.
3. visually differentiate the first floor from upper floors, with a greater degree of material texture, articulation and transparency through the



*Design influence: East Village brick loft buildings*

- use of vision glass or other glazing.
4. provide articulation of wall surfaces, such as plane changes, to reduce long expanses of wall and unrelieved volume.
  5. incorporate materials, textures, colors and detailing that reinforce an overall sense of scale appropriate for a pedestrian-oriented environment.
  6. incorporate automobiles carefully so that parking lots have a minimal visual impact on the streetscape, using landscape screens and walls where necessary.
  7. incorporate signage that conforms to City standards, minimizes visual clutter and is integrated with the architecture where possible.

In addition, urban design guidelines contained in the *Midway/Pacific Highway Corridor Community Plan* not specifically amended by this Plan (see Appendix “A”) must be followed. They are organized by land use and include:

Commercial pages 61 – 64

Industrial pages 69 – 70

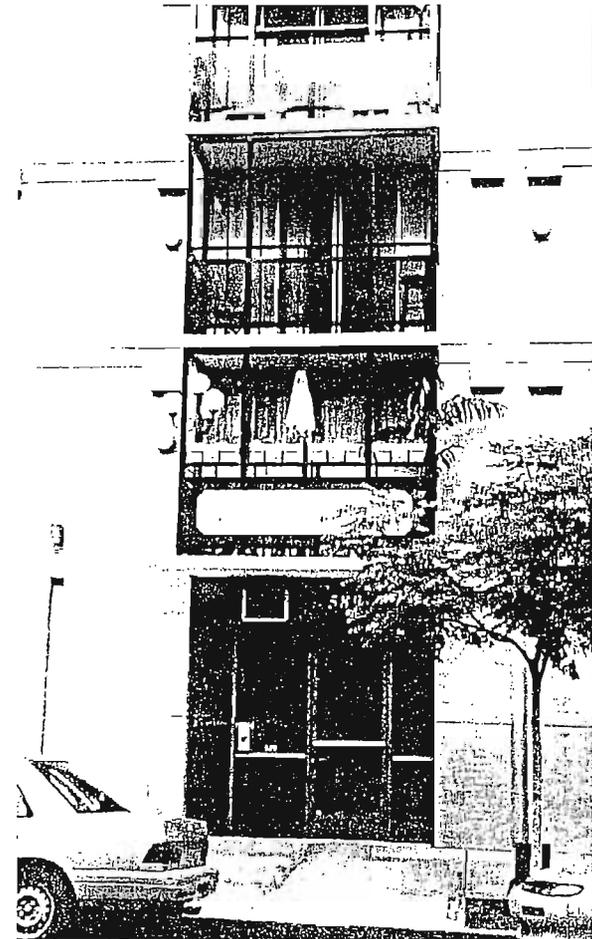
Institutional page 77

Residential (for redevelopment areas, for the purposes of this Plan): pages 83 –84.

Added elements that help provide visual interest, such as trellises, arcades, awnings, decorative light fixtures and canopies compatible with the architectural style are encouraged.

#### IV Architectural Design Standards

In keeping with the goal of developing a positive image for the North Bay District, three architectural “themes” have been identified as being the most appropriate and desirable. “NTC Revival” takes its cues from the older, historic architecture of the former Naval Training Center site. The “Warehouse/Loft Style” is a theme without specific precedence in the North Bay, rather one that is deemed appropriate for the new image being sought. A “Neo-Modernist Style” is designated to allow for architectural expression that is consistent with contemporary development in areas such as biotechnology.



*Design influence—Neo-Modernism in San Diego*

*In addition, approval for development that conforms to none of the designated styles, or single structures greater than 50,000 square feet in size, must be pursued through a discretionary process and Midway Community Planning Board review.*

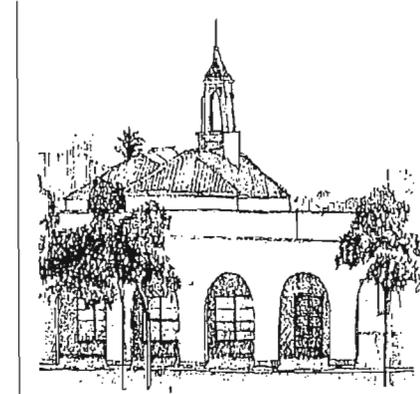
The intentions behind these themes are 1) to guide new development in a manner that promotes visual and physical consistency in building, and 2) to provide a specific set of elements designers will use.

A note about sustainability: every effort should be made to incorporate sustainable design strategies wherever possible. Buildings should exceed Title 24 energy requirements. Windows and skylights should be oriented, if feasible, to avoid solar heat gain in warm months and heat loss in the winter. Alternatively, awnings and trellises may be employed to provide shade as well as architectural relief. Where options exist, the selection of materials requiring lower energy use *in their own production* is encouraged. Adaptive re-use strategies should be considered where appropriate existing buildings are involved. Energy-efficient lighting fixtures and mechanical equipment should be specified, if possible. In summary, designers and their clients are encouraged to consider the wide range of options available under the designation “sustainable architecture”.

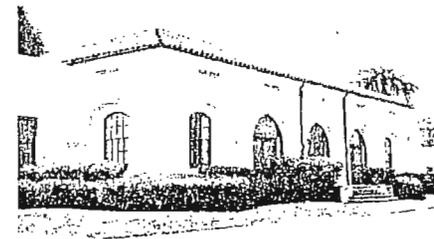
## V NTC Revival

This architectural theme has as its inspiration the historic core of the former Naval Training Center, which includes the original structures around John Paul Jones Court dating to the early 1920’s and designed by Bertram Grosvenor Goodhue, the lead architect for the 1915 Panama-California Exposition in Balboa Park. The generally-accepted name for this particular style is “Spanish Colonial Revival”; slight variations on that title have been used as well.

Spanish Colonial Revival, together with the earlier and typically less ornate “Mission Style”, constitute the two revival styles most readily associated with San Diego by residents and visitors alike. They reflect the influence of Mediterranean imagery and the proximity of Mexico and are, at the same time, highly adaptable to Southern California’s benign climate. Excellent examples of these styles exist throughout the region, beyond NTC: the Museum of Man



*Naval Training Center (NTC)*



*VOCABULARY: Typical massing showing hipped roof.*



*VOCABULARY: Distinctive window and wall mass relationships.*

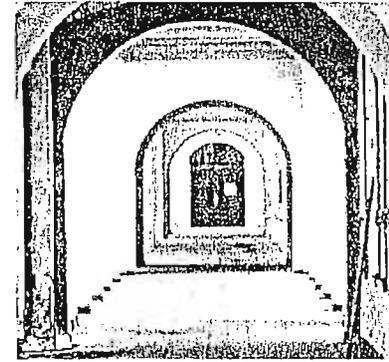
and other exposition buildings at Balboa Park, the Santa Fe Depot downtown, numerous churches and of course countless private residences throughout the county. One style sourcebook dates the Mission Style from 1890 to 1920; the Spanish Colonial Revival from 1915-1940. The best-known practitioner in Spanish Colonial was the architect Bertram Goodhue, noted previously.

The NTC Revival is characterized by most of the same features that constitute Spanish Colonial. There exists the possibility of great variety within a very well defined vocabulary of architectural elements, colors, forms and materials. Buildings and groupings of buildings may exhibit biaxial symmetry or, to a lesser extent, a more picturesque composition. Building scale, too, is adaptable, from an "intimate" scale suitable for domestic architecture to a more monumental one for civic or institutional buildings. The NTC site offers good examples of all of these qualities, in such buildings as the chapel and the structures of the parade grounds. The style employed at NTC differs only slightly from the standard image of Spanish Colonial: it is a bit more formal and symmetrical, sharper in profile and more uniform in expression.

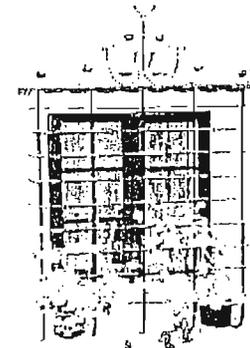
### Vocabulary

The NTC Revival has a well-established architectural vocabulary. Buildings have pitched tile roofs with hipped or gabled ends; wood roof framing, often exposed to view and shaped for decorative effect; plastered (stuccoed) exterior walls of various textures and of expressed solidity and mass; a high proportion of solid wall to window or opening; semi-circular or segmental arched openings or arched arcades; decorative ironwork for brackets, window grilles, railings and lighting fixtures; and prominent entrances, often with elaborate doors and/or flanking pilasters. The play of sunlight and shade on wall surfaces is an appealing attribute of this style. A number of elements may be incorporated to cast shadows, such as projecting balconies, roof overhangs, planter boxes and decorative iron or wood work. Windows are recessed in walls, which further emphasizes the surface mass. The effect of landscape shadow patterns on walls may be an attractive design consideration also (see **Landscape**).

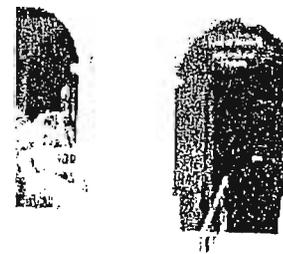
Architectural elements may extend the vocabulary beyond the exterior walls of the structure. Well-defined exterior space is characteristic of the style. Low plastered site walls may enclose hardscape patios or courtyards. Arcades or



*VOCABULARY: Typical arcade at NTC*



*VOCABULARY: Characteristic window treatment in ironwork. (Balboa Park)*



*VOCABULARY: Pilasters and decorative moldings highlight point of entry at Balboa Park.*

pergolas may be used to similar effect.

### Composition

As noted previously, the NTC Revival is suitable for a range of planning and massing configurations, from highly symmetrical buildings to much more “casual” compositions. In plan, an irregular building “footprint” may be appropriate in order to reinforce a smaller, pedestrian scale. Larger structures, on the other hand, may demand a more orderly treatment. In elevation, windows and doors may be arranged in similarly various ways. Site orientation should take into account solar exposure (and the resultant heat gain) and other environmental issues.

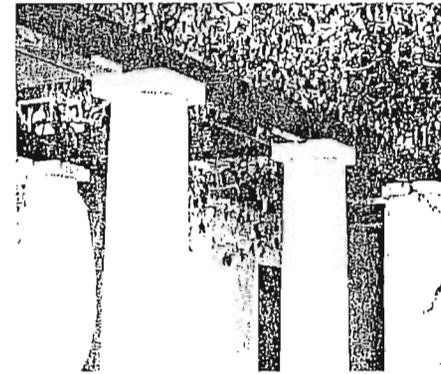
### Materials

There is a limited “palette” of materials associated with this style. Primary exterior materials include tile roofing (either clay or colored concrete), wood framing and plaster wall finishes. Windows may be framed in painted or dark anodized metal or in wood. Glazing may be tinted but should not be highly reflective. Doors and frames may be painted metal or wood. Brackets and trim elements should be painted metal or wood. Ornamentation, which usually occurs around doors, windows, soffits and parapets, may be of wood, plaster, stone, precast concrete (or other simulated stone products) or glazed tile.

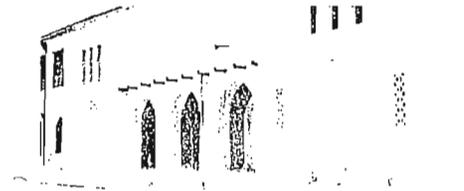
### Color

As with materials, there is a limited range of color choices associated with the style. Roof tile is in the red/orange/brown range. Exposed wood, while most typically painted or stained dark brown, may be treated in a wide range of colors. Ironwork is usually painted black, but color is also often used. Prominent features may warrant bright accent colors, such as tile-covered domes or inset panels.

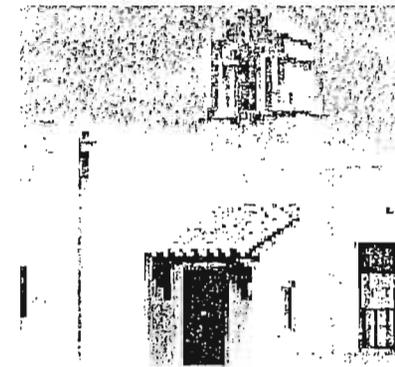
One of the most recognizable features of the architecture of the NTC is the vibrant cream/ochre wall color. This color should be employed as the primary hue of wall surfaces, with accents in the white/beige/tan range common to much



*VOCABULARY: Pergola at NTC with Doric columns.*



*VOCABULARY: Typical variegated massing with symmetrical.*



*VOCABULARY: Characteristic irregular massing.*

Spanish Colonial Revival work.

### Landscape

Landscape planting should complement the architecture. The enhancements plants bring to this style of building, through cast shadows on plastered walls, bright highlights of color, foundation plantings and vines that visually “soften” edges, contribute greatly to its charm. As with the architectural composition, planting plans may vary from formal, ordered arrangements to highly asymmetrical groupings.

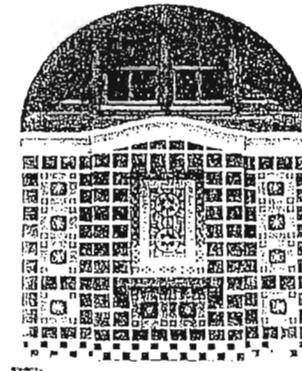
### Requirements

1. The elements of the architectural vocabulary outlined above are to be employed, based upon the historic buildings of the NTC as a direct model.
2. Exterior materials and colors shall conform to the palettes discussed above.
3. Landscaping shall complement the architecture and reinforce the larger goals of the conceptual plan.

### VI Warehouse/Loft Style

In recognition of the fact that the NTC Revival theme may not be suitable for all types of new construction, a second theme has been established: the “Warehouse/Loft Style”. Unlike the NTC Revival, there is no precedent for a warehouse theme in North Bay, the nearest equivalent being the simple, utilitarian long-span structures of the aerospace industry nearby. However, this theme has a number of advantages: it may be relatively inexpensive to build, it provides a high degree of flexibility, and the loft-type space made possible is appealing and adaptable to mixed uses.

In the United States, the warehouse or loft building type originated in the first half of the nineteenth century with the textile mills of New England. These brick buildings, simple multi-story forms based on a repetitive structural bay, are the model for later developments. Windows are large in order to allow as much daylight as possible into the workspace and the internal structure is a minimal column “grid” allowing maximum flexibility. Some later examples of



*VOCABULARY: Decorative tile work.*



*VOCABULARY: Typical cream/ochre plaster color.*



*Historic brick factory/Warehouse architecture.  
(Lowell, MA)*

this architecture still exist in San Diego, primarily in the Gaslamp Quarter and East Village area of downtown, such as the Levi Lofts, Pioneer Warehouse Lofts and T. R. Produce Warehouse/Wellman Peck and Company.

The Warehouse/Loft theme implies large-scale building. This style is most appropriate for use along the major commercial/retail corridors and in transitional areas where light industrial uses occur. It is ideally suited to functions requiring large amounts of flexible space.

### Vocabulary

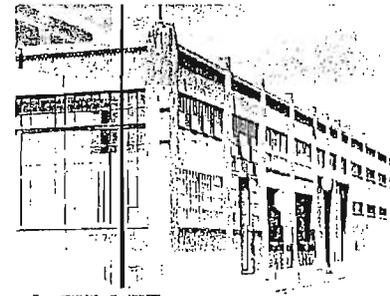
The architectural vocabulary of the Warehouse/Loft Style incorporates a few important exterior elements. In overall expression it is industrial and Modernist. Primary walls, usually brick, are plain and may use horizontal banding or contrasting materials to reinforce a “base” or “datum” at pedestrian level. Sills, headers and other similar elements are often created in brick. Walls may have brick patterns as well as a typical running-bond pattern. Plaster (stucco) may be used as a secondary material; it typically has a smooth or sand-based texture. Windows are large and may have a vertical or horizontal emphasis. Roofs are flat (sloped to drain) or of a low pitch (3:12 and less) or curved. Rooflines may incorporate clerestory windows through the use of a “sawtooth” profile or another form. Doors and entrances are typically simple. Applied ornamentation is generally absent.

### Composition

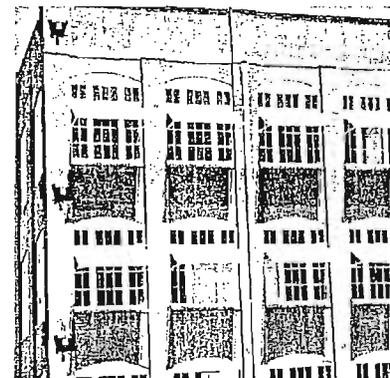
The chief compositional characteristic of this style is the creation of large volumes of space directly abutting public rights-of-way. The overall effect of massiveness should be tempered, where possible, with articulation of surfaces and plane changes. Street corner locations should receive architectural emphasis, through form, height or material contrast to the rest of the building. Articulation of the repetitive nature of the structural framing module is often expressed in the exterior wall treatment through the use of pilasters that create a rhythm in combination with fenestration. As noted above, visual emphasis may be horizontal or vertical. Windows may be expressed as continuous horizontal bands or as openings “punched” in the wall. In some instances, the exterior wall is



*VOCABULARY: Levi Lofts, Downtown San Diego, showing simple massing and direct street frontage.*



*COMPOSITION: Pilasters create vertical rhythm as a counterpoint to the horizontality of the mass.*



*COMPOSITION: Straightforward articulation of the elements: window, spandrel panel, pilaster.*

articulated as a grid of floor slab edges and column bays, with windows filling the resultant spaces.

### Materials

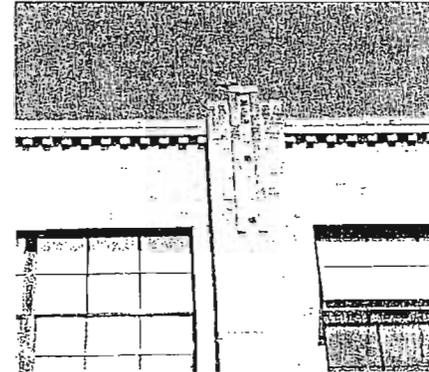
The palette of materials includes brick as the primary wall surfacing, with plaster, tile, wood and colored/textured concrete masonry as accents. Fenestration may consist of steel or aluminum window frames with clear or tinted (not reflective) glazing. Glass block or brick is encouraged as an alternative, where noise, security, vandalism and/or solar heat gain are concerns. Doors and frames should be painted wood, hollow metal or aluminum. Flat (low-slope) roofs may be of any suitable material. Pitched or curved roofs, especially those readily visible from surrounding buildings and streets, may be metal.

### Color

Unlike the NTC Revival Style, there is no precedent for the use of a particular set of colors. However, brick is most commonly in the red range, with accents often in contrasting colors. There are no specific limitations on color as regards other materials, except that unduly bright or garish hues are not characteristic and must be avoided. Aesthetic consideration of surroundings, and particularly of the NTC Revival, should be encouraged. Glare is a special concern with metal roofs and large expanses of wall in light tones. Accordingly, metal roofs must have a non-glossy finish. Galvanized metal is permissible as a roofing material in a pre-weathered form such as "Galvalume".

### Landscape

As with the NTC Revival Style, the landscape design should complement the architecture. Plants should be selected that are appropriate for the large scale of the Warehouse/Loft Style and consistent with the intent of the Conceptual Plan. They may be used to soften the bulk and size of building mass where necessary, or to provide a counterpoint to the structural rhythm of columns and pilasters expressed on the exterior.



*MATERIALS: Basic palette: brick, colored tile accent glaze.*



*MATERIALS: Transition from Brick at most visible areas to plaster.*

## Requirements

1. The architectural vocabulary and composition shall be consistent with the elements described above.
2. Exterior materials and colors shall conform to the pallettes discussed above.
3. Landscaping shall complement the architecture and reinforce the larger goals of the conceptual plan.

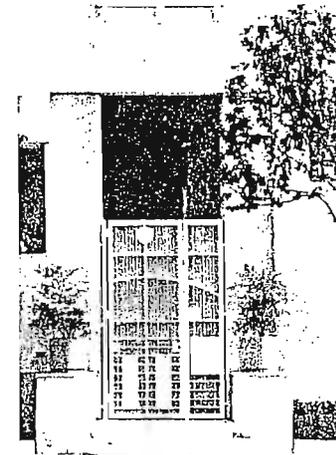
## VII NEO-MODERNIST STYLE

The third architectural theme, the “Neo-Modernist Style”, takes as its inspiration contemporary design that builds upon the modernist tradition. As with the Warehouse/Loft Style, this is an “introduced” theme: there is little precedent in the North Bay area. One intent behind permitting a third style is to allow greater variety in the new development of the area; Neo-Modernist is highly adaptable to contemporary uses.

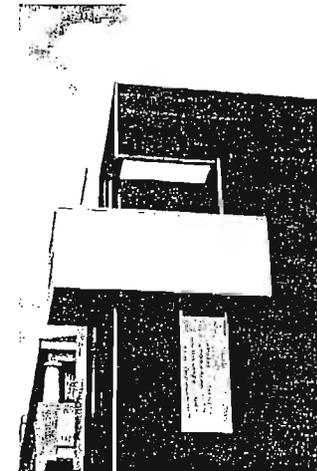
The origins of Neo-Modernism may be traced, unsurprisingly, to the Modernist movement of the first half of the twentieth century. It is the successor to Post-Modernism, the reactionary set of movements that sought an alternative to “mainstream modernism”, the vocabulary labeled in the 1930’s as the “International Style”. Neo-Modernism revives forms and materials from the so-called “heroic” period of the 20<sup>th</sup> century, when pioneering work was being done by architects such as LeCorbusier in Paris, Ludwig Mies van der Rohe in Berlin and Walter Gropius at the Bauhaus, Dessau.

What distinguishes Neo-Modernism from its predecessor is a freer attitude about the use of materials, colors and forms. Whereas early modernism, in its most characteristic form, was often limited to a very select palette of materials (concrete, steel, glass and other elements that resembled these), colors (white predominated) and forms (rectilinear, smooth, planar, strongly geometric) there is greater freedom for expression in the contemporary version. Much of the best work is sculptural and vigorous.

There are a few notable examples of Neo-Modernist architecture in San Diego. The most noteworthy institutional structure is the award-winning Neurosciences



*“SEVEN ON KETTNER” strong street presence;  
good pedestrian scale.*



*Simple, interlocking rectilinear forms.*

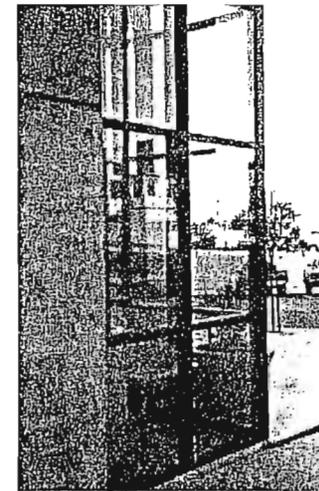
Institute, dating from the 1990's, designed by Todd Williams and Billie Tsien. Several recent small-scale downtown San Diego residential or mixed-use projects exhibit a Neo-Modernist sensibility about design as well, such as the East-West Building (517 Fourth Avenue, Roesling Nakamura Architects). What distinguishes the best work is a careful attention to detail and especially to scale. Many of these examples are urban infill structures that satisfy the requirements of good contemporary urban design practice: they conform to, and reinforce, the street edge; they provide variety and texture as well as consistency; their scale is comfortable for the pedestrians who pass them every day. These are attributes that must be present in Neo-Modernist-influenced projects in the North Bay.

### Vocabulary

Neo-Modernism features generally simple forms with little or no applied ornament. The expression is typically rectilinear, with volumes appearing to be interlocking and forms more angular than curvilinear. Pattern and repetition are judiciously controlled. Visual transparency is a hallmark of this architecture. The vocabulary of this style emphasizes a Modernist-inspired set of forms (see **Composition**, below) as well as materials (**Materials**). The defining principles of early modernism, as promulgated by H. R. Hitchcock and Philip Johnson in their seminal book, *The International Style*, apply here: 1) an overall sense of volume, as opposed to mass; 2) "regularity"—expression of the modularity and repetition of the structural frame—versus symmetry; and 3) a lack of applied ornamentation coupled with a straightforward use of materials in their natural states.

### Composition

Building forms are seen as simple volumes of space, typically flat-roofed with little or no emphasis on a parapet or other roof projections. Elevations may be asymmetrically composed. There is articulation of wall planes, which are in and of themselves usually smooth and planar. Windows and other openings are important compositional elements and may be vertical and/or horizontal. Scale and size must be carefully considered and addressed, particularly along sidewalks and streets at the ground floor level. Building mass should not



VOCABULARY— Transparency



COMPOSITION— simple volumes.

“overwhelm” pedestrians. Horizontal elements, such as projecting canopies, may be employed to maintain an appropriate human scale.

### **Materials**

A wide range of materials comprises the palette associated with Neo-Modernism. Primary wall surfaces include concrete, steel and glass finishes. Concrete masonry, brick and wood are also employed. Metal may be used, in limited quantities for wall surfacing, and in decorative panels. As with metal for wall surfaces, plaster should be used in limited applications. Doors and windows may be wood, aluminum or steel, painted, anodized or finished in other ways. Mirrored or highly reflective glazing must be avoided. Exposed structural framing is typically painted steel or natural-color concrete.

### **Color**

There are few limitations on color. Consistent with the Modernist approach to materials, the natural hue of the building material is often emphasized. Similar to the Warehouse/Loft Style, the primary consideration is to avoid particularly bright, intense colors that appear garish. Accents in bright colors are acceptable to highlight features.

### **Landscape**

As with the other styles, the landscape design must complement the architecture. Plants may be used to accentuate horizontality or verticality, for example; they may be selected also for the contrast they provide. They should also provide relief and visual stimulation at the ground level. Thoughtful landscape design will help mitigate the unpleasant effects of bulk in large-scale buildings, especially critical in areas where pedestrian activity is encouraged.

### **Requirements**

1. The architectural vocabulary and composition shall be consistent with the elements described above.
2. Exterior materials and colors shall conform to the palettes discussed above.

3. Landscaping shall complement the architecture and reinforce the larger goals of the conceptual plan.

### VIII SOURCES CITED AND/OR RECOMMENDED (SELECTED)

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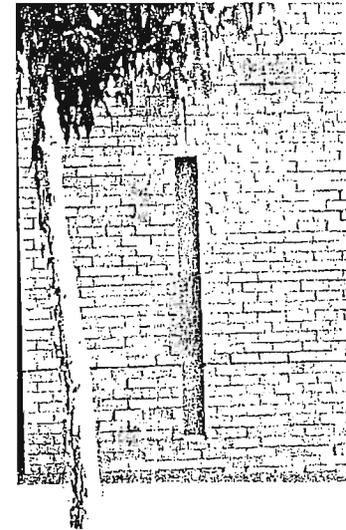
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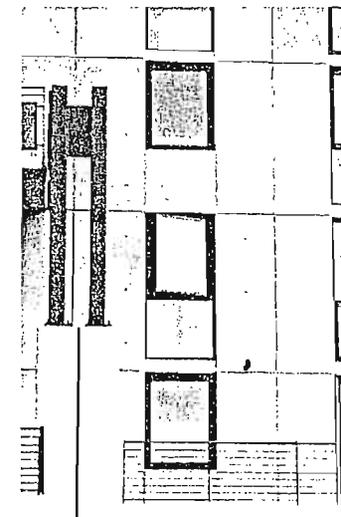
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*MATERIALS—Two tones of brick.*



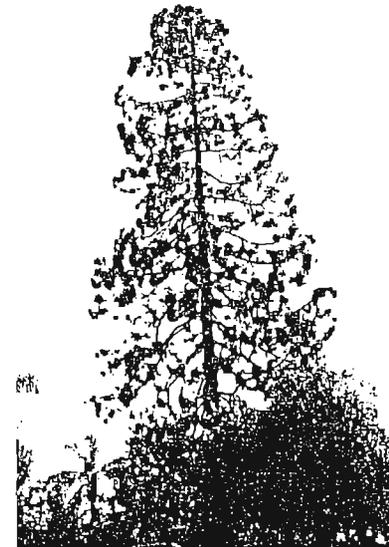
*MATERIALS—Primary elements : steel, concrete, glass.*

## CHAPTER FIVE - LANDSCAPE DESIGN STANDARDS

- I Background
- II Landscape Perception Issues
- III Intent and Goals
- IV Portals
- V Streetscape Nodes
- VI Project Areas
- VII Site Embellishments
- VII Tree/Plants Selection List
- VII Recommended Street Amenities



*Existing Landscape: Canary Island Pines along Midway Drive behind Home Depot.*



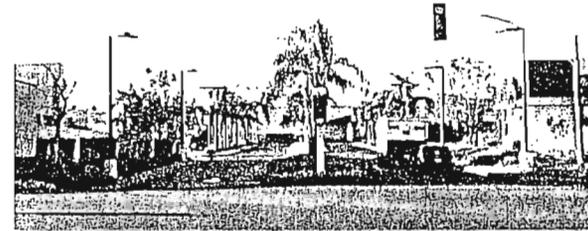
*Canary Island Pine*

## I Background

Similar to the issues regarding architecture, the image of the North Bay area landscape is perceived as a 'weak' or undesirable area of the City of San Diego. Development in the North Bay Planning Area has occurred in phases over the past 100 years. Until the mid 1980's, the City of San Diego had not had landscape requirements in place to assure that urban forests, tree canopies and minimum landscaping for the roadways and the area developments were required and implemented. With most of the North Bay having been developed prior to that time, a majority of the streets and properties are without landscaping or have minimal landscaping. Also, street and right-of-way improvements that are consistent with current standards are lacking on sections of Midway and Taylor Street.

Several large commercial developments on Sports Arena Blvd., Midway and Rosecrans have been developed in the last 15 years that have had the landscape requirements applied for approval and implementation. However, recent revisions to the City Landscape Requirements have rendered some of these projects non-compliant with the updated standards. The Sports Arena and surrounding parking lot has received some improvements for building pads with landscaping along Sports Arena Blvd., but the majority of the site remains an expanse of asphalt. Additionally, until recently, Cal Trans was responsible for Rosecrans as Highway 209. The focus of Cal Trans was the fast and efficient movement of vehicles, not the beautification and embellishment of the community along with traffic calming improvements. Road widening has resulted in 6 wide lanes of traffic, but little or no opportunity for landscape and trees, except in the center median. CalTrans has since relinquished Rosecrans back to the City of San Diego in 2001 along with some funding to pay for the short term maintenance of the roadway.

The communities and areas that surround the North Bay – Old Town, Mission Hills, the Marine Corps Recruit Depot and the former Naval Training Center, Point Loma, Ocean Beach and the I-8 and I-5 Freeways - all have extensive and mature trees and landscaping as a component of their images. The North Bay Planning Area and the Pacific Highway Corridor is not consistent, or of the quality that exists in the adjacent communities. Little or no landscaping exists in the North Bay, and the areas that do exist are typically inconsistent and of



*Existing Landscape: Queen Palms along Sports Arena Boulevard.*



*Torrey Pine Tree Mission Valley near Hazard Center.*

minimal impact and quality.

As discussed previously in this Conceptual Plan, the City of San Diego has established the North Bay Redevelopment Area to act as a vehicle in the securing and retention of funding to initiate the design and redevelopment of the North Bay Redevelopment Area.

## II Landscape Perception Issues

There is no visual identity for the entire community or “portal” imagery to signify the North Bay Community

The roadways dominate – six lane majors (Camino Del Rio West and Rosecrans and Sports Arena and Pacific Highway), and four lane majors (Midway and West Point Loma and Taylor and Barnett/Lytton)

The existing visual of the community is about expansive roadways and traffic volume circulating to the shopping and recreation destinations, or through the community to Navy bases and facilities.

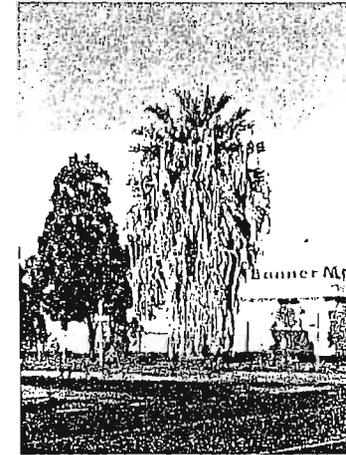
Expansive parking lots facilitate the large buildings – the Sports Arena, Home Depot, Target, Midway and Rosecrans, Main Post Office, etc.

There is a lack of significant landscaping. There is no memorable or quality of landscape areas or features in the community, excluding the Famosa Slough (which is on the periphery and therefore of minimal impact) and the San Diego River (also at the periphery).

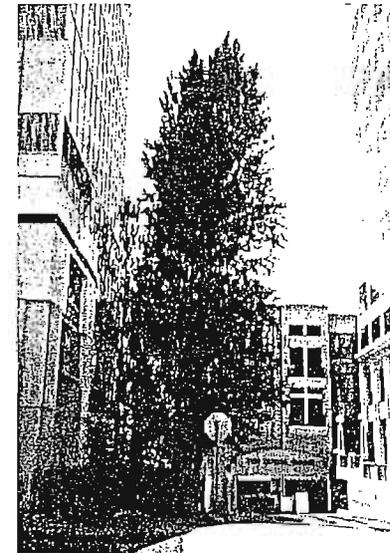
The mix of land uses and the age of the project area has resulted in a bland commercial center / industrial park image for the entire community.

## III Intent and Goals

This portion of the conceptual plan has been prepared as a guideline for the creation and reinforcement of landscape features, images and requirements for the community. This section on landscape architecture for the community is



*Existing Landscape: untrimmed Mexican Fan Palm along Midway Drive.*



*Lombardy Poplar*

both an “umbrella” document for the creation of an image and quality for the community and a complement to the information and design guidelines identified in the Architecture Chapter that precedes this chapter.

There is great potential for landscape architecture to create positive change and ultimately improvement and benefit to the North Bay Community. The existing conditions in the community do not afford many opportunities to provide and install trees of the size and scale envisioned for the gateways and streetscapes. The mechanism to address these constraints is the City of San Diego Street Design Manual. This document was updated in 2002, and has new standards and requirements that addresses both the roadway design for vehicles and the requirements, and facilitation for street trees and landscape. The standards contained in the document will be referenced and applied to the North Bay to create the opportunity for a “urban forest” in the community to create an image of quality and the additional benefit of heat reduction and air quality.

The following sections on “Portals,” “Streetscapes,” “Nodes,” “Project Areas” and “Site Embellishments” compliment the urban and architectural standards of Chapter Four on as well as the elements of the Conceptual Plan found in Chapter Three.

In addition, a “tree/plant selection list” details appropriate landscape materials mandated by this Plan.

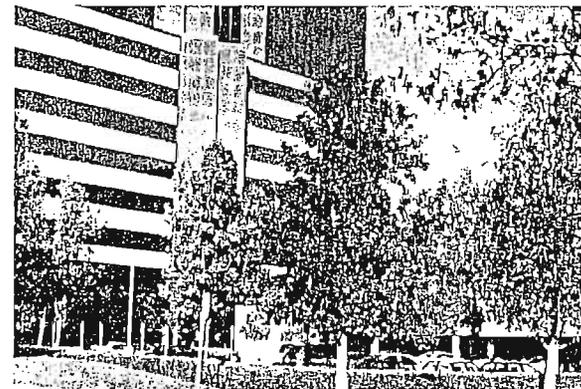
#### IV Portals

I-8

This is the main entry into the North Bay Community for a majority of the San Diego Region. The access from Interstate – 5 South and Interstate – 8 West, and the Taylor Street from Old Town and Pacific Highway brings the major amount of traffic to Rosecrans and Sports Arena Blvd. With the volume of traffic that moves through here and the scale of roadway / right-of-way, the opportunity to create a “portal” of significance is achievable. Coordination with Cal-Trans has already occurred to locate and install Canary Island Pines and white



*Existing Landscape: palms along Sports Arena Boulevard.*



*Liquid Ambers in parkway near Marriott in Mission Valley.*

flowering Oleander trees in conjunction with the landscape renovations of 2002.

This “portal” is the opportunity to establish the streetscape for Rosecrans to the Barnett / Lytton intersection at the west end of the North Bay Community Planning Area.

#### WEST I-8

The entry at the north “portal” into the North Bay Community has a unique and strong image opportunity for the North Portal into the community. Circulation is coming over the San Diego River at the Ingraham Street bridge and then under the I – 8 bridge creating a very focused image and physical transition from Mission Bay Park.

Currently, the Park has a beautiful picturesque setting with hundreds of acres of park and water. On the south side of the bridge, the North Bay Community image is of buildings, roadways and signage. The challenge is to create an image that identifies the North Bay Community and introduce landscape in a quantity and quality to hold up the visual that exists in Mission Bay Park and along Interstate 8. CalTrans has recently completed a landscape upgrade project in 2002 along this stretch of I-8.

This “gateway” is the opportunity to establish the streetscape for Midway to the Barnett intersection at the south end of the North Bay Community Planning Area.

#### PACIFIC HIGHWAY / BARNETT

This “portal” at the south entry in the North Bay Community is currently an expanse of asphalt, fencing and guardrails. The six lanes of Pacific Highway and the expansive buildings and asphalt for the SPAWARS facility gives this location an industrial / warehouse appearance. Unlike the previous two portal locations described above, this location has no adjacent project or installation to draw from and expand upon in the creation of a project image. The opportunity to introduce a new landscape design and image therefore is not constrained by looking to match any existing installation. At the same time, the ability to intro-



*Chinese Tallow tree used extensively as street tree along India Street in Little Italy.*

duce a significant landscape at this location is contingent on the ability to modify the existing roadways and lane widths to create planting areas.

The opportunities for making memorable improvements are substantial, but the cost for those improvements will also be high due to the need to create all landscape installation locations.

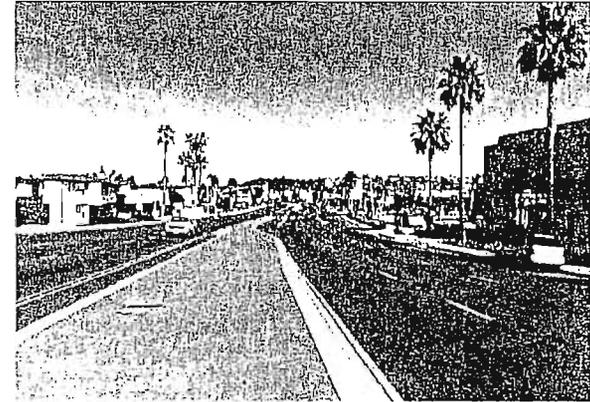
This “portal” is the opportunity to establish the streetscape for Barnett to the Rosecrans intersection at the west end of the North Bay Community Planning Area.

### POINT LOMA

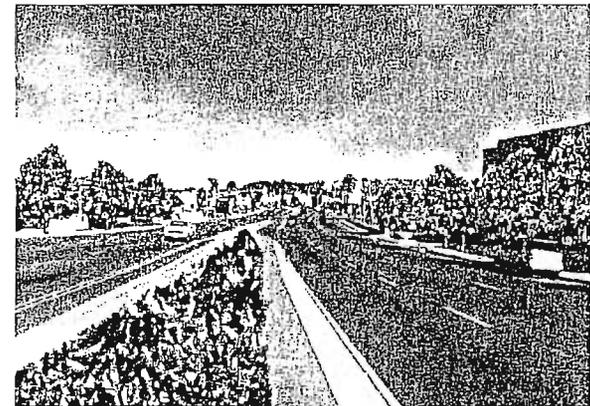
The “portal” at the west end of the community is similar to the north “portal” at Midway and Interstate 8 in that a very strong image and quality exists in the adjacent area to this location. The former Naval Training Center and Bay Ho Golf Course is a mature landscape with massive trees and expansive lawn areas. The existing residential area to the north of Rosecrans is an established neighborhood from the early part of the century, with beautiful homes and mature landscapes.

The visual is mature and lush landscaping west of the intersection. In the North Bay Community area east of the intersection however, the impact of CalTrans is apparent. The road widening and 'improvements' left a six lane major with a center median of hardscape, sidewalks and retaining walls on the outer edges of the right-of-way. These areas have little to no opportunities to implement landscape improvements. The design at this “portal” will need to investigate the potential for narrowing of the lanes and creation of landscape areas that will transition to the east.

This “portal” is the opportunity to establish the streetscape for Rosecrans at the Barnett / Lytton intersection at the west end of the North Bay Community Planning Area.



*Before—Rosecrans looking Northeast.*



*After—Landscaping with parkway trees and planted median.*

## OLD TOWN

At the eastern boundary and perimeter of the North Bay Community is the Pacific Highway Corridor and the intersection at Taylor Street. This intersection is both one of the eastern “portals” into the North Bay Community and is also the western portal into Old Town. This location is another expanse of asphalt and concrete, in addition to the location and alignment of the rail corridor that services San Diego to points north.

The opportunity to utilize the Interstate 5 bridge and adjacent landform as a portal to create an identity and visual is the positive opportunity at this location. Consistent with all of the other portal locations, is that the work and cost required to implement landscape and features for this gateway will be substantial.

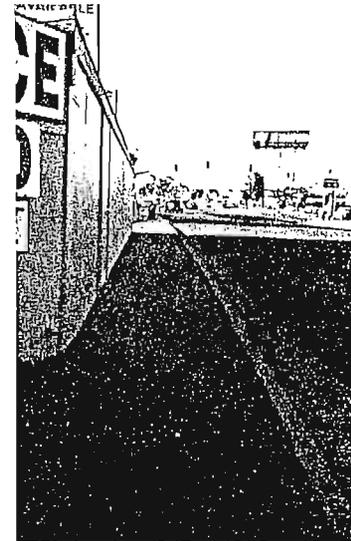
This “portal” is the opportunity to establish the streetscape for Taylor Street from Pacific Highway to Rosecrans in the North Bay Community Planning Area.

### V Streetscapes

#### CAMINO DEL RIO WEST / ROSECRANS 6 LANE MAJOR

As described above in the gateway component, this roadway is the primary circulation component in the North Bay Community. The opportunity to initiate change and improvements in the North Bay begins with Camino Del Rio West and Rosecrans. A detailed site inspection also identified that numerous obstacles exist to the implementation of landscape and streetscape. The most common issue being the multiple curb cuts and driveways fronting onto Rosecrans. The previous road widening implemented by CalTrans has left the only landscape opportunity in the center median.

To create the opportunities to design and implement a quality landscape of substance, an analysis of the entire roadway through North Bay will need to be conducted with the new standards from the updated City of San Diego Street Design Manual used as a reference. This analysis will identify what modifica-



*Before—Camino Del Rio West*



*After—Sidewalk and Landscaping Improvements*

tions will be necessary to provide landscape in the existing center median and to modify the outer edges of the right-of-way to create sidewalks, planting areas and site amenity locations for the length of the roadway.

**SPORTS ARENA**  
*6 LANE MAJOR*

After Rosecrans, Sports Arena Blvd. currently provides the next highest volume of circulation to access the shopping centers and the Sports arena site. This six lane major roadway is also identified as the selected roadway to receive the alignment of the new MTDB rubber tire train. With the constraint of facilitating a mass transit component in the center median, the only landscape opportunity is in the parkways on the outer edges of the right-of-way, unless a tree component can be incorporated into the transit component.

Similar to Rosecrans above, a detailed analysis of the proposed improvements and the existing site conditions will be required to identify the opportunities to for landscaping and streetscapes. The City of San Diego Street Design Manual shall be referenced to evaluate the existing condition for compliance, and opportunities to reconfigure the roadway for lane widths and the creation of landscape and streetscape areas.

**PACIFIC HIGHWAY**  
*6 LANE HIGHWAY / MAJOR*

The entire eastern edge of the North Bay Community is bordered by Pacific Highway. This historic roadway used to be a portion of the north-south link to Los Angeles. It is still a main connection from downtown San Diego to the North Bay area and is envisioned to facilitate additional vehicle traffic in downtown as a component of the North Embarcadero Plan and through Middletown.

With this redirection of emphasis coming back to Pacific Highway from current planning efforts, this roadway could become a major promenade street linking the North Bay, Old Town and Mission Bay Park to Downtown San Diego. The Federal Government also commissioned a planning report for the SPAWARS facility that fronts the east side of Pacific Highway and has a major parking lot



*Norfolk Star Pine at NTC*



*Proposed portal tree—Lombardy Poplar*

between Pacific Highway and Midway. The report makes recommendations regarding access and street frontage improvements to benefit the complex. These improvements may not go to the extent envisioned to benefit the North Bay Community, and should be analyzed to determine how these proposed improvements integrate into the proposal for Pacific Highway in this Concept Plan.

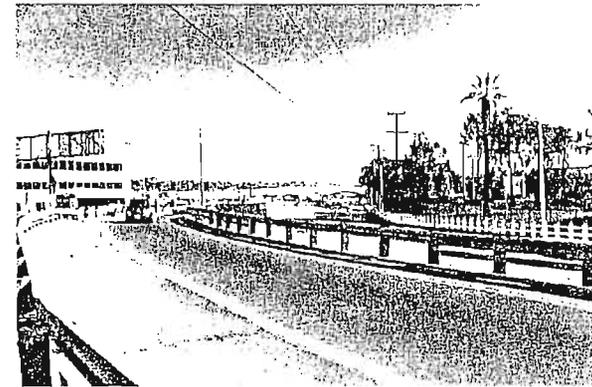
The improvements and modifications to this roadway necessary to bring it to the level envisioned will be substantial and will require construction of a center median to replace the existing metal guardrail. Parkways will provide opportunity locations for landscape and streetscapes. Consistent with the other streets above, the City of San Diego Street Design Manual shall be referenced to evaluate the existing condition for compliance, and opportunities to reconfigure the roadway for lane widths and the creation of landscape and streetscape areas.

**MIDWAY**  
*4 LANE MAJOR*

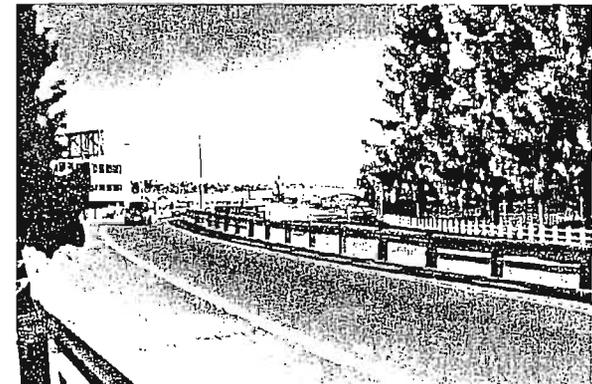
Though only four lanes, Midway Drive is the primary north-south roadway through the center of the North Bay Community. The existing condition of Midway Drive right-of-way is highly variable throughout the North Bay. The width varies from a standard road to very narrow north of Rosecrans and again approaching West Point Loma Avenue. The flow of traffic at certain locations also tends to “bottleneck”. With the eclectic nature of development that has occurred on this street over the past 80 years, disparate conditions are in place.

The opportunity is to create an image and quality that establishes this central spine as a community core, and the primary pedestrian circulation component. With Sports Arena Blvd. retaining the focus of vehicular and mass transit, Midway Drive is the street that will best facilitate that need.

Consistent with the other streets above, the City of San Diego Street Design Manual shall be referenced to evaluate the existing condition for compliance, and opportunities to reconfigure the roadway for lane widths and the creation of landscape and streetscape areas.



*Existing—Pacific Highway*



*After inclusion of Canary Island Pines*

**TAYLOR STREET**  
*4 LANE MAJOR*

As described in the "portal" narrative above, Taylor Street is a portal to both the North Bay Community and into Old Town. The physical structure of Interstate 5 and the adjacent landforms represents a clear delineation between the two areas. With a total length of six blocks, Taylor Street is not substantial in scale, but it does have importance in terms of connection to Old Town, Pacific Highway and the MTDB trolley and Amtrak stations.

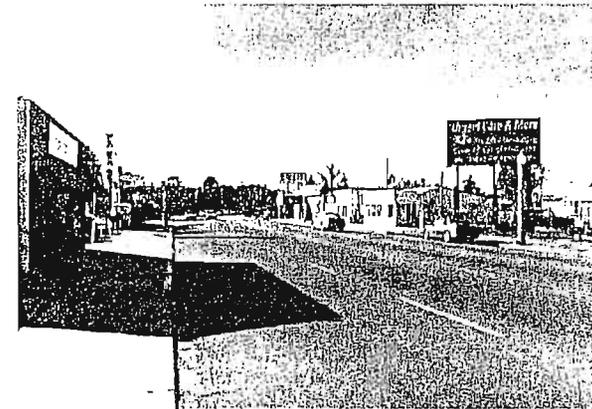
Also consistent with Midway Drive, is that Taylor Street has sections in some of the blocks that do not have standard City improvements in the right-of-way – curb, gutter, sidewalk, pedestrian ramps, etc. Currently, this street circulates as an extension of Sports Arena Blvd. The design intent is for Taylor Street to be a distinct and separate image to identify this street as a connector to Old Town and to the major intersection / node at Sports Arena Blvd.

Consistent with the other streets above, the City of San Diego Street Design Manual shall be referenced to evaluate the existing condition for compliance, and opportunities to reconfigure the roadway for lane widths and the creation of landscape and streetscape areas.

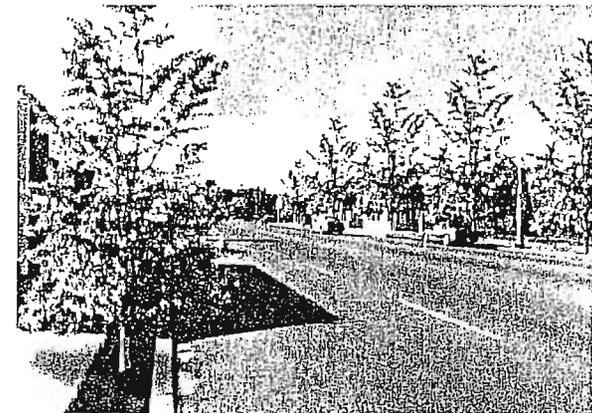
**WEST POINT LOMA**  
*4 LANE MAJOR*

West Point Loma is a transitional street from the intersection at Midway Drive to the western edge of the North Bay Community. Land uses range from commercial and services to apartments and residential. This street is also the transition to Ocean Beach. Currently, no landscape embellishments exist within the right-of-way. To be able to implement any landscape elements, a complete analysis of the existing conditions will need to be prepared to identify opportunities and what types of modifications to the roadway and right-of-way is required to install landscape and streetscape improvements.

Consistent with the other streets above, the City of San Diego Street Design Manual shall be referenced to evaluate the existing condition for compliance,



*Existing streetscape—Midway Drive*



*Inclusion of landscaping on Midway Drive*

and opportunities to reconfigure the roadway for lane widths and the creation of landscape and streetscape areas.

## VI Nodes

### EAST NODE

This major node is the grand design opportunity for the North Bay Community. The proposal is to design architecture, landscape, streetscape and hardscape into a comprehensive whole to create a visual and experience unlike any other in the region. This Grand Place is the center point of the community both in terms of circulation and geographically.

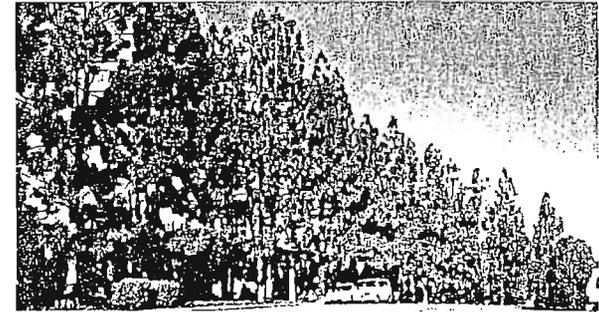
The use of large scale trees as exclamation points, unified with site furnishings, hardscape materials, patterns, and the design of the mass transit component, is a major undertaking that will result in a landmark for the community.

### WEST NODE

This northern node is the confluence of four separate streetscapes – Midway, Sports Arena Blvd., West Point Loma and Ingraham Street. The opportunity is to create a design that is able to take advantage of different streetscapes, and tie this intersection into a dramatic place. It will also tie into the Major Node to the south and going down both Sports Arena Blvd. and Midway Drive.

All of the above locations and land uses will be required to be upgraded and brought into conformance with the Architectural Design Standards and the City of San Diego Landscape Standards and Requirements.

The landscape designs shall draw from the North Bay Community Landscape Architecture Design Standards and Plant Palette to reinforce the improvements identified in these Design Guidelines.



*Approved parkway, median and portal tree—  
Canary Island Pine*



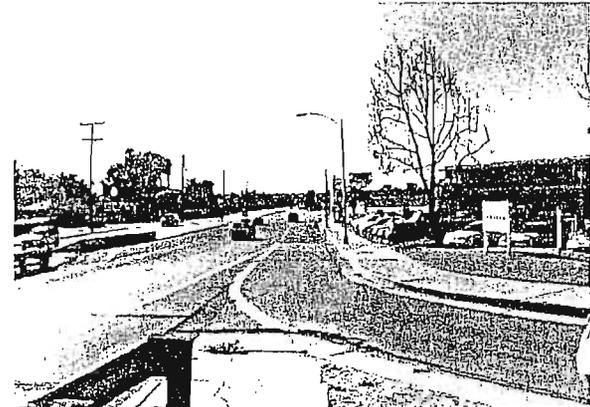
*Deciduous Canopy Trees Framing Street.*

## VII Tree/Plant Selection List

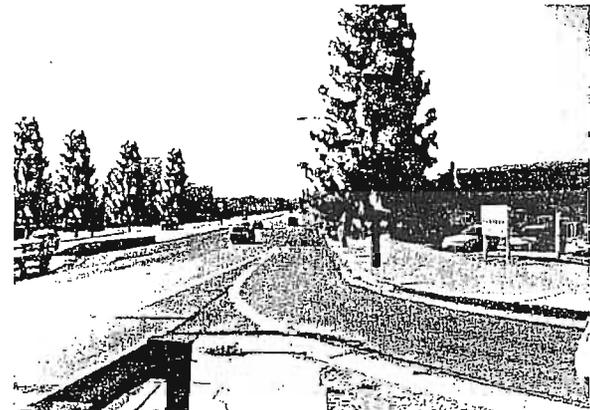
The intent of the recommended tree list is to:

- 1) Strengthen the character of the new North Bay District;
- 2) Demonstrate a commitment to the future with large distinguished trees;
- 3) Use trees that have adapted well to the San Diego climate. Significant landscaping can distinguish a project or community.

For the same costs, a rustic image, strong urban forestry concept can be established in the North Bay District.



*Existing merge at Barnett.*



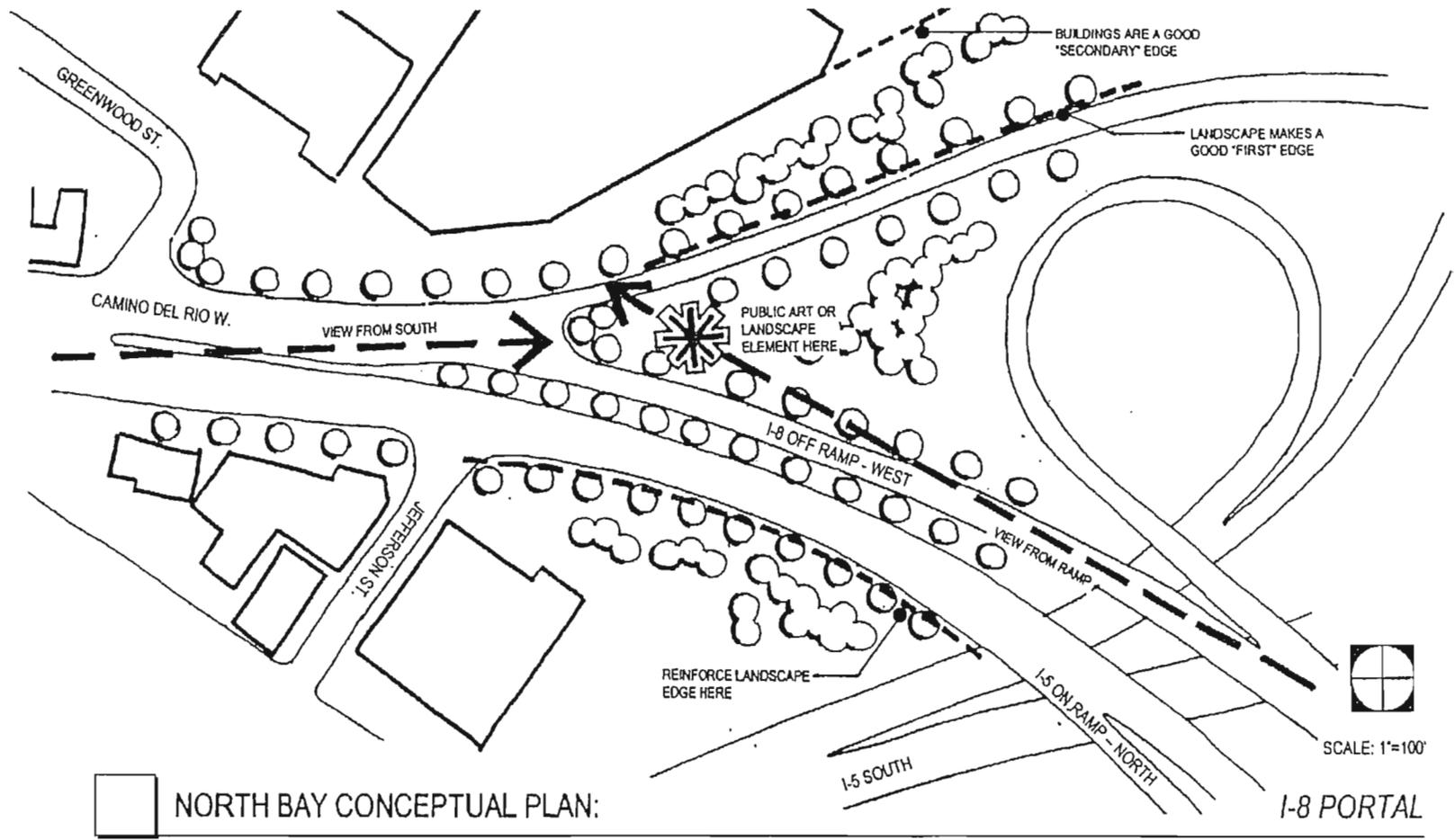
*Inclusion of landscaping.*

## TREE SELECTION LIST

Street: ROSECRANS & CAMINO DEL RIO WEST / I-8 N ODE

<u>Botanical Name</u>	<u>Common Name</u>	<u>Location</u>	<u>Type</u>
* Araucaria heterophylla <i>Note: Dramatic height (Skyline tree).</i>	Norfolk Island Pine	Center Me dian/ Portals	Evergreen
* Liquidambar styracifwa <i>Note: Beautiful fall color.</i>	Sweet Gum	Center Me dian/ Portals	Deciduous
* Tipuana tipu <i>Note: Large canopy tree.</i>	Tipu Tree	Center Me dian/ Portals	Semi-Deciduou s
Pinus torreyana <i>Note: Broad crown pine.</i>	Torrey Pine	Center Me dian/ Portals	Evergreen
Phoenix canariensis <i>Note: Fast growing and can get up to 60' tall.</i>	Canary Island Date Palm	Center Me dian/ Portals	Palm
* Populus italica 'nigra' <i>Note: Fast vertical growing tree.</i>	Lombardy Poplar	Portals	Deciduous
* Jacaranda acutifolia <i>Note: Beautiful canopy and purple spring flowering.</i>	Jacaranda	Parkways	Semi-Deciduou s
* Sapium Sebiferum <i>Note: Fast growing with beautiful fall color.</i>	Chinese Tallow	Parkways	Deciduous
Pinus canariensis <i>Note: Fast vertical growing tree.</i>	Canary Island Pine	Parkways	Evergreen
Pistache chinensis <i>Note: Fast growing with beautiful fall color.</i>	Chinese Pistache	Parkways	Deciduous

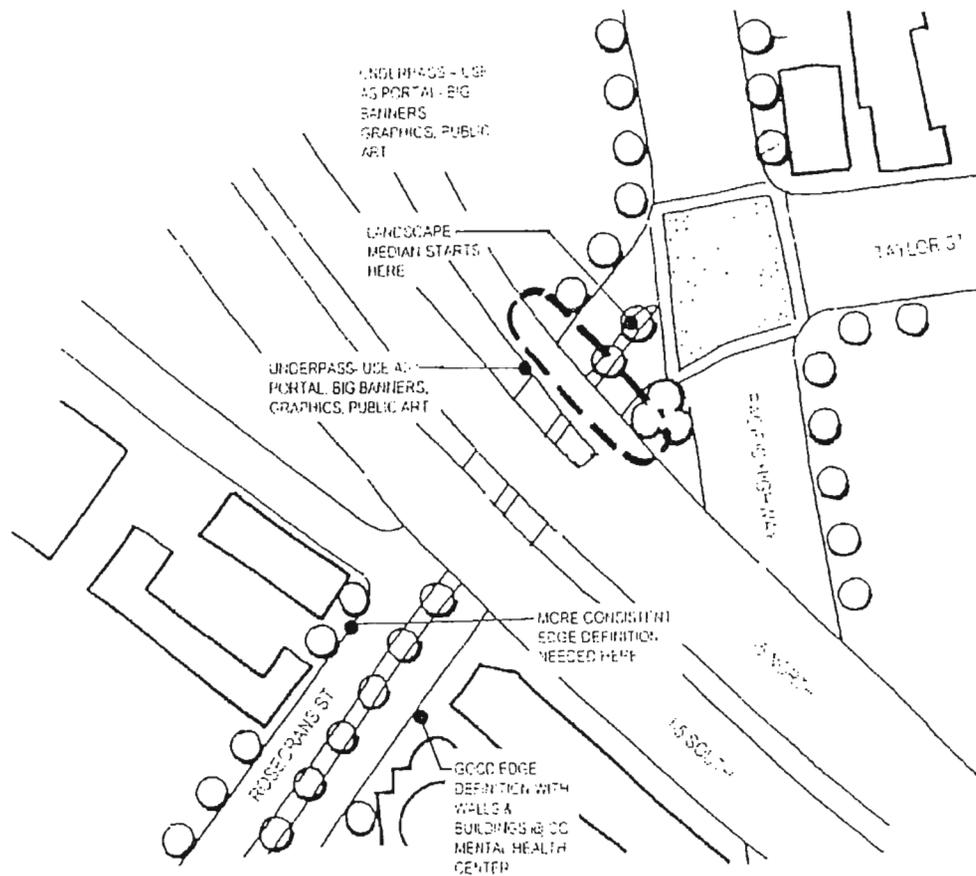
\* Prioritized tree for this street and location.



**Street: PACIFIC HIGHWAY**

<u>Botanical Name</u>	<u>Common Name</u>	<u>Location</u>	<u>Type</u>
* Tipuana tipu <i>Note: Large canopy tree.</i>	Tipu Tree	Center Me dian/ Portals	Semi-Deciduou s
* Populus fremontil <i>Note: Fast vertical growing tree.</i>	Western cottonwood	Center Me dian/ Portals	Deciduous
Araucaria heterophylla <i>Note: Dramatic height (Skyline tree).</i>	Norfolk Island Pine	Center Me dian/ Portals	Evergreen
Phoenix canariensis <i>Note:</i>	Canary Island Date Palm	Center Me dian/ Portals	Evergreen
Pinus torreyana <i>Note: Broad crown pine.</i>	Torrey Pine	Center Me dian/ Portals	Evergreen
* Populus italica 'nigra' <i>Note: Fast vertical growing tree.</i>	Lombardy Poplar	Portals	Deciduous
* Pinus canariensis <i>Note: Fast vertical growing tree.</i>	Canary Island Pine	Parkways	Evergreen
* Sapium Sebiferum <i>Note: Fast growing with beautiful fall color.</i>	Chinese Tallow	Parkways	Deciduous
Pistache chinensis <i>Note: Fast growing with beautiful fall color.</i>	Chinese Pistache	Parkways	Deciduous
Jacaranda acutifolia <i>Note: Beautiful canopy and purple spring flowering.</i>	Jacaranda	Parkways	Semi-Deciduou s

\* Prioritized tree for this street and location.



UNDERPASS - USE ART:  
PORTAL, BIG BANNERS,  
GRAPHICS, PUBLIC ART

LANDSCAPE  
MEDIAN STARTS  
HERE

UNDERPASS - USE ART:  
PORTAL, BIG BANNERS,  
GRAPHICS, PUBLIC ART

MORE CONSISTENT  
EDGE DEFINITION  
NEEDED HERE

GOOD EDGE  
DEFINITION WITH  
WALLS &  
BUILDINGS NBCC  
MENTAL HEALTH  
CENTER

TAYLOR ST

ROSECRANS ST

US 50



SCALE 1"=100'

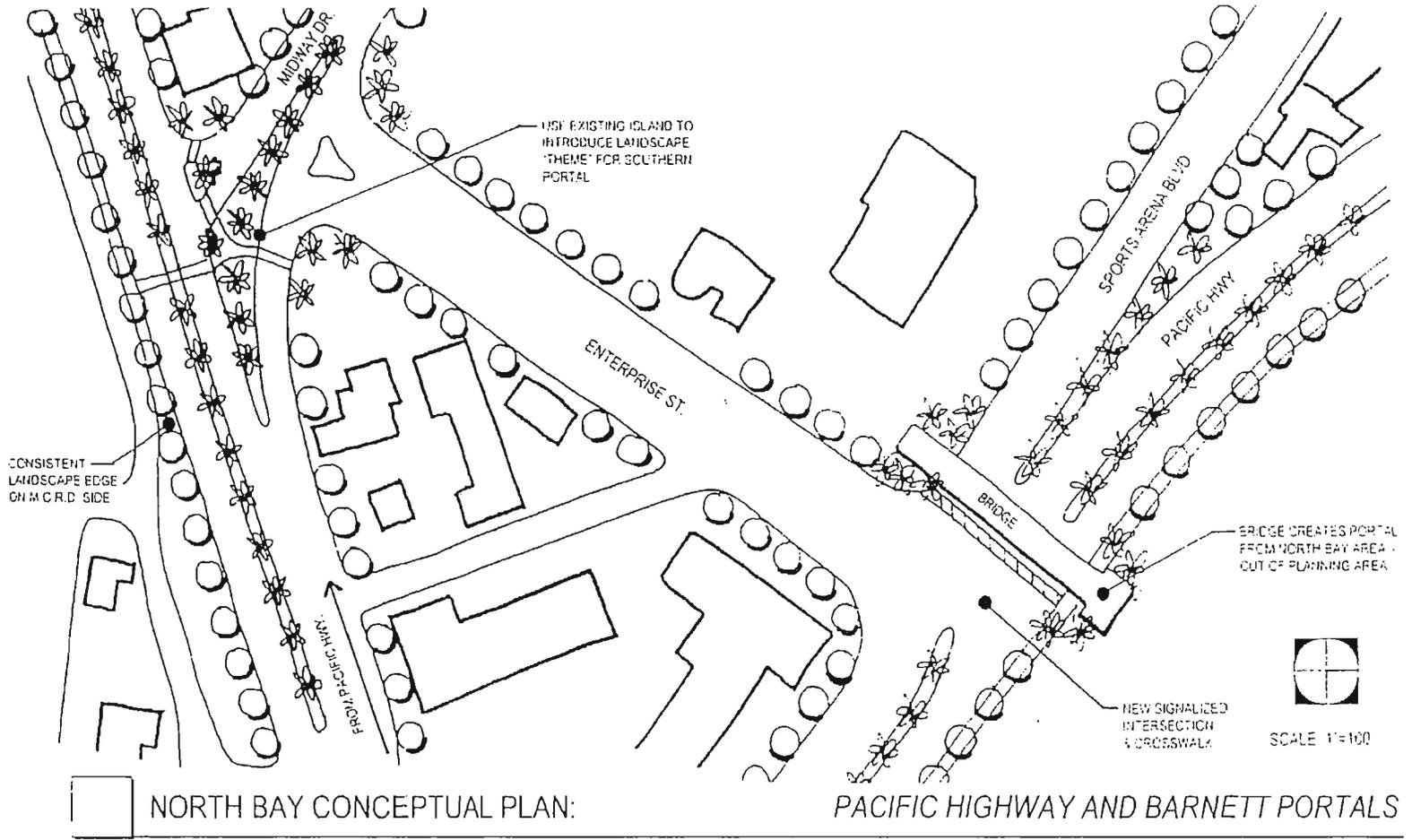
NORTH BAY CONCEPTUAL PLAN:

OLD TOWN PORTAL

**Street: MIDWAY DRIVE**

<u>Botanical Name</u>	<u>Common Name</u>	<u>Location</u>	<u>Type</u>
* Jacaranda acutifolia <i>Note: Beautiful canopy and purple spring flowering.</i>	Jacaranda	Parkways	Semi-Deciduous
* Pinus canariensis <i>Note: Fast vertical growing tree.</i>	Canary Island Pine	Parkways	Evergreen
* Pistache chinensis <i>Note: Fast growing with beautiful fall color.</i>	Chinese Pistache	Parkways	Deciduous
Sapium Sebiferum <i>Note: Fast growing with beautiful fall color.</i>	Chinese Tallow	Parkways	Deciduous
* Tipuana tipu <i>Note: Large canopy tree.</i>	Tipu Tree	Portals	Semi-Deciduous
Populus italica 'nigra' <i>Note: Fast vertical growing tree.</i>	Lombardy Poplar	Portals	Deciduous

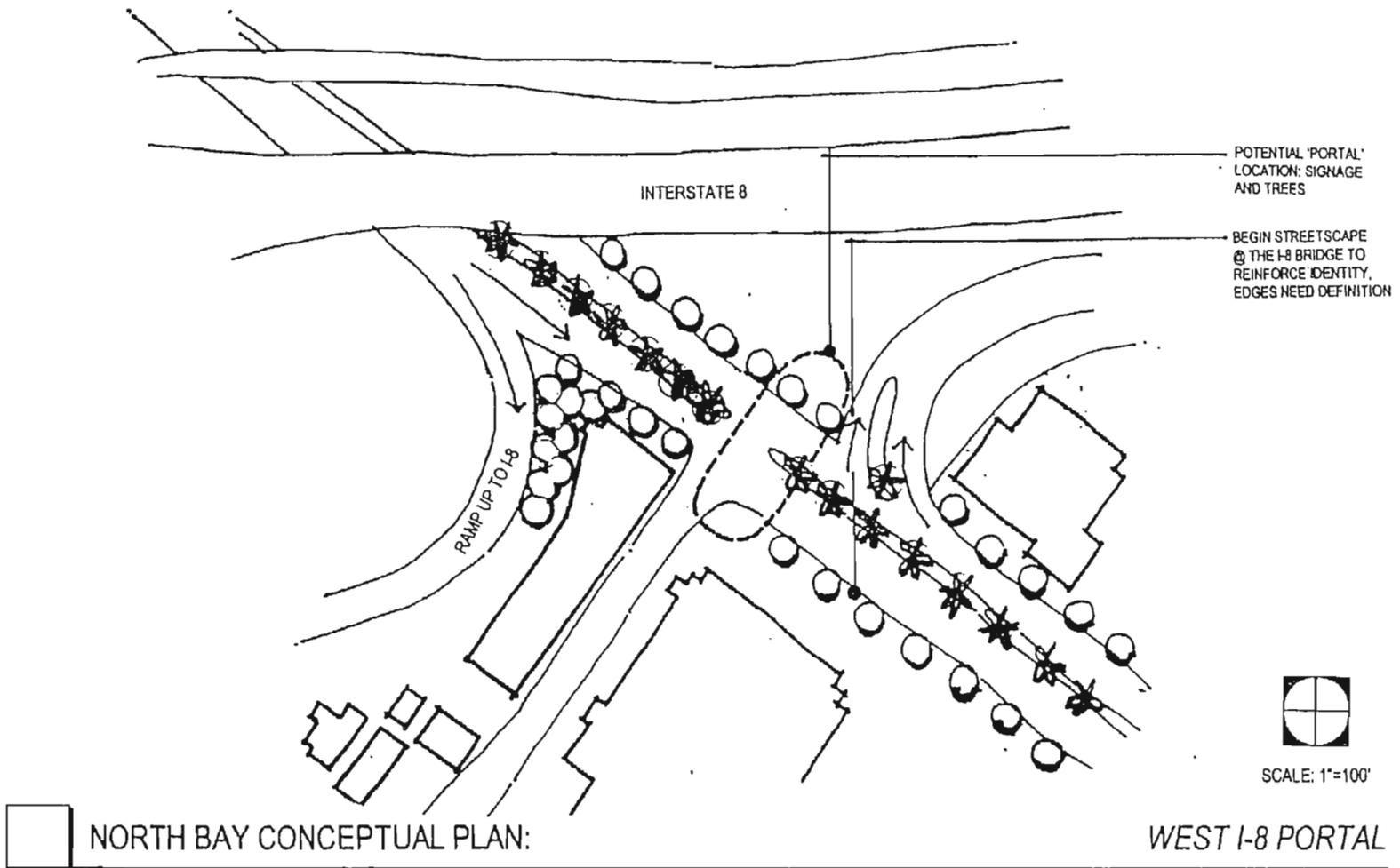
\* Prioritized tree for this street and location.



**Street: SPORTS ARENA BLVD / WEST POINT LOMA**

<u>Botanical Name</u>	<u>Common Name</u>	<u>Location</u>	<u>Type</u>
* Araucaria heterophylla <i>Note: Dramatic height (Skyline tree).</i>	Norfolk Island Pine	Center Me dian/ Portals	Evergreen
* Phoenix canariensis <i>Note:</i>	Canary Island Date Palm	Center Me dian/ Portals	Evergreen
* Tipuana tipu <i>Note: Large canopy tree.</i>	Tipu Tree	Center Me dian/ Portals	Semi-Deciduou s
Pinus torreyana <i>Note: Broad crown pine.</i>	Torrey Pine	Center Me dian/ Portals	Evergreen
* Populus italica 'nigra' <i>Note: Fast vertical growing tree.</i>	Lombardy Poplar	Portals	Deciduous
* Jacaranda acutifolia <i>Note: Beautiful canopy and purple spring flowering.</i>	Jacaranda	Parkways	Semi-Deciduou s
* Tristania conferta <i>Note: Existing tree.</i>	Brisbane Box	Parkways	Evergreen
* Sapium Sebiferum <i>Note: Fast growing with beautiful fall color.</i>	Chinese Tallow	Parkways	Deciduous
Pinus canariensis <i>Note: Fast vertical growing tree.</i>	Canary Island Pine	Parkways	Evergreen
Pistache chinensis <i>Note: Fast growing with beautiful fall color.</i>	Chinese Pistache	Parkways	Deciduous

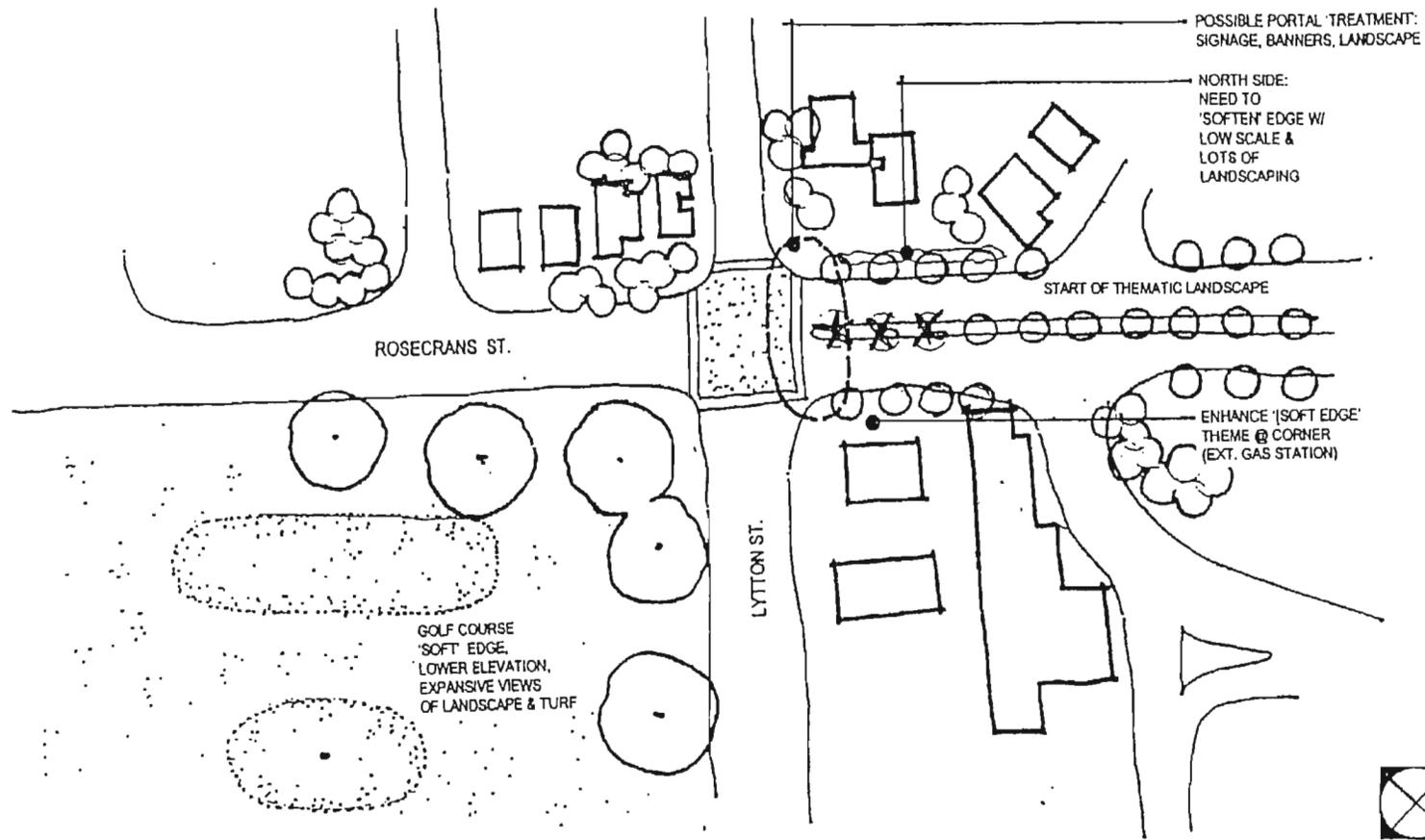
\* Prioritized tree for this street and location.



## SHRUB SELECTION LIST

<u>Botanical Name</u>	<u>Common Name</u>	<u>Location</u>	<u>Type</u>
Raphiolepis indica <i>Note: Pink and white flowers with no thorns.</i>	Indian hawthorn	Parkway	Evergreen Shrub
Corprosma repens <i>Note: White flowers, different color leaves and red berries.</i>	Mirror plant	Parkway	
Carrisa grandiflora <i>Note: Fast growing, white flowers and has thorns.</i>	Natal plum	Parkway	Evergreen Shrub
Euonymus fortunei <i>Note: Green leaves with white edges and is a climbing plant.</i>	Emerald gaiety	Parkway	
Escallonia <i>Note: Ideal in direct sun and blossoms pink flowers.</i>	Apple blossom	Parkway	Evergreen Shrub
Nandina <i>Note: Red tinted leaves that turn purple in the fall.</i>	Heavenly Bamboo	Parkway	
Phormium tenax <i>Note: Bold greenish gray to reddish brown sword shaped leaves.</i>	New Zealand Flax	Median	Evergreen Shrub
Hemerocallis callis <i>Note: Some evergreen varieties bloom year round.</i>	Day Lily	Median	Flowering Shrub
Bougainvillea hawaii <i>Note: Shrubby, mounding, spreading and one of the hardiest.</i>	Bougainvillea	Median	Flowering Shrub
Echium fastuosum <i>Note: Picturesque plant with great spike Like clusters of bluish-purple flowers.</i>	Pride of Madeira	Median	Flowering Shrub

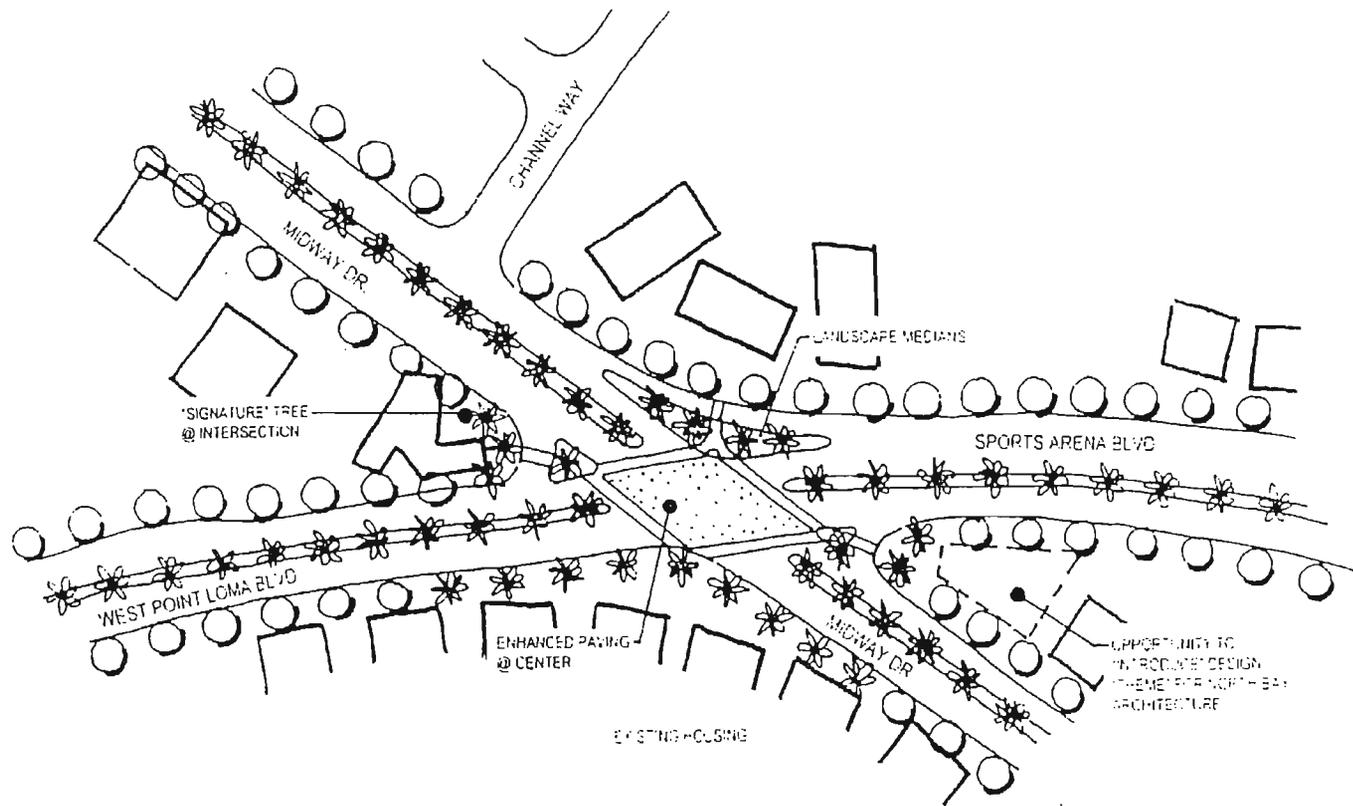
\* All shrub recommendations from Walter Anderson's Nursery.



SCALE: 1"=100'

NORTH BAY CONCEPTUAL PLAN:

POINT LOMA PORTAL



NORTH BAY CONCEPTUAL PLAN:

WEST NODE

## CHAPTER SIX - STREET AMMENITIES STANDARDS

The architectural and landscape standards proposed in this plan are intended to visually unify the North Bay community. This would strengthen and solidify its sense of identity and build its concept to the community at large.

Street amenities also represent an important component of the plan. Their intent is to help visitors identify the entrances and exits to the district.

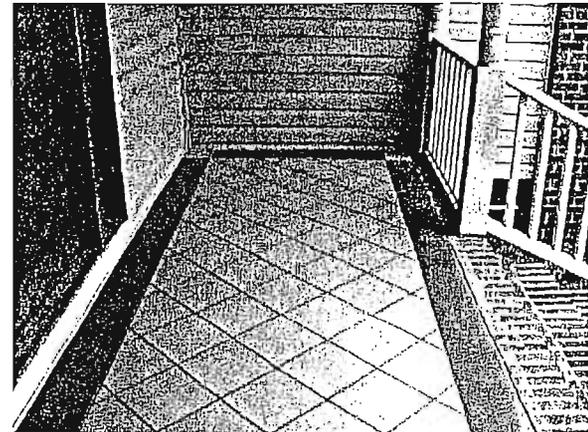
This section will make recommendations on the following street amenities and designs:

- Sidewalk scoring;
- Trash receptacles;
- Sidewalk furniture;
- News racks;
- Bike racks; and
- Utility Boxes.

### **Sidewalk Scoring**

There is currently no historic pattern of the sidewalk width and scoring within the North Bay area. The traditional width can be seen in front of Pacers Nightclub on Midway Drive where the width between the curb and the building frontage is approximately 12—14 feet. At the time this building was constructed it probably had a zero setback requirement with wide sidewalks to provide for pedestrian lighting, street trees and ample walking space.

Using this as a model it is recommended that all newly constructed side-



*SA-1: Diamond Square Scoring*

walks, where possible, have the following features:

Color: French Grey with a broom finish.

Width: Minimum 10 feet.

Tree Wells: Minimum 4'x6'

Scoring: Diamond Square 24" x 24" or larger (Image SA-1)

### **Trash Receptacles**

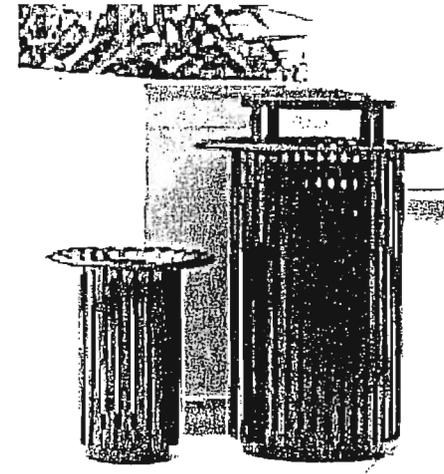
The following trash receptacles are recommended for all new developments and construction in the district. Trash receptacles should have spacing of 200 feet where possible. Acquisition and installation should be funded through public sources as capital improvements to the district.

The trash receptacles recommended for the North Bay District are to be found in the UpBeat, Inc. Winter 2003 catalogue, #SIW-LR100 (Page 103) in black. (Image SA-2)

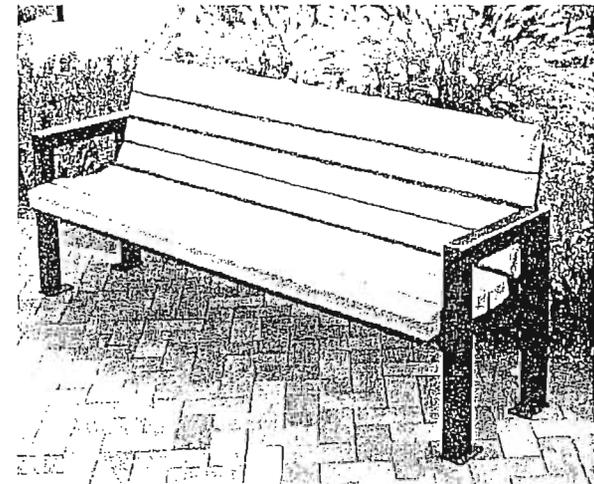
### **Sidewalk Furniture**

Two types of seating should be provided throughout the district. Near bus stops and along narrow stretches of sidewalk, permanent benches should be provided. In areas that evolve into public spaces, permanent benches and/or movable chairs may be used.

The movable chair concept is used in Bryant Park in Midtown Manhattan and was initiated by the Bryant Park Restoration Corporation. In San Diego they are presently used along India Street in Little Italy. Movable chairs are flexible and can be arranged to accommodate groups of varying sizes. They have been very success in Little Italy, but must be placed in areas with heavy public usage so as to discourage theft and inappropriate usage. They should clearly display the districts logo.



*SA-2: Trash Receptacles*



*SA-3: Permanent Benches*

These chairs should be inexpensive, metal and durable. They can be purchased at any major big box retailer.

The permanent benches should be UpBeat, Inc. These benches are recommended because they look historic, yet contemporary, with clean lines and a streamline look. They are commercial grade, durable and attractive. (Image SA-3)

### **Newsracks**

Though the North Bay currently does not suffer from newsrack clutter, as in Old Town, Gaslamp and Garnet Avenue, the intent of the Conceptual Plan is to promote increased pedestrian use. As land uses change to support residential development, visitor industry related businesses and overall increased density, independent newsracks will inevitably pop up.

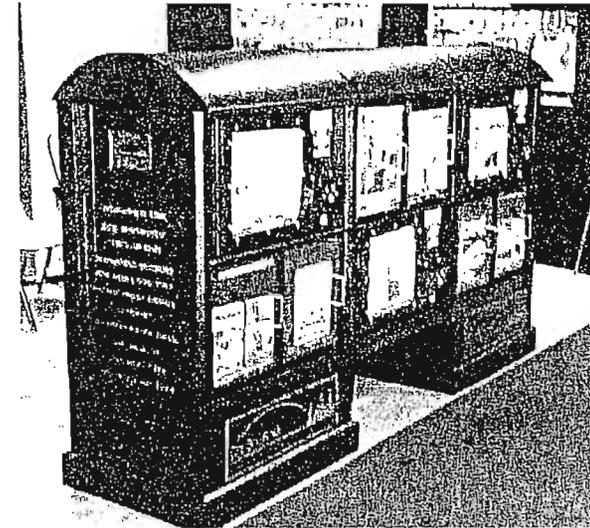
Therefore to anticipate such growth, the Association should select a newsrack to accommodate paid and free papers and place them throughout the district.

The recommended newsrack, currently used in Little Italy, is manufactured by City Solutions Outdoor. The newsrack houses 4 paid and 4 unpaid units and would be recommended to be done in black. (Image SA-4)

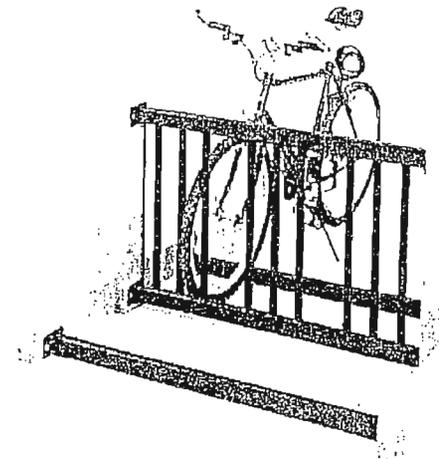
### **Bike Racks**

Though bicyclist might find it hard maneuvering throughout the intense volumes of traffic along the streets of North Bay, a design of bike racks should be selected.

Bike racks should be installed in areas of dense commercial areas and dense employment centers. Since the North Bay is relatively flat, it is a



*SA-4: Newsrack*



*SA-5: Bike Rack*

great area to bike from business to business.

The bike rack is recommended for placement in the North Bay District and can be found in UpBeat, Inc. catalogue. (Image SA-5)

### **Utility Boxes**

Utility boxes often mar the streetscape, and appear to be out of place “street furniture.” It is recommended that utility boxes be recessed wherever possible to limit their negative visual impact, and removing them as impediments to pedestrians and handicapped. It is also suggested utility boxes be painted a color that allows blending into its surroundings, such as the landscape and building architecture. The painting of artistic images on utility boxes should also be encouraged.

## **CHAPTER SEVEN - TRANSIT PLAN**

While the bulk of this chapter focuses on transit—related issues, private vehicular traffic in the North Bay is recognized as a significant element nonetheless. It is both positive ( the high volume is a boom to commercial enterprises) and negative (there is virtually no pedestrian realm, due to the dominance of automobile).

### **TRAFFIC IN THE DISTRICT: Existing Conditions and Historical Data**

One of the most frequent complaints about the Midway area is the huge volume of daily traffic serviced by the area's main commercial corridors. Listed below are average weekly count tables which show that in fact, traffic volume has been declining in the area. Much of this decline could probably be attributed to the Naval Training Center closure of the 1990s. This is a temporary phenomenon and traffic will probably increase with the ongoing development of Liberty Station on the former NTC site in the late decade.

## CHAPTER SEVEN - TRANSIT PLAN

While the bulk of this chapter focuses on transit—related issues, private vehicular traffic in the North Bay is recognized as a significant element nonetheless. It is both positive ( the high volume is a boom to commercial enterprises) and negative (there is virtually no pedestrian realm, due to the dominance and number of automobiles).

### **Traffic in the District: Existing Conditions and Historical Data**

One of the most frequent complaints about the Midway area is the huge volume of daily traffic, corresponding congestion and wait times at the area's main and secondary commercial corridors. One of the main reasons sited for such conditions is the lack of a functional street grid, forcing vehicles to utilize the same routes in and out of the area. Contributing to the congestion are irregular traffic patterns caused by numerous curb cuts.

The tables below depict the high volumes of traffic at four key intersections in North Bay. Due to the closure of the Naval Training Center in 19\_\_, traffic volumes at three of the four key intersections actually decreased. This is a temporary phenomena as the development of the site will surely cause a similar increase, guaranteeing that traffic congestion will remain a significant problem and possibly an impediment to growth in the area.

**AVERAGE WEEKLY TRAFFIC—MIDWAY AREA 1977-1981**

<u>Primary Street</u>	<u>First Cross Street</u>	<u>Second Cross Street</u>	<u>1977</u>	<u>1978</u>	<u>1979</u>	<u>1980</u>	<u>1981</u>	<u>5-Year Avg.</u>
1080 Rosecrans St./ Route 209	Lytton St.	Midway Dr.	26.8	29.8	32.8	33.1	28.1	30.12
1081 Rosecrans St./ Route 209	Midway Dr.	Sports Arena Blvd.	31.9	39.6	44.7	45.2	38.0	39.88
1082 Rosecrans St.	Sports Arena Blvd.	Kurtz St.	16.8	17.9	18.4	17.5	18.5	17.82
1083 Rosecrans St.	Kurtz St.	Pacific Hgwy.	13.5	15.7	14.5	15.8	16.8	15.26

\*Traffic counts are a represented to be 1,000 vehicles per 1 point.

**AVERAGE WEEKLY TRAFFIC—MIDWAY AREA 1989-1993**

<u>Primary Street</u>	<u>First Cross Street</u>	<u>Second Cross Street</u>	<u>1989</u>	<u>1990</u>	<u>1991</u>	<u>1992</u>	<u>1993</u>	<u>5-Year Avg.</u>
1080 Rosecrans St./ Route 209	Lytton St.	Midway Dr.	39.3	44.1	39.1	39.7	39.7	40.38
1081 Rosecrans St./ Route 209	Midway Dr.	Sports Arena Blvd.	56.6	63.8	58.6	58.0	58.0	59.00
1082 Rosecrans St.	Sports Arena Blvd.	Kurtz St.	21.0	17.8	20.3	20.2	20.2N	19.19
1083 Rosecrans St.	Kurtz St.	Pacific Hgwy.	18.2	19.0	18.2	18.2N	15.9	17.19

\*Traffic counts are a represented to be 1,000 vehicles per 1 point.

**AVERAGE WEEKLY TRAFFIC—MIDWAY AREA 1996-2000**

<u>Primary Street</u>	<u>First Cross Street</u>	<u>Second Cross Street</u>	<u>1996</u>	<u>1997</u>	<u>1998</u>	<u>1999</u>	<u>2000</u>	<u>5-Year Avg.</u>
1080 Rosecrans St./ Route 209	Lytton St.	Midway Dr.	35.2	36.8	36.3	35.8	34.8	35.78
1081 Rosecrans St./ Route 209	Midway Dr.	Sports Arena Blvd.	53.1	53.4	52.4	51.9	51.3	52.42
1082 Rosecrans St.	Sports Arena Blvd.	Kurtz St.	20.3N	20.3N	20.3	20.0	20.0N	20.18
1083 Rosecrans St.	Kurtz St.	Pacific Hgwy.	14.9	14.9N	14.9	10.8	10.8N	13.26

\*Traffic counts are a represented to be 1,000 vehicles per 1 point.

## **TRAFFIC PROJECTS**

To address community concerns relative to traffic congestion and other relevant traffic issues, former Councilman Byron Wear partnered with the North Bay Association and established the North Bay/Peninsula Traffic Task Force (TTF) in 2001. The TTF is made up of representatives from seven local community groups, and city staff from Traffic and Engineering, Planning, Redevelopment, and the District 2 Council Office. The group meets quarterly identifying the area's traffic problems and possible solutions. Below are the TTF's identified "Projects" for the area, listed in short, mid, and long-term categories:

### **Short-term projects**

Project G: Right turn lane on West Point Loma, turning into Midway.

Project I: Extend left turn pockets on Rosecrans at Midway Drive, going North and South.

Project J: Add a new North bound dedicated right turn lane on Rosecrans at Pacific Highway.

### **Mid-term projects**

Project D: Widening Sports Arena Boulevard through to Pacific Highway.

### **Long-term projects**

Project A: Complete Old Town and Barnett I—5 ramp, plus reconstruction of ramp at Pacific Coast Highway.

Project B: Complete interchange from I—5 southbound to I—8 west-

bound.

Project C: Complete interchange from I—8 eastbound to I—5 northbound.

This Plan supports the implantation of these recommendations.

## TRAFFIC & PEDESTRIANS

In order to better accommodate the anticipated increased pedestrian population of the North Bay, the following guidelines are mandated:

<i>Select Matter</i>	<i>Page # Sited</i>	<i>Recommendations</i>
Vehicle Travel Lanes	49	“Within pedestrian supportive areas, the vehicle travel lanes should be 10 to 11 feet in width and parking lanes 7 feet in width.”
Pedestrian Crossings	49	“The number of pedestrian crossings should be maximized in order to prevent a street from becoming a barrier in the community.”
Zero Set-back Buildings	84	“Increased buffering between fast-moving traffic and abutting properties created by wider sidewalks or local access lanes makes the street more attractive for buildings to front directly onto the street.”
Sidewalk Widths	85	“Curb to curb roadway width can be reduced when traffic volumes are less than 12,000—15,000 per day by restriping a road from a four-lane roadway to a two or three lane roadway. Also, if travel lanes are more than 12 feet, the road width can be reduced to create 12 foot lanes, and in some cases, as narrow as ten feet.”
Sidewalk Widths	89	“In commercial and mixed-use areas with storefronts close to the street, the preferred width of a sidewalk is 13 to 16 feet. This allows for pedestrian circulation and window-shopping. The minimum possible width for new development should be 10 feet.”

## CHAPTER EIGHT - COMMUNITY SIGNAGE

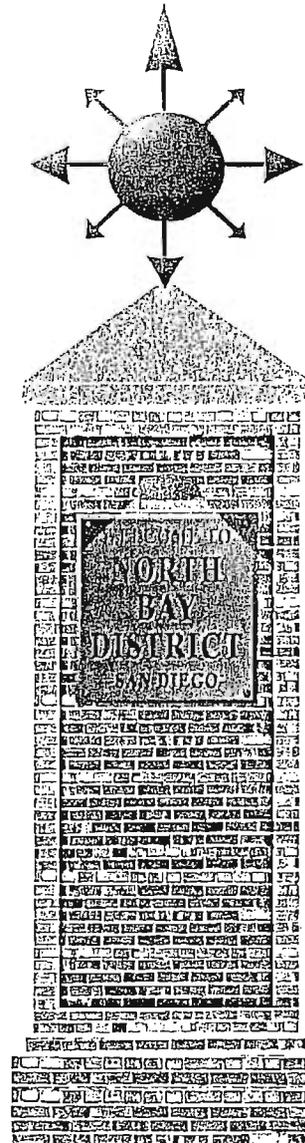
This chapter discusses two types of signage: 1) new community identity signs used to create a specific area identity, and 2) controls on private signage.

There will be three types of community identity signs for the North Bay District. The function of these three types of signs will be to integrate the various sub-districts and bring cohesion to the community as a whole.

Three symbols of community identity are proposed here: 1) Gateway Posts, 2) a Landmark Sign, 3) Centerline Median Monuments. Examples of each are attached in the following pages:. Current City standards for “Community Identification Signs” are necessarily restrictive in nature. They are laid out in Section 141.1104 of the General Regulations of the San Diego Municipal Code. The current code states the following:

- “(a) Community Entry Signs:
- Nor more than one sign shall be installed on each major approach to the community, with no more than four signs total for the community;
  - Community entry signs shall not encroach into the public right of way but shall be located within 12 feet of the public right of way;
  - The signs shall be ground mounted and stationary;
  - The area of the sign that identifies the community shall not exceed 4 feet in height or 8 feet in length.
  - Community entry signs may be illuminated with low intensity, ground mounted flood lights.”

Noting the above regulatory restrictions variances will be required to implement the community identity signs recommended here.

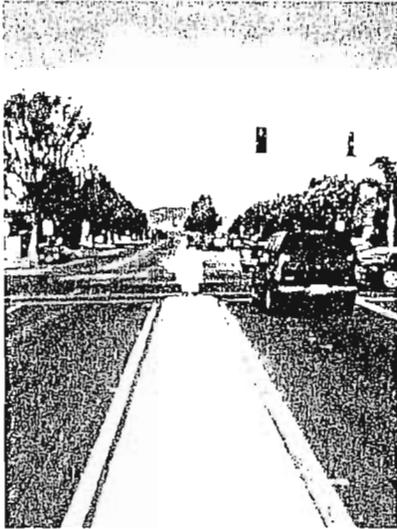


*Gateway Post  
(Conceptual Only)*

**Recommendations (Signage and Locations)**

The following recommendations are made with the understanding that certain conditions and land—use decisions might necessitate changes to both proposed signs and locations. Nevertheless, a “Landmark Sign” and “Welcome/Identity Signs” at nodes and portals are critical to establishing a neighborhood and strongly recommended.

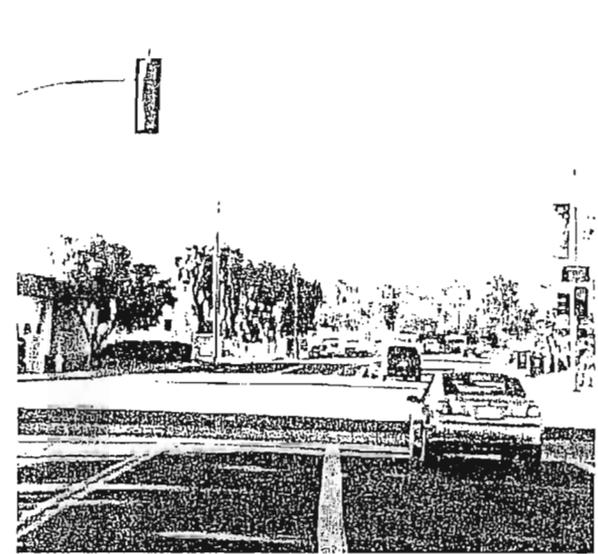
<i>Type of Sign/Lighting</i>	<i>Function</i>	<i>Proposed Location(s)</i>
Gateway Posts (Back lit with light blue solar power lights)	Serve as markers for entry and reinforcement at street corners	At Nodes and some intersections.
Landmark Sign (neon traditional San Diego sign)	Center point of community, heart of district	See Map “Community Signs,” on Page ____
Banner Monuments	Installed with minimum of 8 foot medians or corners	Entry to district (portals).



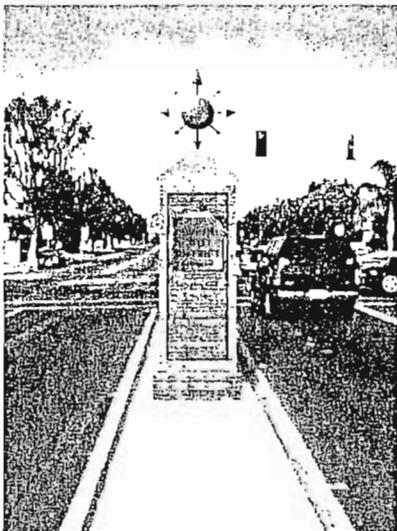
*Before*



*Before*



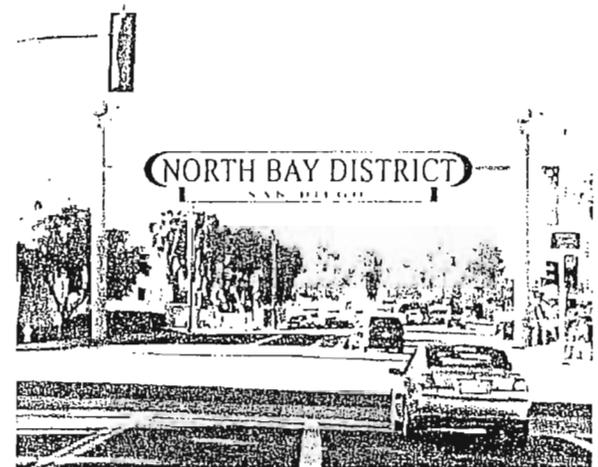
*Before*



*After  
Gateway Post  
(Conceptual Only)*

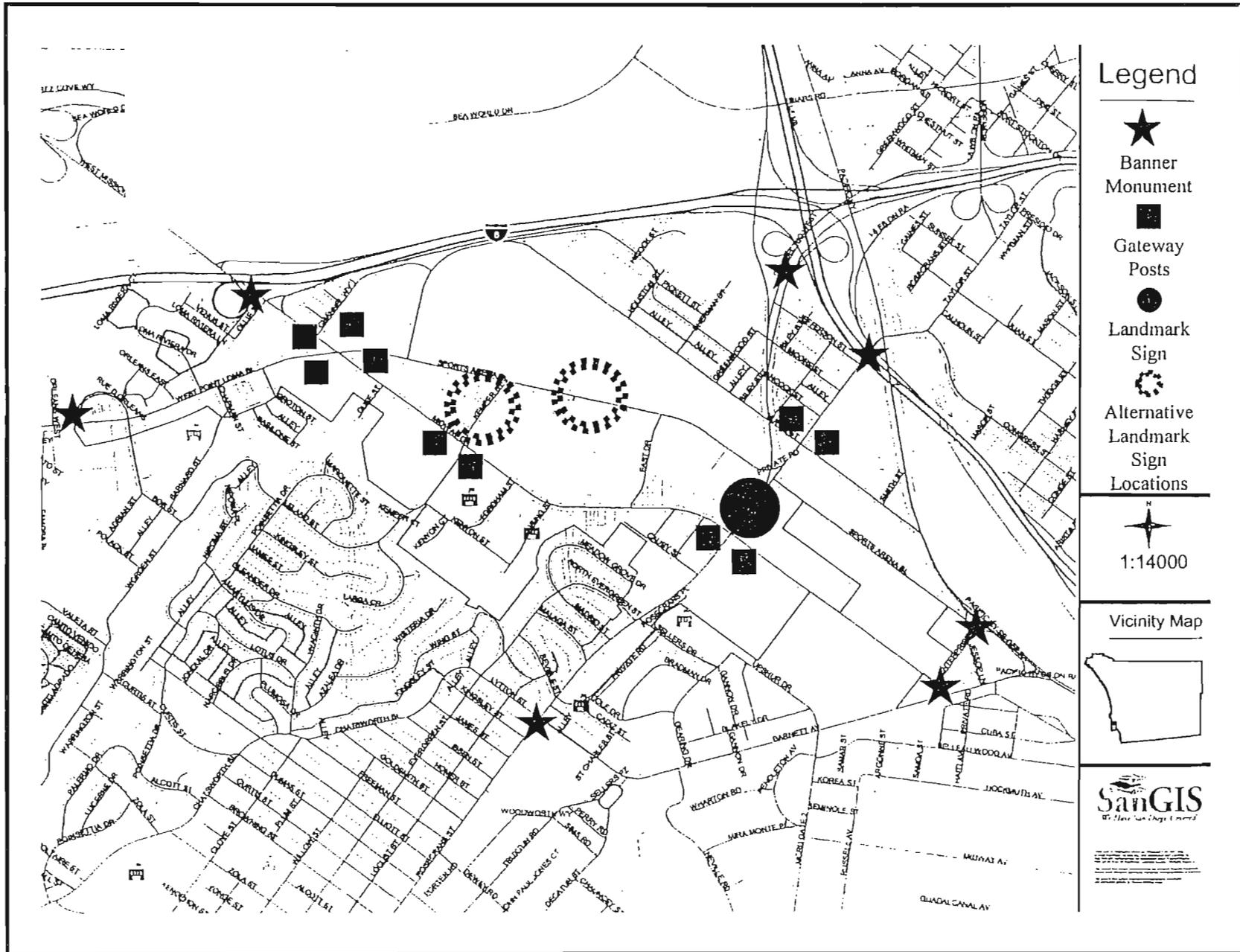


*After  
Banner Monument  
(Conceptual Only)*



*After  
Landmark Sign  
(Conceptual Only)*

# COMMUNITY SIGNAGE



MAP 8 – Various signage as symbols of community identity.

## Private Signage

The reduction of “visual clutter” through the careful control of signage is a priority of this Plan. Toward that end, new signs must adhere to the City’s sign code ordinance and must follow the guidelines set forth in the *Midway/Pacific Highway Corridor Community Plan* for:

- commercial signage, pages 62 and 63;
- industrial signage, page 70;
- residential signage, page 84.

For new projects, signage must be compatible with the architectural style adopted. It should be integral with the architecture where possible and consistent with its character, including such considerations as color, texture, graphics, type, style, shape and materials.

Neon may be used in signage only with internally—mounted displays, such as those installed on the inside face of shop windows. Other applications of neon on signs, buildings and other structures is not allowed, except for neon used in art installations, which shall be subject to community review.

In addition, signs may not be pole—mounted.

It is also recommended that the City of San Diego removes existing billboards when redevelopment of a site presents such an opportunity, and that the City solicits community input from the Midway Community Planning and Advisory Committee when signage variances are requested.



*Uncontrolled private signage: visual chaos*

## CHAPTER NINE - STREET LIGHTING STANDARDS

Pedestrian lighting in the District is a element in neighborhood safety, the overall atmosphere, and the maintenance of order. It is recommended that new pedestrian lights remain constant with the historic acorn lights previously used and still seen in certain locations throughout the district.

Granite stemmed, single acorn, glass topped pedestrian lights, a minimum of 14 feet in height, with "hats" reflecting light downward, will be installed throughout the district at a horizontal spacing recommended by the manufacturer to provide the required light levels. The main, wide corridors along Sports Arena Blvd., Camino del Rio West and Rosecrans Street would have double acorn lights; the peripheral areas would use single acorn lights. These lights are based upon the standards recently installed throughout Orange Avenue in Coronado. They replace existing light types.



*Proposed lighting for North Bay  
(Double Acorn Light )*



*Proposed Lighting for North Bay.  
(Single Acorn Light)*

## CHAPTER TEN - LAND-USE & ZONING

As an area that was designated as blighted under redevelopment law, it is recognized that changes to the existing land—use and zoning in North Bay should be considered. The City’s need for housing, coupled with the benefits residential development will deliver to our community, supports rezoning to accommodate such land—use. Rezoning will also be required to accommodate mixed—use development and higher density. The North Bay Redevelopment Plan and the North Bay 5-Year Implementation Plan are two guiding policy documents for the area that envision building market rate and affordable housing, as well as, new retail and open space. In addition, all development should be consistent with the policies of the Midway Community Plan.

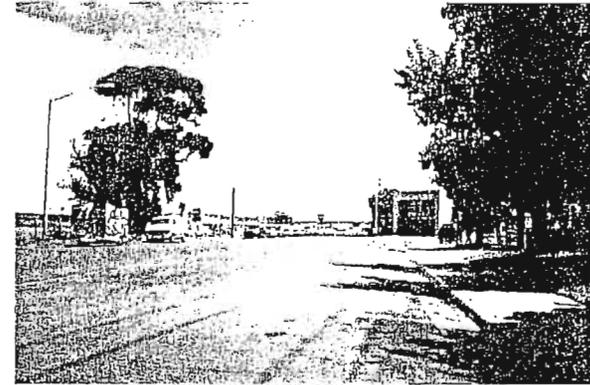
As for specific recommendations, this chapter focuses on and supports three key concepts:

- 1) Orientation to the San Diego River;
- 2) Public Spaces and the North Bay Commons; and
- 3) Redevelopment of city-owned property.

### *Orientation to the San Diego River*

Ever since the extension of Interstate 8 west to Ocean Beach and the creation of the San Diego River Flood Control Channel, the river has been a remnant of what it once was. Though the river was certainly not the significant waterway to be found in other major cities, it was, in fact, the lifeblood of this city at one time and the home of the European explorers to the West Coast.

The orientation of this district has always been away from the river. The picture at the beginning of this document taken from the hill at the present—day University of San Diego, depicts the Midway area as a watershed linking the current Old Town site to Point Loma. The land fill through Dutch Flats, NTC, MCRD, and Shelter and Harbor Island, and the creation of Mission Bay sought to control this watershed and mitigate against the possibility of one hundred year floods.



*Existing conditions: No linkage to the San Diego River (Hancock Street).*

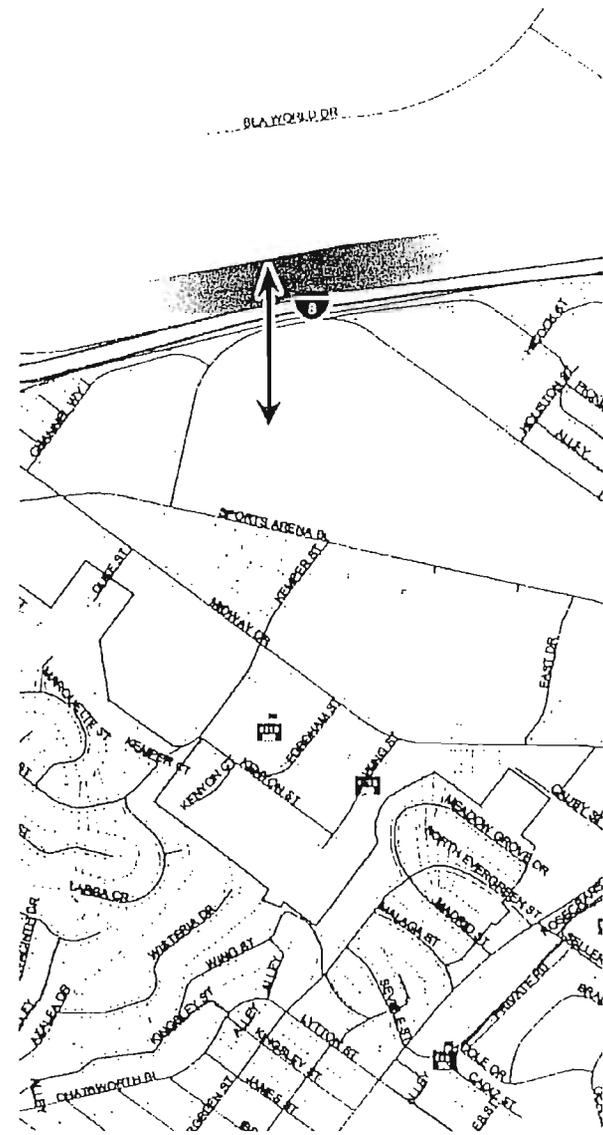
With significant amounts of land in close proximity to the river either owned by the city or underutilized or blighted, making substantial redevelopment desirable, the possibility and value of reorienting such development towards the river should be considered. Rather than seeing the River as an inconvenience that would shut down roads in heavy rainstorms, the river should be seen as an asset providing open space and recreational opportunities, and linking the beach to the Old Mission Gorge Dam in Santee.

The idea of linking the beach to Old Mission Gorge Dam has been embraced by San Diego’s Mayor. On April 5<sup>th</sup>, 2003, the San Diego Union Tribune reported that Mayor Murphy replaced his 10 goals with more specific objectives. Murphy reported, that his *“top objective is establishing the San Diego River Conservancy – a series of parks, trails and parcels of land preserved as wildlife habitat along the entire length of the river”* Murphy stated... *“That will have an impact that will last a century. It is very ambitious. We are talking about a river that has been abused for decades. To restore and reclaim it is no minor task.”*

No discussion about restoring the river can proceed without discussing access. The new North Bay District, a series of neighborhoods, has as a primary asset access to the river. Just as Park West along 6<sup>th</sup> Avenue is related to Balboa Park, North Park is related to Morley Field, and San Carlos is related to Mission Trails Park, the North Bay District should be physically and conceptually related to the “rebirth” of the San Diego River.

Examples exist of major public parks isolated from surrounding communities: the east end of Mission Bay Park, the south end of Balboa Park across from Cortez, the east side of Balboa Park along Park Blvd., etc. What is needed are parks that are more integrated into residential and business districts.

To see the San Diego River become a year round flowing waterway would lead to an explosion of new watershed life, creating an environmentally distinct community where one currently does not exist. The North Bay and its orientation to the north, linked by a pedestrian bridge might do more to change the image of this district than almost any other single factor.



River Linkage

## Public Spaces: The North Bay “Commons”

San Diego is not a city of great public spaces. Public spaces have only occasionally been seen as a necessity for making the city more livable. Cities such as Philadelphia, New York, Boston and Chicago have excelled in the creation of small public spaces dispersed throughout the city. San Diego is just beginning to realize the value of such land—use.

When areas convert from commercial to residential land—uses, or whenever residential density is increased, residents and visitors alike will need public spaces. Public spaces are what define a community. Unfortunately the building of these public spaces can be costly, requiring either the acquisition of private land, the donation of public or private land, or the expansion of sidewalks. Recognizing the burden to the City with such capital expenditures it is envisioned that private sector entrepreneurs would incorporate public spaces into new developments.

In North Bay three kinds of open space are envisioned:

1. “Commons” or Pocket—Parks;
2. Full-Service Parks; and
3. Mixed—Use Open Space.

### *Commons*

Public spaces in the North Bay District should be known as “Commons”. Little Italy will have its piazzas and Sherman Heights and San Ysidro will have their plazas. The Commons of North Bay should be constructed in each of the new sub—districts or neighborhoods.

The Commons should be a place to relax, be heavily landscaped and serve as an oasis next to highly trafficked streets. They should be small, compact, provide for non-permanent seating, and be accented with flowers, colors and shade. One good example of this use of public space in small areas is to be found in Herald Square off Broadway in mid-town Manhattan.

The Commons will necessitate ongoing non—City funded maintenance ser-

vices, and should be funded by a North Bay Property Assessment District. The Commons are key to altering the image of this area and making it an attractive place to live, work and visit. Linking the Commons would be heavily landscaped streets and pedestrian corridors.

#### *Full Service Parks*

Although North Bay is an area of few residents, and surrounded by significant full service parks in Mission Beach, Point Loma, Ocean Beach, and Old Town, it is suggested that substantial new residential development in the area be accompanied by the creation of a full service park or parks. Noting the high cost of land in the area, such space would either have to be donated by the City, or acquired from a land—owner or developer for a fee or other incentive(s). While converting underutilized City, County, or government land seems to be the easiest and most cost effective solution, incentivising the private sector is usually the most rapid and realistic way of accomplishing most any goal. Consequently we encourage and recommend that the City incentivising land owners and developers in an effort to facilitate residential and open space development.

#### *Mixed—Use Open Space*

In addition to Commons and traditional parks, valuable open space can be created by incorporating commercial—uses with recreational—uses. The cost of these areas of mixed—use open space is absorbed by the property or business owner, but enjoyed by the public. Mixed—use open space can take the form of open—air markets, promenades, plazas, and even amphitheaters surrounded by retail. This form of open space is not just cost effective, it supports and energizes the business community while improving the communities image and everyone's quality of life.

## **Redevelopment of City—Owned Property**

Approximately 95—acres of land in North Bay are owned by the City of San Diego. 43 of those acres represent the Sports Arena site, while the other 52-acres represent land which is either contiguous or in close proximity. The City's Real-Estate Assets Division has indicated their intention of redeveloping select portions of this asset, with the goal of increasing annual revenue to the City from the present approximate 2.5 million to approximately 10 million. Redevelopment of any significant portion of this significant land—mass will be a catalyst for additional development and improvements to the area, and may become the bridge between the historic Midway area and the San Diego River.

The following paragraphs offer broad concepts on what should be considered for new development in the area:

### **Overall Land Use**

The use of these 95 acres, particularly the Sports Arena site, will single-handedly reshape the future of this district. The land uses should include a mixture of retail/commercial, residential, high-tech, office spaces and possibly and at least one entertainment venue, perhaps an amphitheatre facing the San Diego River, developed in conjunction with the open space park.

### **Orientation**

As mentioned previously, the orientation of some development should be to the north. Whereas the retail corridor will face south along North Bay Avenue, (See below) some residential, office, visitor and entertainment uses should be oriented to the north and northwest, towards the riverfront.

### **Street Names**

If at some point the Sports Arena is moved or demolished, the name of the boulevard will no longer make sense. Therefore, it is proposed that Sports Arena Blvd. become *North Bay Avenue* to define the new image of the district.

## **Height**

As mentioned in Section One, Part E, the People's Ordinance of 1972 regulated building height in the "Coastal Zone." Serious consideration must be given to exempting the Sports Arena site from the Coastal Zone restrictions to allow it to develop into a higher density, transportation-oriented, pedestrian-oriented twenty-first century district.

An example of the indirect benefits: the ability to create a land bridge over the freeway to the renewed San Diego River will, in part, need to be financed privately. The revenues to realize this linkage to the river can be accomplished through the increased developer fees or the tax increment generated from the more intensively developed Sports Arena property.

## **Natural Resources**

The North Bay Association should encourage San Diego's Mayor and political leadership to formalize his San Diego River Conservancy and earn a seat on this significant body. Water has always been key to the development of this district, dating back to its function as a land link between Point Loma and Old Town. Just as the link to the Bay to the southwest initiated this district as the original commercial corridor on the west coast, the link to the River to the northwest will re-create this district.

## **Street Grid and Street Naming**

The street grid in and around the old Frontier Street (Sports Arena Blvd.) was eliminated to accommodate the footprint of the Sports Arena site and its related parking lot. The Conceptual Plan team is proposing that the street grid be re-established, much like what is being done around Ground Zero/The World Trade Center site in lower Manhattan. Maps 9 and 10 illustrate two possibilities.

Zero setbacks, wherever possible, should be mandated to prioritize street life in the new district. The urban design guidelines set forth in Chapter 4 will also help provide the framework for a vibrant new area.

## CHAPTER ELEVEN - IMPLEMENTATION

In the early part of 1999, the North Bay Design Committee, embarked on a discussion of design standards and called for architectural design in one of three styles: Brick Warehouse, NTC Spanish Revival and Neo-Modern. These have been revised to the “styles” detailed in Chapter 4.

The design concepts were presented to the Business Improvement District Board of Directors, the Midway Community Planning Group and the North Bay Planning Area Committee and adopted by each organization. A clarification of the guidelines was that they be applied only to the major commercial corridors, primarily Rosecrans Street, Camino del Rio West, Sports Arena Boulevard, Midway Drive and West Point Loma Boulevard.

Existing methods of codifying design standards were reviewed:

- ~ A *Precise Plan* is a smaller area designated within a larger Community Plan, which would not be applicable here since due to size;
- ~ The Planning Department is no longer allowing the *Planned District Ordinance* to serve as the regulatory document in the City of San Diego; therefore, that would not be relevant to this task;
- ~ A *Specific Plan* is not being used any longer in the City; that would not be functional here;

Thus, through a process of elimination, the most appropriate vehicle for implementing the design guidelines for the district would be through the amending of the existing The Midway Community Plan. A series of strikeouts and amendments are proposed to the existing plan, citing page numbers and wording (See Appendices). The process for initiating change to the Community Plan is as follows:

If the Department initiates the proposal through advice from the Community Planning Group, the Planning Department embarks upon a “Process Five” sequence of events.

1. Community Planner would report the desire for amending the plan to the Planning Commission;
2. The Planning Commission would then hold an initiation hearing and vote whether or not to proceed with the amendments;
3. If the Commission decides to initiate the process, it instructs the Planning Department to bring back the proposed amended document;
4. The Department then presents the amended document to the Commission and they vote the proposed changes up or down;
5. If the Planning Commission approves the changes, the amended plan goes to the full City Council;
6. If the Planning Commission denies the proposed changes, the Planning Group can then appeal their decision to the full City Council.

## SECTION III - APPENDICES

### CONTENTS

- A) Amendments to Midway/Pacific Highway Corridor Community Plan for architectural and urban design standards.
- B) Transit Recommendations:
  - Potential Routings of Fun 'n' Sun Line
  - Potential Point Loma Transit First Lines
  - Connecting the North Bay Regionally

## AMENDMENTS TO THE MIDWAY/PACIFIC HIGHWAY CORRIDOR COMMUNITY PLAN

The architectural design standards (“design standards”) outlined above modify language in the *Midway/Pacific Highway Corridor Community Plan (and Local Coastal Program Land Use Plan)* dated May 28, 1991 and amended January 19, 1999. The following references are to specific pages in that document. “The Plan” refers to the North Bay Conceptual Plan. NOTE: the following amendments apply only to the streets designated by The Plan and within the areas covered by The Plan: Rosecrans Street, Sports Arena Boulevard, Midway Drive, Camino del Rio West and West Point Loma Boulevard.

1. “Introduction”, page 4, paragraph 3: “design controls” shall include the design standards mandated by The Plan.
2. “Commercial Land Use”, pp. 61 through 64, Urban Design Guide lines: amended to mandate the design standards of The Plan. The phrase, “...flexibility in architectural style...” (page 61) shall be revised to “...flexibility in architectural style, *within the parameters of the design standards of The Plan...*”.
3. “Industrial Land Use”, pp. 69 and 70, Urban Design Guidelines: amended to mandate the design standards of The Plan.
4. “Institutional Land Use”, page 71, Policy: amended to add the following statement: *Institutional facilities located within The Plan area shall conform to the design standards of The Plan.*
5. “Institutional Land Use”, page 77, Urban Design Guidelines: amended to mandate the design standards of The Plan.

6. “Residential Land Use” , pp. 83 and 84, Urban Design Guidelines: amended to mandate the design standards of The Plan.
7. “Circulation”, page 101, Specific Recommendation “C”: “design guidelines” shall mandate the design standards of The Plan for architectural components of transit improvements.
8. “Community Facilities and Services”, pp. 113 and 114, Policy: amend to mandate the design standards of The Plan.
9. “Relationship to General Plan Goals”, page 137, Urban Design: amend to mandate the design standards of The Plan.