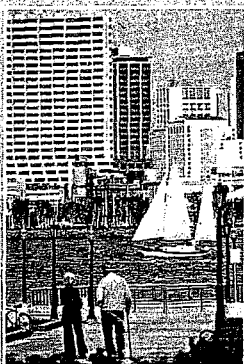


M A R I N A



U R B A N D E S I G N P L A N D E V E L O P M E N T G U I D E L I N E S P L A N N E D D I S T R I C T O R D I N A N C E

ROSTER OF OFFICIALS

City Council and Redevelopment Agency of the City of San Diego

Maureen O'Connor
Mayor

Abbe Wolfsheimer
Ron Roberts
Gloria McColl
Wes Pratt
Ed Struiksma
Bruce Henderson
Judy McCarty
Bob Filner

Other Officials
John Lockwood
City Manager/Executive
Director

John W. Witt
City Attorney

Centre City Development Corporation

Board of Directors
John Davies
President

Gil Ontai
Philip Blair
Janay Kruger
Thomas Carter
Patrick Kruer
Henri Lagatella

Pamela M. Hamilton
Executive Vice President

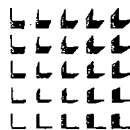
Planning Commission

Karl Zobell
Chairman

Ralph Pesqueira
Vice Chairman

Lynn Benn
Chris Calkins
Albert Kercheval
Yvonne Larson
Paula Oquita

Robert Spauling
Planning Director



Centre City
Development
Corporation



M A R I N A

U R B A N D E S I G N P L A N
D E V E L O P M E N T G U I D E L I N E S

Adopted By Redevelopment Agency Resolution R 1626

July 20, 1988

Table of Contents

MARINA URBAN DESIGN PLAN

Introduction 1

The Challenge

The Purpose

Key Background Findings

The Context

The Setting

URBAN DESIGN VISION

1 Create Discrete Neighborhoods 9

A. Existing and Adjacent Uses

B. Arterial Roadways Hierarchy

C. Neighborhood Parks

2 Enhance the Pedestrian Environment 11

A. Major Pedestrian Linkages

B. Streetscape and Landscape Plan

C. Building Mass Standards

3 Retain the Marina Area's Heritage and Character 12

A. Conserve Heritage Buildings

B. Preserve Street Grid

C. Small and Large Scale Development

PLAN ELEMENTS

1 Land Use Plan 14

A. Residential Uses

B. Commercial Uses

C. Small Development

2 Development Regulations 21

A. FAR

B. Height

3	Open Space	22
	A. Open Space Plan	
	B. View Corridors	
4	Circulation	26
	A. Vehicular Circulation	
	B. Transit Circulation	
	C. Pedestrian Circulation	
	D. Other Considerations	
5	Streetscape/Landscape Plan	29
	A. Boulevard Streets	
	1. Harbor Drive Corridor	
	2. Market Street	
	3. Pacific Highway	
	4. Front and First	
	5. Other Streets	
	B. Other Spaces	
6	Heritage Conservation Plan	44
	A. Historic Sites and Buildings	
	B. Eligible For National Register	
	C. Chinese/Asian thematic Historic District	
	D. Architecturally Significant Buildings	

DEVELOPMENT GUIDELINES

	Introduction	49
--	---------------------	-----------

STREET FACADES

1	Objectives	50
2	Design Guidelines	50
	A. Build to Property Lines	
	B. Reinforce Street Corners	
	C. Street Level Design, Retail Frontages	
	D. Street Level Design, Residential Frontage	
	E. Street Level Parking	
	F. Parking above Street Level	

- G. Parking without Active Uses Above
- H. Vertical Articulation of Corners and Entrances
- I. Vertical Articulation and Facade Modulation
- J. Horizontal Facade Treatment
- K. Unscreened Private Outdoor Space
- L. Roofs
- M. Prohibited Items

SOLAR ACCESS

- 1** Solar Access Objectives 60
- 2** Solar Access Requirements 60
 - A. Public Parks and Open Spaces
 - B. Opposing Sidewalks
 - C. Publicly Accessible Spaces
- 3** Wind Tunnel Avoidance Objectives 60
- 4** Wind Tunnel Avoidance Requirements 60

BUILDING MATERIALS AND COLORS

- 1** Objectives 61
- 2** Requirements 61
 - A. Exterior Building Walls
 - B. Building Roofs
 - C. Window Glazing
 - D. Architectural Details and Decorative Features
 - E. Trim
 - F. Visual Continuity
 - G. Sense of Permanence

SECURITY

- 1** Objectives 64
- 2** Requirements 64
 - A. Visible Entrances
 - B. Visible Recesses

C. Transparent Corners

D. Columns

E. Landscaping

F. Lighting

G. Shadows and Trees

SIGNAGE

1 Objectives

67

2 Requirements

67

A. Building Identification Signs

B. Highrise Logos

C. Business Identification Signs

D. Parking Signs

E. Animated Signs & Unique Design

F. Prohibited Signs

HERITAGE **S**TRUCTURES

1 Objectives

70

2 Requirements

70

A. New Adjacent Development

B. End Walls

C. Preservation and Renovation

D. Maintenance

E. Additional Guidelines For The Chinese/Asian Thematic District

List of Figures

URBAN DESIGN PLAN

1. Study Area in Downtown San Diego	4
2. Discrete Neighborhoods	9
3. Arterial Roadways Hierarchy	10
4. Neighborhood Parks	10
5. Pedestrian Linkages	11
6. Streetscape and Landscape Plan	11
7. Building Mass Standards	12
8. Heritage Buildings	12
9. Street Grid	13
10. Small and Large Scale Development	13
11. Land Use	16
12. Preferred Locations for Commercial Uses	18
13. Small Development Sites	19
14. Floor Area Ratios (FAR)	20
15. Height Limit	23
16. Open Space Plan	25
17. Vehicular Circulation	27
18. Transit Circulation	30
19. Pedestrian Circulation	31
20. Sidewalk Paving Programs	33
21. Street Tree Program	34
22. Mid-block Street Lighting Program	35
23. Market Street Landscape Cross Section	37
24. Market Street Cross Section West of Kettner	37
25. Pacific Highway Cross Section	39
26. Front and First Cross Section	40
27. Woonerfs	41
28. Market Street and Harbor Drive	43
29. Front/First and Harbor Drive	43
30. Heritage Buildings	47
31. Chinese/Asian Thematic Historic District	48

List of Figures

DEVELOPMENT GUIDELINES

32. Property Line	51
33. Street Corners	51
34. Storefront Design	52
35. Openings to Street	52
36. Awnings and Canopies	53
37. Street Level Terraces	53
38. Street Level Parking	54
39. Parking above Street Level	54
40. Vertical Articulation	55
41. High-Rise Articulation & Modulation	56
42. Cornices and Eaves	57
43. Screening of Balconies	58
44. Roof Lines	58
45. Screening of Mechanical Equipment	59
46. Treatment of Flat Roofs	61
47. Sense of Permanence	63
48. Visible Entrances	64
49. Visible Recesses	65
50. Transparent Corners	65
51. Column Size	66
52. Landscaping for Security	66
53. Respect Historic Structures	70
54. Recall Architectural Elements	71

M A R I N A

U R B A N D E S I G N P L A N

INTRODUCTION

The Challenge

The goal of the Marina Urban Design Plan is:

The revitalization of the Marina area into a downtown residential community recognized for superior design of the built environment.

The Purpose

The purpose of the Marina Urban Design Plan is:

- ° to describe and illustrate the urban design vision which guides the comprehensive development of the Marina area, and
- ° to establish urban design guidelines to be used as benchmarks to evaluate specific development proposals.

Key Background Findings

The real estate market analysis of Keyser Marston Associates, June, 1986, confirmed that there is a significant demand for downtown residential units provided that sales prices and rental rates are within an affordable range. Residential densities of 75 to 100 dwelling units per acre are achievable. Financial analyses indicate greater financial feasibility for low-rise construction. Buildings higher than 50 feet were financially feasible only with excessive sales prices or rental rates or, alternatively, a greater mixture of non-residential uses. Surveys of comparable projects indicate that a ratio of one parking space per bedroom may be necessary for market acceptance.

Based on the projected residential population in the Marina area, the pedestrian activity from adjacent uses and other factors, market consultants have recommended that 75,000 to 125,000 square feet of neighborhood-serving convenience and eating and drinking establishments be planned.

An opinion survey of downtown residents confirmed many of the findings of an earlier questionnaire and provided the following:

- ° More than 50% of the sample lived in downtown for less than one year, and 89% lived in the San Diego region for less than three years.
- ° Nearly 40% of downtown residents were former homeowners.
- ° Low-rise buildings were highly preferred by the majority of respondents.
- ° The most influential factors in the decision to move downtown were the desire for a unique urban atmosphere and proximity to the waterfront. Security was a major concern of downtown residents.
- ° Amenities considered important; i.e., worth paying more for, included air conditioning, fireplaces and private open space.
- ° The majority of respondents had two or more cars, hence a second parking space was considered very important.

A companion survey of non-residents revealed that one-third of the sample would consider living downtown. Characteristics of those considering moving downtown closely mirrored the profile of the existing residents.

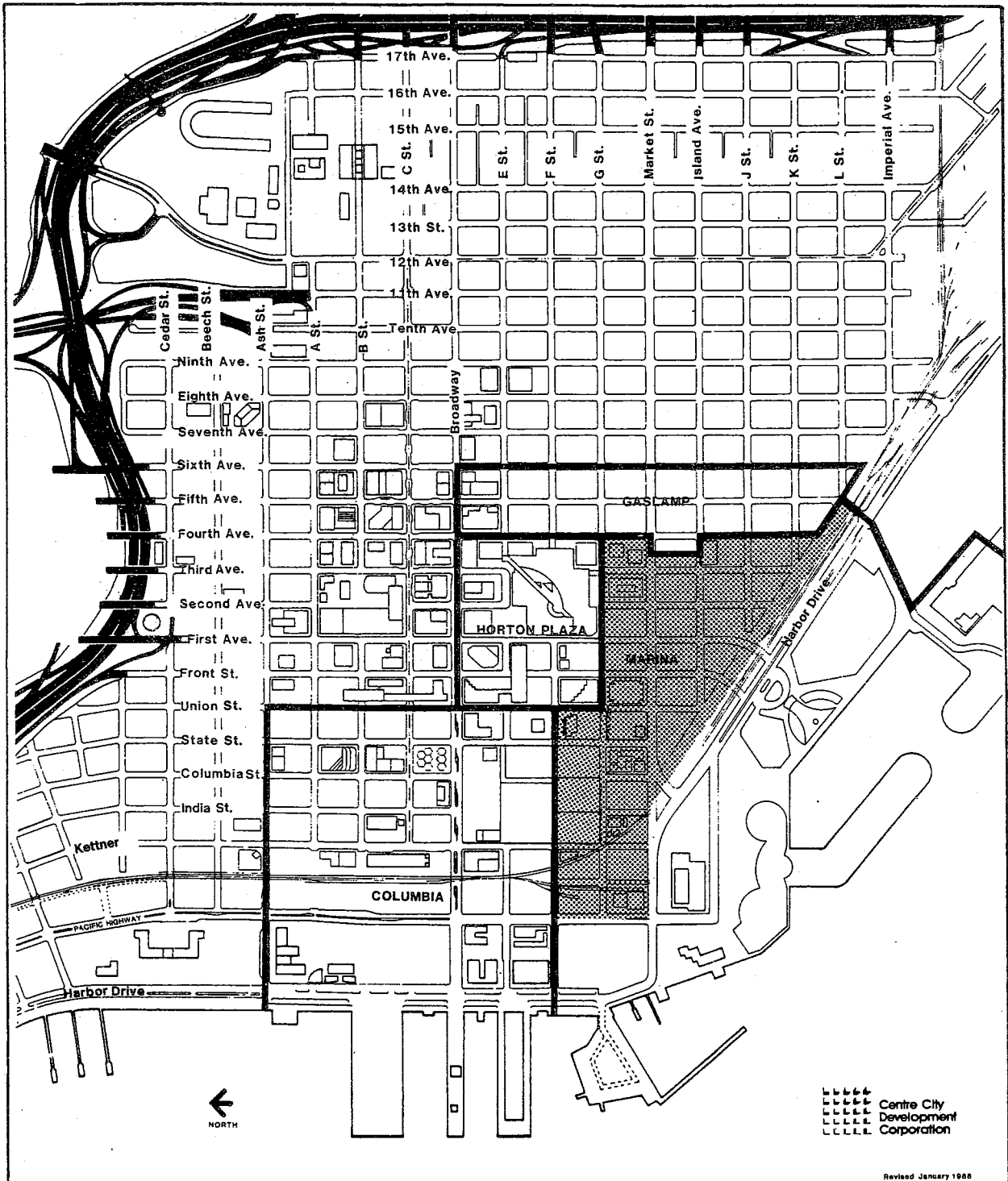
The Context

The Marina area, as part of Centre City San Diego, lies at the heart of a metropolitan area with a population of about 1.9 million. Between 1970 and 1980, the metropolitan area grew by 40%, with a high level of growth continuing today. The 1986 mid-year population of the city itself was 1,013,000.

Within this metropolitan context, however, few opportunities comparable to those in the Marina area exist for the development of an urban residential neighborhood. The Marina area benefits not only from its adjacency to the city's commercial core and its daytime population of nearly 100,000, but also from nearby amenities such as San Diego Bay, Balboa Park, San Diego's famed zoo, Coronado Island, Mission Bay, Seaport Village, and the historic Gaslamp Quarter. The Marina area in the context of downtown San Diego is shown in Figure 1.

Access to the area is via major surface routes from Interstate 5, Route 163, Highway 94, including the First/Front couplet, Broadway, Market Street, Harbor Drive and Pacific Highway. San Diego International Airport is within 2-1/2 miles.

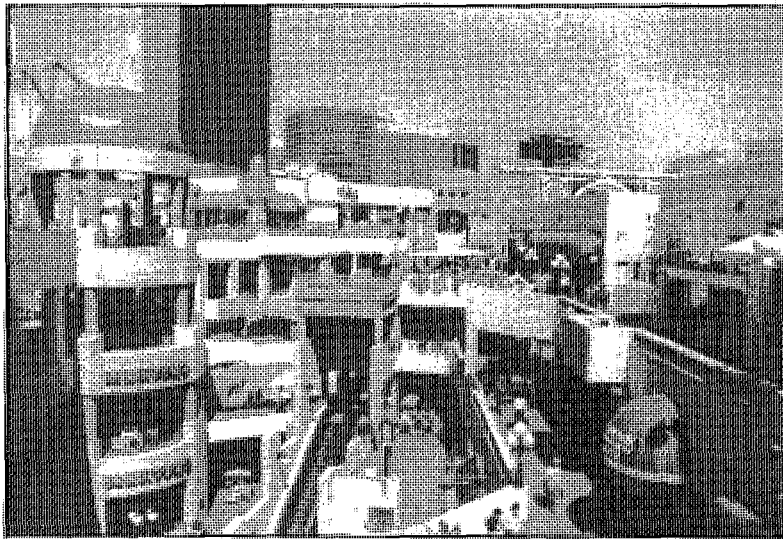
Figure 1
Study Area in Downtown San Diego



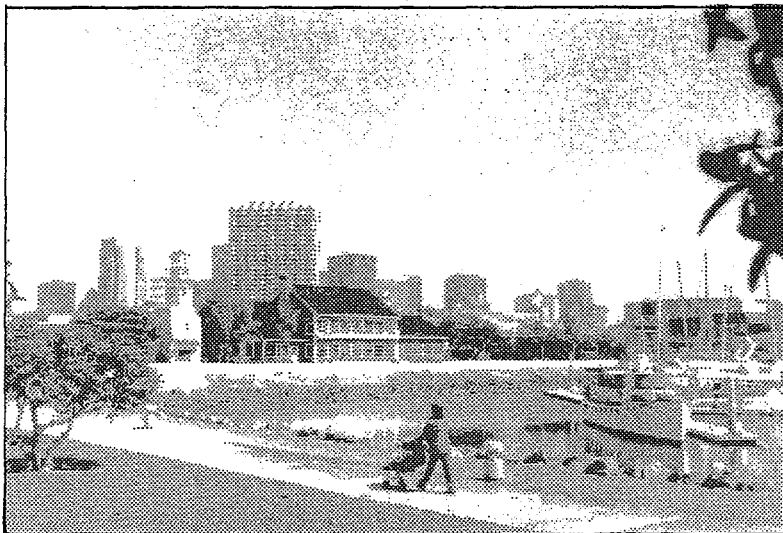
The Setting

The Marina area is surrounded by numerous assets which will facilitate the effort to redevelop it into a downtown residential community. Some of the major physical and cultural assets that are incorporated in the Marina Urban Design Plan are described below.

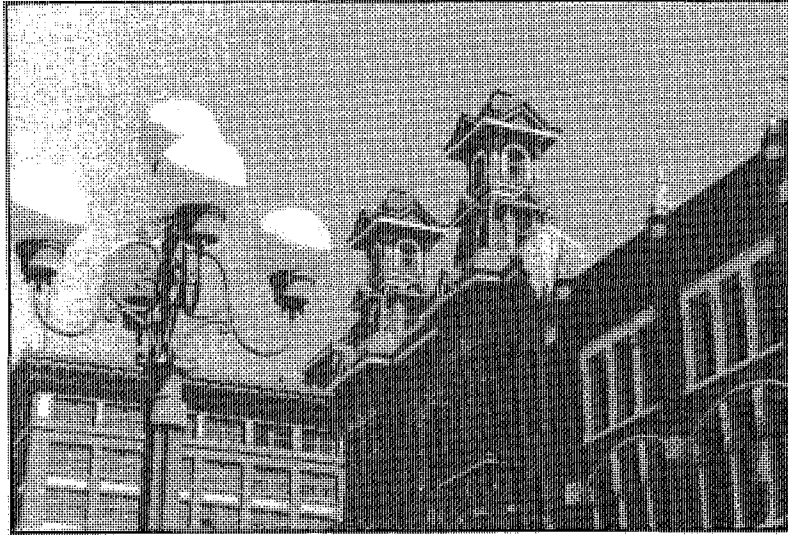
The Marina area is directly adjacent to the downtown core of San Diego, a relationship that provides the potential resident with walking proximity to employment, shopping, civic, entertainment and cultural opportunities.



Similarly, the Marina resident will enjoy a walking-distance proximity to San Diego's famed waterfront with its marinas, parks, specialty shopping and the convention center.



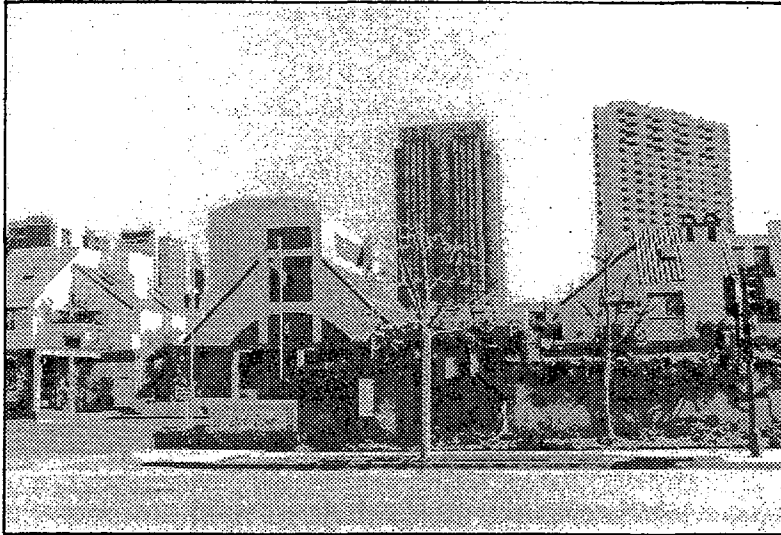
Because the Marina area shares a third edge with the historic Gaslamp Quarter, it provides the future resident with entertainment, shopping and workplace activities.



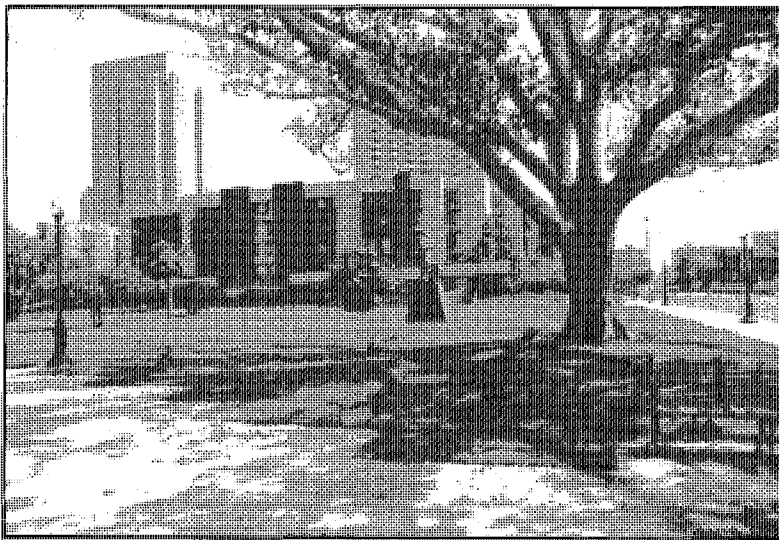
This architectural heritage extends into the Marina area with its several buildings of historic and artistic appeal. The three themes of this heritage -- 19th Century Mercantile, Spanish-Colonial and Utilitarian Warehousing -- provide the design base for future development.



The residential pioneers who have moved into recent housing developments provide evidence that the Marina area is a viable urban residential neighborhood. The quality of urban living to which they are committed must be fostered and strengthened by future development.



Pantoja Park, as the area's major open space and Columbia Pathway provide the basis upon which an open space system can be structured to benefit existing and future residents.



Finally, the area's accessibility is a key asset. Major arterials provide direct access to the region's freeway system. Existing and planned bus and trolley routes permit alternatives to the automobile. And the nearness of major activities makes walking and biking viable choices for many trips.



URBAN DESIGN VISION

The Urban Design Plan for the Marina redevelopment area of San Diego is the vision of a residential community complemented by new parks, superior architecture and attractive pedestrian access to the waterfront and downtown. The following objectives were used to develop the major elements of the plan.

- ° Create discrete neighborhoods
- ° Enhance the pedestrian environment
- ° Retain the Marina district's heritage and character.

1. Create Discrete Neighborhoods

- A. Build on the themes and character of existing and adjacent uses to create distinct neighborhoods. Pantoja Park and the Horton Grand neighborhoods are envisioned to be primarily residential. Horton Plaza is the commercial focus of the community.

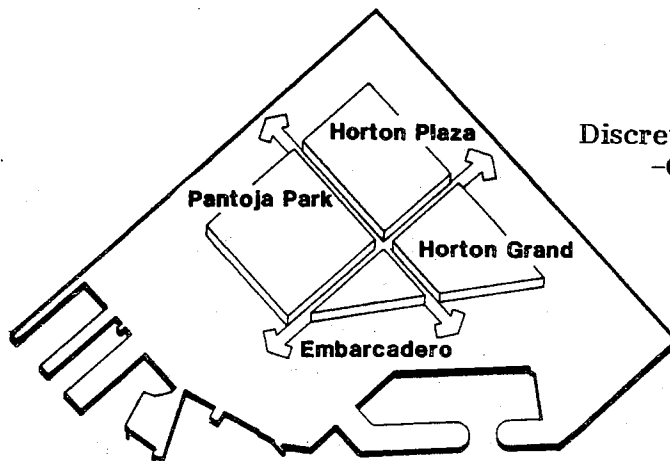


Figure 2
Discrete Neighborhoods
-Conceptual-

- B. Utilize the planned arterial roadways of Broadway, Pacific Highway, Harbor Drive, Market Street, the First/Front couplet and Fourth Avenue which surround and bisect the Marina as a framework for identifying these discrete neighborhoods. Minimize the incursion of vehicular traffic into neighborhoods by establishing a clear street hierarchy, use of attractive streetscape design, and visually concealed off-street parking.

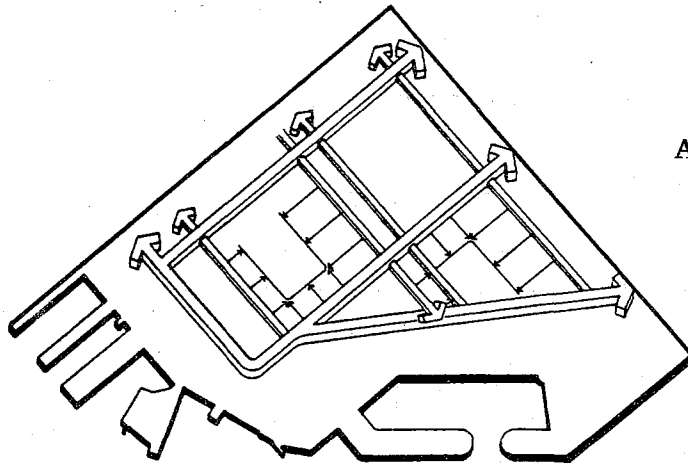


Figure 3
Arterial Roadway
-Conceptual-

- C. Organize the neighborhoods around parks. Horton Plaza, Pantoja and Embarcadero are existing parks; a proposed linear park paralleling the railroad right of way on the southerly edge of the Marina area will provide neighborhood parks for the area south of Market Street.

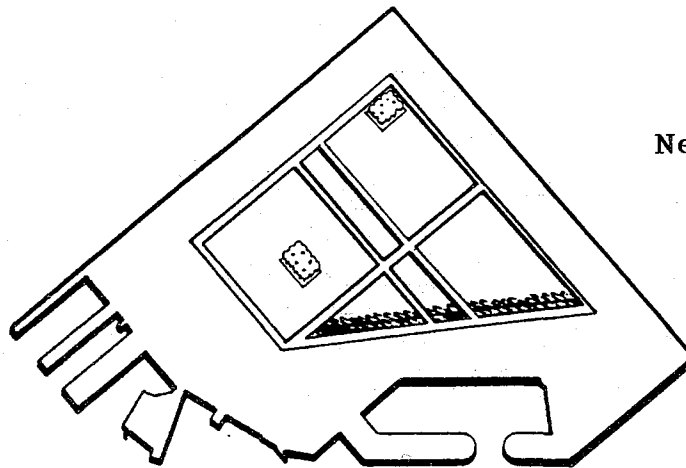


Figure 4
Neighborhood Parks
-Conceptual-

2. Enhance the Pedestrian Environment

- A. Establish and develop major pedestrian linkages from the Marina area to the waterfront, the Gaslamp Quarter and the downtown core.

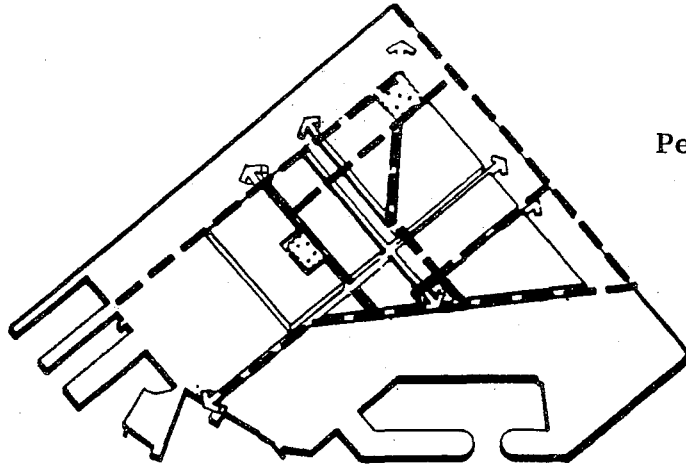


Figure 5
Pedestrian Linkages
-Conceptual-

- B. Develop a streetscape and landscape plan to activate pedestrian streets and create a linear park from excess railroad right of way. Emphasize retail uses on arterial streets to bind neighborhoods together, enliven the pedestrian environment and buffer traffic impacts.

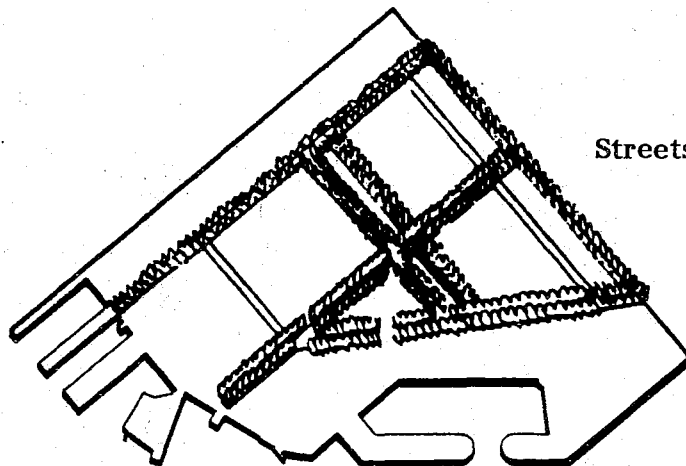


Figure 6
Streetscape/Landscape Plan
-Conceptual-

- C. Prescribe building mass standards that conserve solar access, mitigate wind and respect the scale of adjoining development.

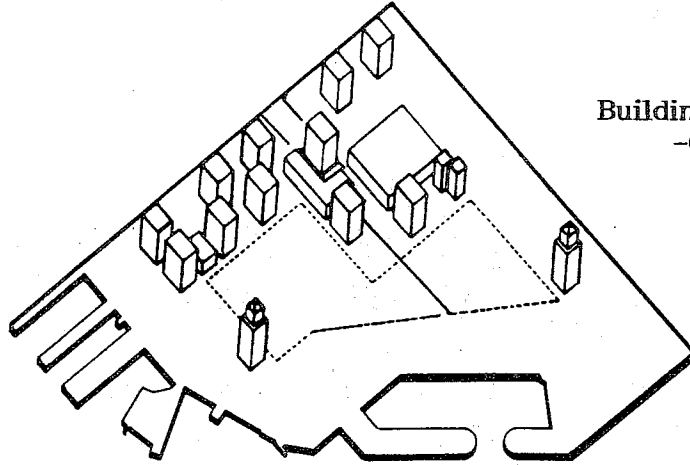


Figure 7
Building Mass Standards
-Conceptual-

3. Retain the Area's Heritage and Character

- A. Conserve heritage buildings where possible. Utilize the architectural vocabulary of heritage buildings as cues for new development.

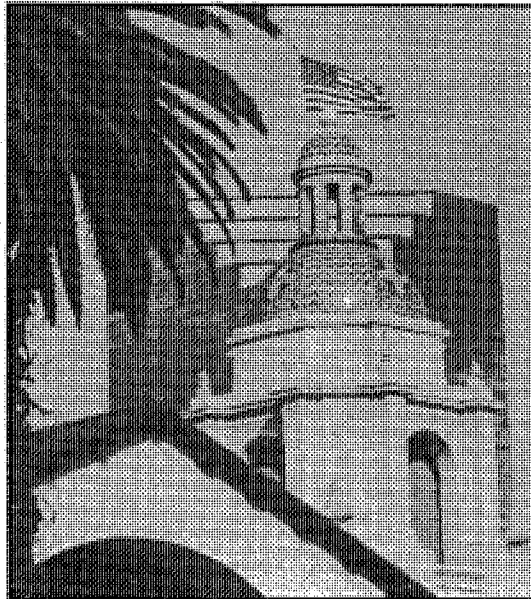


Figure 8
Heritage Buildings
-Conceptual-

- B. Preserve the historic character of the existing street grid by visually extending it where it does not exist and discouraging consolidation of blocks.

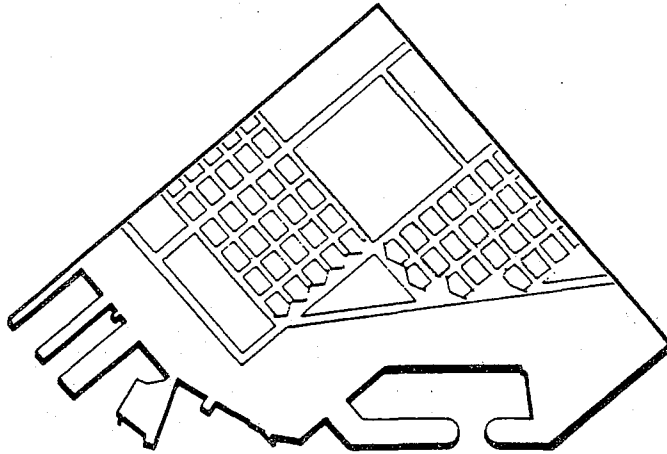


Figure 9
Street Grid
-Conceptual-

- C. Provide opportunities for small scale development. Ensure that all new development reflects a small scale residential character.

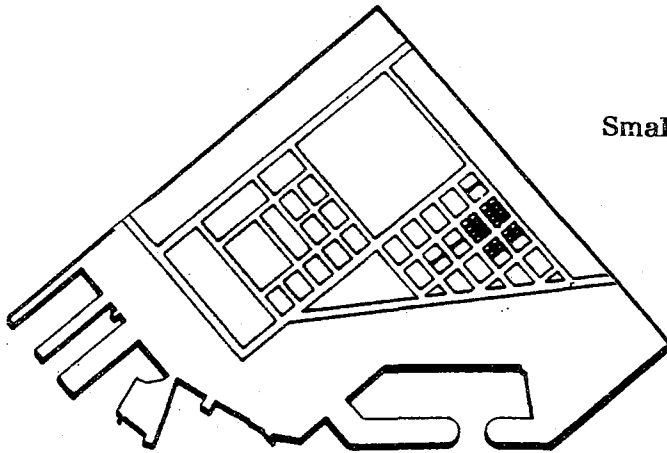


Figure 10
Small and Large Development
-Conceptual-

PLAN ELEMENTS

This section identifies six major determinants (elements) of development for the Marina area. These are: land use, development regulation, open space, circulation, streetscape and heritage conservation. The major recommendations of the plan are summarized below:

- LAND USE

The designation of residential as the predominant land use, permitting mixed-use development in which residential and non-residential uses share the same site and convention hotels are located on the periphery of the Marina area or between the Front and First Street corridor.

- DEVELOPMENT REGULATIONS

Establish standards expressed in FAR and building heights which complement the development of residential units as the predominant land use of the Marina area.

- OPEN SPACE

Proposals for new parks and open spaces that provide a focus for residential neighborhoods or facilitate access between neighborhoods, the downtown core, Gaslamp Quarter and the Embarcadero.

- CIRCULATION

A circulation system that emphasizes non-motorized movement yet also facilitates vehicular and transit accessibility.

- STREETSCAPE/LANDSCAPE

A streetscape plan that accentuates the formal, ceremonial qualities of such major streets as Harbor Drive and Market Street while giving local streets an informal, more human-scale character compatible with their residential nature.

- HERITAGE CONSERVATION

A heritage conservation plan that recommends preservation or restoration of historic and architecturally significant resources.

1. Land Use Plan

Objectives

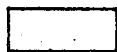
- (1) Emphasize the development of residential neighborhoods.

- (2) Provide for other land uses that are complementary to a residential neighborhood.
- (3) Encourage small scale residential development.
- (4) Provide sites for hotels to support the convention center.
- (5) Provide variety in the scale of the development of each block to avoid a monolithic appearance.
- (6) Encourage visual interest through architectural diversity.
- (7) Provide opportunities for adaptive reuse of significant structures.

Recommendations

A. Residential Uses

The Land Use Plan, Figure 11, illustrates the recommendations to guide the development of land use in the Marina area.

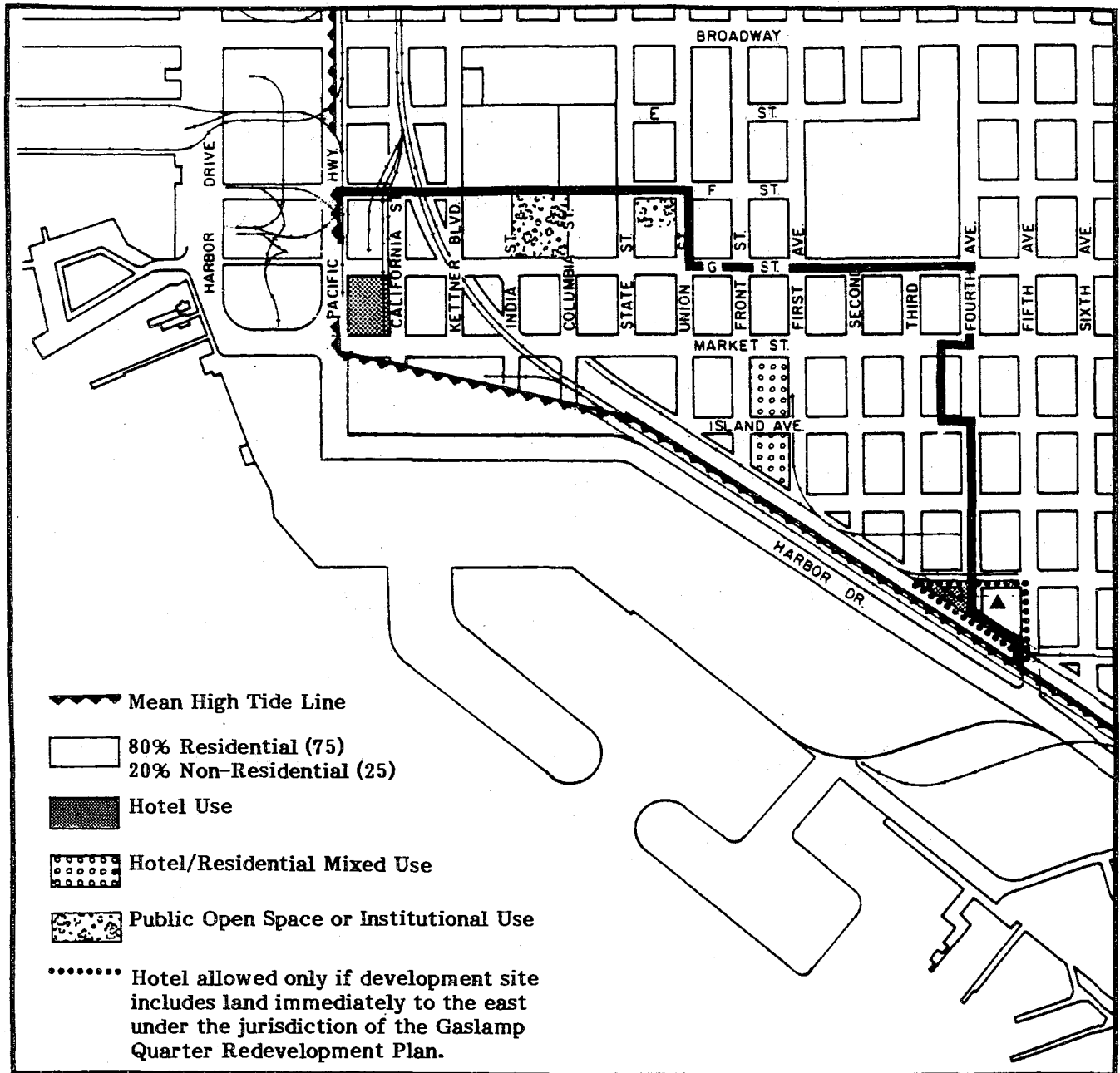


Designate the majority of the area for predominantly residential development in which at least 80% of the gross floor area is residential in use and the remainder may be commercial.



Designate certain blocks or combinations of blocks for the development of convention-related hotels.

Figure 11
Land Use



B. Commercial Uses

Commercial uses are recommended at street level frontage along those streets in which the highest pedestrian activity levels can be expected at the street level. These include "G" Street opposite Horton Plaza, Market Street, First Avenue and Front Street. In the event that commercial use is not economically viable, residential use should be developed.

The preferred locations for commercial facilities are illustrated in Figure 12.

C. Small Development

Small development will be encouraged throughout the entire project area subject to the following criteria:

- ° The maximum lot size for redevelopment should be 20,000 square feet.
- ° Where the Redevelopment Agency participates in the assembly of a site, every effort should be made to:
 - ° Subdivide sites into development parcels of approximately 20,000 square feet or less.
 - ° Encourage the image of small-scale by modulating the facades of development and the appearance of individual structures.
 - ° Encourage the development of individual ownerships, particularly those 20,000 square feet or less within each block.

Figure 12
Preferred Locations for
Commercial Uses

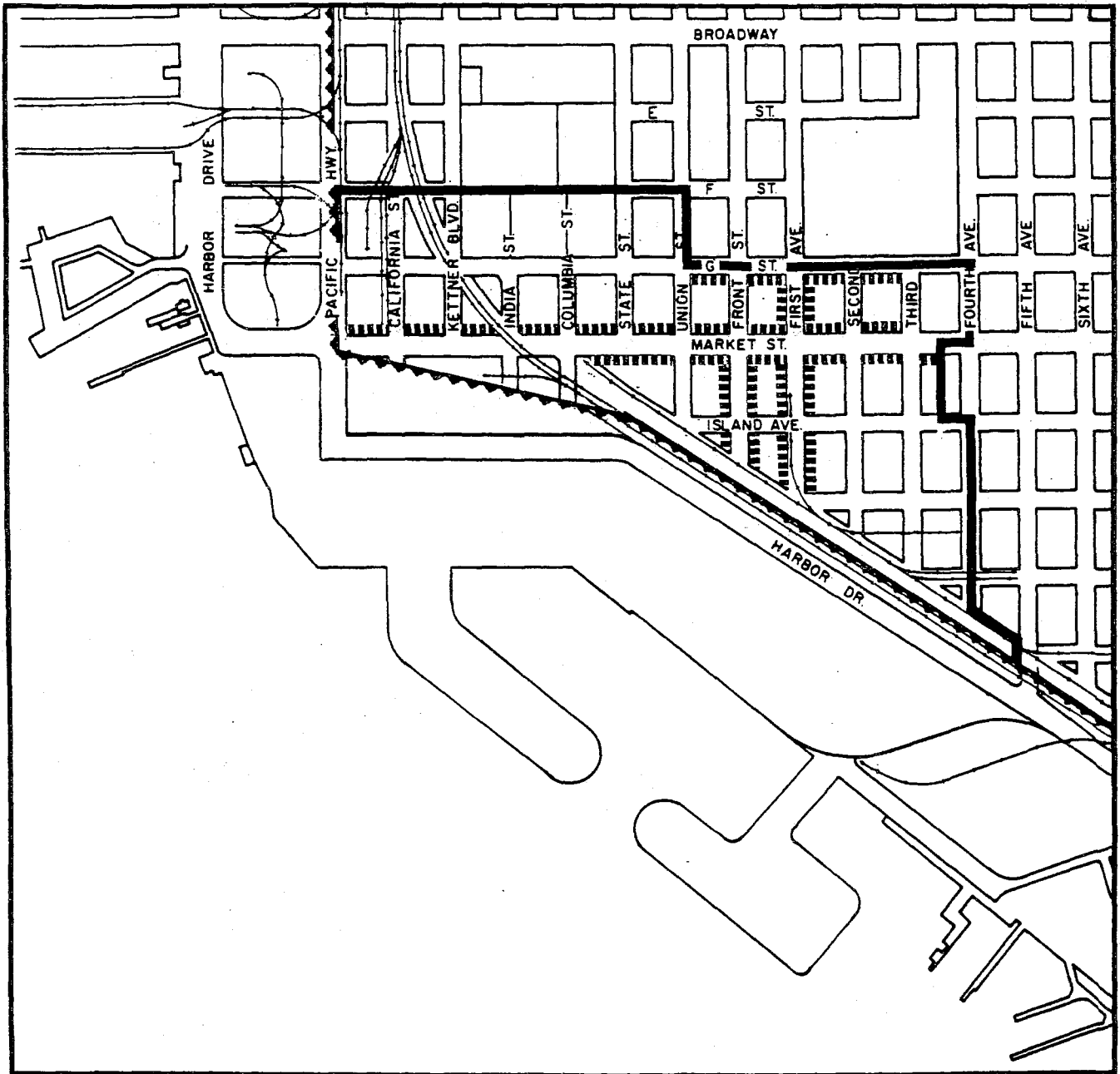


Figure 13
Small Development Sites

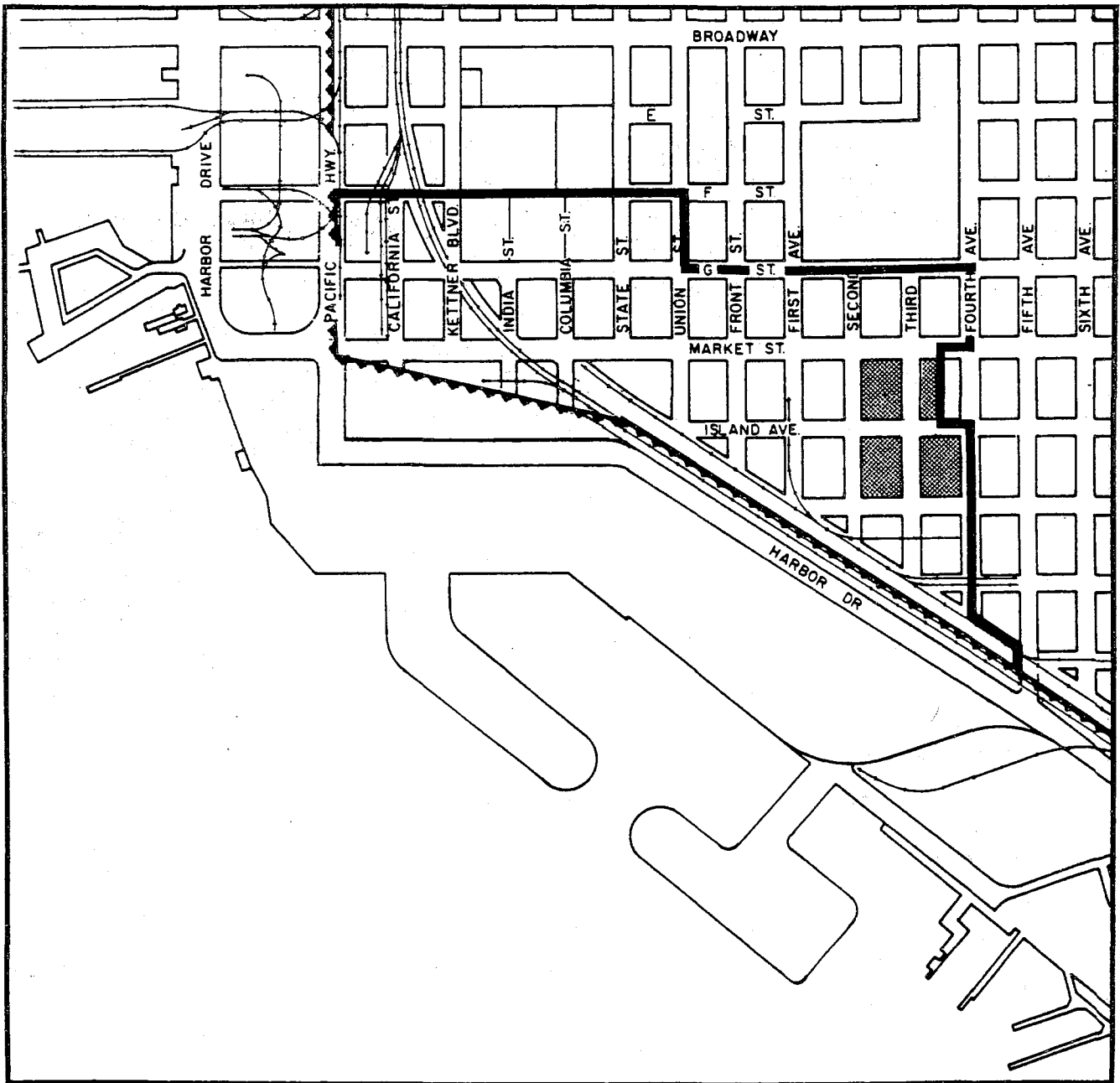
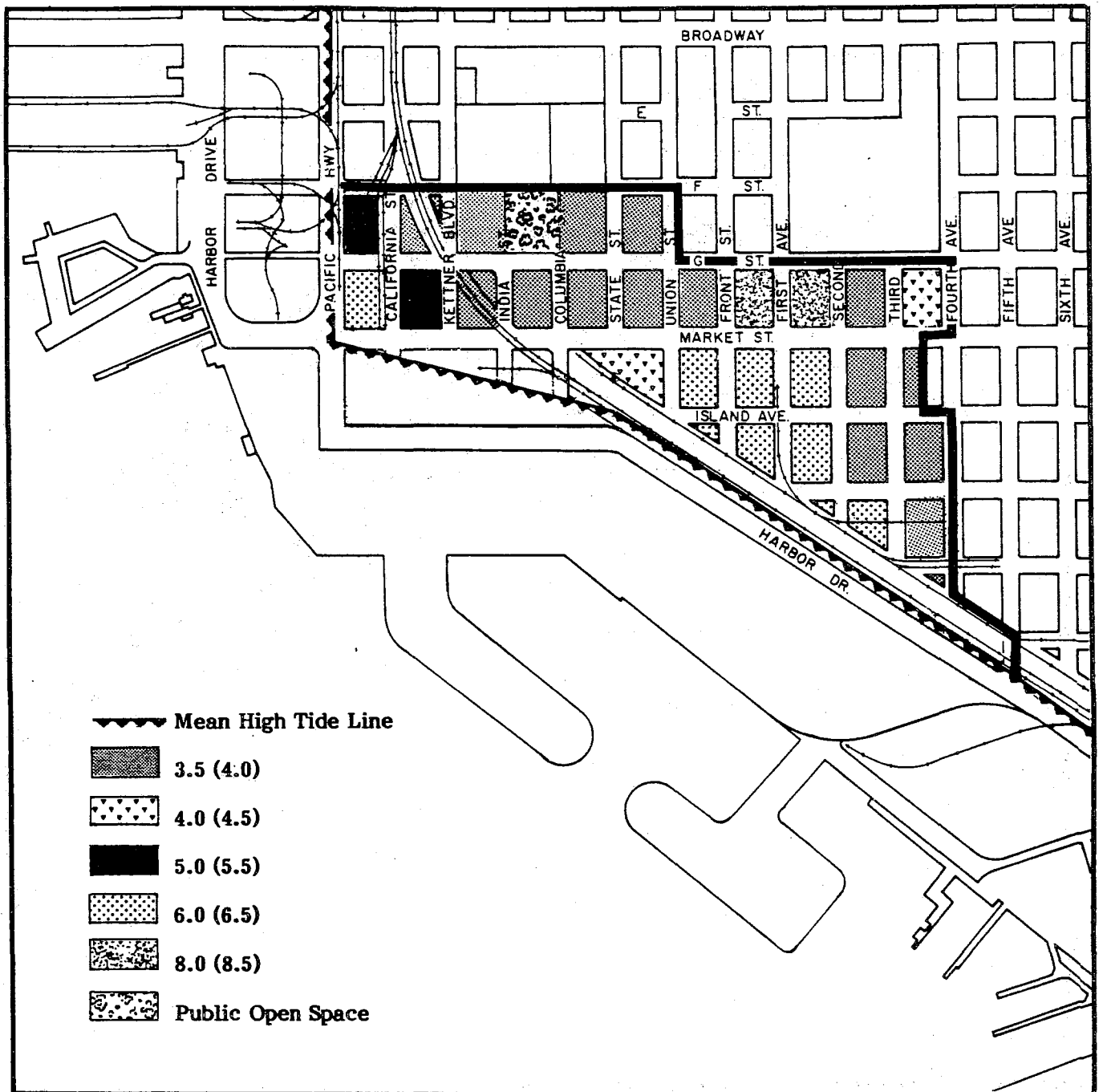


Figure 14
Floor Area Ratios
(FARS)



2. Development Regulations

Objectives

- ° Establish height and bulk standards which will guide the intensity of all development.
- ° Encourage a development scale which complements a residential neighborhood.
- ° Provide the opportunity to establish cohesive residential areas.
- ° Relate building heights and construction costs to the economic market.

Recommendations

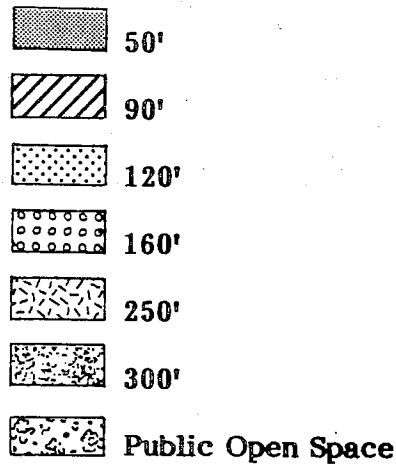
A. Floor Area Ratios (FARs)

Floor area ratios to guide the mass and scale of development are illustrated on Figure 14.

- 3.5 to 4 This FAR maintains the scale of the Park Row and Marina Park Residential Development adjoining Pantoja Park and will accommodate Type V construction.
- 4.0-4.5 This FAR permits increased development which slightly exceeds the intensity of development of Park Row and Marina Park.
- 5.0-5.5 This FAR corresponds to the two blocks which adjoin the Embassy Suites Hotel between Pacific Highway and Kettner Boulevard.
This FAR provides a transition between the residential neighborhood and waterfront land uses.
- 6.0-6.5 This increased FAR permits vertical development connecting the convention center/hotel complex to Horton Plaza and at Embassy Suites Hotel on Pacific Highway.
- 8-8.5 The most intense development is located immediately south of Horton Plaza fronting on First Street.

B. Height

Height limitations to guide vertical development are illustrated in Figure 15.



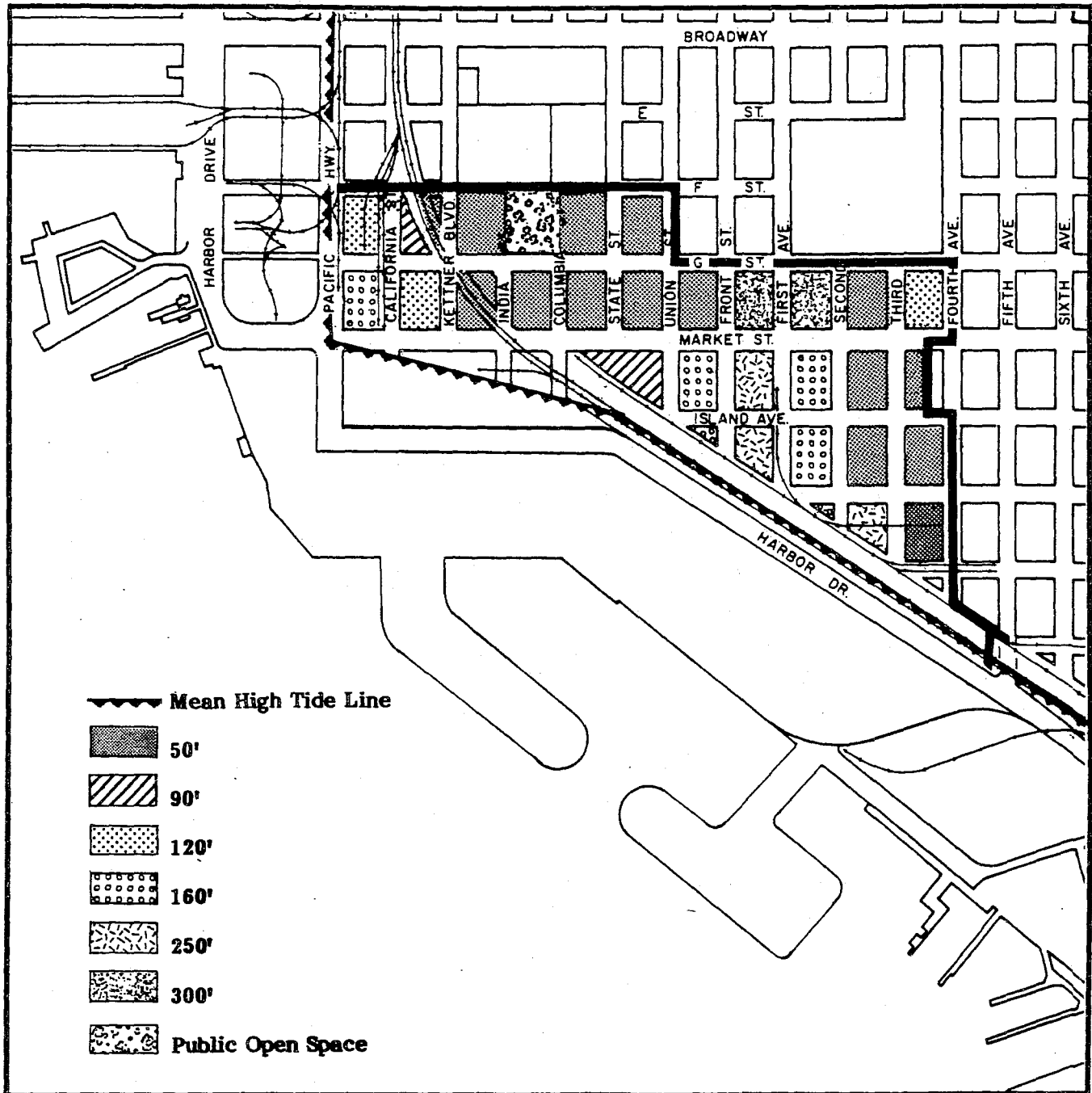
All structures which exceed 30 feet governed by the City of San Diego Airport Approach Overlay Zone.

3. Open Space

Objectives

- ° Maintain and enhance the existing public/private open space system.
- ° Complement the existing system with the designation of additional public/private open space systems.
- ° Utilize the railroad rights of way and Harbor Drive to establish an enhanced transportation corridor with park-like characteristics.
- ° Utilize and enhance existing streets/rights-of-way as park-like linkages between existing and proposed open spaces.
- ° Preserve street level view corridors from the core area to the waterfront.

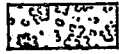
Figure 15
Height Limit



Recommendations

A. Open Space Plan

The open space plan (Figure 16) illustrates recommendations for the establishment of an open space system for the Marina area.



Existing public/private open space system



Proposed public/private open space system.



Proposed right of way-linkage systems.

A linear park adjoining the Light Rail Transit transportation corridor is a key design feature of the proposed open space plan. In order to maximize the environmental potential of the transportation corridor, the entire right of way from the north edge of Harbor Drive to the south edge of the Marina housing area should be improved with paving, lighting, street furnishings and landscaping. The proposed improvement will provide an aesthetic edge for the residential community as well as recreational features such as a pedestrian/bicycle path. (See Figure 16).

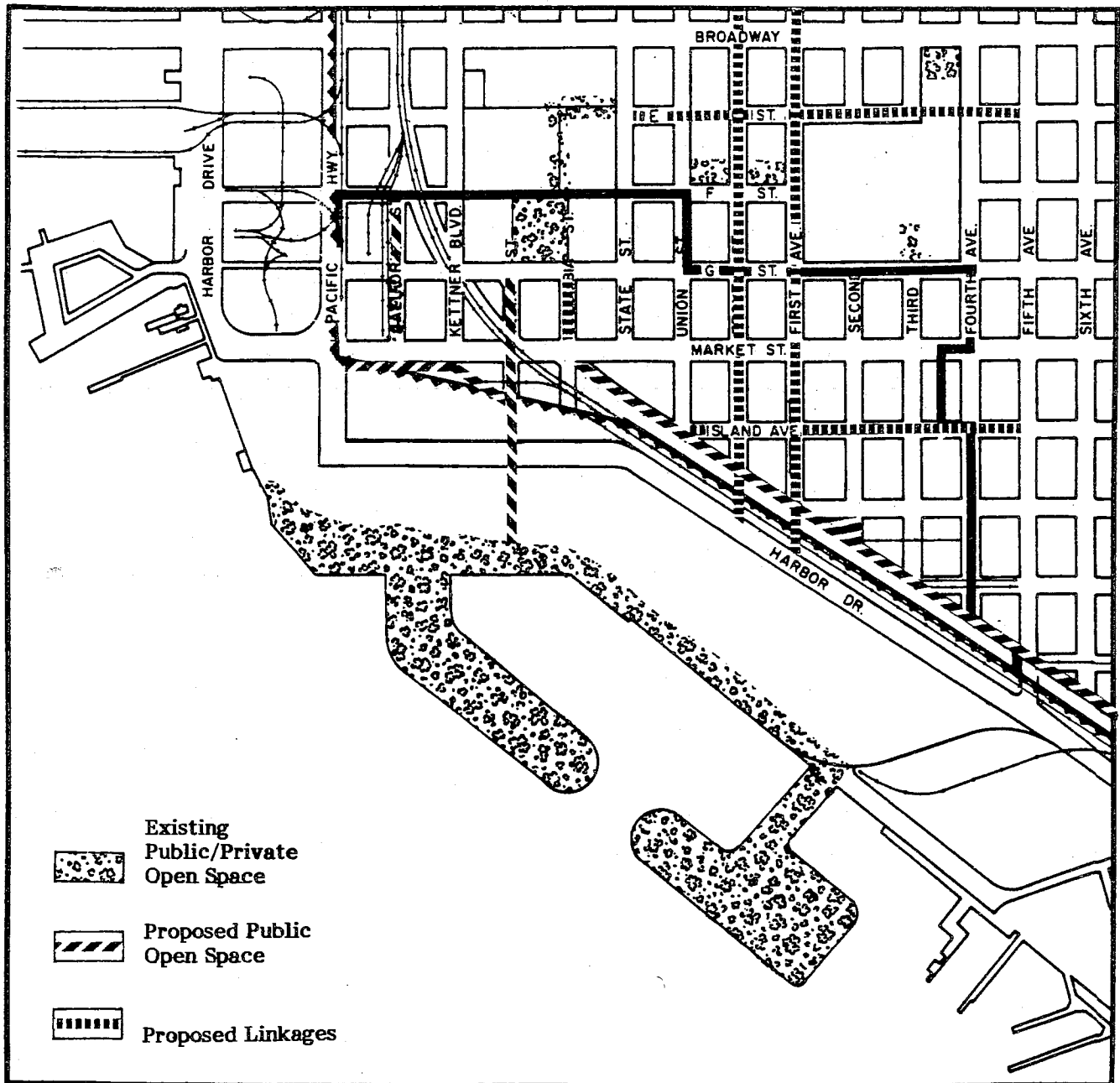
Private developers will be encouraged to provide small parks, landscaped setback areas, widened sidewalks and enhanced public rights-of-way.

Private improvements adjoining the park will be set back 50 feet from the park edge and landscaped to complement the design of the Linear Park.

B. View Corridors

In addition to conserving open space at the ground level, conservation of views from upper levels is desirable. In general, there are two basic considerations:

Figure 16
Open Space Plan



1. As individual projects are proposed, buildings can be sited to acknowledge views from the city to the waterfront. Implementation of this guideline will require the cooperation of each developer.
2. Establish a setback for the tower element of individual structures from the perimeter of each adjoining street. Implementation of this requirement will preserve street level street views.

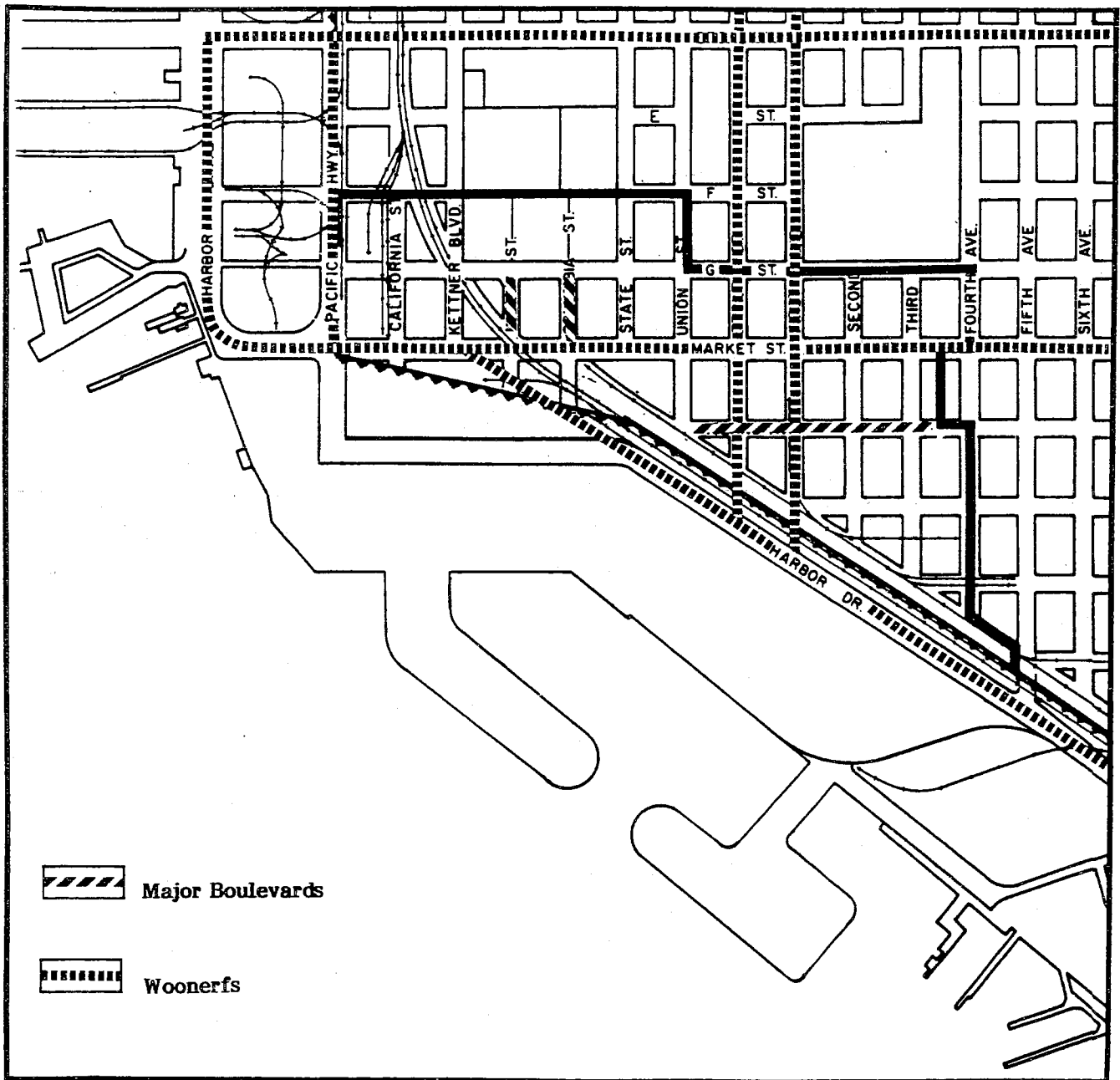
4. Circulation

The guidelines contained in this section, particularly in reference to travel lane dimensions, are the recommendations of the Centre City Development Corporation (CCDC). They do not reflect the current standard used by the City Traffic Engineer on City streets. CCDC has reduced the City Traffic Engineer's standard lane widths in order to provide a wider sidewalk area on the premise that the streets in Centre City are the area's major open spaces and therefore should be attractively furnished with adequate sidewalks and trees.

Objectives

- ° Retain the small-block grid that characterizes the historic subdivision of downtown San Diego.
- ° Encourage a multi-modal transportation system that includes vehicular, transit, pedestrian and non-motorized circulation.
- ° Establish safe, convenient and interesting grade-level pedestrian connections to the downtown and the waterfront.

Figure 17
Vehicular Circulation



- ° Accommodate through traffic with minimal adverse impacts on residential areas.
- ° Accommodate local traffic and service access as appropriate for a residential neighborhood.
- ° Accommodate the Bayside and Gaslamp trolley alignment.

Recommendations

A. Vehicular Circulation

The vehicular circulation is shown in Figure 17.



Designate Broadway, Market Street, Harbor Drive, Pacific Highway, Front and First Street as major boulevards (arterials) and enhance these streets as gateways to the Marina Area.



Designate selected streets as local residential streets or "Woonerfs" on which considerations for the pedestrians take precedence over those for the automobile.

B. Transit Circulation

Transit circulation for the Marina area is illustrated in Figure 18.



Designate the approximate alignment for the LRT trolley.



Designate the approximate alignment for the Gaslamp trolley.



Designate the approximate alignment for the bus routes.

C. Pedestrian Circulation

The pedestrian circulation plan is shown in Figure 19. Designate select streets as residential-pedestrian streets and encourage retail or other pedestrian-oriented activity on these street frontages.

D. Other Considerations

Coordinate the open space plan with the traffic circulation plan, particularly the linear park, the LRT station, locations, and the designation of woonerf/pedestrian streets.

5. Streetscape/Landscape Plan

Objectives

- ° Utilize the streetscape plan and street improvements as organizing elements for implementing the Marina Urban Design Plan and provide a special identity for the Marina area.
- ° Utilize landscape themes to reinforce neighborhood identity.

Figure 18
Transit Circulation

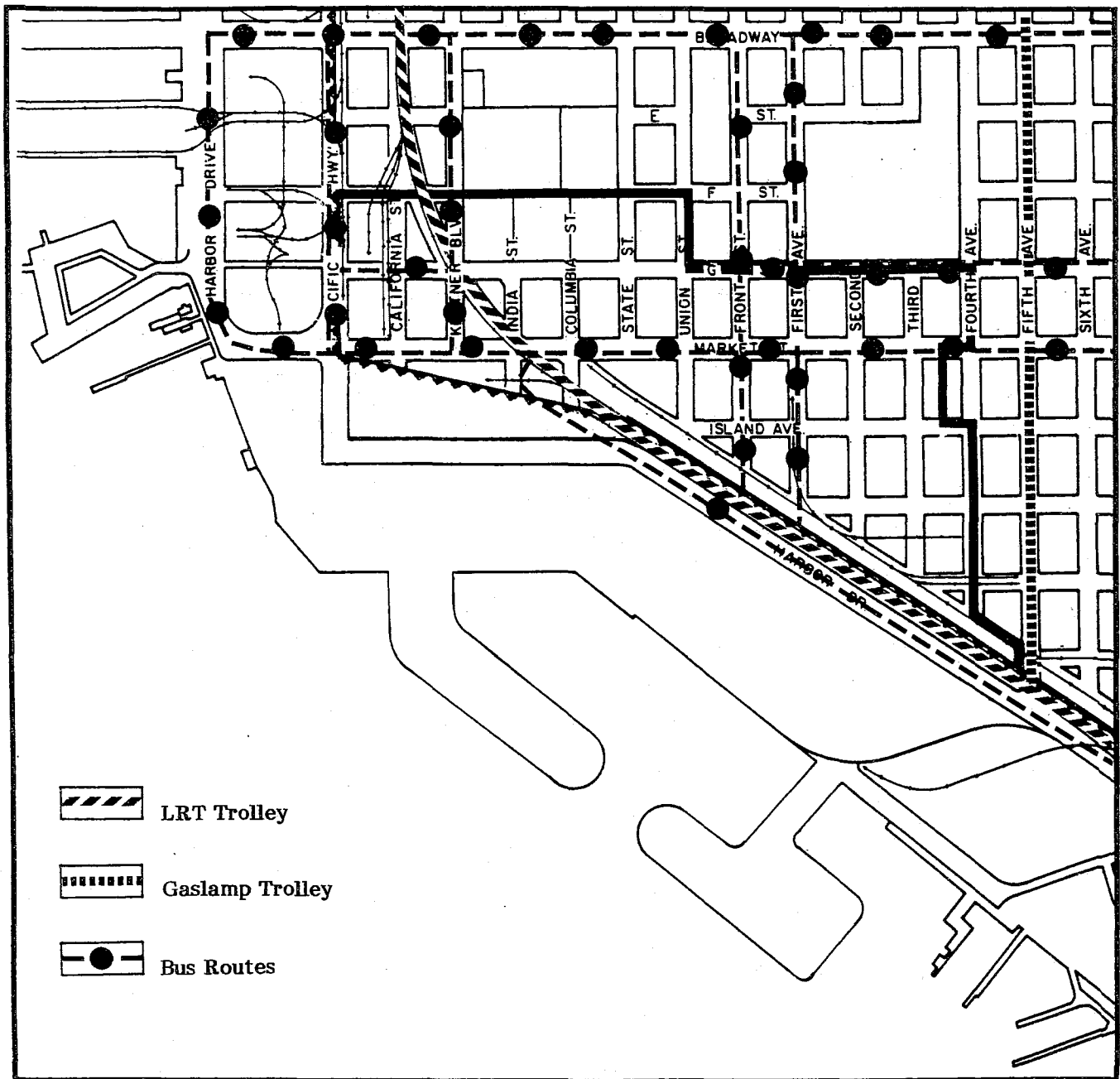
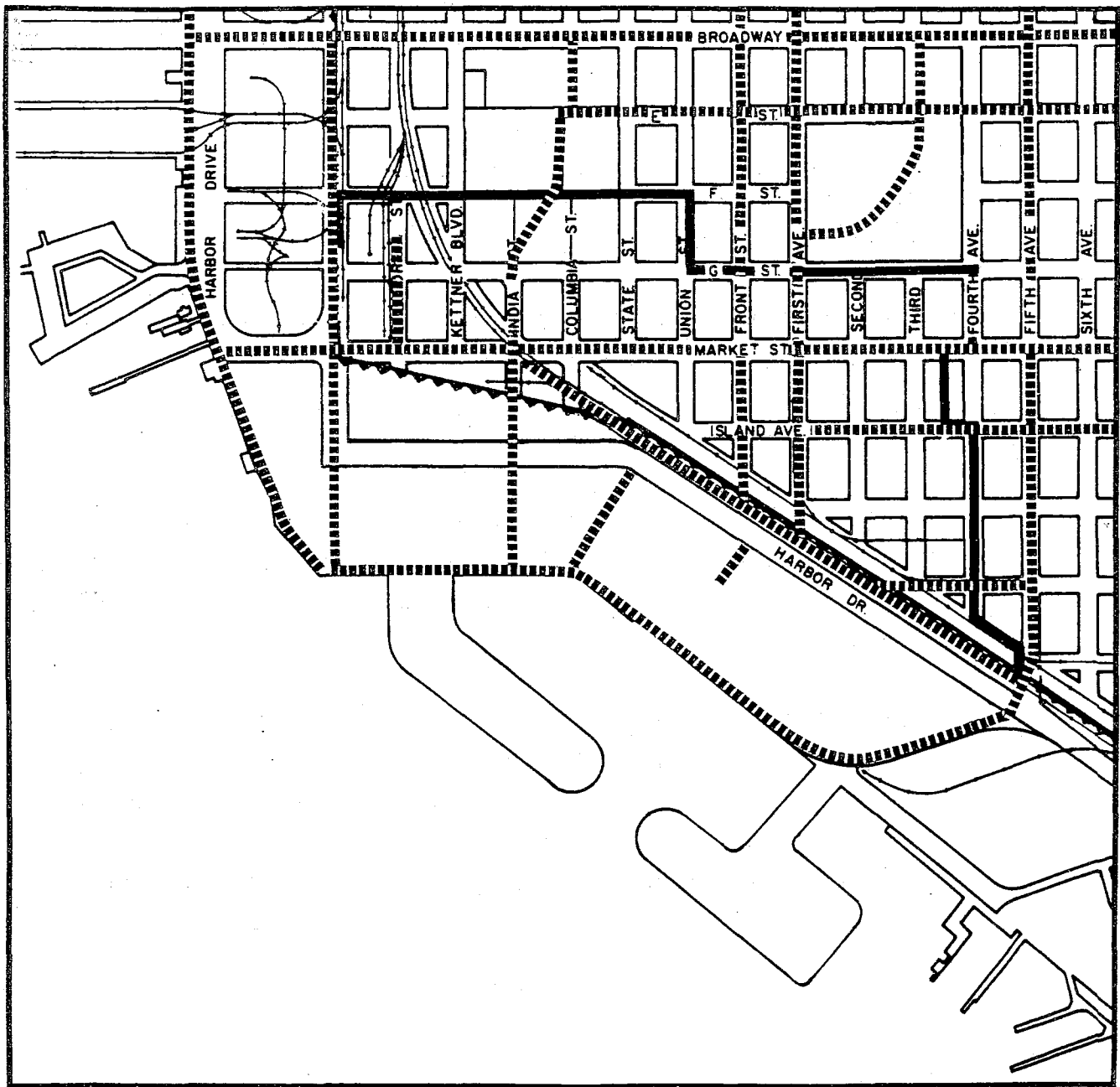


Figure 19
Pedestrian Circulation



Recommendations

- Implement a landscape and off-street improvement program for streets which can have a special boulevard character such as Harbor Drive, Market Street, Pacific Highway, Front and First Street and Broadway.
- Distinguish the intersections of major streets such as Harbor Drive and Front and First Street, Market Street and Harbor Drive with special landscape or sculptural features.
- Utilize the Streetscape Design Manual, Technical Supplement and Figures 20, 21 and 22 to guide the physical off-site improvements of all other streets in the Marina area.
- Where local traffic conditions warrant, redesign street rights of way to establish a pedestrian environment.

A. Special Recommendations for Boulevard Streets

Five streets fit this category and warrant special attention; they are: Harbor Drive, Market Street, Pacific Highway, Front and First Street.

1. Harbor Drive Corridor

The Harbor Drive corridor will accommodate several transit modes. These include vehicles -- buses, light rail, Gaslamp trolley, Santa Fe Railroad and pedestrians. The use of the corridor will require careful coordination between the Port, MTDB, City, Redevelopment Agency and the railroad and therefore, a specific plan jointly financed by the affected entities is recommended.

◦ Landscaping

The waterfront is the most appropriate location for palm trees in the Marina area. Rows of Mexican fan palms complemented by shorter date palms or similar species are recommended.

Figure 20
Sidewalk Paving Program

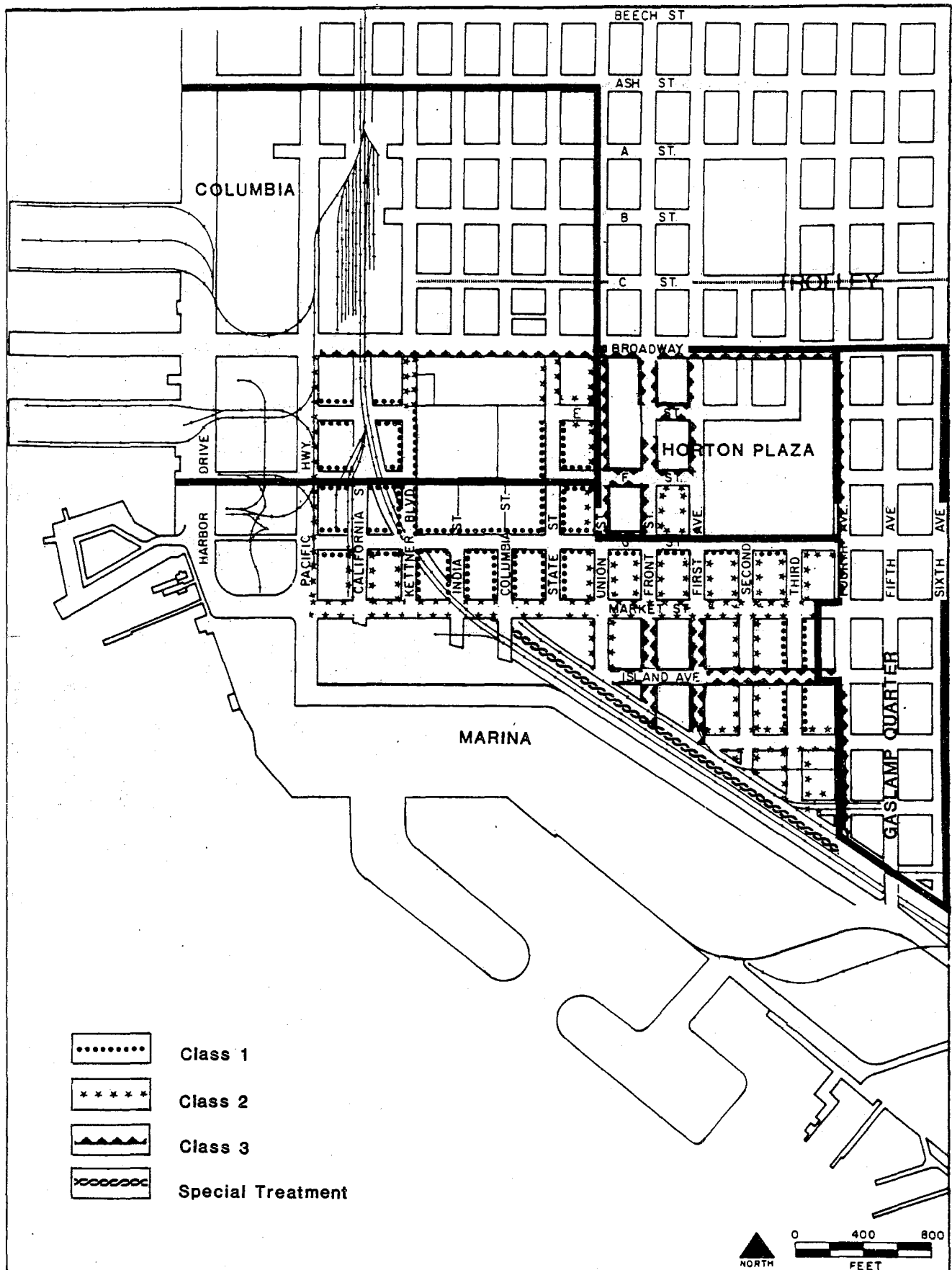


Figure 21
Street Tree Program

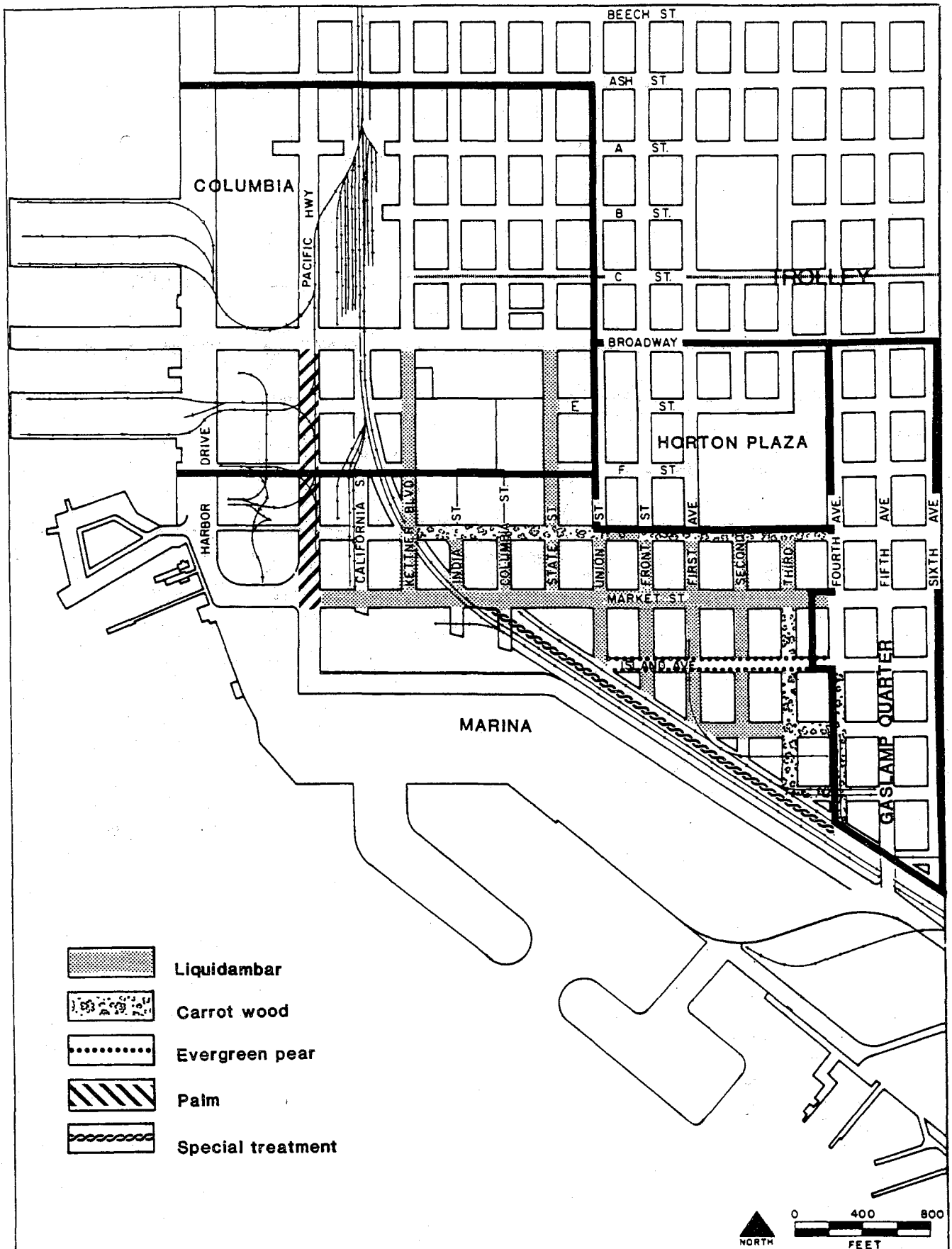
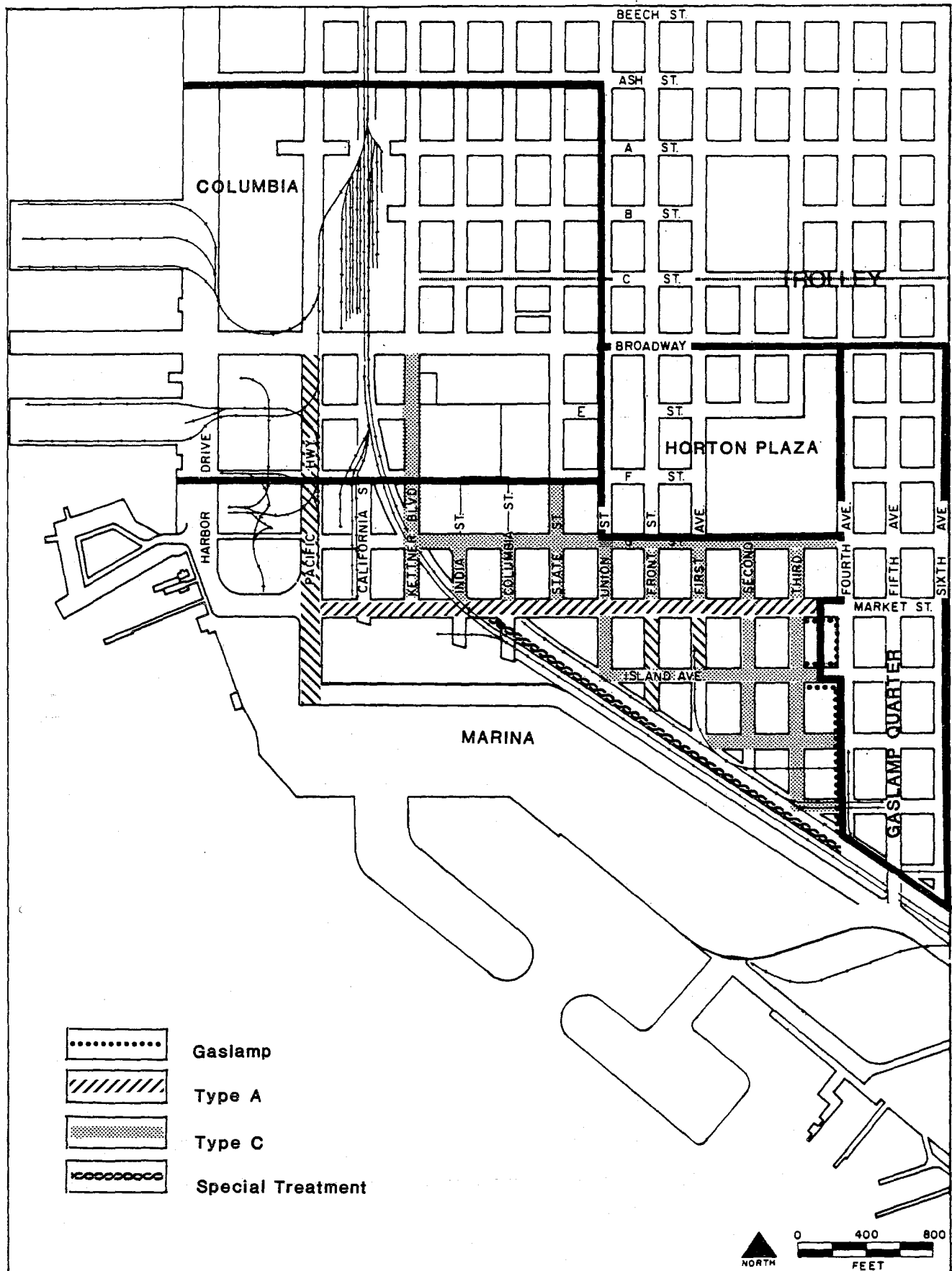


Figure 22
Mid-Block Street
Lighting Program



- Vehicular Operation

Three moving lanes in each direction with a taxi and bus loading zone to serve the convention center. (The right-of-way cross section will be subject to input of the City Traffic Engineer).

- Trolley/Railroad Right of Way

A combination of hardscape and soft paving such as grasscrete to mitigate the visual impact of the tracks.

- Trolley Stations

Trolley stations should be located to serve the Gaslamp, Seaport Village and convention center. The stations should be designed as an integral part of the linear park.

- Linear Park

The linear park should be designed to establish a landscaped edge to the Marina residential community. Both pedestrian and bicycle paths should be incorporated into the park. The linear park may vary in width depending on the excess right of way available; however, a width varying from 40 to 50 feet is recommended.

Private improvements fronting on the linear park will be set back 50 feet from the park boundary and will be landscaped to complement the design of the linear park.

2. Market Street

Market Street is the major ceremonial street within the Marina area. This street extends from San Diego Bay to approximately 60th Street in southeast San Diego.

- Landscaping

Regularly spaced trees such as liquidambar, the officially designated tree, should be planted for its entire length within Centre City. Where the opportunity for a median strip exists, low flower shrubs should be planted.

° Vehicular Operations

Two moving lanes in each direction with a left-turn pocket in the middle of the street would provide an opportunity to create the environmental amenity characteristic of a ceremonial street. The desirable cross-section of the street east of Kettner Boulevard is illustrated below (Figure 23).

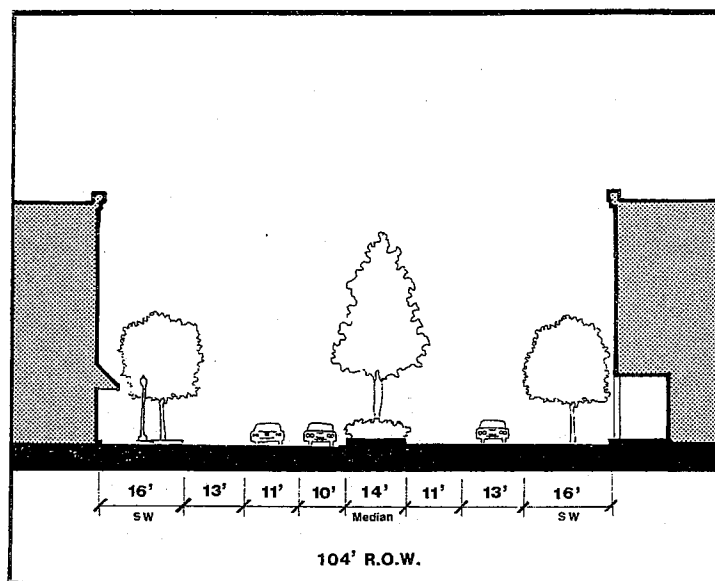


Figure 23
Market Street Landscape
Cross Section

West of Kettner Boulevard, three westbound and two eastbound lanes for Market Street are recommended. The desirable cross section of the street is illustrated in Figure 24.

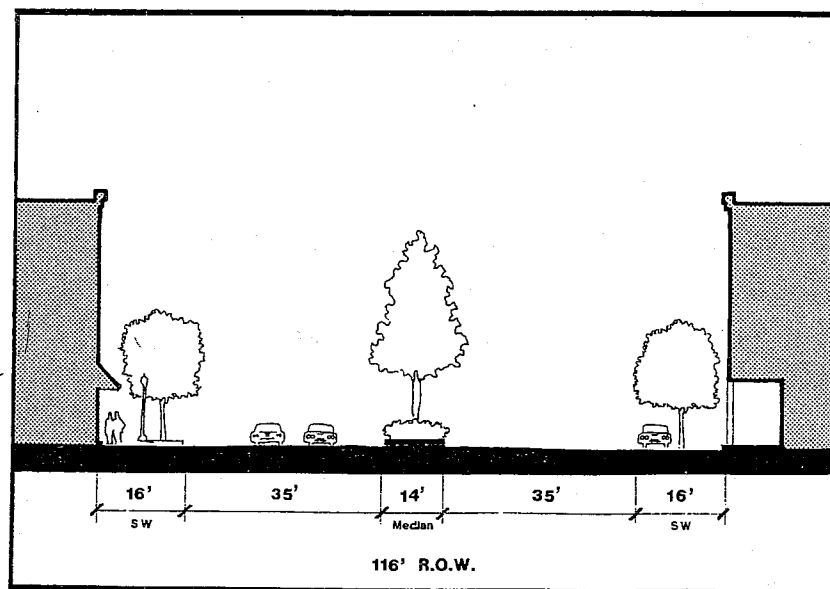


Figure 24
Market St. Cross Section
West of Kettner

- ° Sidewalks, Paving and Lighting

Market Street is conceived as one of the principal linkages between the residential community and the waterfront and pedestrian activities should line this street; the retention of the maximum possible sidewalk width is desirable. A distinctive paving pattern and attractive light fixtures should be provided comparable to the recommendations contained in the Centre City Development's Streetscape Design Manual, Technical Supplement.

- ° Curbcuts

Curbcuts on Market Street should not be permitted. Access should be taken from adjoining north/south streets where possible.

3. Pacific Highway

Pacific Highway is a major gateway street located on the western edge of the Marina area. It is expected to accommodate increased traffic between the hotel/convention center complex and Lindbergh Field.

- ° Landscaping

The landscape quality of Harbor Drive should be continued onto Pacific Highway. Regularly spaced Mexican fan palms or similar species should be planted on the entire length of the street in the central area.

- ° Vehicular Operations

The traffic generated on the street is expected to require three lanes in each direction. This can be accommodated within the existing right of way of 120 feet as illustrated in the cross-section in Figure 25.

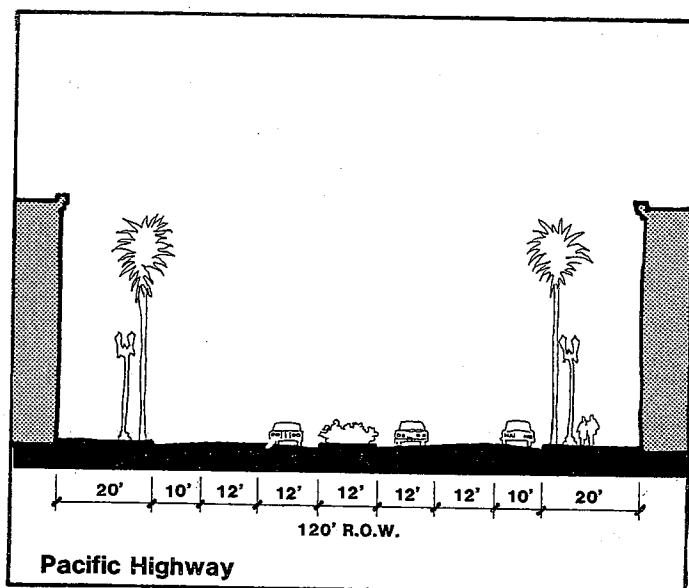


Figure 25
Pacific Highway Cross Section

° Sidewalks, Paving and Lighting

The existing right of way will permit sidewalks at least 20 feet wide. This width should be reconciled with traffic needs of the street. However, in no case should the sidewalk width be less than 16 feet. The highest standard of paving, lighting and landscaping should be required on the street based on Centre City Development Corporation's Streetscape Design Manual, Technical Supplement for right-of-way improvement.

° Curbcuts

Curbcuts on Pacific Highway will not be permitted unless no alternative access to a parcel is available.

4. Front and First

Front and First Streets will function as a one-way couplet between I-5 and Harbor Drive in order to mitigate the impact of heavy traffic on the Marina residential area. Commercial traffic destined for the convention center/hotel complex should be signed for alternate routes such as Pacific Highway, Eighth Avenue and Imperial Avenue. Pacific Highway/Harbor Drive should be connected to I-5 on the north and at the approximate intersection of Imperial Avenue and I-5 on the southeast.

◦ Landscape

The landscape quality of Front and First Streets should establish a ceremonial entrance to the Marina area from the convention center/hotel complex located on Harbor Drive.

◦ Vehicular Operations

No more than three lanes of traffic should be accommodated on each street. In general, curbside parking should be controlled and sidewalk width increased. Cross-sections illustrated below (Figure 26) identify possible alternative designs for the right of way.

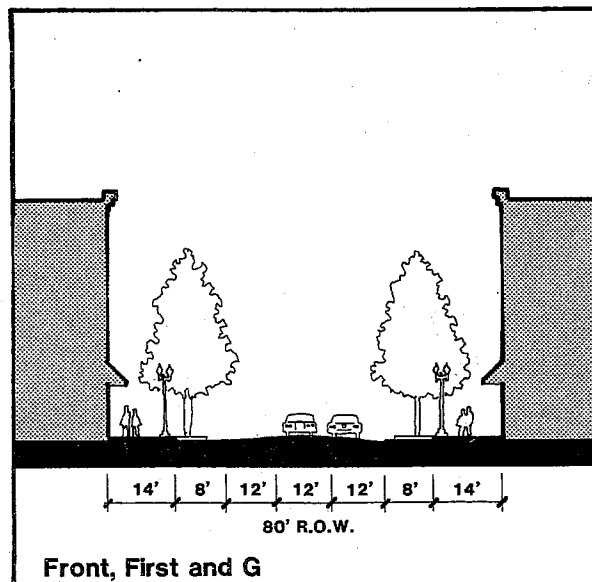


Figure 26
First and Front Cross Section

- Sidewalk, Paving and Lighting

The existing right of way will permit sidewalks at least 20 feet wide. Because of the importance of these streets as gateways to Centre City, the highest standards of paving, lighting and landscaping should be required from Harbor Drive to Broadway.

- Curbcuts

Curbcuts on Front and First Streets should be carefully designed to avoid pedestrian/vehicular conflicts. To the extent possible, access should be eliminated south of Market Street. No parking should be permitted on either street.

5. Other Streets

To the extent possible, all other streets should become more pedestrian- and less vehicle-oriented. The streets which appear to be the best for pedestrian enhancement are portions of "E," Columbia, State and Island Avenue. The term used to convey the concept of pedestrian dominance, "Woonerfs," taken from the Dutch, is illustrated in Figure 27.

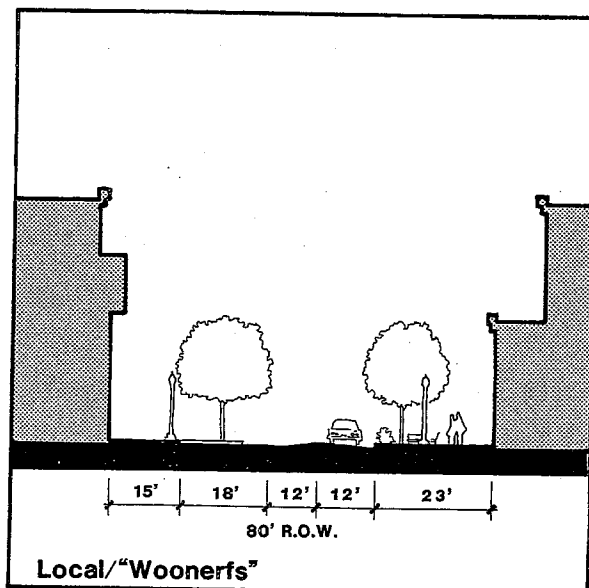


Figure 27
Woonerfs

- ° Vehicular Operation

One lane in each direction. Alternating perpendicular parking or parallel parking can be accommodated based on traffic requirements.

- ° Sidewalks, Paving and Lighting

The concept for sidewalk treatment should be based on the Streetscape Design Manual, Technical Supplement.

- ° Curbcuts

Curbcuts are permitted on Woonerf streets.

B. Special Spaces

Remnant street rights of way, particularly south of Market Street provide an opportunity to create quasi-public open spaces which can enhance the residential development of the area and also reinforce the open space/pedestrian circulation systems. Such possibilities should not be lost through street closures. Conversely, every opportunity to use the street for public purposes, exclusive of vehicular access, should be explored. The following graphics illustrate examples of opportunities to enhance the environmental quality of the Marina area.

- ° The intersection of Market Street with Harbor Drive represents a design and engineering challenge to accommodate vehicular movement, trolley and railroad passage and pedestrian circulation from Pantoja Park to the waterfront, while retaining the ceremonial quality of Market Street and its bay view. The sketch shows one way this complex intersection might be designed.

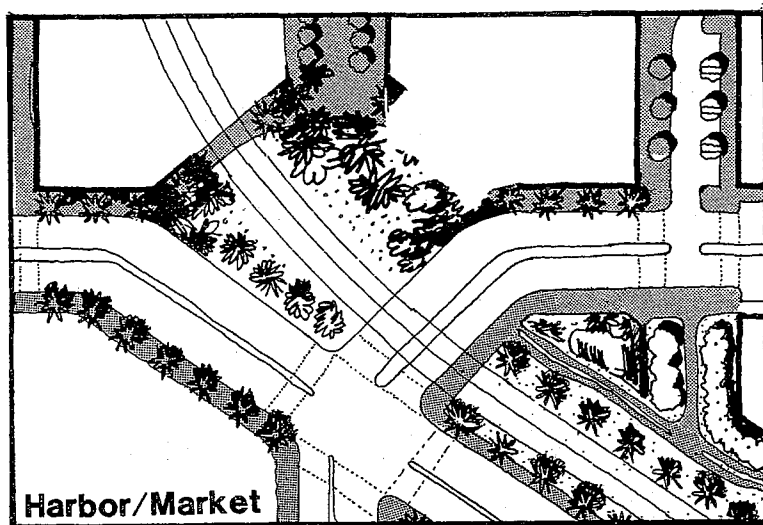


Figure 28
Market Street
and Harbor Drive
Intersection
-conceptual-

- ° The intersection of Harbor Drive and the First/Front couplet represents an opportunity for feature landscaping accentuating the importance of the intersection as a gateway into and beyond the Marina area. The sketch is one way this space might be treated.

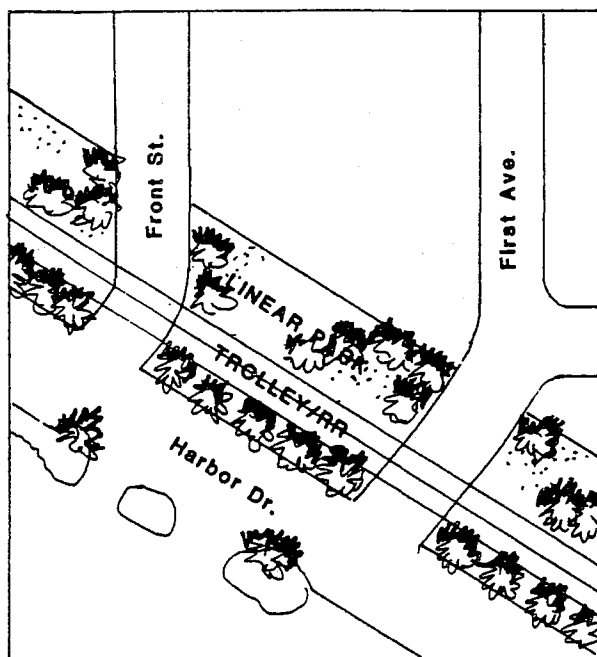


Figure 29
Front/First
and Harbor Drive
-conceptual-

6. Heritage Conservation Plan

Objectives

- ° Retain the existing historic buildings in the Marina area.
- ° Retain the existing architecturally significant buildings in the Marina area where possible.

Recommendations

Conserve historic and architecturally significant buildings, encourage adaptive reuse and infill development around existing structures.

A. Historic Sites and Buildings

The historic buildings in Figure 30, as identified in the National Register or by San Diego's Historical Site Board, include the following structures within the Marina area.

1. U.S. Customs and Courthouse Building (National Register, Historical Site Board #111).
2. Horton Grand Hotel, renovated 1986 (National Register, Historical Site Board #95).
3. Pantoja Park (Historical Site Board #7).

B. The following buildings have been determined eligible for the National Register of Historic Places:

4. Soap Factory complex (The Historical Site Board of San Diego will consider designation of the Soap Factory to the Local Register on May 25, 1988).
5. Frost Lumber Corner Building (the construction of the Bayside Trolley line and the realignment of Harbor Drive will cause the Frost Lumber Corner Building to be torn down).

C. The Chinese/Asian Thematic Historic District

In April, 1987, twenty-two sites as part of a Chinese/Asian Thematic Historic District were placed on the local register of Historic Places. The thematic district encompasses buildings and locations of San Diego's original Chinatown.

The City Council/Redevelopment Agency on March 22, 1988, concurred with the action of the Historical Site Board and voted to place the twenty-two structures on the Local Register of Historical Places.

The Council/Agency referred the disposition of the Chinese Mission Building and Anita/Regal Hotel to Centre City Development Corporation for the preparation of negotiation agreements which would call for their dismantlement and reconstruction within Subarea 2 of the Chinese/Asian Thematic Historic District.

The Thematic District is composed of two distinct subareas which are illustrated in Figure 31.

Subarea 1 is located in the Gaslamp Quarter and bounded by Market Street to the north, "J" Street to the south, Fourth Avenue to the west and Sixth Avenue to the east. Fourteen Chinese- or Asian-related structures are included within Subarea 1.

Subarea 2 is located in the Marina Redevelopment Project and is bounded by Market Street to the north, "J" Street to the south, Fourth Avenue to the east and mid-block between Second and Third Avenues to the west. Chinese buildings located within Subarea 2 include:

6. Ying-On Labor and Merchants Building (Historical Site Board #151)
7. Chinese Benevolent Society Building (Historical Site Board #150)
8. Plants and Fireproofing Building
9. Gim Wing/Woo Chee Chong Building
10. Ying-On Annex
11. Joseph Quin Residence

12. Quong Building
13. Regal and Anita Hotels/Raid Building
14. Chinese Mission Building

(The Chinese Mission Building is currently located at 643-645 First Avenue, but will be reconstructed in Subarea 2 of the Chinese/Asian Thematic Historic District).

Design Guidelines for the historical buildings are located in the Heritage Structures section of the Marina Development Guidelines beginning on Page 70.

D. Architecturally Significant Buildings

The following buildings are generally recognized as architecturally significant, although they are not specifically acknowledged by the National Register nor the Historical Site Board; they are identified in Figure 30.

15. Warehouse Limited Buildings
16. Cracker Factory
17. Royster Building.

Figure 30
Heritage Buildings

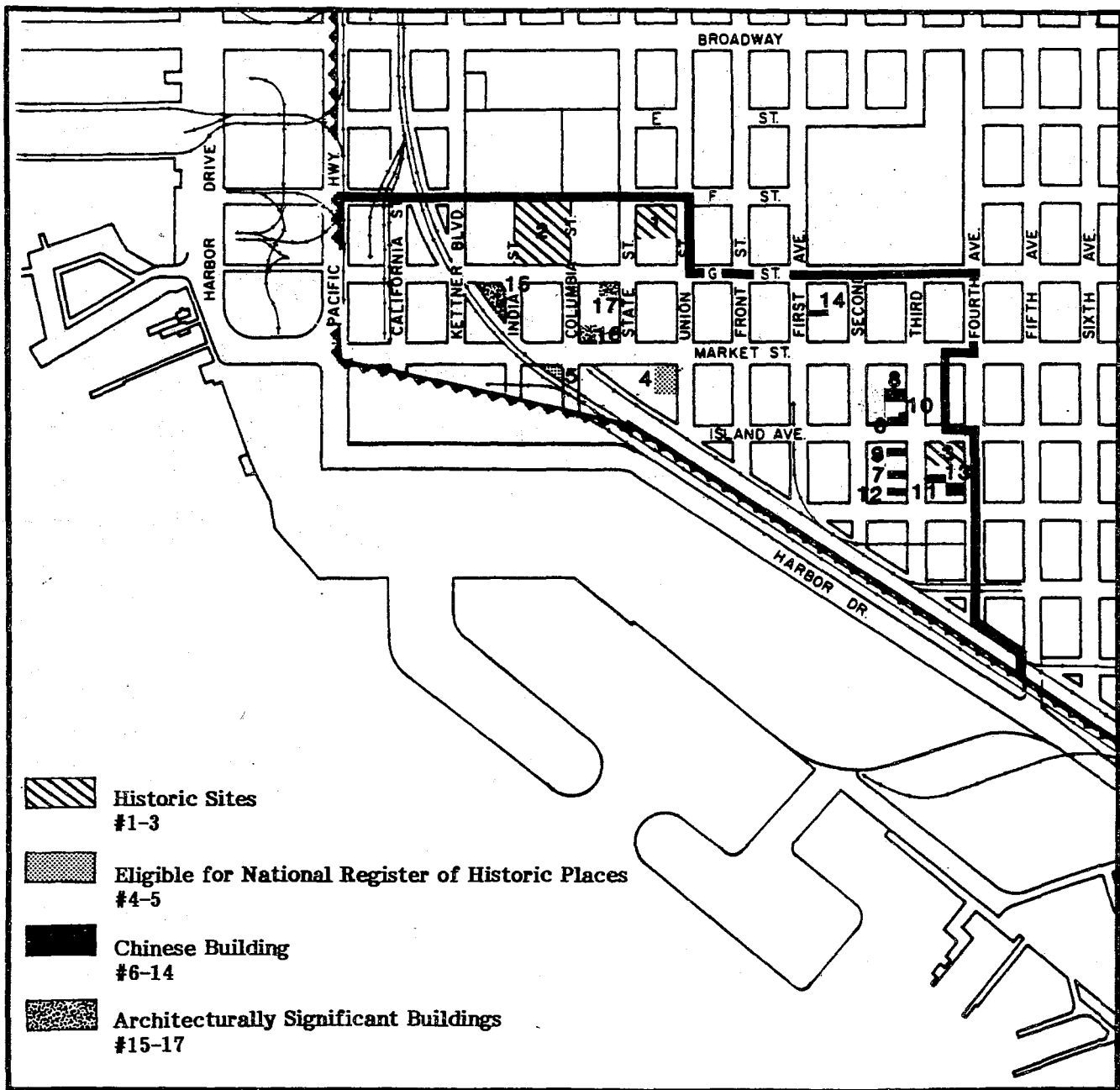
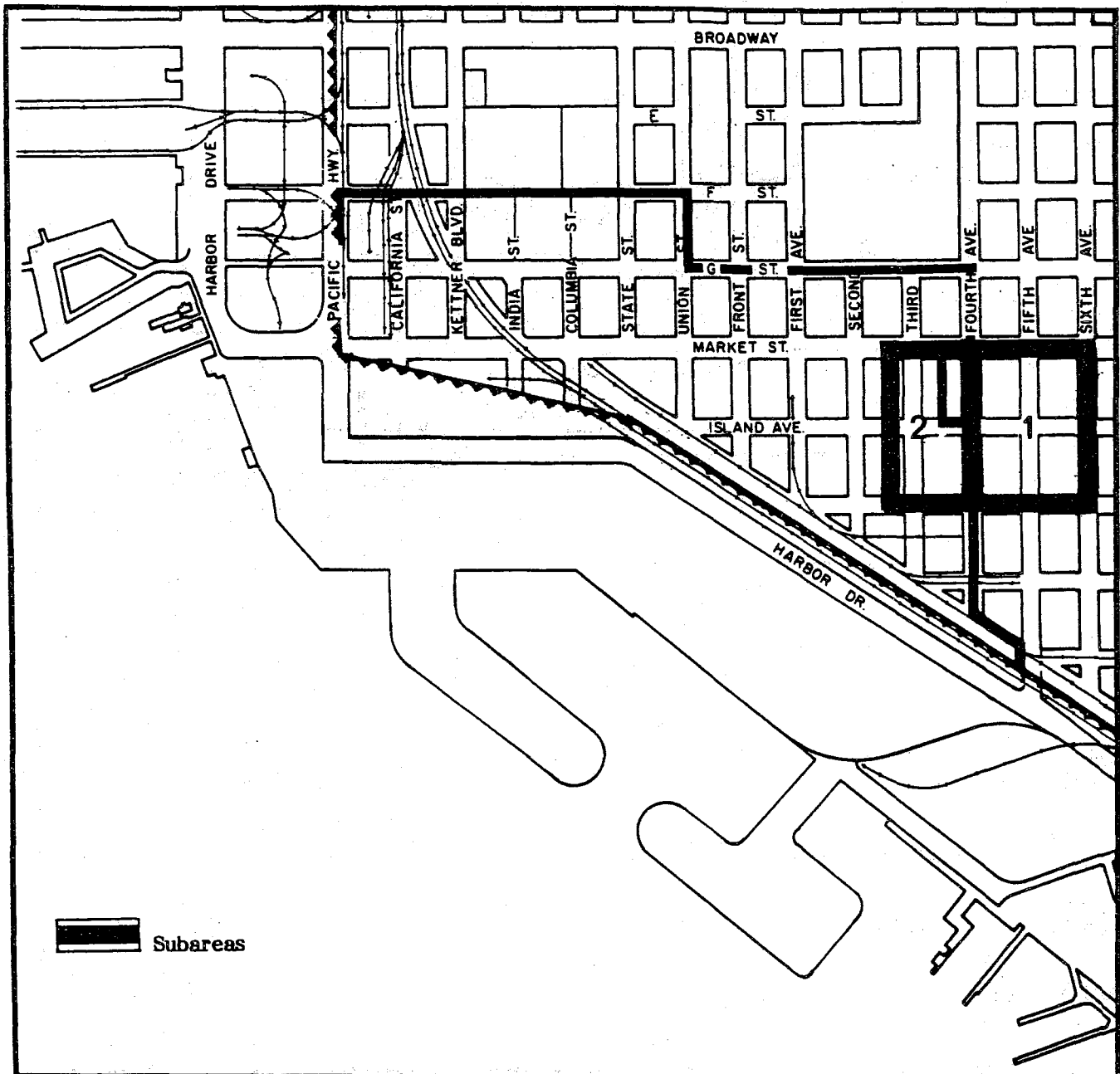


Figure 30A
Chinese/Asian Thematic Historic District



M A R I N A

D E V E L O P M E N T **G** U I D E L I N E S

INTRODUCTION

Certain standards for architectural design are an integral part of the urban design plan for the Marina area. The standards have been developed to help ensure that redevelopment of the area produces an attractive, secure urban residential neighborhood of the highest possible design quality, a viable, new option for the citizens of San Diego.

- ° Streets in the Marina area should be as active as possible.
- ° Architectural design within the area should maintain a residential character, regardless of the predominant use within the structure.
- ° Buildings should have the characteristics of small-scale development.
- ° Development should respect the architectural character of historic buildings.
- ° Personal security should be aided through the use of defensive design techniques.
- ° Parking facilities should be concealed from general view.

The design standards address these areas of building design: street facades, building materials and colors, security, heritage structures and signage.

S TREET **F** ACADES

1. STREET FACADE OBJECTIVES

A. Reinforce the existing street patterns

B. Animate streets

- ° Enhance the pedestrian experience at grade
- ° Avoid continuous blank walls
- ° Design frontage to provide a secure environment

C. Maintain residential character

- ° Provide direct access to residential development from the adjoining street frontage.
- ° Design developments which are in scale and sensitive to the creation of a residential environment.

2. STREET FACADE DESIGN GUIDELINES

- A. Build to Property Line:** To create a sense of enclosure, at least 75% of the street facade of the building's base should be built to the street frontage as shown in Figure 32. Arcades, colonnades, punched wall openings, recesses, ornamental fences or other architectural features can be used to maintain the street wall.

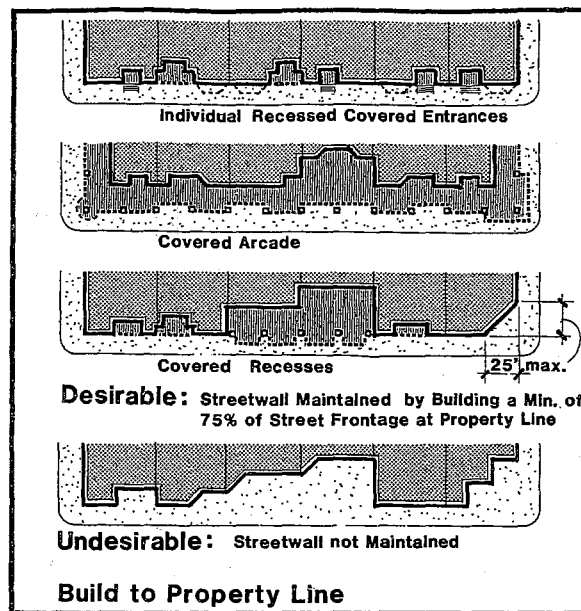


Figure 32
Build to Property Line

- B. Reinforce Street Corners:** Buildings should reinforce street corners as shown in Figure 33.

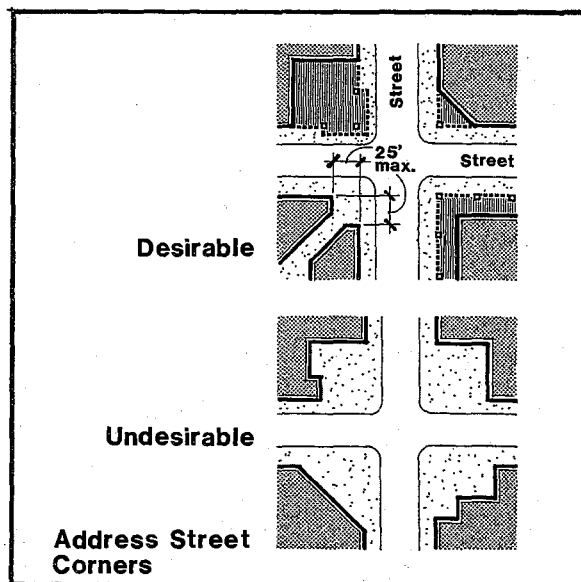


Figure 33
Reinforce Street Corners

- C. Street Level Design, Retail Frontages:** Storefronts should be designed to create a lively facade at the street level and to give individual identity to each store.

- ° Bay Widths: The 50 foot facade modulation should be integrated into the storefront design. A smaller module may be superimposed within the larger bay width to create smaller stores.
- ° Repetitive Elements: In order to achieve both diversity and design cohesion, certain elements of the storefront design should be repeated throughout. These elements include a base, a sill member and a transom member at doorhead height.

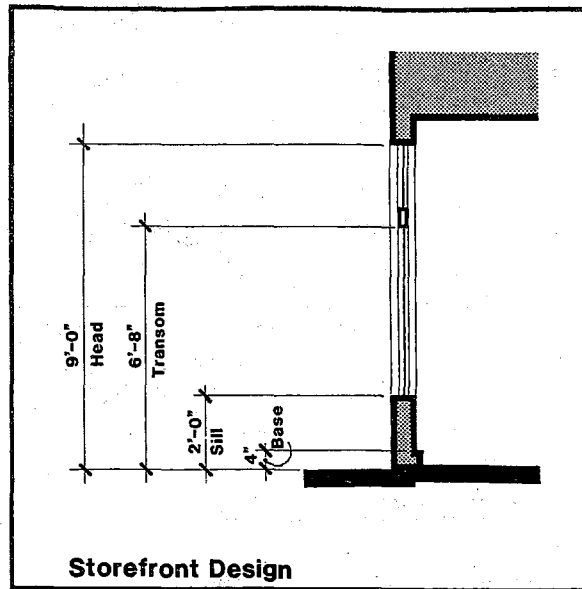


Figure 34
Storefront Design

- ° Openings to Street: Fifty percent (50%) of the length of any facade should be devoted to pedestrian entrances, display windows or windows affording views into retail stores, offices or lobby space. No blank wall, garage door or combination of the two should exceed 30 feet in length without significant architectural treatment, such as pilasters, recesses for planting or other devices (Figure 35)

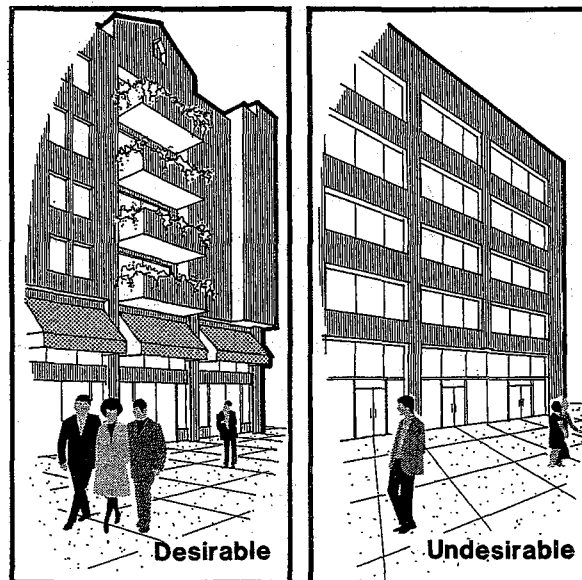


Figure 35
Openings To Street

- ° Awnings and Canopies: Projections for weather protection such as awnings and canopies are encouraged. They should be at least 7 feet, 6 inches above the sidewalk and encroaching into the public right of way no more than 4 feet from the property line.

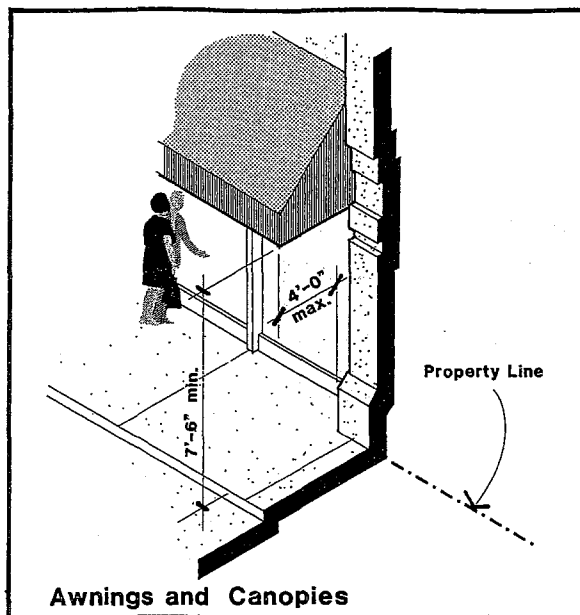


Figure 36
Awnings & Canopies

D. Street Level Design, Residential Frontage: Residential frontages should be designed to ensure both privacy for the resident and activity and visual interest on the street:

- ° Openings to Street: No more than 60 % of the length of any facade on a residential street should be blank walls and garage doors. No blank wall, garage door or combination of the two should exceed 30 feet in length without significant architectural treatment, such as pilasters, recesses for planting or other devices.
- ° Street Level Terraces: Terraces at or within 5 feet of street level should have a privacy and security wall no taller than 8 feet from the sidewalk elevation (Figure 37). The two-story minimum streetwall standard is waived for the length of terrace walls; however, solid terrace walls higher than 5 feet from the sidewalk should be considered blank walls.

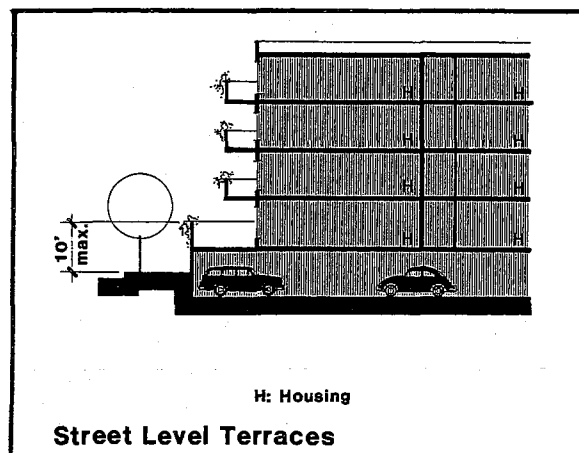


Figure 37
Street Level Terraces

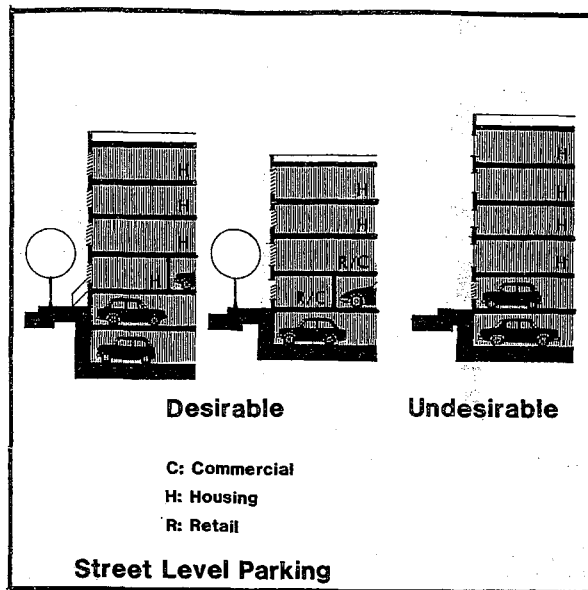


Figure 38
Street Level Parking

- E. Street Level Parking:** All off-street parking should be screened from view. Parking garages at street level should be screened by another use. No surface lots are permitted (Figure 38).

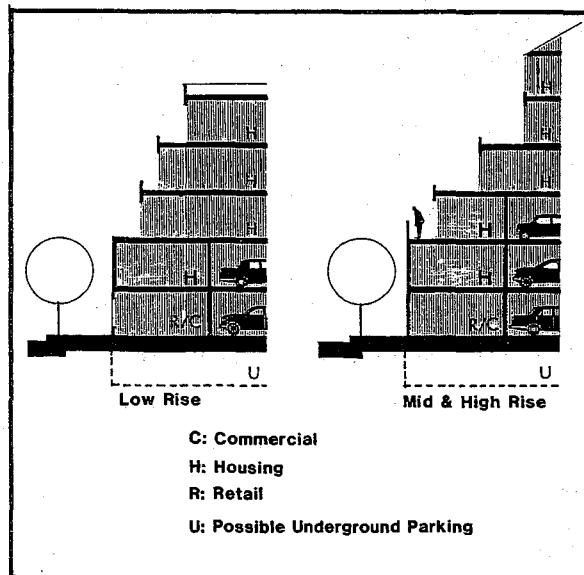


Figure 39
Parking Above Street

- F. Parking above Street Level:** Parking below grade may extend to within 4 feet of the curbline. A minimum of 50% of the parking located at grade should be buffered with residential, retail or other non-residential uses and landscaping. Fifty percent of all parking located above the first level should be buffered with residential uses or landscaped terraces. The exterior wall of these parking levels should provide a visual screen and be architecturally integrated with the rest of the building. The screen should be a minimum of 50% opaque over the entire garage elevation to obscure the view of mechanical equipment, automobiles and glare from light. Rooftop parking should be concealed from public view (Figure 39).

- G. **Parking without Active Uses Above:** Parking facilities without active commercial or residential uses above them should be concealed from view by a deck usable as active or passive open space over at least 50% of the garage.
- H. **Vertical Articulation of Corners and Entrances:** Corners of buildings, particularly those at street corners and major entrances should be articulated vertically.

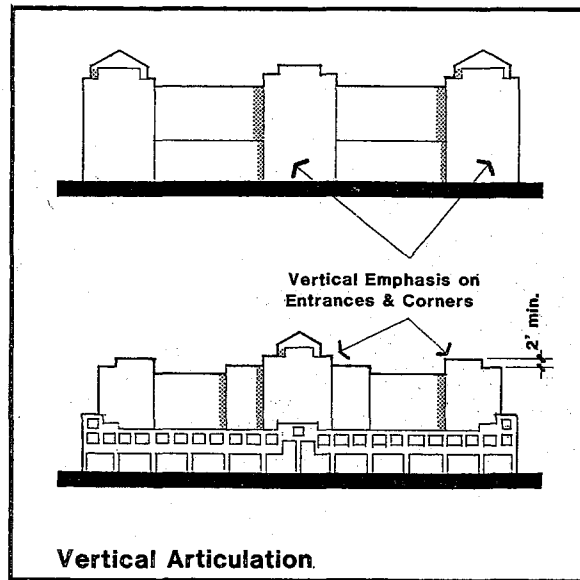


Figure 40
Vertical Articulation

- I. **Vertical Articulation and Facade Modulation:** In order to emphasize the image of residential development and to reflect the historic scale of adjoining development, the facade of the structure should be divided into units which complement a residential scale. This can be achieved in a number of ways:

1. **Low-rise and Mid-rise Residential:**

- a. Changes of at least 2 feet in apparent roof height at approximately 50 foot intervals.
- b. Changes of at least 2 feet in the plane of the facade at approximately 50 foot intervals.
- c. Changes in the rhythmic pattern of window openings, balconies, combination of building materials from one 50 foot module to the next.

2. High-rise (over 90 ft.):

No facade wall should exceed 100 feet in width without an interruption of the vertical plane parallel to the facade as exemplified below (Figure 41).



Figure 41
High-Rise Articulation
and Modulation

J. Horizontal Facade Treatment: In addition to the variation of the street facade, variety should be added to the structure through the incorporation of any combination of the following architectural devices:

- ° Bay Windows and Private Balconies: The maximum projection of 4 feet from the property line is subject to existing policy of the Building Inspection Department.

- ° Combined Bay Windows and Balconies: Permitted with a maximum projection of 4 feet from the property line.
- ° Chimneys
- ° Awnings: Projecting no more than 4 feet beyond the property line.
- ° Loggias
- ° Cornices and Eaves: At the roof, these items may project 3 feet beyond the property line or 1 foot beyond the outermost face of any projecting bay window or balcony. Cornices not at the roof line may project 1 foot beyond the building facade.(Figure 42).

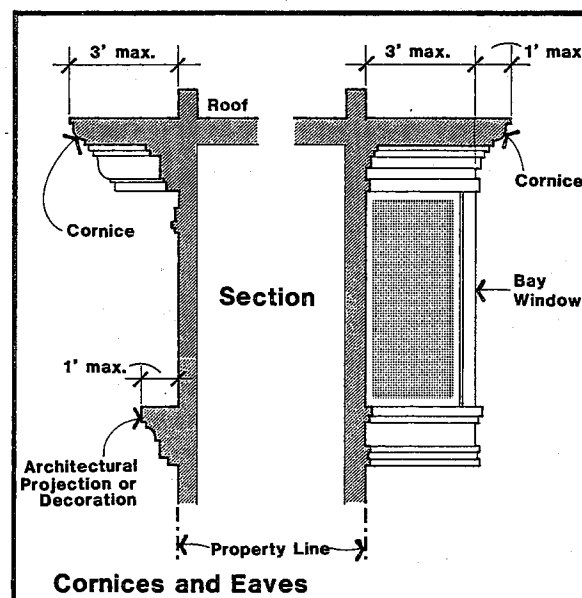


Figure 42
Cornices & Eaves

- ° Sills and Beltcourses: These elements may project 1 foot beyond the building facade at the property line.
- ° Dormers

K. Unscreened Private Outdoor Space: At least 80% of the length of railings to private balconies should be solid for a height of at least 2 feet in order to screen stored personal items from public view.(Figure 43).

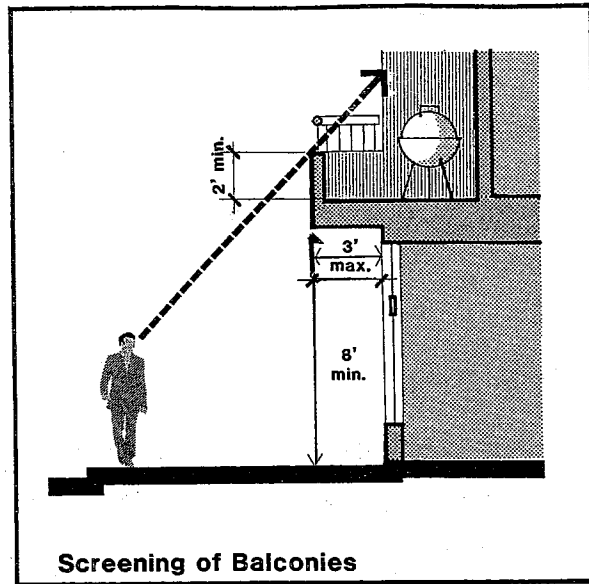


Figure 43
Screening of Balconies

- L. **Roofs:** The roof line should be significantly enriched by incorporating architectural elements such as cross gables, dormer windows, cornices, false fronts, crenellated or decorated parapet walls, attic vents, turrets, etc., which reduce the scale of continuous roofs and break the line where the building meets the sky.

- ° Unbroken Roof Lines: The maximum length of unbroken roof line should be 50 feet as shown in Figure 44.

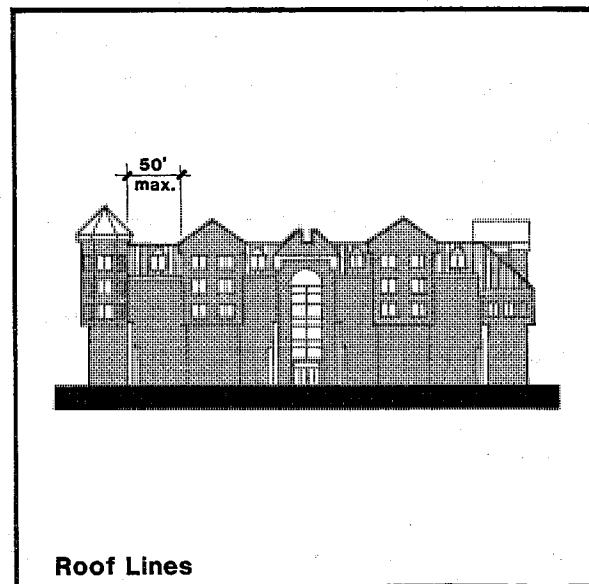
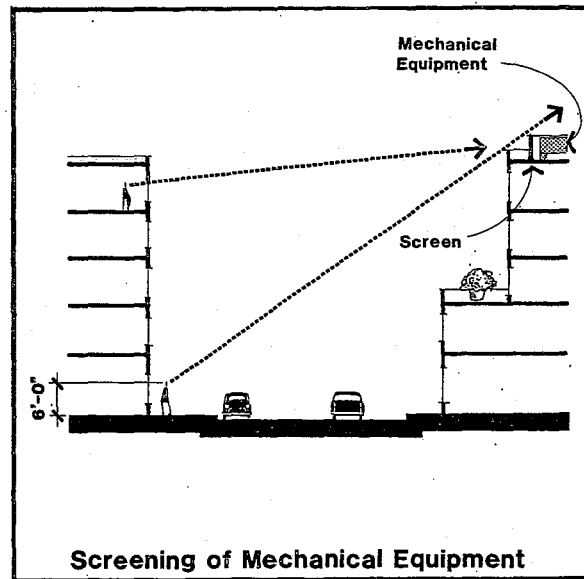


Figure 44
Roof Lines

M. Prohibited Items:

- ° No parking should be permitted on the roof of garage structures.
- ° Wall Air Conditioners: Wall air conditioners are prohibited unless they are concealed from the public view by railings of private balconies.
- ° Unscreened Mechanical Equipment: Unscreened mechanical equipment areas are prohibited. Screening of mechanical equipment should be integrated with the design of the building. The screening should be set back a sufficient distance from the building facade so that it is concealed from view when measured 6 feet above grade at the front property line of the opposite building. Mechanical equipment should also be screened from view of adjoining high-rise structures as shown in Figure 45.



**Figure 45
Screening of
Mechanical
Equipment**

- ° Exposed Metal Fire Escapes: Exposed metal fire escapes are prohibited on the street facade.

WIND TUNNEL AVOIDANCE

1. SOLAR ACCESS OBJECTIVES

- ° To ensure a maximum level of sunlight in public spaces at all times.
- ° To ensure stepbacks of building structures above streetwalls to maximize the opportunity for sunlight penetration at street level.

SOLAR ACCESS REQUIREMENTS

- A. **Public Parks and Open Spaces:** No more than 50% of the area of a park or open space should be shaded by a development for more than one hour between the hours of 11 a.m. to 2 p.m. on the winter solstice.
- B. **Opposing Sidewalks:** No more than 50% of the area of a sidewalk on the opposite side of a development should be shaded by that development for at least one hour between 11 a.m. and 2 p.m. on the winter solstice. For purposes of this section only, the sidewalk shall be assumed to be 14 feet wide.
- C. **Publicly Accessible Spaces:** Publicly accessible outdoor spaces in an otherwise private development, such as Horton Plaza's Food Court, should receive the same consideration as public parks and open spaces.

2. WIND TUNNEL AVOIDANCE OBJECTIVES

- ° To minimize ground level wind conditions.
- ° To avoid eddies and turbulence between buildings and on public open spaces.

WIND TUNNEL AVOIDANCE REQUIREMENTS

Wind tunnel testing may be required for proposed projects which could impact the microclimate.

BUILDING MATERIALS AND COLORS

1. BUILDING MATERIALS AND COLORS OBJECTIVES

- **Visual Compatibility:** Building materials and colors should be chosen to blend harmoniously within a neighborhood.
- **A Sense of Permanence and Solidity:** Buildings should be detailed so that they give a sense of permanence and solidity.

2. BUILDING MATERIALS AND COLORS REQUIREMENTS

A. **Exterior Building Walls:** All materials used on the exterior walls of new structures should be masonry, stone, brick, stucco or other high quality material.

B. **Building Roof:**

1. Sloping Roofs: All roof materials that are sloped and visible from the street should be concrete tile, ceramic tile or architectural metal roofing.
2. Flat Roofs: Flat exposed roofs of more than 30 square feet that are visible from dwelling units above on the same or adjacent properties should be architecturally treated to mitigate the visual impact. Architectural treatment should include landscaping, treatment of the surface materials with special aggregate or paving, duckboards, etc. (Figure 46).

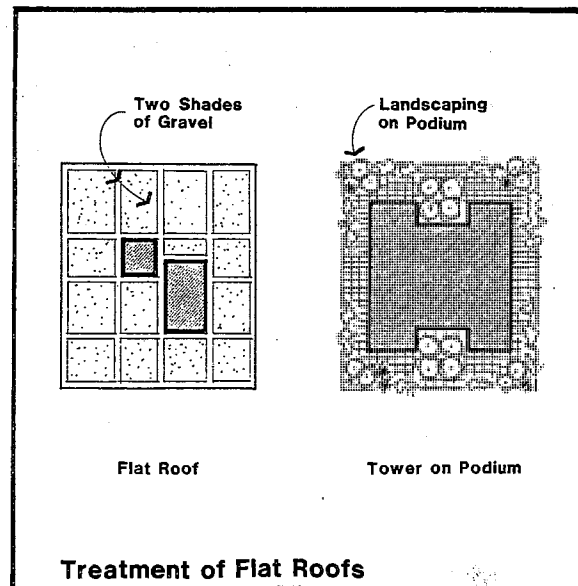


Figure 46
Treatment of Flat
Roofs

C. Window Glazing:

1. Glazing Limits: The total amount of glazing on a building's facade exposed to the street should be a maximum of 80% of the total wall area at the ground floor. On upper floors, no more than fifty percent (50%) of the facade should be vision or spandrel glass, with a reflectivity of no more than 30%.
2. Window Design: Windows above the ground floor should impart a residential appearance. Windows larger than 10 square feet in area should be subdivided by panes, mullions or other devices.
3. Window Transparency: Clear or lightly tinted glass of a gradient no greater than 20% should be used for display windows, street-level doors and residential windows.

D. Architectural Details & Decorative Features: Architectural details with residential character are encouraged. For instance, wrought or cast iron may be used for pilasters, rails, grills, lamps and other trim. Metal and glass or canvas may be used for awnings, marquees and canopies.

E. Trim: Materials used to augment exterior building walls or decorative features such as trim around windows, fascia pieces of parapets, guardrails in railings, etc., may be wood or metal.

F. Visual Continuity: Assure visual coherence of the neighborhood by relating new developments to existing adjacent structures in terms of materials and colors. Avoid extreme or jarring contrasts of color, shape and other characteristics that would make new buildings stand out in excess of their public importance. Align belt courses, cornices and storefronts with the facades in new development where possible.

- G. **Sense of Permanence:** The composition of materials should avoid giving the impression of thinness and artificiality. Veneers should turn corners, avoiding exposed edges as shown on Figure 47.

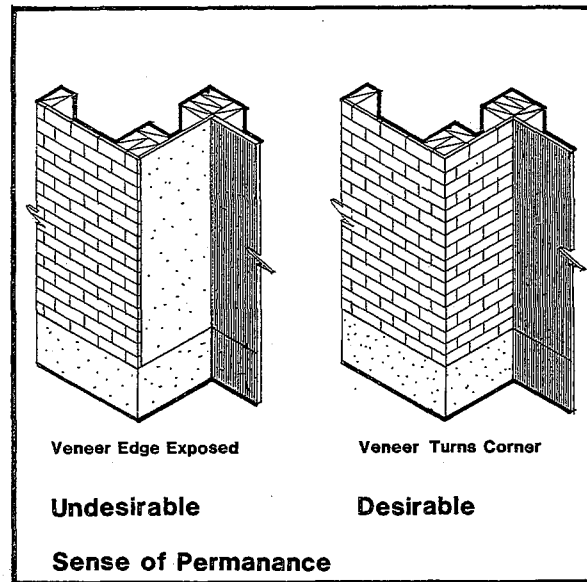


Figure 47
Sense of Permanence

SECURITY

1. SECURITY OBJECTIVE

To provide safety on the City streets and at building entrances by incorporating defensible design techniques into the planning process.

2. SECURITY REQUIREMENTS

- A. **Visible Entrances:** Entrances to shops and residences at the street level should have brightly lit and unobstructed visual access within the area of the width of the entrance plus 45° to the streetwall on either side of the entrance (Figure 48).

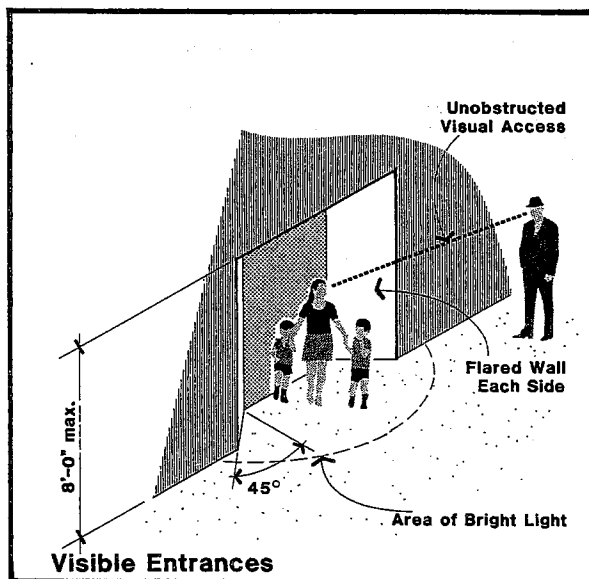


Figure 48
Visible Entrances

- B. **Visible Recesses:** Recesses at retail arcades and shops at corners shall have transparent side and approach walls, the latter at least twice the depth of the recess.

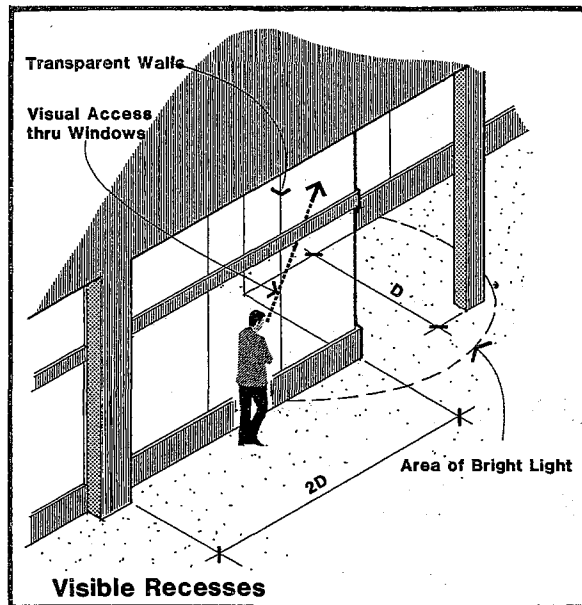


Figure 49
Visible Recesses

- C. **Transparent Corners:** Corners at retail intersections should have transparent walls or other treatment ensuring visual access "around" the corner at least 10 feet from the corner.

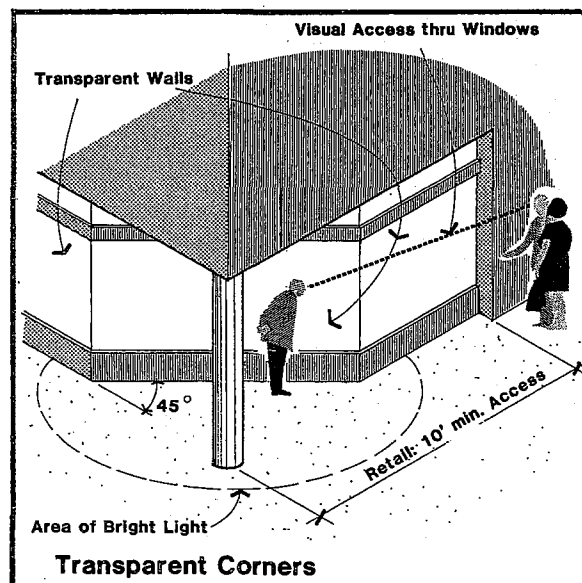


Figure 50
Transparent Corners

- D. **Columns:** Freestanding columns at the street level should be small enough to prevent the concealment of a person.

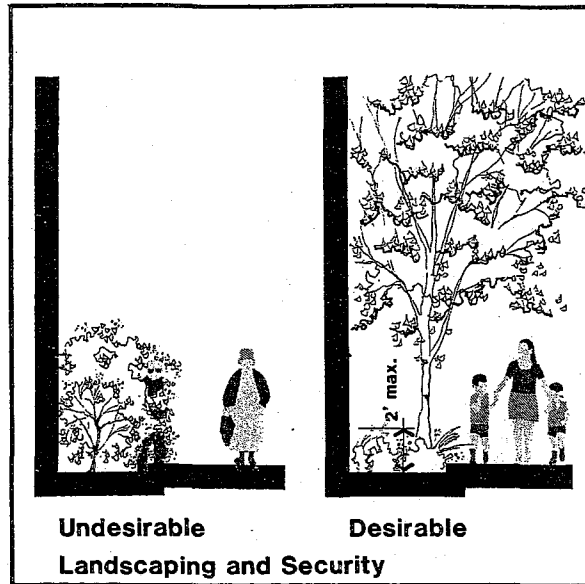


Figure 51
Column Size

- E. **Landscaping:** Planting at the street level should be selected and maintained to prevent the concealment of a person. Shrubbery should be pruned to a height of no more than 2 feet. Trees should be pruned below 6 feet (Figure 52).

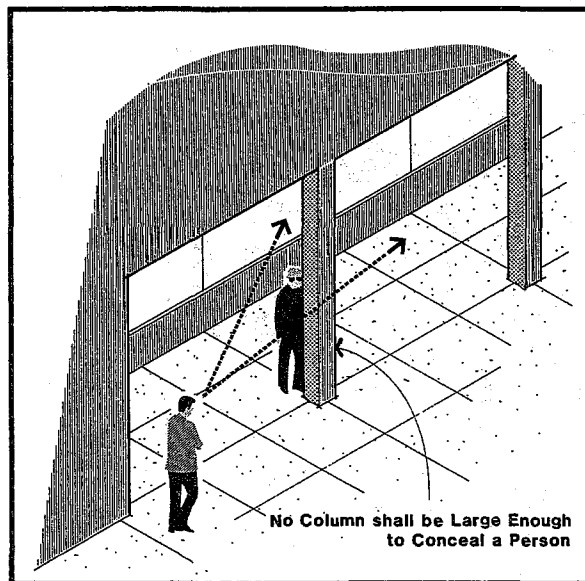


Figure 52
Landscaping and Security

- F. **Lighting:** Any area that has the potential for concealment such as a doorway, columns and arcade corners should be provided with lighting sufficient to create a zone of bright light.

- G. **Shadows and Trees:** Trees should be pruned up to a height of 6 feet.

SIGNAGE

1. SIGNAGE OBJECTIVES

- **Scale:** To encourage signage that will be visible and informative at the pedestrian scale, signage visible from the middle and distant ground perspectives should clearly orient the viewer to the entrance of the building and represent the dominant tenant/identity of the building.
- **Visual Interest:** To promote the use of signage as an element of vitality and visual interest within the overall context of the Marina area.

2. SIGNAGE REQUIREMENTS

A. **Building Identification Signs:** Project signs for hotels, office buildings, or residential developments shall be permitted only adjacent to the entrances of such projects and within the locations listed below:

1. Within one 50 foot bay immediately to the left or right of the entrance.
2. All signs shall be located below the building parapet or roofline.
3. No portion of a sign shall be located more than 65 feet above the elevation of the sidewalk at the street property line closest to the sign.
4. No building identification sign shall be larger than 25 square feet.

B. **HIGHRISE LOGOS:** A maximum of two logos are permitted per building. The logo shall be designed to be an integral element on the exterior of

the building and oriented to the building entrance. Logos cannot occur on any two adjacent facades. The following are maximum logo sizes:

<u>Building Height</u>	<u>Maximum Logo Area</u>
Less than 65 feet	See building identification sign criteria
65 - 120 feet	50 square feet
120 - 200 feet	75 square feet
201 + feet	100 square feet.

C. BUSINESS IDENTIFICATION SIGNS:

Street - and pedestrian-oriented signage which identify a retail or commercial business establishment shall be allowed in a maximum of two of the following locations per establishment:

1. On the fascia band above the store entrance or window.
2. On the vertical or side fascia of any sunscreen or awning.
3. On the window of the establishment.
4. Hung from an arcade, sunscreen or awning as long as it does not impede minimum headroom requirements.
5. Projecting from a column, pilaster or frame.

Fascia, awning or window signage may not be larger than 10 square feet. Projecting and hung signage may not be larger than 2 square feet.

- D. PARKING SIGNS:** Signage pertinent to publicly accessible parking facilities shall be allowed only within a 25-foot distance on the ground floor to either the right or left of the entrance into such parking structures and shall be no larger than 10 square feet.

E. ANIMATED SIGNS OR SIGNS OF A UNIQUE DESIGN OR ART FORM:

The opportunity for moving, rotating, animated or blinking lights may be permitted for commercial-use signs on major streets by conditional use permit only, provided such lights are small in scale, jewel-like in quality, no more than 6 movement sequences per minute and not across a public right-of-way from or adjacent to a residential structure. Signage proposed under this section must conform to the municipal sign code.

F. PROHIBITED SIGNS: The following signs are prohibited:

- ° Ground signs (freestanding)
- ° Off-premise advertising signs
- ° Signs, inflatable displays or banners on the roof of any structure
- ° Billboards

HERITAGE STRUCTURES

The location of historic and architecturally significant structures are illustrated on Figure 30 and are listed on Pages 44 to 46 of the Marina Urban Design Plan.

1. OBJECTIVES

- ° **Retain the Heritage:** The character and integrity of historic and architecturally significant structures should be preserved and/or rehabilitated where possible.
- ° **Adjacent Development:** New structures should be carefully designed to relate to existing, adjacent heritage buildings.

2. REQUIREMENTS

A. **New Adjacent Development:** New development adjacent to heritage structures should relate to the architectural characteristics of the existing buildings to provide visual continuity and coherence. Visual continuity will be enhanced by consideration of the following elements:

1. **Mass:** New adjacent developments should be of massing that complements the massing of an existing heritage structure. Appropriate techniques may include a stepping down in the height of new structures, or stepping back from the street facade to achieve visual continuity (Figure 53).

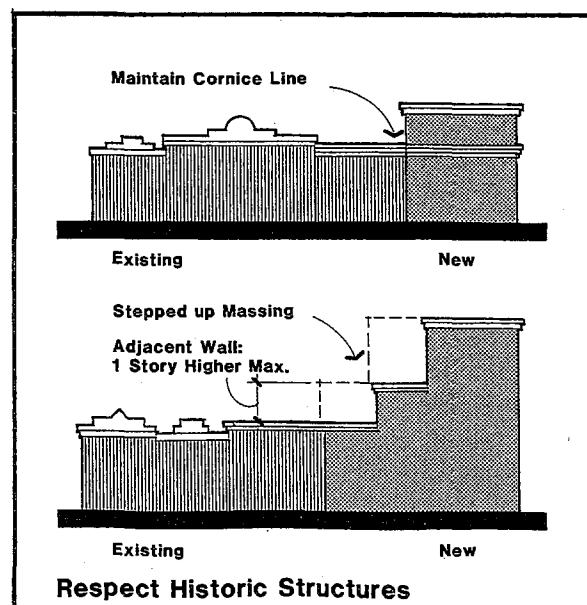


Figure 53
Respect
Historic
Structures

2. Materials & Colors: Similar or compatible building materials and colors should be incorporated in new structures adjacent to existing heritage structures.
 3. Major Architectural Elements: Where appropriate, architectural elements on the facade of heritage structures should be recalled in the design of adjacent new developments. These elements may include the cornice, belt courses, string courses, parapets, windows, arches, etc. It is not intended that all architectural elements be re-used in adjacent new structures, only the most significant that will help to achieve visual continuity.
- B. **End Walls:** End, side and party walls of heritage buildings or blank walls of existing structures which are exposed or may be exposed by the removal of adjacent buildings should be enhanced to be architecturally integrated with adjoining structures or to blend in with other structures in the immediate area; e.g., painting, graphics, murals, etc.

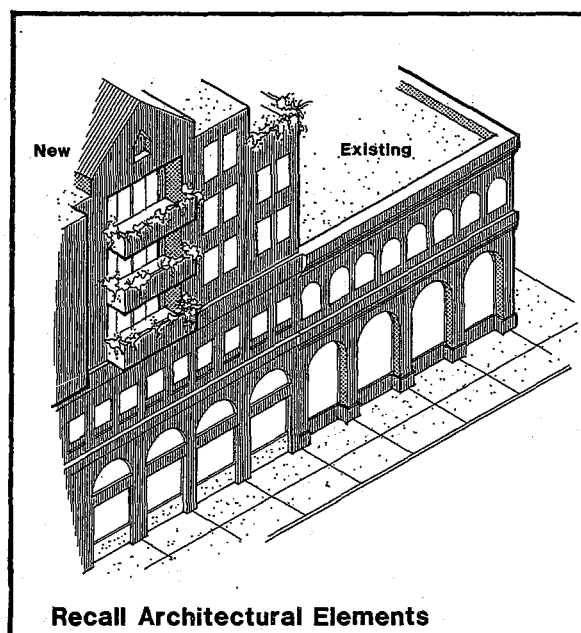


Figure 54
Recall Architectural
Elements

C. **Preservation and Renovation:** No alterations or modifications may be made to historic structures without full review and permit from the appropriate regulatory agencies. Where renovations take place, all applicable codes, laws and regulations shall apply.

D. **Maintenance:** Buildings should be preserved from deliberate or inadvertent neglect.

E. **Additional Guidelines for the Chinese/Asian Thematic Historic District:**

The area bounded by Market Street on the north, "J" Street to the south, Fourth Avenue to the east and mid-block between Second and Third Avenues to the west, as well as the Chinese Mission Building site at 643 First Avenue has been designated Subarea 2 of a Chinese/Asian Thematic Historic District and placed on the local Register of Historic Places. The following guidelines should be considered for historic structures and infill development in this area.

1. **Review Process:** All projects within the area shall be reviewed by the staff of the Historical Site Board for consistency with the Guidelines prior to consideration by the Redevelopment Agency. Historical Site Board review shall be concurrent with review by the City of San Diego Planning Department and Historical Site Board recommendations shall be forwarded to the Planning Commission and the Redevelopment Agency.
2. **Retain the Heritage:** Structures which have been identified for retention due to their historical significance to the City's Chinese/Asian Thematic Historic District and are located within the boundaries described above should be preserved and rehabilitated by retaining or restoring the original fabric and materials consistent with the U.S. Secretary of Interior guidelines.
3. **Materials and Colors:** Facade materials should be wood, stucco or masonry. Consideration may be given to the use of colors or accents that are consistent with Asian color schemes. Examples of such color

are Chinese red, green, yellow, gold for accents, warm pastel colors for buildings.

4. **Major Architectural Elements:** The following architectural details reflect the historic Chinese/Asian buildings and may be considered to enhance the facades of new buildings:

Sloping red tile roofs

Cornice overhangs

Parapets

Corbels

Pediments

Ornate and linear wrought iron balustrades

Wood frame doors and windows

Bay windows on upper stories

Overhanging second story balconies

Tiled entrances at the street floor level.

5. **Signage:** Signage for new structures adjacent to existing heritage structures should be compatible with historic Asian styles.
6. **Permitted Uses:** Heritage structures shall not be subject to the permitted use requirements of other structures in the district.

M A R I N A

An Ordinance Amending Chapter X, Article 3 Of The
San Diego Municipal Code By Adding Division 20,
Relating To The Marina Planned District Regulations.

PLANNED DISTRICT ORDINANCE

Adopted By City Council Ordinance 0-17123

August 1, 1988

Table of Contents

MARINA PLANNED DISTRICT ORDINANCE

Division 20	Marina Planned District	
Section 103.2001	Purpose and Intent	1
Section 103.2002	Boundaries	2
Section 103.2003	Administrative Regulations	3
	A. Administration	
	B. Activities Regulated	
	C. Marina Planned District Permit Process	
	D. Marina Planned District Exception Permit Process	
Section 103.2004	Applicable Planning, Zoning and Subdivision Regulations	7
Section 103.2005	Use Classifications For The Marina District	8
	A. Residential Uses	
	B. Nonresidential Uses	
	C. Mixed Hotel/Residential Development	
	D. Hotel Subarea 2	
Section 103.2006	Property Development Regulations	13
	A. Planning Standards Urban Design Guidelines	
	B. Regulations	
	C. General Provisions	
Section 103.2007	Exceptions To The Provisions Of The Marina Planned District Regulations	23
	A. Authorization	
	B. Conditions For Exceptions	
	C. Review Procedures	
Appendix A	Design Review Procedures Glossary	28
Section 2	Effective Date Of Ordinance	31
Exhibit 1	Land Use	32
Exhibit 2	Hotel/Residential Subareas	33
Exhibit 3	Height Limit	34
Exhibit 4	Floor Area Ratio	35
Exhibit 5	Marina Planned District Map	36

BE IT ORDAINED, by the Council of The City of San Diego, as follows:

Section 1. That Chapter X, Article 3, of the San Diego Municipal Code be and the same is hereby amended by adding Division 20, Sections 103.2001, 103.2002, 103.2003, 103.2004, 103.2005, 103.2006 and 103.2007, to read as follows:

DIVISION 20

MARINA PLANNED DISTRICT

Sec. 103.2001 - PURPOSE AND INTENT

The purpose of the Marina Planned District is to establish development controls that will:

1. Create discrete neighborhoods.
2. Encourage new housing.
3. Conserve heritage buildings.
4. Permit mixed use developments.
5. Provide opportunities for both large- and small-scale development.
6. Guide the location of high-rise development intensity and land use characteristics.
7. Establish strong linkages to the waterfront.
8. Prescribe building mass standards.
9. Establish a strong sense of pedestrian orientation at the street level.

Sec. 103.2002 - BOUNDARIES

The Marina Planned District is within the boundaries of the Centre City Community Plan and is comprised of a portion of the area within the boundaries of the Marina Redevelopment Plan in the City of San Diego, California, designated on that certain Map Drawing No. C-741, filed in the office of the City Clerk under Document No. 757738.

Sec. 103.2003 - ADMINISTRATIVE REGULATIONS

A. Administration

The Centre City Development Corporation shall administer the Marina Planned District as the designee of the City Council in accordance with the provisions of this division.

B. Activities Regulated

1. No building, structure or improvement or portion thereof shall be erected, constructed, converted, established, altered or enlarged, nor shall the use of any building or structure be changed, nor shall any such building, structure or improvement be used or occupied unless it shall comply with the requirements of this division.
2. The Department of Building Inspection shall not issue any permit for such activities in any portion of the Marina Planned District until approval of a Marina Planned District Permit or a Marina Planned District Exception Permit by the Centre City Development Corporation has been obtained signifying compliance with the provisions of this division.

C. Marina Planned District Permit Process

1. Permit Required.

A Marina Planned District Permit shall be required prior to issuance of any City building permit within the Marina Planned District.

2. Application for Planned District Permit.

a. A Marina Planned District Permit shall be issued after the applicant has completed a four (4)-step design review process consisting of the submission of:

- (1) Basic Concept/Schematic Drawings.
- (2) Design Development Drawings.
- (3) Fifty-percent (50%) Construction Drawings.
- (4) One-hundred percent (100%) Construction Drawings.

Based on the size of the project, nature of improvement or the participation of the Redevelopment Agency, Steps 2, 3 or 4 may be consolidated to facilitate review of the project at the discretion of the Centre City Development Corporation.

b. The criteria for submitting Basic Concept/Schematic Drawings and a description of the design review process are contained in Appendix A.

3. Review Procedures.

Application for a Marina Planned District Permit shall begin with the submission of Basic Concept/Schematic Drawings to the Centre City Development Corporation. These shall be reviewed by:

- ° The Centre City Development Corporation-Resident Advisory Committee.
- ° The Centre City Development Corporation Board.
- ° The City Planning Commission.
- ° The Redevelopment Agency of the City of San Diego.

Interested citizens and area residents may review and comment on individual projects prior to or at the time of the above reviews. The Planning Commission shall review the Basic Concept/Schematic Drawings and make recommendations to the Redevelopment Agency within thirty (30) days from the receipt of said Drawings.

4. Determination - Conditions of Approval.

The Centre City Development Corporation and Redevelopment Agency may approve, modify or disapprove any application initiated by the submission of Basic Concept/Schematic Drawings for a Marina Planned District Permit. In approving the Marina Planned District Permit, the Centre City Development Corporation may propose to the Agency reasonable conditions to ensure compliance with these regulations.

5. Conditional Planned District Permit.

The Redevelopment Agency's approval of the Basic Concept/Schematic Drawings constitutes a Conditional Planned District Permit. Following the Agency's approval, the applicant shall submit the Design Development Drawings, Fifty Percent (50%) Construction Drawings and One Hundred Percent (100%) Construction Drawings to the Centre City Development Corporation for its approval.

6. Issuance of Planned District Permit.

If the Centre City Development Corporation finds that the One Hundred Percent (100%) Construction Drawings conform to the conditions approving the Basic Concept/Schematic Drawings, then the applicant may apply for other necessary permits from the Building Inspection Director, Planning Director or City Engineer.

7. Permit Time Limits and Time Extensions.

Any permit approved under this division will be effective for a period of two (2) years. One (1) time extension of one (1) year may be given if the Centre City Development Corporation finds from the evidence submitted that there have been no material changes of circumstances since the permit was originally granted.

8. Revocation.

The Centre City Development Corporation may, at any time, revoke a Marina Planned District Permit issued under this Section. Prior to such a revocation, the holder of the Marina Planned District Permit shall be given a hearing after thirty (30) days' notice. After such a hearing, the Centre City Development Corporation may revoke a permit upon determining:

- a. That the building or structure for which the permit was issued is being used for purposes other than or in addition to the purposes and manner described in the permit application.
- b. That such use or operation of the building or structure for which the permit was issued is contrary to these regulations.
- c. That the building or structure for which the Marina Planned District Permit was issued was not constructed in compliance with the terms and conditions of the Marina Planned District Permit.

D. Marina Planned District Exception Permit Process

Exceptions sought under Sec. 103.2005 shall be applied for concurrently with the Planned District Permit process set forth in Sec. 103.2003.C. If exceptions are granted a Planned District Exception Permit shall be affixed to the Marina Planned District Permit.

Sec. 103.2004 - APPLICABLE PLANNING, ZONING AND SUBDIVISION REGULATIONS

Where not otherwise specified in this division, the provisions of the San Diego Municipal Code, Chapter X, Article 1, Divisions 1 through 3, 5, 6 and 8 through 18 and Chapter X, Article 2 shall apply. All other provisions of Chapter X, Article 1 of the San Diego Municipal Code are superseded by the regulations of this division. Where there is a conflict between the provisions of Chapter X, Article 1 and Chapter X, Article 2 and the provisions of this division, the provisions of this division shall apply. All applicable building code requirements must be met for construction alteration, addition or repair of a building.

Sec. 103.2005 – USE CLASSIFICATIONS FOR THE MARINA DISTRICT

In the area designated eighty percent (80%) residential/twenty percent (20%) nonresidential (Exhibit 1), at least eighty percent (80%) of the gross floor area shall be residential use and up to twenty percent (20%) of the gross floor area may be nonresidential use. The total of all corridor, storage, utility, parking and other support space shall be allocated to residential or nonresidential on an eighty percent (80%) residential/twenty percent (20%) nonresidential basis.

A. Residential Uses

The following permanent residential uses are permitted in the Marina Planned District:

1. Multi-family.
2. The following uses may be considered by conditional use permit if the use is the primary residence of the occupants and the major use of the structure:
 - ° Single Room Occupancy.
 - ° Live/Work Quarters.

B. Nonresidential Uses

The following nonresidential uses are permitted in the Marina Planned District for that portion of the area designated twenty percent (20%) nonresidential:

1. Eating and Drinking Establishments.

Businesses serving prepared food or beverages for consumption on or off the premises.

2. Food Sales.

Retail sales of prepared food or food for home preparation including bakeries, candy stores, ice cream stores, delicatessens, grocery stores and supermarkets.

3. Small Office—Business and Professional Services.

Services in this category include offices such as architectural design, medical-dental, travel or administrative services, real estate, insurance and legal offices.

4. Personal and Convenience Services.

Services in this category include those of a frequent reoccurring nature such as barber and beauty shops, drug stores, dry cleaning, self-service laundries, shoe repair and tailors.

5. Conditional Use.

The following uses may be considered by conditional use permit if determined to be compatible with the adjoining residential use:

a. Cultural/institutional.

b. Religious assembly.

c. Schools, public/private and day care centers.

d. Recreation-health facilities.

e. Business and Home Services

Establishments providing appliance repair, office machine repair, building maintenance (janitorial), upholstering, graphic design, drafting, blueprinting, typesetting, printing, copying or photographic services.

f. Broadcasting, recording and other communication services accomplished through electronic or telephonic mechanisms.

This classification includes radio, television or recording studios, telephone switching centers and telegraph offices, but excludes receiving towers and satellite dishes.

g. Personal Improvement Services

Provision of instructional services or facilities, including photography, fine arts, crafts, dance or music studios, driving schools, business and trade schools, reducing salons and fitness studios.

h. Retail Sales.

(1) Arts and Crafts.

Establishments selling art objects, art supplies, antiques, jewelry, handmade glass, custom photographs, pottery, porcelain, leather goods and similar products. This classification includes art galleries, antique shops, ceramic studios, craft shops, jewelry design and creation studios and photo studios.

(2) Home Furnishings and Hardware.

Establishments selling awnings and canvas, home appliances, carpets and floor coverings, hardware, furniture, kitchen utensils, paint and wallpaper.

(3) Other Retail Sales.

This classification includes small department stores, drugstores, dispensing opticians, clothing stores and fabric stores and businesses retailing the following goods: toys, hobby materials, books, cameras, photographic supplies, electronic equipment, records, sporting goods, shoes, office supplies, stationery, medical supplies, bicycles and new automotive parts and accessories (excluding service and installation).

(4) Display windows provided, however, that such windows have a minimum depth of approximately three (3) feet and displays are changed every sixty (60) days.

i. Existing Structures.

Historic and/or architecturally significant structures which are restored, recreated or adaptively reused may be developed entirely with nonresidential uses and are exempt from the requirements to provide residential units.

j. Utility Substations.

Structures and facilities which provide franchised or city utility services to land use and transportation systems located within the Marina Planned District.

C. Mixed Hotel/Residential Development

In the area designated Subarea 1 (Exhibit 2), mixed uses including hotel and residential are permitted.

1. Subarea 1 permits hotel use if accompanying residential use is provided.
2. Development of the entire two (2) block site requires one hundred fifty (150) residential dwelling units. Development of individual blocks requires a minimum residential density of eighty-five (85) dwelling units per acre (dupa).
3. Specialty commercial and entertainment uses may be permitted on a conditional use basis.
4. Residential development may be provided as an alternate to hotel use. Such development shall allocate at least eighty percent (80 %) of the gross floor area to residential use and may allocate up to twenty percent (20%) of the gross floor area to nonresidential land use from the listed uses provided for in this section. Exceptions to the percentage ratio of eighty percent (80 %) residential/twenty percent (20%) nonresidential are contained in B.5, "Conditions for Exceptions," Section 103.2006.

D. Hotel Subarea 2

In the area designated Subarea 2 (Exhibit 2), the following hotel uses are permitted:

1. Subarea 2 permits hotel use and ancillary hotel uses such as meeting rooms, food establishments and gift shops.
2. Residential development may be provided as an alternate use; such development shall allocate at least eighty percent (80 %) of the gross floor area to residential use and may allocate up to twenty percent (20%) of the gross floor area to nonresidential land use from the listed uses provided for in this section. Exceptions to the percentage ratio of eighty percent (80 %) residential/twenty percent (20%) nonresidential are contained in B.5, "Conditions for Exceptions," Section 103.2006.
3. Specialty commercial may be permitted on a conditional use permit basis.

Sec. 103.2006 - PROPERTY DEVELOPMENT REGULATIONS

A. Planning Standards and Urban Design Guidelines

Concurrent with the adoption of this division, the Redevelopment Agency shall by resolution also adopt architectural and design standards used in evaluating the appropriateness of any development for which a permit is applied under this division. Such architectural and design standards shall be entitled, "Marina Urban Design Plan and Development Guidelines," a copy of which is on file in the office of the City Clerk as Document No. 00-17 123.

B. Regulations

The following regulations shall apply to the specific areas shown on accompanying maps:

1. Mixed Residential/Nonresidential Land Use Areas.

The major land use in the Marina Planned District shall be residential (Exhibit 1). In the area designated eighty percent (80 %) residential/twenty percent (20%) nonresidential (except in structures fifty (50) feet or less in height), at least eighty percent (80 %) of the gross floor area shall be residential use and up to twenty percent (20%) of the gross floor area may be nonresidential. Where structures are fifty (50) feet or less in height or meet the height requirements of the Uniform Building Code for Type V construction, gross floor area in nonresidential use may exceed twenty percent (20%) if the entire nonresidential use is accessible to and located at the street level of the project. The permitted uses are described in Section 103.2005. Exceptions to the percentage ratio of eighty percent to twenty percent (80 %-20%) for high-rise structures are contained in Section 103.2006.B.5., "Conditions for Exceptions."

2. Permitted Heights .

The heights in the Marina Planned District range from fifty (50) feet to three hundred (300) feet as illustrated in Exhibit 3.

The intent of height limits as established in this ordinance is to guide the location of vertical development within the Marina Redevelopment Project area to accomplish the following objectives:

- Enhance view corridors.
- Provide variety in the provision of dwelling unit types.
- Create distinct residential neighborhoods.
- Conserve the character of existing residential development.
- Minimize the impact of shadow on existing and future development.

3. Exceptions to Height Limits.

Exceptions to height limits may be permitted as follows:

- Fifty (50) foot height limits may be increased to a maximum of ninety (90) feet.
- Ninety (90) foot height limits may be increased to a maximum of one-hundred twenty (120) feet.
- Heights designated one-hundred twenty (120) feet or greater as illustrated on Exhibit 3 may be increased without a maximum height.

The following criteria shall be used to evaluate requests for height exceptions.

- Provision of one (1) or more parks, setback areas or widened and enhanced public rights-of-way. Such areas shall be landscaped by the developer. Their location shall complement the adjoining public right-of-way and while either public or private in nature, should be designed to be visually or physically enjoyed by residents, residents of adjoining structures and the general public.

- ° Increased nonresidential or residential activity at the street level of the development from fifty percent (50%) of the total frontage to all the remaining street frontage with the exception of vehicular access and truck service delivery to serve the site.
- ° Such activity must be directly accessible to the public right-of-way. Entrances to activity must be provided at intervals which are approximately fifty (50) feet or less in distance apart.
- ° Accommodation of all or a substantial amount of all parking to serve the proposed development below grade.
- ° Mitigating the mass and scale of a project by reducing the size of the floor plate and creating a more slender tower which enhances view corridors or reduces the effect of shadow on adjoining developments.

The procedure for considering exceptions to height are subject to Section 103.2007, "Exceptions to the Provisions of the Marina Planned District Ordinance."

4. Floor Area Ratios (FARs).

A FAR of 3.5 to 4.0, 4.0 to 4.5, 5.0 to 5.5, 6.0 to 6.5 and 8.0 to 8.5 shall be the maximum development intensity in the Marina Planned District for those areas identified in Exhibit 4 with the following exceptions:

- a. Existing structures which are historically or architecturally significant, which are restored or adaptively reused, may transfer unused FAR to contiguous development within the block if such structures are part of an integrated/architectural development for the total site.
- b. Atria or common interior/enclosed space shall be included in the calculation of FAR. For each floor that an atrium penetrates through a structure, the square footage per floor shall be included in the calculations of the FAR.

5. Conditions for Exceptions.

An increase in FAR in the amount of 0.5 of the base FAR may be permitted as follows:

<u>Base FAR</u>	<u>Permitted Increase in FAR</u>
3.5	4.0
4.0	4.5
5.0	5.5
6.0	6.5
8.0	8.5

An increase in the ratio of nonresidential use in the amount of five percent (5%) may be permitted as follows:

<u>Base Land Use Mix</u>	<u>Permitted Land Use Mix</u>
80% Residential/20% Nonresidential	75% Residential/25% Nonresidential

Exceptions to either FAR or mixed land use standards may be granted by the Centre City Development Corporation Board and the Redevelopment Agency where the project complies with the development standards contained in this ordinance and the Marina Urban Design Plan and Development Guidelines provided that three (3) of the following seven (7) conditions are met.

In the event that exceptions are requested for both land use mix and FAR, four (4) of the seven (7) conditions must be met.

- a. Development is infilled on sites or blocks which contain historic or architecturally significant structures and/or where historic or architecturally significant buildings are rehabilitated and integrated into the proposed new development.
- b. The average size of twenty-five percent (25%) of all units exceed one thousand (1,000) square feet.

- c. That a minimum of ten percent (10%) of all units contain three (3) bedrooms.
- d. The required ground floor activity increases from fifty percent (50%) to sixty percent (60%) of the street frontage. Such increase in activity shall be consumer-oriented commercial or residential land use.
- e. At least one full level of underground parking is provided.
- f. The site is a receiver of a major public amenity including park, plaza, public art and sculpture or other equivalent amenity designed primarily for public use.
- g. No Redevelopment Agency financial assistance, excluding off-site public improvements, is required for the project.

C. General Provisions

This section defines the general provisions of the Marina Planned District ordinance. Exceptions to the criteria contained in this section may be considered subject to the procedures established in Sec. 103.2007. The following regulations apply to all areas:

- 1. Minimum lot requirements - five thousand (5,000) square feet.
- 2. Minimum side yard, interior and rear yard setbacks - none.
- 3. Development pattern.

A variety of development sites (single full block development and blocks with multiple developments) shall be encouraged throughout the project area. Guidelines for smaller site development are contained in the Marina Urban Design Plan and Development Guidelines.

4. Permitted Projections

- a. Subject to encroachment permits as may be required by the City of San Diego, projections such as balconies and window bays are permitted to encroach up to four (4) feet into the public right-of-way starting at no less than twelve (12) feet above street level or above the height of the first floor.
- b. Projections from tower elements which are set back from the street may be considered by Conditional Use Permit.
- c. Surface structures within the public right-of-way up to three (3) feet from the curb.

5. Signs.

Proposed signage will be evaluated for conformance to the signing objectives of the Marina Urban Design Plan and Development Guidelines and the City of San Diego for compliance with Chapter X, Article 1, Division 11 and Chapter IX, Article 5 of the San Diego Municipal Code.

6. Streetwall.

- a. All buildings shall have a minimum streetwall of two (2) stories or twenty-five (25) feet.
- b. All buildings shall have a maximum streetwall of fifty (50) feet or the height of an adjoining existing structure which is a part of an integrated development.
- c. At least fifty percent (50%) of each side of the total ground floor frontage of all new or reconstructed first story building walls that face a public street shall be devoted to pedestrian entrances, residential use, or windows affording views into retail consumer services, offices or lobby space or display windows. All blank facades shall be enhanced by architectural detailing, artwork, landscaping or similar features having visual interest.

- d. At least seventy-five percent (75%) of the street facade of the building base shall be constructed to the street property line.
- e. Residential/nonresidential activities located at the street level shall provide one direct at-grade entrance from the public right-of-way for each street frontage. Where such frontages exceed seventy-five (75) feet, one entrance shall be provided each seventy-five (75) feet of frontage or portion thereof. Such entrances shall conform to the State of California Administrative Code, State Building Code, Part 2, Title 24 (Handicapped requirements).

7. Setbacks.

- a. The tower portion of any structure shall be setback at the podium level at least twenty (20) feet from the base of the streetwall on all street frontages except one as may be required for fire/safety access.
- b. Two (2) or more towers on the same parcel shall be separated by at least forty (40) feet.
- c. The tower element of mid-rise and high-rise structures that share a common property line with another parcel shall be set back at least twenty (20) feet from that common property line.
- d. All properties fronting on the proposed linear park shall observe a minimum landscaped setback of fifty (50) feet.

8. Height.

- a. The height of any structure over one-hundred sixty (160) feet shall observe a minimum height-to-width ratio of 1-width to 1.6-height.
- b. The average floor plate for the top 1/3 of any tower structure over one hundred sixty (160) feet shall diminish in size to create a slimmer silhouette; such reduction shall be in scale with the lower 2/3 of the structure.

9. Open Space Required.

- a. All development shall maintain a minimum of thirty percent (30%) of the site open to the sky, occurring at grade and/or podium levels.
This area including courtyards and terraces shall be considered common open space.
- b. Private Open Space. Seventy-five percent (75%) of all multi-family residential units above the first floor shall have a private open space of thirty-six (36) square feet or five percent (5%) of the unit's gross floor area whichever is larger. The private open space shall have a minimum dimension of three (3) feet.
- c. First floor residential units which front on common interior courtyards may utilize up to twenty-five percent (25%) of the interior area for private enclosed open space related to individual units.
- d. Hotels. Balconies shall be provided for at least twenty-five percent (25%) of all hotel rooms.
- e. Private open space or common interior courtyards open to the sky shall not be calculated in the FAR of any structure.
- f. No structure shall be sited in such a way so as to terminate a skyview down an existing street or potential view of San Diego Bay from a street in a southerly or westerly direction.

10. Screening and Landscape.

- a. All refuse storage and mechanical equipment shall be screened by walls, fences, buildings or combinations thereof to a height of six (6) feet.
- b. All on-site open space or setback areas shall be landscaped or architecturally enhanced.
- c. Mechanical equipment or appurtenances on the roof shall be architecturally screened or enclosed and/or painted to blend with the roof surface to mitigate the view of cluttered roof surfaces.

11. Parking.

- a. All above grade parking structures shall be architecturally integrated into the structure and screened from view by landscaping, architectural detailing, or buffered by residential or nonresidential use.
- b. Outdoor or garage lighting shall be indirect and diffused.

12. Off-Street Loading.

- a. Off-street loading facilities shall be required for all developments which exceed a gross floor area of one-hundred thousand (100,000) square feet.
- b. All off-street loading areas shall be screened from view.
- c. All trash container areas shall be enclosed within and integrated into the structure and not visible from the public right-of-way.

13. Building Materials.

- a. With the exception of the first level, no more than fifty percent (50%) of each of the facades of the structure shall be vision glass or spandrel construction of material which is up to thirty percent (30%) in reflectivity. The remaining fifty percent (50%) may be masonry or stone or other appropriate material.
- b. Clear or lightly tinted glass for doors and display window shall be installed at the first or pedestrian level.

14. Roof Tops.

- a. No exposed rooftop parking shall be permitted.
- b. Roof surfaces shall be simplified and appurtenances shall be grouped and screened.
- c. Flat exposed roofs at any level shall be designed as an architectural/landscape amenity to enhance the views from the proposed structure or adjacent structures. Such enhancement may consider roof gardens, architectural features, special pavings and patterns or other comparable treatment.

15. Solar Access.

- a. No more than fifty percent (50%) of the area of a sidewalk on the opposite side of a development shall be shaded by that development for no more than one (1) hour between 11:00 a.m. and 2:00 p.m.
- b. The location of existing and proposed development shall be considered in the siting of new structures in order to maximize to the extent possible, views and sun penetration.

**Sec. 103.2007 - EXCEPTIONS TO THE PROVISIONS OF THE MARINA PLANNED
DISTRICT REGULATIONS**

- A. The Centre City Development Corporation and the Redevelopment Agency are authorized to permit exceptions from certain limits, restrictions and controls of this ordinance as provided in Sections 103.2006. B.3. and 103.2006.C.
- B. Conditions under which exceptions to this division may be considered will be based on the developer's demonstration that the implementation of a proposed exception will meet the criteria described below. Application for exception shall be submitted concurrently with application for a Marina Planned District Permit in accordance with Sec. 103.2003.D (Marina Planned District Exception Permit Process) and shall include:
1. Demonstration that the proposed exceptions implement the intent of this Marina Planned District, comply with the objectives of the Marina Urban Design Plan and Development Guidelines and are design solutions which enhance the livability of the Marina Planned District.
 2. The granting of exceptions to the division does not adversely impact the development of the residential community.
 3. The granting of an exception will have a beneficial impact on the residential community.
 4. The granting of an exception will not establish an adverse precedent for the consideration of future requests for exception.
 5. The design of the proposed development is distinctively San Diegan and architecturally superior. Such distinction will be evaluated against the objectives of the Marina Urban Design Plan and Development Guidelines and the following:
 - The degree to which the architecture characterizes the historical context of San Diego as contrasted to any other location.

- ° Incorporation or adoption of design elements from San Diego's architectural past into the proposed development.
- ° Innovation or creativity of the project design.

C. Review Procedures.

Application for exceptions to the provisions of the Marina Planned District Regulations shall follow the review procedures outlined in Sec. 103.2003, Administrative Regulations, with the following additional requirements:

Two (2) public hearings are required, the first before the Centre City Development Corporation Board of Directors and a subsequent hearing before the Redevelopment Agency of the City of San Diego. Notice shall be given at least ten (10) days prior to the hearings of the time, place and purpose of such hearing in the following manner:

1. By publication in the official City newspaper and by mail to all property owners of record within three-hundred (300) feet of the property under consideration. This notice shall also be mailed to any group(s), organization or individual which has requested notification by written request to the Centre City Development Corporation.
2. Submission of an application fee of two hundred dollars (\$200) to cover the cost of noticing.
3. The notice shall be prepared as required under Sec. 101.0220 of the San Diego Municipal Code.

APPENDIX A

Design Review Process

The Centre City Development Corporation will review all Basic Concept/Schematic Drawings, Design Development Drawings, Fifty Percent (50%) Construction Drawings and One Hundred Percent (100%) Construction Drawings as identified in the Marina Planned District regulations. The following narrative describes the design review process:

Basic Concept/Schematic Drawings

- ° This design submittal shall illustrate the basic organization of the site. Plans are reviewed for two-dimensional considerations such as the relationship of land use within the project, relationship of the project to proposed and existing land uses adjoining the site, siting considerations such as vehicular and pedestrian circulation, provision for public open space and three-dimensional images of the project. A narrative explanation of the design concept should be provided. Submission of the Basic Concept/Schematic Drawings should consist of the following specific items:
 1. Description of the development concept including the density, dwelling unit composition, gross area devoted to specific land use, number of floors, type of construction and floor area ratio (FAR).
 2. Site plan at a scale no smaller than one thirty-second equals one inch ($1/32" = 1'$) inch equals one foot which indicates the relationship of the proposed project to adjoining development within approximately three-hundred (300) feet and which illustrates the dimensions of the site and the proposed project.
 3. Ground floor plan which illustrates subsurface and ground floor plans at a scale not smaller than one sixteenth equals one foot ($1/16" = 1'$).
 4. Two (2) project sections at a scale not smaller than one sixteenth equals one foot ($1/16" = 1'$).

5. Exterior concept elevations of each street frontage.
6. One (1) exterior perspective drawn from a street level view.
7. Tabulation of the net leasable and gross building area including floor area ratio, building coverage, common open space areas, private open space, total area devoted to parking and number of spaces.
8. Preliminary identification of materials, finishings, colors and landscaping.
9. Preliminary off-site improvements, landscape and grading plans which illustrate how the design elements of on-site public spaces are coordinated with the off-sites.
10. Preliminary evaluation of environmental factors such as sun and shade on open space and adjoining properties.
11. Statement of conformity to or variation from the Marina Planned District regulations.
12. Massing model which illustrates the scale and architectural design concept of the project.
 - ° Design Development Drawings - These are a refinement of the Basic Concept/Schematic Drawings in which precise design elements of the project are determined. This submittal includes accurate site surveys, floor plans, elevations, sections, design details and a palette of exterior colors and materials. Other considerations include pedestrian and vehicular circulation, provision for servicing, utility infrastructure and exterior architectural and urban design features.
 - ° Fifty Percent (50%) Construction Drawings - These are expected to resolve concerns identified during the Design Development Drawings review. Additional material submitted for this review shall include off-site improvement drawings and landscape plans.

- ° One Hundred Percent (100%) Construction Drawings - These represent the final plans, specifications and other documentation for the proposed project. These drawings shall illustrate how the conditions approving the previous submissions have been accommodated. These drawings are in sufficient detail to obtain a building permit.

GLOSSARY

ATRIUM: An opening through two (2) or more floor levels other than enclosed stairways, elevators, hoistways, escalators, plumbing, electrical, air-conditioning or other equipment, which is enclosed at the top.

BUILDING HEIGHT: The linear measurement of a structure's height from the highest topographic elevation of the property line contiguous with a public right-of-way vertically to the cornice line above the uppermost habitable floor.

BUILDING MATERIALS: All materials visible from the exterior of a development, including materials used for walls, roofs, structure windows, doors, architectural or decorative features applied to the facade and trim.

COMMON OPEN SPACE: Those usable spaces commonly accessible to all residents and users of the building.

CONDITIONAL PLANNED DISTRICT PERMIT: A term applied to a permit which conditions development and is in effect after the Redevelopment Agency approves the Basic Concept/Schematic Drawings. Signals preliminary approval and allows the completion of the design review process.

CULTURAL/INSTITUTIONAL: Any use which provides a public or quasi-public function or which is non-profit.

FLOOR AREA: The area included within the surrounding exterior walls of a building or portion thereof, exclusive of vent shafts and courts. Floor area shall not include areas below grade which are considered basements under the Uniform Building Code.

FLOOR AREA RATIO (FAR): The ratio of total enclosed building area including parking at and above grade to the area of the site. The FAR is an indication of the intensity of development; as such, it identifies the upper limit of building space permissible on a given lot.

HEALTH FACILITIES: Club rooms, gymnasias and fitness centers contained within a structure and ancillary to residential or commercial use.

HEIGHT-TO-WIDTH RATIO: The ratio described for a high-rise building in which the vertical dimension of all facades is greater than 1.6 times the widest horizontal dimension.

HIGH RISE: A building exceeding ninety (90) feet in height.

HIGH-RISE BUILDING ELEMENTS: High-rise buildings have the following elements: base, tower and roof. The tower may be subdivided into transition floors, a lower tower and an upper tower.

LAND USE MIX: A percentage distribution of a building's gross floor area devoted to residential or nonresidential uses.

LIVE/WORK QUARTERS: An area comprised of one (1) or more rooms or floors in a building which includes: (1) cooking space and sanitary facilities, and (2) working space reserved for persons residing therein. Not over thirty-three percent (33%) of each live/work quarters shall be used or arranged for residential purposes such as a sleeping area, kitchen, bathroom and closet areas.

LOW-RISE: A building with maximum height of fifty (50) feet.

MASS AND SCALE: The visual perception of the organization of the gross floor area of the structure compared to adjoining development.

MID-RISE: A building ranging in height from fifty (50) feet to ninety (90) feet.

MIXED USE DEVELOPMENT: Developments in which two (2) or more major land uses are permitted.

MULTI-FAMILY RESIDENTIAL: A building used or designed to be used for housing three (3) or more families.

PLANNED DISTRICT EXCEPTION PERMIT: Allows construction of projects which vary from the requirements of the Marina Planned District regulations but comply with the Marina Urban Design Plan and Development Guidelines and facilitate the establishment of a residential community.

PLANNED DISTRICT PERMIT: A term applied to all permits which are required pursuant to the Marina Planned District regulations. This permit is issued after the Centre City Development Corporation determines that a project's One Hundred Percent (100%) Construction Drawings conform to the Marina Planned District regulations. The Marina Planned District Permit allows the applicant to apply for all other permits required for construction of a project.

PRIVATE OPEN SPACE: Those usable spaces accessible only by the residents of a single dwelling unit.

PUBLIC OPEN SPACE: Those usable spaces accessible by the general public.

RECREATIONAL FACILITIES: Outdoor facilities ancillary to a residential complex, including swimming pools, saunas and courts.

REFLECTIVE GLASS: A glazing material which obscures vision and has limited transparent qualities.

RELIGIOUS ASSEMBLY: Facilities for religious worship and incidental religious education.

RESIDENTIAL DENSITY: A measure of housing, expressed in dwelling units per acre (dupa).

SCHOOLS -- PUBLIC & PRIVATE: A building housing students for the primary purpose of education.

SETBACK: A horizontal separation between a vertical element and a property line.

SINGLE ROOM OCCUPANCY (SRO): Any hotel room within a hotel intended or designed to be used, or which is used, rented or hired out to be occupied for sleeping purposes by guests and which is also the primary residence of such guests. The term shall not include any hotel room which is used by transient guests who do not occupy such hotel room as their primary residence.

SKYVIEWS: A horizontal and near-horizontal view of the sky. Often in San Diego, such views include the Coronado Bridge.

SMALL SITE DEVELOPMENT: A designation applied to a block in which multiple developments occur.

STEPBACK: The horizontal separation between two (2) major vertical elements. Stepbacks occur at upper levels.

STREET LEVEL: Any access opposite the public right-of-way which ranges from four (4) feet below grade to five (5) feet above grade.


STREETWALL: The facade of buildings, or frontage, along property line shared with a public right-of-way.

TOWER: Any structure which exceeds a height of ninety (90) feet.

Section 2. This ordinance shall take effect and be in force on the thirtieth day from and after its passage.

APPROVED: JOHN W. WITT, City Attorney

By


Frederick C. Conrad
Chief Deputy City Attorney

10/07/87

05/05/88 REV. 1

07/22/88 REV. 2

08/30/88 REV. 3

09/22/88 REV. 4

EXHIBIT 1 Land Use

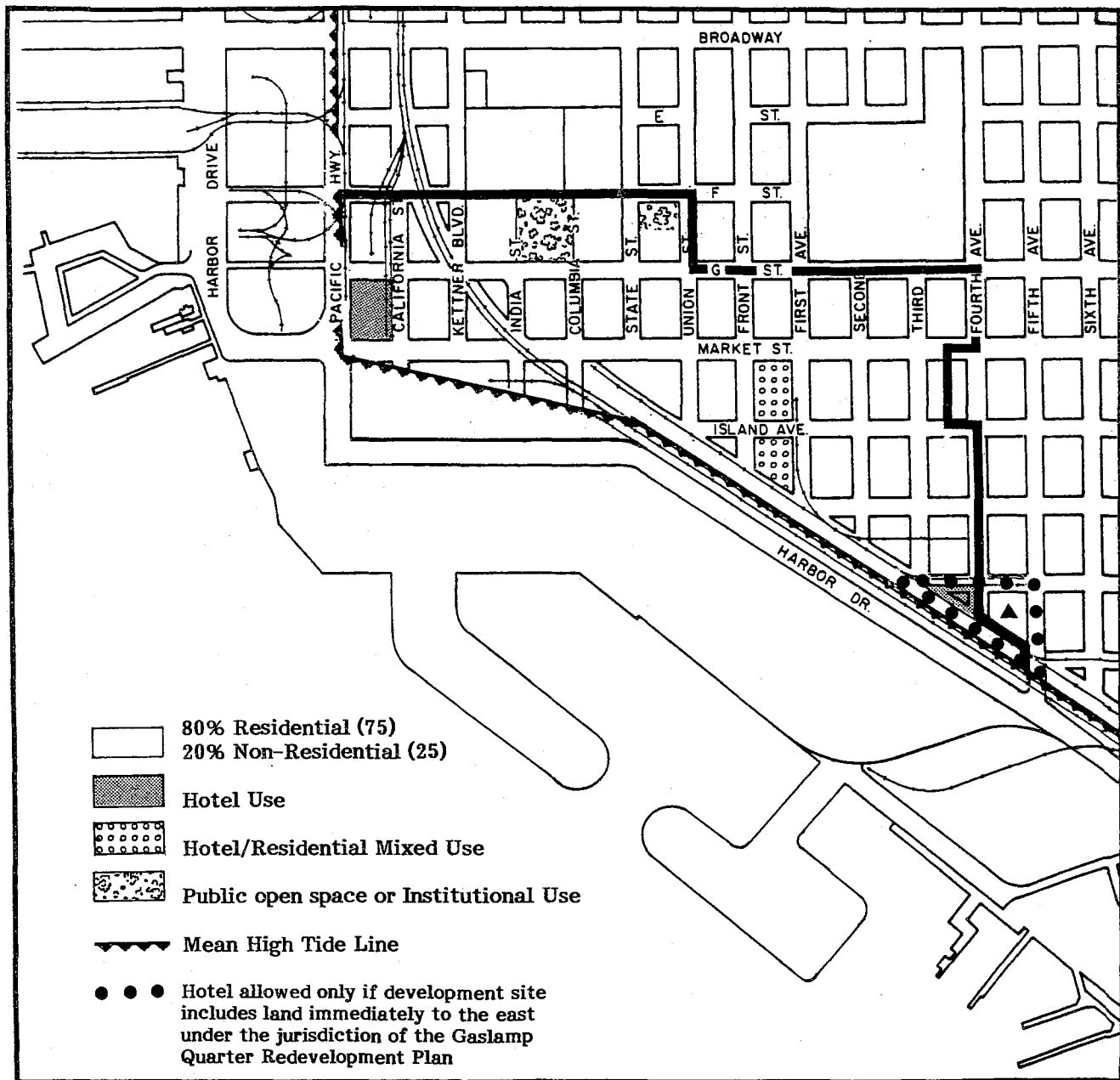


EXHIBIT 2
Hotel/Residential Subareas

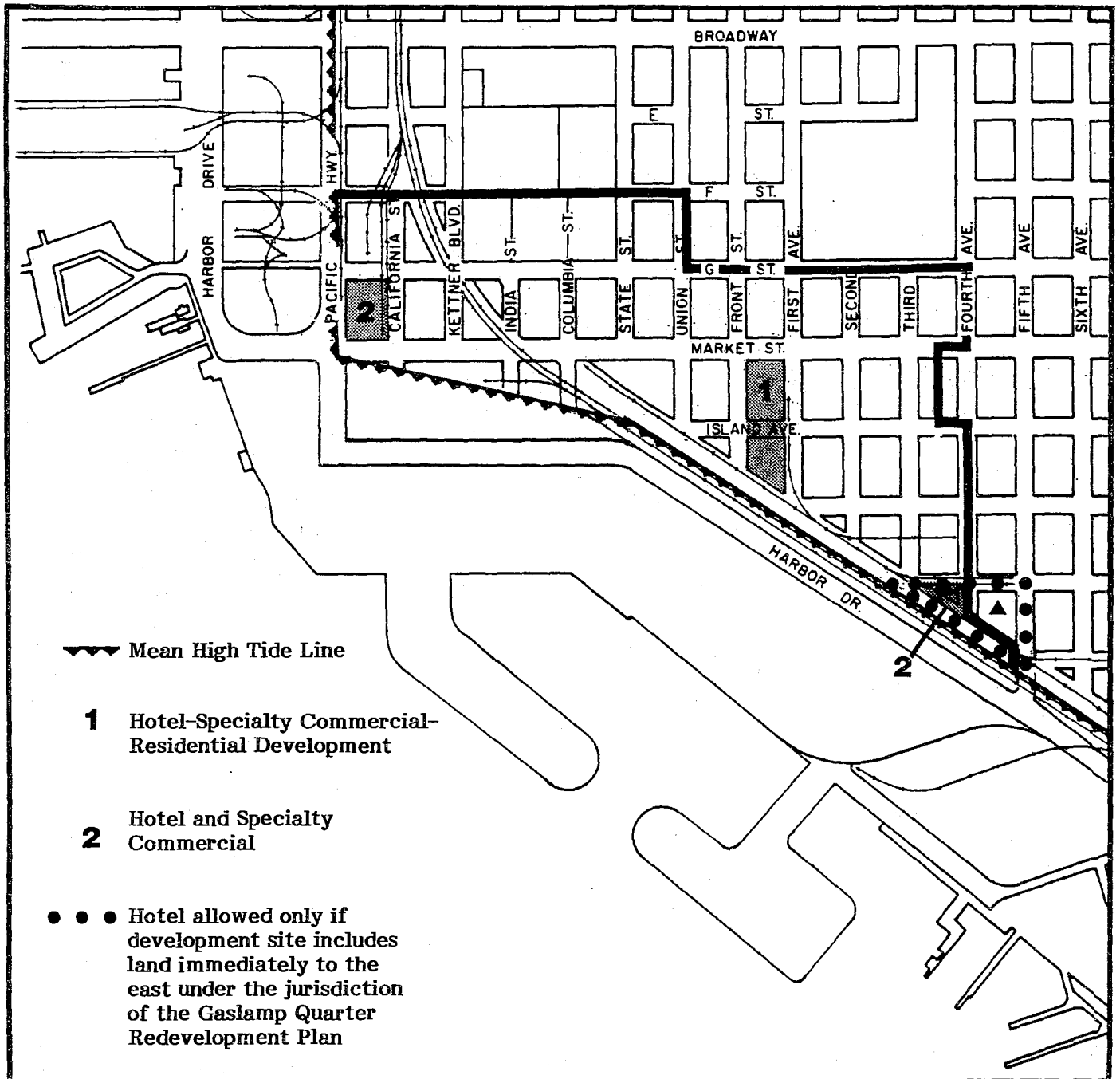
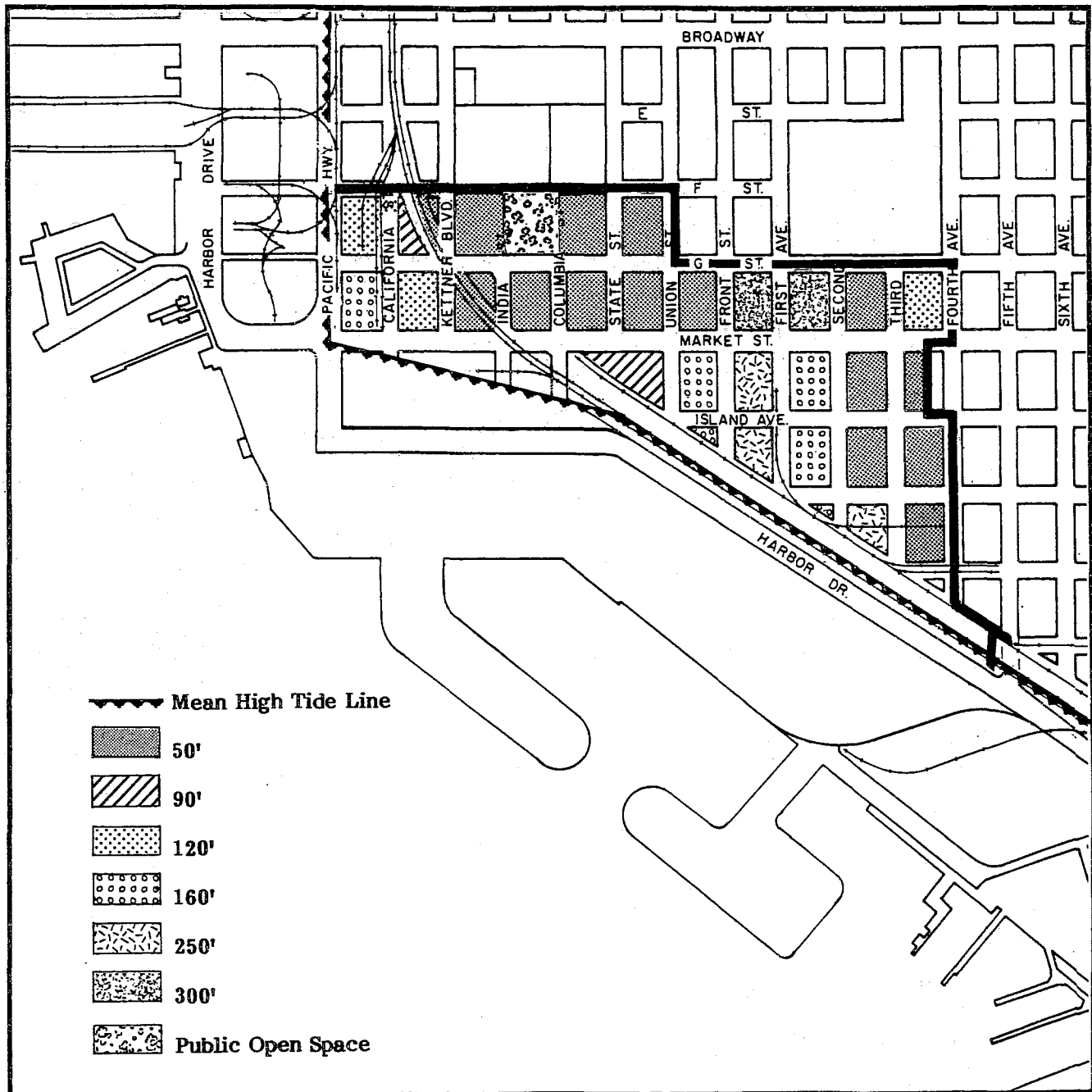
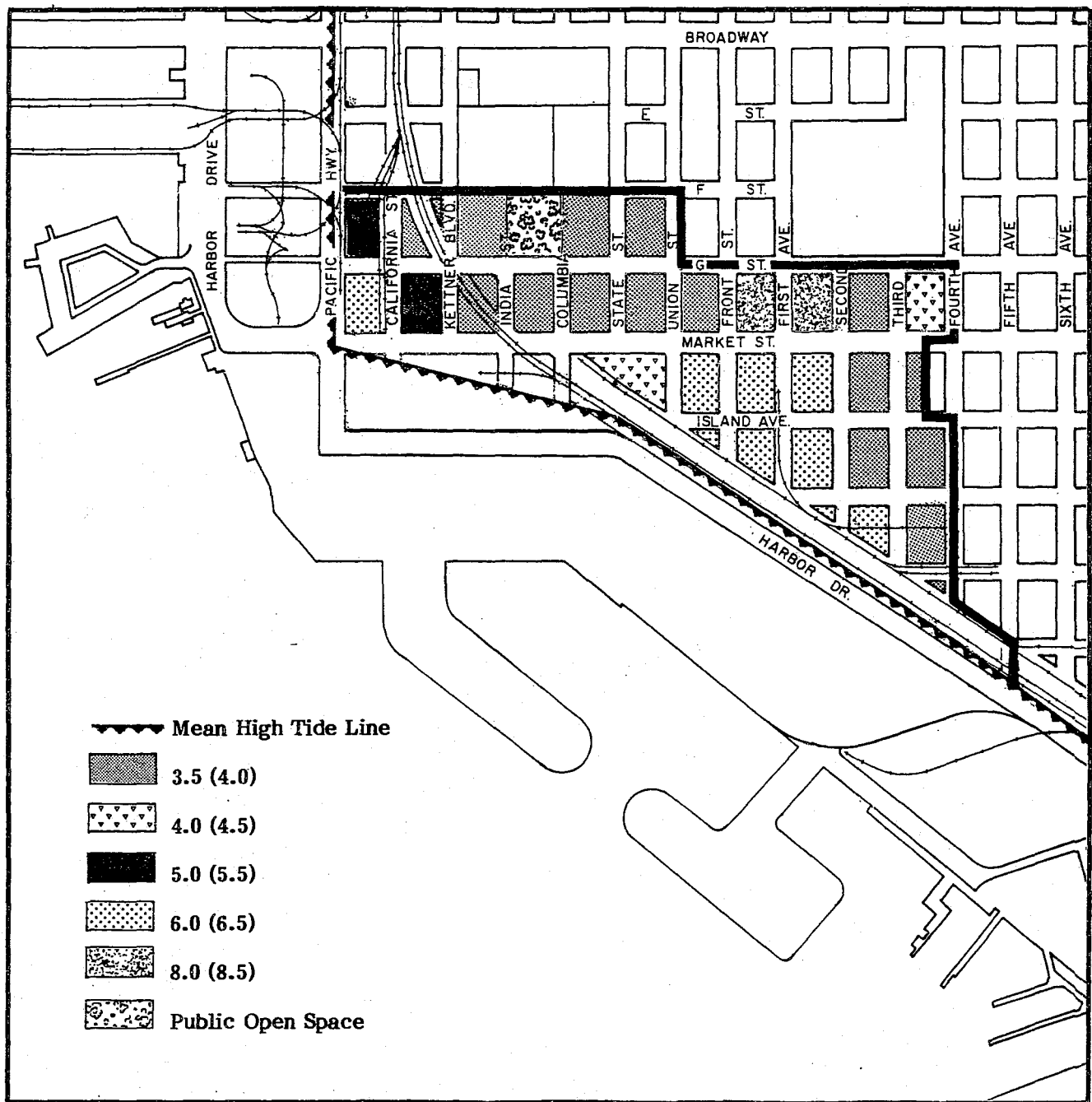


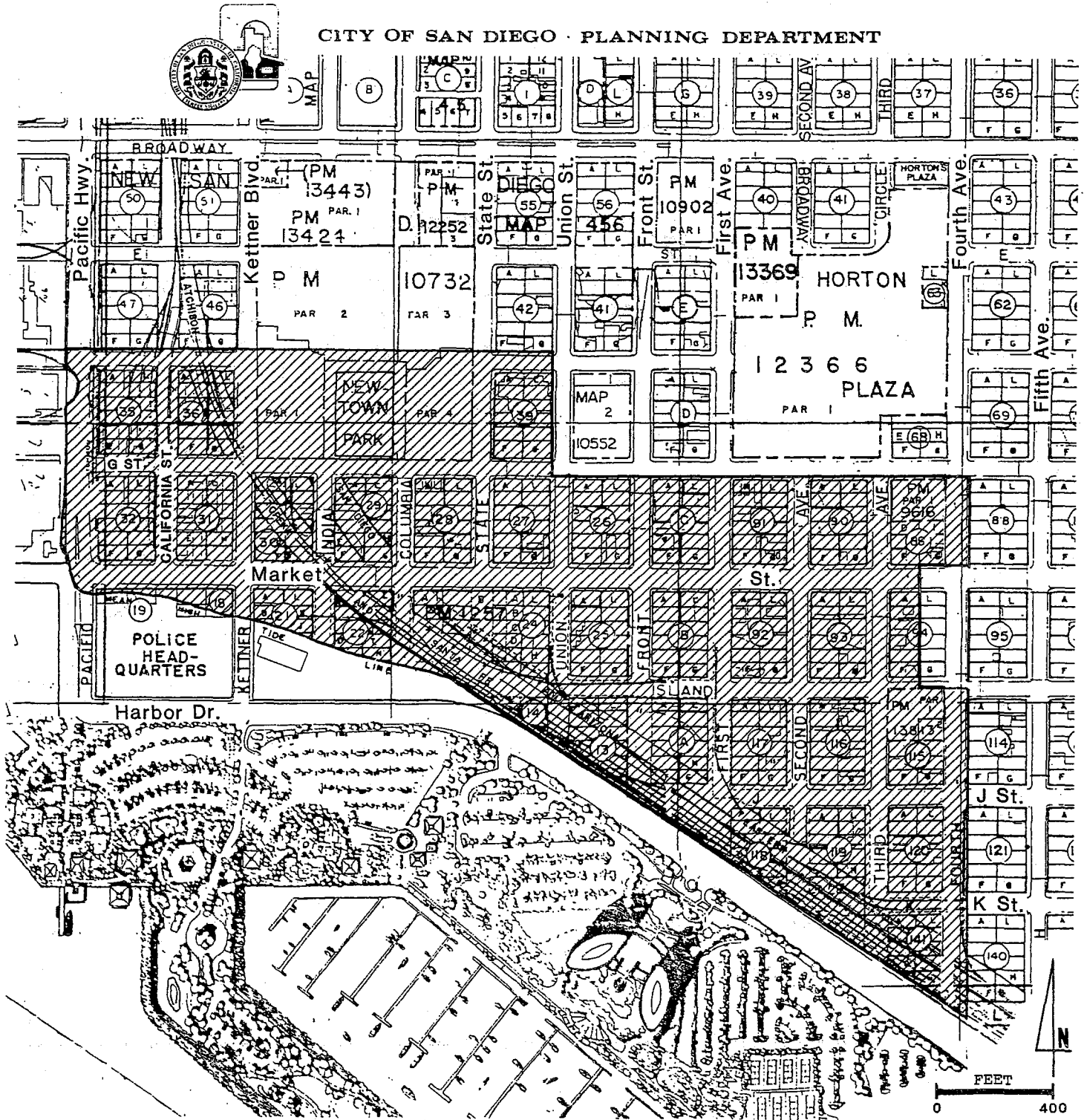
EXHIBIT 3 Height Limit



35



MARINA PLANNED DISTRICT MAP



ACKNOWLEDGEMENTS

**Centre City
Development Corporation**

Pamela M. Hamilton
Executive Vice President

Max Schmidt
Assistant Vice President

Beverly Schroeder
Associate Planner

Judith Riffle
Associate Planner

Sandra Schweinsberg
Graphics

Bruce Ballmer
Agency Special Counsel

City of San Diego

Maureen A. Stapleton
Deputy City Manager/
Deputy Executive Director

Michael Stepner
Assistant Planning Director
City Planning Department

Marianne Munsell
Senior Planner
City Planning Department

Resident Advisory Committee

Murray Galinson, Chair
Cecilia Contini
Charles Loomis
Bill Sauls
Jonathan Segal
Christopher Stewart
Marilyn Yick

Urban Design Team

Hall, Goodhue, Haisley and Barker
Bryan Grunwald, AIA, AICP
Supporting Principal

Sandy and Babcock
James A. Babcock, AIA

Keyser Marston Associates
Kate Earle Funk

Hideo Saski
Landscape Architect